

APPENDICES

Eklutna Native Village Gaming Facility EA

Appendix A

Traffic Impact Analysis Report

Traffic Impact Study Report

Eklutna Native Village Gaming Facility

October 10, 2024

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Executive Summary

This report summarizes the results of the Traffic Impact Study (TIS) conducted for the proposed Eklutna Native Village Gaming Facility development located on Township 12 North, Range 1 West, Section 5, Lot 66, Seward Meridian and Township 15 North, Range 1 West, Section 5, Lots 64 and 67. The project site is also located within the Indian Base and Meridian in the Anchorage B-7 NW United States Geological Survey (USGS) 7.5' quadrangle map. The project will be located on Birchwood Spur Road, on the south side of the Alaska Railroad crossing, within the Municipality of Anchorage, Alaska. The project currently has two site circulation Options that will be analyzed, Option 1, and Option 2. Option 1 will consist of a single parking lot with dedicated inbound and outbound access with circulation. Option 2 will consist of a parking lot with one approach accessible by inbound and outbound access. The different options will not change the projected vehicle trips. Trip Generation will be based on the facility size and will remain consistent for all three site circulation options. The Gaming Facility itself would consist of a single building with a 33,000 square foot gaming floor, restrooms, support space etc., totaling 56,000 square feet. The planned staffing for the facility will be 30 employees per shift for three shifts, so 90 employees per day who will serve a planned 3,000 patrons daily on average.

The purpose of this report is to provide summaries of impacts of the development on traffic in the surrounding area and to provide recommendations on site circulation, consistent with the State of Alaska Department of Transportation requirements.

The following study intersections were selected based on their proximity to the project site and major throughfares in the area, as well as the availability of existing traffic volume data:

1. Birchwood Spur Road and Birchwood Loop Road (One-Way Stop)
2. Birchwood Loop Road and Pilots road (One-Way Stop)
3. Birchwood Loop Road and Glenn Highway (AK1) Southbound on/off ramp (Stop on Off Ramp)
4. Birchwood Loop Road and Glenn Highway (AK1) Northbound on/off ramp (Stop on Off Ramp)
5. Birchwood Loop Road and Old Glenn Highway (One-Way Stop)

Project Trip Generation

Pannone Engineering Services developed estimated project trip generation for the proposed project based on published trip generation rates from the Institute of Transportation Engineers (ITE) publication Trip Generation (11th Edition), Land Use 473 (Casino).

Based on number of gaming stations (1,000 gaming stations).

- Per the ITE the proposed project is expected to generate 8,010 weekday trips and 10,550 Saturday trips.
- The am peak hour volume on a weekday is expected to generate 400 trips, 228 entering and 172 exiting.
- The 4pm-6pm one hour (peak hour of adjacent street traffic) volume on a weekday is expected to generate 460 trips, 235 entering and 225 exiting.
- The pm peak hour volume on a weekday is expected to generate 590 trips, 307 entering and 283

exiting.

- The Saturday peak hour volume is expected to generate 620 trips, 335 entering and 285 exiting.

Existing Conditions

The State of Alaska defaults to USDOT/ Federal Highways Administration on many issues including Level of Service (LOS) designations. The USDOT refers to the most recent addition of the Highway Capacity Manual. During this scenario, all of the study intersections operate within LOS A (Free flow, with low volumes and high speeds.) as defined by the HCM.

The project location is on a road with a functional classification of “Minor Collector” road as defined by ADOT, and published at <https://akdot.maps.arcgis.com/apps/mapviewer/index.html?webmap=8d34059bbfed4fada20a4fdc2a138aca>. It is located approximately 1 mile from the end of the road at a small local airport with no scheduled passenger service, with access additionally to a small residential neighborhood and a shooting range. The traffic passing the development along this segment has an existing Annual Average Daily Traffic (AADT) of 1,140 trips per day. From the project to the south/southeast in the direction of typical access it is approximately 0.5 miles to the intersection of Birchwood Spur Road and N. Birchwood Loop Road. The ‘T’ intersection allows uninterrupted traffic flow from Birchwood Spur Road onto N Birchwood Loop Road, with a stop sign controlled N Birchwood Loop Road continuing to the West (AADT 415). The 45mph speed limit road (AADT 1,456-2,625) follows rolling terrain and curves past stop sign controlled local roads accumulating additional residential traffic for one additional mile until it reaches Pilots Road and the Glenn Highway. Pilots Road (AADT 701) accesses N Birchwood Loop Road from the west at a ‘T’ intersection with a one way stop for Pilots Road, and a left turn lane for northbound N Birchwood Loop Road access. Approximately 280 feet to the southeast of the Pilots Road intersection is the intersection with the southbound Glenn Highway on/off ramp. The southbound Glenn Highway off ramp (AADT 961) is controlled by a stop sign. The southbound Glenn Highway on ramp (AADT 1,610) is accessed by a right turn lane from the southbound N Birchwood Loop Road traffic and by a left turn lane from the northbound N Birchwood Loop Road traffic. An additional 450 feet south on N Birchwood Loop Road (AADT 4,402) is the intersection with the northbound Glenn Highway on/off ramps. The northbound Glenn Highway off ramp (AADT 1,826) is controlled by a stop sign. The northbound Glenn Highway on ramp (AADT 1,438) is accessed from N Birchwood Loop Road through a southbound left turn lane and a northbound right turn lane.

Existing Conditions plus Post Development Project Conditions

During the post-development scenario the trips generated would not meet Service Level D under the ITE volume based on gross floor area (using only gaming area). N Birchwood Loop and associated study intersections would have to be upgraded under this estimate.

During the post-development scenario the trips generated would meet Service Level D on the roadway, based on number of gaming stations.

Site Circulation Options

Option 1: Option 1 will consist of a single parking lot with dedicated inbound and outbound access with circulation. Nearly all traffic accessing the site will access from the south on Birchwood Spur Road. The inbound access would route on a one way access into the site accessed by a right turn from Birchwood Spur Road eastward through the 50' Right-of-Way along the south side of the lot. Outbound traffic would leave the site from a one way access on the west side of the lot, left turning directly onto Birchwood Spur Road.

Option 2: Option 1 will consist of a parking lot with one approach accessible by inbound and outbound access. The access would be located on the west lot line of the development, with access directly onto Birchwood Spur Road.

1.0 Introduction

Project Description

This report summarizes/will summarize the results of the TIS being conducted for the proposed Gaming Facility project located on Birchwood Spur Road, on the south side of the Alaska Railroad crossing, near the Eklutna Native Village, within the Municipality of Anchorage, Alaska. There is one proposed project alternative that consists of a Gaming Facility with a 33,000 square foot gaming floor and support area with a total area of 58,000 square feet. Access will be through Birchwood Spur Road.

This chapter discusses the TIS purpose, project study area, and analysis scenarios. Figure 1 shows the study area, project location, study intersections, and study segments that were analyzed. Figure 2 and Figure 3 show Site circulation options. Figures 6-8 show existing available traffic counts. Figures 9-11 show the existing lane configurations of the study intersections.

Study Purpose

The purpose of this report is to provide summaries of changes in Vehicle Trips and traffic impacts on the surrounding transportation system with the proposed project. The Alaska DOT has not published criteria on impact thresholds for evaluating Vehicle Trips, PES is using Level of Service as defined by the Federal Highway Administration with a threshold of C/D for peak traffic hours.

Study Intersections

PES is evaluating traffic conditions at five study intersections during the a.m. and p.m. peak hours for a typical weekday, as well as the Saturday peak period to account for the “recreational” nature of the project. Data collection efforts included measuring existing traffic counts and utilizing material available through Alaska Department of Transportation.

1. Birchwood Spur Road and Birchwood Loop Road (One-Way Stop)
2. Birchwood Loop Road and Pilots road (One-Way Stop)
3. Birchwood Loop Road and Glenn Highway (AK1) Southbound on/off ramp (Stop on Off Ramp)
4. Birchwood Loop Road and Glenn Highway (AK1) Northbound on/off ramp (Stop on Off Ramp)
5. Birchwood Loop Road and Old Glenn Highway (One-Way Stop)

Study Scenarios

- Existing Conditions – This scenario evaluates the study intersections based on existing traffic volumes, lane geometry and traffic controls.
- Existing conditions plus additional project vehicle traffic – This scenario evaluates the study intersections based on existing traffic volumes with the addition of the proposed project traffic, lane geometry and traffic controls remaining existing.
- Existing Conditions plus Additional Project Vehicle Traffic with Roadway Improvements – This scenario evaluates the study intersections based on existing traffic volumes with the addition of the proposed development traffic and recommended roadway improvements in the study area.

Figure 1: Study area, Project location, Study Intersections, and Study Segments that were analyzed.

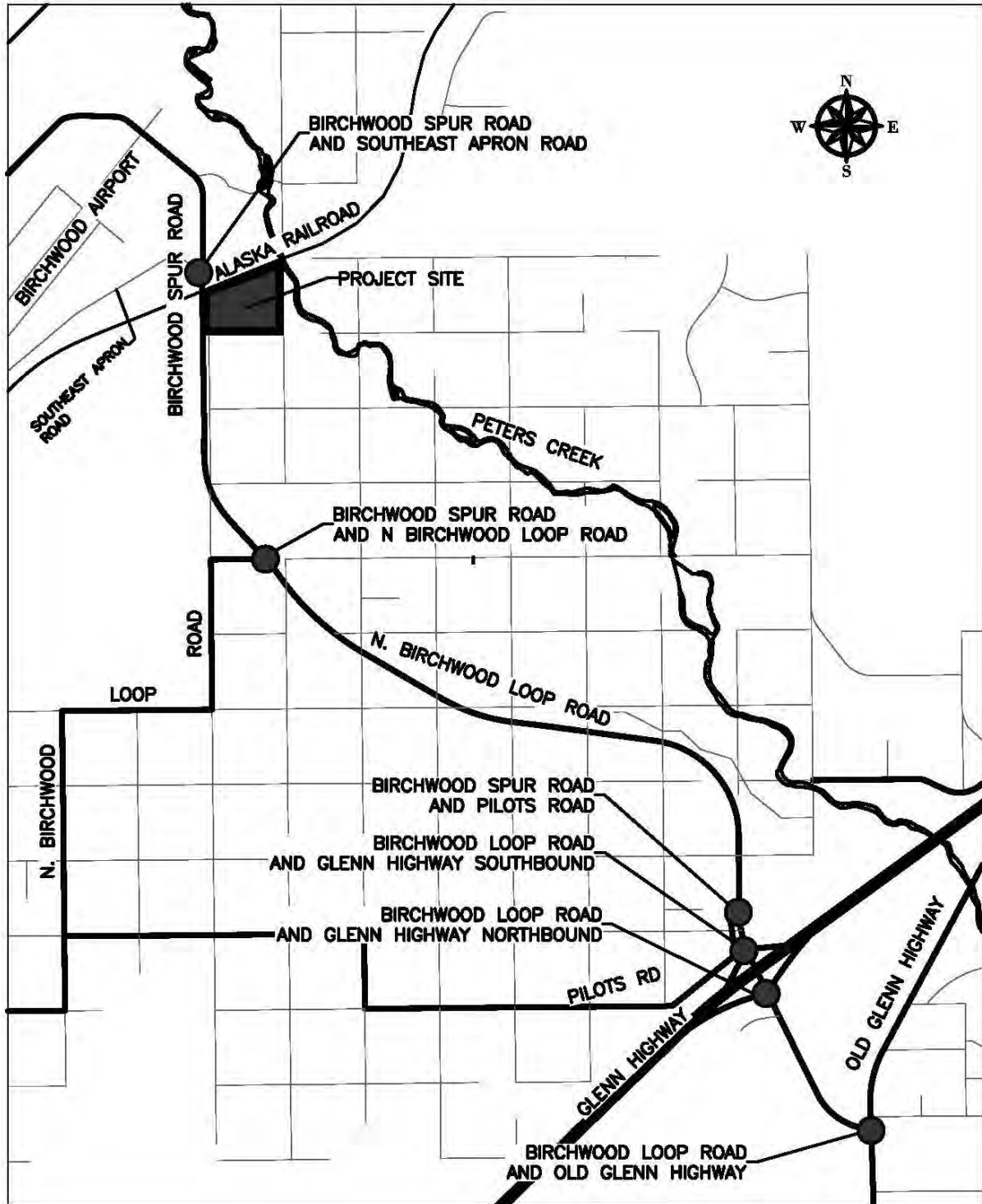


Figure 2: Site Circulation Option 1

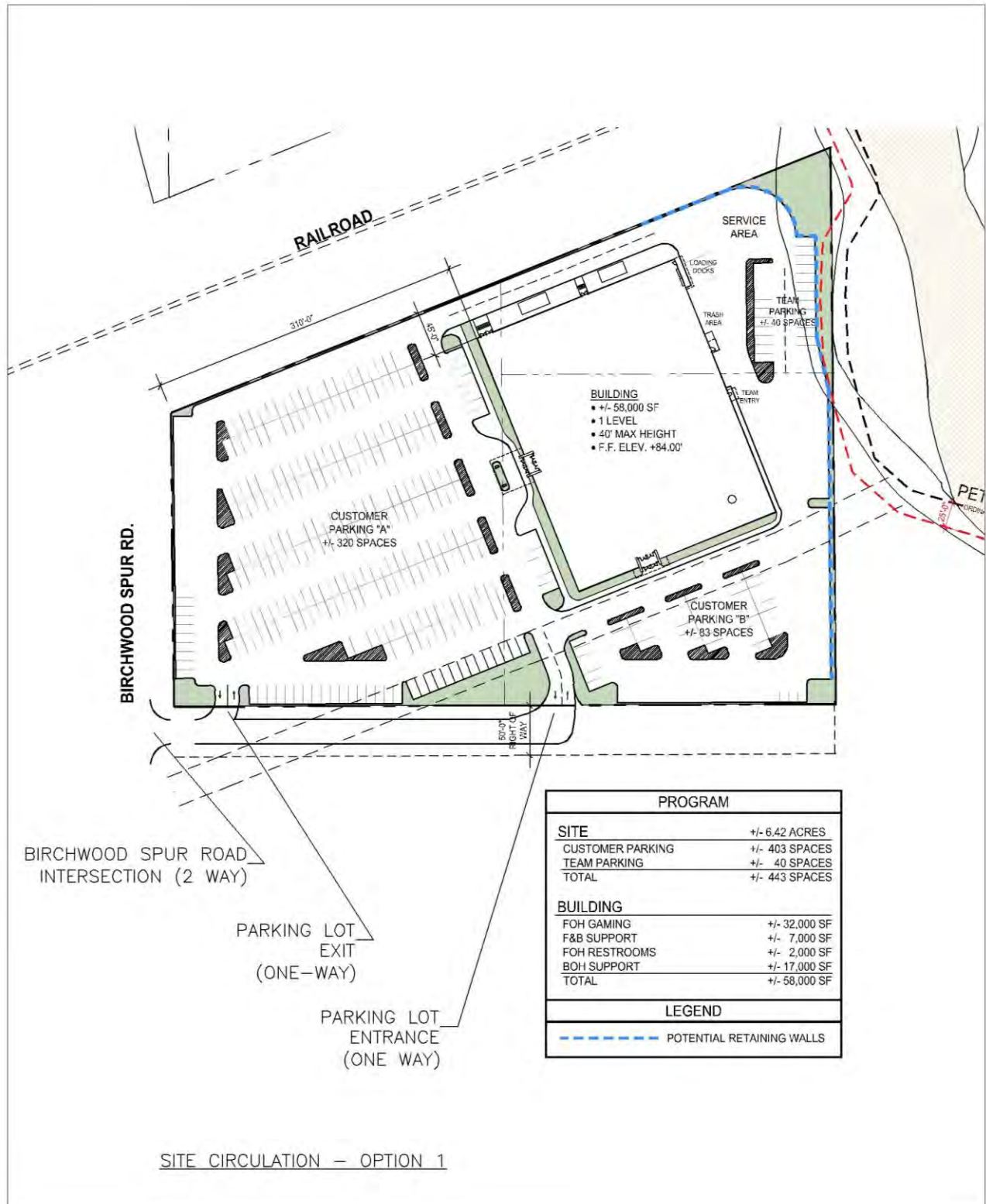
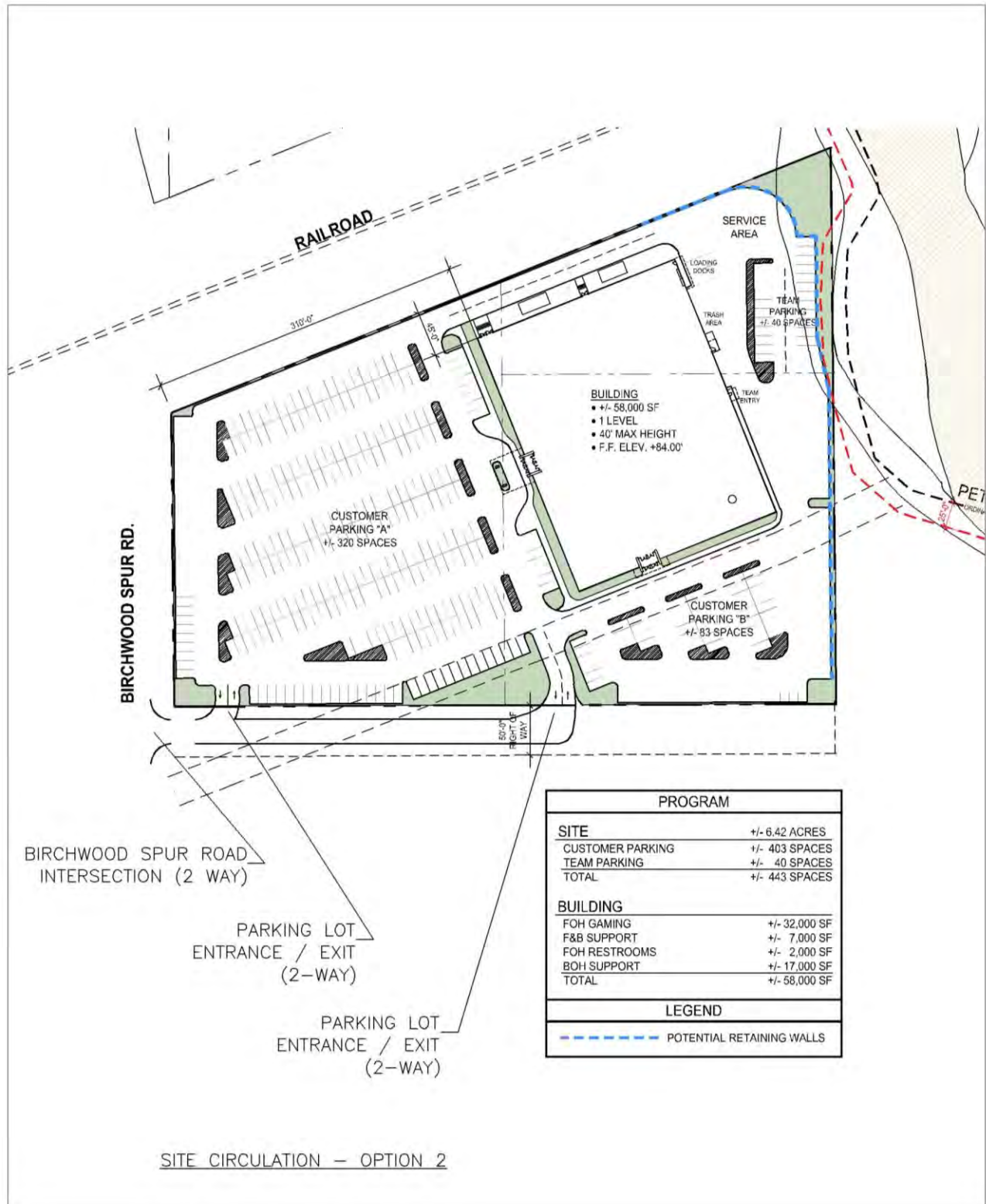


Figure 3: Site Circulation Option 2



2.0 Study Methodology

Traffic impacts related to the proposed project were evaluated for Level of Service based on Average Annual Daily Trips and Peak Hourly Trips for select days and times.

Level of Service

LOS is a quantitative stratification of a performance measure or measures that represent quality of service, measured on an A-F scale, with LOS A representing the best operating conditions from the travelers perspective and LOS F the worst. There are three classes of two-lane highways defined in the USDOT FHA Simplified Highway Capacity Manual. The study area is most closely defined as Class III. A Class III two-lane highway is defined as *“Class III two-lane highways serve moderately developed areas. They may be portions of a Class I or Class II highway that pass through small towns or developed recreational areas. Local traffic often mixes with through traffic on these segments, and the number of unsignalized driveways and cross-streets is noticeably higher than in a purely rural area. Class III highways can include longer roadway segments passing through more spread-out recreational areas, also with increased roadside densities. Such segments are often accompanied by reduced speed limits that reflect the higher activity level.”*

Level of Service Description Percent of Free-Flow Speed for Class III Two-Lane Highways

The LOS on Class III two lane road is based on Percent of Free-Flow Speed (PFFS). PFFS was found by dividing the average travel speed (ATS) by the free flow speed (FFS).

- LOS A PFFS >91.7
- LOS B PFFS >83.3-91.7
- LOS C PFFS >75.0-83.3
- LOS D PFFS >66.7-75.0
- LOS E PFFS <=66.7
- LOS F Demand greater than capacity.

The confining factors on a Class III are lane and shoulder width, access point density, grades, truck traffic, and no passing zones. Free Flow Speed (FFS) measured on N Birchwood Loop through the study section was 40.4mph. The generalized service volume to maintain LOS D per HCM on a Class III two-lane highway is 13,900 trips per day through rolling terrain under optimal conditions. The capacity in either direction is 1,700 vehicles per hour (vph), maintaining service level D. The PFFS at the existing and existing plus project conditions flow was found to be 99+ from the project site to just northwest of the Glenn Highway interchange. All two-lane segments will maintain LOS A.

Unsignalized Intersections

There are 13 minor unsignalized intersections between the project site and the Glenn Highway On/Off ramps. The intersections were each analyzed using Turn Lane Warrant tables found in the “1967 Highway Record 211 (Highway Research Board)” and the 1985 “National Cooperative Research Program Report 279”, both used in the development of the HCM. The tables were developed by “consensus of traffic engineers, based on the study of existing intersections” and output maximum volumes in vehicles per hour (vph) that can turn without increasing the probability of bringing an arriving through vehicle to

a stop at a rate more than 0.020 at the design speed of 50 mph. See figures 4 and 5. Additionally, all intersections were analyzed for intersection capacity with CAPX “Capacity Analysis for Planning of Junctions” software provided by U.S. Department of Transportation Federal Highway Administration.

Level of Service Description Volume-to-Capacity Ratio for Unsignalized Intersections

Level of Service is determined with: $S = sf / (1 + a(v/c)^b)$

S: Level of Service

Sf: Free Flow Speed

V: Volume

C: Capacity

a: 0.15

b: 4

LOS A S=0.0 to 0.2

LOS B S=0.2 to 0.4

LOS C S=0.4 to 0.7

LOS D S=0.7 to 0.8

LOS E S=0.8 to 1.0

LOS F S>1

Source: Highway Capacity Manual 6th Ed., Chapter 20 (Transportation Research Board, 2010)

Figure 4: Right Turn Warrant Table

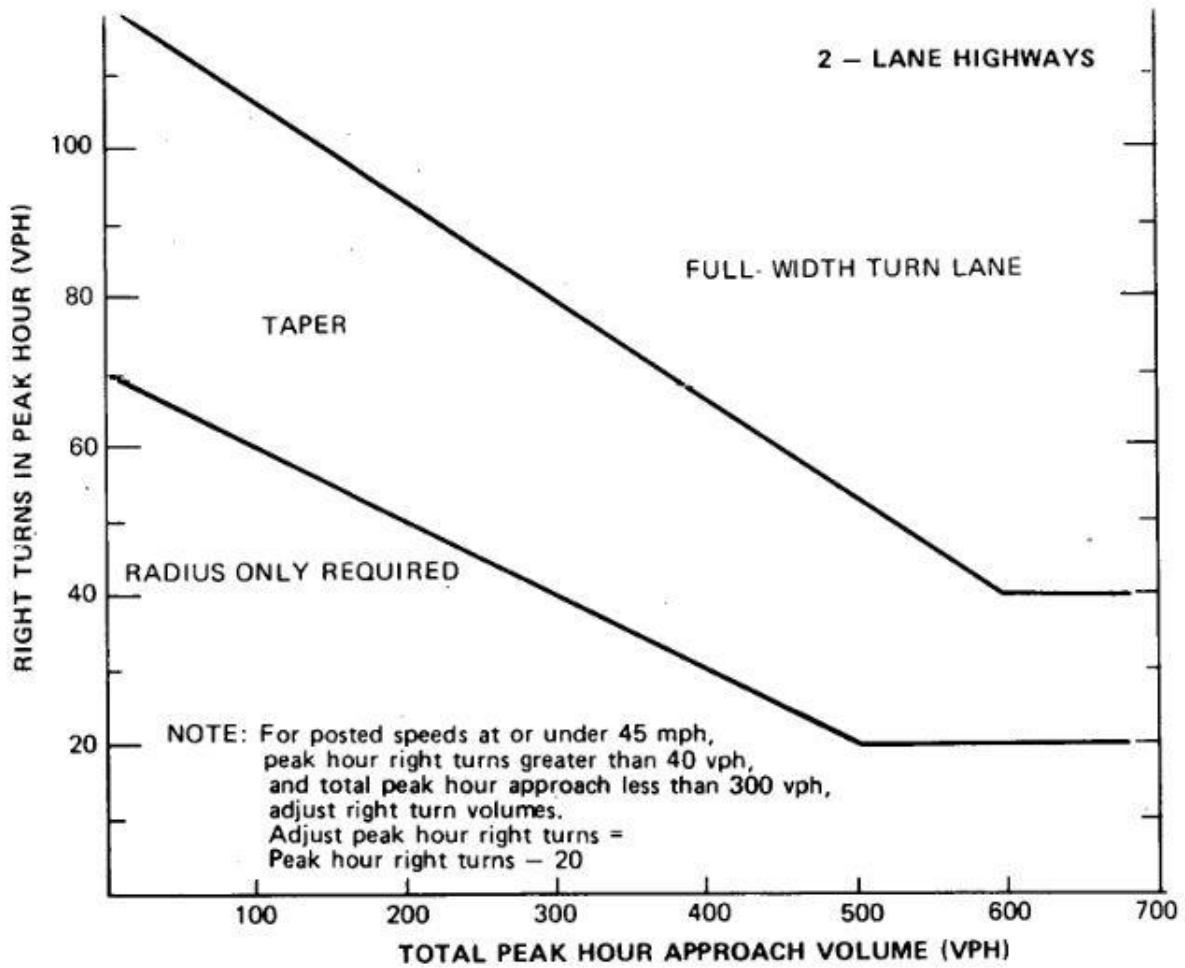


Figure 5: Left Turn Warrant Tables

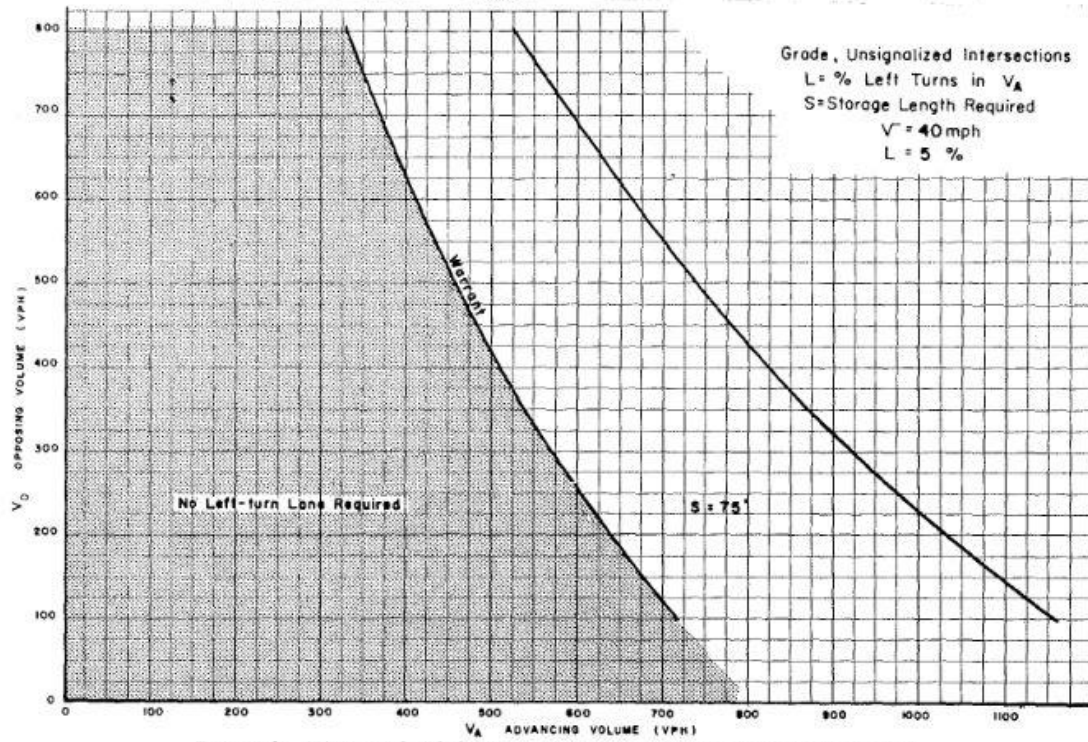


Figure 2. Warrant for left-turn storage lanes on two-lane highways.

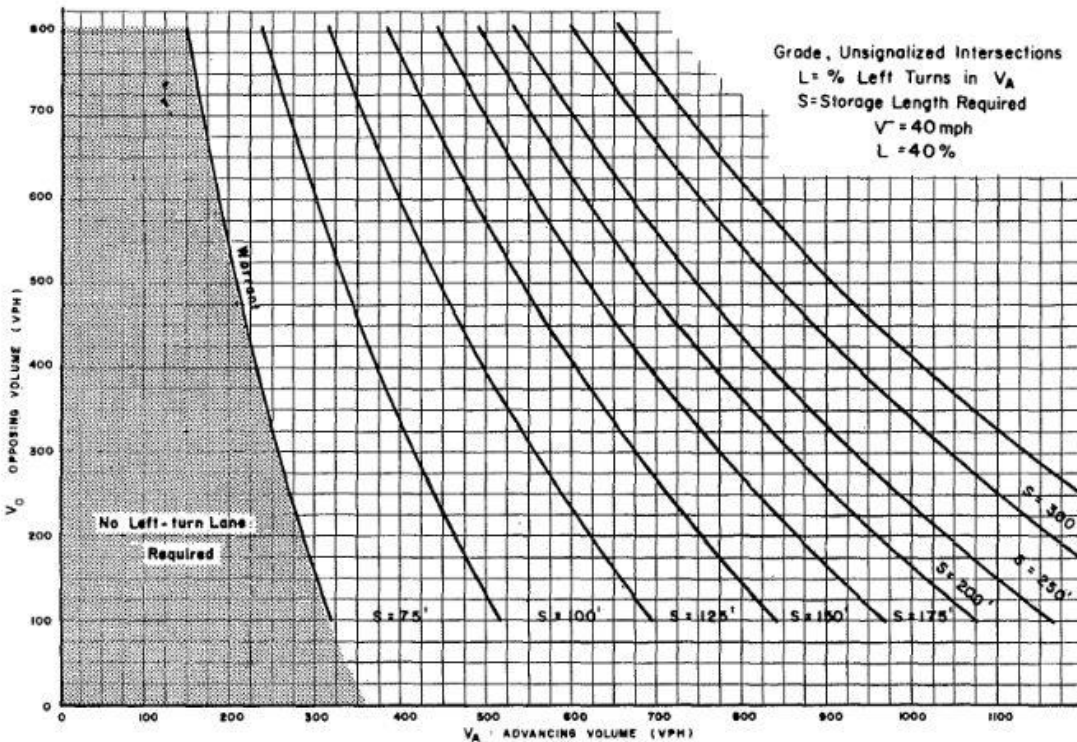


Figure 7. Warrant for left-turn storage lanes on two-lane highways.

3.0 Existing Conditions

This Section describes existing traffic volumes and operating conditions at the study intersections, including the results of the LOS calculations.

Existing Traffic Conditions

PES evaluated existing traffic conditions at selected study intersections and study segments during the a.m. and p.m. peak hours on a typical weekday, and during the peak hours on a typical Saturday. Intersection turning movement counts were estimated based on measured volumes on each roadway. Alaska DOT record average annual daily traffic (AADT) counts available for the year 2020 were used in the initial version of this report for each study segment, and updated with AADT counts from Alaska DOT collected in the year 2012, and AADT collected by PES from August 23, 2024 to September 20, 2024. The AADT counts are shown on Figures 6-8. Peak hour traffic counts from 2024 are shown on figures 9-11. Additional traffic volumes are available at <https://akdot.maps.arcgis.com/home/webmap/viewer.html?webmap=7c1e1029fdb64d7a86449d55ef05e21c> dating back annually to 2012. The existing traffic volume has been stable for a number of years.

Existing Alaska Railroad Crossing

The existing railroad crossing adjacent to the site is owned/ operated by a low volume railroad. The railroad does not exceed one train per hour and during the off-peak season may only see one or two trains per day. The effect on traffic will be insignificant.

Figure 6: Average Annual Daily Traffic Volumes (Alaska DOT Record Data 2012)

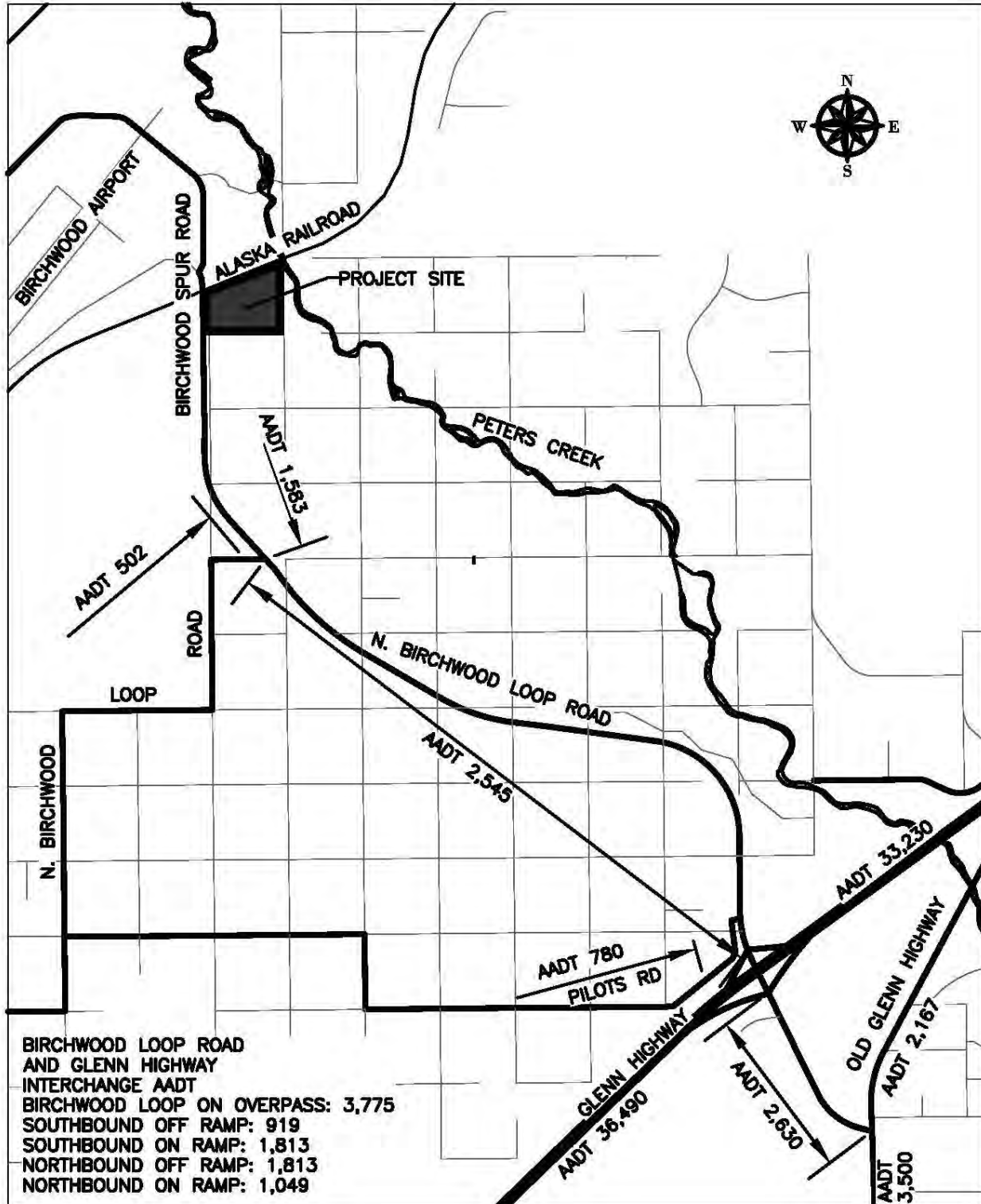


Figure 7: Average Annual Daily Traffic Volumes (Alaska DOT Record Data 2020)

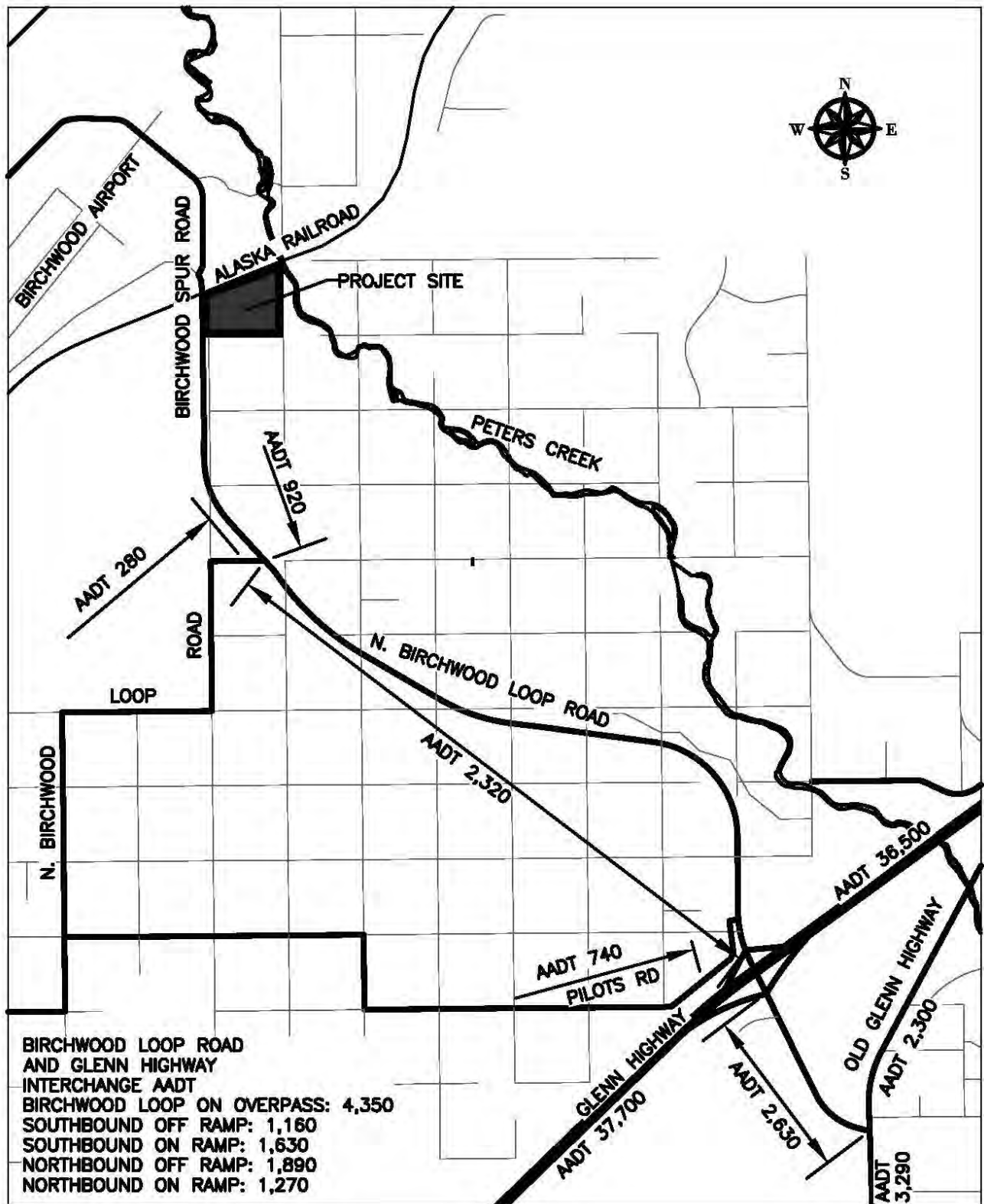


Figure 8: Average Annual Daily Traffic Volumes (August 23 to September 20, 2024)

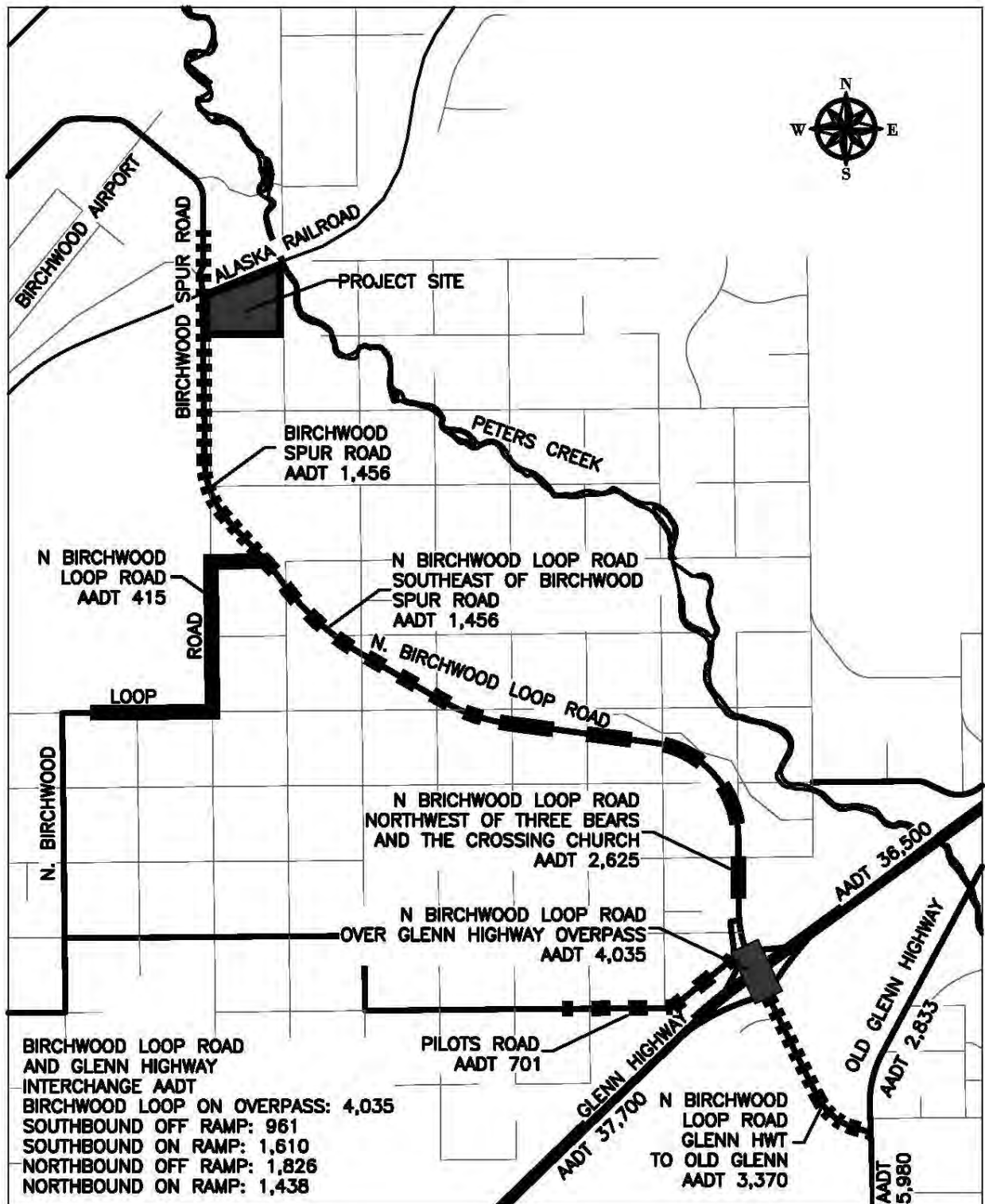


Figure 9: Project Lane Geometry Existing Conditions

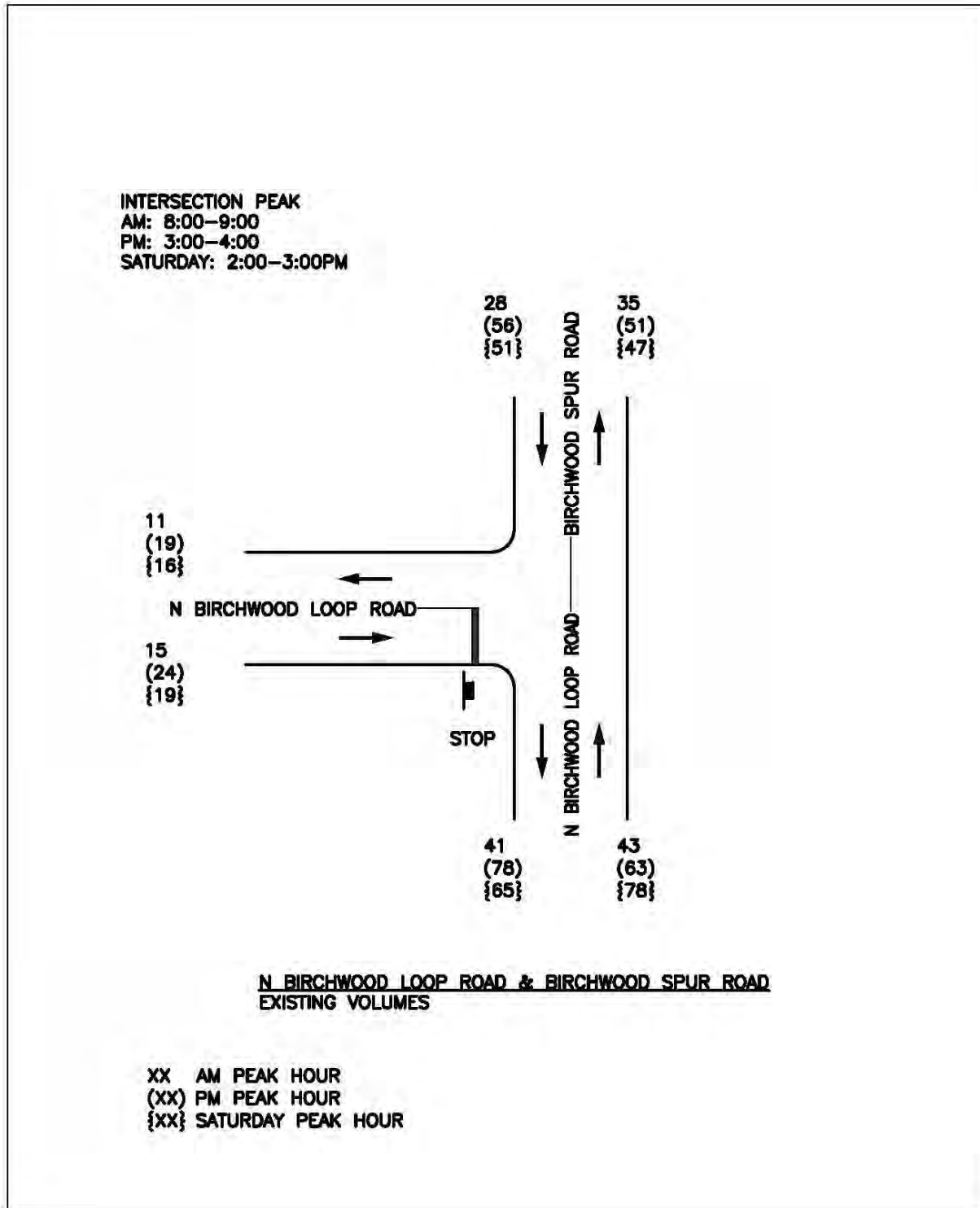


Figure 10: Project Lane Geometry Existing Conditions

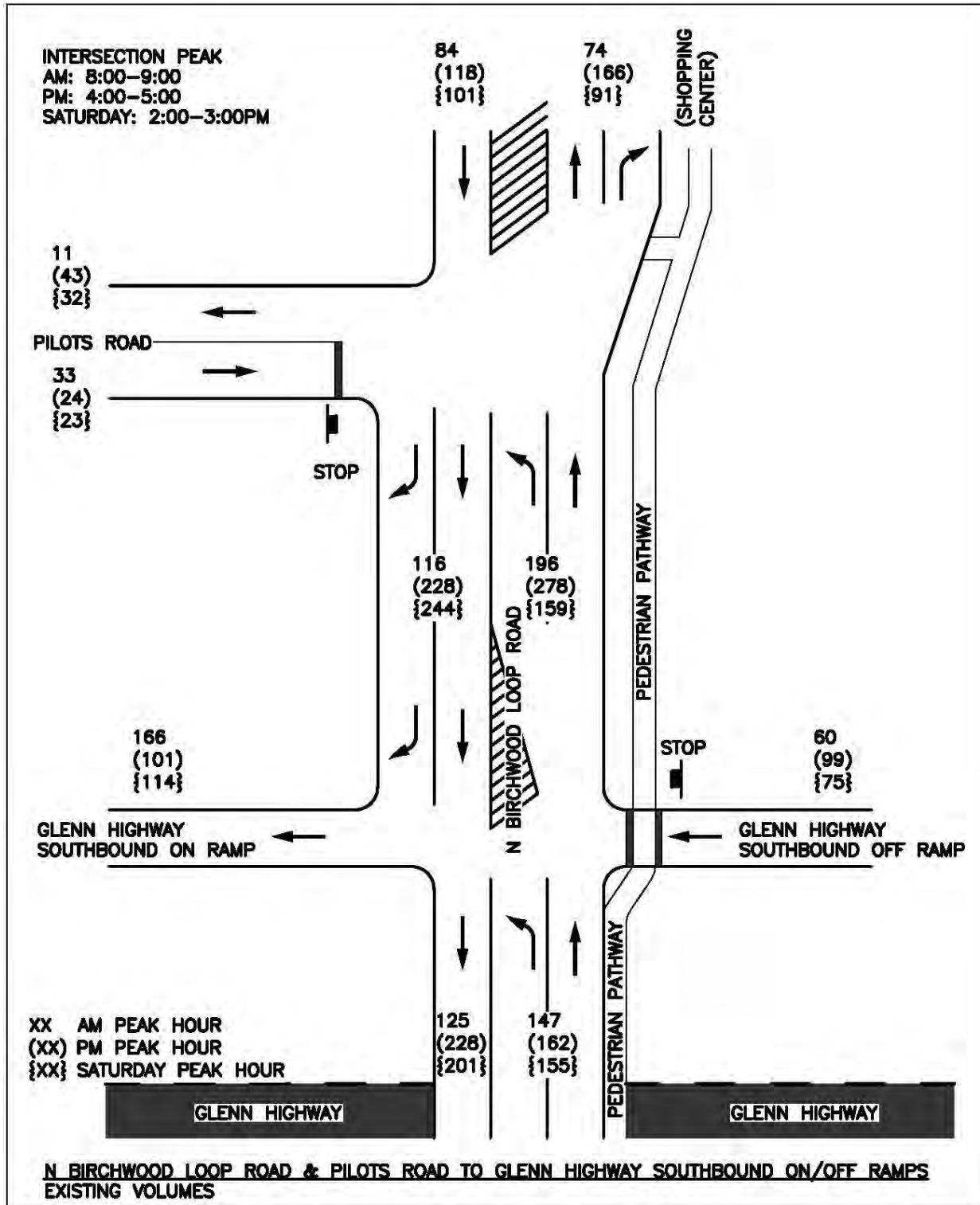
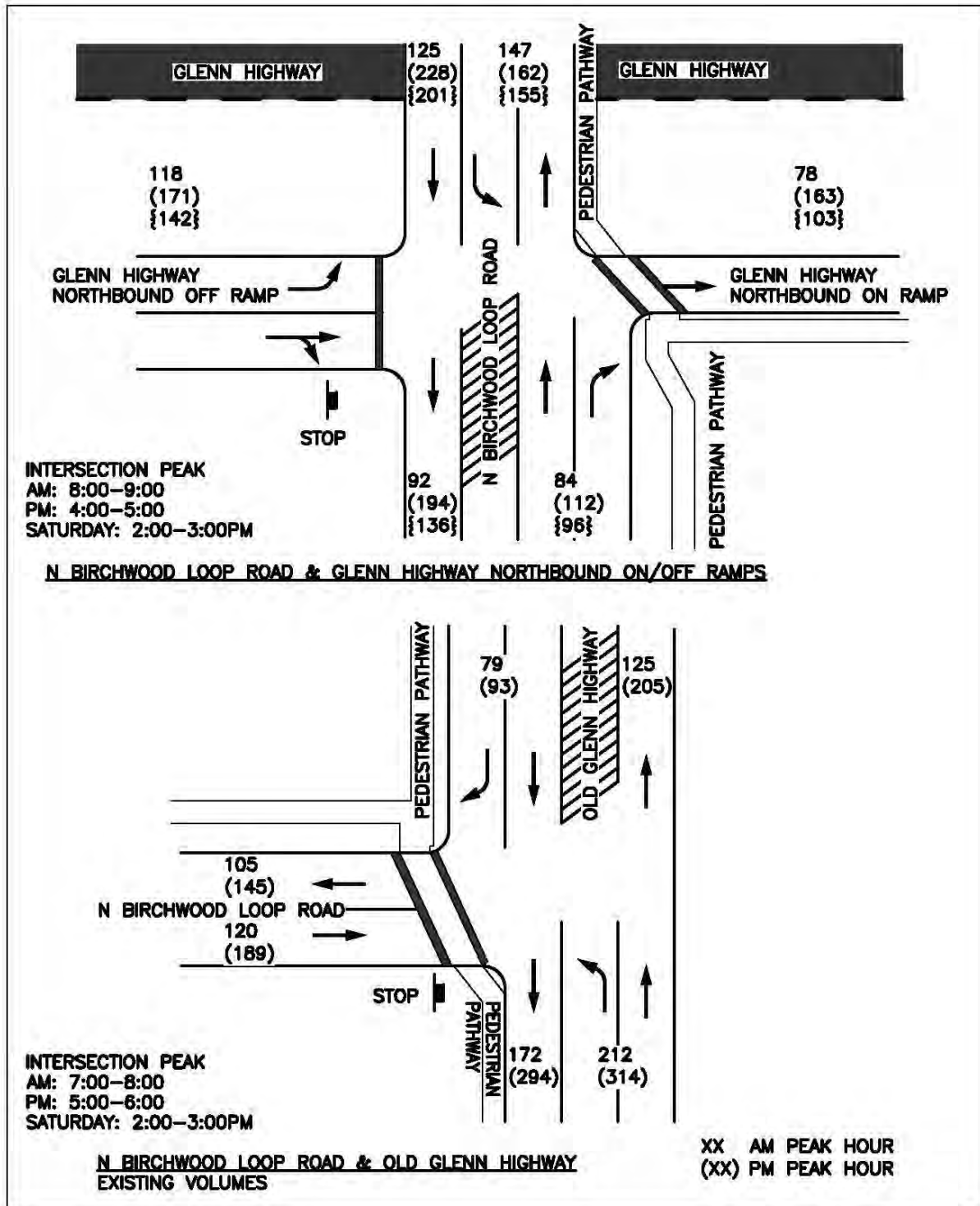


Figure 11: Project Lane Geometry Existing Conditions



Intersection Level of Service Analysis – Existing Conditions

This scenario evaluates the study intersections based on adjacent existing traffic volumes, and existing lane geometry and traffic controls, as described above. Peak hour traffic was observed through the study intersections from August 23, 2024 to September 20, 2024. Turning volumes are in the range of 5% or less of total intersection volumes at all minor intersections (those north and west of the highway interchange). Based on the generally light traffic in the area all study intersections currently operate within LOS A. The intersections are all low volume with peak hour right turns and left turns below the right turn lane and left turn lane warrant level with an advancing and opposing vehicle rate of 450vph and a turning volume of <5%.

The highway interchange with the Glenn Highway was analyzed using the same tables. A left turn lane with advancing and opposing traffic at a volume of 450vph each, and a turning volume of 40% of vehicles well require a turn lane with a queueing lane of 100 feet. The existing queueing lanes range in length from 100 feet to 185 feet. While the warrant tables above are a good indicator of capacity the intersections were also analyzed per the HCM below.

The PFFS on each segment of the study area was 99+ equating to LOS A.

The busiest intersection in the study area was analyzed with existing conditions in CAPX and found to have a Volume-to-Capacity Ratio (VC) of 0.0 to 0.1, and a LOS of A. The queueing length was calculated to by PES with the equation below (Section 4.0) and was found to be less than 1 vehicle at the peak hours. See Figures 12-13 for CAPX output for the Glenn Highway Southbound Ramps and N Birchwood Loop intersection.

Volume to Capacity Ratios and Level of Service:

Birchwood Spur Road and Birchwood Loop Road	VC: ≤ 0.1	LOS: A
Birchwood Loop Road and Pilots road	VC: ≤ 0.1	LOS: A
Birchwood Loop Road and Glenn Highway (AK1) Southbound on/off ramp	VC: ≤ 0.1	LOS: A
Birchwood Loop Road and Glenn Highway (AK1) Northbound on/off ramp	VC: ≤ 0.1	LOS A
Birchwood Loop Road and Old Glenn Highway	VC: ≤ 0.1	LOS A

Figure 12: Glenn Highway Southbound Ramps and N Birchwood Loop Intersection CAPX output Existing Conditions

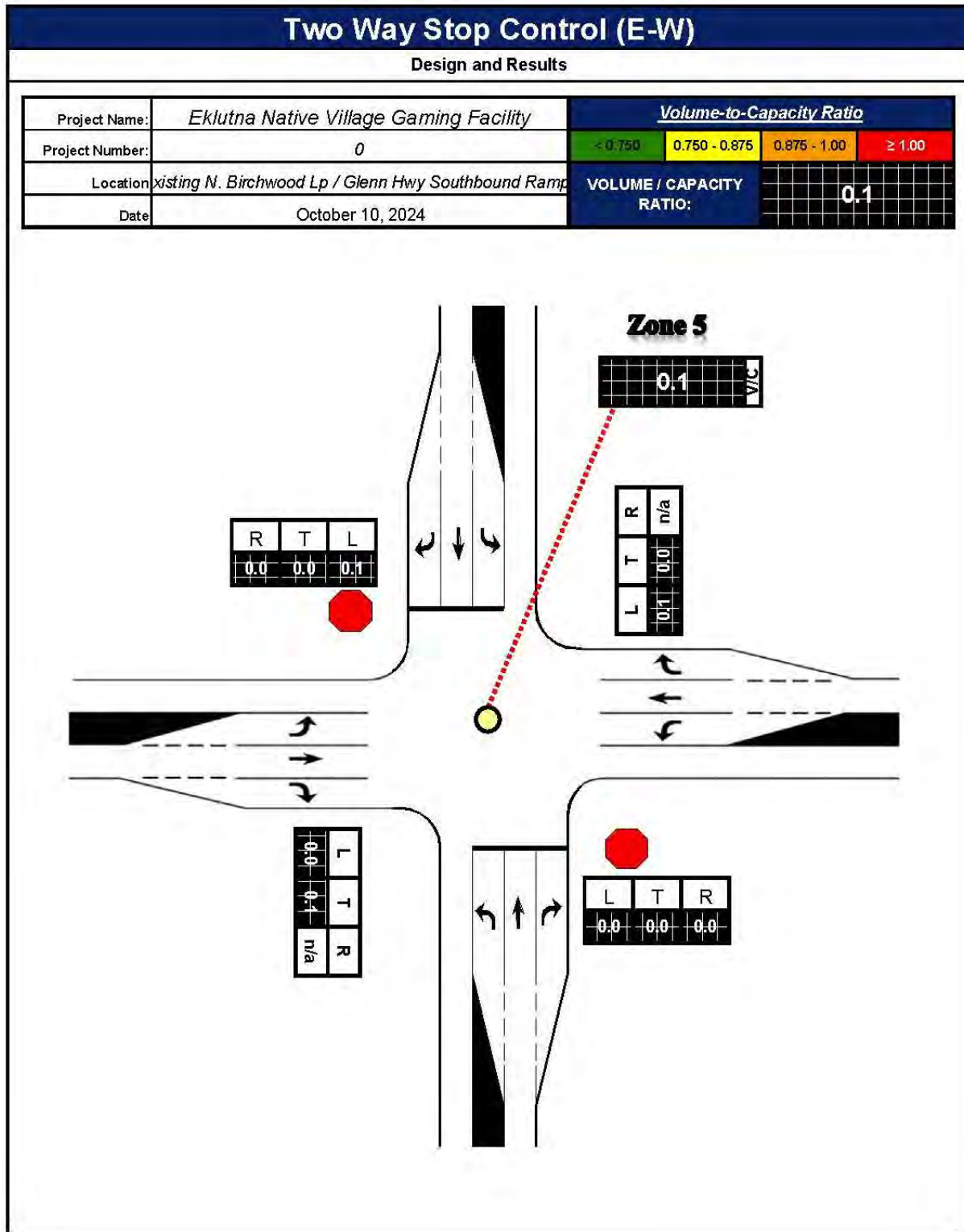
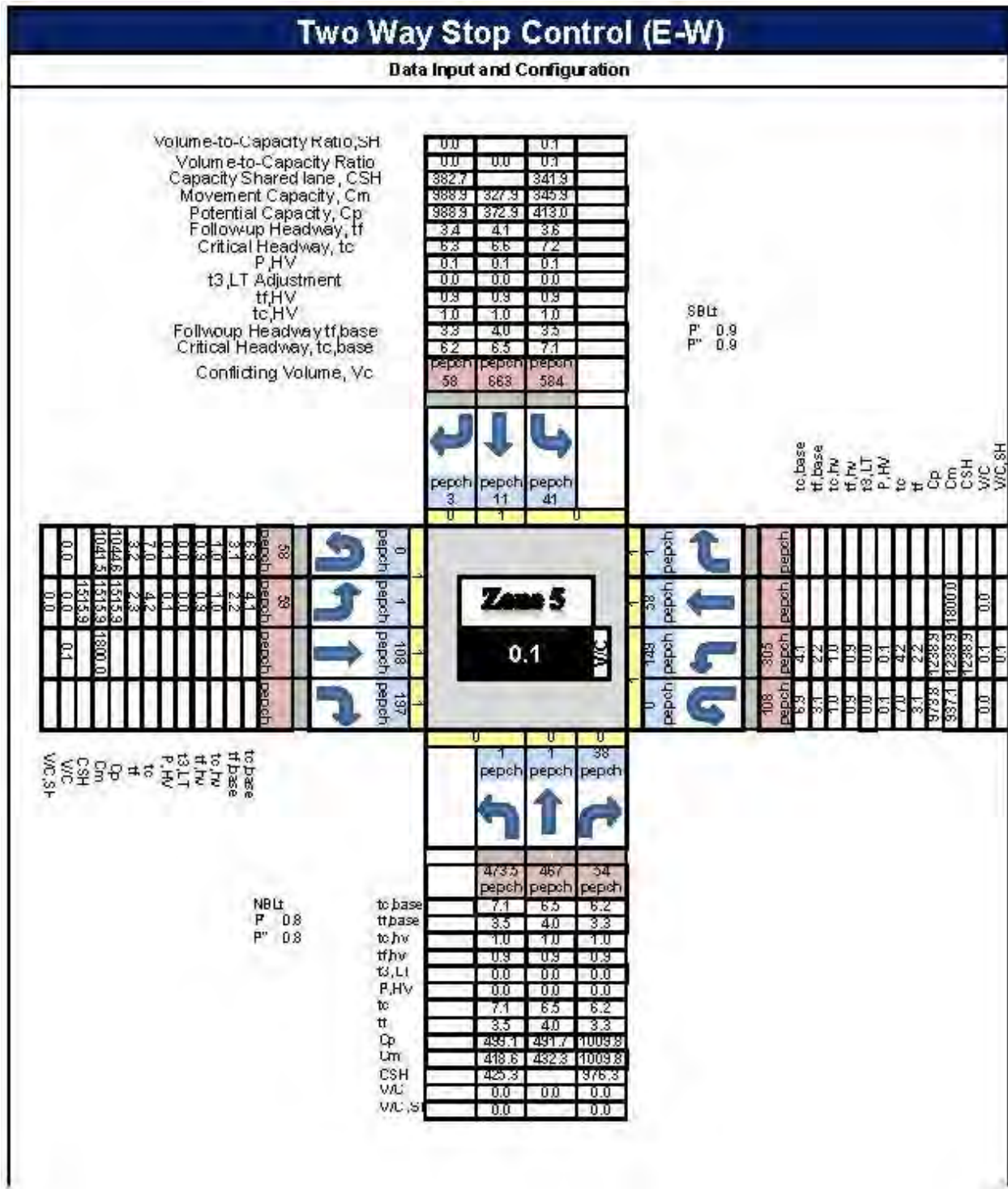


Figure 13: Glenn Highway Southbound Ramps and N Birchwood Loop Intersection CAPX output Existing Conditions



4.0 Existing Conditions Plus Project Conditions

This analysis scenario presents the impacts of the proposed project at the study intersections and surrounding roadway system. This scenario evaluates existing conditions with the addition of traffic from the proposed project. The proposed project will develop a currently vacant lot and construct a Gaming Facility with 33,000 square feet of gaming area and 1,000 gaming positions.

Project Trip Generation

Pannone Engineering Services developed estimated project trip generation for the proposed project based on a combination of published trip generation rates from the Institute of Transportation Engineers (ITE) publication Trip Generation (11th Edition), Land Use 473 (Casino).

Based on number of gaming stations (1,000 gaming stations) per the ITE the proposed project is expected to generate-

- 8,010 weekday trips
- 10,550 Saturday trips
- The am peak hour volume on a weekday is expected to generate 400 trips, 228 entering and 172 exiting.
- The 4pm-6pm one hour volume on a weekday is expected to generate 460 trips, 235 entering and 225 exiting
- The pm peak hour volume on a weekday is expected to generate 590 trips, 307 entering and 283 exiting.
- The Saturday peak hour volume is expected to generate 620 trips, 335 entering and 285 exiting.

Project Trip Distribution and Assignment

Trip distribution is a process that determines in what proportion vehicles would be expected to travel between the project site and various destinations outside the project study area. Assignment determines the various routes that vehicles would take from the project site to each destination using the calculated trip distribution. Trip distribution assumptions for the proposed development project were developed based on the distribution of the population served by the development. The site is served by one major throughfare with a local population small enough to be considered negligible. The distribution assumption is as follows:

- 40 percent to/from Glenn Highway (AK1) to the North
- 60 percent to/from Glenn Highway (AK1) to the South

Intersection Level of Service Analysis – Existing Plus Project Conditions

The study intersections were analyzed for level of service, turn lane warrant, and queueing length. The intersection LOS analysis results for Existing plus Project Conditions are shown for Saturday Peak Hour, the busiest time on the route. Each intersection was analyzed and assigned a volume/capacity ratio. Turn lane warrant was analyzed for each intersection and lane queueing calculations were performed for turn lanes. Traffic volumes collected in 2024 plus trips generated above were used for analysis. PES used CAPX to generate a volume/capacity ratio and a maximum capacity for each intersection.

For required queueing length the following equation was used per the HCM.

$$Q_{95} \sim \left[\frac{V_x}{C_{mx}} - 1 + \sqrt{\left(\frac{V_x}{C_{mx}} - 1 \right)^2 + \frac{\left(\frac{3600}{C_{mx}} \right) \left(\frac{V_x}{C_{mx}} \right)}{150T}} \right] \left(\frac{C_{mx}}{3,600} \right)$$

Where

Q_{95} = 95th percentile queue (veh),

V_x = flow rate for movement x (veh/h),

C_{mx} = capacity of movement X (veh/h), and

T = analysis time period (0.25h for a 15-min period) (h).

See Figures 14-16 for 2024 traffic counts plus project conditions at peak hours.

The left turn lane from the Northbound Glenn Highway Off Ramp onto N Birchwood Loop has a 4 car queue under the existing plus project conditions Saturday Peak Hour Volume. The existing left turn lane has a length of 130 feet, and will accommodate this. The Southbound Glenn Highway Off Ramp has a combined left-through-right lane and a queueing length of 2 cars. No other existing turn lane in the study area had a queueing length of greater than one vehicle with added project conditions. None of the intersections without existing turn lanes had a queueing length. See intersection analysis CAPX output figures 17-26.

Volume to Capacity Ratios and Level of Service:

Birchwood Spur Road and Birchwood Loop Road Saturday Peak Hour	VC: 0.2 LOS: A	
Birchwood Loop Road and Pilots road Saturday Peak Hour	VC: 0.3 LOS: B	
Birchwood Loop Road and Glenn Highway (AK1) Southbound on/off ramp Saturday Peak Hour	VC: 0.4 LOS: B	*SB L-T-R lane Queue 2 veh.
Birchwood Loop Road and Glenn Highway (AK1) Northbound on/off ramp Saturday Peak Hour	VC: 0.7 LOS C/D	*NB Left Turn Queue 5 veh.
Birchwood Loop Road and Old Glenn Highway Saturday Peak Hour	VC: 0.2 LOS	

Figure 14: Project Lane Geometry Existing Conditions Plus Project Conditions

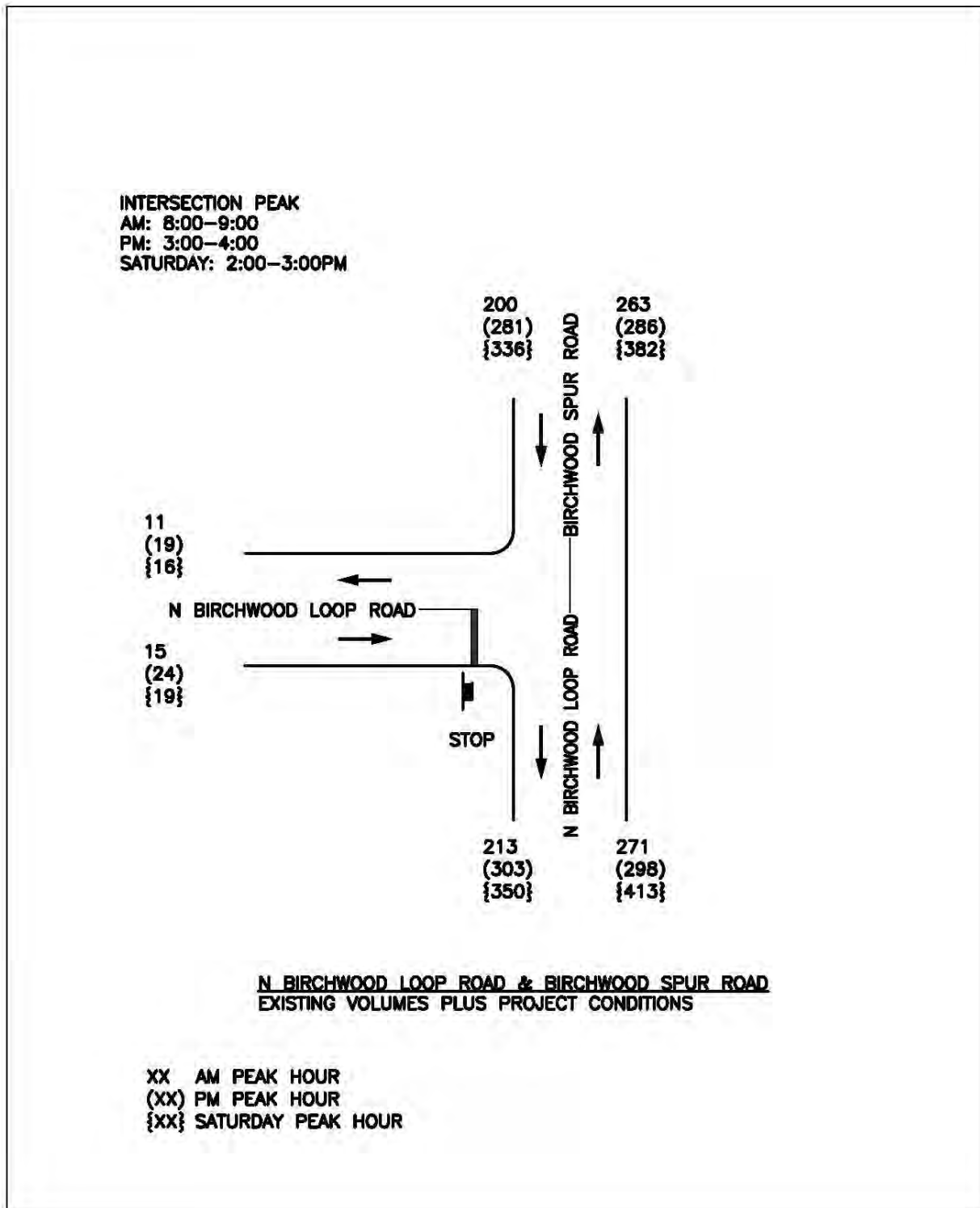


Figure 15: Project Lane Geometry Existing Conditions Plus Project Conditions

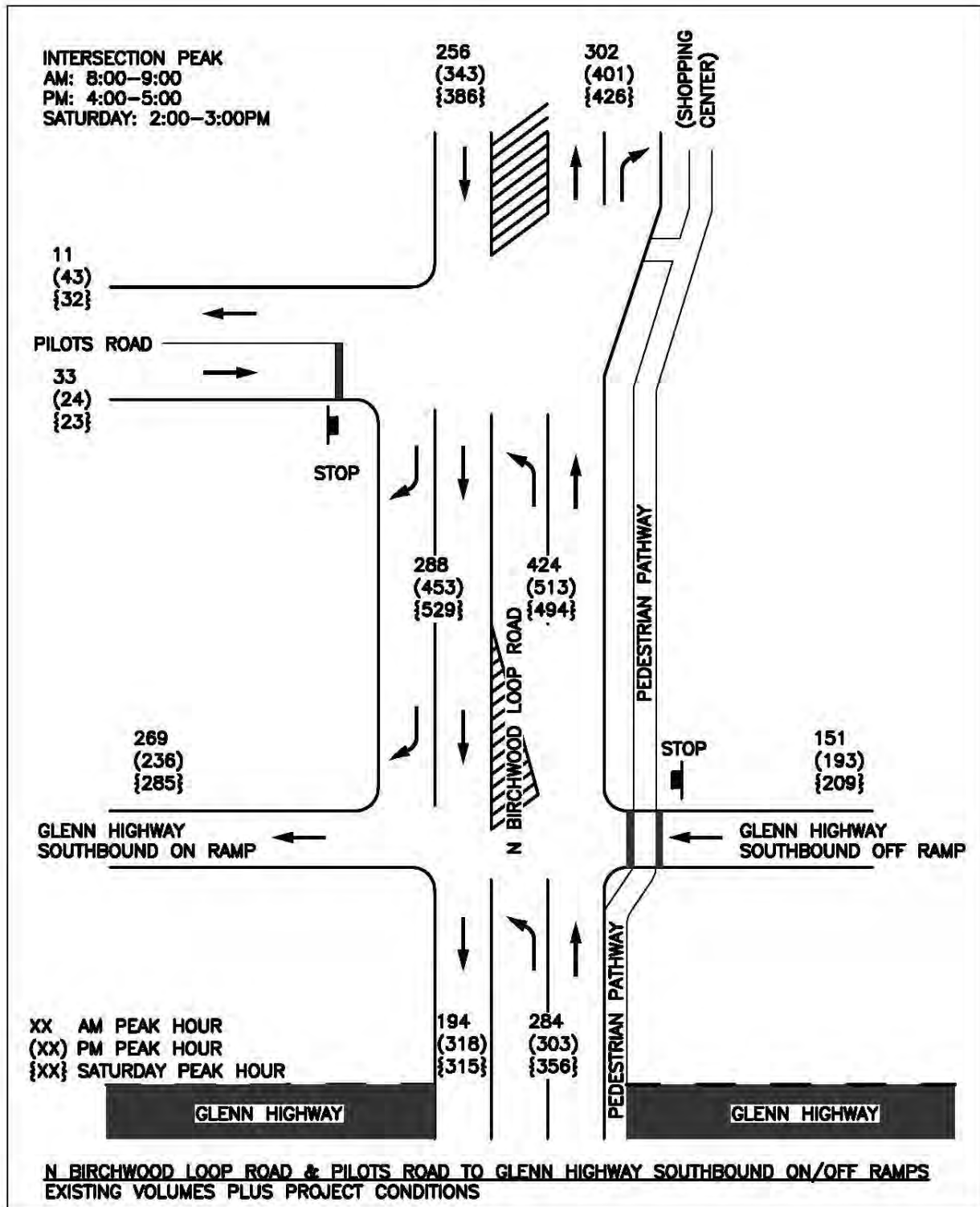


Figure 16: Project Lane Geometry Existing Conditions Plus Project Conditions

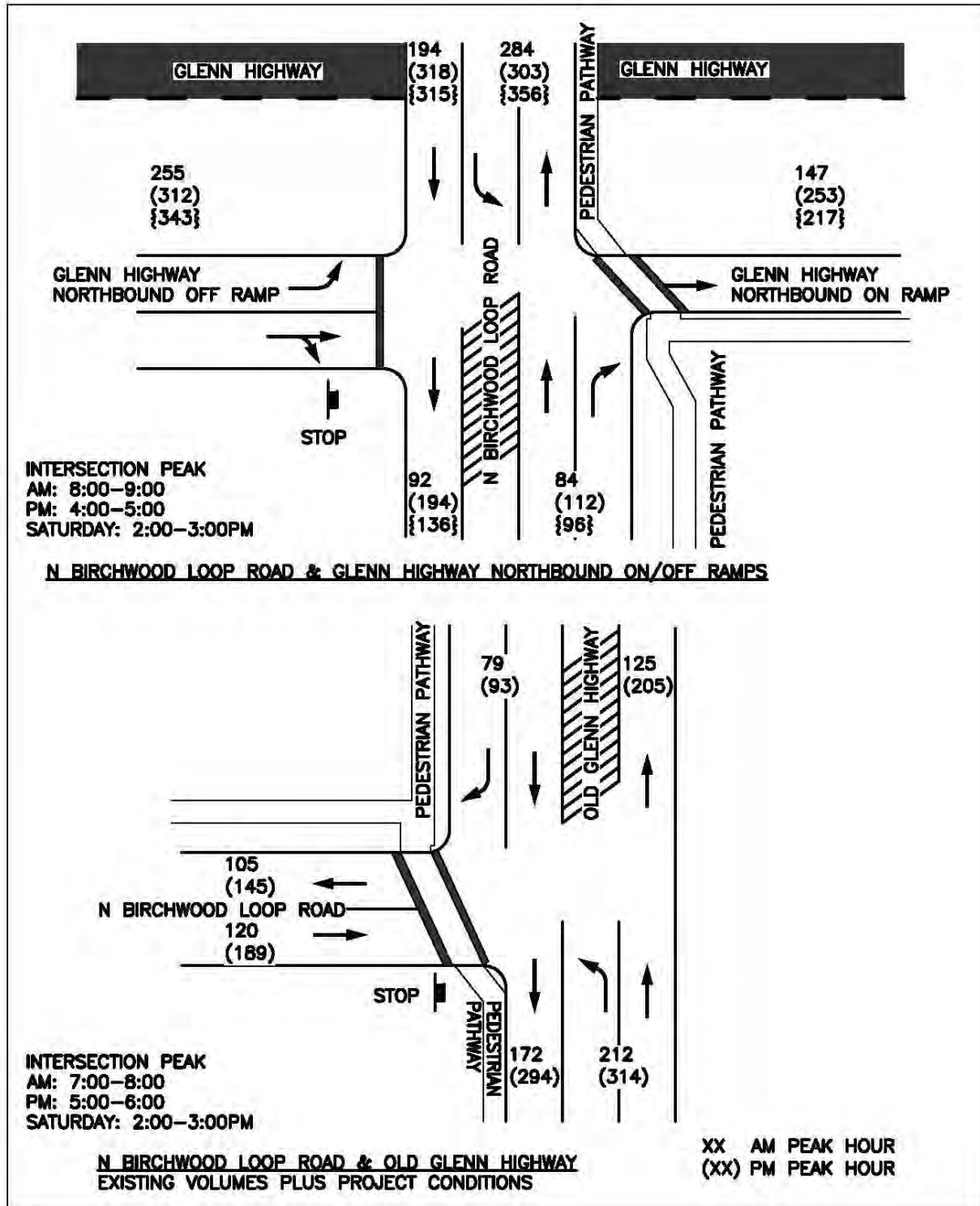


Figure 17: N Birchwood Loop and Birchwood Spur Road Intersection CAPX output Existing Plus Project Conditions

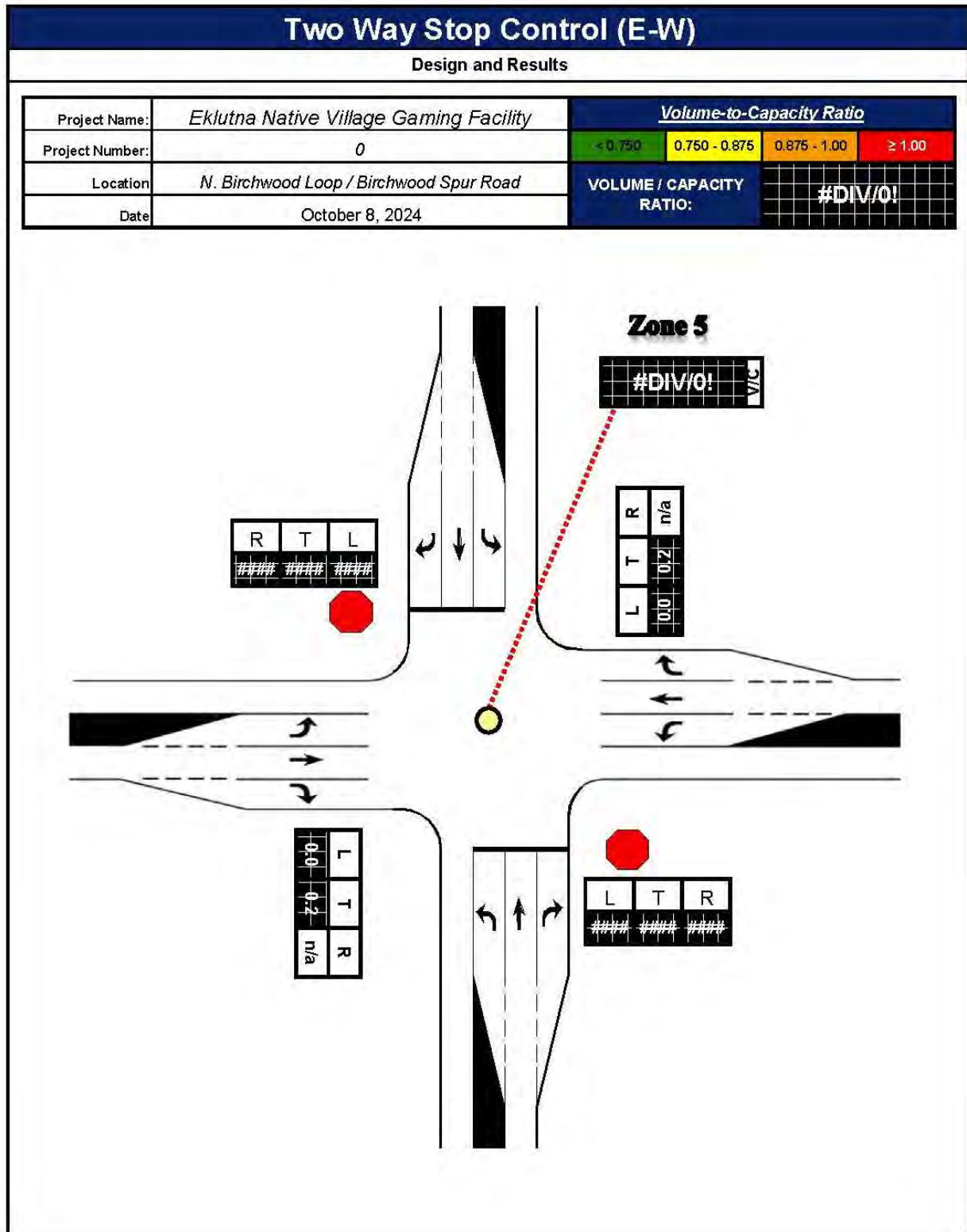


Figure 18: N Birchwood Loop and Birchwood Spur Road Intersection CAPX output Existing Plus Project Conditions

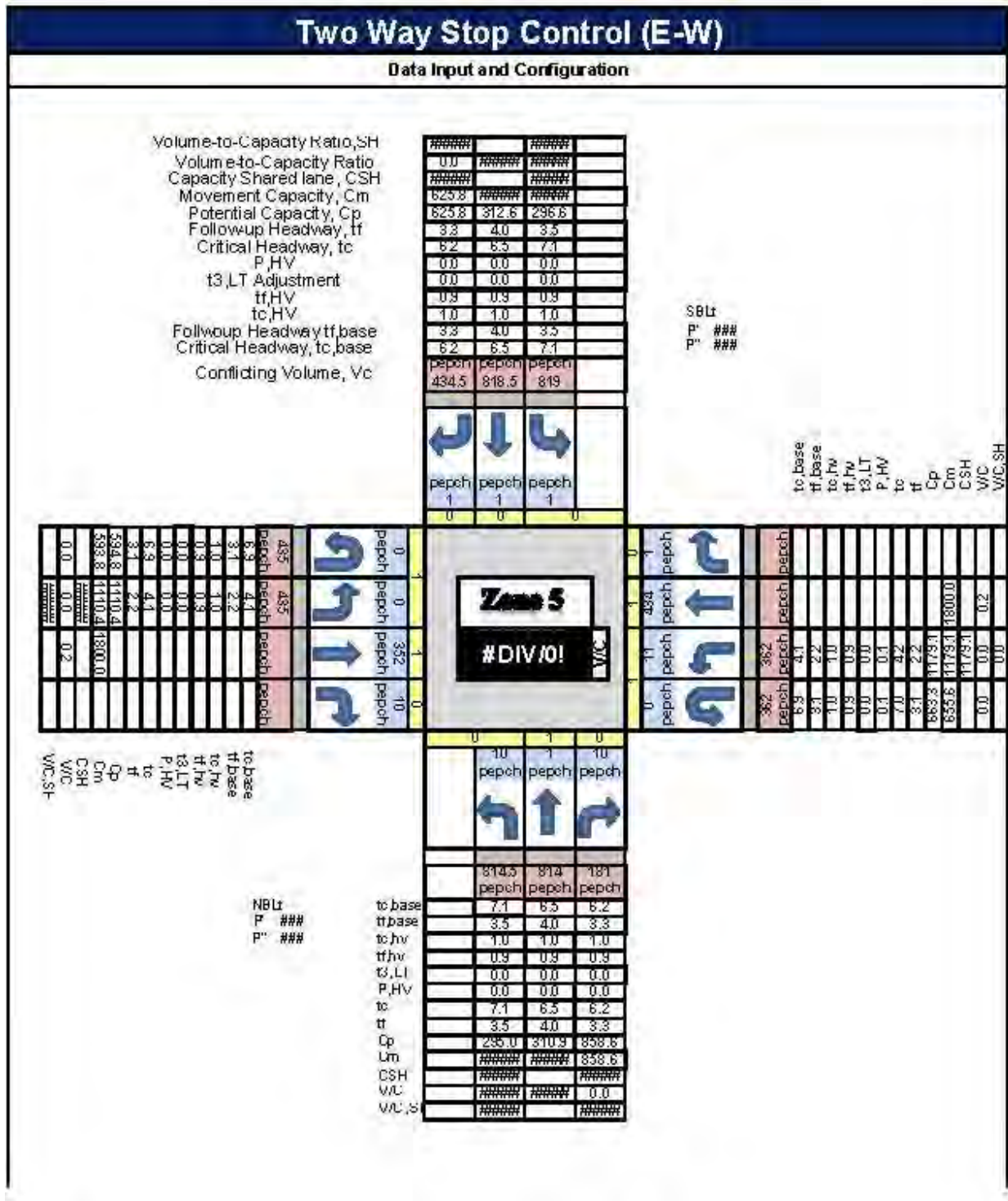


Figure 19: N Birchwood Loop and Pilots Road Intersection CAPX output Existing Plus Project Conditions

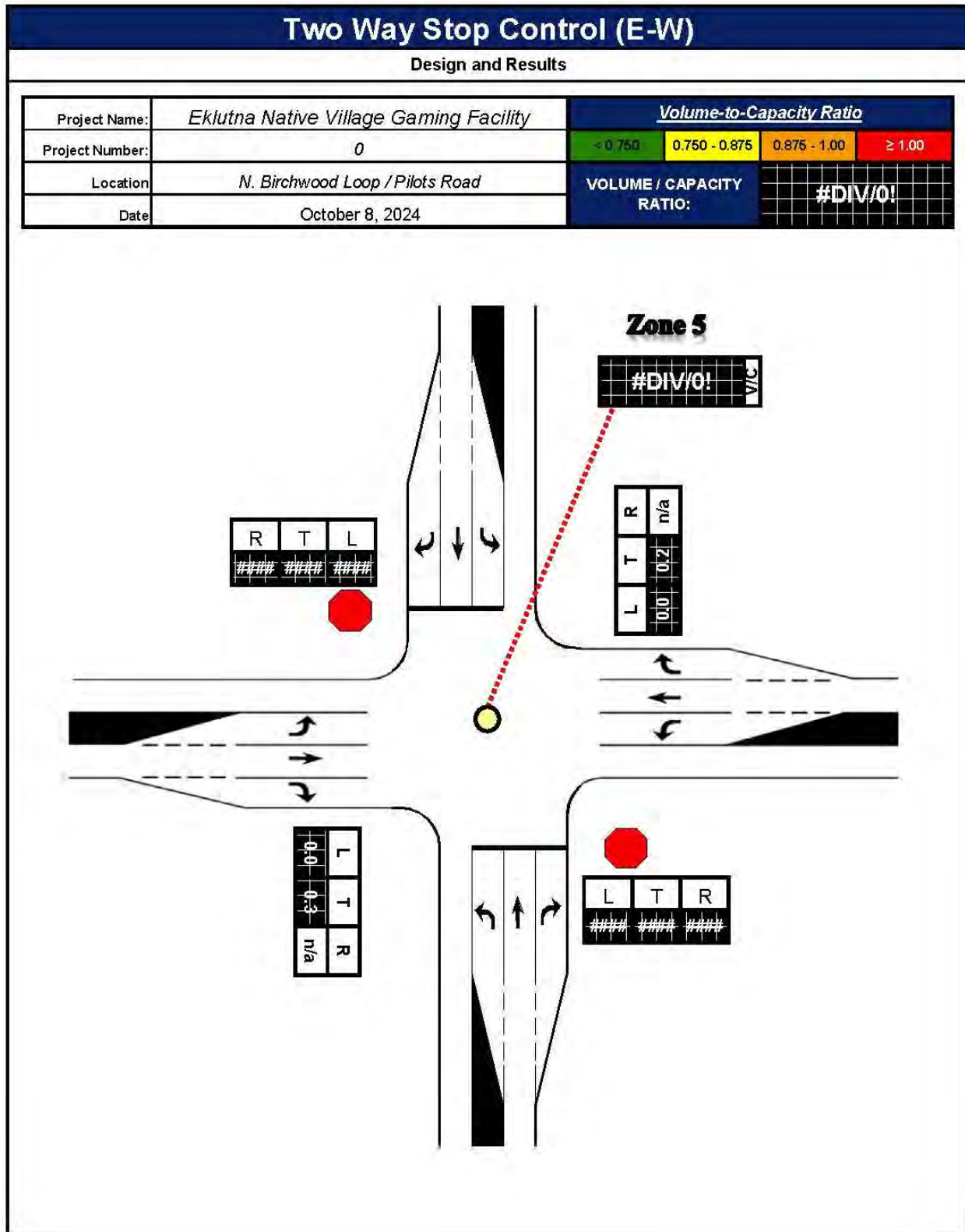


Figure 20: N Birchwood Loop and Pilots Road Intersection CAPX output Existing Plus Project Conditions

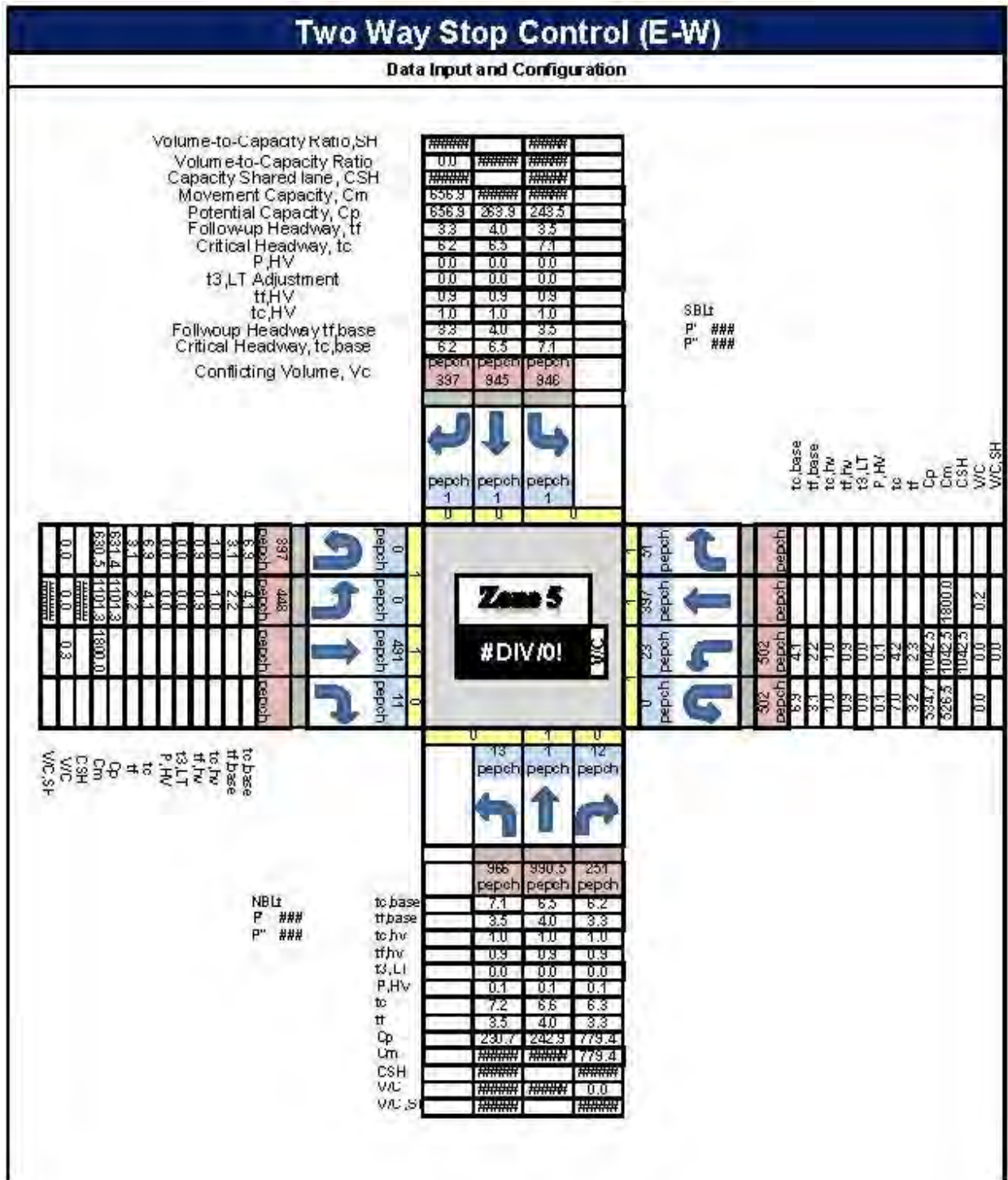


Figure 21: N Birchwood Loop and Southbound Glenn Highway Ramps Intersection CAPX output Existing Plus Project Conditions

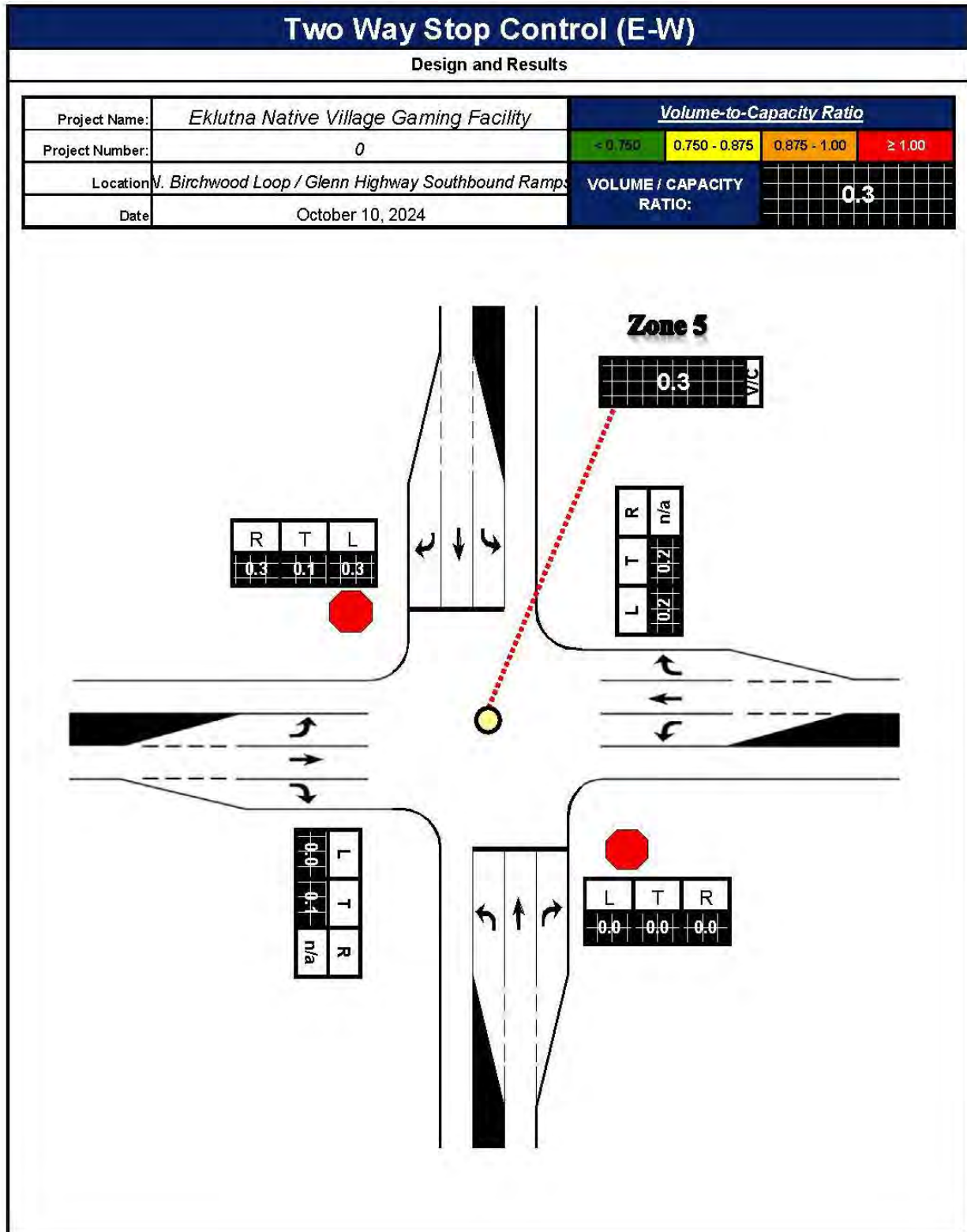


Figure 22: N Birchwood Loop and Glenn Highway Southbound Ramps Intersection CAPX output Existing Plus Project Conditions

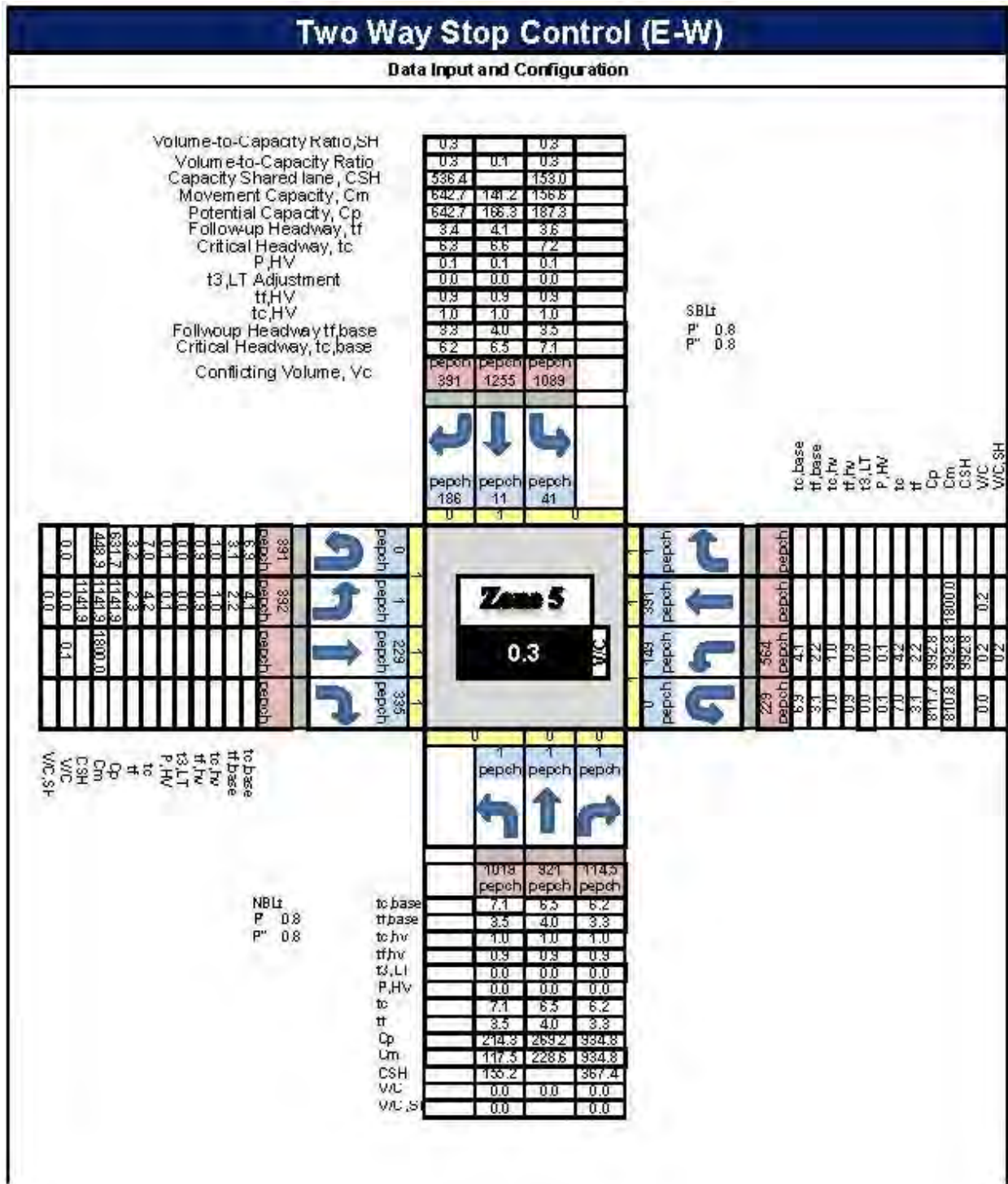


Figure 23: N Birchwood Loop and Northbound Glenn Highway Ramps Intersection CAPX output Existing Plus Project Conditions

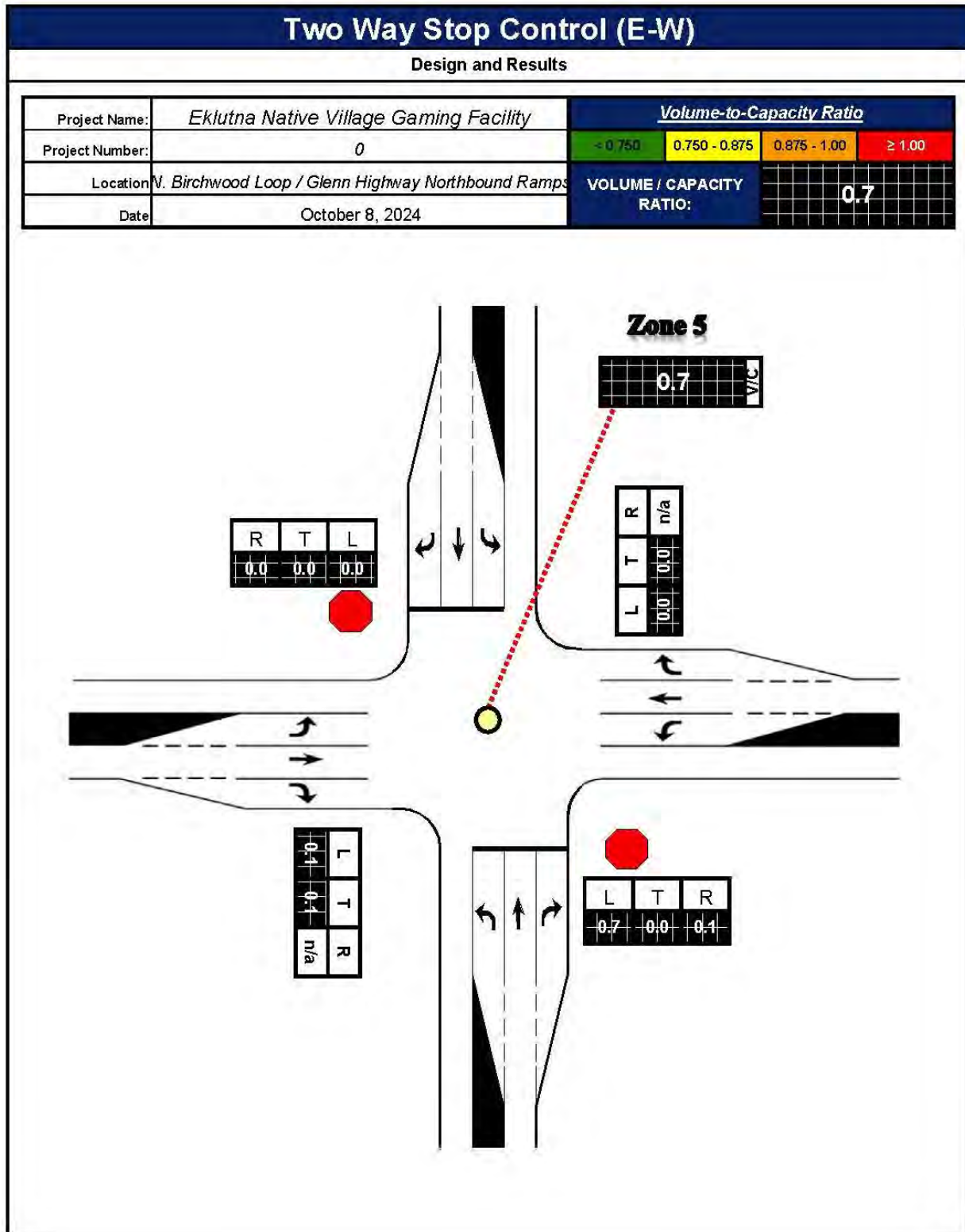


Figure 24: N Birchwood Loop and Northbound Glenn Highway Ramps Intersection CAPX output Existing Plus Project Conditions

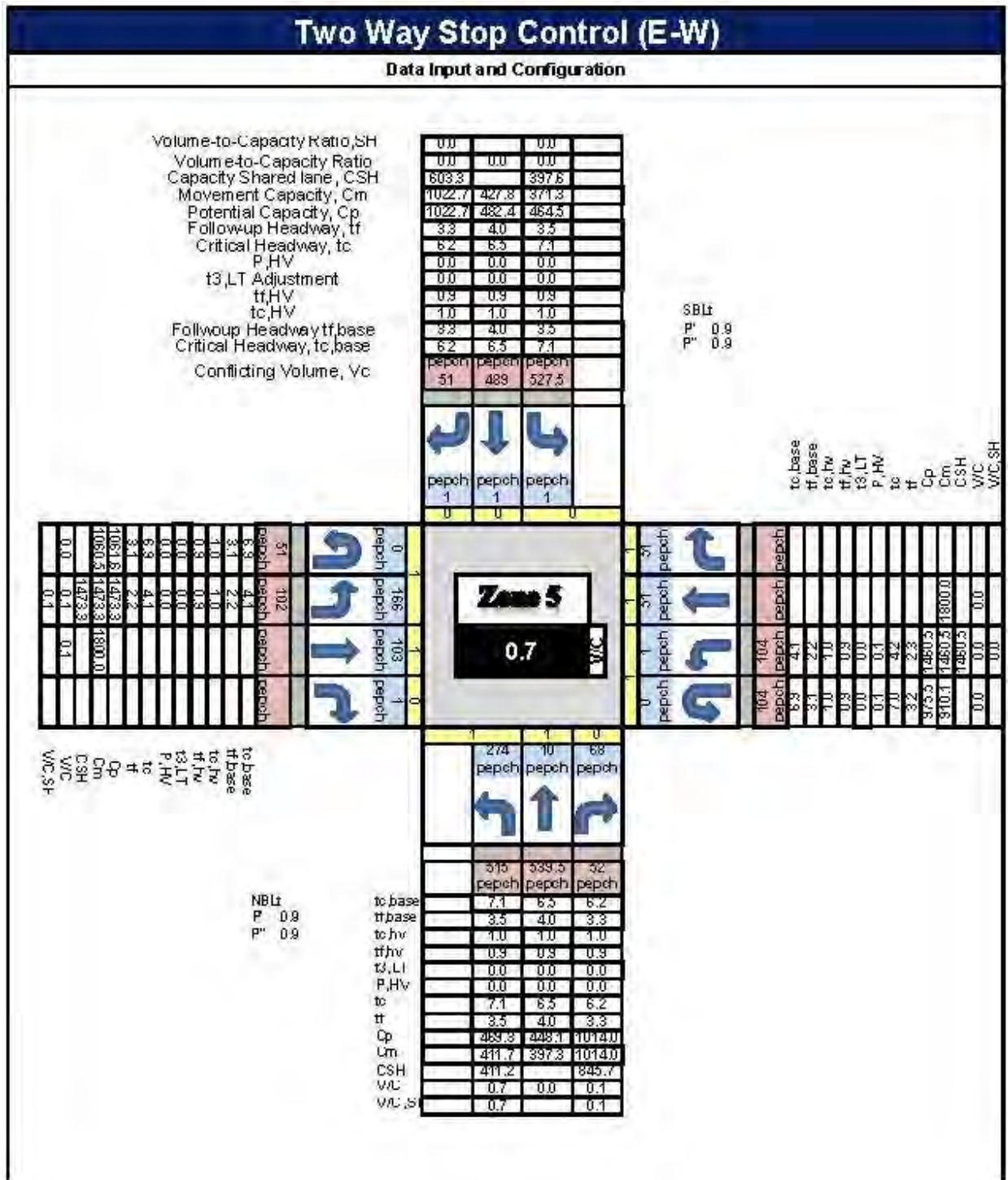


Figure 25: S Birchwood Loop and Old Glenn Highway Intersection CAPX output Existing Plus Project Conditions

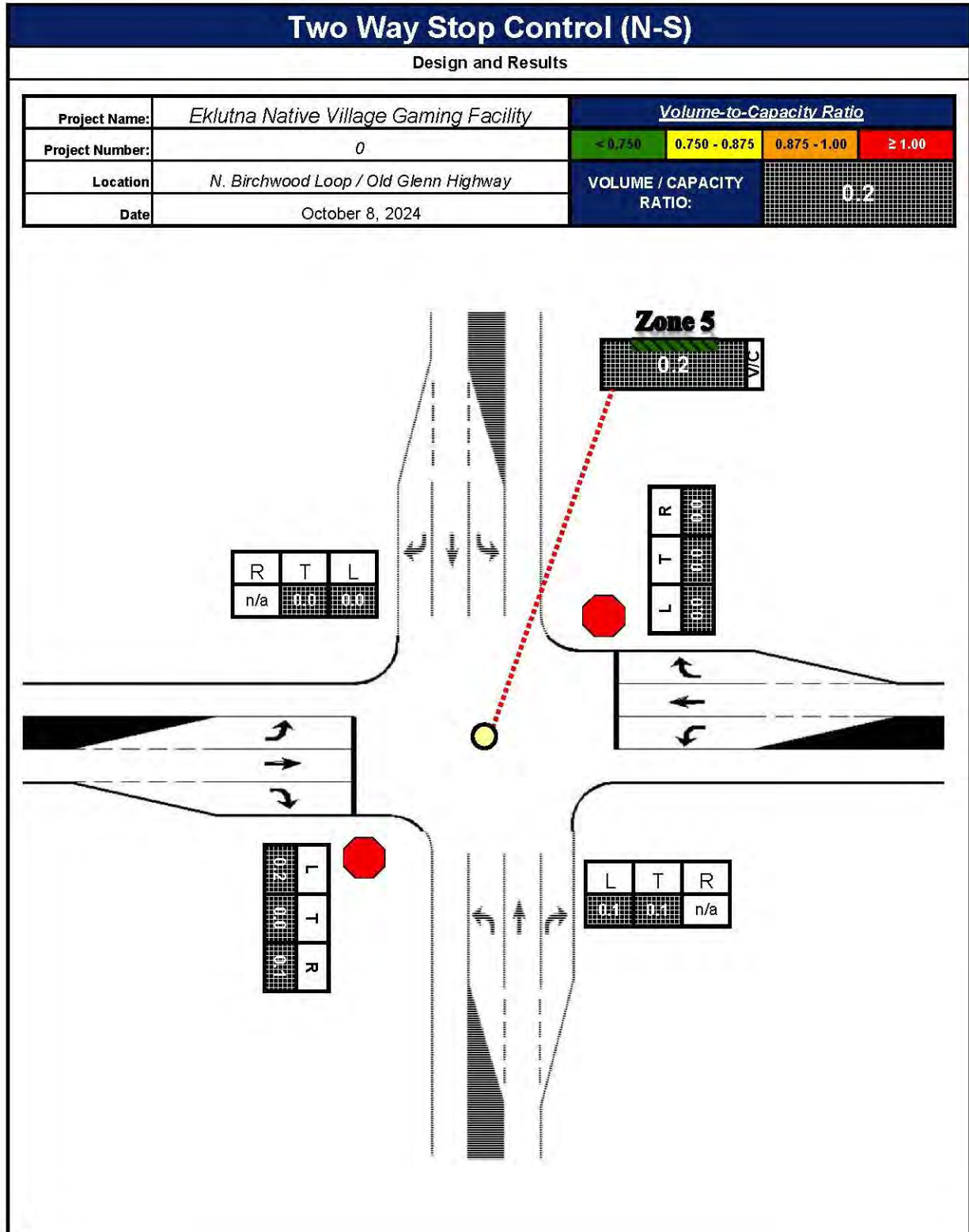
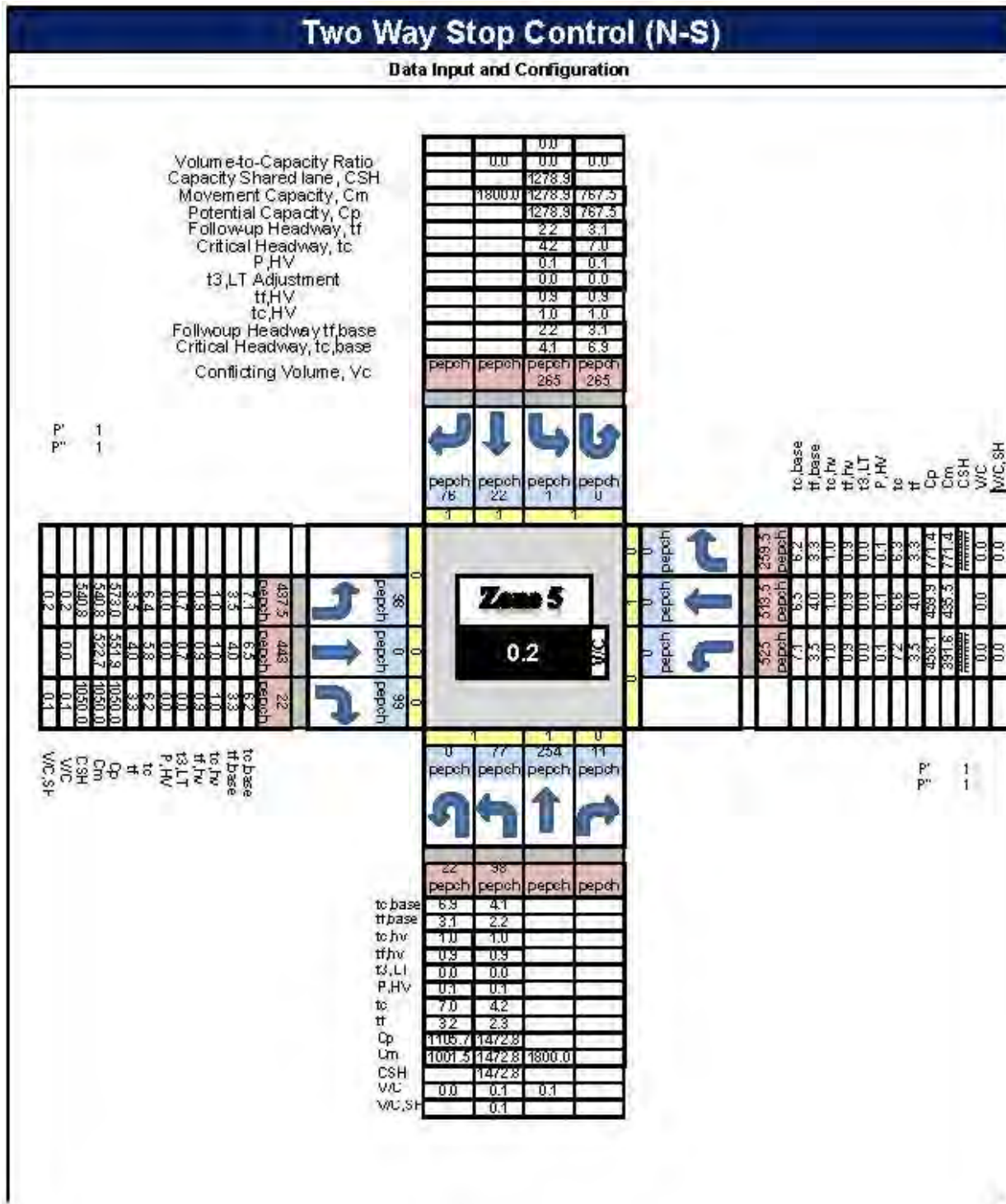


Figure 26: S Birchwood Loop and Old Glenn Highway Intersection CAPX output Existing Plus Project Conditions



Opening Year and Cumulative Horizon Year

The traffic in the subject area has been steady and has not increased in recent years. The current “2050 Metropolitan Transportation Plan” forecasts the total population growth in the Chugiak – Eagle River area to increase by 3% from the year 2019 to the year 2050. A 3% increase in traffic will not add to the Existing plus Project Conditions service level. The cumulative setting includes growth and development envisioned in the Anchorage 2040 Land Use Plan and Chugiak-Eagle River Comprehensive Plan. The Chugiak-Eagle River Comprehensive Plan assumes an approximate growth rate of 3 percent per year. The Anchorage 2040 Land Use Plan forecasts an average annual population growth rate of at least 0.8 percent between 2015 and 2040. The Anchorage 2040 Land Use Plan reflects Chugiak-Eagle River Comprehensive Plan’s anticipated growth at a somewhat higher rate than the rest of the Municipality of Anchorage, such that the Chugiak-Eagle River area, which includes the Project Site, is presumed to accommodate 15 percent of the Municipality’s overall population by 2040.

Recommendations and Improvements

The existing infrastructure supporting traffic in the study area will be sufficient to meet the additional traffic from the facility, but will be near the limit for maintaining the current LOS. Currently the roads have excess capacity, presumably to accommodate future population growth in the area. The peak hour trips will approximately double in the project area. PES recommends consulting with the Alaska DOT in regards to their requirements for future capacity and potential costs to recover that capacity through traffic control and/or reconfiguration specifically in the area surrounding the Glenn Highway Interchange. Specifically the On Ramps and Off Ramps at the interchange with generated additional traffic will reduce the LOS from A to C/D at the intersection.

5.0 Site Circulation Review

Proposed Site Circulation Review

Parking Requirements

The project site is in Native Allotment Land, so it is not governed by state or local agencies or jurisdictions. Thus, Anchorage Municipal Codes AMC etc. do not apply. The project will be constructed to IFC, IBC, etc. With approximately 400 customer parking spaces available, a recreational parking generation rate would require 348 parking spaces based on the gross floor area.

Option 1 (Recommended Option): Option 1 will consist of a single parking lot with dedicated inbound and outbound access with circulation. Nearly all traffic accessing the site will access from the south on Birchwood Spur Road. The access to the site would be through the 50' Right-Of-Way along the south side of the lot with dedicated one-way inbound and outbound access from the ROW to the parking lot. This is the preferred option due to the ease of circulation (separating the inbound and outbound traffic, and the lack of a pedestrian railroad crossing.

Option 2: Option 2 will consist of a single parking lot with two way access. Nearly all traffic accessing the site will access from the south on Birchwood Spur Road. The access to the site would be through the 50' Right-Of-Way along the south side of the lot with two two-way accesses to the lot.

The Project is located on a lot with approximately 6.4 acres of space available. Circulation requirements set by the International Fire Code (IFC) will be met and will have an effect on the layout of the site. Per IFC the building must be accessible by a fire lane 26' wide with turning space available for a fire truck to maneuver on the lot. AMC does not apply to the site because it is in Native Allotment Land but for reference, but may be followed to be consistent with local codes. AMC 21.07 requires that a lot with over 200 parking spaces contain a landscaped area equivalent to 10% of the paved area on the lot and while this is not a requirement it is assumed that this will be met. Additionally, a landscaped perimeter would be required under municipal code. Depending on zoning of the subject lot to be developed and the surrounding area the landscape perimeter buffer would be required to be 8-feet to 15-feet wide.

Appendix: Raw Traffic Data

For Project: Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:24:31 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 8/23/2024 1:00:00 PM through 8/29/2024 10:59:59 AM

85th Percentile Speed 46 MPH

85th Percentile Vehicles 3595

Max Speed 74 MPH on 8/28/2024 8:52:06 PM

Total Vehicles 4229

AADT: 714

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	600	604
AM Peak 8:00 AM	45	40
PM Peak 3:00 PM	64	62

Speed

Speed Limit: 45

85th Percentile Speed: 46

50th Percentile Speed: 40

10 MPH Pace Interval: 35.0 MPH to 45.0 MPH

Average Speed: 40.31

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	125	123	137	21	81	91	119
% over limit	16.4	15.9	16.9	12.7	16.5	15.4	18.7
Avg Speeder	48.0	49.4	48.9	49.2	48.8	48.8	48.6
Avg Speed	40.0	40.5	40.2	39.8	40.4	40.2	40.8

Class Counts

	Number	%
VEH_SM	10	0.2
VEH_MED	3999	94.6
VEH_LG	220	5.2
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/23/2024 2:00:00 PM	44.0	52	61	59	52.1	13.1%	41.0
8/23/2024 3:00:00 PM	45.0	42	50	57	50.3	16.0%	41.6
8/23/2024 4:00:00 PM	45.0	58	68	47	46.6	10.3%	39.6
8/23/2024 5:00:00 PM	47.0	52	61	54	48.2	26.2%	41.9
8/23/2024 6:00:00 PM	46.0	44	52	51	47.1	19.2%	39.6
8/23/2024 7:00:00 PM	47.0	59	69	56	49.9	21.7%	40.6
8/23/2024 8:00:00 PM	46.0	42	50	51	48.0	18.0%	40.6
8/23/2024 9:00:00 PM	43.0	31	37	47	46.3	8.1%	39.3
8/23/2024 10:00:00 PM	43.0	13	15	47	47.0	6.7%	38.1
8/23/2024 11:00:00 PM	46.0	14	16	51	48.0	18.8%	39.5
8/24/2024 12:00:00 AM	38.0	10	12	52	52.0	8.3%	36.3
8/24/2024 1:00:00 AM	45.0	3	3	45	0.0	0.0%	41.7
8/24/2024 2:00:00 AM	43.0	4	5	52	52.0	20.0%	42.2
8/24/2024 3:00:00 AM	48.0	2	2	48	48.0	50.0%	44.5
8/24/2024 4:00:00 AM	35.0	4	5	35	0.0	0.0%	32.6
8/24/2024 5:00:00 AM	37.0	3	3	37	0.0	0.0%	35.7
8/24/2024 6:00:00 AM	**No Data**						
8/24/2024 7:00:00 AM	44.0	1	1	44	0.0	0.0%	44.0
8/24/2024 8:00:00 AM	45.0	12	14	49	48.0	14.3%	40.4
8/24/2024 9:00:00 AM	45.0	14	17	50	49.3	17.6%	40.0
8/24/2024 10:00:00 AM	47.0	31	37	53	48.4	21.6%	42.2
8/24/2024 11:00:00 AM	47.0	26	30	56	48.8	26.7%	41.7
8/24/2024 12:00:00 PM	46.0	37	43	50	48.4	16.3%	40.5
8/24/2024 1:00:00 PM	46.0	44	52	52	49.3	17.3%	40.9
8/24/2024 2:00:00 PM	46.0	37	43	58	49.7	16.3%	41.0
8/24/2024 3:00:00 PM	46.0	37	44	51	47.9	20.5%	40.0
8/24/2024 4:00:00 PM	45.0	50	59	54	48.8	10.2%	40.6
8/24/2024 5:00:00 PM	43.0	31	37	48	48.0	2.7%	38.5
8/24/2024 6:00:00 PM	46.0	30	35	51	48.0	20.0%	41.8
8/24/2024 7:00:00 PM	44.0	42	49	54	50.7	6.1%	39.0
8/24/2024 8:00:00 PM	47.0	39	46	51	48.8	19.6%	39.8
8/24/2024 9:00:00 PM	46.0	20	24	53	49.0	20.8%	39.3
8/24/2024 10:00:00 PM	43.0	15	18	52	49.5	11.1%	38.4
8/24/2024 11:00:00 PM	41.0	8	10	49	48.0	20.0%	36.8
8/25/2024 12:00:00 AM	39.0	10	12	47	47.0	8.3%	36.6
8/25/2024 1:00:00 AM	39.0	4	5	43	0.0	0.0%	38.2
8/25/2024 2:00:00 AM	38.0	4	5	39	0.0	0.0%	34.6
8/25/2024 3:00:00 AM	42.0	3	3	42	0.0	0.0%	37.7
8/25/2024 4:00:00 AM	35.0	2	2	35	0.0	0.0%	27.5
8/25/2024 5:00:00 AM	40.0	3	3	40	0.0	0.0%	37.3
8/25/2024 6:00:00 AM	**No Data**						
8/25/2024 7:00:00 AM	**No Data**						
8/25/2024 8:00:00 AM	44.0	8	9	49	49.0	11.1%	39.3
8/25/2024 9:00:00 AM	46.0	17	20	51	48.4	25.0%	41.2
8/25/2024 10:00:00 AM	46.0	19	22	56	49.5	18.2%	41.6
8/25/2024 11:00:00 AM	47.0	20	24	55	48.6	33.3%	43.2
8/25/2024 12:00:00 PM	45.0	29	34	52	48.6	14.7%	40.9

Incoming Summary
Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

8/25/2024 1:00:00 PM	45.0	46	54	51	47.7	13.0%	40.6
8/25/2024 2:00:00 PM	45.0	50	59	59	48.0	15.3%	40.6
8/25/2024 3:00:00 PM	48.0	66	78	63	49.1	26.9%	41.9
8/25/2024 4:00:00 PM	45.0	48	57	54	48.2	10.5%	41.4
8/25/2024 5:00:00 PM	46.0	42	49	50	48.4	16.3%	40.0
8/25/2024 6:00:00 PM	46.0	46	54	56	48.0	22.2%	40.9
8/25/2024 7:00:00 PM	47.0	33	39	59	49.4	28.2%	42.2
8/25/2024 8:00:00 PM	47.0	36	42	53	47.9	28.6%	41.8
8/25/2024 9:00:00 PM	46.0	26	30	53	49.0	16.7%	39.0
8/25/2024 10:00:00 PM	45.0	22	26	53	50.7	11.5%	38.9
8/25/2024 11:00:00 PM	44.0	8	9	48	48.0	11.1%	40.9
8/26/2024 12:00:00 AM	42.0	11	13	47	47.0	7.7%	37.9
8/26/2024 1:00:00 AM	42.0	7	8	57	57.0	12.5%	37.4
8/26/2024 2:00:00 AM	41.0	4	5	44	0.0	0.0%	39.4
8/26/2024 3:00:00 AM	46.0	2	2	46	46.0	50.0%	45.0
8/26/2024 4:00:00 AM	**No Data**						
8/26/2024 5:00:00 AM	**No Data**						
8/26/2024 6:00:00 AM	41.0	1	1	41	0.0	0.0%	41.0
8/26/2024 7:00:00 AM	49.0	14	16	52	49.5	25.0%	43.9
8/26/2024 8:00:00 AM	43.0	16	19	51	51.0	5.3%	40.6
8/26/2024 9:00:00 AM	44.0	25	29	57	51.5	6.9%	40.1
8/26/2024 10:00:00 AM	45.0	34	40	50	47.0	15.0%	38.4
8/26/2024 11:00:00 AM	46.0	43	51	57	47.9	27.5%	40.2
8/26/2024 12:00:00 PM	46.0	40	47	52	47.4	21.3%	39.8
8/26/2024 1:00:00 PM	46.0	42	49	56	49.3	16.3%	40.6
8/26/2024 2:00:00 PM	45.0	45	53	55	49.3	13.2%	40.8
8/26/2024 3:00:00 PM	44.0	44	52	56	49.1	13.2%	40.0
8/26/2024 4:00:00 PM	45.0	67	79	53	48.2	13.9%	40.1
8/26/2024 5:00:00 PM	46.0	53	62	51	47.0	16.1%	39.5
8/26/2024 6:00:00 PM	44.0	54	64	52	47.6	10.9%	38.7
8/26/2024 7:00:00 PM	46.0	45	53	51	47.4	20.8%	40.3
8/26/2024 8:00:00 PM	47.0	36	42	54	47.8	38.1%	42.2
8/26/2024 9:00:00 PM	42.0	28	33	46	46.0	3.0%	38.0
8/26/2024 10:00:00 PM	46.0	29	34	48	47.0	17.6%	40.6
8/26/2024 11:00:00 PM	44.0	12	14	47	47.0	7.1%	37.6
8/27/2024 12:00:00 AM	41.0	8	10	46	46.0	10.0%	38.5
8/27/2024 1:00:00 AM	35.0	4	5	41	0.0	0.0%	34.6
8/27/2024 2:00:00 AM	50.0	3	3	50	50.0	33.3%	37.0
8/27/2024 3:00:00 AM	36.0	2	2	36	0.0	0.0%	34.5
8/27/2024 4:00:00 AM	37.0	2	2	37	0.0	0.0%	32.5
8/27/2024 5:00:00 AM	35.0	3	3	35	0.0	0.0%	28.0
8/27/2024 6:00:00 AM	**No Data**						
8/27/2024 7:00:00 AM	46.0	10	12	52	47.5	33.3%	43.6
8/27/2024 8:00:00 AM	46.0	23	27	51	48.2	18.5%	42.1
8/27/2024 9:00:00 AM	47.0	38	45	57	49.5	31.1%	42.3
8/27/2024 10:00:00 AM	45.0	36	42	51	48.0	14.3%	39.3
8/27/2024 11:00:00 AM	45.0	27	32	50	47.6	15.6%	39.6
8/27/2024 12:00:00 PM	44.0	37	43	52	49.2	11.6%	41.0

Incoming Summary
 Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

8/27/2024 1:00:00 PM	45.0	37	43	62	52.8	11.6%	40.2
8/27/2024 2:00:00 PM	44.0	48	57	50	47.8	7.0%	40.4
8/27/2024 3:00:00 PM	45.0	37	44	56	50.1	15.9%	41.5
8/27/2024 4:00:00 PM	49.0	40	47	56	51.3	21.3%	40.9
8/27/2024 5:00:00 PM	47.0	45	53	54	48.8	22.6%	41.9
8/27/2024 6:00:00 PM	47.0	50	59	52	49.6	16.9%	39.3
8/27/2024 7:00:00 PM	44.0	65	77	55	48.7	13.0%	39.7
8/27/2024 8:00:00 PM	46.0	70	82	57	50.2	15.9%	41.7
8/27/2024 9:00:00 PM	46.0	39	46	52	47.9	17.4%	40.9
8/27/2024 10:00:00 PM	45.0	26	30	51	48.5	6.7%	40.4
8/27/2024 11:00:00 PM	42.0	8	10	47	47.0	10.0%	37.5
8/28/2024 12:00:00 AM	44.0	8	9	59	59.0	11.1%	36.8
8/28/2024 1:00:00 AM	39.0	4	5	41	0.0	0.0%	37.2
8/28/2024 2:00:00 AM	37.0	3	4	61	61.0	25.0%	40.0
8/28/2024 3:00:00 AM	34.0	1	1	34	0.0	0.0%	34.0
8/28/2024 4:00:00 AM	38.0	1	1	38	0.0	0.0%	38.0
8/28/2024 5:00:00 AM	47.0	3	3	47	47.0	33.3%	36.3
8/28/2024 6:00:00 AM	**No Data**						
8/28/2024 7:00:00 AM	46.0	10	12	48	46.7	25.0%	41.4
8/28/2024 8:00:00 AM	47.0	19	22	49	47.8	22.7%	41.8
8/28/2024 9:00:00 AM	46.0	48	56	53	48.4	16.1%	39.9
8/28/2024 10:00:00 AM	45.0	37	43	52	49.0	14.0%	38.2
8/28/2024 11:00:00 AM	45.0	33	39	49	48.0	15.4%	40.4
8/28/2024 12:00:00 PM	46.0	31	37	53	49.3	18.9%	41.4
8/28/2024 1:00:00 PM	46.0	43	51	54	48.7	19.6%	41.2
8/28/2024 2:00:00 PM	44.0	36	42	50	47.6	11.9%	38.5
8/28/2024 3:00:00 PM	45.0	55	65	53	49.1	13.8%	40.9
8/28/2024 4:00:00 PM	44.0	54	64	54	48.4	10.9%	39.0
8/28/2024 5:00:00 PM	46.0	65	76	62	49.4	15.8%	40.3
8/28/2024 6:00:00 PM	46.0	60	70	54	48.5	18.6%	39.1
8/28/2024 7:00:00 PM	47.0	51	60	56	48.5	28.3%	42.6
8/28/2024 8:00:00 PM	47.0	54	64	64	49.8	20.3%	41.5
8/28/2024 9:00:00 PM	46.0	35	41	74	51.4	17.1%	40.3
8/28/2024 10:00:00 PM	46.0	25	29	49	47.6	17.2%	40.6
8/28/2024 11:00:00 PM	42.0	12	14	48	48.0	7.1%	39.3
8/29/2024 12:00:00 AM	38.0	10	12	44	0.0	0.0%	35.3
8/29/2024 1:00:00 AM	39.0	1	1	39	0.0	0.0%	39.0
8/29/2024 2:00:00 AM	40.0	3	3	40	0.0	0.0%	33.7
8/29/2024 3:00:00 AM	44.0	3	3	44	0.0	0.0%	38.3
8/29/2024 4:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
8/29/2024 5:00:00 AM	**No Data**						
8/29/2024 6:00:00 AM	**No Data**						
8/29/2024 7:00:00 AM	44.0	9	11	61	56.5	18.2%	42.5
8/29/2024 8:00:00 AM	44.0	8	10	52	50.0	20.0%	42.1
8/29/2024 9:00:00 AM	45.0	43	51	52	48.4	13.7%	40.3
8/29/2024 10:00:00 AM	45.0	39	46	58	48.0	15.2%	40.2
8/29/2024 11:00:00 AM	44.0	33	39	50	48.7	7.7%	38.0

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/24/2024 12:00:00 AM	46.0	417	491	59	48.8	16.5%	40.4
8/25/2024 12:00:00 AM	46.0	501	589	58	48.8	15.4%	40.2
8/26/2024 12:00:00 AM	46.0	541	637	63	48.6	18.7%	40.8
8/27/2024 12:00:00 AM	46.0	649	763	57	48.0	16.4%	40.0
8/28/2024 12:00:00 AM	46.0	657	773	62	49.4	15.9%	40.5
8/29/2024 12:00:00 AM	46.0	689	811	74	48.9	16.9%	40.2
8/29/2024 10:59:59 AM	45.0	140	165	61	49.2	12.7%	39.8

Incoming Weekly Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	3	5	0	4	0
1 - 2	*	*	*	*	*	5	5	0	5	0
2 - 3	*	*	*	*	*	2	3	0	2.5	0
3 - 4	*	*	*	*	*	5	2	0	3.5	0
4 - 5	*	*	*	*	*	3	3	0	3	0
5 - 6	*	*	*	*	*	0	0	0	0	0
6 - 7	*	*	*	*	*	1	0	0	0.5	0
7 - 8	*	*	*	*	*	14	9	0	11.5	0
8 - 9	*	*	*	*	*	17	20	0	18.5	0
9 - 10	*	*	*	*	*	37	22	0	29.5	0
10 - 11	*	*	*	*	*	30	24	0	27	0
11 - 12	*	*	*	*	*	43	34	0	38.5	0
12 - 13	*	*	*	*	*	52	54	0	53	0
13 - 14	*	*	*	*	61	43	59	61	51	44
14 - 15	*	*	*	*	50	44	78	50	61	45
15 - 16	*	*	*	*	68	59	57	68	58	44.2
16 - 17	*	*	*	*	61	37	49	61	43	46.4
17 - 18	*	*	*	*	52	35	54	52	44.5	45.3
18 - 19	*	*	*	*	69	49	39	69	44	47
19 - 20	*	*	*	*	50	46	42	50	44	45.2
20 - 21	*	*	*	*	37	24	30	37	27	42.7
21 - 22	*	*	*	*	15	18	26	15	22	43
22 - 23	*	*	*	*	16	10	9	16	9.5	46
23 - 24	*	*	*	*	12	12	13	12	12.5	38
Totals	0	0	0	0	491	589	637			
% of Total	0%	0%	0%	0%	28.6%	34.3%	37.1%			

Incoming Weekly Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	8	5	5	1	*	*	*	4.75	0	38.75
1 - 2	5	3	4	3	*	*	*	3.75	0	42
2 - 3	2	2	1	3	*	*	*	2	0	40
3 - 4	0	2	1	1	*	*	*	1	0	34.33
4 - 5	0	3	3	0	*	*	*	1.5	0	41
5 - 6	1	0	0	0	*	*	*	0.25	0	41
6 - 7	16	12	12	11	*	*	*	12.75	0	46.05
7 - 8	19	27	22	10	*	*	*	19.5	0	45
8 - 9	29	45	56	51	*	*	*	45.25	0	45.18
9 - 10	40	42	43	46	*	*	*	42.75	0	45
10 - 11	51	32	39	39	*	*	*	40.25	0	44.98
11 - 12	47	43	37	*	*	*	*	42.33	0	45.23
12 - 13	49	43	51	*	*	*	*	47.67	0	45.5
13 - 14	53	57	42	*	*	*	*	50.67	0	44.2
14 - 15	52	44	65	*	*	*	*	53.67	0	44.63
15 - 16	79	47	64	*	*	*	*	63.33	0	45.73
16 - 17	62	53	76	*	*	*	*	63.67	0	45.6
17 - 18	64	59	70	*	*	*	*	64.33	0	45.33
18 - 19	53	77	60	*	*	*	*	63.33	0	45.57
19 - 20	42	82	64	*	*	*	*	62.67	0	46.5
20 - 21	33	46	41	*	*	*	*	40	0	44.23
21 - 22	34	30	29	*	*	*	*	31	0	45.43
22 - 23	14	10	14	*	*	*	*	12.67	0	42.5
23 - 24	10	9	12	*	*	*	*	10.33	0	41
Totals	763	773	811	165	0	0	0			
% of Total	30.37%	30.77%	32.29%	6.57%	0%	0%	0%			

Incoming Monthly Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	8	5	5	1	*	3	5	4.75	4	38.75
1 - 2	5	3	4	3	*	5	5	3.75	5	42
2 - 3	2	2	1	3	*	2	3	2	2.5	40
3 - 4	0	2	1	1	*	5	2	1	3.5	34.33
4 - 5	0	3	3	0	*	3	3	1.5	3	41
5 - 6	1	0	0	0	*	0	0	0.25	0	41
6 - 7	16	12	12	11	*	1	0	12.75	0.5	46.05
7 - 8	19	27	22	10	*	14	9	19.5	11.5	45
8 - 9	29	45	56	51	*	17	20	45.25	18.5	45.18
9 - 10	40	42	43	46	*	37	22	42.75	29.5	45
10 - 11	51	32	39	39	*	30	24	40.25	27	44.98
11 - 12	47	43	37	*	*	43	34	42.33	38.5	45.23
12 - 13	49	43	51	*	*	52	54	47.67	53	45.5
13 - 14	53	57	42	*	61	43	59	53.25	51	44.15
14 - 15	52	44	65	*	50	44	78	52.75	61	44.72
15 - 16	79	47	64	*	68	59	57	64.5	58	45.35
16 - 17	62	53	76	*	61	37	49	63	43	45.8
17 - 18	64	59	70	*	52	35	54	61.25	44.5	45.32
18 - 19	53	77	60	*	69	49	39	64.75	44	45.92
19 - 20	42	82	64	*	50	46	42	59.5	44	46.18
20 - 21	33	46	41	*	37	24	30	39.25	27	43.85
21 - 22	34	30	29	*	15	18	26	27	22	44.82
22 - 23	14	10	14	*	16	10	9	13.5	9.5	43.38
23 - 24	10	9	12	*	12	12	13	10.75	12.5	40.25
Totals	763	773	811	165	491	589	637			
% of Total	18.04%	18.28%	19.18%	3.9%	11.61%	13.93%	15.06%			

Incoming Weekly Speeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	41.67	38.2	0	39.5	0
1 - 2	*	*	*	*	*	42.2	34.6	0	38.4	0
2 - 3	*	*	*	*	*	44.5	37.67	0	40.4	0
3 - 4	*	*	*	*	*	32.6	27.5	0	31.14	0
4 - 5	*	*	*	*	*	35.67	37.33	0	36.5	0
5 - 6	*	*	*	*	*	0	0	0	0	0
6 - 7	*	*	*	*	*	44	0	0	44	0
7 - 8	*	*	*	*	*	40.36	39.33	0	39.96	0
8 - 9	*	*	*	*	*	40	41.2	0	40.65	0
9 - 10	*	*	*	*	*	42.19	41.64	0	41.98	0
10 - 11	*	*	*	*	*	41.67	43.17	0	42.33	0
11 - 12	*	*	*	*	*	40.53	40.94	0	40.71	0
12 - 13	*	*	*	*	*	40.88	40.59	0	40.74	0
13 - 14	*	*	*	*	41.11	41.02	40.63	41.11	40.79	44
14 - 15	*	*	*	*	41.6	40.05	41.94	41.6	41.25	45
15 - 16	*	*	*	*	39.59	40.64	41.35	39.59	40.99	44.2
16 - 17	*	*	*	*	41.93	38.49	39.98	41.93	39.34	46.4
17 - 18	*	*	*	*	39.6	41.8	40.85	39.6	41.22	45.3
18 - 19	*	*	*	*	40.61	39.02	42.18	40.61	40.42	47
19 - 20	*	*	*	*	40.62	39.78	41.81	40.62	40.75	45.2
20 - 21	*	*	*	*	39.27	39.25	39	39.27	39.11	42.7
21 - 22	*	*	*	*	38.13	38.44	38.92	38.13	38.73	43
22 - 23	*	*	*	*	39.5	36.8	40.89	39.5	38.74	46
23 - 24	*	*	*	*	36.33	36.58	37.92	36.33	37.28	38
Totals	0	0	0	0	40.4	40.2	40.8			
% of Total	0%	0%	0%	0%	33.28%	33.11%	33.61%			

Incoming Weekly Speeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	37.38	34.6	37.2	39	*	*	*	36.68	*	38.75
1 - 2	39.4	37	40	33.67	*	*	*	37.93	*	42
2 - 3	45	34.5	34	38.33	*	*	*	38.5	*	40
3 - 4	0	32.5	38	28	*	*	*	32.75	*	34.33
4 - 5	0	28	36.33	0	*	*	*	32.17	*	41
5 - 6	41	0	0	0	*	*	*	41	*	41
6 - 7	43.88	43.58	41.42	42.55	*	*	*	42.94	*	46.05
7 - 8	40.63	42.15	41.82	42.1	*	*	*	41.68	*	45
8 - 9	40.14	42.27	39.89	40.31	*	*	*	40.64	*	45.18
9 - 10	38.38	39.29	38.23	40.17	*	*	*	39.05	*	45
10 - 11	40.2	39.56	40.36	38.03	*	*	*	39.58	*	44.98
11 - 12	39.79	41.02	41.35	*	*	*	*	40.66	*	45.23
12 - 13	40.59	40.16	41.18	*	*	*	*	40.67	*	45.5
13 - 14	40.77	40.44	38.52	*	*	*	*	40.03	*	44.2
14 - 15	40.02	41.52	40.89	*	*	*	*	40.78	*	44.63
15 - 16	40.06	40.91	39.05	*	*	*	*	39.93	*	45.73
16 - 17	39.53	41.87	40.26	*	*	*	*	40.47	*	45.6
17 - 18	38.72	39.32	39.11	*	*	*	*	39.05	*	45.33
18 - 19	40.28	39.73	42.58	*	*	*	*	40.78	*	45.57
19 - 20	42.21	41.66	41.48	*	*	*	*	41.72	*	46.5
20 - 21	37.97	40.87	40.27	*	*	*	*	39.87	*	44.23
21 - 22	40.65	40.43	40.62	*	*	*	*	40.57	*	45.43
22 - 23	37.64	37.5	39.29	*	*	*	*	38.21	*	42.5
23 - 24	38.5	36.78	35.25	*	*	*	*	36.74	*	41
Totals	40	40.5	40.2	39.7	0	0	0			
% of Total	24.94%	25.25%	25.06%	24.75%	0%	0%	0%			

Incoming Monthly Speeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	Aug 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	37.38	34.6	37.2	39	*	41.67	38.2	36.68	39.5	38.75
1 - 2	39.4	37	40	33.67	*	42.2	34.6	37.93	38.4	42
2 - 3	45	34.5	34	38.33	*	44.5	37.67	38.5	40.4	40
3 - 4	0	32.5	38	28	*	32.6	27.5	32.75	31.14	34.33
4 - 5	0	28	36.33	0	*	35.67	37.33	32.17	36.5	41
5 - 6	41	0	0	0	*	0	0	41	0	41
6 - 7	43.88	43.58	41.42	42.55	*	44	0	42.94	44	46.05
7 - 8	40.63	42.15	41.82	42.1	*	40.36	39.33	41.68	39.96	45
8 - 9	40.14	42.27	39.89	40.31	*	40	41.2	40.64	40.65	45.18
9 - 10	38.38	39.29	38.23	40.17	*	42.19	41.64	39.05	41.98	45
10 - 11	40.2	39.56	40.36	38.03	*	41.67	43.17	39.58	42.33	44.98
11 - 12	39.79	41.02	41.35	*	*	40.53	40.94	40.66	40.71	45.23
12 - 13	40.59	40.16	41.18	*	*	40.88	40.59	40.67	40.74	45.5
13 - 14	40.77	40.44	38.52	*	41.11	41.02	40.63	40.34	40.79	44.15
14 - 15	40.02	41.52	40.89	*	41.6	40.05	41.94	40.98	41.25	44.72
15 - 16	40.06	40.91	39.05	*	39.59	40.64	41.35	39.84	40.99	45.35
16 - 17	39.53	41.87	40.26	*	41.93	38.49	39.98	40.83	39.34	45.8
17 - 18	38.72	39.32	39.11	*	39.6	41.8	40.85	39.16	41.22	45.32
18 - 19	40.28	39.73	42.58	*	40.61	39.02	42.18	40.74	40.42	45.92
19 - 20	42.21	41.66	41.48	*	40.62	39.78	41.81	41.49	40.75	46.18
20 - 21	37.97	40.87	40.27	*	39.27	39.25	39	39.73	39.11	43.85
21 - 22	40.65	40.43	40.62	*	38.13	38.44	38.92	40.23	38.73	44.82
22 - 23	37.64	37.5	39.29	*	39.5	36.8	40.89	38.59	38.74	43.38
23 - 24	38.5	36.78	35.25	*	36.33	36.58	37.92	36.63	37.28	40.25
Totals	40	40.5	40.2	39.7	40.4	40.2	40.8			
% of Total	14.19%	14.37%	14.27%	14.09%	14.34%	14.27%	14.48%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	45	39	0	42	0
1 - 2	*	*	*	*	*	43	38	0	40.5	0
2 - 3	*	*	*	*	*	48	42	0	45	0
3 - 4	*	*	*	*	*	34.5	35	0	34.75	0
4 - 5	*	*	*	*	*	37	40	0	38.5	0
5 - 6	*	*	*	*	*	0	0	0	0	0
6 - 7	*	*	*	*	*	44	0	0	44	0
7 - 8	*	*	*	*	*	45	44	0	44.5	0
8 - 9	*	*	*	*	*	45	46	0	45.5	0
9 - 10	*	*	*	*	*	46.3	46	0	46.15	0
10 - 11	*	*	*	*	*	47	46.5	0	46.75	0
11 - 12	*	*	*	*	*	46	45	0	45.5	0
12 - 13	*	*	*	*	*	46	44.5	0	45.25	0
13 - 14	*	*	*	*	44	45.5	45	44	45.25	44
14 - 15	*	*	*	*	45	45.7	47.5	45	46.6	45
15 - 16	*	*	*	*	44.2	44.4	44.5	44.2	44.45	44.2
16 - 17	*	*	*	*	46.4	43	46	46.4	44.5	46.4
17 - 18	*	*	*	*	45.3	46	45.7	45.3	45.85	45.3
18 - 19	*	*	*	*	47	43.3	46.7	47	45	47
19 - 20	*	*	*	*	45.2	47	46.5	45.2	46.75	45.2
20 - 21	*	*	*	*	42.7	46	46	42.7	46	42.7
21 - 22	*	*	*	*	43	43	44.5	43	43.75	43
22 - 23	*	*	*	*	46	41	44	46	42.5	46
23 - 24	*	*	*	*	38	39	42	38	40.5	38
Totals	0	0	0	0	486.8	1010.7	964.4			
% of Total	0%	0%	0%	0%	19.77%	41.05%	39.17%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	42	35	39	39	*	*	*	38.75	0	38.75
1 - 2	41	50	37	40	*	*	*	42	0	42
2 - 3	46	36	34	44	*	*	*	40	0	40
3 - 4	0	37	38	28	*	*	*	34.33	0	34.33
4 - 5	0	35	47	0	*	*	*	41	0	41
5 - 6	41	0	0	0	*	*	*	41	0	41
6 - 7	49	45.7	45.5	44	*	*	*	46.05	0	46.05
7 - 8	43	46	47	44	*	*	*	45	0	45
8 - 9	43.5	47	45.5	44.7	*	*	*	45.18	0	45.18
9 - 10	45	45	45	45	*	*	*	45	0	45
10 - 11	45.9	45	45	44	*	*	*	44.98	0	44.98
11 - 12	45.8	43.9	46	*	*	*	*	45.23	0	45.23
12 - 13	46	44.5	46	*	*	*	*	45.5	0	45.5
13 - 14	44.8	43.8	44	*	*	*	*	44.2	0	44.2
14 - 15	44	45	44.9	*	*	*	*	44.63	0	44.63
15 - 16	44.5	49	43.7	*	*	*	*	45.73	0	45.73
16 - 17	45.2	46.3	45.3	*	*	*	*	45.6	0	45.6
17 - 18	43.5	46.5	46	*	*	*	*	45.33	0	45.33
18 - 19	45.8	44	46.9	*	*	*	*	45.57	0	45.57
19 - 20	47	46	46.5	*	*	*	*	46.5	0	46.5
20 - 21	41.5	45.2	46	*	*	*	*	44.23	0	44.23
21 - 22	46	44.3	46	*	*	*	*	45.43	0	45.43
22 - 23	43.5	42	42	*	*	*	*	42.5	0	42.5
23 - 24	41	44	38	*	*	*	*	41	0	41
Totals	975	1006.2	1004.3	372.7	0	0	0			
% of Total	29.03%	29.96%	29.91%	11.1%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	Aug 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	42	35	39	39	*	45	39	38.75	42	38.75
1 - 2	41	50	37	40	*	43	38	42	40.5	42
2 - 3	46	36	34	44	*	48	42	40	45	40
3 - 4	0	37	38	28	*	34.5	35	34.33	34.75	34.33
4 - 5	0	35	47	0	*	37	40	41	38.5	41
5 - 6	41	0	0	0	*	0	0	41	0	41
6 - 7	49	45.7	45.5	44	*	44	0	46.05	44	46.05
7 - 8	43	46	47	44	*	45	44	45	44.5	45
8 - 9	43.5	47	45.5	44.7	*	45	46	45.18	45.5	45.18
9 - 10	45	45	45	45	*	46.3	46	45	46.15	45
10 - 11	45.9	45	45	44	*	47	46.5	44.98	46.75	44.98
11 - 12	45.8	43.9	46	*	*	46	45	45.23	45.5	45.23
12 - 13	46	44.5	46	*	*	46	44.5	45.5	45.25	45.5
13 - 14	44.8	43.8	44	*	44	45.5	45	44.15	45.25	44.15
14 - 15	44	45	44.9	*	45	45.7	47.5	44.72	46.6	44.72
15 - 16	44.5	49	43.7	*	44.2	44.4	44.5	45.35	44.45	45.35
16 - 17	45.2	46.3	45.3	*	46.4	43	46	45.8	44.5	45.8
17 - 18	43.5	46.5	46	*	45.3	46	45.7	45.32	45.85	45.32
18 - 19	45.8	44	46.9	*	47	43.3	46.7	45.92	45	45.92
19 - 20	47	46	46.5	*	45.2	47	46.5	46.18	46.75	46.18
20 - 21	41.5	45.2	46	*	42.7	46	46	43.85	46	43.85
21 - 22	46	44.3	46	*	43	43	44.5	44.82	43.75	44.82
22 - 23	43.5	42	42	*	46	41	44	43.38	42.5	43.38
23 - 24	41	44	38	*	38	39	42	40.25	40.5	40.25

Summary of Violators

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hrfrom Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	27	37.5	1	57.0
01:00:00	25	38.1	3	54.3
02:00:00	13	39.2	2	47.0
03:00:00	11	31.7	0	0.0
04:00:00	12	34.3	1	47.0
05:00:00	1	41.0	0	0.0
06:00:00	52	43.0	13	49.3
07:00:00	101	41.3	16	48.5
08:00:00	218	40.6	40	49.0
09:00:00	230	39.8	37	48.2
10:00:00	215	40.3	44	48.2
11:00:00	204	40.7	34	48.4
12:00:00	249	40.7	39	49.3
13:00:00	315	40.5	40	49.3
14:00:00	333	41.1	61	49.2
15:00:00	374	40.2	47	48.7
16:00:00	338	40.4	59	48.4
17:00:00	334	39.7	59	48.2
18:00:00	347	40.7	67	48.9
19:00:00	326	41.3	72	48.8
20:00:00	211	39.6	29	48.9
21:00:00	152	39.8	19	48.2
22:00:00	73	38.6	9	47.8
23:00:00	68	36.9	5	50.2

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/23/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	13:00	0	0	0	0	7	15	30	3	4	2	0	0	0	0	0	0	0	0	61	41.1	44	34 to 44	77.0	8	13.1	0	57	4	
8/23/2024	14:00	0	0	0	1	3	12	22	8	3	1	0	0	0	0	0	0	0	0	50	41.6	45	38 to 48	78.0	8	16.0	1	43	6	
8/23/2024	15:00	0	0	1	0	12	15	29	11	0	0	0	0	0	0	0	0	0	0	68	39.6	44.2	34 to 44	75.0	7	10.3	1	62	5	
8/23/2024	16:00	0	0	0	1	2	18	20	15	5	0	0	0	0	0	0	0	0	0	61	41.9	46.4	37 to 47	78.7	16	26.2	0	55	6	
8/23/2024	17:00	0	0	4	0	2	14	22	9	1	0	0	0	0	0	0	0	0	0	52	39.6	45.3	36 to 46	75.0	10	19.2	0	48	4	
8/23/2024	18:00	0	0	0	3	12	15	22	10	5	2	0	0	0	0	0	0	0	0	69	40.6	47	36 to 46	60.9	15	21.7	0	65	4	
8/23/2024	19:00	0	0	0	3	5	9	22	8	3	0	0	0	0	0	0	0	0	0	50	40.6	45.2	37 to 47	72.0	9	18.0	0	48	2	
8/23/2024	20:00	0	0	0	1	3	14	15	4	0	0	0	0	0	0	0	0	0	0	37	39.3	42.7	33 to 43	81.1	3	8.1	0	36	1	
8/23/2024	21:00	0	1	0	0	1	7	4	2	0	0	0	0	0	0	0	0	0	0	15	38.1	43	35 to 45	80.0	1	6.7	0	15	0	
8/23/2024	22:00	0	0	0	2	0	6	5	2	1	0	0	0	0	0	0	0	0	0	16	39.5	46	34 to 44	68.8	3	18.8	0	16	0	
8/23/2024	23:00	0	0	0	1	3	6	1	0	1	0	0	0	0	0	0	0	0	0	12	36.3	38	31 to 41	83.3	1	8.3	0	11	1	
24 Hr Summary		0	1	5	12	50	131	192	72	23	5	0	0	0	0	0	0	0	0	491	40.4	46	36 to 46	70.7	81	16.5	2	456	33	

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/24/2024	00:00	0	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	3	41.7	45	35 to 45	100.0	*	*	0	3	0	
8/24/2024	01:00	0	0	0	0	0	2	2	0	1	0	0	0	0	0	0	0	0	0	5	42.2	43	33 to 43	80.0	1	20.0	0	5	0	
8/24/2024	02:00	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	44.5	48	38 to 48	100.0	1	50.0	0	2	0	
8/24/2024	03:00	0	0	0	1	2	2	0	0	0	0	0	0	0	0	0	0	0	0	5	32.6	34.5	25 to 35	100.0	*	*	0	5	0	
8/24/2024	04:00	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3	35.7	37	27 to 37	100.0	*	*	0	3	0	
8/24/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	06:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	44	44	34 to 44	100.0	*	*	0	1	0	
8/24/2024	07:00	0	0	0	0	1	5	5	3	0	0	0	0	0	0	0	0	0	0	14	40.4	45	33 to 43	78.6	2	14.3	0	13	1	
8/24/2024	08:00	0	0	0	1	0	9	3	3	1	0	0	0	0	0	0	0	0	0	17	40	45	35 to 45	76.5	3	17.6	0	17	0	
8/24/2024	09:00	0	0	0	0	3	6	15	11	2	0	0	0	0	0	0	0	0	0	37	42.2	46.3	37 to 47	78.4	8	21.6	0	37	0	
8/24/2024	10:00	0	0	0	1	2	8	9	8	1	1	0	0	0	0	0	0	0	0	30	41.7	47	37 to 47	70.0	8	26.7	0	29	1	
8/24/2024	11:00	0	0	0	1	4	15	15	6	2	0	0	0	0	0	0	0	0	0	43	40.5	46	34 to 44	76.7	7	16.3	0	42	1	
8/24/2024	12:00	0	0	1	1	3	13	21	8	5	0	0	0	0	0	0	0	0	0	52	40.9	46	35 to 45	73.1	9	17.3	0	52	0	
8/24/2024	13:00	0	0	0	1	3	13	17	6	2	1	0	0	0	0	0	0	0	0	43	41	45.5	34 to 44	76.7	7	16.3	0	42	1	
8/24/2024	14:00	0	0	0	0	7	14	12	9	2	0	0	0	0	0	0	0	0	0	44	40	45.7	35 to 45	63.6	9	20.5	0	43	1	
8/24/2024	15:00	0	0	0	0	2	26	20	9	2	0	0	0	0	0	0	0	0	0	59	40.6	44.4	35 to 45	86.4	6	10.2	0	58	1	
8/24/2024	16:00	0	0	0	1	11	7	15	3	0	0	0	0	0	0	0	0	0	0	37	38.5	43	33 to 43	75.7	1	2.7	0	37	0	
8/24/2024	17:00	0	0	0	0	4	7	13	9	2	0	0	0	0	0	0	0	0	0	35	41.8	46	38 to 48	77.1	7	20.0	0	35	0	
8/24/2024	18:00	0	0	0	1	8	17	20	2	1	0	0	0	0	0	0	0	0	0	49	39	43.3	34 to 44	81.6	3	6.1	1	46	2	
8/24/2024	19:00	0	0	0	2	7	13	14	7	3	0	0	0	0	0	0	0	0	0	46	39.8	47	33 to 43	65.2	9	19.6	0	42	4	
8/24/2024	20:00	0	0	0	0	5	9	5	3	2	0	0	0	0	0	0	0	0	0	24	39.3	46	31 to 41	75.0	5	20.8	0	24	0	
8/24/2024	21:00	0	0	0	1	4	6	5	1	1	0	0	0	0	0	0	0	0	0	18	38.4	43	33 to 43	72.2	2	11.1	0	18	0	
8/24/2024	22:00	0	0	0	2	1	3	2	2	0	0	0	0	0	0	0	0	0	0	10	36.8	41	25 to 35	50.0	2	20.0	0	10	0	
8/24/2024	23:00	0	0	0	1	3	6	1	1	0	0	0	0	0	0	0	0	0	0	12	36.6	39	28 to 38	75.0	1	8.3	0	12	0	
24 Hr Summary		0	0	1	14	71	184	197	93	27	2	0	0	0	0	0	0	0	0	589	40.2	46	35 to 45	69.9	91	15.4	1	576	12	

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/25/2024	00:00	0	0	0	0	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5	38.2	39	33 to 43	100.0	*	*	0	5	0	
8/25/2024	01:00	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	5	34.6	38	29 to 39	100.0	*	*	0	5	0	
8/25/2024	02:00	0	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3	37.7	42	32 to 42	100.0	*	*	0	3	0	
8/25/2024	03:00	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	27.5	35	10 to 20	50.0	*	*	0	2	0	
8/25/2024	04:00	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	3	37.3	40	30 to 40	100.0	*	*	0	3	0	
8/25/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	07:00	0	0	0	0	2	3	3	1	0	0	0	0	0	0	0	0	0	0	9	39.3	44	34 to 44	66.7	1	11.1	0	9	0	
8/25/2024	08:00	0	0	0	0	2	6	6	4	2	0	0	0	0	0	0	0	0	0	20	41.2	46	34 to 44	65.0	5	25.0	0	20	0	
8/25/2024	09:00	0	0	0	0	2	7	6	6	0	1	0	0	0	0	0	0	0	0	22	41.6	46	36 to 46	77.3	4	18.2	0	22	0	
8/25/2024	10:00	0	0	0	0	0	6	9	6	2	1	0	0	0	0	0	0	0	0	24	43.2	46.5	37 to 47	79.2	8	33.3	0	24	0	
8/25/2024	11:00	0	0	0	0	2	11	15	5	1	0	0	0	0	0	0	0	0	0	34	40.9	45	34 to 44	79.4	5	14.7	0	31	3	
8/25/2024	12:00	0	0	0	0	5	16	24	8	1	0	0	0	0	0	0	0	0	0	54	40.6	44.5	34 to 44	77.8	7	13.0	0	53	1	
8/25/2024	13:00	0	0	0	0	8	17	21	12	0	1	0	0	0	0	0	0	0	0	59	40.6	45	37 to 47	76.3	9	15.3	0	58	1	
8/25/2024	14:00	0	0	0	0	7	23	23	20	4	0	1	0	0	0	0	0	0	0	78	41.9	47.5	39 to 49	67.9	21	26.9	0	78	0	
8/25/2024	15:00	0	0	0	0	1	16	28	11	1	0	0	0	0	0	0	0	0	0	57	41.4	44.5	35 to 45	87.7	6	10.5	0	54	3	
8/25/2024	16:00	1	0	0	0	4	20	12	9	3	0	0	0	0	0	0	0	0	0	49	40	46	35 to 45	73.5	8	16.3	0	47	2	
8/25/2024	17:00	0	0	1	1	1	21	17	11	0	2	0	0	0	0	0	0	0	0	54	40.9	45.7	36 to 46	81.5	12	22.2	0	54	0	
8/25/2024	18:00	0	0	0	0	2	13	11	10	1	2	0	0	0	0	0	0	0	0	39	42.2	46.7	37 to 47	76.9	11	28.2	0	39	0	
8/25/2024	19:00	0	0	0	0	2	15	11	11	3	0	0	0	0	0	0	0	0	0	42	41.8	46.5	38 to 48	81.0	12	28.6	0	41	1	
8/25/2024	20:00	0	0	0	3	6	4	12	3	2	0	0	0	0	0	0	0	0	0	30	39	46	38 to 48	60.0	5	16.7	0	30	0	
8/25/2024	21:00	0	0	0	1	5	9	6	3	2	0	0	0	0	0	0	0	0	0	26	38.9	44.5	34 to 44	65.4	3	11.5	0	25	1	
8/25/2024	22:00	0	0	0	0	0	3	5	1	0	0	0	0	0	0	0	0	0	0	9	40.9	44	34 to 44	88.9	1	11.1	0	8	1	
8/25/2024	23:00	0	0	0	0	5	3	4	1	0	0	0	0	0	0	0	0	0	0	13	37.9	42	32 to 42	84.6	1	7.7	0	13	0	
24 Hr Summary		1	0	2	5	58	203	216	122	22	7	1	0	0	0	0	0	0	0	637	40.8	46	36 to 46	72.1	119	18.7	0	624	13	

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/26/2024	00:00	0	0	1	0	2	2	2	0	0	1	0	0	0	0	0	0	0	0	8	37.4	42	30 to 40	62.5	1	12.5	0	8	0	
8/26/2024	01:00	0	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5	39.4	41	34 to 44	100.0	*	*	0	5	0	
8/26/2024	02:00	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	45	46	36 to 46	100.0	1	50.0	0	2	0	
8/26/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	05:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	41	41	31 to 41	100.0	*	*	0	1	0	
8/26/2024	06:00	0	0	0	0	0	2	9	3	2	0	0	0	0	0	0	0	0	0	16	43.9	49	39 to 49	87.5	4	25.0	0	16	0	
8/26/2024	07:00	0	0	0	0	1	5	11	1	1	0	0	0	0	0	0	0	0	0	19	40.6	43	35 to 45	89.5	1	5.3	0	19	0	
8/26/2024	08:00	0	0	0	0	3	10	13	2	0	1	0	0	0	0	0	0	0	0	29	40.1	43.5	34 to 44	89.7	2	6.9	0	29	0	
8/26/2024	09:00	0	0	0	5	6	10	11	7	1	0	0	0	0	0	0	0	0	0	40	38.4	45	37 to 47	65.0	6	15.0	0	37	3	
8/26/2024	10:00	1	1	1	2	2	12	15	15	1	1	0	0	0	0	0	0	0	0	51	40.2	45.9	36 to 46	70.6	14	27.5	1	44	6	
8/26/2024	11:00	0	0	1	1	8	10	16	10	1	0	0	0	0	0	0	0	0	0	47	39.8	45.8	37 to 47	66.0	10	21.3	0	41	6	
8/26/2024	12:00	0	0	1	1	2	17	19	6	2	1	0	0	0	0	0	0	0	0	49	40.6	46	35 to 45	75.5	8	16.3	0	41	8	
8/26/2024	13:00	0	0	1	1	3	16	20	10	1	1	0	0	0	0	0	0	0	0	53	40.8	44.8	35 to 45	77.4	7	13.2	0	49	4	
8/26/2024	14:00	0	0	0	1	4	20	19	6	1	1	0	0	0	0	0	0	0	0	52	40	44	34 to 44	78.8	7	13.5	1	46	5	
8/26/2024	15:00	0	0	0	1	6	29	30	11	2	0	0	0	0	0	0	0	0	0	79	40.1	44.5	34 to 44	79.7	11	13.9	0	76	3	
8/26/2024	16:00	0	0	1	2	4	25	18	11	1	0	0	0	0	0	0	0	0	0	62	39.5	45.2	36 to 46	75.8	10	16.1	0	56	6	
8/26/2024	17:00	1	1	1	1	6	24	23	6	1	0	0	0	0	0	0	0	0	0	64	38.7	43.5	34 to 44	78.1	7	10.9	0	63	1	
8/26/2024	18:00	0	0	1	0	6	18	14	13	1	0	0	0	0	0	0	0	0	0	53	40.3	45.8	36 to 46	69.8	11	20.8	0	53	0	
8/26/2024	19:00	0	0	0	0	1	15	10	13	3	0	0	0	0	0	0	0	0	0	42	42.2	47	37 to 47	69.0	16	38.1	0	42	0	
8/26/2024	20:00	0	0	1	0	5	13	13	1	0	0	0	0	0	0	0	0	0	0	33	38	41.5	34 to 44	84.8	1	3.0	0	33	0	
8/26/2024	21:00	0	0	0	0	3	12	12	7	0	0	0	0	0	0	0	0	0	0	34	40.6	46	37 to 47	85.3	6	17.6	0	34	0	
8/26/2024	22:00	0	0	0	0	5	5	3	1	0	0	0	0	0	0	0	0	0	0	14	37.6	43.5	34 to 44	78.6	1	7.1	0	14	0	
8/26/2024	23:00	0	0	0	0	1	6	2	1	0	0	0	0	0	0	0	0	0	0	10	38.5	41	31 to 41	80.0	1	10.0	0	10	0	
24 Hr Summary		2	2	9	15	68	254	264	125	18	6	0	0	0	0	0	0	0	0	763	40	46	34 to 44	71.6	125	16.4	2	719	42	

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/27/2024	00:00	0	0	0	0	3	1	1	0	0	0	0	0	0	0	0	0	0	0	5	34.6	35	25 to 35	80.0	*	*	0	5	0	
8/27/2024	01:00	0	0	0	1	0	1	0	0	1	0	0	0	0	0	0	0	0	0	3	37	50	25 to 35	66.7	1	33.3	0	3	0	
8/27/2024	02:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34.5	36	26 to 36	100.0	*	*	0	2	0	
8/27/2024	03:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	32.5	37	27 to 37	100.0	*	*	0	2	0	
8/27/2024	04:00	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	28	35	15 to 25	66.7	*	*	0	3	0	
8/27/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	06:00	0	0	0	0	0	3	4	4	1	0	0	0	0	0	0	0	0	0	12	43.6	45.7	36 to 46	83.3	4	33.3	0	12	0	
8/27/2024	07:00	0	0	0	0	2	5	12	7	1	0	0	0	0	0	0	0	0	0	27	42.1	46	36 to 46	77.8	5	18.5	1	25	1	
8/27/2024	08:00	0	0	0	3	2	6	18	11	3	2	0	0	0	0	0	0	0	0	45	42.3	47	37 to 47	71.1	14	31.1	0	42	3	
8/27/2024	09:00	0	0	0	2	6	13	13	6	2	0	0	0	0	0	0	0	0	0	42	39.3	45	34 to 44	71.4	6	14.3	0	37	5	
8/27/2024	10:00	0	0	0	1	5	8	12	5	1	0	0	0	0	0	0	0	0	0	32	39.6	45	34 to 44	65.6	5	15.6	0	29	3	
8/27/2024	11:00	0	0	0	0	1	17	20	3	2	0	0	0	0	0	0	0	0	0	43	41	43.9	34 to 44	86.0	5	11.6	0	35	8	
8/27/2024	12:00	0	0	0	1	4	16	15	4	1	1	1	0	0	0	0	0	0	0	43	40.2	44.5	34 to 44	76.7	5	11.6	0	38	5	
8/27/2024	13:00	0	0	0	0	2	20	27	7	1	0	0	0	0	0	0	0	0	0	57	40.4	43.8	35 to 45	89.5	4	7.0	0	53	4	
8/27/2024	14:00	0	0	0	0	3	14	16	9	0	2	0	0	0	0	0	0	0	0	44	41.5	45	35 to 45	77.3	7	15.9	0	43	1	
8/27/2024	15:00	0	0	0	2	6	13	14	5	5	2	0	0	0	0	0	0	0	0	47	40.9	49	33 to 43	63.8	10	21.3	1	38	8	
8/27/2024	16:00	0	0	0	0	4	13	22	9	5	0	0	0	0	0	0	0	0	0	53	41.9	46.3	36 to 46	75.5	12	22.6	1	51	1	
8/27/2024	17:00	0	0	3	4	6	13	19	8	6	0	0	0	0	0	0	0	0	0	59	39.3	46.5	35 to 45	61.0	10	16.9	1	54	4	
8/27/2024	18:00	0	0	0	2	10	25	28	9	2	1	0	0	0	0	0	0	0	0	77	39.7	44	36 to 46	74.0	10	13.0	0	72	5	
8/27/2024	19:00	0	0	0	0	7	20	37	12	4	2	0	0	0	0	0	0	0	0	82	41.7	46	35 to 45	75.6	13	15.9	0	81	1	
8/27/2024	20:00	0	0	1	0	2	19	12	10	2	0	0	0	0	0	0	0	0	0	46	40.9	45.2	36 to 46	82.6	8	17.4	0	45	1	
8/27/2024	21:00	0	0	0	0	3	8	14	4	1	0	0	0	0	0	0	0	0	0	30	40.4	44.3	35 to 45	83.3	2	6.7	0	28	2	
8/27/2024	22:00	0	0	0	0	5	1	3	1	0	0	0	0	0	0	0	0	0	0	10	37.5	42	32 to 42	70.0	1	10.0	0	10	0	
8/27/2024	23:00	0	1	0	0	3	3	1	0	0	1	0	0	0	0	0	0	0	0	9	36.8	44	28 to 38	66.7	1	11.1	0	9	0	
24 Hr Summary		0	1	5	18	75	222	288	114	38	11	1	0	0	0	0	0	0	0	773	40.5	46	35 to 45	71.3	123	15.9	4	717	52	

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/28/2024	00:00	0	0	0	0	2	2	1	0	0	0	0	0	0	0	0	0	0	0	5	37.2	39	31 to 41	100.0	*	*	0	5	0	
8/28/2024	01:00	0	0	0	0	2	1	0	0	0	0	1	0	0	0	0	0	0	0	4	40	37	27 to 37	75.0	1	25.0	0	4	0	
8/28/2024	02:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	34	34	24 to 34	100.0	*	*	0	1	0	
8/28/2024	03:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	38	38	28 to 38	100.0	*	*	0	1	0	
8/28/2024	04:00	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	3	36.3	47	37 to 47	66.7	1	33.3	0	3	0	
8/28/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	06:00	0	0	0	1	0	2	5	4	0	0	0	0	0	0	0	0	0	0	12	41.4	45.5	36 to 46	75.0	3	25.0	0	12	0	
8/28/2024	07:00	0	0	0	0	1	5	9	7	0	0	0	0	0	0	0	0	0	0	22	41.8	47	35 to 45	72.7	5	22.7	0	21	1	
8/28/2024	08:00	0	0	0	5	5	13	21	9	3	0	0	0	0	0	0	0	0	0	56	39.9	45.5	37 to 47	69.6	9	16.1	0	54	2	
8/28/2024	09:00	0	0	3	2	3	16	11	5	3	0	0	0	0	0	0	0	0	0	43	38.2	45	35 to 45	67.4	6	14.0	0	37	6	
8/28/2024	10:00	0	0	0	0	5	13	14	7	0	0	0	0	0	0	0	0	0	0	39	40.4	45	34 to 44	76.9	6	15.4	0	39	0	
8/28/2024	11:00	0	0	0	0	4	10	14	6	3	0	0	0	0	0	0	0	0	0	37	41.4	46	35 to 45	70.3	7	18.9	0	33	4	
8/28/2024	12:00	0	0	0	0	6	13	18	11	3	0	0	0	0	0	0	0	0	0	51	41.2	46	36 to 46	70.6	10	19.6	0	47	4	
8/28/2024	13:00	0	0	0	1	11	14	10	5	1	0	0	0	0	0	0	0	0	0	42	38.5	44	33 to 43	76.2	5	11.9	1	36	5	
8/28/2024	14:00	0	0	0	0	7	18	22	14	4	0	0	0	0	0	0	0	0	0	65	40.9	44.9	35 to 45	75.4	9	13.8	0	57	8	
8/28/2024	15:00	0	0	0	1	13	17	25	6	2	0	0	0	0	0	0	0	0	0	64	39	43.7	34 to 44	67.2	7	10.9	0	57	7	
8/28/2024	16:00	0	0	0	2	10	20	29	11	3	0	1	0	0	0	0	0	0	0	76	40.3	45.3	34 to 44	71.1	12	15.8	0	69	7	
8/28/2024	17:00	0	0	3	3	8	21	20	11	4	0	0	0	0	0	0	0	0	0	70	39.1	46	34 to 44	61.4	13	18.6	0	64	6	
8/28/2024	18:00	0	0	0	1	1	13	23	19	1	2	0	0	0	0	0	0	0	0	60	42.6	46.9	38 to 48	80.0	17	28.3	0	57	3	
8/28/2024	19:00	0	1	0	0	9	12	26	12	3	0	1	0	0	0	0	0	0	0	64	41.5	46.5	39 to 49	70.3	13	20.3	0	62	2	
8/28/2024	20:00	0	0	0	2	6	11	14	6	1	0	0	0	1	0	0	0	0	0	41	40.3	46	33 to 43	68.3	7	17.1	0	41	0	
8/28/2024	21:00	0	0	0	0	3	8	12	6	0	0	0	0	0	0	0	0	0	0	29	40.6	46	36 to 46	75.9	5	17.2	0	29	0	
8/28/2024	22:00	0	0	0	0	1	8	3	2	0	0	0	0	0	0	0	0	0	0	14	39.3	42	35 to 45	85.7	1	7.1	0	14	0	
8/28/2024	23:00	0	0	0	1	6	3	2	0	0	0	0	0	0	0	0	0	0	0	12	35.3	38	28 to 38	75.0	*	*	0	12	0	
24 Hr Summary		0	2	6	19	104	221	280	142	31	2	3	0	1	0	0	0	0	0	811	40.2	46	35 to 45	67.0	137	16.9	1	755	55	

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG		
8/29/2024	00:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	39	39	29 to 39	100.0	*	*	0	1	0	
8/29/2024	01:00	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	33.7	40	23 to 33	66.7	*	*	0	2	1	
8/29/2024	02:00	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	38.3	44	28 to 38	66.7	*	*	0	3	0	
8/29/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	0	1	
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	06:00	0	0	0	0	1	3	5	0	1	0	1	0	0	0	0	0	0	0	0	11	42.5	44	34 to 44	81.8	2	18.2	0	11	0	
8/29/2024	07:00	0	0	0	0	1	2	5	1	1	0	0	0	0	0	0	0	0	0	0	10	42.1	44	38 to 48	80.0	2	20.0	0	9	1	
8/29/2024	08:00	0	0	0	1	6	11	23	8	2	0	0	0	0	0	0	0	0	0	0	51	40.3	44.7	35 to 45	72.5	7	13.7	0	49	2	
8/29/2024	09:00	0	0	0	3	4	12	15	11	0	1	0	0	0	0	0	0	0	0	0	46	40.2	45	36 to 46	78.3	7	15.2	0	41	5	
8/29/2024	10:00	0	0	1	0	9	19	4	4	2	0	0	0	0	0	0	0	0	0	0	39	38	44	29 to 39	71.8	3	7.7	0	36	3	
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		0	0	1	6	23	49	54	24	6	1	1	0	0	0	0	0	0	0	0	165	39.8	45	36 to 46	70.3	21	12.7	0	152	13	

Incoming: Average Hourly Volume for Week of 8/19/2024
Average Counts By Hour (8/19/2024) — Average Counts By Hour (8/19/2024)

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr
Average Weekly Volumes



Incoming:Average Hourly WEEKDAY Speeds for Week of 8/19/2024
Average Hourly WEEKDAY Speeds By Hour (8/19/2024) — Average Hourly WEEKDAY Speeds By Hour (8/19/2024)

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr
Average Weekly Speeds



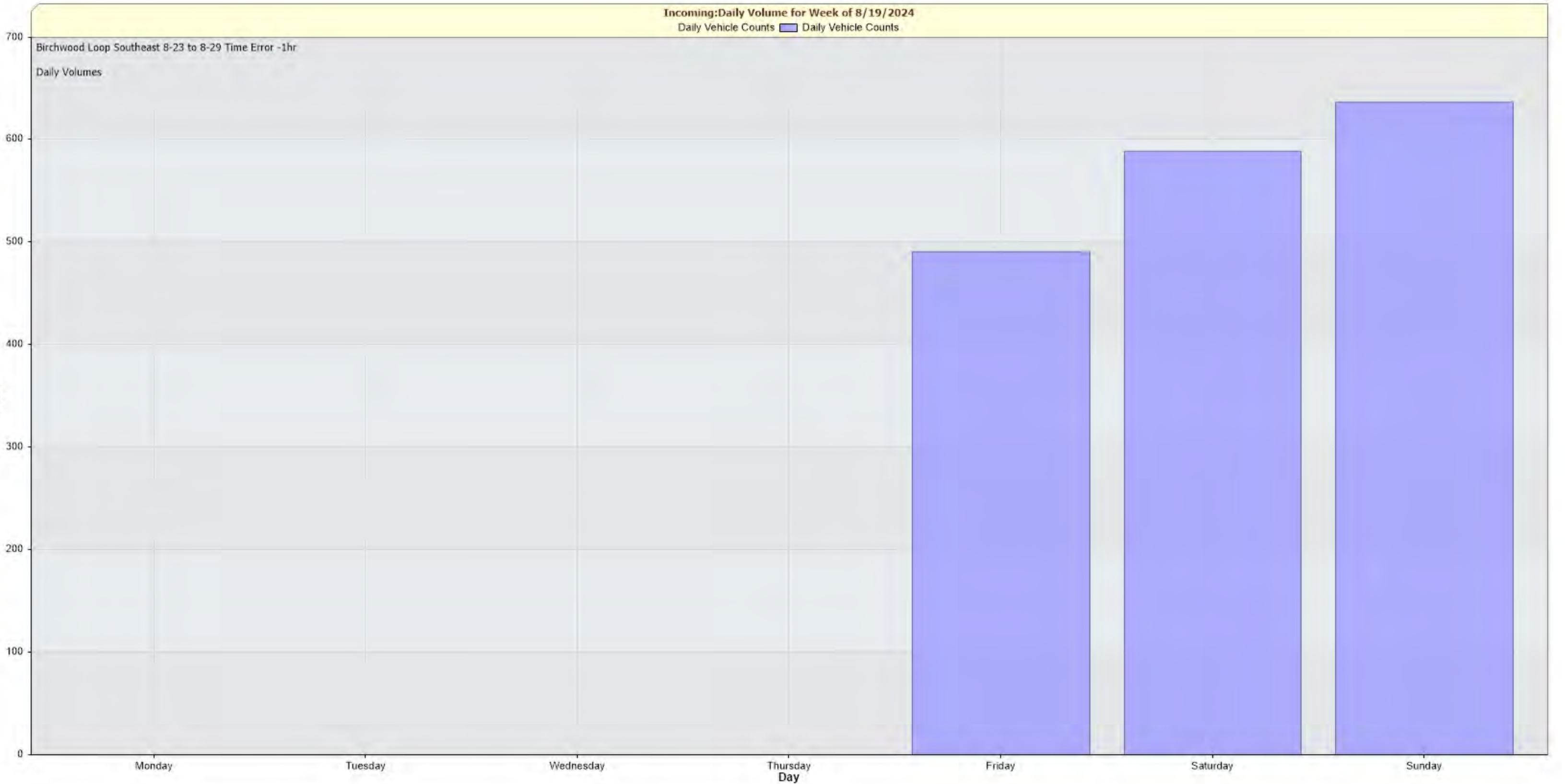
Incoming:Daily Volume for Week of 8/19/2024

Daily Vehicle Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

Daily Volumes

Vehicles



Monday

Tuesday

Wednesday

Thursday
Day

Friday

Saturday

Sunday

For Project: Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:24:31 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 8/23/2024 1:00:00 PM through 8/29/2024 10:59:59 AM

85th Percentile Speed 45 MPH

85th Percentile Vehicles 3730

Max Speed 79 MPH on 8/25/2024 4:35:30 PM

Total Vehicles 4388

AADT: 741

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	626	626
AM Peak 9:00 AM	40	37
PM Peak 4:00 PM	77	73

Speed

Speed Limit: 45

85th Percentile Speed: 45

50th Percentile Speed: 38

10 MPH Pace Interval: 35.0 MPH to 45.0 MPH

Average Speed: 37.79

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	90	88	95	15	87	73	87
% over limit	11.9	10.9	11.6	10.0	14.6	11.9	13.5
Avg Speeder	48.2	48.4	48.6	48.7	48.4	48.3	48.9
Avg Speed	37.6	37.6	37.8	35.3	38.3	37.5	38.7

Class Counts

	Number	%
VEH_SM	8	0.2
VEH_MED	4155	94.7
VEH_LG	225	5.1
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/23/2024 2:00:00 PM	45.0	37	43	53	49.8	11.6%	37.4
8/23/2024 3:00:00 PM	46.0	50	59	52	47.8	20.3%	39.7
8/23/2024 4:00:00 PM	45.0	69	81	53	48.5	14.8%	37.7
8/23/2024 5:00:00 PM	44.0	90	106	57	48.7	9.4%	38.3
8/23/2024 6:00:00 PM	46.0	62	73	52	47.9	16.4%	38.7
8/23/2024 7:00:00 PM	46.0	60	71	58	48.9	16.9%	39.4
8/23/2024 8:00:00 PM	45.0	48	57	55	48.4	15.8%	39.0
8/23/2024 9:00:00 PM	44.0	33	39	52	48.3	7.7%	37.1
8/23/2024 10:00:00 PM	47.0	20	24	52	48.3	29.2%	38.2
8/23/2024 11:00:00 PM	43.0	25	29	48	46.7	10.3%	34.8
8/24/2024 12:00:00 AM	44.0	13	15	49	47.5	13.3%	37.9
8/24/2024 1:00:00 AM	41.0	7	8	48	48.0	12.5%	34.3
8/24/2024 2:00:00 AM	46.0	3	3	46	46.0	33.3%	38.7
8/24/2024 3:00:00 AM	41.0	3	4	42	0.0	0.0%	34.8
8/24/2024 4:00:00 AM	41.0	2	2	41	0.0	0.0%	40.5
8/24/2024 5:00:00 AM	37.0	2	2	37	0.0	0.0%	33.5
8/24/2024 6:00:00 AM	**No Data**						
8/24/2024 7:00:00 AM	37.0	2	2	37	0.0	0.0%	33.0
8/24/2024 8:00:00 AM	46.0	8	9	47	46.5	22.2%	38.3
8/24/2024 9:00:00 AM	42.0	10	12	44	0.0	0.0%	36.8
8/24/2024 10:00:00 AM	43.0	20	24	47	46.7	12.5%	37.6
8/24/2024 11:00:00 AM	44.0	27	32	48	47.0	9.4%	37.7
8/24/2024 12:00:00 PM	45.0	33	39	49	47.2	12.8%	37.6
8/24/2024 1:00:00 PM	44.0	44	52	54	48.3	11.5%	36.2
8/24/2024 2:00:00 PM	45.0	46	54	57	50.1	14.8%	38.2
8/24/2024 3:00:00 PM	42.0	41	48	47	46.5	8.3%	36.6
8/24/2024 4:00:00 PM	44.0	55	65	49	47.0	7.7%	37.6
8/24/2024 5:00:00 PM	45.0	50	59	50	47.5	13.6%	38.1
8/24/2024 6:00:00 PM	43.0	37	44	52	49.0	6.8%	38.3
8/24/2024 7:00:00 PM	44.0	34	40	51	48.5	9.8%	38.6
8/24/2024 8:00:00 PM	47.0	23	27	55	49.7	22.2%	36.4
8/24/2024 9:00:00 PM	40.0	26	31	49	47.7	9.7%	35.0
8/24/2024 10:00:00 PM	43.0	21	25	55	50.0	12.0%	37.2
8/24/2024 11:00:00 PM	42.0	12	14	46	46.0	14.3%	37.6
8/25/2024 12:00:00 AM	49.0	13	15	60	51.2	40.0%	43.6
8/25/2024 1:00:00 AM	29.0	3	4	53	53.0	25.0%	34.8
8/25/2024 2:00:00 AM	57.0	1	1	57	57.0	100.0%	57.0
8/25/2024 3:00:00 AM	35.0	2	2	35	0.0	0.0%	33.5
8/25/2024 4:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
8/25/2024 5:00:00 AM	38.0	3	4	43	0.0	0.0%	35.5
8/25/2024 6:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
8/25/2024 7:00:00 AM	**No Data**						
8/25/2024 8:00:00 AM	43.0	6	7	44	0.0	0.0%	37.6
8/25/2024 9:00:00 AM	47.0	5	6	47	47.0	33.3%	38.0
8/25/2024 10:00:00 AM	46.0	17	20	54	48.0	20.0%	37.7
8/25/2024 11:00:00 AM	41.0	24	28	50	48.0	7.1%	36.7
8/25/2024 12:00:00 PM	44.0	27	32	52	50.7	9.4%	39.3

Outgoing Summary
 Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

8/25/2024 1:00:00 PM	45.0	37	44	57	51.2	13.6%	40.6
8/25/2024 2:00:00 PM	46.0	36	42	54	49.6	19.0%	38.9
8/25/2024 3:00:00 PM	46.0	47	55	53	47.8	21.8%	39.6
8/25/2024 4:00:00 PM	44.0	43	51	52	48.5	11.8%	38.1
8/25/2024 5:00:00 PM	45.0	60	70	79	52.1	10.0%	39.2
8/25/2024 6:00:00 PM	43.0	46	54	51	47.8	7.4%	37.2
8/25/2024 7:00:00 PM	45.0	64	75	52	48.1	12.0%	39.2
8/25/2024 8:00:00 PM	46.0	40	47	53	47.8	25.5%	39.7
8/25/2024 9:00:00 PM	44.0	39	46	52	48.3	8.7%	37.2
8/25/2024 10:00:00 PM	46.0	19	22	49	47.3	18.2%	38.7
8/25/2024 11:00:00 PM	43.0	15	18	45	0.0	0.0%	37.7
8/26/2024 12:00:00 AM	45.0	11	13	53	50.0	15.4%	39.5
8/26/2024 1:00:00 AM	34.0	2	2	34	0.0	0.0%	32.0
8/26/2024 2:00:00 AM	39.0	2	2	39	0.0	0.0%	32.5
8/26/2024 3:00:00 AM	28.0	2	2	28	0.0	0.0%	27.5
8/26/2024 4:00:00 AM	34.0	2	2	34	0.0	0.0%	33.0
8/26/2024 5:00:00 AM	19.0	1	1	19	0.0	0.0%	19.0
8/26/2024 6:00:00 AM	34.0	1	1	34	0.0	0.0%	34.0
8/26/2024 7:00:00 AM	45.0	5	6	52	52.0	16.7%	38.8
8/26/2024 8:00:00 AM	47.0	14	16	52	47.6	31.3%	40.1
8/26/2024 9:00:00 AM	44.0	22	26	52	49.5	7.7%	37.2
8/26/2024 10:00:00 AM	46.0	31	37	53	50.4	18.9%	37.6
8/26/2024 11:00:00 AM	43.0	45	53	47	46.3	5.7%	36.8
8/26/2024 12:00:00 PM	44.0	32	38	49	48.0	7.9%	36.7
8/26/2024 1:00:00 PM	44.0	44	52	48	46.7	11.5%	37.2
8/26/2024 2:00:00 PM	44.0	42	49	49	46.8	8.2%	37.9
8/26/2024 3:00:00 PM	46.0	45	53	52	48.2	17.0%	39.1
8/26/2024 4:00:00 PM	42.0	58	68	55	49.8	7.4%	36.4
8/26/2024 5:00:00 PM	46.0	68	80	58	49.0	17.5%	38.4
8/26/2024 6:00:00 PM	45.0	60	70	51	47.6	12.9%	38.6
8/26/2024 7:00:00 PM	45.0	64	75	50	47.1	10.7%	37.3
8/26/2024 8:00:00 PM	46.0	39	46	51	47.8	17.4%	38.8
8/26/2024 9:00:00 PM	44.0	18	21	46	46.0	9.5%	36.5
8/26/2024 10:00:00 PM	41.0	18	21	49	48.5	9.5%	37.3
8/26/2024 11:00:00 PM	42.0	19	22	43	0.0	0.0%	37.5
8/27/2024 12:00:00 AM	45.0	12	14	50	48.5	14.3%	39.0
8/27/2024 1:00:00 AM	42.0	3	4	47	47.0	25.0%	38.3
8/27/2024 2:00:00 AM	26.0	2	2	26	0.0	0.0%	22.0
8/27/2024 3:00:00 AM	33.0	3	4	36	0.0	0.0%	32.5
8/27/2024 4:00:00 AM	41.0	2	2	41	0.0	0.0%	37.5
8/27/2024 5:00:00 AM	35.0	2	2	35	0.0	0.0%	31.0
8/27/2024 6:00:00 AM	40.0	1	1	40	0.0	0.0%	40.0
8/27/2024 7:00:00 AM	48.0	10	12	54	51.0	25.0%	37.9
8/27/2024 8:00:00 AM	41.0	14	17	46	46.0	5.9%	36.3
8/27/2024 9:00:00 AM	47.0	27	32	54	49.5	18.8%	37.7
8/27/2024 10:00:00 AM	44.0	34	40	48	47.0	10.0%	35.8
8/27/2024 11:00:00 AM	42.0	22	26	49	47.0	11.5%	34.4
8/27/2024 12:00:00 PM	42.0	37	43	48	48.0	2.3%	37.0

Outgoing Summary
 Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

8/27/2024 1:00:00 PM	45.0	45	53	54	48.8	15.1%	40.0
8/27/2024 2:00:00 PM	44.0	41	48	49	48.0	10.4%	37.3
8/27/2024 3:00:00 PM	45.0	42	49	53	47.8	12.2%	38.0
8/27/2024 4:00:00 PM	42.0	44	52	48	47.0	7.7%	35.5
8/27/2024 5:00:00 PM	46.0	55	65	53	48.8	21.5%	38.1
8/27/2024 6:00:00 PM	44.0	66	78	55	49.3	9.0%	38.4
8/27/2024 7:00:00 PM	45.0	55	65	53	48.7	13.8%	37.8
8/27/2024 8:00:00 PM	45.0	49	58	50	47.6	8.6%	38.0
8/27/2024 9:00:00 PM	42.0	26	30	47	47.0	3.3%	36.8
8/27/2024 10:00:00 PM	44.0	63	74	49	47.5	5.4%	39.1
8/27/2024 11:00:00 PM	41.0	36	42	51	47.5	9.5%	38.6
8/28/2024 12:00:00 AM	43.0	8	10	52	50.5	20.0%	36.3
8/28/2024 1:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
8/28/2024 2:00:00 AM	30.0	1	1	30	0.0	0.0%	30.0
8/28/2024 3:00:00 AM	36.0	2	2	36	0.0	0.0%	35.5
8/28/2024 4:00:00 AM	**No Data**						
8/28/2024 5:00:00 AM	35.0	5	6	37	0.0	0.0%	29.8
8/28/2024 6:00:00 AM	38.0	2	2	38	0.0	0.0%	34.5
8/28/2024 7:00:00 AM	42.0	9	11	47	46.5	18.2%	38.5
8/28/2024 8:00:00 AM	44.0	14	16	49	49.0	6.3%	37.3
8/28/2024 9:00:00 AM	45.0	29	34	53	50.8	14.7%	37.4
8/28/2024 10:00:00 AM	44.0	30	35	53	48.6	14.3%	37.6
8/28/2024 11:00:00 AM	44.0	34	40	52	49.3	7.5%	36.6
8/28/2024 12:00:00 PM	44.0	30	35	46	46.0	2.9%	38.0
8/28/2024 1:00:00 PM	42.0	42	49	50	49.5	4.1%	37.4
8/28/2024 2:00:00 PM	43.0	36	42	58	50.3	9.5%	36.2
8/28/2024 3:00:00 PM	43.0	47	55	50	47.7	10.9%	37.7
8/28/2024 4:00:00 PM	45.0	63	74	59	49.9	13.5%	36.6
8/28/2024 5:00:00 PM	43.0	50	59	51	49.3	11.9%	36.6
8/28/2024 6:00:00 PM	45.0	63	74	52	48.6	13.5%	39.1
8/28/2024 7:00:00 PM	46.0	65	76	51	47.6	19.7%	40.1
8/28/2024 8:00:00 PM	45.0	46	54	52	47.7	13.0%	38.5
8/28/2024 9:00:00 PM	46.0	48	57	49	47.2	17.5%	38.2
8/28/2024 10:00:00 PM	44.0	42	50	50	47.8	10.0%	38.2
8/28/2024 11:00:00 PM	43.0	27	32	45	0.0	0.0%	37.9
8/29/2024 12:00:00 AM	45.0	14	16	54	53.0	12.5%	37.8
8/29/2024 1:00:00 AM	40.0	6	7	47	47.0	14.3%	34.3
8/29/2024 2:00:00 AM	53.0	2	2	53	53.0	50.0%	43.5
8/29/2024 3:00:00 AM	41.0	2	2	41	0.0	0.0%	33.0
8/29/2024 4:00:00 AM	**No Data**						
8/29/2024 5:00:00 AM	**No Data**						
8/29/2024 6:00:00 AM	36.0	2	2	36	0.0	0.0%	34.0
8/29/2024 7:00:00 AM	41.0	7	8	41	0.0	0.0%	34.5
8/29/2024 8:00:00 AM	45.0	14	17	49	47.0	17.6%	38.5
8/29/2024 9:00:00 AM	46.0	26	31	51	49.2	18.8%	37.4
8/29/2024 10:00:00 AM	42.0	42	50	51	48.5	4.0%	33.9
8/29/2024 11:00:00 AM	38.0	26	31	52	49.0	6.5%	33.8

Outgoing Summary
Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/24/2024 12:00:00 AM	45.0	507	597	58	48.4	14.6%	38.3
8/25/2024 12:00:00 AM	44.0	519	611	60	48.3	11.9%	37.5
8/26/2024 12:00:00 AM	45.0	547	643	79	48.9	13.5%	38.7
8/27/2024 12:00:00 AM	45.0	643	757	58	48.2	11.9%	37.6
8/28/2024 12:00:00 AM	44.0	688	809	55	48.4	10.9%	37.6
8/29/2024 12:00:00 AM	45.0	698	821	59	48.6	11.6%	37.8
8/29/2024 10:59:59 AM	45.0	128	150	53	48.7	10.0%	35.3

Outgoing Weekly Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	8	4	0	6	0
1 - 2	*	*	*	*	*	3	1	0	2	0
2 - 3	*	*	*	*	*	4	2	0	3	0
3 - 4	*	*	*	*	*	2	1	0	1.5	0
4 - 5	*	*	*	*	*	2	4	0	3	0
5 - 6	*	*	*	*	*	0	1	0	0.5	0
6 - 7	*	*	*	*	*	2	0	0	1	0
7 - 8	*	*	*	*	*	9	7	0	8	0
8 - 9	*	*	*	*	*	12	6	0	9	0
9 - 10	*	*	*	*	*	24	20	0	22	0
10 - 11	*	*	*	*	*	32	28	0	30	0
11 - 12	*	*	*	*	*	39	32	0	35.5	0
12 - 13	*	*	*	*	*	52	44	0	48	0
13 - 14	*	*	*	*	43	54	42	43	48	44.5
14 - 15	*	*	*	*	59	48	55	59	51.5	45.8
15 - 16	*	*	*	*	81	65	51	81	58	45
16 - 17	*	*	*	*	106	59	70	106	64.5	43.5
17 - 18	*	*	*	*	73	44	54	73	49	45.3
18 - 19	*	*	*	*	71	40	75	71	57.5	45.3
19 - 20	*	*	*	*	57	27	47	57	37	45
20 - 21	*	*	*	*	39	31	46	39	38.5	43.5
21 - 22	*	*	*	*	24	25	22	24	23.5	47
22 - 23	*	*	*	*	29	14	18	29	16	43
23 - 24	*	*	*	*	15	15	13	15	14	44
Totals	0	0	0	0	597	611	643			
% of Total	0%	0%	0%	0%	32.25%	33.01%	34.74%			

Outgoing Weekly Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	2	4	1	7	*	*	*	3.5	0	36.25
1 - 2	2	2	1	2	*	*	*	1.75	0	37
2 - 3	2	4	2	2	*	*	*	2.5	0	34.5
3 - 4	2	2	0	0	*	*	*	1	0	37.5
4 - 5	1	2	6	0	*	*	*	2.25	0	29.67
5 - 6	1	1	2	2	*	*	*	1.5	0	37
6 - 7	6	12	11	8	*	*	*	9.25	0	43.88
7 - 8	16	17	16	17	*	*	*	16.5	0	44.12
8 - 9	26	32	34	31	*	*	*	30.75	0	45.38
9 - 10	37	40	35	50	*	*	*	40.5	0	43.88
10 - 11	53	26	40	31	*	*	*	37.5	0	41.62
11 - 12	38	43	35	*	*	*	*	38.67	0	43.23
12 - 13	52	53	49	*	*	*	*	51.33	0	43.67
13 - 14	49	48	42	*	*	*	*	46.33	0	43.2
14 - 15	53	49	55	*	*	*	*	52.33	0	44.43
15 - 16	68	52	74	*	*	*	*	64.67	0	42.73
16 - 17	80	65	59	*	*	*	*	68	0	44.7
17 - 18	70	78	74	*	*	*	*	74	0	44.3
18 - 19	75	65	76	*	*	*	*	72	0	44.97
19 - 20	46	58	54	*	*	*	*	52.67	0	44.8
20 - 21	21	30	57	*	*	*	*	36	0	43.73
21 - 22	21	74	50	*	*	*	*	48.33	0	42.63
22 - 23	22	42	32	*	*	*	*	32	0	41.67
23 - 24	14	10	16	*	*	*	*	13.33	0	44.33
Totals	757	809	821	150	0	0	0			
% of Total	29.84%	31.89%	32.36%	5.91%	0%	0%	0%			

Outgoing Monthly Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	Aug 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	2	4	1	7	*	8	4	3.5	6	36.25
1 - 2	2	2	1	2	*	3	1	1.75	2	37
2 - 3	2	4	2	2	*	4	2	2.5	3	34.5
3 - 4	2	2	0	0	*	2	1	1	1.5	37.5
4 - 5	1	2	6	0	*	2	4	2.25	3	29.67
5 - 6	1	1	2	2	*	0	1	1.5	0.5	37
6 - 7	6	12	11	8	*	2	0	9.25	1	43.88
7 - 8	16	17	16	17	*	9	7	16.5	8	44.12
8 - 9	26	32	34	31	*	12	6	30.75	9	45.38
9 - 10	37	40	35	50	*	24	20	40.5	22	43.88
10 - 11	53	26	40	31	*	32	28	37.5	30	41.62
11 - 12	38	43	35	*	*	39	32	38.67	35.5	43.23
12 - 13	52	53	49	*	*	52	44	51.33	48	43.67
13 - 14	49	48	42	*	43	54	42	45.5	48	43.52
14 - 15	53	49	55	*	59	48	55	54	51.5	44.78
15 - 16	68	52	74	*	81	65	51	68.75	58	43.3
16 - 17	80	65	59	*	106	59	70	77.5	64.5	44.4
17 - 18	70	78	74	*	73	44	54	73.75	49	44.55
18 - 19	75	65	76	*	71	40	75	71.75	57.5	45.05
19 - 20	46	58	54	*	57	27	47	53.75	37	44.85
20 - 21	21	30	57	*	39	31	46	36.75	38.5	43.68
21 - 22	21	74	50	*	24	25	22	42.25	23.5	43.72
22 - 23	22	42	32	*	29	14	18	31.25	16	42
23 - 24	14	10	16	*	15	15	13	13.75	14	44.25
Totals	757	809	821	150	597	611	643			
% of Total	17.25%	18.44%	18.71%	3.42%	13.61%	13.92%	14.65%			

Outgoing Weekly Speeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	34.25	34.75	0	34.42	0
1 - 2	*	*	*	*	*	38.67	57	0	43.25	0
2 - 3	*	*	*	*	*	34.75	33.5	0	34.33	0
3 - 4	*	*	*	*	*	40.5	35	0	38.67	0
4 - 5	*	*	*	*	*	33.5	35.5	0	34.83	0
5 - 6	*	*	*	*	*	0	29	0	29	0
6 - 7	*	*	*	*	*	33	0	0	33	0
7 - 8	*	*	*	*	*	38.33	37.57	0	38	0
8 - 9	*	*	*	*	*	36.83	38	0	37.22	0
9 - 10	*	*	*	*	*	37.62	37.7	0	37.66	0
10 - 11	*	*	*	*	*	37.66	36.71	0	37.22	0
11 - 12	*	*	*	*	*	37.64	39.31	0	38.39	0
12 - 13	*	*	*	*	*	36.21	40.59	0	38.22	0
13 - 14	*	*	*	*	37.51	38.22	38.9	37.51	38.52	44.5
14 - 15	*	*	*	*	39.68	36.56	39.58	39.68	38.17	45.8
15 - 16	*	*	*	*	37.65	37.58	38.14	37.65	37.83	45
16 - 17	*	*	*	*	38.29	38.08	39.2	38.29	38.69	43.5
17 - 18	*	*	*	*	38.71	38.32	37.19	38.71	37.69	45.3
18 - 19	*	*	*	*	39.41	38.6	39.23	39.41	39.01	45.3
19 - 20	*	*	*	*	38.96	36.41	39.72	38.96	38.51	45
20 - 21	*	*	*	*	37.13	35	37.24	37.13	36.34	43.5
21 - 22	*	*	*	*	38.21	37.24	38.73	38.21	37.94	47
22 - 23	*	*	*	*	34.76	37.64	37.72	34.76	37.69	43
23 - 24	*	*	*	*	37.93	43.6	39.46	37.93	41.68	44
Totals	0	0	0	0	38.3	37.5	38.7			
% of Total	0%	0%	0%	0%	33.45%	32.75%	33.8%			

Outgoing Weekly Speeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	32	38.25	29	34.29	*	*	*	34.71	*	36.25
1 - 2	32.5	22	30	43.5	*	*	*	32.29	*	37
2 - 3	27.5	32.5	35.5	33	*	*	*	32.2	*	34.5
3 - 4	33	37.5	0	0	*	*	*	35.25	*	37.5
4 - 5	19	31	29.83	0	*	*	*	28.89	*	29.67
5 - 6	34	40	34.5	34	*	*	*	35.17	*	37
6 - 7	38.83	37.92	38.55	34.5	*	*	*	37.51	*	43.88
7 - 8	40.06	36.29	37.31	38.47	*	*	*	38.02	*	44.12
8 - 9	37.19	37.66	37.38	37.42	*	*	*	37.42	*	45.38
9 - 10	37.62	35.78	37.6	33.9	*	*	*	36.01	*	43.88
10 - 11	36.79	34.42	36.58	33.84	*	*	*	35.71	*	41.62
11 - 12	36.68	36.95	37.97	*	*	*	*	37.17	*	43.23
12 - 13	37.23	40.04	37.41	*	*	*	*	38.25	*	43.67
13 - 14	37.92	37.33	36.21	*	*	*	*	37.2	*	43.2
14 - 15	39.11	37.98	37.65	*	*	*	*	38.25	*	44.43
15 - 16	36.44	35.46	36.58	*	*	*	*	36.23	*	42.73
16 - 17	38.39	38.09	36.64	*	*	*	*	37.79	*	44.7
17 - 18	38.59	38.45	39.14	*	*	*	*	38.72	*	44.3
18 - 19	37.28	37.78	40.09	*	*	*	*	38.42	*	44.97
19 - 20	38.8	38.02	38.52	*	*	*	*	38.42	*	44.8
20 - 21	36.48	36.8	38.18	*	*	*	*	37.46	*	43.73
21 - 22	37.33	39.07	38.22	*	*	*	*	38.52	*	42.63
22 - 23	37.45	38.62	37.91	*	*	*	*	38.11	*	41.67
23 - 24	39	36.3	37.81	*	*	*	*	37.85	*	44.33
Totals	37.6	37.6	37.8	35.3	0	0	0			
% of Total	25.35%	25.35%	25.49%	23.8%	0%	0%	0%			

Outgoing Monthly Speeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	32	38.25	29	34.29	*	34.25	34.75	34.71	34.42	36.25
1 - 2	32.5	22	30	43.5	*	38.67	57	32.29	43.25	37
2 - 3	27.5	32.5	35.5	33	*	34.75	33.5	32.2	34.33	34.5
3 - 4	33	37.5	0	0	*	40.5	35	35.25	38.67	37.5
4 - 5	19	31	29.83	0	*	33.5	35.5	28.89	34.83	29.67
5 - 6	34	40	34.5	34	*	0	29	35.17	29	37
6 - 7	38.83	37.92	38.55	34.5	*	33	0	37.51	33	43.88
7 - 8	40.06	36.29	37.31	38.47	*	38.33	37.57	38.02	38	44.12
8 - 9	37.19	37.66	37.38	37.42	*	36.83	38	37.42	37.22	45.38
9 - 10	37.62	35.78	37.6	33.9	*	37.62	37.7	36.01	37.66	43.88
10 - 11	36.79	34.42	36.58	33.84	*	37.66	36.71	35.71	37.22	41.62
11 - 12	36.68	36.95	37.97	*	*	37.64	39.31	37.17	38.39	43.23
12 - 13	37.23	40.04	37.41	*	*	36.21	40.59	38.25	38.22	43.67
13 - 14	37.92	37.33	36.21	*	37.51	38.22	38.9	37.27	38.52	43.52
14 - 15	39.11	37.98	37.65	*	39.68	36.56	39.58	38.64	38.17	44.78
15 - 16	36.44	35.46	36.58	*	37.65	37.58	38.14	36.65	37.83	43.3
16 - 17	38.39	38.09	36.64	*	38.29	38.08	39.2	37.96	38.69	44.4
17 - 18	38.59	38.45	39.14	*	38.71	38.32	37.19	38.72	37.69	44.55
18 - 19	37.28	37.78	40.09	*	39.41	38.6	39.23	38.67	39.01	45.05
19 - 20	38.8	38.02	38.52	*	38.96	36.41	39.72	38.56	38.51	44.85
20 - 21	36.48	36.8	38.18	*	37.13	35	37.24	37.37	36.34	43.68
21 - 22	37.33	39.07	38.22	*	38.21	37.24	38.73	38.48	37.94	43.72
22 - 23	37.45	38.62	37.91	*	34.76	37.64	37.72	37.34	37.69	42
23 - 24	39	36.3	37.81	*	37.93	43.6	39.46	37.87	41.68	44.25
Totals	37.6	37.6	37.8	35.3	38.3	37.5	38.7			
% of Total	14.31%	14.31%	14.38%	13.43%	14.57%	14.27%	14.73%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	41	29	0	35	0
1 - 2	*	*	*	*	*	46	57	0	51.5	0
2 - 3	*	*	*	*	*	41	35	0	38	0
3 - 4	*	*	*	*	*	41	35	0	38	0
4 - 5	*	*	*	*	*	37	38	0	37.5	0
5 - 6	*	*	*	*	*	0	29	0	29	0
6 - 7	*	*	*	*	*	37	0	0	37	0
7 - 8	*	*	*	*	*	46	43	0	44.5	0
8 - 9	*	*	*	*	*	41.5	46.5	0	44	0
9 - 10	*	*	*	*	*	43	45.3	0	44.15	0
10 - 11	*	*	*	*	*	43.5	41	0	42.25	0
11 - 12	*	*	*	*	*	44.5	43.7	0	44.1	0
12 - 13	*	*	*	*	*	43.8	44.5	0	44.15	0
13 - 14	*	*	*	*	44.5	45	46	44.5	45.5	44.5
14 - 15	*	*	*	*	45.8	42	45.6	45.8	43.8	45.8
15 - 16	*	*	*	*	45	43.5	43.8	45	43.65	45
16 - 17	*	*	*	*	43.5	44.7	44.5	43.5	44.6	43.5
17 - 18	*	*	*	*	45.3	43	43	45.3	43	45.3
18 - 19	*	*	*	*	45.3	43.7	44.7	45.3	44.2	45.3
19 - 20	*	*	*	*	45	47	45.8	45	46.4	45
20 - 21	*	*	*	*	43.5	40	43.2	43.5	41.6	43.5
21 - 22	*	*	*	*	47	43	45.5	47	44.25	47
22 - 23	*	*	*	*	43	42	43	43	42.5	43
23 - 24	*	*	*	*	44	49	45	44	47	44
Totals	0	0	0	0	491.9	988.2	977.1			
% of Total	0%	0%	0%	0%	20.02%	40.22%	39.76%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	34	42	29	40	*	*	*	36.25	0	36.25
1 - 2	39	26	30	53	*	*	*	37	0	37
2 - 3	28	33	36	41	*	*	*	34.5	0	34.5
3 - 4	34	41	0	0	*	*	*	37.5	0	37.5
4 - 5	19	35	35	0	*	*	*	29.67	0	29.67
5 - 6	34	40	38	36	*	*	*	37	0	37
6 - 7	45	48	42	40.5	*	*	*	43.88	0	43.88
7 - 8	46.5	41	44	45	*	*	*	44.12	0	44.12
8 - 9	43.5	47	45	46	*	*	*	45.38	0	45.38
9 - 10	46	43.5	44	42	*	*	*	43.88	0	43.88
10 - 11	42.5	42	44	38	*	*	*	41.62	0	41.62
11 - 12	43.7	42	44	*	*	*	*	43.23	0	43.23
12 - 13	44	45	42	*	*	*	*	43.67	0	43.67
13 - 14	43.3	43.3	43	*	*	*	*	43.2	0	43.2
14 - 15	46	44.5	42.8	*	*	*	*	44.43	0	44.43
15 - 16	42	41.7	44.5	*	*	*	*	42.73	0	42.73
16 - 17	45.4	45.7	43	*	*	*	*	44.7	0	44.7
17 - 18	44.7	43.4	44.8	*	*	*	*	44.3	0	44.3
18 - 19	44.2	44.7	46	*	*	*	*	44.97	0	44.97
19 - 20	45.5	44.2	44.7	*	*	*	*	44.8	0	44.8
20 - 21	44	42	45.2	*	*	*	*	43.73	0	43.73
21 - 22	41	43.2	43.7	*	*	*	*	42.63	0	42.63
22 - 23	41.5	41	42.5	*	*	*	*	41.67	0	41.67
23 - 24	45	43	45	*	*	*	*	44.33	0	44.33
Totals	981.8	1002.2	958.2	381.5	0	0	0			
% of Total	29.54%	30.15%	28.83%	11.48%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	34	42	29	40	*	41	29	36.25	35	36.25
1 - 2	39	26	30	53	*	46	57	37	51.5	37
2 - 3	28	33	36	41	*	41	35	34.5	38	34.5
3 - 4	34	41	0	0	*	41	35	37.5	38	37.5
4 - 5	19	35	35	0	*	37	38	29.67	37.5	29.67
5 - 6	34	40	38	36	*	0	29	37	29	37
6 - 7	45	48	42	40.5	*	37	0	43.88	37	43.88
7 - 8	46.5	41	44	45	*	46	43	44.12	44.5	44.12
8 - 9	43.5	47	45	46	*	41.5	46.5	45.38	44	45.38
9 - 10	46	43.5	44	42	*	43	45.3	43.88	44.15	43.88
10 - 11	42.5	42	44	38	*	43.5	41	41.62	42.25	41.62
11 - 12	43.7	42	44	*	*	44.5	43.7	43.23	44.1	43.23
12 - 13	44	45	42	*	*	43.8	44.5	43.67	44.15	43.67
13 - 14	43.3	43.3	43	*	44.5	45	46	43.52	45.5	43.52
14 - 15	46	44.5	42.8	*	45.8	42	45.6	44.78	43.8	44.78
15 - 16	42	41.7	44.5	*	45	43.5	43.8	43.3	43.65	43.3
16 - 17	45.4	45.7	43	*	43.5	44.7	44.5	44.4	44.6	44.4
17 - 18	44.7	43.4	44.8	*	45.3	43	43	44.55	43	44.55
18 - 19	44.2	44.7	46	*	45.3	43.7	44.7	45.05	44.2	45.05
19 - 20	45.5	44.2	44.7	*	45	47	45.8	44.85	46.4	44.85
20 - 21	44	42	45.2	*	43.5	40	43.2	43.68	41.6	43.68
21 - 22	41	43.2	43.7	*	47	43	45.5	43.72	44.25	43.72
22 - 23	41.5	41	42.5	*	43	42	43	42	42.5	42
23 - 24	45	43	45	*	44	49	45	44.25	47	44.25

Summary of Violators

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	26	34.6	4	48.8
01:00:00	11	36.3	3	52.0
02:00:00	16	33.0	0	0.0
03:00:00	7	36.7	0	0.0
04:00:00	15	31.3	0	0.0
05:00:00	7	34.3	0	0.0
06:00:00	39	37.3	6	49.7
07:00:00	82	38.0	12	47.3
08:00:00	141	37.4	21	49.5
09:00:00	206	36.4	25	48.5
10:00:00	210	36.1	16	47.7
11:00:00	187	37.6	13	48.2
12:00:00	250	38.2	28	48.8
13:00:00	278	37.7	34	49.3
14:00:00	319	38.5	49	47.7
15:00:00	391	37.0	42	48.7
16:00:00	439	38.2	60	49.1
17:00:00	393	38.5	45	48.3
18:00:00	402	38.8	57	48.1
19:00:00	289	38.6	47	48.1
20:00:00	224	37.0	23	47.5
21:00:00	216	38.4	25	48.1
22:00:00	157	37.4	9	46.9
23:00:00	83	39.2	16	50.4

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/23/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	13:00	0	0	3	5	7	9	12	4	3	0	0	0	0	0	0	0	0	0	43	37.5	44.5	33 to 43	53.5	5	11.6	0	41	2	
8/23/2024	14:00	0	0	0	5	7	15	19	12	1	0	0	0	0	0	0	0	0	0	59	39.7	45.8	36 to 46	64.4	12	20.3	0	51	8	
8/23/2024	15:00	0	1	4	7	16	17	23	9	4	0	0	0	0	0	0	0	0	0	81	37.7	45	33 to 43	58.0	12	14.8	0	77	4	
8/23/2024	16:00	0	0	0	11	16	28	38	10	2	1	0	0	0	0	0	0	0	0	106	38.3	43.5	33 to 43	65.1	10	9.4	0	102	4	
8/23/2024	17:00	0	0	2	5	11	20	19	13	3	0	0	0	0	0	0	0	0	0	73	38.7	45.3	36 to 46	61.6	12	16.4	0	70	3	
8/23/2024	18:00	0	0	4	4	6	18	22	13	3	1	0	0	0	0	0	0	0	0	71	39.4	45.3	35 to 45	63.4	12	16.9	2	64	5	
8/23/2024	19:00	0	1	1	4	8	11	21	9	1	1	0	0	0	0	0	0	0	0	57	39	45	34 to 44	61.4	9	15.8	0	55	2	
8/23/2024	20:00	0	0	2	1	11	12	8	4	1	0	0	0	0	0	0	0	0	0	39	37.1	43.5	32 to 42	69.2	3	7.7	0	38	1	
8/23/2024	21:00	0	0	3	3	4	0	6	6	2	0	0	0	0	0	0	0	0	0	24	38.2	47	40 to 50	54.2	7	29.2	0	24	0	
8/23/2024	22:00	0	0	5	2	8	5	5	4	0	0	0	0	0	0	0	0	0	0	29	34.8	43	31 to 41	51.7	3	10.3	0	28	1	
8/23/2024	23:00	0	0	0	1	4	5	3	2	0	0	0	0	0	0	0	0	0	0	15	37.9	44	31 to 41	66.7	2	13.3	0	15	0	
24 Hr Summary		0	2	24	48	98	140	176	86	20	3	0	0	0	0	0	0	0	0	597	38.3	45	36 to 46	57.6	87	14.6	2	565	30	

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/24/2024	00:00	0	0	0	3	2	1	1	1	0	0	0	0	0	0	0	0	0	0	8	34.3	41	26 to 36	75.0	1	12.5	0	8	0	
8/24/2024	01:00	0	0	0	1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	3	38.7	46	36 to 46	66.7	1	33.3	0	3	0	
8/24/2024	02:00	0	0	1	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	4	34.8	41	32 to 42	75.0	*	*	0	4	0	
8/24/2024	03:00	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	40.5	41	31 to 41	100.0	*	*	0	2	0	
8/24/2024	04:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	33.5	37	27 to 37	100.0	*	*	0	1	1	
8/24/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	06:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	33	37	27 to 37	100.0	*	*	0	1	1	
8/24/2024	07:00	0	0	0	2	1	2	0	4	0	0	0	0	0	0	0	0	0	0	9	38.3	46	37 to 47	66.7	2	22.2	0	9	0	
8/24/2024	08:00	0	0	0	1	3	4	4	0	0	0	0	0	0	0	0	0	0	0	12	36.8	41.5	32 to 42	75.0	*	*	0	12	0	
8/24/2024	09:00	0	0	0	2	6	7	5	4	0	0	0	0	0	0	0	0	0	0	24	37.6	43	33 to 43	62.5	3	12.5	0	24	0	
8/24/2024	10:00	0	0	0	4	5	9	11	3	0	0	0	0	0	0	0	0	0	0	32	37.7	43.5	34 to 44	65.6	3	9.4	0	31	1	
8/24/2024	11:00	0	0	3	4	4	9	12	7	0	0	0	0	0	0	0	0	0	0	39	37.6	44.5	36 to 46	61.5	5	12.8	0	39	0	
8/24/2024	12:00	0	0	1	12	9	12	11	6	1	0	0	0	0	0	0	0	0	0	52	36.2	43.8	28 to 38	51.9	6	11.5	0	49	3	
8/24/2024	13:00	0	0	1	5	12	12	15	6	1	2	0	0	0	0	0	0	0	0	54	38.2	45	32 to 42	57.4	8	14.8	0	54	0	
8/24/2024	14:00	0	0	2	5	10	14	13	4	0	0	0	0	0	0	0	0	0	0	48	36.6	42	32 to 42	70.8	4	8.3	0	44	4	
8/24/2024	15:00	0	0	4	4	9	19	21	8	0	0	0	0	0	0	0	0	0	0	65	37.6	43.5	35 to 45	66.2	5	7.7	0	63	2	
8/24/2024	16:00	0	0	2	7	8	15	16	10	1	0	0	0	0	0	0	0	0	0	59	38.1	44.7	32 to 42	61.0	8	13.6	0	57	2	
8/24/2024	17:00	0	0	1	4	5	12	16	5	1	0	0	0	0	0	0	0	0	0	44	38.3	43	35 to 45	70.5	3	6.8	0	44	0	
8/24/2024	18:00	0	0	0	6	5	7	17	4	1	0	0	0	0	0	0	0	0	0	40	38.6	43.7	37 to 47	65.0	4	10.0	0	39	1	
8/24/2024	19:00	0	0	2	6	5	3	5	3	2	1	0	0	0	0	0	0	0	0	27	36.4	47	27 to 37	48.1	6	22.2	0	26	1	
8/24/2024	20:00	0	0	0	7	9	7	5	3	0	0	0	0	0	0	0	0	0	0	31	35	40	25 to 35	64.5	3	9.7	0	30	1	
8/24/2024	21:00	0	0	0	3	8	4	6	3	0	1	0	0	0	0	0	0	0	0	25	37.2	43	33 to 43	60.0	3	12.0	0	24	1	
8/24/2024	22:00	0	0	0	0	4	6	2	2	0	0	0	0	0	0	0	0	0	0	14	37.6	42	30 to 40	78.6	2	14.3	0	14	0	
8/24/2024	23:00	0	0	0	0	2	3	4	4	1	0	1	0	0	0	0	0	0	0	15	43.6	49	39 to 49	66.7	6	40.0	0	15	0	
24 Hr Summary		0	0	17	77	109	148	169	78	8	4	1	0	0	0	0	0	0	0	611	37.5	44	32 to 42	56.3	73	11.9	0	593	18	

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/25/2024	00:00	0	0	0	3	0	0	0	0	1	0	0	0	0	0	0	0	0	0	4	34.8	29	19 to 29	75.0	1	25.0	0	4	0	
8/25/2024	01:00	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	57	57	47 to 57	100.0	1	100.0	0	1	0	
8/25/2024	02:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	33.5	35	25 to 35	100.0	*	*	0	2	0	
8/25/2024	03:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	1	0	
8/25/2024	04:00	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	4	35.5	38	33 to 43	75.0	*	*	0	4	0	
8/25/2024	05:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0	
8/25/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	07:00	0	0	0	1	1	1	4	0	0	0	0	0	0	0	0	0	0	0	7	37.6	43	34 to 44	71.4	*	*	0	7	0	
8/25/2024	08:00	0	0	0	1	2	0	1	2	0	0	0	0	0	0	0	0	0	0	6	38	46.5	24 to 34	50.0	2	33.3	0	6	0	
8/25/2024	09:00	0	0	0	4	4	2	5	4	1	0	0	0	0	0	0	0	0	0	20	37.7	45.3	36 to 46	50.0	4	20.0	0	18	2	
8/25/2024	10:00	0	0	2	3	3	8	10	1	1	0	0	0	0	0	0	0	0	0	28	36.7	41	32 to 42	71.4	2	7.1	0	28	0	
8/25/2024	11:00	0	0	0	4	3	7	15	1	2	0	0	0	0	0	0	0	0	0	32	39.3	43.7	34 to 44	71.9	3	9.4	0	32	0	
8/25/2024	12:00	0	0	0	1	8	8	19	6	0	2	0	0	0	0	0	0	0	0	44	40.6	44.5	34 to 44	68.2	6	13.6	0	42	2	
8/25/2024	13:00	0	0	0	3	11	8	8	8	4	0	0	0	0	0	0	0	0	0	42	38.9	46	31 to 41	54.8	8	19.0	0	40	2	
8/25/2024	14:00	0	0	1	3	6	14	18	10	3	0	0	0	0	0	0	0	0	0	55	39.6	45.6	36 to 46	65.5	12	21.8	1	54	0	
8/25/2024	15:00	0	0	0	9	8	8	19	5	2	0	0	0	0	0	0	0	0	0	51	38.1	43.8	34 to 44	58.8	6	11.8	0	50	1	
8/25/2024	16:00	0	0	0	6	11	16	24	11	1	0	0	0	0	1	0	0	0	0	70	39.2	44.5	35 to 45	65.7	7	10.0	0	69	1	
8/25/2024	17:00	0	0	1	5	12	15	16	4	1	0	0	0	0	0	0	0	0	0	54	37.2	43	34 to 44	61.1	4	7.4	0	54	0	
8/25/2024	18:00	0	0	0	5	14	17	23	13	3	0	0	0	0	0	0	0	0	0	75	39.2	44.7	36 to 46	65.3	9	12.0	0	72	3	
8/25/2024	19:00	0	0	0	3	11	6	14	10	3	0	0	0	0	0	0	0	0	0	47	39.7	45.8	37 to 47	55.3	12	25.5	0	46	1	
8/25/2024	20:00	0	0	0	9	6	13	14	3	1	0	0	0	0	0	0	0	0	0	46	37.2	43.2	34 to 44	60.9	4	8.7	0	46	0	
8/25/2024	21:00	0	0	0	2	3	7	6	4	0	0	0	0	0	0	0	0	0	0	22	38.7	45.5	36 to 46	68.2	4	18.2	0	22	0	
8/25/2024	22:00	0	0	0	4	2	1	9	2	0	0	0	0	0	0	0	0	0	0	18	37.7	43	35 to 45	66.7	*	*	0	18	0	
8/25/2024	23:00	0	0	0	1	3	3	2	3	1	0	0	0	0	0	0	0	0	0	13	39.5	45	27 to 37	53.8	2	15.4	0	12	1	
24 Hr Summary		0	0	4	69	109	138	208	87	24	3	0	0	0	1	0	0	0	0	643	38.7	45	36 to 46	59.4	87	13.5	1	629	13	

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/26/2024	00:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	32	34	24 to 34	100.0	*	*	0	2	0
8/26/2024	01:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	32.5	39	16 to 26	50.0	*	*	0	2	0
8/26/2024	02:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27.5	28	18 to 28	100.0	*	*	0	2	0
8/26/2024	03:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	33	34	24 to 34	100.0	*	*	0	1	1
8/26/2024	04:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	19	19	9 to 19	100.0	*	*	0	1	0
8/26/2024	05:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	34	34	24 to 34	100.0	*	*	0	1	0
8/26/2024	06:00	0	0	0	2	0	0	2	1	1	0	0	0	0	0	0	0	0	0	6	38.8	45	35 to 45	50.0	1	16.7	0	6	0
8/26/2024	07:00	0	0	0	0	6	1	3	5	1	0	0	0	0	0	0	0	0	0	16	40.1	46.5	31 to 41	56.3	5	31.3	0	16	0
8/26/2024	08:00	0	0	2	3	4	6	8	2	1	0	0	0	0	0	0	0	0	0	26	37.2	43.5	34 to 44	57.7	2	7.7	0	23	3
8/26/2024	09:00	0	0	1	5	11	5	5	5	5	0	0	0	0	0	0	0	0	0	37	37.6	46	26 to 36	51.4	7	18.9	0	33	4
8/26/2024	10:00	0	0	1	7	10	15	16	4	0	0	0	0	0	0	0	0	0	0	53	36.8	42.5	34 to 44	66.0	3	5.7	0	47	6
8/26/2024	11:00	0	1	3	1	9	6	13	5	0	0	0	0	0	0	0	0	0	0	38	36.7	43.7	34 to 44	55.3	3	7.9	0	35	3
8/26/2024	12:00	0	1	3	4	6	17	13	8	0	0	0	0	0	0	0	0	0	0	52	37.2	44	36 to 46	65.4	6	11.5	0	46	6
8/26/2024	13:00	0	0	1	2	11	13	17	5	0	0	0	0	0	0	0	0	0	0	49	37.9	43.3	34 to 44	67.3	4	8.2	1	44	4
8/26/2024	14:00	0	0	0	2	12	13	13	11	2	0	0	0	0	0	0	0	0	0	53	39.1	46	31 to 41	58.5	9	17.0	0	48	5
8/26/2024	15:00	0	0	2	10	12	23	13	6	1	1	0	0	0	0	0	0	0	0	68	36.4	42	32 to 42	60.3	5	7.4	0	66	2
8/26/2024	16:00	0	0	2	9	13	19	19	13	4	1	0	0	0	0	0	0	0	0	80	38.4	45.4	36 to 46	55.0	14	17.5	0	76	4
8/26/2024	17:00	0	0	0	6	11	20	21	11	1	0	0	0	0	0	0	0	0	0	70	38.6	44.7	35 to 45	62.9	9	12.9	0	69	1
8/26/2024	18:00	0	0	2	9	15	17	20	11	1	0	0	0	0	0	0	0	0	0	75	37.3	44.2	37 to 47	61.3	8	10.7	0	71	4
8/26/2024	19:00	0	1	1	4	5	10	15	9	1	0	0	0	0	0	0	0	0	0	46	38.8	45.5	35 to 45	58.7	8	17.4	0	46	0
8/26/2024	20:00	0	1	0	4	3	4	6	3	0	0	0	0	0	0	0	0	0	0	21	36.5	44	36 to 46	61.9	2	9.5	0	21	0
8/26/2024	21:00	0	0	0	2	4	10	2	3	0	0	0	0	0	0	0	0	0	0	21	37.3	41	29 to 39	76.2	2	9.5	0	19	2
8/26/2024	22:00	0	0	1	1	2	7	11	0	0	0	0	0	0	0	0	0	0	0	22	37.5	41.5	33 to 43	86.4	*	*	0	22	0
8/26/2024	23:00	0	0	0	1	2	3	4	3	1	0	0	0	0	0	0	0	0	0	14	39	45	35 to 45	64.3	2	14.3	0	14	0
24 Hr Summary		0	5	19	75	141	190	201	105	19	2	0	0	0	0	0	0	0	0	757	37.6	45	35 to 45	56.4	90	11.9	1	711	45

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/27/2024	00:00	0	0	0	1	0	1	1	1	0	0	0	0	0	0	0	0	0	0	4	38.3	42	25 to 35	50.0	1	25.0	0	4	0
8/27/2024	01:00	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	22	26	16 to 26	100.0	*	*	0	2	0
8/27/2024	02:00	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	4	32.5	33	26 to 36	100.0	*	*	0	4	0
8/27/2024	03:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	37.5	41	31 to 41	100.0	*	*	0	2	0
8/27/2024	04:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	31	35	25 to 35	100.0	*	*	0	2	0
8/27/2024	05:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	40	40	30 to 40	100.0	*	*	0	1	0
8/27/2024	06:00	0	0	1	1	2	4	1	1	2	0	0	0	0	0	0	0	0	0	12	37.9	48	28 to 38	58.3	3	25.0	0	11	1
8/27/2024	07:00	0	0	2	1	1	7	5	1	0	0	0	0	0	0	0	0	0	0	17	36.3	41	34 to 44	70.6	1	5.9	0	16	1
8/27/2024	08:00	0	0	0	2	11	9	3	5	2	0	0	0	0	0	0	0	0	0	32	37.7	47	27 to 37	62.5	6	18.8	0	29	3
8/27/2024	09:00	0	0	0	7	13	10	5	5	0	0	0	0	0	0	0	0	0	0	40	35.8	43.5	27 to 37	67.5	4	10.0	0	34	6
8/27/2024	10:00	0	1	2	4	7	4	5	3	0	0	0	0	0	0	0	0	0	0	26	34.4	42	32 to 42	53.8	3	11.5	0	22	4
8/27/2024	11:00	0	0	0	5	8	16	8	6	0	0	0	0	0	0	0	0	0	0	43	37	42	31 to 41	69.8	1	2.3	1	36	6
8/27/2024	12:00	0	0	2	2	4	15	21	6	3	0	0	0	0	0	0	0	0	0	53	40	45	34 to 44	71.7	8	15.1	0	50	3
8/27/2024	13:00	0	0	0	7	10	13	13	5	0	0	0	0	0	0	0	0	0	0	48	37.3	43.3	29 to 39	54.2	5	10.4	0	45	3
8/27/2024	14:00	0	0	0	7	6	17	11	7	1	0	0	0	0	0	0	0	0	0	49	38	44.5	33 to 43	65.3	6	12.2	1	45	3
8/27/2024	15:00	0	0	3	8	12	13	11	5	0	0	0	0	0	0	0	0	0	0	52	35.5	41.7	30 to 40	55.8	4	7.7	0	48	4
8/27/2024	16:00	0	0	4	8	10	14	14	9	6	0	0	0	0	0	0	0	0	0	65	38.1	45.7	36 to 46	50.8	14	21.5	1	59	5
8/27/2024	17:00	0	0	1	3	12	30	23	6	2	1	0	0	0	0	0	0	0	0	78	38.4	43.4	34 to 44	71.8	7	9.0	0	77	1
8/27/2024	18:00	0	0	2	7	12	16	16	10	2	0	0	0	0	0	0	0	0	0	65	37.8	44.7	35 to 45	53.8	9	13.8	0	60	5
8/27/2024	19:00	0	0	0	4	14	11	19	8	2	0	0	0	0	0	0	0	0	0	58	38	44.2	36 to 46	62.1	5	8.6	0	57	1
8/27/2024	20:00	0	0	1	3	5	8	10	3	0	0	0	0	0	0	0	0	0	0	30	36.8	42	31 to 41	66.7	1	3.3	0	29	1
8/27/2024	21:00	0	0	0	5	7	25	30	7	0	0	0	0	0	0	0	0	0	0	74	39.1	43.2	36 to 46	81.1	4	5.4	0	72	2
8/27/2024	22:00	0	0	0	2	5	14	16	4	1	0	0	0	0	0	0	0	0	0	42	38.6	41	31 to 41	78.6	4	9.5	0	42	0
8/27/2024	23:00	0	0	1	3	0	2	2	1	1	0	0	0	0	0	0	0	0	0	10	36.3	43	18 to 28	40.0	2	20.0	0	10	0
24 Hr Summary		0	2	19	83	142	231	216	93	22	1	0	0	0	0	0	0	0	0	809	37.6	44	34 to 44	58.7	88	10.9	3	757	49

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/28/2024	00:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0	
8/28/2024	01:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	30	30	20 to 30	100.0	*	*	0	1	0	
8/28/2024	02:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	35.5	36	26 to 36	100.0	*	*	0	0	2	
8/28/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	04:00	0	0	1	2	1	2	0	0	0	0	0	0	0	0	0	0	0	0	6	29.8	35	25 to 35	66.7	*	*	1	4	1	
8/28/2024	05:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34.5	38	28 to 38	100.0	*	*	0	2	0	
8/28/2024	06:00	0	0	0	1	1	4	3	2	0	0	0	0	0	0	0	0	0	0	11	38.5	42	32 to 42	72.7	2	18.2	0	10	1	
8/28/2024	07:00	0	0	1	2	1	4	6	2	0	0	0	0	0	0	0	0	0	0	16	37.3	44	35 to 45	68.8	1	6.3	0	16	0	
8/28/2024	08:00	0	0	0	4	10	9	5	2	4	0	0	0	0	0	0	0	0	0	34	37.4	45	30 to 40	64.7	5	14.7	0	32	2	
8/28/2024	09:00	0	0	0	5	7	10	8	4	1	0	0	0	0	0	0	0	0	0	35	37.6	44	32 to 42	57.1	5	14.3	0	34	1	
8/28/2024	10:00	0	0	1	5	13	5	10	5	1	0	0	0	0	0	0	0	0	0	40	36.6	44	27 to 37	52.5	3	7.5	0	36	4	
8/28/2024	11:00	0	0	0	3	6	12	9	5	0	0	0	0	0	0	0	0	0	0	35	38	44	35 to 45	71.4	1	2.9	0	33	2	
8/28/2024	12:00	0	0	0	5	9	17	15	2	1	0	0	0	0	0	0	0	0	0	49	37.4	42	32 to 42	71.4	2	4.1	0	43	6	
8/28/2024	13:00	0	0	3	4	12	10	8	4	0	1	0	0	0	0	0	0	0	0	42	36.2	43	31 to 41	64.3	4	9.5	0	36	6	
8/28/2024	14:00	0	0	1	4	12	14	18	4	2	0	0	0	0	0	0	0	0	0	55	37.7	42.8	33 to 43	69.1	6	10.9	0	50	5	
8/28/2024	15:00	0	1	5	7	11	27	11	8	3	1	0	0	0	0	0	0	0	0	74	36.6	44.5	30 to 40	56.8	10	13.5	0	68	6	
8/28/2024	16:00	0	0	1	12	8	16	15	2	5	0	0	0	0	0	0	0	0	0	59	36.6	43	34 to 44	55.9	7	11.9	0	54	5	
8/28/2024	17:00	1	0	2	2	9	20	25	12	3	0	0	0	0	0	0	0	0	0	74	39.1	44.8	35 to 45	67.6	10	13.5	0	71	3	
8/28/2024	18:00	0	0	2	4	12	9	23	24	2	0	0	0	0	0	0	0	0	0	76	40.1	46	38 to 48	67.1	15	19.7	0	71	5	
8/28/2024	19:00	0	0	0	3	11	17	13	9	1	0	0	0	0	0	0	0	0	0	54	38.5	44.7	35 to 45	61.1	7	13.0	0	53	1	
8/28/2024	20:00	0	1	2	3	9	14	16	12	0	0	0	0	0	0	0	0	0	0	57	38.2	45.2	32 to 42	56.1	10	17.5	0	54	3	
8/28/2024	21:00	0	0	0	5	9	14	15	5	2	0	0	0	0	0	0	0	0	0	50	38.2	43.7	33 to 43	64.0	5	10.0	0	48	2	
8/28/2024	22:00	0	0	0	3	5	9	12	3	0	0	0	0	0	0	0	0	0	0	32	37.9	42.5	35 to 45	75.0	*	*	0	32	0	
8/28/2024	23:00	0	0	1	0	5	4	3	1	2	0	0	0	0	0	0	0	0	0	16	37.8	45	28 to 38	56.3	2	12.5	0	15	1	
24 Hr Summary		1	2	20	75	153	220	215	106	27	2	0	0	0	0	0	0	0	0	821	37.8	45	35 to 45	57.9	95	11.6	1	764	56	

Outgoing Histogram

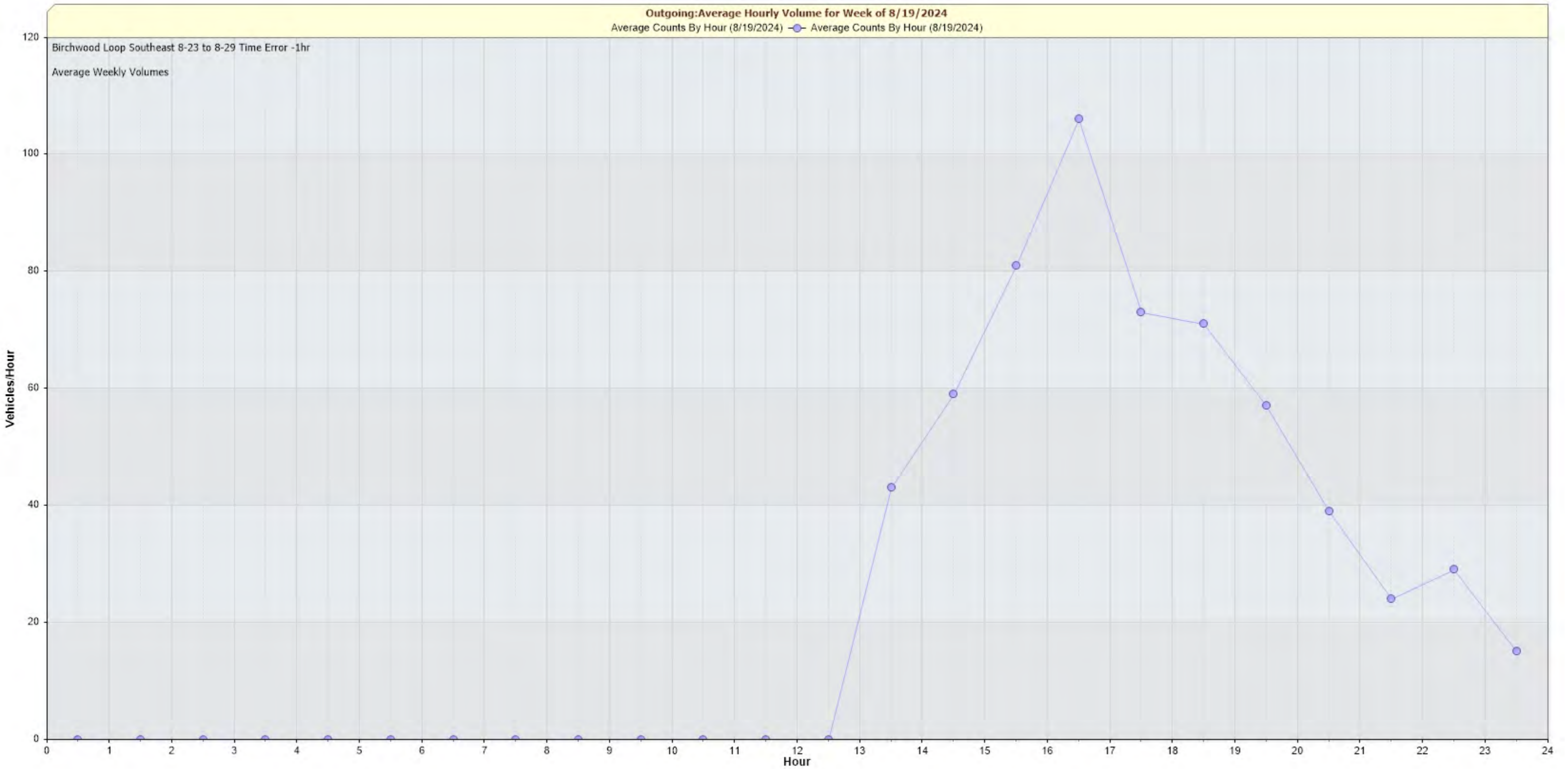
Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	0	0	0	3	1	1	1	1	0	0	0	0	0	0	0	0	0	0	7	34.3	40	22 to 32	57.1	1	14.3	0	7	0	
8/29/2024	01:00	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	2	43.5	53	24 to 34	50.0	1	50.0	0	1	1	
8/29/2024	02:00	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	2	33	41	15 to 25	50.0	*	*	0	1	1	
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	36	26 to 36	100.0	*	*	0	2	0	
8/29/2024	06:00	0	0	1	1	1	3	2	0	0	0	0	0	0	0	0	0	0	0	8	34.5	40.5	31 to 41	75.0	*	*	0	7	1	
8/29/2024	07:00	0	0	1	2	2	4	3	5	0	0	0	0	0	0	0	0	0	0	17	38.5	45	36 to 46	64.7	3	17.6	0	17	0	
8/29/2024	08:00	0	0	1	4	10	4	4	5	3	0	0	0	0	0	0	0	0	0	31	37.4	46	26 to 36	51.6	6	19.4	0	30	1	
8/29/2024	09:00	0	1	5	5	20	6	6	6	1	0	0	0	0	0	0	0	0	0	50	33.9	42	30 to 40	58.0	2	4.0	0	42	8	
8/29/2024	10:00	1	0	0	7	9	9	3	1	1	0	0	0	0	0	0	0	0	0	31	33.8	38	28 to 38	74.2	2	6.5	0	29	2	
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		1	1	8	23	45	28	20	18	6	0	0	0	0	0	0	0	0	0	150	35.3	45	28 to 38	55.3	15	10.0	0	136	14	

Outgoing: Average Hourly Volume for Week of 8/19/2024
Average Counts By Hour (8/19/2024) — Average Counts By Hour (8/19/2024)

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 8/19/2024
Average Hourly WEEKDAY Speeds By Hour (8/19/2024) — Average Hourly WEEKDAY Speeds By Hour (8/19/2024)

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr
Average Weekly Speeds



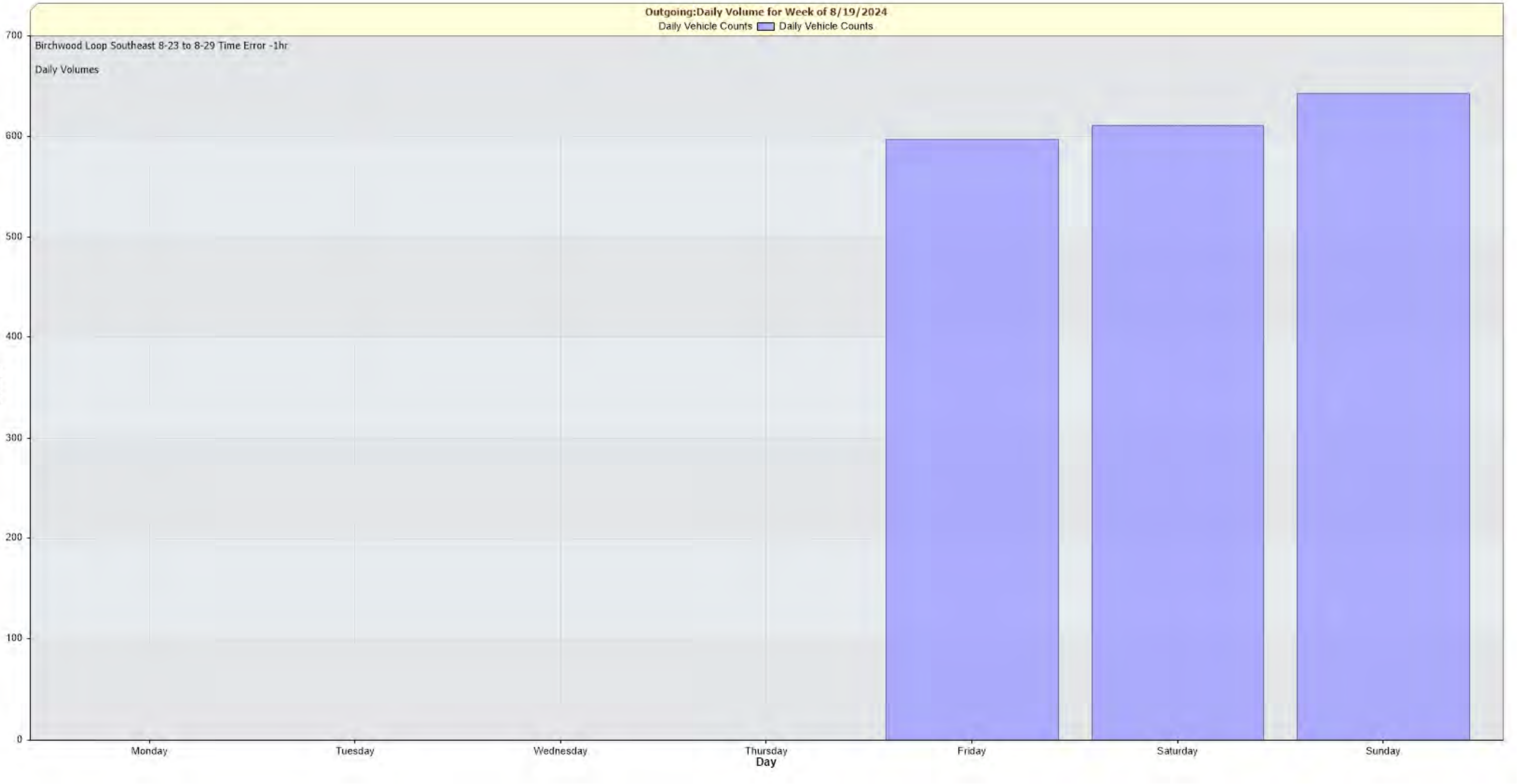
Outgoing:Daily Volume for Week of 8/19/2024

Daily Vehicle Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

Daily Volumes

Vehicles



Monday

Tuesday

Wednesday

Thursday
Day

Friday

Saturday

Sunday

For Project: Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:27:14 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 8/23/2024 3:00:00 PM through 8/29/2024 12:59:59 PM

85th Percentile Speed 35 MPH

85th Percentile Vehicles 1085

Max Speed 56 MPH on 8/24/2024 3:46:20 PM

Total Vehicles 1276

AADT: 215

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	179	182
AM Peak 11:00 AM	15	13
PM Peak 5:00 PM	23	21

Speed

Speed Limit: 45

85th Percentile Speed: 35

50th Percentile Speed: 29

10 MPH Pace Interval: 25.0 MPH to 35.0 MPH

Average Speed: 28.82

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	1	0	0	4	1
% over limit	0.0	0.0	0.5	0.0	0.0	1.9	0.6
Avg Speeder	0.0	0.0	54.0	0.0	0.0	51.3	50.0
Avg Speed	29.0	29.0	28.8	27.8	29.7	28.6	28.3

Class Counts

	Number	%
VEH_SM	2	0.2
VEH_MED	1233	96.6
VEH_LG	41	3.2
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Incoming Summary
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/23/2024 4:00:00 PM	37.0	14	16	37	0.0	0.0%	27.7
8/23/2024 5:00:00 PM	34.0	14	17	40	0.0	0.0%	27.3
8/23/2024 6:00:00 PM	35.0	21	25	39	0.0	0.0%	29.0
8/23/2024 7:00:00 PM	37.0	20	24	42	0.0	0.0%	30.4
8/23/2024 8:00:00 PM	34.0	14	17	37	0.0	0.0%	30.8
8/23/2024 9:00:00 PM	37.0	18	21	43	0.0	0.0%	30.0
8/23/2024 10:00:00 PM	35.0	14	17	41	0.0	0.0%	32.0
8/23/2024 11:00:00 PM	36.0	14	16	40	0.0	0.0%	30.5
8/24/2024 12:00:00 AM	34.0	6	7	37	0.0	0.0%	29.7
8/24/2024 1:00:00 AM	33.0	16	19	37	0.0	0.0%	27.7
8/24/2024 2:00:00 AM	30.0	3	3	30	0.0	0.0%	26.0
8/24/2024 3:00:00 AM	34.0	3	3	34	0.0	0.0%	29.3
8/24/2024 4:00:00 AM	34.0	2	2	34	0.0	0.0%	31.0
8/24/2024 5:00:00 AM	32.0	2	2	32	0.0	0.0%	29.5
8/24/2024 6:00:00 AM	**No Data**						
8/24/2024 7:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
8/24/2024 8:00:00 AM	**No Data**						
8/24/2024 9:00:00 AM	**No Data**						
8/24/2024 10:00:00 AM	27.0	3	3	27	0.0	0.0%	22.7
8/24/2024 11:00:00 AM	25.0	2	2	25	0.0	0.0%	23.5
8/24/2024 12:00:00 PM	33.0	8	9	37	0.0	0.0%	29.1
8/24/2024 1:00:00 PM	34.0	13	15	36	0.0	0.0%	29.9
8/24/2024 2:00:00 PM	35.0	12	14	36	0.0	0.0%	27.1
8/24/2024 3:00:00 PM	34.0	15	18	36	0.0	0.0%	29.2
8/24/2024 4:00:00 PM	34.0	16	19	56	51.7	15.8%	31.4
8/24/2024 5:00:00 PM	37.0	13	15	40	0.0	0.0%	28.9
8/24/2024 6:00:00 PM	33.0	14	16	39	0.0	0.0%	28.7
8/24/2024 7:00:00 PM	34.0	11	13	35	0.0	0.0%	27.0
8/24/2024 8:00:00 PM	35.0	7	8	35	0.0	0.0%	27.0
8/24/2024 9:00:00 PM	31.0	8	10	34	0.0	0.0%	28.5
8/24/2024 10:00:00 PM	35.0	13	15	40	0.0	0.0%	28.0
8/24/2024 11:00:00 PM	35.0	13	15	37	0.0	0.0%	29.7
8/25/2024 12:00:00 AM	31.0	8	10	50	50.0	10.0%	28.2
8/25/2024 1:00:00 AM	32.0	3	4	42	0.0	0.0%	31.3
8/25/2024 2:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
8/25/2024 3:00:00 AM	33.0	4	5	40	0.0	0.0%	30.8
8/25/2024 4:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
8/25/2024 5:00:00 AM	**No Data**						
8/25/2024 6:00:00 AM	**No Data**						
8/25/2024 7:00:00 AM	21.0	2	2	21	0.0	0.0%	21.0
8/25/2024 8:00:00 AM	**No Data**						
8/25/2024 9:00:00 AM	**No Data**						
8/25/2024 10:00:00 AM	30.0	2	2	30	0.0	0.0%	27.5
8/25/2024 11:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
8/25/2024 12:00:00 PM	29.0	8	9	31	0.0	0.0%	20.7
8/25/2024 1:00:00 PM	32.0	10	12	43	0.0	0.0%	28.2
8/25/2024 2:00:00 PM	38.0	6	7	50	50.0	14.3%	35.0

Incoming Summary
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

8/25/2024 3:00:00 PM	32.0	7	8	37	0.0	0.0%	26.4
8/25/2024 4:00:00 PM	32.0	12	14	33	0.0	0.0%	26.2
8/25/2024 5:00:00 PM	39.0	12	14	42	0.0	0.0%	32.2
8/25/2024 6:00:00 PM	31.0	11	13	32	0.0	0.0%	27.5
8/25/2024 7:00:00 PM	33.0	17	20	36	0.0	0.0%	27.1
8/25/2024 8:00:00 PM	31.0	9	11	37	0.0	0.0%	27.5
8/25/2024 9:00:00 PM	38.0	11	13	39	0.0	0.0%	32.5
8/25/2024 10:00:00 PM	34.0	12	14	37	0.0	0.0%	29.9
8/25/2024 11:00:00 PM	30.0	9	11	35	0.0	0.0%	26.6
8/26/2024 12:00:00 AM	33.0	3	4	34	0.0	0.0%	28.5
8/26/2024 1:00:00 AM	33.0	6	7	36	0.0	0.0%	31.3
8/26/2024 2:00:00 AM	34.0	3	3	34	0.0	0.0%	34.0
8/26/2024 3:00:00 AM	30.0	3	3	30	0.0	0.0%	26.7
8/26/2024 4:00:00 AM	23.0	2	2	23	0.0	0.0%	22.5
8/26/2024 5:00:00 AM	27.0	1	1	27	0.0	0.0%	27.0
8/26/2024 6:00:00 AM	**No Data**						
8/26/2024 7:00:00 AM	**No Data**						
						
8/26/2024 9:00:00 AM	24.0	2	2	24	0.0	0.0%	20.5
8/26/2024 10:00:00 AM	36.0	5	6	40	0.0	0.0%	31.5
8/26/2024 11:00:00 AM	34.0	8	10	35	0.0	0.0%	28.8
8/26/2024 12:00:00 PM	35.0	14	16	40	0.0	0.0%	29.1
8/26/2024 1:00:00 PM	32.0	18	21	35	0.0	0.0%	26.7
8/26/2024 2:00:00 PM	35.0	8	9	38	0.0	0.0%	31.2
8/26/2024 3:00:00 PM	32.0	8	9	37	0.0	0.0%	27.8
8/26/2024 4:00:00 PM	34.0	17	20	37	0.0	0.0%	27.8
8/26/2024 5:00:00 PM	36.0	6	7	37	0.0	0.0%	32.3
8/26/2024 6:00:00 PM	33.0	21	25	38	0.0	0.0%	28.4
8/26/2024 7:00:00 PM	36.0	22	26	38	0.0	0.0%	29.5
8/26/2024 8:00:00 PM	32.0	14	16	40	0.0	0.0%	28.2
8/26/2024 9:00:00 PM	33.0	20	24	37	0.0	0.0%	27.9
8/26/2024 10:00:00 PM	38.0	11	13	44	0.0	0.0%	32.0
8/26/2024 11:00:00 PM	32.0	7	8	37	0.0	0.0%	31.0
8/27/2024 12:00:00 AM	34.0	5	6	37	0.0	0.0%	31.5
8/27/2024 1:00:00 AM	38.0	5	6	40	0.0	0.0%	31.5
8/27/2024 2:00:00 AM	23.0	3	3	23	0.0	0.0%	22.3
8/27/2024 3:00:00 AM	30.0	2	2	30	0.0	0.0%	26.5
8/27/2024 4:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
8/27/2024 5:00:00 AM	34.0	3	3	34	0.0	0.0%	23.0
8/27/2024 6:00:00 AM	**No Data**						
8/27/2024 7:00:00 AM	31.0	1	1	31	0.0	0.0%	31.0
8/27/2024 8:00:00 AM	**No Data**						
8/27/2024 9:00:00 AM	37.0	3	3	37	0.0	0.0%	28.0
8/27/2024 10:00:00 AM	32.0	9	11	34	0.0	0.0%	28.1
8/27/2024 11:00:00 AM	33.0	12	14	34	0.0	0.0%	28.8
8/27/2024 12:00:00 PM	32.0	14	17	39	0.0	0.0%	27.9
8/27/2024 1:00:00 PM	33.0	7	8	34	0.0	0.0%	28.0
8/27/2024 2:00:00 PM	35.0	10	12	39	0.0	0.0%	32.5

Incoming Summary
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

8/27/2024 3:00:00 PM	34.0	6	7	34	0.0	0.0%	30.0
8/27/2024 4:00:00 PM	36.0	13	15	43	0.0	0.0%	28.9
8/27/2024 5:00:00 PM	35.0	9	11	40	0.0	0.0%	32.1
8/27/2024 6:00:00 PM	34.0	20	23	36	0.0	0.0%	28.8
8/27/2024 7:00:00 PM	35.0	17	20	40	0.0	0.0%	29.3
8/27/2024 8:00:00 PM	33.0	10	12	39	0.0	0.0%	30.0
8/27/2024 9:00:00 PM	35.0	20	24	37	0.0	0.0%	28.7
8/27/2024 10:00:00 PM	38.0	12	14	38	0.0	0.0%	27.1
8/27/2024 11:00:00 PM	35.0	10	12	36	0.0	0.0%	30.4
8/28/2024 12:00:00 AM	30.0	7	8	33	0.0	0.0%	27.1
8/28/2024 1:00:00 AM	30.0	5	6	36	0.0	0.0%	27.0
8/28/2024 2:00:00 AM	25.0	3	4	30	0.0	0.0%	24.8
8/28/2024 3:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
8/28/2024 4:00:00 AM	**No Data**						
8/28/2024 5:00:00 AM	**No Data**						
8/28/2024 7:00:00 AM	31.0	2	2	31	0.0	0.0%	27.0
8/28/2024 8:00:00 AM	37.0	2	2	37	0.0	0.0%	34.0
8/28/2024 9:00:00 AM	23.0	2	2	23	0.0	0.0%	23.0
8/28/2024 10:00:00 AM	31.0	3	4	35	0.0	0.0%	27.8
8/28/2024 11:00:00 AM	32.0	14	16	35	0.0	0.0%	27.3
8/28/2024 12:00:00 PM	36.0	9	11	40	0.0	0.0%	33.1
8/28/2024 1:00:00 PM	34.0	13	15	36	0.0	0.0%	27.5
8/28/2024 2:00:00 PM	36.0	9	11	38	0.0	0.0%	32.1
8/28/2024 3:00:00 PM	35.0	8	10	37	0.0	0.0%	31.3
8/28/2024 4:00:00 PM	34.0	12	14	39	0.0	0.0%	30.1
8/28/2024 5:00:00 PM	33.0	7	8	36	0.0	0.0%	29.0
8/28/2024 6:00:00 PM	31.0	17	20	36	0.0	0.0%	27.8
8/28/2024 7:00:00 PM	34.0	20	24	39	0.0	0.0%	26.1
8/28/2024 8:00:00 PM	33.0	9	11	40	0.0	0.0%	28.2
8/28/2024 9:00:00 PM	35.0	13	15	41	0.0	0.0%	30.0
8/28/2024 10:00:00 PM	33.0	10	12	37	0.0	0.0%	27.6
8/28/2024 11:00:00 PM	32.0	6	7	54	54.0	14.3%	32.0
8/29/2024 12:00:00 AM	31.0	7	8	35	0.0	0.0%	29.4
8/29/2024 1:00:00 AM	31.0	4	5	34	0.0	0.0%	30.2
8/29/2024 2:00:00 AM	32.0	4	5	43	0.0	0.0%	30.6
8/29/2024 3:00:00 AM	32.0	3	4	36	0.0	0.0%	32.5
8/29/2024 4:00:00 AM	**No Data**						
8/29/2024 5:00:00 AM	**No Data**						
8/29/2024 8:00:00 AM	33.0	1	1	33	0.0	0.0%	33.0
8/29/2024 9:00:00 AM	22.0	2	2	22	0.0	0.0%	21.5
8/29/2024 10:00:00 AM	33.0	6	7	34	0.0	0.0%	26.9
8/29/2024 11:00:00 AM	35.0	14	16	42	0.0	0.0%	28.0
8/29/2024 12:00:00 PM	32.0	14	17	34	0.0	0.0%	26.8
8/29/2024 1:00:00 PM	31.0	14	17	34	0.0	0.0%	26.6

Incoming Summary
Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/24/2024 12:00:00 AM	36.0	136	160	43	0.0	0.0%	29.7
8/25/2024 12:00:00 AM	34.0	180	212	56	51.3	1.9%	28.6
8/26/2024 12:00:00 AM	34.0	141	166	50	50.0	0.6%	28.3
8/27/2024 12:00:00 AM	35.0	199	234	44	0.0	0.0%	29.0
8/28/2024 12:00:00 AM	35.0	193	227	43	0.0	0.0%	29.0
8/29/2024 12:00:00 AM	34.0	173	203	54	54.0	0.5%	28.8
8/29/2024 12:59:59 PM	33.0	63	74	43	0.0	0.0%	27.8

Incoming Weekly Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	19	4	0	11.5	0
1 - 2	*	*	*	*	*	3	1	0	2	0
2 - 3	*	*	*	*	*	3	5	0	4	0
3 - 4	*	*	*	*	*	2	1	0	1.5	0
4 - 5	*	*	*	*	*	2	0	0	1	0
5 - 6	*	*	*	*	*	0	0	0	0	0
6 - 7	*	*	*	*	*	1	2	0	1.5	0
7 - 8	*	*	*	*	*	0	0	0	0	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	3	2	0	2.5	0
10 - 11	*	*	*	*	*	2	1	0	1.5	0
11 - 12	*	*	*	*	*	9	9	0	9	0
12 - 13	*	*	*	*	*	15	12	0	13.5	0
13 - 14	*	*	*	*	*	14	7	0	10.5	0
14 - 15	*	*	*	*	*	18	8	0	13	0
15 - 16	*	*	*	*	16	19	14	16	16.5	36.3
16 - 17	*	*	*	*	17	15	14	17	14.5	34
17 - 18	*	*	*	*	25	16	13	25	14.5	35
18 - 19	*	*	*	*	24	13	20	24	16.5	36.5
19 - 20	*	*	*	*	17	8	11	17	9.5	33.5
20 - 21	*	*	*	*	21	10	13	21	11.5	37
21 - 22	*	*	*	*	17	15	14	17	14.5	35
22 - 23	*	*	*	*	16	15	11	16	13	35.5
23 - 24	*	*	*	*	7	10	4	7	7	34
Totals	0	0	0	0	160	212	166			
% of Total	0%	0%	0%	0%	29.74%	39.41%	30.86%			

Incoming Weekly Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	7	6	6	5	*	*	*	6	0	33
1 - 2	3	3	4	5	*	*	*	3.75	0	28.5
2 - 3	3	2	1	4	*	*	*	2.5	0	31.75
3 - 4	2	1	0	0	*	*	*	0.75	0	25.5
4 - 5	1	3	0	0	*	*	*	1	0	30.5
5 - 6	0	0	0	0	*	*	*	0	0	0
6 - 7	0	1	2	0	*	*	*	0.75	0	31
7 - 8	0	0	2	1	*	*	*	0.75	0	35
8 - 9	2	3	2	2	*	*	*	2.25	0	26.5
9 - 10	6	11	4	7	*	*	*	7	0	32.88
10 - 11	10	14	16	16	*	*	*	14	0	33.3
11 - 12	16	17	11	17	*	*	*	15.25	0	33.58
12 - 13	21	8	15	17	*	*	*	15.25	0	32.5
13 - 14	9	12	11	*	*	*	*	10.67	0	35.33
14 - 15	9	7	10	*	*	*	*	8.67	0	33.33
15 - 16	20	15	14	*	*	*	*	16.33	0	34.5
16 - 17	7	11	8	*	*	*	*	8.67	0	34.5
17 - 18	25	23	20	*	*	*	*	22.67	0	32.57
18 - 19	26	20	24	*	*	*	*	23.33	0	34.63
19 - 20	16	12	11	*	*	*	*	13	0	32.67
20 - 21	24	24	15	*	*	*	*	21	0	34.33
21 - 22	13	14	12	*	*	*	*	13	0	35.93
22 - 23	8	12	7	*	*	*	*	9	0	32.83
23 - 24	6	8	8	*	*	*	*	7.33	0	31.67
Totals	234	227	203	74	0	0	0			
% of Total	31.71%	30.76%	27.51%	10.03%	0%	0%	0%			

Incoming Monthly Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	7	6	6	5	*	19	4	6	11.5	33
1 - 2	3	3	4	5	*	3	1	3.75	2	28.5
2 - 3	3	2	1	4	*	3	5	2.5	4	31.75
3 - 4	2	1	0	0	*	2	1	0.75	1.5	25.5
4 - 5	1	3	0	0	*	2	0	1	1	30.5
5 - 6	0	0	0	0	*	0	0	0	0	0
6 - 7	0	1	2	0	*	1	2	0.75	1.5	31
7 - 8	0	0	2	1	*	0	0	0.75	0	35
8 - 9	2	3	2	2	*	0	0	2.25	0	26.5
9 - 10	6	11	4	7	*	3	2	7	2.5	32.88
10 - 11	10	14	16	16	*	2	1	14	1.5	33.3
11 - 12	16	17	11	17	*	9	9	15.25	9	33.58
12 - 13	21	8	15	17	*	15	12	15.25	13.5	32.5
13 - 14	9	12	11	*	*	14	7	10.67	10.5	35.33
14 - 15	9	7	10	*	*	18	8	8.67	13	33.33
15 - 16	20	15	14	*	16	19	14	16.25	16.5	34.95
16 - 17	7	11	8	*	17	15	14	10.75	14.5	34.38
17 - 18	25	23	20	*	25	16	13	23.25	14.5	33.17
18 - 19	26	20	24	*	24	13	20	23.5	16.5	35.1
19 - 20	16	12	11	*	17	8	11	14	9.5	32.88
20 - 21	24	24	15	*	21	10	13	21	11.5	35
21 - 22	13	14	12	*	17	15	14	14	14.5	35.7
22 - 23	8	12	7	*	16	15	11	10.75	13	33.5
23 - 24	6	8	8	*	7	10	4	7.25	7	32.25
Totals	234	227	203	74	160	212	166			
% of Total	18.34%	17.79%	15.91%	5.8%	12.54%	16.61%	13.01%			

Incoming Weekly Speeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	27.68	31.25	0	28.3	0
1 - 2	*	*	*	*	*	26	26	0	26	0
2 - 3	*	*	*	*	*	29.33	30.8	0	30.25	0
3 - 4	*	*	*	*	*	31	26	0	29.33	0
4 - 5	*	*	*	*	*	29.5	0	0	29.5	0
5 - 6	*	*	*	*	*	0	0	0	0	0
6 - 7	*	*	*	*	*	29	21	0	23.67	0
7 - 8	*	*	*	*	*	0	0	0	0	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	22.67	27.5	0	24.6	0
10 - 11	*	*	*	*	*	23.5	29	0	25.33	0
11 - 12	*	*	*	*	*	29.11	20.67	0	24.89	0
12 - 13	*	*	*	*	*	29.93	28.17	0	29.15	0
13 - 14	*	*	*	*	*	27.14	35	0	29.76	0
14 - 15	*	*	*	*	*	29.17	26.38	0	28.31	0
15 - 16	*	*	*	*	27.69	31.37	26.21	27.69	29.18	36.3
16 - 17	*	*	*	*	27.29	28.93	32.21	27.29	30.52	34
17 - 18	*	*	*	*	29.04	28.69	27.46	29.04	28.14	35
18 - 19	*	*	*	*	30.42	27	27.1	30.42	27.06	36.5
19 - 20	*	*	*	*	30.76	27	27.55	30.76	27.32	33.5
20 - 21	*	*	*	*	29.95	28.5	32.54	29.95	30.78	37
21 - 22	*	*	*	*	32	28	29.86	32	28.9	35
22 - 23	*	*	*	*	30.5	29.67	26.64	30.5	28.38	35.5
23 - 24	*	*	*	*	29.71	28.2	28.5	29.71	28.29	34
Totals	0	0	0	0	29.7	28.6	28.3			
% of Total	0%	0%	0%	0%	34.3%	33.03%	32.68%			

Incoming Weekly Speeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	31.29	31.5	27	30.2	*	*	*	30.04	*	33
1 - 2	34	22.33	24.75	30.6	*	*	*	28.07	*	28.5
2 - 3	26.67	26.5	35	32.5	*	*	*	29.8	*	31.75
3 - 4	22.5	28	0	0	*	*	*	24.33	*	25.5
4 - 5	27	23	0	0	*	*	*	24	*	30.5
5 - 6	0	0	0	0	*	*	*	*	*	0
6 - 7	0	31	27	0	*	*	*	28.33	*	31
7 - 8	0	0	34	33	*	*	*	33.67	*	35
8 - 9	20.5	28	23	21.5	*	*	*	23.78	*	26.5
9 - 10	31.5	28.09	27.75	26.86	*	*	*	28.46	*	32.88
10 - 11	28.8	28.79	27.25	28	*	*	*	28.13	*	33.3
11 - 12	29.12	27.94	33.09	26.82	*	*	*	28.87	*	33.58
12 - 13	26.67	28	27.53	26.59	*	*	*	27.03	*	32.5
13 - 14	31.22	32.5	32.09	*	*	*	*	32	*	35.33
14 - 15	27.78	30	31.3	*	*	*	*	29.73	*	33.33
15 - 16	27.75	28.87	30.07	*	*	*	*	28.76	*	34.5
16 - 17	32.29	32.09	29	*	*	*	*	31.19	*	34.5
17 - 18	28.36	28.78	27.75	*	*	*	*	28.32	*	32.57
18 - 19	29.54	29.3	26.08	*	*	*	*	28.29	*	34.63
19 - 20	28.19	30	28.18	*	*	*	*	28.74	*	32.67
20 - 21	27.88	28.71	30	*	*	*	*	28.7	*	34.33
21 - 22	32	27.14	27.58	*	*	*	*	28.9	*	35.93
22 - 23	31	30.42	32	*	*	*	*	31	*	32.83
23 - 24	31.5	27.12	29.38	*	*	*	*	29.14	*	31.67
Totals	29	29	28.8	27.8	0	0	0			
% of Total	25.31%	25.31%	25.13%	24.26%	0%	0%	0%			

Incoming Monthly Speeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	31.29	31.5	27	30.2	*	27.68	31.25	30.04	28.3	33
1 - 2	34	22.33	24.75	30.6	*	26	26	28.07	26	28.5
2 - 3	26.67	26.5	35	32.5	*	29.33	30.8	29.8	30.25	31.75
3 - 4	22.5	28	0	0	*	31	26	24.33	29.33	25.5
4 - 5	27	23	0	0	*	29.5	0	24	29.5	30.5
5 - 6	0	0	0	0	*	0	0	0	0	0
6 - 7	0	31	27	0	*	29	21	28.33	23.67	31
7 - 8	0	0	34	33	*	0	0	33.67	0	35
8 - 9	20.5	28	23	21.5	*	0	0	23.78	0	26.5
9 - 10	31.5	28.09	27.75	26.86	*	22.67	27.5	28.46	24.6	32.88
10 - 11	28.8	28.79	27.25	28	*	23.5	29	28.13	25.33	33.3
11 - 12	29.12	27.94	33.09	26.82	*	29.11	20.67	28.87	24.89	33.58
12 - 13	26.67	28	27.53	26.59	*	29.93	28.17	27.03	29.15	32.5
13 - 14	31.22	32.5	32.09	*	*	27.14	35	32	29.76	35.33
14 - 15	27.78	30	31.3	*	*	29.17	26.38	29.73	28.31	33.33
15 - 16	27.75	28.87	30.07	*	27.69	31.37	26.21	28.49	29.18	34.95
16 - 17	32.29	32.09	29	*	27.29	28.93	32.21	29.65	30.52	34.38
17 - 18	28.36	28.78	27.75	*	29.04	28.69	27.46	28.52	28.14	33.17
18 - 19	29.54	29.3	26.08	*	30.42	27	27.1	28.83	27.06	35.1
19 - 20	28.19	30	28.18	*	30.76	27	27.55	29.36	27.32	32.88
20 - 21	27.88	28.71	30	*	29.95	28.5	32.54	29.01	30.78	35
21 - 22	32	27.14	27.58	*	32	28	29.86	29.84	28.9	35.7
22 - 23	31	30.42	32	*	30.5	29.67	26.64	30.81	28.38	33.5
23 - 24	31.5	27.12	29.38	*	29.71	28.2	28.5	29.28	28.29	32.25
Totals	29	29	28.8	27.8	29.7	28.6	28.3			
% of Total	14.41%	14.41%	14.31%	13.82%	14.76%	14.21%	14.07%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	33	32	0	32.5	0
1 - 2	*	*	*	*	*	30	26	0	28	0
2 - 3	*	*	*	*	*	34	33	0	33.5	0
3 - 4	*	*	*	*	*	34	26	0	30	0
4 - 5	*	*	*	*	*	32	0	0	32	0
5 - 6	*	*	*	*	*	0	0	0	0	0
6 - 7	*	*	*	*	*	29	21	0	25	0
7 - 8	*	*	*	*	*	0	0	0	0	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	27	30	0	28.5	0
10 - 11	*	*	*	*	*	25	29	0	27	0
11 - 12	*	*	*	*	*	33	29	0	31	0
12 - 13	*	*	*	*	*	34	32	0	33	0
13 - 14	*	*	*	*	*	34.5	38	0	36.25	0
14 - 15	*	*	*	*	*	33.3	32	0	32.65	0
15 - 16	*	*	*	*	36.3	34	31.5	36.3	32.75	36.3
16 - 17	*	*	*	*	34	37	38.5	34	37.75	34
17 - 18	*	*	*	*	35	33	31	35	32	35
18 - 19	*	*	*	*	36.5	34	32.5	36.5	33.25	36.5
19 - 20	*	*	*	*	33.5	34.5	31	33.5	32.75	33.5
20 - 21	*	*	*	*	37	31	38	37	34.5	37
21 - 22	*	*	*	*	35	35	34	35	34.5	35
22 - 23	*	*	*	*	35.5	35	30	35.5	32.5	35.5
23 - 24	*	*	*	*	34	31	33	34	32	34
Totals	0	0	0	0	316.8	683.3	627.5			
% of Total	0%	0%	0%	0%	19.46%	41.98%	38.55%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	33	38	30	31	*	*	*	33	0	33
1 - 2	34	23	25	32	*	*	*	28.5	0	28.5
2 - 3	30	30	35	32	*	*	*	31.75	0	31.75
3 - 4	23	28	0	0	*	*	*	25.5	0	25.5
4 - 5	27	34	0	0	*	*	*	30.5	0	30.5
5 - 6	0	0	0	0	*	*	*	0	0	0
6 - 7	0	31	31	0	*	*	*	31	0	31
7 - 8	0	0	37	33	*	*	*	35	0	35
8 - 9	24	37	23	22	*	*	*	26.5	0	26.5
9 - 10	36	31.5	31	33	*	*	*	32.88	0	32.88
10 - 11	34	32.5	31.7	35	*	*	*	33.3	0	33.3
11 - 12	35	32	36	31.3	*	*	*	33.58	0	33.58
12 - 13	32	33	34	31	*	*	*	32.5	0	32.5
13 - 14	35	35	36	*	*	*	*	35.33	0	35.33
14 - 15	32	33.5	34.5	*	*	*	*	33.33	0	33.33
15 - 16	34	35.5	34	*	*	*	*	34.5	0	34.5
16 - 17	36	34.5	33	*	*	*	*	34.5	0	34.5
17 - 18	33	33.7	31	*	*	*	*	32.57	0	32.57
18 - 19	36	34.7	33.2	*	*	*	*	34.63	0	34.63
19 - 20	32	33	33	*	*	*	*	32.67	0	32.67
20 - 21	33	35	35	*	*	*	*	34.33	0	34.33
21 - 22	37.5	37.3	33	*	*	*	*	35.93	0	35.93
22 - 23	32	34.5	32	*	*	*	*	32.83	0	32.83
23 - 24	34	30	31	*	*	*	*	31.67	0	31.67
Totals	682.5	726.7	679.4	280.3	0	0	0			
% of Total	28.81%	30.68%	28.68%	11.83%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	33	38	30	31	*	33	32	33	32.5	33
1 - 2	34	23	25	32	*	30	26	28.5	28	28.5
2 - 3	30	30	35	32	*	34	33	31.75	33.5	31.75
3 - 4	23	28	0	0	*	34	26	25.5	30	25.5
4 - 5	27	34	0	0	*	32	0	30.5	32	30.5
5 - 6	0	0	0	0	*	0	0	0	0	0
6 - 7	0	31	31	0	*	29	21	31	25	31
7 - 8	0	0	37	33	*	0	0	35	0	35
8 - 9	24	37	23	22	*	0	0	26.5	0	26.5
9 - 10	36	31.5	31	33	*	27	30	32.88	28.5	32.88
10 - 11	34	32.5	31.7	35	*	25	29	33.3	27	33.3
11 - 12	35	32	36	31.3	*	33	29	33.58	31	33.58
12 - 13	32	33	34	31	*	34	32	32.5	33	32.5
13 - 14	35	35	36	*	*	34.5	38	35.33	36.25	35.33
14 - 15	32	33.5	34.5	*	*	33.3	32	33.33	32.65	33.33
15 - 16	34	35.5	34	*	36.3	34	31.5	34.95	32.75	34.95
16 - 17	36	34.5	33	*	34	37	38.5	34.38	37.75	34.38
17 - 18	33	33.7	31	*	35	33	31	33.17	32	33.17
18 - 19	36	34.7	33.2	*	36.5	34	32.5	35.1	33.25	35.1
19 - 20	32	33	33	*	33.5	34.5	31	32.88	32.75	32.88
20 - 21	33	35	35	*	37	31	38	35	34.5	35
21 - 22	37.5	37.3	33	*	35	35	34	35.7	34.5	35.7
22 - 23	32	34.5	32	*	35.5	35	30	33.5	32.5	33.5
23 - 24	34	30	31	*	34	31	33	32.25	32	32.25

Summary of Violators

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	47	29.2	0	0.0
01:00:00	19	27.6	0	0.0
02:00:00	18	30.0	0	0.0
03:00:00	6	26.8	0	0.0
04:00:00	6	25.8	0	0.0
05:00:00	0	N/A	N/A	N/A
06:00:00	6	26.0	0	0.0
07:00:00	3	33.7	0	0.0
08:00:00	9	23.8	0	0.0
09:00:00	33	27.9	0	0.0
10:00:00	59	28.0	0	0.0
11:00:00	79	28.0	0	0.0
12:00:00	88	27.7	0	0.0
13:00:00	53	31.1	1	50.0
14:00:00	52	29.0	0	0.0
15:00:00	98	28.7	3	51.7
16:00:00	72	30.0	0	0.0
17:00:00	122	28.4	0	0.0
18:00:00	127	28.4	0	0.0
19:00:00	75	28.8	0	0.0
20:00:00	107	29.4	0	0.0
21:00:00	85	29.5	0	0.0
22:00:00	69	29.9	1	54.0
23:00:00	43	29.0	1	50.0

Incoming Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/23/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	15:00	1	1	2	6	3	3	0	0	0	0	0	0	0	0	0	0	0	0	0	16	27.7	36.3	22 to 32	68.8	*	*	1	14	1
8/23/2024	16:00	2	2	1	3	6	2	1	0	0	0	0	0	0	0	0	0	0	0	0	17	27.3	34	26 to 36	64.7	*	*	0	17	0
8/23/2024	17:00	1	0	4	8	7	5	0	0	0	0	0	0	0	0	0	0	0	0	0	25	29	35	22 to 32	72.0	*	*	0	24	1
8/23/2024	18:00	2	0	3	3	9	6	1	0	0	0	0	0	0	0	0	0	0	0	0	24	30.4	36.5	29 to 39	66.7	*	*	0	23	1
8/23/2024	19:00	0	0	0	6	9	2	0	0	0	0	0	0	0	0	0	0	0	0	0	17	30.8	33.5	26 to 36	94.1	*	*	0	17	0
8/23/2024	20:00	1	0	3	7	6	3	1	0	0	0	0	0	0	0	0	0	0	0	0	21	30	37	23 to 33	66.7	*	*	0	20	1
8/23/2024	21:00	0	0	1	2	10	2	2	0	0	0	0	0	0	0	0	0	0	0	0	17	32	35	26 to 36	82.4	*	*	0	17	0
8/23/2024	22:00	0	0	4	2	7	2	1	0	0	0	0	0	0	0	0	0	0	0	0	16	30.5	35.5	23 to 33	68.8	*	*	0	15	1
8/23/2024	23:00	0	1	0	1	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	7	29.7	34	27 to 37	85.7	*	*	0	7	0
24 Hr Summary		7	4	18	38	61	26	6	0	0	0	0	0	0	0	0	0	0	0	160	29.7	36	27 to 37	69.4	*	*	1	154	5	

Incoming Histogram
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/24/2024	00:00	0	2	5	3	8	1	0	0	0	0	0	0	0	0	0	0	0	0	19	27.7	33	24 to 34	68.4	*	*	0	19	0	
8/24/2024	01:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26	30	20 to 30	100.0	*	*	0	3	0	
8/24/2024	02:00	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	29.3	34	24 to 34	100.0	*	*	0	2	1	
8/24/2024	03:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	31	34	24 to 34	100.0	*	*	0	2	0	
8/24/2024	04:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	29.5	32	22 to 32	100.0	*	*	0	2	0	
8/24/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	06:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0	
8/24/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	09:00	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	22.7	27	17 to 27	66.7	*	*	0	3	0	
8/24/2024	10:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	23.5	25	15 to 25	100.0	*	*	0	2	0	
8/24/2024	11:00	0	1	0	4	3	1	0	0	0	0	0	0	0	0	0	0	0	0	9	29.1	33	23 to 33	77.8	*	*	0	9	0	
8/24/2024	12:00	0	0	2	4	7	2	0	0	0	0	0	0	0	0	0	0	0	0	15	29.9	34	26 to 36	86.7	*	*	1	14	0	
8/24/2024	13:00	2	0	2	5	2	3	0	0	0	0	0	0	0	0	0	0	0	0	14	27.1	34.5	26 to 36	71.4	*	*	0	14	0	
8/24/2024	14:00	1	1	0	5	10	1	0	0	0	0	0	0	0	0	0	0	0	0	18	29.2	33.3	26 to 36	88.9	*	*	0	17	1	
8/24/2024	15:00	0	1	4	5	6	0	0	1	1	1	0	0	0	0	0	0	0	0	19	31.4	34	21 to 31	57.9	3	15.8	0	19	0	
8/24/2024	16:00	0	1	3	5	3	2	1	0	0	0	0	0	0	0	0	0	0	0	15	28.9	37	21 to 31	73.3	*	*	0	15	0	
8/24/2024	17:00	0	0	2	8	4	2	0	0	0	0	0	0	0	0	0	0	0	0	16	28.7	33	23 to 33	81.3	*	*	0	16	0	
8/24/2024	18:00	0	2	3	3	3	2	0	0	0	0	0	0	0	0	0	0	0	0	13	27	34	25 to 35	61.5	*	*	0	13	0	
8/24/2024	19:00	0	2	0	3	1	2	0	0	0	0	0	0	0	0	0	0	0	0	8	27	34.5	25 to 35	75.0	*	*	0	8	0	
8/24/2024	20:00	0	0	1	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	10	28.5	31	22 to 32	90.0	*	*	0	10	0	
8/24/2024	21:00	1	1	1	6	3	2	1	0	0	0	0	0	0	0	0	0	0	0	15	28	35	26 to 36	73.3	*	*	0	15	0	
8/24/2024	22:00	1	0	0	5	6	3	0	0	0	0	0	0	0	0	0	0	0	0	15	29.7	35	25 to 35	80.0	*	*	0	14	1	
8/24/2024	23:00	0	2	1	3	3	0	0	0	1	0	0	0	0	0	0	0	0	0	10	28.2	31	23 to 33	70.0	1	10.0	0	10	0	
24 Hr Summary		5	14	26	73	67	21	2	1	2	1	0	0	0	0	0	0	0	0	212	28.6	34	25 to 35	70.3	4	1.9	1	208	3	

Incoming Histogram
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/25/2024	00:00	0	0	1	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	4	31.3	32	22 to 32	75.0	*	*	0	4	0	
8/25/2024	01:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	1	0	
8/25/2024	02:00	0	0	0	3	1	0	1	0	0	0	0	0	0	0	0	0	0	0	5	30.8	33	23 to 33	80.0	*	*	0	5	0	
8/25/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	1	0	
8/25/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	06:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	21	21	11 to 21	100.0	*	*	0	2	0	
8/25/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	09:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27.5	30	20 to 30	100.0	*	*	0	2	0	
8/25/2024	10:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0	
8/25/2024	11:00	3	1	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	9	20.7	29	13 to 23	66.7	*	*	0	9	0	
8/25/2024	12:00	0	1	4	3	2	0	2	0	0	0	0	0	0	0	0	0	0	0	12	28.2	32	21 to 31	66.7	*	*	0	11	1	
8/25/2024	13:00	0	0	0	2	1	3	0	0	1	0	0	0	0	0	0	0	0	0	7	35	38	25 to 35	57.1	1	14.3	0	7	0	
8/25/2024	14:00	1	0	1	4	1	1	0	0	0	0	0	0	0	0	0	0	0	0	8	26.4	32	22 to 32	75.0	*	*	0	8	0	
8/25/2024	15:00	1	1	2	7	3	0	0	0	0	0	0	0	0	0	0	0	0	0	14	26.2	31.5	23 to 33	85.7	*	*	0	14	0	
8/25/2024	16:00	0	0	2	4	2	5	1	0	0	0	0	0	0	0	0	0	0	0	14	32.2	38.5	29 to 39	71.4	*	*	0	14	0	
8/25/2024	17:00	0	0	3	5	5	0	0	0	0	0	0	0	0	0	0	0	0	0	13	27.5	31	22 to 32	92.3	*	*	0	13	0	
8/25/2024	18:00	0	1	6	6	5	2	0	0	0	0	0	0	0	0	0	0	0	0	20	27.1	32.5	23 to 33	75.0	*	*	0	20	0	
8/25/2024	19:00	0	0	3	5	2	1	0	0	0	0	0	0	0	0	0	0	0	0	11	27.5	31	19 to 29	72.7	*	*	0	10	1	
8/25/2024	20:00	0	0	0	3	7	3	0	0	0	0	0	0	0	0	0	0	0	0	13	32.5	38	29 to 39	84.6	*	*	0	13	0	
8/25/2024	21:00	0	1	1	4	6	2	0	0	0	0	0	0	0	0	0	0	0	0	14	29.9	34	27 to 37	78.6	*	*	0	14	0	
8/25/2024	22:00	1	1	0	5	3	1	0	0	0	0	0	0	0	0	0	0	0	0	11	26.6	30	25 to 35	81.8	*	*	0	11	0	
8/25/2024	23:00	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28.5	33	24 to 34	75.0	*	*	0	4	0	
24 Hr Summary		6	6	28	59	43	18	5	0	1	0	0	0	0	0	0	0	0	0	166	28.3	34	23 to 33	66.3	1	0.6	0	164	2	

Incoming Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/26/2024	00:00	0	0	0	2	4	1	0	0	0	0	0	0	0	0	0	0	0	0	7	31.3	33	26 to 36	100.0	*	*	0	7	0	
8/26/2024	01:00	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3	34	34	24 to 34	100.0	*	*	0	3	0	
8/26/2024	02:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26.7	30	20 to 30	100.0	*	*	0	3	0	
8/26/2024	03:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	22.5	23	13 to 23	100.0	*	*	0	2	0	
8/26/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	27	27	17 to 27	100.0	*	*	0	1	0	
8/26/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	08:00	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	20.5	24	14 to 24	100.0	*	*	0	2	0	
8/26/2024	09:00	0	1	0	0	2	2	1	0	0	0	0	0	0	0	0	0	0	0	6	31.5	36	30 to 40	83.3	*	*	0	6	0	
8/26/2024	10:00	0	1	1	4	2	2	0	0	0	0	0	0	0	0	0	0	0	0	10	28.8	34	25 to 35	80.0	*	*	0	8	2	
8/26/2024	11:00	0	1	2	5	5	2	1	0	0	0	0	0	0	0	0	0	0	0	16	29.1	35	22 to 32	75.0	*	*	0	15	1	
8/26/2024	12:00	0	4	3	6	7	1	0	0	0	0	0	0	0	0	0	0	0	0	21	26.7	32	22 to 32	66.7	*	*	0	21	0	
8/26/2024	13:00	0	0	1	2	3	3	0	0	0	0	0	0	0	0	0	0	0	0	9	31.2	35	28 to 38	88.9	*	*	0	9	0	
8/26/2024	14:00	0	0	3	3	2	1	0	0	0	0	0	0	0	0	0	0	0	0	9	27.8	32	22 to 32	77.8	*	*	0	9	0	
8/26/2024	15:00	2	1	2	4	8	3	0	0	0	0	0	0	0	0	0	0	0	0	20	27.8	34	25 to 35	65.0	*	*	0	19	1	
8/26/2024	16:00	0	0	0	1	4	2	0	0	0	0	0	0	0	0	0	0	0	0	7	32.3	36	27 to 37	85.7	*	*	0	7	0	
8/26/2024	17:00	0	0	8	6	9	2	0	0	0	0	0	0	0	0	0	0	0	0	25	28.4	33	22 to 32	76.0	*	*	0	24	1	
8/26/2024	18:00	1	2	3	4	9	7	0	0	0	0	0	0	0	0	0	0	0	0	26	29.5	36	27 to 37	69.2	*	*	0	22	4	
8/26/2024	19:00	0	3	0	6	6	0	1	0	0	0	0	0	0	0	0	0	0	0	16	28.2	32	23 to 33	75.0	*	*	0	16	0	
8/26/2024	20:00	0	1	4	10	6	3	0	0	0	0	0	0	0	0	0	0	0	0	24	27.9	33	22 to 32	70.8	*	*	0	24	0	
8/26/2024	21:00	0	0	2	1	7	2	1	0	0	0	0	0	0	0	0	0	0	0	13	32	37.5	28 to 38	76.9	*	*	0	13	0	
8/26/2024	22:00	0	0	0	3	4	1	0	0	0	0	0	0	0	0	0	0	0	0	8	31	32	27 to 37	100.0	*	*	0	8	0	
8/26/2024	23:00	0	0	0	2	3	1	0	0	0	0	0	0	0	0	0	0	0	0	6	31.5	34	24 to 34	83.3	*	*	0	6	0	
24 Hr Summary		3	15	33	61	85	33	4	0	0	0	0	0	0	0	0	0	0	0	234	29	35	25 to 35	66.7	*	*	0	225	9	

Incoming Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/27/2024	00:00	0	0	1	2	1	1	1	0	0	0	0	0	0	0	0	0	0	0	6	31.5	38	24 to 34	66.7	*	*	0	6	0	
8/27/2024	01:00	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	22.3	23	13 to 23	100.0	*	*	0	3	0	
8/27/2024	02:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	26.5	30	20 to 30	100.0	*	*	0	2	0	
8/27/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	0	1	
8/27/2024	04:00	1	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	23	34	12 to 22	66.7	*	*	0	3	0	
8/27/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	06:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	31	31	21 to 31	100.0	*	*	0	1	0	
8/27/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	08:00	0	0	2	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	28	37	14 to 24	66.7	*	*	0	3	0	
8/27/2024	09:00	0	1	1	4	5	0	0	0	0	0	0	0	0	0	0	0	0	0	11	28.1	31.5	24 to 34	90.9	*	*	0	10	1	
8/27/2024	10:00	0	1	3	0	10	0	0	0	0	0	0	0	0	0	0	0	0	0	14	28.8	32.5	23 to 33	78.6	*	*	0	13	1	
8/27/2024	11:00	0	3	3	2	6	3	0	0	0	0	0	0	0	0	0	0	0	0	17	27.9	32	22 to 32	58.8	*	*	0	17	0	
8/27/2024	12:00	0	0	2	3	3	0	0	0	0	0	0	0	0	0	0	0	0	0	8	28	33	24 to 34	75.0	*	*	0	7	1	
8/27/2024	13:00	0	0	0	4	4	4	0	0	0	0	0	0	0	0	0	0	0	0	12	32.5	35	26 to 36	91.7	*	*	0	12	0	
8/27/2024	14:00	0	0	0	3	4	0	0	0	0	0	0	0	0	0	0	0	0	0	7	30	33.5	24 to 34	100.0	*	*	0	7	0	
8/27/2024	15:00	1	0	3	4	3	3	1	0	0	0	0	0	0	0	0	0	0	0	15	28.9	35.5	22 to 32	66.7	*	*	0	15	0	
8/27/2024	16:00	0	0	1	1	6	2	1	0	0	0	0	0	0	0	0	0	0	0	11	32.1	34.5	25 to 35	81.8	*	*	0	11	0	
8/27/2024	17:00	0	3	2	5	11	2	0	0	0	0	0	0	0	0	0	0	0	0	23	28.8	33.7	25 to 35	73.9	*	*	0	19	4	
8/27/2024	18:00	0	3	2	4	6	4	1	0	0	0	0	0	0	0	0	0	0	0	20	29.3	34.7	25 to 35	65.0	*	*	0	16	4	
8/27/2024	19:00	0	0	2	3	5	2	0	0	0	0	0	0	0	0	0	0	0	0	12	30	33	23 to 33	75.0	*	*	0	12	0	
8/27/2024	20:00	0	2	4	7	5	6	0	0	0	0	0	0	0	0	0	0	0	0	24	28.7	35	27 to 37	70.8	*	*	0	23	1	
8/27/2024	21:00	0	2	3	5	1	3	0	0	0	0	0	0	0	0	0	0	0	0	14	27.1	37.3	17 to 27	71.4	*	*	0	14	0	
8/27/2024	22:00	0	1	1	2	5	3	0	0	0	0	0	0	0	0	0	0	0	0	12	30.4	34.5	26 to 36	83.3	*	*	0	12	0	
8/27/2024	23:00	0	0	2	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	8	27.1	30	23 to 33	100.0	*	*	0	8	0	
24 Hr Summary		2	16	37	54	80	34	4	0	0	0	0	0	0	0	0	0	0	0	227	29	35	25 to 35	65.6	*	*	0	214	13	

Incoming Histogram
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/28/2024	00:00	0	1	1	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	6	27	30	20 to 30	66.7	*	*	0	6	0	
8/28/2024	01:00	0	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	24.8	25	20 to 30	100.0	*	*	0	4	0	
8/28/2024	02:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	1	0	
8/28/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/28/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/28/2024	06:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27	31	21 to 31	100.0	*	*	0	2	0	
8/28/2024	07:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	37	27 to 37	100.0	*	*	0	2	0	
8/28/2024	08:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	23	23	13 to 23	100.0	*	*	0	2	0	
8/28/2024	09:00	0	0	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	4	27.8	31	25 to 35	75.0	*	*	0	4	0	
8/28/2024	10:00	1	2	1	5	6	1	0	0	0	0	0	0	0	0	0	0	0	0	16	27.3	31.7	22 to 32	75.0	*	*	0	16	0	
8/28/2024	11:00	0	0	0	2	6	2	1	0	0	0	0	0	0	0	0	0	0	0	11	33.1	36	26 to 36	81.8	*	*	0	11	0	
8/28/2024	12:00	0	1	5	4	3	2	0	0	0	0	0	0	0	0	0	0	0	0	15	27.5	34	24 to 34	73.3	*	*	0	13	2	
8/28/2024	13:00	0	0	0	3	4	4	0	0	0	0	0	0	0	0	0	0	0	0	11	32.1	36	26 to 36	81.8	*	*	0	10	1	
8/28/2024	14:00	0	0	0	3	4	3	0	0	0	0	0	0	0	0	0	0	0	0	10	31.3	34.5	25 to 35	90.0	*	*	0	10	0	
8/28/2024	15:00	0	0	0	9	3	2	0	0	0	0	0	0	0	0	0	0	0	0	14	30.1	34	24 to 34	85.7	*	*	0	14	0	
8/28/2024	16:00	0	0	2	2	3	1	0	0	0	0	0	0	0	0	0	0	0	0	8	29	33	23 to 33	87.5	*	*	0	7	1	
8/28/2024	17:00	0	0	4	11	4	1	0	0	0	0	0	0	0	0	0	0	0	0	20	27.8	31	22 to 32	80.0	*	*	0	19	1	
8/28/2024	18:00	3	1	7	4	8	1	0	0	0	0	0	0	0	0	0	0	0	0	24	26.1	33.2	20 to 30	50.0	*	*	0	23	1	
8/28/2024	19:00	1	0	2	3	3	1	1	0	0	0	0	0	0	0	0	0	0	0	11	28.2	33	25 to 35	63.6	*	*	0	11	0	
8/28/2024	20:00	0	1	1	5	5	2	1	0	0	0	0	0	0	0	0	0	0	0	15	30	35	25 to 35	73.3	*	*	0	15	0	
8/28/2024	21:00	1	0	2	3	5	1	0	0	0	0	0	0	0	0	0	0	0	0	12	27.6	33	23 to 33	66.7	*	*	0	12	0	
8/28/2024	22:00	0	0	0	4	2	0	0	0	1	0	0	0	0	0	0	0	0	0	7	32	32	22 to 32	85.7	1	14.3	0	7	0	
8/28/2024	23:00	0	0	1	2	4	1	0	0	0	0	0	0	0	0	0	0	0	0	8	29.4	31	21 to 31	87.5	*	*	0	8	0	
24 Hr Summary		6	6	32	64	65	26	3	0	1	0	0	0	0	0	0	0	0	0	203	28.8	34	25 to 35	68.0	1	0.5	0	197	6	

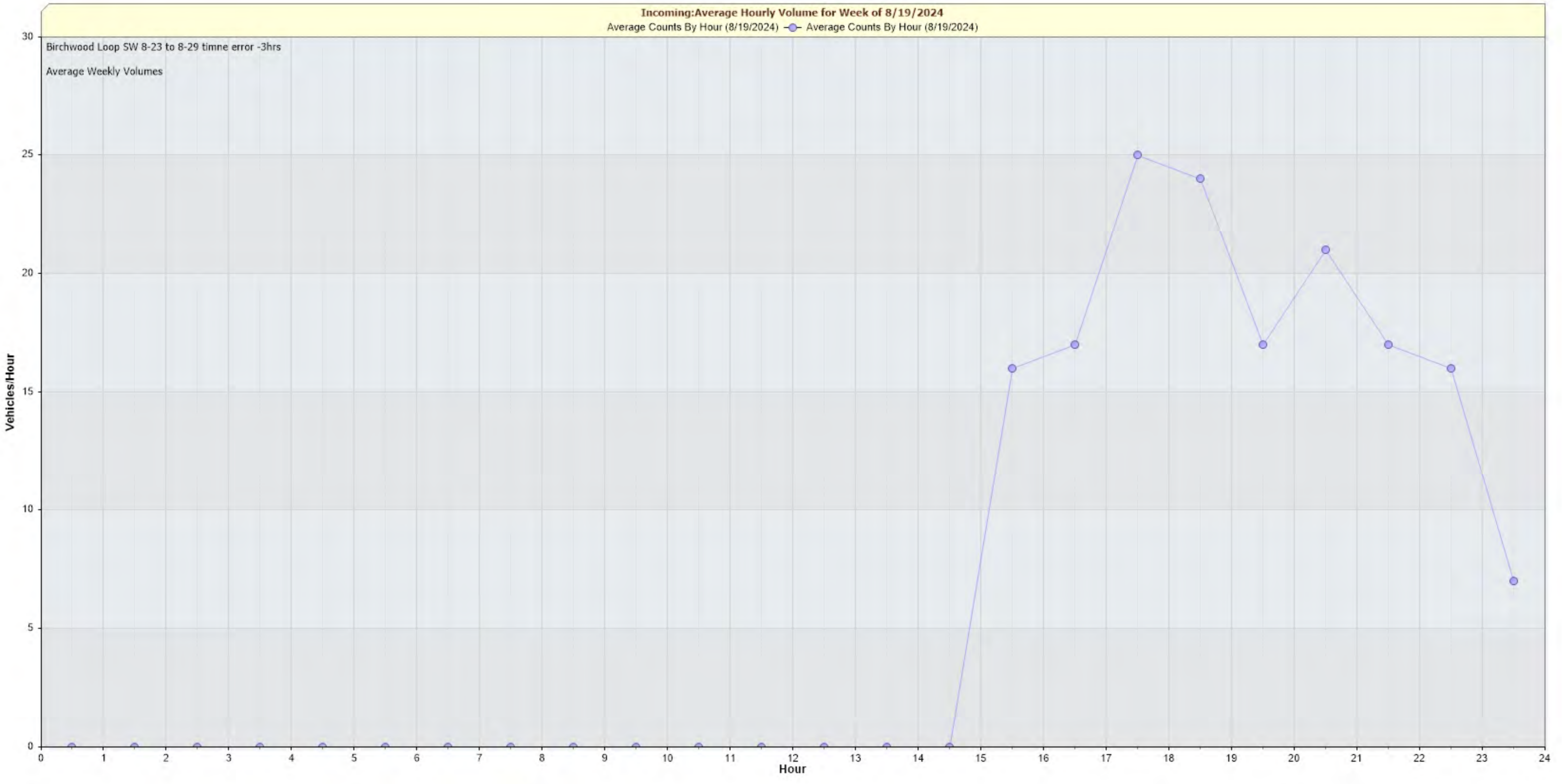
Incoming Histogram
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	0	0	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	5	30.2	31	24 to 34	100.0	*	*	0	5	0	
8/29/2024	01:00	0	0	1	2	1	0	1	0	0	0	0	0	0	0	0	0	0	0	5	30.6	32	22 to 32	80.0	*	*	0	5	0	
8/29/2024	02:00	0	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	0	0	4	32.5	32	26 to 36	100.0	*	*	0	4	0	
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	07:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	33	33	23 to 33	100.0	*	*	0	1	0	
8/29/2024	08:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	21.5	22	12 to 22	100.0	*	*	0	2	0	
8/29/2024	09:00	0	2	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	7	26.9	33	24 to 34	71.4	*	*	0	7	0	
8/29/2024	10:00	3	0	1	3	6	2	1	0	0	0	0	0	0	0	0	0	0	0	16	28	35	26 to 36	68.8	*	*	0	16	0	
8/29/2024	11:00	1	1	3	4	8	0	0	0	0	0	0	0	0	0	0	0	0	0	17	26.8	31.3	24 to 34	70.6	*	*	0	16	1	
8/29/2024	12:00	0	2	3	8	4	0	0	0	0	0	0	0	0	0	0	0	0	0	17	26.6	31	24 to 34	76.5	*	*	0	15	2	
8/29/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		4	5	10	21	29	3	2	0	0	0	0	0	0	0	0	0	0	0	74	27.8	33	24 to 34	68.9	*	*	0	71	3	

Incoming: Average Hourly Volume for Week of 8/19/2024
Average Counts By Hour (8/19/2024) — Average Counts By Hour (8/19/2024)

Birchwood Loop SW 8-23 to 8-29 time error -3hrs
Average Weekly Volumes



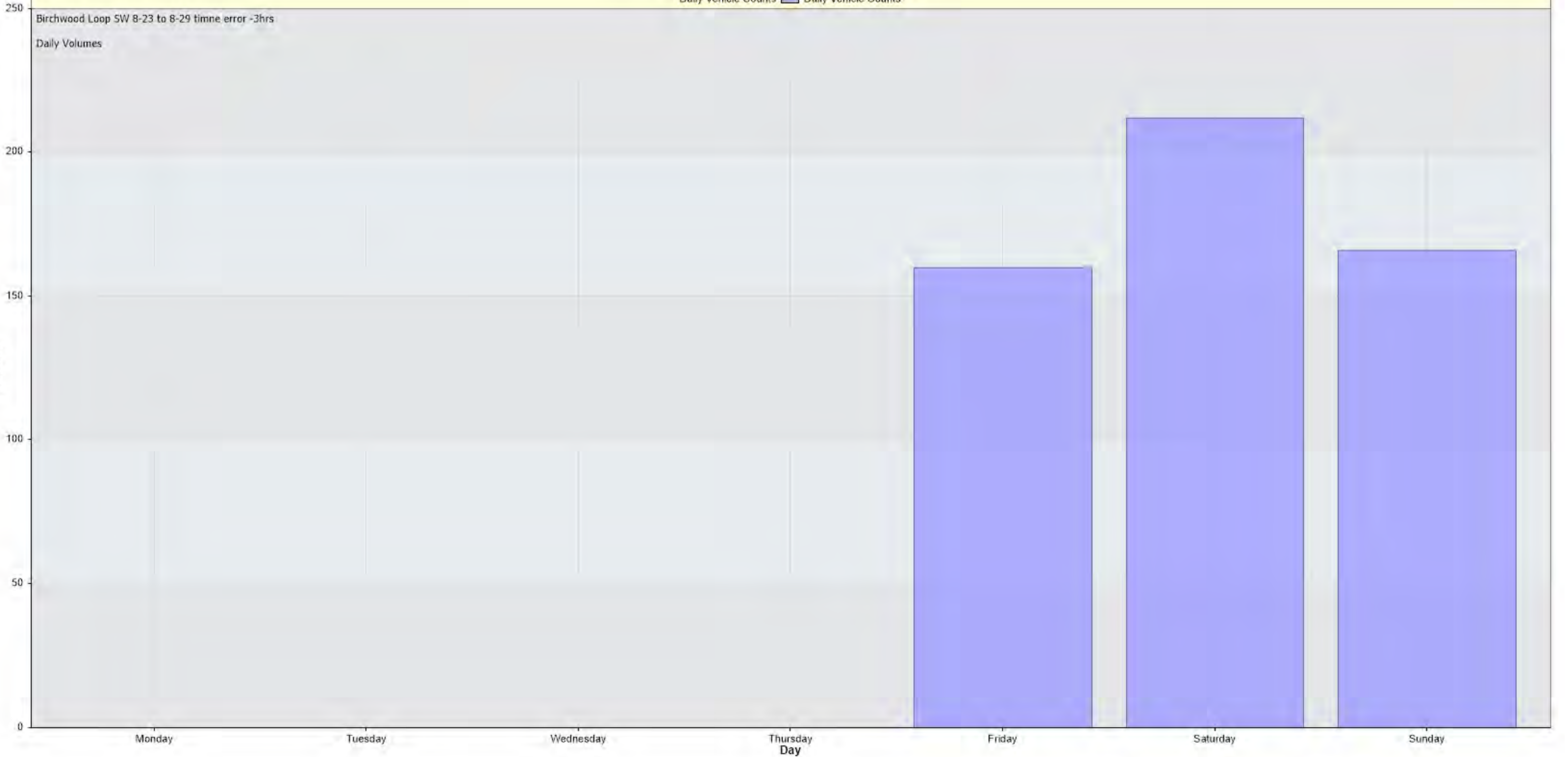
Incoming: Average Hourly WEEKDAY Speeds for Week of 8/19/2024
Average Hourly WEEKDAY Speeds By Hour (8/19/2024) — Average Hourly WEEKDAY Speeds By Hour (8/19/2024)

Birchwood Loop SW 8-23 to 8-29 time error -3hrs
Average Weekly Speeds



Incoming: Daily Volume for Week of 8/19/2024

Daily Vehicle Counts



For Project: Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:27:14 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 8/23/2024 3:00:00 PM through 8/29/2024 12:59:59 PM

85th Percentile Speed 27 MPH

85th Percentile Vehicles 1005

Max Speed 41 MPH on 8/23/2024 8:10:36 PM

Total Vehicles 1182

AADT: 199

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	167	168
AM Peak 10:00 AM	11	8
PM Peak 7:00 PM	22	20

Speed

Speed Limit: 45

85th Percentile Speed: 27

50th Percentile Speed: 23

10 MPH Pace Interval: 18.0 MPH to 28.0 MPH

Average Speed: 23.29

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	0	0	0	0	0
% over limit	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speeder	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speed	23.2	23.7	23.3	23.3	24.1	22.6	22.7

Class Counts

	Number	%
VEH_SM	22	1.9
VEH_MED	1137	96.2
VEH_LG	23	1.9
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Outgoing Summary
Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/23/2024 4:00:00 PM	24.0	12	14	29	0.0	0.0%	22.2
8/23/2024 5:00:00 PM	29.0	11	13	35	0.0	0.0%	25.1
8/23/2024 6:00:00 PM	26.0	14	17	31	0.0	0.0%	24.2
8/23/2024 7:00:00 PM	27.0	14	16	28	0.0	0.0%	23.6
8/23/2024 8:00:00 PM	26.0	16	19	31	0.0	0.0%	23.9
8/23/2024 9:00:00 PM	28.0	26	31	41	0.0	0.0%	24.7
8/23/2024 10:00:00 PM	28.0	18	21	32	0.0	0.0%	24.2
8/23/2024 11:00:00 PM	26.0	15	18	32	0.0	0.0%	23.7
8/24/2024 12:00:00 AM	27.0	8	10	31	0.0	0.0%	25.6
8/24/2024 1:00:00 AM	27.0	8	10	28	0.0	0.0%	24.9
8/24/2024 2:00:00 AM	25.0	4	5	27	0.0	0.0%	22.6
8/24/2024 3:00:00 AM	19.0	1	1	19	0.0	0.0%	19.0
8/24/2024 4:00:00 AM	25.0	2	2	25	0.0	0.0%	23.5
8/24/2024 5:00:00 AM	**No Data**						
8/24/2024 6:00:00 AM	20.0	1	1	20	0.0	0.0%	20.0
8/24/2024 7:00:00 AM	17.0	1	1	17	0.0	0.0%	17.0
8/24/2024 8:00:00 AM	**No Data**						
8/24/2024 9:00:00 AM	**No Data**						
8/24/2024 10:00:00 AM	23.0	2	2	23	0.0	0.0%	22.5
8/24/2024 11:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
8/24/2024 12:00:00 PM	22.0	3	4	23	0.0	0.0%	20.5
8/24/2024 1:00:00 PM	22.0	3	3	22	0.0	0.0%	20.3
8/24/2024 2:00:00 PM	22.0	8	10	26	0.0	0.0%	21.2
8/24/2024 3:00:00 PM	24.0	14	16	26	0.0	0.0%	20.2
8/24/2024 4:00:00 PM	24.0	9	11	32	0.0	0.0%	23.5
8/24/2024 5:00:00 PM	26.0	12	14	30	0.0	0.0%	23.5
8/24/2024 6:00:00 PM	26.0	14	16	33	0.0	0.0%	23.8
8/24/2024 7:00:00 PM	26.0	14	16	28	0.0	0.0%	22.4
8/24/2024 8:00:00 PM	24.0	7	8	25	0.0	0.0%	21.4
8/24/2024 9:00:00 PM	28.0	18	21	30	0.0	0.0%	23.6
8/24/2024 10:00:00 PM	25.0	16	19	27	0.0	0.0%	21.8
8/24/2024 11:00:00 PM	26.0	10	12	34	0.0	0.0%	23.2
8/25/2024 12:00:00 AM	25.0	9	11	27	0.0	0.0%	23.5
8/25/2024 1:00:00 AM	24.0	8	9	25	0.0	0.0%	21.7
8/25/2024 2:00:00 AM	25.0	7	8	26	0.0	0.0%	22.3
8/25/2024 3:00:00 AM	28.0	3	3	28	0.0	0.0%	27.0
8/25/2024 4:00:00 AM	21.0	1	1	21	0.0	0.0%	21.0
8/25/2024 5:00:00 AM	**No Data**						
8/25/2024 6:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
8/25/2024 7:00:00 AM	20.0	1	1	20	0.0	0.0%	20.0
8/25/2024 8:00:00 AM	**No Data**						
8/25/2024 9:00:00 AM	**No Data**						
8/25/2024 10:00:00 AM	14.0	1	1	14	0.0	0.0%	14.0
8/25/2024 11:00:00 AM	17.0	1	1	17	0.0	0.0%	17.0
8/25/2024 12:00:00 PM	15.0	1	1	15	0.0	0.0%	15.0
8/25/2024 1:00:00 PM	35.0	3	3	35	0.0	0.0%	28.7
8/25/2024 2:00:00 PM	24.0	3	4	25	0.0	0.0%	22.3

Outgoing Summary
Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

8/25/2024 3:00:00 PM	25.0	8	10	28	0.0	0.0%	23.2
8/25/2024 4:00:00 PM	26.0	14	16	30	0.0	0.0%	22.1
8/25/2024 5:00:00 PM	29.0	11	13	34	0.0	0.0%	24.5
8/25/2024 6:00:00 PM	23.0	8	9	24	0.0	0.0%	19.4
8/25/2024 7:00:00 PM	27.0	12	14	31	0.0	0.0%	24.1
8/25/2024 8:00:00 PM	26.0	14	17	30	0.0	0.0%	22.4
8/25/2024 9:00:00 PM	29.0	12	14	36	0.0	0.0%	24.1
8/25/2024 10:00:00 PM	28.0	12	14	28	0.0	0.0%	23.4
8/25/2024 11:00:00 PM	25.0	10	12	27	0.0	0.0%	22.0
8/26/2024 12:00:00 AM	24.0	9	11	29	0.0	0.0%	21.9
8/26/2024 1:00:00 AM	25.0	5	6	26	0.0	0.0%	23.8
8/26/2024 2:00:00 AM	25.0	7	8	25	0.0	0.0%	22.4
8/26/2024 3:00:00 AM	24.0	6	7	25	0.0	0.0%	19.9
8/26/2024 4:00:00 AM	26.0	3	3	26	0.0	0.0%	24.7
8/26/2024 5:00:00 AM	**No Data**						
8/26/2024 6:00:00 AM	**No Data**						
						
8/26/2024 10:00:00 AM	25.0	2	2	25	0.0	0.0%	22.0
8/26/2024 11:00:00 AM	25.0	7	8	25	0.0	0.0%	20.5
8/26/2024 12:00:00 PM	29.0	14	16	30	0.0	0.0%	22.4
8/26/2024 1:00:00 PM	25.0	9	11	35	0.0	0.0%	23.5
8/26/2024 2:00:00 PM	25.0	7	8	28	0.0	0.0%	22.5
8/26/2024 3:00:00 PM	25.0	6	7	26	0.0	0.0%	21.0
8/26/2024 4:00:00 PM	22.0	5	6	22	0.0	0.0%	20.3
8/26/2024 5:00:00 PM	26.0	9	11	31	0.0	0.0%	24.9
8/26/2024 6:00:00 PM	27.0	16	19	30	0.0	0.0%	24.5
8/26/2024 7:00:00 PM	26.0	24	28	35	0.0	0.0%	22.6
8/26/2024 8:00:00 PM	26.0	21	25	39	0.0	0.0%	24.0
8/26/2024 9:00:00 PM	27.0	20	24	38	0.0	0.0%	24.2
8/26/2024 10:00:00 PM	30.0	14	16	41	0.0	0.0%	24.6
8/26/2024 11:00:00 PM	27.0	11	13	27	0.0	0.0%	23.3
8/27/2024 12:00:00 AM	25.0	8	9	27	0.0	0.0%	23.6
8/27/2024 1:00:00 AM	26.0	6	7	26	0.0	0.0%	24.0
8/27/2024 2:00:00 AM	25.0	5	6	29	0.0	0.0%	24.2
8/27/2024 3:00:00 AM	24.0	4	5	24	0.0	0.0%	21.2
8/27/2024 4:00:00 AM	22.0	1	1	22	0.0	0.0%	22.0
8/27/2024 5:00:00 AM	**No Data**						
8/27/2024 6:00:00 AM	**No Data**						
8/27/2024 7:00:00 AM	15.0	1	1	15	0.0	0.0%	15.0
8/27/2024 8:00:00 AM	**No Data**						
8/27/2024 9:00:00 AM	**No Data**						
8/27/2024 10:00:00 AM	28.0	3	3	28	0.0	0.0%	23.7
8/27/2024 11:00:00 AM	24.0	9	11	32	0.0	0.0%	21.9
8/27/2024 12:00:00 PM	26.0	9	11	31	0.0	0.0%	23.9
8/27/2024 1:00:00 PM	24.0	3	3	24	0.0	0.0%	21.7
8/27/2024 2:00:00 PM	24.0	7	8	24	0.0	0.0%	22.1
8/27/2024 3:00:00 PM	25.0	5	6	27	0.0	0.0%	24.2
8/27/2024 4:00:00 PM	28.0	9	11	35	0.0	0.0%	24.9

Outgoing Summary
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

8/27/2024 5:00:00 PM	25.0	8	10	33	0.0	0.0%	24.8
8/27/2024 6:00:00 PM	26.0	10	12	27	0.0	0.0%	23.0
8/27/2024 7:00:00 PM	29.0	13	15	36	0.0	0.0%	24.6
8/27/2024 8:00:00 PM	28.0	20	23	31	0.0	0.0%	23.9
8/27/2024 9:00:00 PM	27.0	20	23	29	0.0	0.0%	24.4
8/27/2024 10:00:00 PM	28.0	14	16	34	0.0	0.0%	24.7
8/27/2024 11:00:00 PM	25.0	10	12	29	0.0	0.0%	23.5
8/28/2024 12:00:00 AM	24.0	8	9	29	0.0	0.0%	23.2
8/28/2024 1:00:00 AM	26.0	7	8	27	0.0	0.0%	23.0
8/28/2024 2:00:00 AM	26.0	5	6	27	0.0	0.0%	23.8
8/28/2024 3:00:00 AM	26.0	3	4	27	0.0	0.0%	26.0
8/28/2024 4:00:00 AM	25.0	2	2	25	0.0	0.0%	24.0
8/28/2024 5:00:00 AM	**No Data**						
8/28/2024 6:00:00 AM	**No Data**						
8/28/2024 7:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
8/28/2024 8:00:00 AM	**No Data**						
8/28/2024 9:00:00 AM	22.0	3	3	22	0.0	0.0%	20.3
8/28/2024 10:00:00 AM	23.0	3	4	26	0.0	0.0%	22.3
8/28/2024 11:00:00 AM	27.0	11	13	28	0.0	0.0%	22.7
8/28/2024 12:00:00 PM	26.0	7	8	27	0.0	0.0%	22.0
8/28/2024 1:00:00 PM	24.0	9	11	25	0.0	0.0%	22.8
8/28/2024 2:00:00 PM	26.0	6	7	31	0.0	0.0%	23.7
8/28/2024 3:00:00 PM	26.0	7	8	26	0.0	0.0%	21.1
8/28/2024 4:00:00 PM	25.0	8	10	26	0.0	0.0%	23.7
8/28/2024 5:00:00 PM	25.0	8	10	28	0.0	0.0%	24.1
8/28/2024 6:00:00 PM	25.0	12	14	27	0.0	0.0%	23.1
8/28/2024 7:00:00 PM	26.0	13	15	28	0.0	0.0%	23.3
8/28/2024 8:00:00 PM	28.0	18	21	29	0.0	0.0%	23.5
8/28/2024 9:00:00 PM	23.0	6	7	25	0.0	0.0%	22.3
8/28/2024 10:00:00 PM	28.0	14	16	31	0.0	0.0%	24.3
8/28/2024 11:00:00 PM	26.0	15	18	38	0.0	0.0%	23.8
8/29/2024 12:00:00 AM	27.0	10	12	32	0.0	0.0%	23.7
8/29/2024 1:00:00 AM	31.0	7	8	32	0.0	0.0%	25.6
8/29/2024 2:00:00 AM	29.0	8	9	33	0.0	0.0%	25.2
8/29/2024 3:00:00 AM	**No Data**						
8/29/2024 4:00:00 AM	24.0	1	1	24	0.0	0.0%	24.0
8/29/2024 5:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
8/29/2024 6:00:00 AM	**No Data**						
8/29/2024 7:00:00 AM	**No Data**						
8/29/2024 9:00:00 AM	22.0	2	2	22	0.0	0.0%	22.0
8/29/2024 10:00:00 AM	22.0	1	1	22	0.0	0.0%	22.0
8/29/2024 11:00:00 AM	26.0	10	12	29	0.0	0.0%	21.0
8/29/2024 12:00:00 PM	25.0	8	10	28	0.0	0.0%	22.8
8/29/2024 1:00:00 PM	25.0	12	14	32	0.0	0.0%	22.8

Outgoing Summary
Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/24/2024 12:00:00 AM	28.0	135	159	41	0.0	0.0%	24.1
8/25/2024 12:00:00 AM	26.0	156	184	34	0.0	0.0%	22.6
8/26/2024 12:00:00 AM	27.0	139	163	36	0.0	0.0%	22.7
8/27/2024 12:00:00 AM	26.0	193	227	41	0.0	0.0%	23.2
8/28/2024 12:00:00 AM	27.0	164	193	36	0.0	0.0%	23.7
8/29/2024 12:00:00 AM	27.0	168	198	38	0.0	0.0%	23.3
8/29/2024 12:59:59 PM	28.0	49	58	33	0.0	0.0%	23.3

Outgoing Weekly Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	10	9	0	9.5	0
1 - 2	*	*	*	*	*	5	8	0	6.5	0
2 - 3	*	*	*	*	*	1	3	0	2	0
3 - 4	*	*	*	*	*	2	1	0	1.5	0
4 - 5	*	*	*	*	*	0	0	0	0	0
5 - 6	*	*	*	*	*	1	1	0	1	0
6 - 7	*	*	*	*	*	1	1	0	1	0
7 - 8	*	*	*	*	*	0	0	0	0	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	2	1	0	1.5	0
10 - 11	*	*	*	*	*	1	1	0	1	0
11 - 12	*	*	*	*	*	4	1	0	2.5	0
12 - 13	*	*	*	*	*	3	3	0	3	0
13 - 14	*	*	*	*	*	10	4	0	7	0
14 - 15	*	*	*	*	*	16	10	0	13	0
15 - 16	*	*	*	*	14	11	16	14	13.5	24
16 - 17	*	*	*	*	13	14	13	13	13.5	28.5
17 - 18	*	*	*	*	17	16	9	17	12.5	26
18 - 19	*	*	*	*	16	16	14	16	15	26.7
19 - 20	*	*	*	*	19	8	17	19	12.5	26
20 - 21	*	*	*	*	31	21	14	31	17.5	27.3
21 - 22	*	*	*	*	21	19	14	21	16.5	28
22 - 23	*	*	*	*	18	12	12	18	12	26
23 - 24	*	*	*	*	10	11	11	10	11	27
Totals	0	0	0	0	159	184	163			
% of Total	0%	0%	0%	0%	31.42%	36.36%	32.21%			

Outgoing Weekly Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	6	7	8	8	*	*	*	7.25	0	26.88
1 - 2	8	6	6	9	*	*	*	7.25	0	26.12
2 - 3	7	5	4	0	*	*	*	4	0	24.5
3 - 4	3	1	2	1	*	*	*	1.75	0	24.25
4 - 5	0	0	0	1	*	*	*	0.25	0	28
5 - 6	0	0	0	0	*	*	*	0	0	0
6 - 7	0	1	1	0	*	*	*	0.5	0	20.5
7 - 8	0	0	0	0	*	*	*	0	0	0
8 - 9	0	0	3	2	*	*	*	1.25	0	22
9 - 10	2	3	4	1	*	*	*	2.5	0	24.5
10 - 11	8	11	13	12	*	*	*	11	0	25.38
11 - 12	16	11	8	10	*	*	*	11.25	0	26.38
12 - 13	11	3	11	14	*	*	*	9.75	0	24.45
13 - 14	8	8	7	*	*	*	*	7.67	0	24.83
14 - 15	7	6	8	*	*	*	*	7	0	25.17
15 - 16	6	11	10	*	*	*	*	9	0	24.83
16 - 17	11	10	10	*	*	*	*	10.33	0	25.33
17 - 18	19	12	14	*	*	*	*	15	0	25.9
18 - 19	28	15	15	*	*	*	*	19.33	0	27
19 - 20	25	23	21	*	*	*	*	23	0	27.1
20 - 21	24	23	7	*	*	*	*	18	0	25.67
21 - 22	16	16	16	*	*	*	*	16	0	28.33
22 - 23	13	12	18	*	*	*	*	14.33	0	25.77
23 - 24	9	9	12	*	*	*	*	10	0	25.33
Totals	227	193	198	58	0	0	0			
% of Total	33.58%	28.55%	29.29%	8.58%	0%	0%	0%			

Outgoing Monthly Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	6	7	8	8	*	10	9	7.25	9.5	26.88
1 - 2	8	6	6	9	*	5	8	7.25	6.5	26.12
2 - 3	7	5	4	0	*	1	3	4	2	24.5
3 - 4	3	1	2	1	*	2	1	1.75	1.5	24.25
4 - 5	0	0	0	1	*	0	0	0.25	0	28
5 - 6	0	0	0	0	*	1	1	0	1	0
6 - 7	0	1	1	0	*	1	1	0.5	1	20.5
7 - 8	0	0	0	0	*	0	0	0	0	0
8 - 9	0	0	3	2	*	0	0	1.25	0	22
9 - 10	2	3	4	1	*	2	1	2.5	1.5	24.5
10 - 11	8	11	13	12	*	1	1	11	1	25.38
11 - 12	16	11	8	10	*	4	1	11.25	2.5	26.38
12 - 13	11	3	11	14	*	3	3	9.75	3	24.45
13 - 14	8	8	7	*	*	10	4	7.67	7	24.83
14 - 15	7	6	8	*	*	16	10	7	13	25.17
15 - 16	6	11	10	*	14	11	16	10.25	13.5	24.62
16 - 17	11	10	10	*	13	14	13	11	13.5	26.12
17 - 18	19	12	14	*	17	16	9	15.5	12.5	25.92
18 - 19	28	15	15	*	16	16	14	18.5	15	26.92
19 - 20	25	23	21	*	19	8	17	22	12.5	26.82
20 - 21	24	23	7	*	31	21	14	21.25	17.5	26.08
21 - 22	16	16	16	*	21	19	14	17.25	16.5	28.25
22 - 23	13	12	18	*	18	12	12	15.25	12	25.82
23 - 24	9	9	12	*	10	11	11	10	11	25.75
Totals	227	193	198	58	159	184	163			
% of Total	19.2%	16.33%	16.75%	4.91%	13.45%	15.57%	13.79%			

Outgoing Weekly Speeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	24.9	21.67	0	23.37	0
1 - 2	*	*	*	*	*	22.6	22.25	0	22.38	0
2 - 3	*	*	*	*	*	19	27	0	25	0
3 - 4	*	*	*	*	*	23.5	21	0	22.67	0
4 - 5	*	*	*	*	*	0	0	0	0	0
5 - 6	*	*	*	*	*	20	25	0	22.5	0
6 - 7	*	*	*	*	*	17	20	0	18.5	0
7 - 8	*	*	*	*	*	0	0	0	0	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	22.5	14	0	19.67	0
10 - 11	*	*	*	*	*	25	17	0	21	0
11 - 12	*	*	*	*	*	20.5	15	0	19.4	0
12 - 13	*	*	*	*	*	20.33	28.67	0	24.5	0
13 - 14	*	*	*	*	*	21.2	22.25	0	21.5	0
14 - 15	*	*	*	*	*	20.19	23.2	0	21.35	0
15 - 16	*	*	*	*	22.21	23.55	22.06	22.21	22.67	24
16 - 17	*	*	*	*	25.08	23.5	24.54	25.08	24	28.5
17 - 18	*	*	*	*	24.18	23.81	19.44	24.18	22.24	26
18 - 19	*	*	*	*	23.62	22.38	24.14	23.62	23.2	26.7
19 - 20	*	*	*	*	23.95	21.38	22.41	23.95	22.08	26
20 - 21	*	*	*	*	24.71	23.62	24.07	24.71	23.8	27.3
21 - 22	*	*	*	*	24.19	21.84	23.36	24.19	22.48	28
22 - 23	*	*	*	*	23.67	23.17	22	23.67	22.58	26
23 - 24	*	*	*	*	25.6	23.55	21.91	25.6	22.73	27
Totals	0	0	0	0	24.1	22.6	22.7			
% of Total	0%	0%	0%	0%	34.73%	32.56%	32.71%			

Outgoing Weekly Speeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	23.83	24	23	25.62	*	*	*	24.14	*	26.88
1 - 2	22.38	24.17	23.83	25.22	*	*	*	23.93	*	26.12
2 - 3	19.86	21.2	26	0	*	*	*	21.81	*	24.5
3 - 4	24.67	22	24	24	*	*	*	24	*	24.25
4 - 5	0	0	0	28	*	*	*	28	*	28
5 - 6	0	0	0	0	*	*	*	*	*	0
6 - 7	0	15	26	0	*	*	*	20.5	*	20.5
7 - 8	0	0	0	0	*	*	*	*	*	0
8 - 9	0	0	20.33	22	*	*	*	21	*	22
9 - 10	22	23.67	22.25	22	*	*	*	22.6	*	24.5
10 - 11	20.5	21.91	22.69	21	*	*	*	21.64	*	25.38
11 - 12	22.44	23.91	22	22.8	*	*	*	22.8	*	26.38
12 - 13	23.55	21.67	22.82	22.79	*	*	*	22.92	*	24.45
13 - 14	22.5	22.12	23.71	*	*	*	*	22.74	*	24.83
14 - 15	21	24.17	21.12	*	*	*	*	21.95	*	25.17
15 - 16	20.33	24.91	23.7	*	*	*	*	23.44	*	24.83
16 - 17	24.91	24.8	24.1	*	*	*	*	24.61	*	25.33
17 - 18	24.53	23	23.14	*	*	*	*	23.69	*	25.9
18 - 19	22.64	24.6	23.33	*	*	*	*	23.33	*	27
19 - 20	24.04	23.91	23.48	*	*	*	*	23.83	*	27.1
20 - 21	24.21	24.39	22.29	*	*	*	*	24.04	*	25.67
21 - 22	24.62	24.69	24.25	*	*	*	*	24.52	*	28.33
22 - 23	23.31	23.5	23.78	*	*	*	*	23.56	*	25.77
23 - 24	23.56	23.22	23.67	*	*	*	*	23.5	*	25.33
Totals	23.2	23.7	23.3	23.3	0	0	0			
% of Total	24.81%	25.35%	24.92%	24.92%	0%	0%	0%			

Outgoing Monthly Speeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	Aug 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	23.83	24	23	25.62	*	24.9	21.67	24.14	23.37	26.88
1 - 2	22.38	24.17	23.83	25.22	*	22.6	22.25	23.93	22.38	26.12
2 - 3	19.86	21.2	26	0	*	19	27	21.81	25	24.5
3 - 4	24.67	22	24	24	*	23.5	21	24	22.67	24.25
4 - 5	0	0	0	28	*	0	0	28	0	28
5 - 6	0	0	0	0	*	20	25	0	22.5	0
6 - 7	0	15	26	0	*	17	20	20.5	18.5	20.5
7 - 8	0	0	0	0	*	0	0	0	0	0
8 - 9	0	0	20.33	22	*	0	0	21	0	22
9 - 10	22	23.67	22.25	22	*	22.5	14	22.6	19.67	24.5
10 - 11	20.5	21.91	22.69	21	*	25	17	21.64	21	25.38
11 - 12	22.44	23.91	22	22.8	*	20.5	15	22.8	19.4	26.38
12 - 13	23.55	21.67	22.82	22.79	*	20.33	28.67	22.92	24.5	24.45
13 - 14	22.5	22.12	23.71	*	*	21.2	22.25	22.74	21.5	24.83
14 - 15	21	24.17	21.12	*	*	20.19	23.2	21.95	21.35	25.17
15 - 16	20.33	24.91	23.7	*	22.21	23.55	22.06	23.02	22.67	24.62
16 - 17	24.91	24.8	24.1	*	25.08	23.5	24.54	24.75	24	26.12
17 - 18	24.53	23	23.14	*	24.18	23.81	19.44	23.82	22.24	25.92
18 - 19	22.64	24.6	23.33	*	23.62	22.38	24.14	23.39	23.2	26.92
19 - 20	24.04	23.91	23.48	*	23.95	21.38	22.41	23.85	22.08	26.82
20 - 21	24.21	24.39	22.29	*	24.71	23.62	24.07	24.28	23.8	26.08
21 - 22	24.62	24.69	24.25	*	24.19	21.84	23.36	24.42	22.48	28.25
22 - 23	23.31	23.5	23.78	*	23.67	23.17	22	23.59	22.58	25.82
23 - 24	23.56	23.22	23.67	*	25.6	23.55	21.91	24.02	22.73	25.75
Totals	23.2	23.7	23.3	23.3	24.1	22.6	22.7			
% of Total	14.24%	14.55%	14.3%	14.3%	14.79%	13.87%	13.93%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	26.5	24	0	25.25	0
1 - 2	*	*	*	*	*	25	25	0	25	0
2 - 3	*	*	*	*	*	19	28	0	23.5	0
3 - 4	*	*	*	*	*	25	21	0	23	0
4 - 5	*	*	*	*	*	0	0	0	0	0
5 - 6	*	*	*	*	*	20	25	0	22.5	0
6 - 7	*	*	*	*	*	17	20	0	18.5	0
7 - 8	*	*	*	*	*	0	0	0	0	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	23	14	0	18.5	0
10 - 11	*	*	*	*	*	25	17	0	21	0
11 - 12	*	*	*	*	*	22	15	0	18.5	0
12 - 13	*	*	*	*	*	22	35	0	28.5	0
13 - 14	*	*	*	*	*	22	24	0	23	0
14 - 15	*	*	*	*	*	23.5	25	0	24.25	0
15 - 16	*	*	*	*	24	24	26	24	25	24
16 - 17	*	*	*	*	28.5	26	29	28.5	27.5	28.5
17 - 18	*	*	*	*	26	26	23	26	24.5	26
18 - 19	*	*	*	*	26.7	25.5	27	26.7	26.25	26.7
19 - 20	*	*	*	*	26	24	25.3	26	24.65	26
20 - 21	*	*	*	*	27.3	28	28.5	27.3	28.25	27.3
21 - 22	*	*	*	*	28	24.8	27.3	28	26.05	28
22 - 23	*	*	*	*	26	26	25	26	25.5	26
23 - 24	*	*	*	*	27	24.5	24	27	24.25	27
Totals	0	0	0	0	239.5	498.8	508.1			
% of Total	0%	0%	0%	0%	19.22%	40.02%	40.77%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	25	25.5	26	31	*	*	*	26.88	0	26.88
1 - 2	24.5	25	26	29	*	*	*	26.12	0	26.12
2 - 3	24	23.5	26	0	*	*	*	24.5	0	24.5
3 - 4	26	22	25	24	*	*	*	24.25	0	24.25
4 - 5	0	0	0	28	*	*	*	28	0	28
5 - 6	0	0	0	0	*	*	*	0	0	0
6 - 7	0	15	26	0	*	*	*	20.5	0	20.5
7 - 8	0	0	0	0	*	*	*	0	0	0
8 - 9	0	0	22	22	*	*	*	22	0	22
9 - 10	25	28	23	22	*	*	*	24.5	0	24.5
10 - 11	24.5	24	27	26	*	*	*	25.38	0	25.38
11 - 12	28.5	26	26	25	*	*	*	26.38	0	26.38
12 - 13	25	24	23.8	25	*	*	*	24.45	0	24.45
13 - 14	25	23.5	26	*	*	*	*	24.83	0	24.83
14 - 15	25	25	25.5	*	*	*	*	25.17	0	25.17
15 - 16	21.7	28	24.8	*	*	*	*	24.83	0	24.83
16 - 17	26	25	25	*	*	*	*	25.33	0	25.33
17 - 18	26.7	26	25	*	*	*	*	25.9	0	25.9
18 - 19	26	29	26	*	*	*	*	27	0	27
19 - 20	26	28	27.3	*	*	*	*	27.1	0	27.1
20 - 21	27	27	23	*	*	*	*	25.67	0	25.67
21 - 22	29.5	28	27.5	*	*	*	*	28.33	0	28.33
22 - 23	26.3	25	26	*	*	*	*	25.77	0	25.77
23 - 24	25	24	27	*	*	*	*	25.33	0	25.33
Totals	486.7	501.5	533.9	232	0	0	0			
% of Total	27.75%	28.59%	30.44%	13.23%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	25	25.5	26	31	*	26.5	24	26.88	25.25	26.88
1 - 2	24.5	25	26	29	*	25	25	26.12	25	26.12
2 - 3	24	23.5	26	0	*	19	28	24.5	23.5	24.5
3 - 4	26	22	25	24	*	25	21	24.25	23	24.25
4 - 5	0	0	0	28	*	0	0	28	0	28
5 - 6	0	0	0	0	*	20	25	0	22.5	0
6 - 7	0	15	26	0	*	17	20	20.5	18.5	20.5
7 - 8	0	0	0	0	*	0	0	0	0	0
8 - 9	0	0	22	22	*	0	0	22	0	22
9 - 10	25	28	23	22	*	23	14	24.5	18.5	24.5
10 - 11	24.5	24	27	26	*	25	17	25.38	21	25.38
11 - 12	28.5	26	26	25	*	22	15	26.38	18.5	26.38
12 - 13	25	24	23.8	25	*	22	35	24.45	28.5	24.45
13 - 14	25	23.5	26	*	*	22	24	24.83	23	24.83
14 - 15	25	25	25.5	*	*	23.5	25	25.17	24.25	25.17
15 - 16	21.7	28	24.8	*	24	24	26	24.62	25	24.62
16 - 17	26	25	25	*	28.5	26	29	26.12	27.5	26.12
17 - 18	26.7	26	25	*	26	26	23	25.92	24.5	25.92
18 - 19	26	29	26	*	26.7	25.5	27	26.92	26.25	26.92
19 - 20	26	28	27.3	*	26	24	25.3	26.82	24.65	26.82
20 - 21	27	27	23	*	27.3	28	28.5	26.08	28.25	26.08
21 - 22	29.5	28	27.5	*	28	24.8	27.3	28.25	26.05	28.25
22 - 23	26.3	25	26	*	26	26	25	25.82	25.5	25.82
23 - 24	25	24	27	*	27	24.5	24	25.75	24.25	25.75

Summary of Violators

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	48	23.8	0	0.0
01:00:00	42	23.5	0	0.0
02:00:00	20	22.5	0	0.0
03:00:00	10	23.6	0	0.0
04:00:00	1	28.0	0	0.0
05:00:00	2	22.5	0	0.0
06:00:00	4	19.5	0	0.0
07:00:00	0	N/A	N/A	N/A
08:00:00	5	21.0	0	0.0
09:00:00	13	21.9	0	0.0
10:00:00	46	21.6	0	0.0
11:00:00	50	22.5	0	0.0
12:00:00	45	23.1	0	0.0
13:00:00	37	22.3	0	0.0
14:00:00	47	21.6	0	0.0
15:00:00	68	22.9	0	0.0
16:00:00	71	24.5	0	0.0
17:00:00	87	23.4	0	0.0
18:00:00	104	23.3	0	0.0
19:00:00	113	23.5	0	0.0
20:00:00	120	24.1	0	0.0
21:00:00	102	23.8	0	0.0
22:00:00	85	23.3	0	0.0
23:00:00	62	23.6	0	0.0

Outgoing Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/23/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	15:00	0	1	11	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	22.2	24	16 to 26	92.9	*	*	0	14	0
8/23/2024	16:00	1	1	2	8	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	13	25.1	28.5	19 to 29	76.9	*	*	1	12	0
8/23/2024	17:00	0	2	6	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17	24.2	26	18 to 28	88.2	*	*	0	16	1
8/23/2024	18:00	0	0	9	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16	23.6	26.7	18 to 28	100.0	*	*	0	16	0
8/23/2024	19:00	0	1	12	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19	23.9	26	19 to 29	89.5	*	*	0	19	0
8/23/2024	20:00	0	4	13	11	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	31	24.7	27.3	18 to 28	87.1	*	*	0	28	3
8/23/2024	21:00	0	1	14	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	21	24.2	28	19 to 29	85.7	*	*	0	21	0
8/23/2024	22:00	0	2	9	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	18	23.7	26	18 to 28	88.9	*	*	0	18	0
8/23/2024	23:00	0	0	4	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	25.6	27	21 to 31	100.0	*	*	0	10	0
24 Hr Summary		1	12	80	55	9	1	1	0	0	0	0	0	0	0	0	0	0	0	0	159	24.1	28	19 to 29	87.4	*	*	1	154	4

Outgoing Histogram
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/24/2024	00:00	0	0	5	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	24.9	26.5	18 to 28	100.0	*	*	0	10	0	
8/24/2024	01:00	0	2	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	22.6	25	17 to 27	100.0	*	*	0	5	0	
8/24/2024	02:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	19	19	9 to 19	100.0	*	*	0	1	0	
8/24/2024	03:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	23.5	25	15 to 25	100.0	*	*	0	2	0	
8/24/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	05:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	20	20	10 to 20	100.0	*	*	0	1	0	
8/24/2024	06:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	17	17	7 to 17	100.0	*	*	0	1	0	
8/24/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	09:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	22.5	23	13 to 23	100.0	*	*	0	2	0	
8/24/2024	10:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0	
8/24/2024	11:00	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	20.5	22	13 to 23	100.0	*	*	0	4	0	
8/24/2024	12:00	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	20.3	22	12 to 22	100.0	*	*	0	3	0	
8/24/2024	13:00	0	3	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	21.2	22	16 to 26	100.0	*	*	0	10	0	
8/24/2024	14:00	1	3	11	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16	20.2	23.5	14 to 24	93.8	*	*	1	15	0	
8/24/2024	15:00	0	0	9	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	11	23.5	24	15 to 25	90.9	*	*	1	10	0	
8/24/2024	16:00	0	2	6	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	14	23.5	26	20 to 30	85.7	*	*	0	14	0	
8/24/2024	17:00	0	0	12	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	23.8	26	18 to 28	93.8	*	*	0	16	0	
8/24/2024	18:00	1	2	8	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16	22.4	25.5	16 to 26	87.5	*	*	0	16	0	
8/24/2024	19:00	0	1	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	21.4	24	15 to 25	100.0	*	*	0	8	0	
8/24/2024	20:00	0	2	12	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	21	23.6	28	19 to 29	90.5	*	*	0	21	0	
8/24/2024	21:00	0	5	8	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19	21.8	24.8	17 to 27	100.0	*	*	0	19	0	
8/24/2024	22:00	0	4	4	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	12	23.2	26	18 to 28	91.7	*	*	1	11	0	
8/24/2024	23:00	0	1	7	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	23.5	24.5	17 to 27	100.0	*	*	0	11	0	
24 Hr Summary		2	30	101	45	6	0	0	0	0	0	0	0	0	0	0	0	0	0	184	22.6	26	18 to 28	90.2	*	*	3	181	0	

Outgoing Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/25/2024	00:00	0	2	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	21.7	24	15 to 25	100.0	*	*	0	9	0	
8/25/2024	01:00	0	1	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	22.3	25	15 to 25	87.5	*	*	0	8	0	
8/25/2024	02:00	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	27	28	18 to 28	100.0	*	*	0	3	0	
8/25/2024	03:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	21	21	11 to 21	100.0	*	*	0	1	0	
8/25/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	05:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0	
8/25/2024	06:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	20	20	10 to 20	100.0	*	*	0	1	0	
8/25/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	09:00	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	14	14	4 to 14	100.0	*	*	0	1	0	
8/25/2024	10:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	17	17	7 to 17	100.0	*	*	0	1	0	
8/25/2024	11:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	15	15	5 to 15	100.0	*	*	0	1	0	
8/25/2024	12:00	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	28.7	35	17 to 27	66.7	*	*	1	2	0	
8/25/2024	13:00	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	22.3	24	15 to 25	100.0	*	*	0	4	0	
8/25/2024	14:00	0	1	6	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	23.2	25	17 to 27	90.0	*	*	0	10	0	
8/25/2024	15:00	1	3	9	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	22.1	26	17 to 27	87.5	*	*	0	16	0	
8/25/2024	16:00	0	1	6	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	13	24.5	29	17 to 27	76.9	*	*	0	13	0	
8/25/2024	17:00	1	2	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	19.4	23	14 to 24	88.9	*	*	0	8	1	
8/25/2024	18:00	0	2	6	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	14	24.1	27	17 to 27	85.7	*	*	1	13	0	
8/25/2024	19:00	0	3	9	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	17	22.4	25.3	16 to 26	88.2	*	*	1	16	0	
8/25/2024	20:00	0	2	8	3	0	1	0	0	0	0	0	0	0	0	0	0	0	0	14	24.1	28.5	19 to 29	85.7	*	*	0	14	0	
8/25/2024	21:00	0	2	7	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	23.4	27.3	18 to 28	92.9	*	*	1	13	0	
8/25/2024	22:00	0	3	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	22	25	17 to 27	91.7	*	*	0	12	0	
8/25/2024	23:00	1	1	7	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	21.9	24	15 to 25	81.8	*	*	0	11	0	
24 Hr Summary		4	26	85	41	5	2	0	0	0	0	0	0	0	0	0	0	0	0	163	22.7	27	17 to 27	81.6	*	*	4	158	1	

Outgoing Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/26/2024	00:00	0	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	23.8	25	16 to 26	100.0	*	*	0	6	0	
8/26/2024	01:00	0	2	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	22.4	24.5	15 to 25	100.0	*	*	0	8	0	
8/26/2024	02:00	0	3	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	19.9	24	15 to 25	100.0	*	*	0	7	0	
8/26/2024	03:00	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	24.7	26	16 to 26	100.0	*	*	0	3	0	
8/26/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	09:00	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	22	25	15 to 25	100.0	*	*	0	2	0	
8/26/2024	10:00	1	2	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	20.5	24.5	15 to 25	87.5	*	*	0	8	0	
8/26/2024	11:00	0	4	9	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	22.4	28.5	14 to 24	81.3	*	*	1	15	0	
8/26/2024	12:00	0	2	6	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	11	23.5	25	17 to 27	90.9	*	*	0	10	1	
8/26/2024	13:00	0	2	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	22.5	25	18 to 28	100.0	*	*	0	8	0	
8/26/2024	14:00	0	3	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	21	25	16 to 26	100.0	*	*	0	7	0	
8/26/2024	15:00	0	2	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	20.3	21.7	12 to 22	100.0	*	*	0	5	1	
8/26/2024	16:00	0	0	6	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	11	24.9	26	21 to 31	100.0	*	*	1	10	0	
8/26/2024	17:00	0	1	11	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	19	24.5	26.7	18 to 28	94.7	*	*	0	19	0	
8/26/2024	18:00	3	3	12	8	1	1	0	0	0	0	0	0	0	0	0	0	0	0	28	22.6	26	17 to 27	78.6	*	*	1	23	4	
8/26/2024	19:00	0	3	10	11	0	1	0	0	0	0	0	0	0	0	0	0	0	0	25	24	26	18 to 28	84.0	*	*	0	25	0	
8/26/2024	20:00	1	2	12	7	1	1	0	0	0	0	0	0	0	0	0	0	0	0	24	24.2	27	18 to 28	83.3	*	*	1	23	0	
8/26/2024	21:00	0	1	9	3	2	0	1	0	0	0	0	0	0	0	0	0	0	0	16	24.6	29.5	20 to 30	87.5	*	*	1	15	0	
8/26/2024	22:00	0	2	5	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13	23.3	26.3	17 to 27	92.3	*	*	0	13	0	
8/26/2024	23:00	0	0	6	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	23.6	25	17 to 27	100.0	*	*	0	9	0	
24 Hr Summary		5	33	112	65	7	4	1	0	0	0	0	0	0	0	0	0	0	0	227	23.2	26	17 to 27	84.6	*	*	5	216	6	

Outgoing Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/27/2024	00:00	0	0	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	24	25.5	16 to 26	100.0	*	*	0	7	0	
8/27/2024	01:00	0	0	3	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	24.2	25	19 to 29	100.0	*	*	0	6	0	
8/27/2024	02:00	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	21.2	23.5	14 to 24	100.0	*	*	0	5	0	
8/27/2024	03:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	22	22	12 to 22	100.0	*	*	0	1	0	
8/27/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	06:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	15	15	5 to 15	100.0	*	*	0	1	0	
8/27/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	09:00	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	23.7	28	18 to 28	100.0	*	*	0	3	0	
8/27/2024	10:00	0	3	6	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	11	21.9	24	15 to 25	90.9	*	*	1	9	1	
8/27/2024	11:00	0	1	7	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	11	23.9	26	18 to 28	90.9	*	*	0	10	1	
8/27/2024	12:00	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	21.7	24	14 to 24	100.0	*	*	0	3	0	
8/27/2024	13:00	0	1	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	22.1	23.5	14 to 24	100.0	*	*	0	7	1	
8/27/2024	14:00	0	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	24.2	25	17 to 27	100.0	*	*	0	6	0	
8/27/2024	15:00	0	1	4	5	0	1	0	0	0	0	0	0	0	0	0	0	0	0	11	24.9	28	19 to 29	81.8	*	*	0	10	1	
8/27/2024	16:00	0	0	7	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	10	24.8	25	21 to 31	90.0	*	*	1	9	0	
8/27/2024	17:00	1	0	7	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	23	26	17 to 27	91.7	*	*	0	12	0	
8/27/2024	18:00	0	1	9	3	1	1	0	0	0	0	0	0	0	0	0	0	0	0	15	24.6	29	16 to 26	80.0	*	*	2	12	1	
8/27/2024	19:00	0	3	10	7	3	0	0	0	0	0	0	0	0	0	0	0	0	0	23	23.9	28	21 to 31	82.6	*	*	1	22	0	
8/27/2024	20:00	0	2	9	12	0	0	0	0	0	0	0	0	0	0	0	0	0	0	23	24.4	27	19 to 29	100.0	*	*	0	23	0	
8/27/2024	21:00	0	1	7	6	2	0	0	0	0	0	0	0	0	0	0	0	0	0	16	24.7	28	18 to 28	87.5	*	*	0	16	0	
8/27/2024	22:00	0	2	4	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	23.5	25	16 to 26	91.7	*	*	0	12	0	
8/27/2024	23:00	0	0	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	23.2	24	19 to 29	100.0	*	*	0	9	0	
24 Hr Summary		1	19	105	56	10	2	0	0	0	0	0	0	0	0	0	0	0	0	193	23.7	27	18 to 28	86.0	*	*	5	183	5	

Outgoing Histogram
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/28/2024	00:00	0	0	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	23	26	17 to 27	100.0	*	*	0	8	0	
8/28/2024	01:00	0	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	23.8	26	17 to 27	100.0	*	*	0	6	0	
8/28/2024	02:00	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26	26	17 to 27	100.0	*	*	0	4	0	
8/28/2024	03:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24	25	15 to 25	100.0	*	*	0	2	0	
8/28/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/28/2024	06:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	1	0	
8/28/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/28/2024	08:00	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	20.3	22	12 to 22	100.0	*	*	0	3	0	
8/28/2024	09:00	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	22.3	23	16 to 26	100.0	*	*	0	4	0	
8/28/2024	10:00	1	1	6	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13	22.7	27	18 to 28	84.6	*	*	0	11	2	
8/28/2024	11:00	0	2	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	22	26	17 to 27	100.0	*	*	0	8	0	
8/28/2024	12:00	0	1	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	22.8	23.8	15 to 25	100.0	*	*	0	11	0	
8/28/2024	13:00	0	1	4	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	7	23.7	26	16 to 26	85.7	*	*	0	6	1	
8/28/2024	14:00	0	4	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	21.1	25.5	16 to 26	100.0	*	*	0	8	0	
8/28/2024	15:00	0	0	5	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	23.7	24.8	16 to 26	100.0	*	*	0	10	0	
8/28/2024	16:00	0	0	7	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	24.1	25	18 to 28	100.0	*	*	0	9	1	
8/28/2024	17:00	0	1	8	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	23.1	25	17 to 27	100.0	*	*	0	14	0	
8/28/2024	18:00	0	1	10	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	15	23.3	26	18 to 28	100.0	*	*	0	14	1	
8/28/2024	19:00	1	3	8	9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	21	23.5	27.3	18 to 28	90.5	*	*	0	21	0	
8/28/2024	20:00	0	1	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	22.3	23	15 to 25	100.0	*	*	0	7	0	
8/28/2024	21:00	0	0	9	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	24.3	27.5	18 to 28	93.8	*	*	0	15	1	
8/28/2024	22:00	0	3	9	4	1	1	0	0	0	0	0	0	0	0	0	0	0	0	18	23.8	26	16 to 26	83.3	*	*	1	17	0	
8/28/2024	23:00	0	3	4	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	12	23.7	27	17 to 27	75.0	*	*	0	11	1	
24 Hr Summary		2	23	104	63	5	1	0	0	0	0	0	0	0	0	0	0	0	0	198	23.3	27	18 to 28	91.4	*	*	1	190	7	

Outgoing Histogram

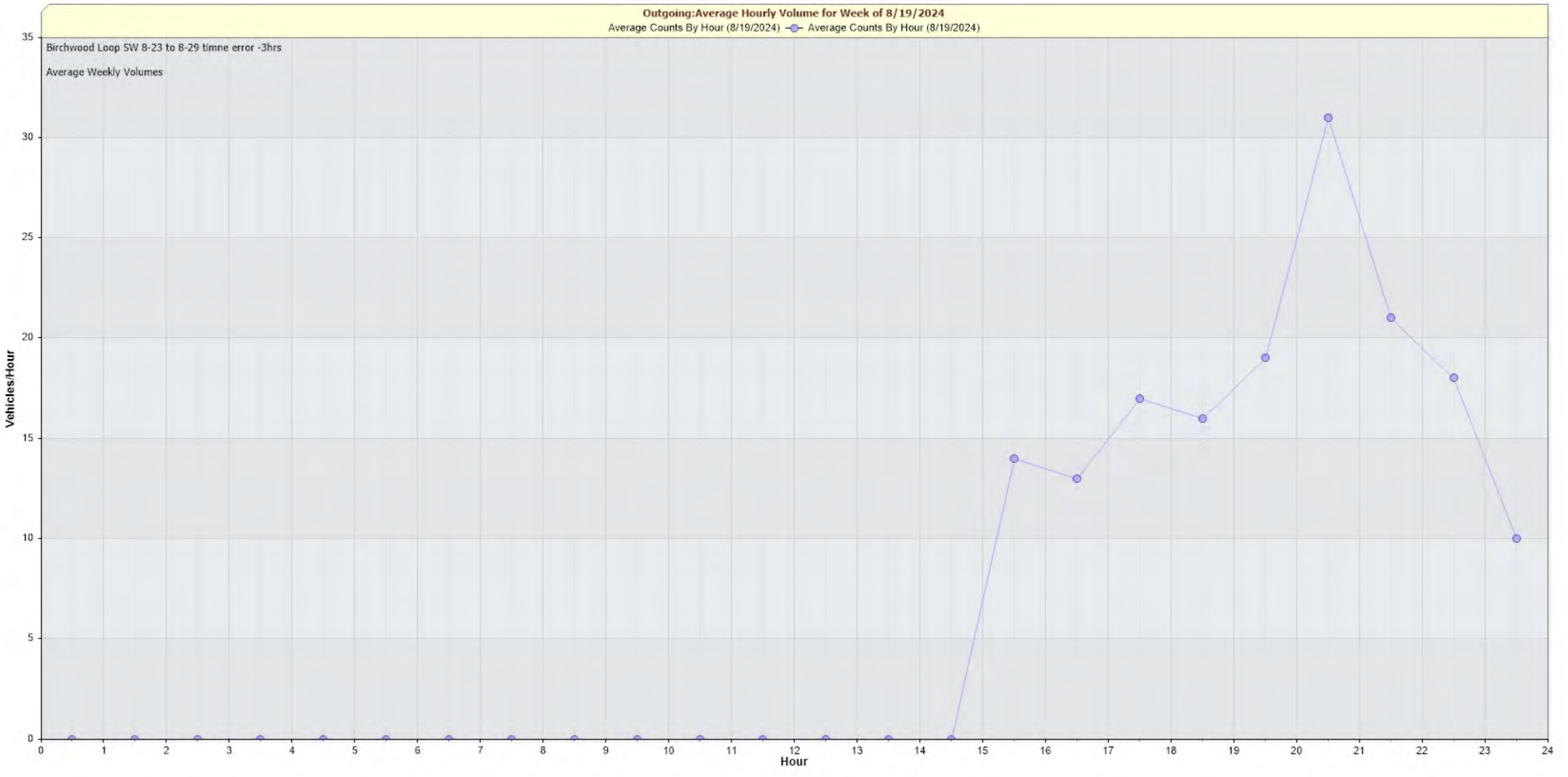
Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	0	1	2	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	8	25.6	31	22 to 32	87.5	*	*	1	7	0	
8/29/2024	01:00	0	1	2	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	9	25.2	29	19 to 29	77.8	*	*	0	9	0	
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	24	24	14 to 24	100.0	*	*	0	1	0	
8/29/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0	
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	22	22	12 to 22	100.0	*	*	0	2	0	
8/29/2024	09:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	22	22	12 to 22	100.0	*	*	0	1	0	
8/29/2024	10:00	3	2	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	21	26	18 to 28	66.7	*	*	2	10	0	
8/29/2024	11:00	0	4	2	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	22.8	25	18 to 28	100.0	*	*	0	10	0	
8/29/2024	12:00	0	2	9	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	14	22.8	25	18 to 28	92.9	*	*	0	14	0	
8/29/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		3	10	21	20	4	0	0	0	0	0	0	0	0	0	0	0	0	0	58	23.3	28	18 to 28	82.8	*	*	3	55	0	

Outgoing: Average Hourly Volume for Week of 8/19/2024
Average Counts By Hour (8/19/2024) — Average Counts By Hour (8/19/2024)

Birchwood Loop SW 8-23 to 8-29 time error -3hrs
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 8/19/2024
Average Hourly WEEKDAY Speeds By Hour (8/19/2024) — Average Hourly WEEKDAY Speeds By Hour (8/19/2024)

Birchwood Loop SW 8-23 to 8-29 time error -3hrs
Average Weekly Speeds



Outgoing:Daily Volume for Week of 8/19/2024

Daily Vehicle Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

Daily Volumes

Vehicles

200
180
160
140
120
100
80
60
40
20
0

Monday

Tuesday

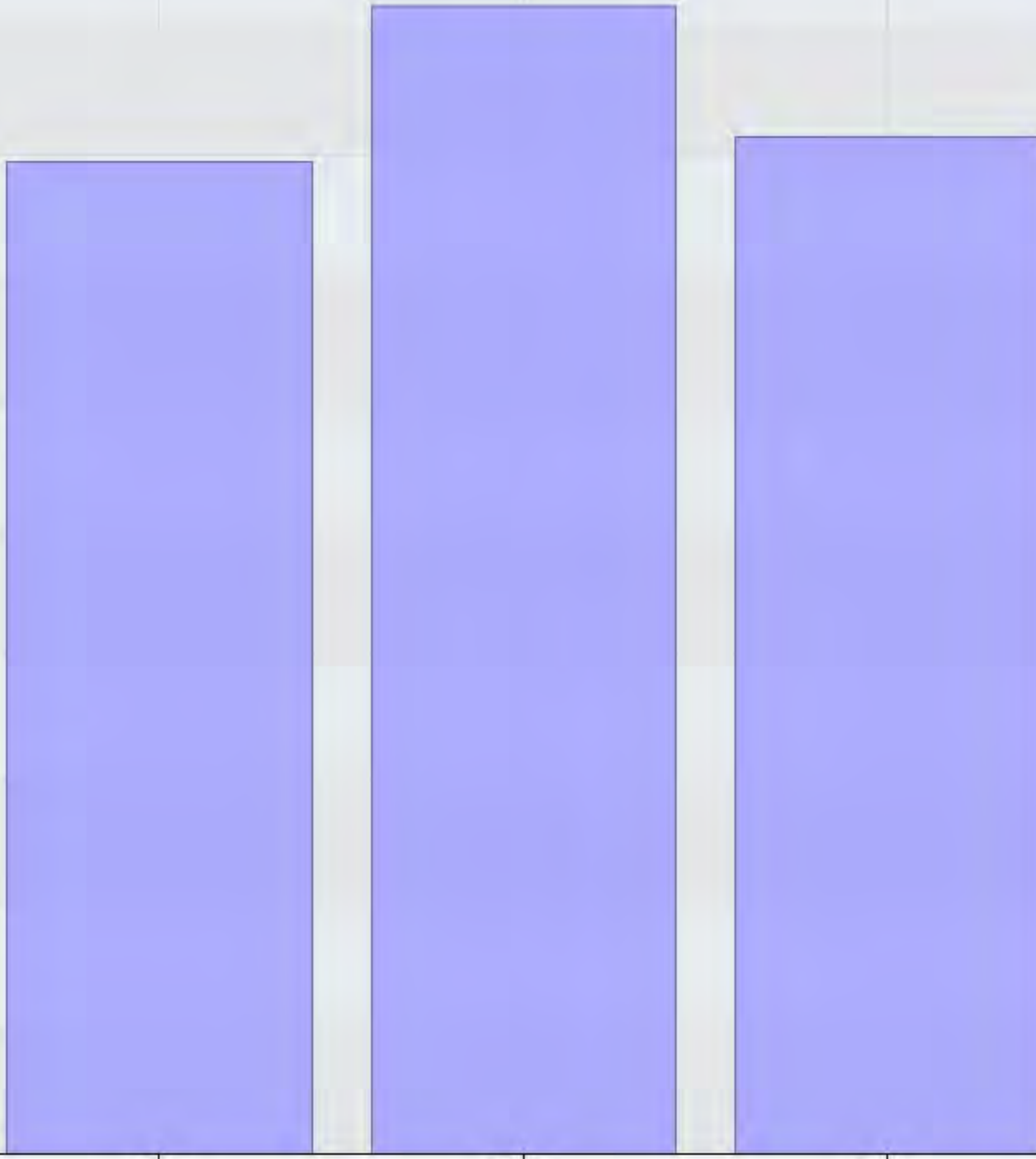
Wednesday

Thursday
Day

Friday

Saturday

Sunday



For Project: Birchwood Spur 8-23 to 8-29 time error -4hrs
 Project Notes:
 Location/Name: Incoming
 Report Generated: 10/3/2024 3:28:40 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 8/23/2024 3:00:00 PM through 8/29/2024 1:59:59 PM
 85th Percentile Speed: 48 MPH
 85th Percentile Vehicles: 2869
 Max Speed: 61 MPH on 8/26/2024 6:10:01 PM
 Total Vehicles: 3375
 AADT: 566

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	481	482
AM Peak	12:00 AM 36	30
PM Peak	8:00 PM 65	57

Speed

Speed Limit: 45
 85th Percentile Speed: 48
 50th Percentile Speed: 42
 10 MPH Pace Interval: 38.0 MPH to 48.0 MPH
 Average Speed: 41.96

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	159	137	149	38	130	122	165
% over limit	27.9	24.8	22.4	20.7	29.8	26.6	32.4
Avg Speeder	48.4	49.1	48.9	48.6	48.5	48.5	48.8
Avg Speed	41.7	41.9	41.6	40.5	42.8	41.7	42.8

Class Counts

	Number	%
VEH_SM	3	0.1
VEH_MED	3213	95.2
VEH_LG	159	4.7
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Incoming Summary
Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/23/2024 4:00:00 PM	44.0	8	9	49	49.0	11.1%	42.6
8/23/2024 5:00:00 PM	47.0	46	54	56	48.8	29.6%	42.7
8/23/2024 6:00:00 PM	49.0	40	47	55	48.9	34.0%	43.1
8/23/2024 7:00:00 PM	48.0	53	62	57	50.6	22.6%	41.8
8/23/2024 8:00:00 PM	47.0	68	80	57	47.7	30.0%	42.9
8/23/2024 9:00:00 PM	47.0	51	60	51	47.9	30.0%	42.7
8/23/2024 10:00:00 PM	48.0	47	55	59	48.4	40.0%	44.1
8/23/2024 11:00:00 PM	48.0	38	45	54	48.5	31.1%	42.7
8/24/2024 12:00:00 AM	46.0	20	24	50	47.2	20.8%	42.0
8/24/2024 1:00:00 AM	49.0	16	19	52	48.7	47.4%	43.7
8/24/2024 2:00:00 AM	45.0	16	19	49	47.3	15.8%	37.8
8/24/2024 3:00:00 AM	44.0	11	13	51	48.5	15.4%	39.6
8/24/2024 4:00:00 AM	43.0	5	6	48	48.0	16.7%	35.5
8/24/2024 5:00:00 AM	47.0	1	1	47	47.0	100.0%	47.0
8/24/2024 6:00:00 AM	42.0	2	2	42	0.0	0.0%	41.5
8/24/2024 7:00:00 AM	40.0	1	1	40	0.0	0.0%	40.0
8/24/2024 8:00:00 AM	37.0	1	1	37	0.0	0.0%	37.0
8/24/2024 9:00:00 AM	**No Data**						
8/24/2024 10:00:00 AM	21.0	1	1	21	0.0	0.0%	21.0
8/24/2024 11:00:00 AM	47.0	5	6	48	47.0	50.0%	41.8
8/24/2024 12:00:00 PM	44.0	8	9	44	0.0	0.0%	39.3
8/24/2024 1:00:00 PM	47.0	15	18	51	47.6	44.4%	41.8
8/24/2024 2:00:00 PM	47.0	16	19	50	48.3	21.1%	41.6
8/24/2024 3:00:00 PM	48.0	26	30	52	47.7	43.3%	43.3
8/24/2024 4:00:00 PM	47.0	33	39	51	47.9	30.8%	41.0
8/24/2024 5:00:00 PM	49.0	31	37	60	49.8	32.4%	43.6
8/24/2024 6:00:00 PM	44.0	31	36	52	49.0	13.9%	40.6
8/24/2024 7:00:00 PM	47.0	43	51	57	48.8	23.5%	42.4
8/24/2024 8:00:00 PM	47.0	37	43	51	47.8	32.6%	42.6
8/24/2024 9:00:00 PM	45.0	33	39	53	48.8	15.4%	41.9
8/24/2024 10:00:00 PM	47.0	29	34	53	48.8	17.6%	41.6
8/24/2024 11:00:00 PM	51.0	14	17	55	51.5	35.3%	43.5
8/25/2024 12:00:00 AM	48.0	14	17	50	48.8	29.4%	40.8
8/25/2024 1:00:00 AM	45.0	15	18	48	47.3	16.7%	41.2
8/25/2024 2:00:00 AM	46.0	8	10	47	46.7	30.0%	41.9
8/25/2024 3:00:00 AM	49.0	13	15	53	50.2	33.3%	42.9
8/25/2024 4:00:00 AM	49.0	1	1	49	49.0	100.0%	49.0
8/25/2024 5:00:00 AM	56.0	2	2	56	56.0	50.0%	44.0
8/25/2024 6:00:00 AM	37.0	2	2	37	0.0	0.0%	36.0
8/25/2024 7:00:00 AM	36.0	1	1	36	0.0	0.0%	36.0
8/25/2024 8:00:00 AM	46.0	2	2	46	46.0	50.0%	41.5
8/25/2024 9:00:00 AM	37.0	1	1	37	0.0	0.0%	37.0
8/25/2024 10:00:00 AM	**No Data**						
8/25/2024 11:00:00 AM	45.0	4	5	46	46.0	20.0%	41.2
8/25/2024 12:00:00 PM	46.0	4	5	51	48.5	40.0%	42.2
8/25/2024 1:00:00 PM	48.0	10	12	55	48.5	50.0%	44.7
8/25/2024 2:00:00 PM	46.0	15	18	50	47.3	22.2%	42.0

Incoming Summary
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

8/25/2024 3:00:00 PM	51.0	19	22	54	49.2	40.9%	45.0
8/25/2024 4:00:00 PM	49.0	33	39	57	49.3	33.3%	43.9
8/25/2024 5:00:00 PM	51.0	26	30	57	49.9	43.3%	43.3
8/25/2024 6:00:00 PM	49.0	40	47	55	49.5	38.3%	43.5
8/25/2024 7:00:00 PM	48.0	37	44	53	48.5	31.8%	42.5
8/25/2024 8:00:00 PM	47.0	49	58	53	48.3	24.1%	41.9
8/25/2024 9:00:00 PM	47.0	37	44	54	48.2	34.1%	42.4
8/25/2024 10:00:00 PM	48.0	52	61	55	48.6	31.1%	42.8
8/25/2024 11:00:00 PM	49.0	31	37	55	48.8	43.2%	44.1
8/26/2024 12:00:00 AM	47.0	31	36	51	49.0	19.4%	41.5
8/26/2024 1:00:00 AM	45.0	17	20	52	50.3	15.0%	42.1
8/26/2024 2:00:00 AM	45.0	12	14	47	47.0	14.3%	41.9
8/26/2024 3:00:00 AM	47.0	9	11	50	47.8	54.5%	43.6
8/26/2024 4:00:00 AM	**No Data**						
8/26/2024 5:00:00 AM	43.0	2	2	43	0.0	0.0%	33.0
8/26/2024 6:00:00 AM	27.0	2	2	27	0.0	0.0%	20.0
8/26/2024 7:00:00 AM	30.0	1	1	30	0.0	0.0%	30.0
8/26/2024 8:00:00 AM	**No Data**						
8/26/2024 9:00:00 AM	36.0	1	1	36	0.0	0.0%	36.0
8/26/2024 10:00:00 AM	47.0	4	5	54	50.5	40.0%	45.4
8/26/2024 11:00:00 AM	47.0	8	10	51	48.8	40.0%	43.2
8/26/2024 12:00:00 PM	46.0	14	17	52	48.3	23.5%	41.1
8/26/2024 1:00:00 PM	51.0	21	25	55	50.8	44.0%	42.5
8/26/2024 2:00:00 PM	46.0	35	41	49	47.4	17.1%	40.9
8/26/2024 3:00:00 PM	47.0	26	30	52	47.5	36.7%	41.1
8/26/2024 4:00:00 PM	47.0	37	44	52	48.5	22.7%	41.1
8/26/2024 5:00:00 PM	46.0	30	35	52	48.1	20.0%	40.9
8/26/2024 6:00:00 PM	48.0	40	47	55	48.3	31.9%	41.2
8/26/2024 7:00:00 PM	45.0	40	47	61	50.0	14.9%	41.3
8/26/2024 8:00:00 PM	47.0	49	58	59	49.0	24.1%	42.3
8/26/2024 9:00:00 PM	47.0	48	57	51	48.0	29.8%	42.3
8/26/2024 10:00:00 PM	48.0	45	53	55	47.7	39.6%	42.7
8/26/2024 11:00:00 PM	47.0	30	35	53	47.8	37.1%	42.4
8/27/2024 12:00:00 AM	47.0	13	15	51	47.8	33.3%	43.2
8/27/2024 1:00:00 AM	45.0	14	17	49	48.3	17.6%	40.3
8/27/2024 2:00:00 AM	41.0	14	17	44	0.0	0.0%	40.1
8/27/2024 3:00:00 AM	48.0	10	12	51	50.0	25.0%	43.4
8/27/2024 4:00:00 AM	49.0	2	2	49	48.0	100.0%	48.0
8/27/2024 5:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
8/27/2024 6:00:00 AM	37.0	2	2	37	0.0	0.0%	35.5
8/27/2024 7:00:00 AM	39.0	2	2	39	0.0	0.0%	36.5
8/27/2024 8:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
8/27/2024 9:00:00 AM	42.0	2	2	42	0.0	0.0%	41.0
8/27/2024 10:00:00 AM	49.0	6	7	53	51.0	28.6%	40.0
8/27/2024 11:00:00 AM	45.0	8	10	45	0.0	0.0%	42.2
8/27/2024 12:00:00 PM	52.0	20	23	56	51.5	34.8%	41.9
8/27/2024 1:00:00 PM	48.0	20	23	53	48.8	26.1%	40.9
8/27/2024 2:00:00 PM	45.0	13	15	52	50.5	13.3%	40.1

Incoming Summary
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

8/27/2024 3:00:00 PM	43.0	28	33	48	47.5	6.1%	40.4
8/27/2024 4:00:00 PM	48.0	42	49	57	48.5	36.7%	43.5
8/27/2024 5:00:00 PM	47.0	31	37	50	48.5	21.6%	41.4
8/27/2024 6:00:00 PM	46.0	36	42	56	48.9	21.4%	41.8
8/27/2024 7:00:00 PM	45.0	26	31	51	49.2	16.1%	42.2
8/27/2024 8:00:00 PM	51.0	37	44	55	50.1	40.9%	44.0
8/27/2024 9:00:00 PM	47.0	65	76	55	48.8	25.0%	41.3
8/27/2024 10:00:00 PM	50.0	35	41	55	49.2	41.5%	43.9
8/27/2024 11:00:00 PM	48.0	38	45	53	48.3	26.7%	42.2
8/28/2024 12:00:00 AM	44.0	18	21	49	47.3	14.3%	41.2
8/28/2024 1:00:00 AM	46.0	57	67	50	47.5	17.9%	42.3
8/28/2024 2:00:00 AM	45.0	32	38	51	48.2	15.8%	41.8
8/28/2024 3:00:00 AM	47.0	5	6	52	48.3	50.0%	44.0
8/28/2024 4:00:00 AM	**No Data**						
8/28/2024 5:00:00 AM	**No Data**						
8/28/2024 6:00:00 AM	43.0	2	2	43	0.0	0.0%	40.5
8/28/2024 7:00:00 AM	**No Data**						
8/28/2024 8:00:00 AM	39.0	3	3	39	0.0	0.0%	36.0
8/28/2024 9:00:00 AM	**No Data**						
8/28/2024 10:00:00 AM	47.0	7	8	49	48.0	25.0%	38.8
8/28/2024 11:00:00 AM	49.0	8	9	51	50.0	22.2%	43.3
8/28/2024 12:00:00 PM	53.0	17	20	56	52.1	35.0%	42.8
8/28/2024 1:00:00 PM	46.0	24	28	57	48.7	25.0%	41.4
8/28/2024 2:00:00 PM	46.0	20	24	56	50.4	20.8%	41.3
8/28/2024 3:00:00 PM	46.0	21	25	47	46.8	20.0%	41.2
8/28/2024 4:00:00 PM	45.0	36	42	53	48.0	14.3%	39.1
8/28/2024 5:00:00 PM	46.0	27	32	57	49.8	18.8%	40.4
8/28/2024 6:00:00 PM	45.0	41	48	51	48.9	14.6%	40.3
8/28/2024 7:00:00 PM	47.0	47	55	57	49.9	21.8%	41.2
8/28/2024 8:00:00 PM	47.0	34	40	52	48.5	25.0%	41.9
8/28/2024 9:00:00 PM	48.0	58	68	54	49.4	26.5%	42.9
8/28/2024 10:00:00 PM	48.0	49	58	54	48.3	37.9%	43.5
8/28/2024 11:00:00 PM	47.0	35	41	52	48.6	22.0%	41.6
8/29/2024 12:00:00 AM	47.0	42	50	51	48.8	20.0%	41.3
8/29/2024 1:00:00 AM	47.0	37	43	54	48.0	23.3%	41.9
8/29/2024 2:00:00 AM	47.0	21	25	50	47.7	28.0%	42.4
8/29/2024 3:00:00 AM	42.0	10	12	53	53.0	8.3%	39.5
8/29/2024 4:00:00 AM	39.0	3	4	50	50.0	25.0%	39.8
8/29/2024 5:00:00 AM	55.0	2	2	55	55.0	50.0%	46.5
8/29/2024 6:00:00 AM	43.0	1	1	43	0.0	0.0%	43.0
8/29/2024 7:00:00 AM	**No Data**						
8/29/2024 8:00:00 AM	**No Data**						
8/29/2024 9:00:00 AM	39.0	1	1	39	0.0	0.0%	39.0
8/29/2024 10:00:00 AM	41.0	5	6	42	0.0	0.0%	35.8
8/29/2024 11:00:00 AM	49.0	8	10	49	48.3	40.0%	44.0
8/29/2024 12:00:00 PM	49.0	18	21	51	48.6	38.1%	41.9
8/29/2024 1:00:00 PM	45.0	30	35	50	48.0	11.4%	38.5
8/29/2024 2:00:00 PM	43.0	20	24	52	49.5	8.3%	37.2

Incoming Summary
Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/24/2024 12:00:00 AM	48.0	371	436	59	48.5	29.8%	42.8
8/25/2024 12:00:00 AM	47.0	389	458	60	48.5	26.6%	41.7
8/26/2024 12:00:00 AM	48.0	434	510	57	48.8	32.4%	42.8
8/27/2024 12:00:00 AM	47.0	484	570	61	48.4	27.9%	41.7
8/28/2024 12:00:00 AM	48.0	470	553	57	49.1	24.8%	41.9
8/29/2024 12:00:00 AM	47.0	564	664	57	48.9	22.4%	41.6
8/29/2024 1:59:59 PM	47.0	156	184	55	48.6	20.7%	40.5

Incoming Weekly Counts
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	19	18	0	18.5	0
1 - 2	*	*	*	*	*	19	10	0	14.5	0
2 - 3	*	*	*	*	*	13	15	0	14	0
3 - 4	*	*	*	*	*	6	1	0	3.5	0
4 - 5	*	*	*	*	*	1	2	0	1.5	0
5 - 6	*	*	*	*	*	2	2	0	2	0
6 - 7	*	*	*	*	*	1	1	0	1	0
7 - 8	*	*	*	*	*	1	2	0	1.5	0
8 - 9	*	*	*	*	*	0	1	0	0.5	0
9 - 10	*	*	*	*	*	1	0	0	0.5	0
10 - 11	*	*	*	*	*	6	5	0	5.5	0
11 - 12	*	*	*	*	*	9	5	0	7	0
12 - 13	*	*	*	*	*	18	12	0	15	0
13 - 14	*	*	*	*	*	19	18	0	18.5	0
14 - 15	*	*	*	*	*	30	22	0	26	0
15 - 16	*	*	*	*	9	39	39	9	39	44
16 - 17	*	*	*	*	54	37	30	54	33.5	47
17 - 18	*	*	*	*	47	36	47	47	41.5	48.5
18 - 19	*	*	*	*	62	51	44	62	47.5	47.7
19 - 20	*	*	*	*	80	43	58	80	50.5	46.3
20 - 21	*	*	*	*	60	39	44	60	41.5	47
21 - 22	*	*	*	*	55	34	61	55	47.5	48
22 - 23	*	*	*	*	45	17	37	45	27	47.3
23 - 24	*	*	*	*	24	17	36	24	26.5	45.3
Totals	0	0	0	0	436	458	510			
% of Total	0%	0%	0%	0%	31.05%	32.62%	36.32%			

Incoming Weekly Counts
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	20	17	67	43	*	*	*	36.75	0	45.42
1 - 2	14	17	38	25	*	*	*	23.5	0	44.42
2 - 3	11	12	6	12	*	*	*	10.25	0	46
3 - 4	0	2	0	4	*	*	*	1.5	0	44
4 - 5	2	1	0	2	*	*	*	1.25	0	41
5 - 6	2	2	2	1	*	*	*	1.75	0	37.5
6 - 7	1	2	0	0	*	*	*	0.75	0	34.5
7 - 8	0	1	3	0	*	*	*	1	0	37
8 - 9	1	2	0	1	*	*	*	1	0	39
9 - 10	5	7	8	6	*	*	*	6.5	0	46
10 - 11	10	10	9	10	*	*	*	9.75	0	47.15
11 - 12	17	23	20	21	*	*	*	20.25	0	49.62
12 - 13	25	23	28	35	*	*	*	27.75	0	47.3
13 - 14	41	15	24	24	*	*	*	26	0	44.88
14 - 15	30	33	25	*	*	*	*	29.33	0	45.33
15 - 16	44	49	42	*	*	*	*	45	0	46.6
16 - 17	35	37	32	*	*	*	*	34.67	0	46.13
17 - 18	47	42	48	*	*	*	*	45.67	0	46.1
18 - 19	47	31	55	*	*	*	*	44.33	0	45.67
19 - 20	58	44	40	*	*	*	*	47.33	0	48
20 - 21	57	76	68	*	*	*	*	67	0	47.27
21 - 22	53	41	58	*	*	*	*	50.67	0	48.23
22 - 23	35	45	41	*	*	*	*	40.33	0	47.07
23 - 24	15	21	50	*	*	*	*	28.67	0	46
Totals	570	553	664	184	0	0	0			
% of Total	28.92%	28.06%	33.69%	9.34%	0%	0%	0%			

Incoming Monthly Counts

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	20	17	67	43	*	19	18	36.75	18.5	45.42
1 - 2	14	17	38	25	*	19	10	23.5	14.5	44.42
2 - 3	11	12	6	12	*	13	15	10.25	14	46
3 - 4	0	2	0	4	*	6	1	1.5	3.5	44
4 - 5	2	1	0	2	*	1	2	1.25	1.5	41
5 - 6	2	2	2	1	*	2	2	1.75	2	37.5
6 - 7	1	2	0	0	*	1	1	0.75	1	34.5
7 - 8	0	1	3	0	*	1	2	1	1.5	37
8 - 9	1	2	0	1	*	0	1	1	0.5	39
9 - 10	5	7	8	6	*	1	0	6.5	0.5	46
10 - 11	10	10	9	10	*	6	5	9.75	5.5	47.15
11 - 12	17	23	20	21	*	9	5	20.25	7	49.62
12 - 13	25	23	28	35	*	18	12	27.75	15	47.3
13 - 14	41	15	24	24	*	19	18	26	18.5	44.88
14 - 15	30	33	25	*	*	30	22	29.33	26	45.33
15 - 16	44	49	42	*	9	39	39	36	39	45.95
16 - 17	35	37	32	*	54	37	30	39.5	33.5	46.35
17 - 18	47	42	48	*	47	36	47	46	41.5	46.7
18 - 19	47	31	55	*	62	51	44	48.75	47.5	46.18
19 - 20	58	44	40	*	80	43	58	55.5	50.5	47.58
20 - 21	57	76	68	*	60	39	44	65.25	41.5	47.2
21 - 22	53	41	58	*	55	34	61	51.75	47.5	48.18
22 - 23	35	45	41	*	45	17	37	41.5	27	47.12
23 - 24	15	21	50	*	24	17	36	27.5	26.5	45.82
Totals	570	553	664	184	436	458	510			
% of Total	16.89%	16.39%	19.67%	5.45%	12.92%	13.57%	15.11%			

Incoming Weekly Speeds

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	43.68	41.22	0	42.49	0
1 - 2	*	*	*	*	*	37.84	41.9	0	39.24	0
2 - 3	*	*	*	*	*	39.62	42.87	0	41.36	0
3 - 4	*	*	*	*	*	35.5	49	0	37.43	0
4 - 5	*	*	*	*	*	47	44	0	45	0
5 - 6	*	*	*	*	*	41.5	36	0	38.75	0
6 - 7	*	*	*	*	*	40	36	0	38	0
7 - 8	*	*	*	*	*	37	41.5	0	40	0
8 - 9	*	*	*	*	*	0	37	0	37	0
9 - 10	*	*	*	*	*	21	0	0	21	0
10 - 11	*	*	*	*	*	41.83	41.2	0	41.55	0
11 - 12	*	*	*	*	*	39.33	42.2	0	40.36	0
12 - 13	*	*	*	*	*	41.83	44.67	0	42.97	0
13 - 14	*	*	*	*	*	41.63	42	0	41.81	0
14 - 15	*	*	*	*	*	43.3	45.05	0	44.04	0
15 - 16	*	*	*	*	42.56	40.97	43.9	42.56	42.44	44
16 - 17	*	*	*	*	42.69	43.62	43.27	42.69	43.46	47
17 - 18	*	*	*	*	43.13	40.56	43.49	43.13	42.22	48.5
18 - 19	*	*	*	*	41.84	42.37	42.45	41.84	42.41	47.7
19 - 20	*	*	*	*	42.91	42.58	41.91	42.91	42.2	46.3
20 - 21	*	*	*	*	42.68	41.87	42.43	42.68	42.17	47
21 - 22	*	*	*	*	44.07	41.59	42.79	44.07	42.36	48
22 - 23	*	*	*	*	42.69	43.47	44.14	42.69	43.93	47.3
23 - 24	*	*	*	*	41.96	40.82	41.5	41.96	41.28	45.3
Totals	0	0	0	0	42.8	41.7	42.8			
% of Total	0%	0%	0%	0%	33.62%	32.76%	33.62%			

Incoming Weekly Speeds

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	42.1	40.29	42.34	41.86	*	*	*	41.93	*	45.42
1 - 2	41.93	40.06	41.76	42.36	*	*	*	41.64	*	44.42
2 - 3	43.64	43.42	44	39.5	*	*	*	42.41	*	46
3 - 4	0	48	0	39.75	*	*	*	42.5	*	44
4 - 5	33	25	0	46.5	*	*	*	36.8	*	41
5 - 6	20	35.5	40.5	43	*	*	*	33.57	*	37.5
6 - 7	30	36.5	0	0	*	*	*	34.33	*	34.5
7 - 8	0	35	36	0	*	*	*	35.75	*	37
8 - 9	36	41	0	39	*	*	*	39.25	*	39
9 - 10	45.4	40	38.75	35.83	*	*	*	39.69	*	46
10 - 11	43.2	42.2	43.33	44	*	*	*	43.18	*	47.15
11 - 12	41.12	41.87	42.75	41.9	*	*	*	41.94	*	49.62
12 - 13	42.48	40.91	41.43	38.54	*	*	*	40.65	*	47.3
13 - 14	40.88	40.13	41.33	37.17	*	*	*	40.02	*	44.88
14 - 15	41.1	40.36	41.16	*	*	*	*	40.84	*	45.33
15 - 16	41.09	43.47	39.14	*	*	*	*	41.35	*	46.6
16 - 17	40.86	41.35	40.38	*	*	*	*	40.88	*	46.13
17 - 18	41.17	41.79	40.33	*	*	*	*	41.07	*	46.1
18 - 19	41.34	42.16	41.18	*	*	*	*	41.47	*	45.67
19 - 20	42.29	44.02	41.9	*	*	*	*	42.72	*	48
20 - 21	42.33	41.25	42.93	*	*	*	*	42.12	*	47.27
21 - 22	42.66	43.85	43.48	*	*	*	*	43.3	*	48.23
22 - 23	42.4	42.2	41.56	*	*	*	*	42.04	*	47.07
23 - 24	43.2	41.24	41.26	*	*	*	*	41.59	*	46
Totals	41.7	41.9	41.6	40.5	0	0	0			
% of Total	25.17%	25.29%	25.11%	24.44%	0%	0%	0%			

Incoming Monthly Speeds

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	42.1	40.29	42.34	41.86	*	43.68	41.22	41.93	42.49	45.42
1 - 2	41.93	40.06	41.76	42.36	*	37.84	41.9	41.64	39.24	44.42
2 - 3	43.64	43.42	44	39.5	*	39.62	42.87	42.41	41.36	46
3 - 4	0	48	0	39.75	*	35.5	49	42.5	37.43	44
4 - 5	33	25	0	46.5	*	47	44	36.8	45	41
5 - 6	20	35.5	40.5	43	*	41.5	36	33.57	38.75	37.5
6 - 7	30	36.5	0	0	*	40	36	34.33	38	34.5
7 - 8	0	35	36	0	*	37	41.5	35.75	40	37
8 - 9	36	41	0	39	*	0	37	39.25	37	39
9 - 10	45.4	40	38.75	35.83	*	21	0	39.69	21	46
10 - 11	43.2	42.2	43.33	44	*	41.83	41.2	43.18	41.55	47.15
11 - 12	41.12	41.87	42.75	41.9	*	39.33	42.2	41.94	40.36	49.62
12 - 13	42.48	40.91	41.43	38.54	*	41.83	44.67	40.65	42.97	47.3
13 - 14	40.88	40.13	41.33	37.17	*	41.63	42	40.02	41.81	44.88
14 - 15	41.1	40.36	41.16	*	*	43.3	45.05	40.84	44.04	45.33
15 - 16	41.09	43.47	39.14	*	42.56	40.97	43.9	41.42	42.44	45.95
16 - 17	40.86	41.35	40.38	*	42.69	43.62	43.27	41.5	43.46	46.35
17 - 18	41.17	41.79	40.33	*	43.13	40.56	43.49	41.59	42.22	46.7
18 - 19	41.34	42.16	41.18	*	41.84	42.37	42.45	41.58	42.41	46.18
19 - 20	42.29	44.02	41.9	*	42.91	42.58	41.91	42.79	42.2	47.58
20 - 21	42.33	41.25	42.93	*	42.68	41.87	42.43	42.25	42.17	47.2
21 - 22	42.66	43.85	43.48	*	44.07	41.59	42.79	43.5	42.36	48.18
22 - 23	42.4	42.2	41.56	*	42.69	43.47	44.14	42.22	43.93	47.12
23 - 24	43.2	41.24	41.26	*	41.96	40.82	41.5	41.67	41.28	45.82
Totals	41.7	41.9	41.6	40.5	42.8	41.7	42.8			
% of Total	14.23%	14.3%	14.2%	13.82%	14.61%	14.23%	14.61%			

Incoming Weekly EightyFifthSpeeds
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	49	45	0	47	0
1 - 2	*	*	*	*	*	45	46	0	45.5	0
2 - 3	*	*	*	*	*	44	49	0	46.5	0
3 - 4	*	*	*	*	*	43	49	0	46	0
4 - 5	*	*	*	*	*	47	56	0	51.5	0
5 - 6	*	*	*	*	*	42	37	0	39.5	0
6 - 7	*	*	*	*	*	40	36	0	38	0
7 - 8	*	*	*	*	*	37	46	0	41.5	0
8 - 9	*	*	*	*	*	0	37	0	37	0
9 - 10	*	*	*	*	*	21	0	0	21	0
10 - 11	*	*	*	*	*	47	45	0	46	0
11 - 12	*	*	*	*	*	43.5	46	0	44.75	0
12 - 13	*	*	*	*	*	46.8	47.5	0	47.15	0
13 - 14	*	*	*	*	*	46.5	45.5	0	46	0
14 - 15	*	*	*	*	*	48	51	0	49.5	0
15 - 16	*	*	*	*	44	46.8	48.5	44	47.65	44
16 - 17	*	*	*	*	47	48.3	51	47	49.65	47
17 - 18	*	*	*	*	48.5	44	49	48.5	46.5	48.5
18 - 19	*	*	*	*	47.7	47	48	47.7	47.5	47.7
19 - 20	*	*	*	*	46.3	47	46.7	46.3	46.85	46.3
20 - 21	*	*	*	*	47	45	47	47	46	47
21 - 22	*	*	*	*	48	46.3	47.5	48	46.9	48
22 - 23	*	*	*	*	47.3	51	48.5	47.3	49.75	47.3
23 - 24	*	*	*	*	45.3	48	47	45.3	47.5	45.3
Totals	0	0	0	0	421.1	1023.2	1069.2			
% of Total	0%	0%	0%	0%	16.75%	40.71%	42.54%			

Incoming Weekly EightyFifthSpeeds
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	45	45	45.5	46.2	*	*	*	45.42	0	45.42
1 - 2	45	41	45	46.7	*	*	*	44.42	0	44.42
2 - 3	47	48	47	42	*	*	*	46	0	46
3 - 4	0	49	0	39	*	*	*	44	0	44
4 - 5	43	25	0	55	*	*	*	41	0	41
5 - 6	27	37	43	43	*	*	*	37.5	0	37.5
6 - 7	30	39	0	0	*	*	*	34.5	0	34.5
7 - 8	0	35	39	0	*	*	*	37	0	37
8 - 9	36	42	0	39	*	*	*	39	0	39
9 - 10	47	49	47	41	*	*	*	46	0	46
10 - 11	47	44.3	49	48.3	*	*	*	47.15	0	47.15
11 - 12	45.5	52	52.5	48.5	*	*	*	49.62	0	49.62
12 - 13	51	47.7	46	44.5	*	*	*	47.3	0	47.3
13 - 14	46	45	45.5	43	*	*	*	44.88	0	44.88
14 - 15	47	43	46	*	*	*	*	45.33	0	45.33
15 - 16	47	47.8	45	*	*	*	*	46.6	0	46.6
16 - 17	45.7	46.7	46	*	*	*	*	46.13	0	46.13
17 - 18	47.3	46	45	*	*	*	*	46.1	0	46.1
18 - 19	45	45	47	*	*	*	*	45.67	0	45.67
19 - 20	46.8	50.5	46.7	*	*	*	*	48	0	48
20 - 21	47	47	47.8	*	*	*	*	47.27	0	47.27
21 - 22	47.2	49.5	48	*	*	*	*	48.23	0	48.23
22 - 23	47	47.2	47	*	*	*	*	47.07	0	47.07
23 - 24	47	44	47	*	*	*	*	46	0	46
Totals	976.5	1065.7	925	536.2	0	0	0			
% of Total	27.87%	30.42%	26.4%	15.31%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	45	45	45.5	46.2	*	49	45	45.42	47	45.42
1 - 2	45	41	45	46.7	*	45	46	44.42	45.5	44.42
2 - 3	47	48	47	42	*	44	49	46	46.5	46
3 - 4	0	49	0	39	*	43	49	44	46	44
4 - 5	43	25	0	55	*	47	56	41	51.5	41
5 - 6	27	37	43	43	*	42	37	37.5	39.5	37.5
6 - 7	30	39	0	0	*	40	36	34.5	38	34.5
7 - 8	0	35	39	0	*	37	46	37	41.5	37
8 - 9	36	42	0	39	*	0	37	39	37	39
9 - 10	47	49	47	41	*	21	0	46	21	46
10 - 11	47	44.3	49	48.3	*	47	45	47.15	46	47.15
11 - 12	45.5	52	52.5	48.5	*	43.5	46	49.62	44.75	49.62
12 - 13	51	47.7	46	44.5	*	46.8	47.5	47.3	47.15	47.3
13 - 14	46	45	45.5	43	*	46.5	45.5	44.88	46	44.88
14 - 15	47	43	46	*	*	48	51	45.33	49.5	45.33
15 - 16	47	47.8	45	*	44	46.8	48.5	45.95	47.65	45.95
16 - 17	45.7	46.7	46	*	47	48.3	51	46.35	49.65	46.35
17 - 18	47.3	46	45	*	48.5	44	49	46.7	46.5	46.7
18 - 19	45	45	47	*	47.7	47	48	46.18	47.5	46.18
19 - 20	46.8	50.5	46.7	*	46.3	47	46.7	47.58	46.85	47.58
20 - 21	47	47	47.8	*	47	45	47	47.2	46	47.2
21 - 22	47.2	49.5	48	*	48	46.3	47.5	48.18	46.9	48.18
22 - 23	47	47.2	47	*	47.3	51	48.5	47.12	49.75	47.12
23 - 24	47	44	47	*	45.3	48	47	45.82	47.5	45.82

Summary of Violators

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	184	42.0	40	48.2
01:00:00	123	41.1	21	47.6
02:00:00	69	42.0	20	49.2
03:00:00	13	39.8	5	48.6
04:00:00	8	39.9	3	52.7
05:00:00	11	35.5	0	0.0
06:00:00	5	35.8	0	0.0
07:00:00	7	37.6	1	46.0
08:00:00	5	38.8	0	0.0
09:00:00	27	39.0	6	49.8
10:00:00	50	42.8	14	48.2
11:00:00	95	41.7	29	50.2
12:00:00	141	41.1	42	49.0
13:00:00	141	40.5	24	48.6
14:00:00	140	42.0	40	47.9
15:00:00	222	41.8	60	48.5
16:00:00	225	42.1	62	49.2
17:00:00	267	41.8	70	48.9
18:00:00	290	41.9	64	49.5
19:00:00	323	42.6	94	48.5
20:00:00	344	42.2	93	48.5
21:00:00	302	43.1	107	48.4
22:00:00	220	42.6	70	48.7
23:00:00	163	41.5	35	48.3

Incoming Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/23/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	15:00	0	0	0	0	0	1	7	1	0	0	0	0	0	0	0	0	0	0	9	42.6	44	39 to 49	100.0	1	11.1	0	9	0	
8/23/2024	16:00	0	0	0	1	2	8	23	15	3	2	0	0	0	0	0	0	0	0	54	42.7	47	39 to 49	74.1	16	29.6	0	52	2	
8/23/2024	17:00	0	0	0	0	3	8	18	12	5	1	0	0	0	0	0	0	0	0	47	43.1	48.5	40 to 50	70.2	16	34.0	0	42	5	
8/23/2024	18:00	0	1	0	0	9	7	26	13	3	3	0	0	0	0	0	0	0	0	62	41.8	47.7	39 to 49	64.5	14	22.6	0	59	3	
8/23/2024	19:00	0	0	0	0	5	12	32	28	2	1	0	0	0	0	0	0	0	0	80	42.9	46.3	39 to 49	85.0	24	30.0	0	77	3	
8/23/2024	20:00	0	0	0	0	2	16	18	21	3	0	0	0	0	0	0	0	0	0	60	42.7	47	37 to 47	80.0	18	30.0	0	58	2	
8/23/2024	21:00	0	0	0	1	0	8	20	20	5	1	0	0	0	0	0	0	0	0	55	44.1	48	38 to 48	78.2	22	40.0	0	53	2	
8/23/2024	22:00	0	1	0	0	2	6	21	12	3	0	0	0	0	0	0	0	0	0	45	42.7	47.3	39 to 49	77.8	14	31.1	0	45	0	
8/23/2024	23:00	0	0	0	0	0	7	10	6	1	0	0	0	0	0	0	0	0	0	24	42	45.3	36 to 46	87.5	5	20.8	0	22	2	
24 Hr Summary		0	2	0	2	23	73	175	128	25	8	0	0	0	0	0	0	0	0	436	42.8	48	39 to 49	75.2	130	29.8	0	417	19	

Incoming Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/24/2024	00:00	0	0	0	0	2	3	4	7	3	0	0	0	0	0	0	0	0	0	19	43.7	49	42 to 52	68.4	9	47.4	0	19	0	
8/24/2024	01:00	0	1	0	4	0	4	4	6	0	0	0	0	0	0	0	0	0	0	19	37.8	45	35 to 45	57.9	3	15.8	0	18	1	
8/24/2024	02:00	0	0	0	1	2	2	6	1	1	0	0	0	0	0	0	0	0	0	13	39.6	44	34 to 44	69.2	2	15.4	0	13	0	
8/24/2024	03:00	0	1	0	0	1	1	2	1	0	0	0	0	0	0	0	0	0	0	6	35.5	43	30 to 40	50.0	1	16.7	0	6	0	
8/24/2024	04:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	47	47	37 to 47	100.0	1	100.0	0	1	0	
8/24/2024	05:00	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	41.5	42	32 to 42	100.0	*	*	0	2	0	
8/24/2024	06:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	40	40	30 to 40	100.0	*	*	0	1	0	
8/24/2024	07:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	37	37	27 to 37	100.0	*	*	0	1	0	
8/24/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	09:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	21	21	11 to 21	100.0	*	*	0	1	0	
8/24/2024	10:00	0	0	0	0	1	1	1	3	0	0	0	0	0	0	0	0	0	0	6	41.8	47	38 to 48	66.7	3	50.0	0	6	0	
8/24/2024	11:00	0	0	0	1	1	1	6	0	0	0	0	0	0	0	0	0	0	0	9	39.3	43.5	34 to 44	77.8	*	*	0	9	0	
8/24/2024	12:00	0	0	0	0	4	2	4	6	2	0	0	0	0	0	0	0	0	0	18	41.8	46.8	37 to 47	66.7	8	44.4	0	18	0	
8/24/2024	13:00	0	0	0	1	1	3	10	3	1	0	0	0	0	0	0	0	0	0	19	41.6	46.5	37 to 47	78.9	4	21.1	0	19	0	
8/24/2024	14:00	0	0	0	0	1	5	11	11	2	0	0	0	0	0	0	0	0	0	30	43.3	48	39 to 49	80.0	13	43.3	0	30	0	
8/24/2024	15:00	0	0	0	2	3	11	10	10	3	0	0	0	0	0	0	0	0	0	39	41	46.8	37 to 47	66.7	12	30.8	0	38	1	
8/24/2024	16:00	0	0	0	0	2	5	14	12	3	0	1	0	0	0	0	0	0	0	37	43.6	48.3	39 to 49	73.0	12	32.4	0	37	0	
8/24/2024	17:00	0	0	0	0	4	12	15	3	2	0	0	0	0	0	0	0	0	0	36	40.6	44	34 to 44	80.6	5	13.9	0	35	1	
8/24/2024	18:00	0	0	0	1	4	7	21	16	1	1	0	0	0	0	0	0	0	0	51	42.4	47	38 to 48	80.4	12	23.5	0	51	0	
8/24/2024	19:00	0	0	0	0	3	7	15	15	3	0	0	0	0	0	0	0	0	0	43	42.6	47	38 to 48	76.7	14	32.6	0	43	0	
8/24/2024	20:00	0	0	1	0	2	5	21	8	2	0	0	0	0	0	0	0	0	0	39	41.9	45	36 to 46	79.5	6	15.4	0	39	0	
8/24/2024	21:00	0	0	0	3	0	7	17	5	2	0	0	0	0	0	0	0	0	0	34	41.6	46.3	37 to 47	82.4	6	17.6	0	33	1	
8/24/2024	22:00	0	0	0	0	2	2	7	1	4	1	0	0	0	0	0	0	0	0	17	43.5	51	38 to 48	58.8	6	35.3	0	16	1	
8/24/2024	23:00	0	0	0	0	5	3	3	5	1	0	0	0	0	0	0	0	0	0	17	40.8	48	33 to 43	58.8	5	29.4	0	17	0	
24 Hr Summary		0	2	2	13	38	82	174	114	30	2	1	0	0	0	0	0	0	0	458	41.7	47	39 to 49	69.7	122	26.6	0	453	5	

Incoming Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/25/2024	00:00	0	0	0	0	2	4	7	5	0	0	0	0	0	0	0	0	0	0	18	41.2	45	38 to 48	83.3	3	16.7	0	17	1	
8/25/2024	01:00	0	0	0	0	0	3	4	3	0	0	0	0	0	0	0	0	0	0	10	41.9	46	37 to 47	90.0	3	30.0	0	10	0	
8/25/2024	02:00	0	0	0	1	0	3	5	4	2	0	0	0	0	0	0	0	0	0	15	42.9	49	35 to 45	60.0	5	33.3	0	15	0	
8/25/2024	03:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	49	49	39 to 49	100.0	1	100.0	0	1	0	
8/25/2024	04:00	0	0	0	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	2	44	56	22 to 32	50.0	1	50.0	0	2	0	
8/25/2024	05:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	36	37	27 to 37	100.0	*	*	0	2	0	
8/25/2024	06:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	36	36	26 to 36	100.0	*	*	0	1	0	
8/25/2024	07:00	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	41.5	46	36 to 46	100.0	1	50.0	0	2	0	
8/25/2024	08:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	37	37	27 to 37	100.0	*	*	0	1	0	
8/25/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	10:00	0	0	0	0	0	2	1	2	0	0	0	0	0	0	0	0	0	0	5	41.2	45	36 to 46	100.0	1	20.0	0	5	0	
8/25/2024	11:00	0	0	0	0	1	0	2	1	1	0	0	0	0	0	0	0	0	0	5	42.2	46	32 to 42	60.0	2	40.0	0	5	0	
8/25/2024	12:00	0	0	0	0	1	1	4	5	0	1	0	0	0	0	0	0	0	0	12	44.7	47.5	38 to 48	83.3	6	50.0	0	11	1	
8/25/2024	13:00	0	0	0	0	0	5	8	4	1	0	0	0	0	0	0	0	0	0	18	42	45.5	36 to 46	88.9	4	22.2	0	18	0	
8/25/2024	14:00	0	0	0	0	1	1	8	8	4	0	0	0	0	0	0	0	0	0	22	45	51	38 to 48	77.3	9	40.9	0	22	0	
8/25/2024	15:00	0	0	0	0	1	6	13	14	4	1	0	0	0	0	0	0	0	0	39	43.9	48.5	39 to 49	71.8	13	33.3	0	38	1	
8/25/2024	16:00	0	1	0	1	1	5	6	10	4	2	0	0	0	0	0	0	0	0	30	43.3	51	42 to 52	60.0	13	43.3	0	27	3	
8/25/2024	17:00	0	1	0	1	0	8	17	13	6	1	0	0	0	0	0	0	0	0	47	43.5	49	39 to 49	70.2	18	38.3	0	46	1	
8/25/2024	18:00	0	0	1	1	2	7	15	14	4	0	0	0	0	0	0	0	0	0	44	42.5	48	40 to 50	72.7	14	31.8	0	44	0	
8/25/2024	19:00	0	0	0	2	3	14	15	20	4	0	0	0	0	0	0	0	0	0	58	41.9	46.7	38 to 48	77.6	14	24.1	0	56	2	
8/25/2024	20:00	0	0	0	0	4	9	14	13	4	0	0	0	0	0	0	0	0	0	44	42.4	47	36 to 46	70.5	15	34.1	0	44	0	
8/25/2024	21:00	0	0	0	1	4	11	20	18	6	1	0	0	0	0	0	0	0	0	61	42.8	47.5	38 to 48	75.4	19	31.1	0	58	3	
8/25/2024	22:00	0	0	0	0	0	7	13	13	3	1	0	0	0	0	0	0	0	0	37	44.1	48.5	39 to 49	75.7	16	43.2	0	37	0	
8/25/2024	23:00	0	0	0	0	2	9	15	6	4	0	0	0	0	0	0	0	0	0	36	41.5	47	35 to 45	75.0	7	19.4	0	36	0	
24 Hr Summary		0	2	1	7	23	100	167	155	47	8	0	0	0	0	0	0	0	0	510	42.8	48	38 to 48	69.4	165	32.4	0	498	12	

Incoming Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/26/2024	00:00	0	0	0	0	2	4	10	2	2	0	0	0	0	0	0	0	0	0	20	42.1	45	34 to 44	75.0	3	15.0	0	20	0	
8/26/2024	01:00	0	0	0	0	0	3	8	3	0	0	0	0	0	0	0	0	0	0	14	41.9	45	37 to 47	92.9	2	14.3	0	14	0	
8/26/2024	02:00	0	0	0	0	1	2	1	5	2	0	0	0	0	0	0	0	0	0	11	43.6	47	40 to 50	72.7	6	54.5	0	10	1	
8/26/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	04:00	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	2	33	43	13 to 23	50.0	*	*	0	2	0	
8/26/2024	05:00	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	20	27	3 to 13	50.0	*	*	0	2	0	
8/26/2024	06:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	30	30	20 to 30	100.0	*	*	0	0	1	
8/26/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	08:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	36	36	26 to 36	100.0	*	*	0	1	0	
8/26/2024	09:00	0	0	0	0	0	0	3	1	1	0	0	0	0	0	0	0	0	0	5	45.4	47	37 to 47	80.0	2	40.0	0	4	1	
8/26/2024	10:00	0	0	0	0	2	1	2	3	2	0	0	0	0	0	0	0	0	0	10	43.2	47	41 to 51	70.0	4	40.0	0	10	0	
8/26/2024	11:00	0	0	0	0	6	0	4	6	1	0	0	0	0	0	0	0	0	0	17	41.1	45.5	39 to 49	58.8	4	23.5	0	15	2	
8/26/2024	12:00	0	1	0	0	4	4	4	4	7	1	0	0	0	0	0	0	0	0	25	42.5	51	42 to 52	52.0	11	44.0	0	22	3	
8/26/2024	13:00	0	0	0	1	1	12	17	10	0	0	0	0	0	0	0	0	0	0	41	40.9	46	35 to 45	78.0	7	17.1	0	37	4	
8/26/2024	14:00	1	0	0	1	4	3	7	13	1	0	0	0	0	0	0	0	0	0	30	41.1	47	38 to 48	70.0	11	36.7	0	28	2	
8/26/2024	15:00	0	0	1	1	0	14	18	7	3	0	0	0	0	0	0	0	0	0	44	41.1	47	38 to 48	77.3	10	22.7	0	39	5	
8/26/2024	16:00	0	0	1	2	2	7	13	8	2	0	0	0	0	0	0	0	0	0	35	40.9	45.7	38 to 48	77.1	7	20.0	0	33	2	
8/26/2024	17:00	0	0	1	2	4	12	10	16	1	1	0	0	0	0	0	0	0	0	47	41.2	47.3	36 to 46	61.7	15	31.9	0	44	3	
8/26/2024	18:00	0	0	0	1	4	12	18	10	1	0	1	0	0	0	0	0	0	0	47	41.3	45	35 to 45	74.5	7	14.9	0	46	1	
8/26/2024	19:00	0	0	0	3	2	12	19	19	2	1	0	0	0	0	0	0	0	0	58	42.3	46.8	37 to 47	75.9	14	24.1	0	58	0	
8/26/2024	20:00	0	0	0	0	5	11	22	14	5	0	0	0	0	0	0	0	0	0	57	42.3	47	38 to 48	75.4	17	29.8	0	56	1	
8/26/2024	21:00	0	1	0	0	2	12	14	21	2	1	0	0	0	0	0	0	0	0	53	42.7	47.2	38 to 48	75.5	21	39.6	0	50	3	
8/26/2024	22:00	0	0	1	0	2	7	11	12	2	0	0	0	0	0	0	0	0	0	35	42.4	47	38 to 48	77.1	13	37.1	0	35	0	
8/26/2024	23:00	0	0	0	1	0	1	5	7	1	0	0	0	0	0	0	0	0	0	15	43.2	47	38 to 48	80.0	5	33.3	0	15	0	
24 Hr Summary		2	2	5	13	42	118	187	161	35	4	1	0	0	0	0	0	0	0	570	41.7	47	38 to 48	69.8	159	27.9	0	541	29	

Incoming Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/27/2024	00:00	0	0	0	1	0	6	6	4	0	0	0	0	0	0	0	0	0	0	17	40.3	45	35 to 45	76.5	3	17.6	0	17	0
8/27/2024	01:00	0	0	0	0	0	7	10	0	0	0	0	0	0	0	0	0	0	0	17	40.1	41	34 to 44	100.0	*	*	0	17	0
8/27/2024	02:00	0	0	0	0	0	1	8	1	2	0	0	0	0	0	0	0	0	0	12	43.4	48	34 to 44	75.0	3	25.0	0	12	0
8/27/2024	03:00	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	48	49	39 to 49	100.0	2	100.0	0	2	0
8/27/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0
8/27/2024	05:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	35.5	37	27 to 37	100.0	*	*	0	2	0
8/27/2024	06:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	36.5	39	29 to 39	100.0	*	*	0	2	0
8/27/2024	07:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	1	0
8/27/2024	08:00	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	41	42	32 to 42	100.0	*	*	0	2	0
8/27/2024	09:00	0	0	0	2	0	1	2	1	1	0	0	0	0	0	0	0	0	0	7	40	49	39 to 49	57.1	2	28.6	0	6	1
8/27/2024	10:00	0	0	0	0	0	2	5	3	0	0	0	0	0	0	0	0	0	0	10	42.2	44.3	35 to 45	100.0	*	*	0	10	0
8/27/2024	11:00	0	0	0	1	5	4	5	3	4	1	0	0	0	0	0	0	0	0	23	41.9	52	30 to 40	47.8	8	34.8	0	18	5
8/27/2024	12:00	0	0	0	1	4	4	6	7	1	0	0	0	0	0	0	0	0	0	23	40.9	47.7	38 to 48	60.9	6	26.1	0	19	4
8/27/2024	13:00	0	0	0	2	1	3	5	3	1	0	0	0	0	0	0	0	0	0	15	40.1	45	35 to 45	66.7	2	13.3	0	12	3
8/27/2024	14:00	0	0	0	0	5	7	18	3	0	0	0	0	0	0	0	0	0	0	33	40.4	43	34 to 44	87.9	2	6.1	1	29	3
8/27/2024	15:00	0	0	0	0	2	6	18	19	3	1	0	0	0	0	0	0	0	0	49	43.5	47.8	38 to 48	79.6	18	36.7	0	44	5
8/27/2024	16:00	0	0	0	1	6	6	13	9	2	0	0	0	0	0	0	0	0	0	37	41.4	46.7	37 to 47	67.6	8	21.6	1	35	1
8/27/2024	17:00	0	0	0	1	3	7	21	7	2	1	0	0	0	0	0	0	0	0	42	41.8	46	36 to 46	71.4	9	21.4	0	40	2
8/27/2024	18:00	0	0	0	0	2	8	11	7	3	0	0	0	0	0	0	0	0	0	31	42.2	45	35 to 45	77.4	5	16.1	0	30	1
8/27/2024	19:00	0	0	0	0	2	8	13	12	7	2	0	0	0	0	0	0	0	0	44	44	50.5	38 to 48	59.1	18	40.9	0	42	2
8/27/2024	20:00	0	0	1	2	10	12	27	18	5	1	0	0	0	0	0	0	0	0	76	41.3	47	37 to 47	64.5	19	25.0	0	73	3
8/27/2024	21:00	0	0	0	0	3	6	13	12	6	1	0	0	0	0	0	0	0	0	41	43.9	49.5	40 to 50	65.9	17	41.5	0	40	1
8/27/2024	22:00	0	0	0	0	4	8	21	9	3	0	0	0	0	0	0	0	0	0	45	42.2	47.2	38 to 48	73.3	12	26.7	0	43	2
8/27/2024	23:00	0	0	0	0	1	4	13	3	0	0	0	0	0	0	0	0	0	0	21	41.2	44	34 to 44	85.7	3	14.3	0	20	1
24 Hr Summary		0	0	1	12	50	103	217	123	40	7	0	0	0	0	0	0	0	0	553	41.9	48	39 to 49	67.1	137	24.8	2	517	34

Incoming Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/28/2024	00:00	0	0	0	0	0	13	37	16	1	0	0	0	0	0	0	0	0	0	67	42.3	45.5	36 to 46	85.1	12	17.9	0	67	0	
8/28/2024	01:00	0	0	0	0	2	7	22	6	1	0	0	0	0	0	0	0	0	0	38	41.8	45	35 to 45	78.9	6	15.8	0	38	0	
8/28/2024	02:00	0	0	0	0	0	1	2	2	1	0	0	0	0	0	0	0	0	0	6	44	47	42 to 52	83.3	3	50.0	0	6	0	
8/28/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	05:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	40.5	43	33 to 43	100.0	*	*	0	1	1	
8/28/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	07:00	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3	36	39	29 to 39	100.0	*	*	0	2	1	
8/28/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	09:00	0	0	1	0	0	3	2	2	0	0	0	0	0	0	0	0	0	0	8	38.8	47	39 to 49	75.0	2	25.0	0	8	0	
8/28/2024	10:00	0	0	0	0	0	2	4	2	1	0	0	0	0	0	0	0	0	0	9	43.3	49	35 to 45	77.8	2	22.2	0	9	0	
8/28/2024	11:00	0	0	0	1	3	4	5	2	3	2	0	0	0	0	0	0	0	0	20	42.8	52.5	33 to 43	50.0	7	35.0	0	18	2	
8/28/2024	12:00	0	0	0	0	4	7	9	6	1	1	0	0	0	0	0	0	0	0	28	41.4	46	36 to 46	71.4	7	25.0	0	22	6	
8/28/2024	13:00	0	0	0	1	5	2	8	6	0	2	0	0	0	0	0	0	0	0	24	41.3	45.5	36 to 46	58.3	5	20.8	0	23	1	
8/28/2024	14:00	0	0	0	2	1	4	8	10	0	0	0	0	0	0	0	0	0	0	25	41.2	46	37 to 47	80.0	5	20.0	0	24	1	
8/28/2024	15:00	0	0	1	0	8	12	13	7	1	0	0	0	0	0	0	0	0	0	42	39.1	45	36 to 46	69.0	6	14.3	0	36	6	
8/28/2024	16:00	0	0	1	0	4	9	12	3	2	1	0	0	0	0	0	0	0	0	32	40.4	46	33 to 43	71.9	6	18.8	0	28	4	
8/28/2024	17:00	0	1	0	2	7	8	18	9	3	0	0	0	0	0	0	0	0	0	48	40.3	45	35 to 45	64.6	7	14.6	0	44	4	
8/28/2024	18:00	0	0	0	1	6	16	16	10	5	1	0	0	0	0	0	0	0	0	55	41.2	47	35 to 45	65.5	12	21.8	0	49	6	
8/28/2024	19:00	0	0	0	0	3	10	14	9	4	0	0	0	0	0	0	0	0	0	40	41.9	46.7	38 to 48	77.5	10	25.0	0	36	4	
8/28/2024	20:00	0	0	0	1	1	14	26	18	8	0	0	0	0	0	0	0	0	0	68	42.9	47.8	38 to 48	75.0	18	26.5	0	65	3	
8/28/2024	21:00	0	0	1	0	2	7	20	23	5	0	0	0	0	0	0	0	0	0	58	43.5	48	40 to 50	77.6	22	37.9	0	56	2	
8/28/2024	22:00	0	0	0	0	4	12	12	11	2	0	0	0	0	0	0	0	0	0	41	41.6	47	38 to 48	73.2	9	22.0	0	40	1	
8/28/2024	23:00	0	1	0	1	2	13	18	12	3	0	0	0	0	0	0	0	0	0	50	41.3	47	35 to 45	72.0	10	20.0	0	49	1	
24 Hr Summary		0	2	4	9	53	147	247	154	41	7	0	0	0	0	0	0	0	0	664	41.6	47	37 to 47	69.4	149	22.4	0	621	43	

Incoming Histogram

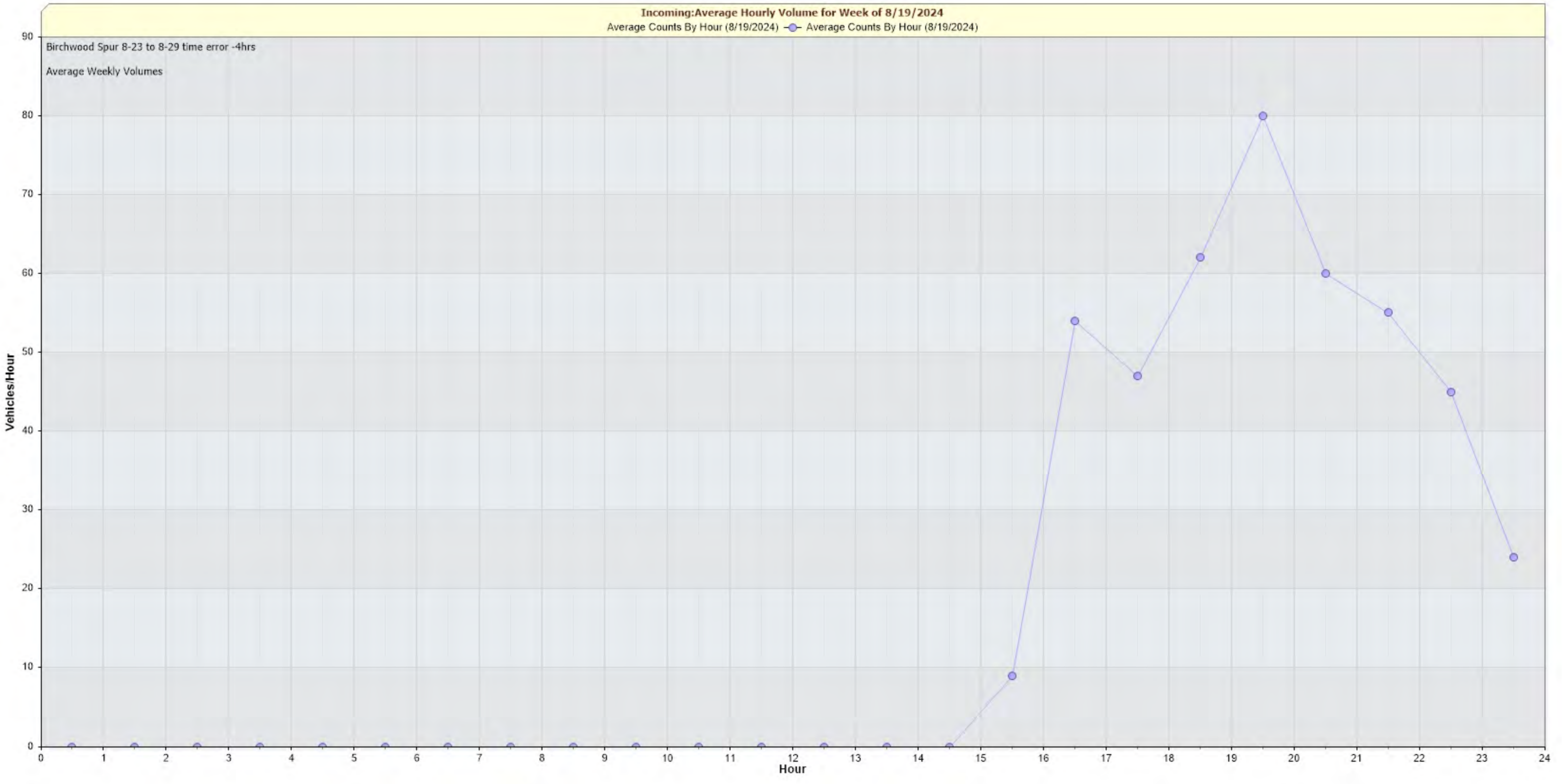
Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	0	0	0	0	2	14	12	13	2	0	0	0	0	0	0	0	0	0	43	41.9	46.2	37 to 47	79.1	10	23.3	0	42	1	
8/29/2024	01:00	0	0	0	0	1	6	9	8	1	0	0	0	0	0	0	0	0	0	25	42.4	46.7	37 to 47	80.0	7	28.0	0	25	0	
8/29/2024	02:00	0	0	0	1	1	2	7	0	1	0	0	0	0	0	0	0	0	0	12	39.5	42	32 to 42	75.0	1	8.3	0	11	1	
8/29/2024	03:00	0	0	0	0	1	2	0	0	1	0	0	0	0	0	0	0	0	0	4	39.8	39	29 to 39	75.0	1	25.0	0	4	0	
8/29/2024	04:00	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2	46.5	55	28 to 38	50.0	1	50.0	0	1	1	
8/29/2024	05:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	43	43	33 to 43	100.0	*	*	0	1	0	
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	39	39	29 to 39	100.0	*	*	0	1	0	
8/29/2024	09:00	0	0	0	2	0	1	3	0	0	0	0	0	0	0	0	0	0	0	6	35.8	41	32 to 42	66.7	*	*	0	5	1	
8/29/2024	10:00	0	0	0	0	0	2	2	6	0	0	0	0	0	0	0	0	0	0	10	44	48.3	39 to 49	80.0	4	40.0	0	10	0	
8/29/2024	11:00	0	0	0	2	1	4	5	7	2	0	0	0	0	0	0	0	0	0	21	41.9	48.5	39 to 49	61.9	8	38.1	0	17	4	
8/29/2024	12:00	0	0	0	1	11	7	10	5	1	0	0	0	0	0	0	0	0	0	35	38.5	44.5	30 to 40	60.0	4	11.4	0	29	6	
8/29/2024	13:00	0	1	0	2	4	8	7	1	1	0	0	0	0	0	0	0	0	0	24	37.2	43	34 to 44	75.0	2	8.3	1	20	3	
8/29/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	1	0	8	21	48	56	40	9	1	0	0	0	0	0	0	0	0	184	40.5	47	37 to 47	63.6	38	20.7	1	166	17	

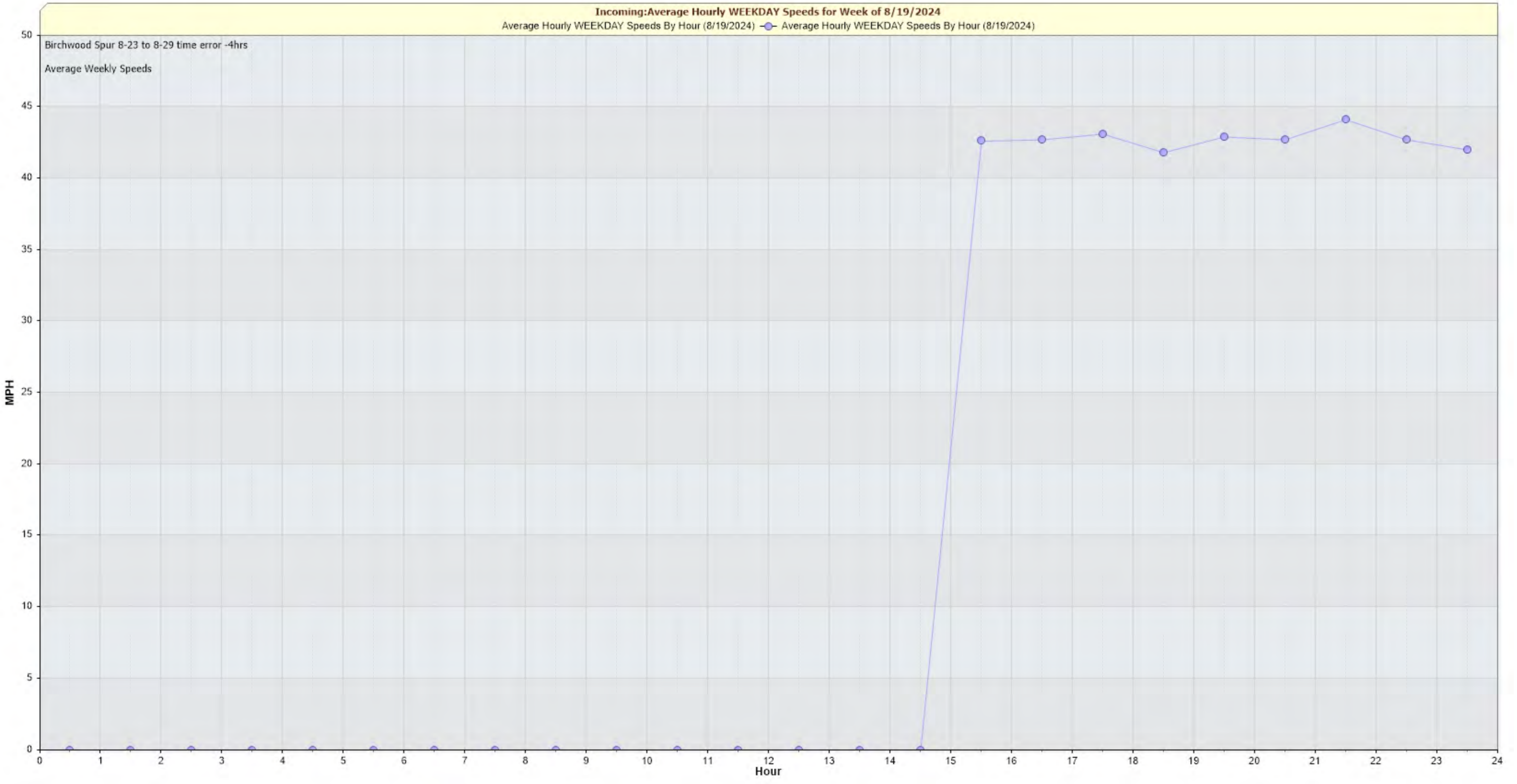
Incoming: Average Hourly Volume for Week of 8/19/2024
Average Counts By Hour (8/19/2024) — Average Counts By Hour (8/19/2024)

Birchwood Spur 8-23 to 8-29 time error -4hrs
Average Weekly Volumes



Incoming:Average Hourly WEEKDAY Speeds for Week of 8/19/2024
Average Hourly WEEKDAY Speeds By Hour (8/19/2024) — Average Hourly WEEKDAY Speeds By Hour (8/19/2024)

Birchwood Spur 8-23 to 8-29 time error -4hrs
Average Weekly Speeds



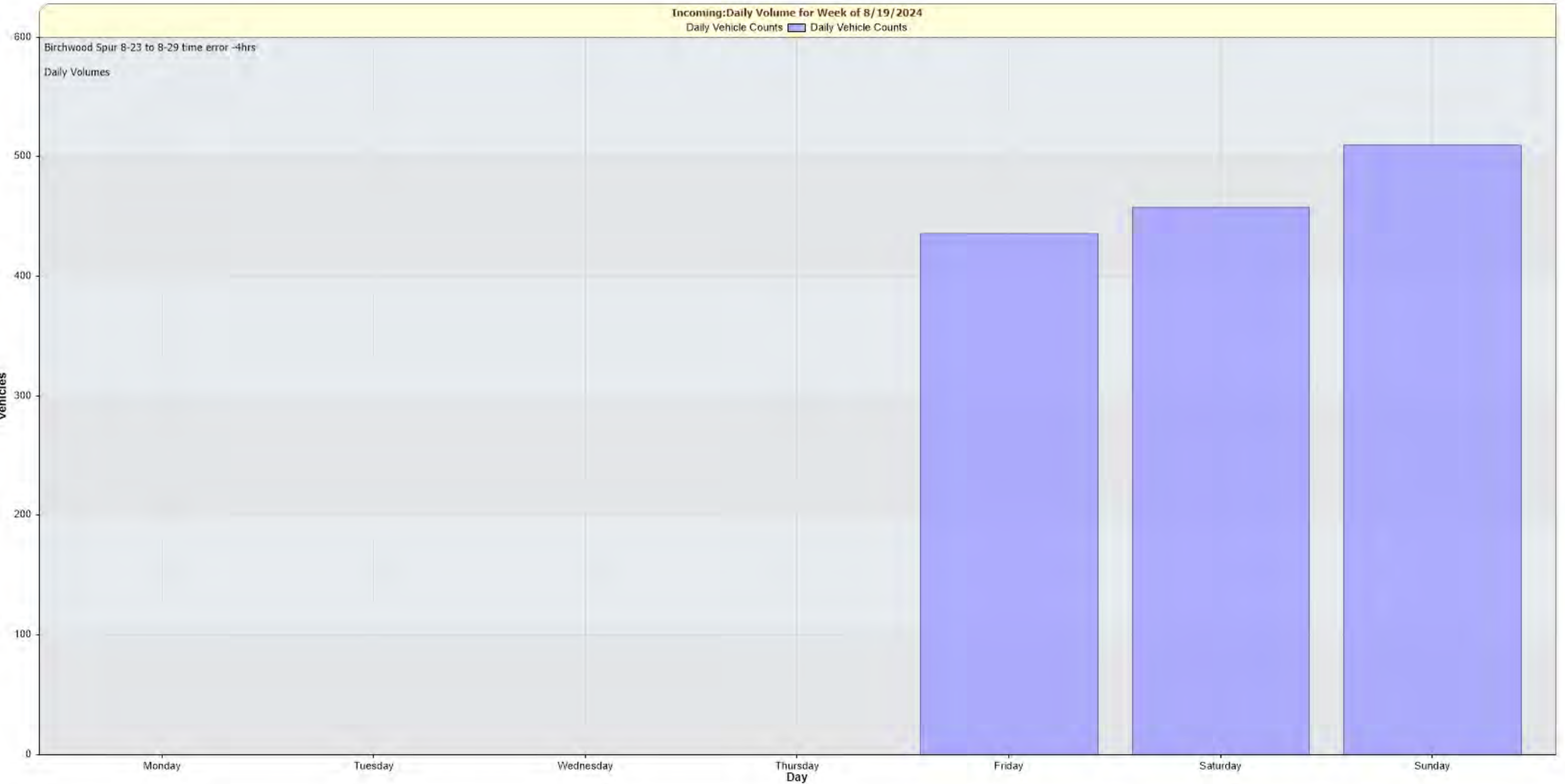
Incoming: Daily Volume for Week of 8/19/2024

Daily Vehicle Counts

Daily Vehicle Counts

Birchwood Spur 8-23 to 8-29 time error -4hrs

Daily Volumes



For Project: Birchwood Spur 8-23 to 8-29 time error -4hrs

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:28:40 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 8/23/2024 3:00:00 PM through 8/29/2024 1:59:59 PM

85th Percentile Speed 46 MPH

85th Percentile Vehicles 2909

Max Speed 71 MPH on 8/28/2024 2:39:33 AM

Total Vehicles 3422

AADT: 574

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	487	488
AM Peak 11:00 AM	39	32
PM Peak 6:00 PM	57	55

Speed

Speed Limit: 45

85th Percentile Speed: 46

50th Percentile Speed: 41

10 MPH Pace Interval: 36.0 MPH to 46.0 MPH

Average Speed: 40.55

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	80	90	111	22	72	80	93
% over limit	13.9	14.0	16.8	11.7	19.5	17.1	18.0
Avg Speeder	48.7	49.0	49.0	49.2	48.6	48.1	48.3
Avg Speed	40.4	40.5	40.5	39.3	41.1	40.5	41.0

Class Counts

	Number	%
VEH_SM	1	0
VEH_MED	3210	93.8
VEH_LG	211	6.2
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/23/2024 4:00:00 PM	46.0	9	11	48	47.0	27.3%	43.6
8/23/2024 5:00:00 PM	45.0	58	68	56	50.3	13.2%	40.5
8/23/2024 6:00:00 PM	47.0	34	40	54	49.3	25.0%	41.4
8/23/2024 7:00:00 PM	45.0	51	60	53	47.6	11.7%	39.8
8/23/2024 8:00:00 PM	47.0	42	50	52	47.8	32.0%	42.0
8/23/2024 9:00:00 PM	45.0	34	40	48	47.0	12.5%	40.7
8/23/2024 10:00:00 PM	49.0	39	46	54	49.3	32.6%	42.4
8/23/2024 11:00:00 PM	46.0	30	35	54	48.4	20.0%	42.0
8/24/2024 12:00:00 AM	44.0	16	19	45	0.0	0.0%	39.6
8/24/2024 1:00:00 AM	44.0	9	11	47	47.0	9.1%	39.3
8/24/2024 2:00:00 AM	44.0	9	11	48	47.0	18.2%	39.5
8/24/2024 3:00:00 AM	38.0	6	7	51	51.0	14.3%	36.4
8/24/2024 4:00:00 AM	46.0	3	3	46	46.0	33.3%	40.3
8/24/2024 5:00:00 AM	40.0	3	4	46	46.0	25.0%	40.5
8/24/2024 6:00:00 AM	45.0	2	2	45	0.0	0.0%	40.5
8/24/2024 7:00:00 AM	32.0	3	4	38	0.0	0.0%	32.3
8/24/2024 8:00:00 AM	35.0	2	2	35	0.0	0.0%	34.0
8/24/2024 9:00:00 AM	**No Data**						
8/24/2024 10:00:00 AM	42.0	1	1	42	0.0	0.0%	42.0
8/24/2024 11:00:00 AM	44.0	10	12	49	47.5	16.7%	40.3
8/24/2024 12:00:00 PM	45.0	14	16	49	48.5	12.5%	38.9
8/24/2024 1:00:00 PM	46.0	30	35	52	47.3	22.9%	42.0
8/24/2024 2:00:00 PM	46.0	25	29	54	49.4	17.2%	40.8
8/24/2024 3:00:00 PM	46.0	36	42	52	48.4	16.7%	41.2
8/24/2024 4:00:00 PM	45.0	37	43	54	48.7	14.0%	40.7
8/24/2024 5:00:00 PM	44.0	30	35	51	48.5	11.4%	40.2
8/24/2024 6:00:00 PM	47.0	28	33	50	47.9	27.3%	39.8
8/24/2024 7:00:00 PM	45.0	40	47	50	47.6	14.9%	41.0
8/24/2024 8:00:00 PM	44.0	19	22	46	46.0	9.1%	40.2
8/24/2024 9:00:00 PM	46.0	26	30	49	47.4	23.3%	42.1
8/24/2024 10:00:00 PM	47.0	28	33	53	49.0	18.2%	39.7
8/24/2024 11:00:00 PM	46.0	24	28	50	48.2	17.9%	40.6
8/25/2024 12:00:00 AM	46.0	14	17	51	49.0	23.5%	40.2
8/25/2024 1:00:00 AM	48.0	8	9	51	49.5	22.2%	40.0
8/25/2024 2:00:00 AM	49.0	2	2	49	48.5	100.0%	48.5
8/25/2024 3:00:00 AM	38.0	5	6	46	46.0	16.7%	38.2
8/25/2024 4:00:00 AM	40.0	3	3	40	0.0	0.0%	34.3
8/25/2024 5:00:00 AM	36.0	5	6	38	0.0	0.0%	32.3
8/25/2024 6:00:00 AM	42.0	3	3	42	0.0	0.0%	37.0
8/25/2024 7:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
8/25/2024 8:00:00 AM	41.0	2	2	41	0.0	0.0%	38.5
8/25/2024 9:00:00 AM	**No Data**						
8/25/2024 10:00:00 AM	**No Data**						
8/25/2024 11:00:00 AM	43.0	7	8	46	46.0	12.5%	39.6
8/25/2024 12:00:00 PM	45.0	16	19	51	48.5	10.5%	39.8
8/25/2024 1:00:00 PM	47.0	20	23	58	53.0	17.4%	42.7
8/25/2024 2:00:00 PM	47.0	18	21	51	48.8	19.0%	43.2

Outgoing Summary
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

8/25/2024 3:00:00 PM	45.0	28	33	53	48.3	12.1%	40.5
8/25/2024 4:00:00 PM	46.0	43	51	51	47.2	17.6%	41.0
8/25/2024 5:00:00 PM	45.0	37	44	48	46.6	11.4%	41.2
8/25/2024 6:00:00 PM	47.0	61	72	59	48.2	26.4%	41.2
8/25/2024 7:00:00 PM	45.0	50	59	51	47.0	10.2%	40.5
8/25/2024 8:00:00 PM	46.0	35	41	52	48.3	17.1%	40.8
8/25/2024 9:00:00 PM	46.0	33	39	53	48.0	23.1%	41.5
8/25/2024 10:00:00 PM	47.0	22	26	53	48.3	23.1%	42.4
8/25/2024 11:00:00 PM	48.0	26	30	53	49.0	26.7%	42.0
8/26/2024 12:00:00 AM	46.0	17	20	52	49.5	20.0%	41.5
8/26/2024 1:00:00 AM	46.0	14	16	51	49.3	18.8%	41.8
8/26/2024 2:00:00 AM	40.0	4	5	45	0.0	0.0%	39.6
8/26/2024 3:00:00 AM	42.0	4	5	44	0.0	0.0%	39.8
8/26/2024 4:00:00 AM	57.0	2	2	57	57.0	50.0%	47.0
8/26/2024 5:00:00 AM	36.0	3	4	42	0.0	0.0%	33.5
8/26/2024 6:00:00 AM	45.0	2	2	45	0.0	0.0%	43.5
8/26/2024 7:00:00 AM	**No Data**						
8/26/2024 8:00:00 AM	**No Data**						
8/26/2024 9:00:00 AM	41.0	1	1	41	0.0	0.0%	41.0
8/26/2024 10:00:00 AM	47.0	14	16	49	47.3	25.0%	42.4
8/26/2024 11:00:00 AM	44.0	14	17	46	46.0	5.9%	40.5
8/26/2024 12:00:00 PM	42.0	23	27	45	0.0	0.0%	39.5
8/26/2024 1:00:00 PM	45.0	24	28	60	52.0	13.8%	39.1
8/26/2024 2:00:00 PM	46.0	37	43	60	49.1	16.3%	41.2
8/26/2024 3:00:00 PM	46.0	33	39	48	47.3	17.9%	39.6
8/26/2024 4:00:00 PM	44.0	37	44	56	48.8	13.6%	40.5
8/26/2024 5:00:00 PM	45.0	42	50	55	50.7	12.0%	40.6
8/26/2024 6:00:00 PM	46.0	35	41	54	49.0	17.1%	40.8
8/26/2024 7:00:00 PM	44.0	60	70	55	48.9	11.4%	39.9
8/26/2024 8:00:00 PM	45.0	38	45	52	47.8	11.1%	40.0
8/26/2024 9:00:00 PM	45.0	33	39	51	47.6	12.8%	39.6
8/26/2024 10:00:00 PM	46.0	26	30	50	47.8	20.0%	40.7
8/26/2024 11:00:00 PM	48.0	29	34	55	49.1	26.5%	43.0
8/27/2024 12:00:00 AM	44.0	16	19	46	46.0	10.5%	39.3
8/27/2024 1:00:00 AM	44.0	20	24	46	46.0	12.5%	40.6
8/27/2024 2:00:00 AM	44.0	8	9	45	0.0	0.0%	38.0
8/27/2024 3:00:00 AM	38.0	3	4	40	0.0	0.0%	37.8
8/27/2024 4:00:00 AM	**No Data**						
8/27/2024 5:00:00 AM	48.0	2	2	48	48.0	50.0%	36.5
8/27/2024 6:00:00 AM	33.0	2	2	33	0.0	0.0%	30.5
8/27/2024 7:00:00 AM	38.0	2	2	38	0.0	0.0%	36.5
8/27/2024 8:00:00 AM	33.0	2	2	33	0.0	0.0%	28.5
8/27/2024 9:00:00 AM	**No Data**						
8/27/2024 10:00:00 AM	46.0	10	12	49	47.3	25.0%	42.3
8/27/2024 11:00:00 AM	45.0	24	28	53	49.0	10.7%	41.3
8/27/2024 12:00:00 PM	46.0	34	40	57	50.3	17.5%	41.1
8/27/2024 1:00:00 PM	44.0	29	34	51	48.8	11.8%	39.8
8/27/2024 2:00:00 PM	44.0	25	29	48	48.0	6.9%	38.6

Outgoing Summary
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

8/27/2024 3:00:00 PM	45.0	32	38	52	49.0	7.9%	40.2
8/27/2024 4:00:00 PM	44.0	34	40	57	52.0	10.0%	39.8
8/27/2024 5:00:00 PM	44.0	44	52	48	47.3	7.7%	39.8
8/27/2024 6:00:00 PM	45.0	32	38	54	51.4	13.2%	40.5
8/27/2024 7:00:00 PM	49.0	36	42	55	50.7	23.8%	41.4
8/27/2024 8:00:00 PM	46.0	38	45	52	47.9	20.0%	42.0
8/27/2024 9:00:00 PM	45.0	34	40	51	48.0	12.5%	39.1
8/27/2024 10:00:00 PM	45.0	48	57	55	48.7	15.8%	40.8
8/27/2024 11:00:00 PM	46.0	60	70	56	49.2	17.1%	41.4
8/28/2024 12:00:00 AM	46.0	28	33	51	47.7	18.2%	41.3
8/28/2024 1:00:00 AM	45.0	20	23	50	48.0	8.7%	40.0
8/28/2024 2:00:00 AM	45.0	5	6	50	50.0	16.7%	37.0
8/28/2024 3:00:00 AM	71.0	3	3	71	71.0	33.3%	45.3
8/28/2024 4:00:00 AM	32.0	1	1	32	0.0	0.0%	32.0
8/28/2024 5:00:00 AM	55.0	2	2	55	55.0	50.0%	40.5
8/28/2024 6:00:00 AM	33.0	1	1	33	0.0	0.0%	33.0
8/28/2024 7:00:00 AM	37.0	1	1	37	0.0	0.0%	37.0
8/28/2024 8:00:00 AM	43.0	3	3	43	0.0	0.0%	38.0
8/28/2024 9:00:00 AM	**No Data**						
8/28/2024 10:00:00 AM	46.0	8	10	65	51.5	40.0%	46.1
8/28/2024 11:00:00 AM	46.0	16	19	47	46.8	21.1%	41.9
8/28/2024 12:00:00 PM	45.0	38	45	51	48.3	13.3%	40.0
8/28/2024 1:00:00 PM	46.0	31	36	51	48.0	16.7%	38.8
8/28/2024 2:00:00 PM	46.0	26	30	49	47.2	16.7%	40.5
8/28/2024 3:00:00 PM	48.0	26	31	53	49.3	22.6%	41.9
8/28/2024 4:00:00 PM	47.0	37	44	54	48.1	22.7%	41.5
8/28/2024 5:00:00 PM	43.0	31	37	53	49.3	8.1%	38.4
8/28/2024 6:00:00 PM	45.0	54	64	53	48.7	14.1%	40.2
8/28/2024 7:00:00 PM	45.0	48	56	57	49.3	14.3%	39.1
8/28/2024 8:00:00 PM	45.0	54	64	51	48.0	10.9%	40.4
8/28/2024 9:00:00 PM	45.0	41	48	53	48.1	14.6%	38.9
8/28/2024 10:00:00 PM	46.0	46	54	53	47.6	25.9%	41.9
8/28/2024 11:00:00 PM	46.0	46	54	69	50.4	18.5%	41.9
8/29/2024 12:00:00 AM	47.0	24	28	66	50.8	21.4%	42.2
8/29/2024 1:00:00 AM	48.0	18	21	51	48.8	23.8%	42.0
8/29/2024 2:00:00 AM	44.0	7	8	45	0.0	0.0%	39.4
8/29/2024 3:00:00 AM	35.0	4	5	44	0.0	0.0%	35.8
8/29/2024 4:00:00 AM	38.0	2	2	38	0.0	0.0%	34.0
8/29/2024 5:00:00 AM	34.0	2	2	34	0.0	0.0%	33.5
8/29/2024 6:00:00 AM	43.0	2	2	43	0.0	0.0%	40.5
8/29/2024 7:00:00 AM	**No Data**						
8/29/2024 8:00:00 AM	**No Data**						
8/29/2024 10:00:00 AM						
8/29/2024 10:00:00 AM	47.0	8	9	56	51.5	22.2%	41.7
8/29/2024 11:00:00 AM	45.0	8	10	51	49.0	20.0%	41.9
8/29/2024 12:00:00 PM	44.0	40	47	50	48.0	8.5%	39.5
8/29/2024 1:00:00 PM	44.0	37	43	58	49.6	11.6%	39.0
8/29/2024 2:00:00 PM	42.0	33	39	52	49.3	10.3%	37.8

Outgoing Summary
Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/24/2024 12:00:00 AM	46.0	314	369	56	48.6	19.5%	41.1
8/25/2024 12:00:00 AM	46.0	397	467	54	48.1	17.1%	40.5
8/26/2024 12:00:00 AM	46.0	440	518	59	48.3	18.0%	41.0
8/27/2024 12:00:00 AM	45.0	490	577	60	48.7	13.9%	40.4
8/28/2024 12:00:00 AM	45.0	547	643	57	49.0	14.0%	40.5
8/29/2024 12:00:00 AM	46.0	561	660	71	49.0	16.8%	40.5
8/29/2024 1:59:59 PM	45.0	160	188	58	49.2	11.7%	39.3

Outgoing Weekly Counts
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	11	9	0	10	0
1 - 2	*	*	*	*	*	11	2	0	6.5	0
2 - 3	*	*	*	*	*	7	6	0	6.5	0
3 - 4	*	*	*	*	*	3	3	0	3	0
4 - 5	*	*	*	*	*	4	6	0	5	0
5 - 6	*	*	*	*	*	2	3	0	2.5	0
6 - 7	*	*	*	*	*	4	1	0	2.5	0
7 - 8	*	*	*	*	*	2	2	0	2	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	1	0	0	0.5	0
10 - 11	*	*	*	*	*	12	8	0	10	0
11 - 12	*	*	*	*	*	16	19	0	17.5	0
12 - 13	*	*	*	*	*	35	23	0	29	0
13 - 14	*	*	*	*	*	29	21	0	25	0
14 - 15	*	*	*	*	*	42	33	0	37.5	0
15 - 16	*	*	*	*	11	43	51	11	47	46
16 - 17	*	*	*	*	68	35	44	68	39.5	44.7
17 - 18	*	*	*	*	40	33	72	40	52.5	46.8
18 - 19	*	*	*	*	60	47	59	60	53	44.6
19 - 20	*	*	*	*	50	22	41	50	31.5	46.8
20 - 21	*	*	*	*	40	30	39	40	34.5	44.8
21 - 22	*	*	*	*	46	33	26	46	29.5	49
22 - 23	*	*	*	*	35	28	30	35	29	45.7
23 - 24	*	*	*	*	19	17	20	19	18.5	43.3
Totals	0	0	0	0	369	467	518			
% of Total	0%	0%	0%	0%	27.25%	34.49%	38.26%			

Outgoing Weekly Counts

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	16	24	23	21	*	*	*	21	0	45.58
1 - 2	5	9	6	8	*	*	*	7	0	43.25
2 - 3	5	4	3	5	*	*	*	4.25	0	46.5
3 - 4	2	0	1	2	*	*	*	1.25	0	42.33
4 - 5	4	2	2	2	*	*	*	2.5	0	43.25
5 - 6	2	2	1	2	*	*	*	1.75	0	38.5
6 - 7	0	2	1	0	*	*	*	0.75	0	37.5
7 - 8	0	2	3	0	*	*	*	1.25	0	38
8 - 9	1	0	0	0	*	*	*	0.25	0	41
9 - 10	16	12	10	9	*	*	*	11.75	0	46.38
10 - 11	17	28	19	10	*	*	*	18.5	0	44.78
11 - 12	27	40	45	47	*	*	*	39.75	0	44.08
12 - 13	28	34	36	43	*	*	*	35.25	0	44.32
13 - 14	43	29	30	39	*	*	*	35.25	0	44.08
14 - 15	39	38	31	*	*	*	*	36	0	45.9
15 - 16	44	40	44	*	*	*	*	42.67	0	44.77
16 - 17	50	52	37	*	*	*	*	46.33	0	43.7
17 - 18	41	38	64	*	*	*	*	47.67	0	44.93
18 - 19	70	42	56	*	*	*	*	56	0	45.77
19 - 20	45	45	64	*	*	*	*	51.33	0	44.93
20 - 21	39	40	48	*	*	*	*	42.33	0	44.67
21 - 22	30	57	54	*	*	*	*	47	0	45.57
22 - 23	34	70	54	*	*	*	*	52.67	0	46.47
23 - 24	19	33	28	*	*	*	*	26.67	0	45.33
Totals	577	643	660	188	0	0	0			
% of Total	27.9%	31.09%	31.91%	9.09%	0%	0%	0%			

Outgoing Monthly Counts

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	16	24	23	21	*	11	9	21	10	45.58
1 - 2	5	9	6	8	*	11	2	7	6.5	43.25
2 - 3	5	4	3	5	*	7	6	4.25	6.5	46.5
3 - 4	2	0	1	2	*	3	3	1.25	3	42.33
4 - 5	4	2	2	2	*	4	6	2.5	5	43.25
5 - 6	2	2	1	2	*	2	3	1.75	2.5	38.5
6 - 7	0	2	1	0	*	4	1	0.75	2.5	37.5
7 - 8	0	2	3	0	*	2	2	1.25	2	38
8 - 9	1	0	0	0	*	0	0	0.25	0	41
9 - 10	16	12	10	9	*	1	0	11.75	0.5	46.38
10 - 11	17	28	19	10	*	12	8	18.5	10	44.78
11 - 12	27	40	45	47	*	16	19	39.75	17.5	44.08
12 - 13	28	34	36	43	*	35	23	35.25	29	44.32
13 - 14	43	29	30	39	*	29	21	35.25	25	44.08
14 - 15	39	38	31	*	*	42	33	36	37.5	45.9
15 - 16	44	40	44	*	11	43	51	34.75	47	45.08
16 - 17	50	52	37	*	68	35	44	51.75	39.5	43.95
17 - 18	41	38	64	*	40	33	72	45.75	52.5	45.4
18 - 19	70	42	56	*	60	47	59	57	53	45.48
19 - 20	45	45	64	*	50	22	41	51	31.5	45.4
20 - 21	39	40	48	*	40	30	39	41.75	34.5	44.7
21 - 22	30	57	54	*	46	33	26	46.75	29.5	46.42
22 - 23	34	70	54	*	35	28	30	48.25	29	46.28
23 - 24	19	33	28	*	19	17	20	24.75	18.5	44.82
Totals	577	643	660	188	369	467	518			
% of Total	16.86%	18.79%	19.29%	5.49%	10.78%	13.65%	15.14%			

Outgoing Weekly Speeds

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	39.27	40	0	39.6	0
1 - 2	*	*	*	*	*	39.45	48.5	0	40.85	0
2 - 3	*	*	*	*	*	36.43	38.17	0	37.23	0
3 - 4	*	*	*	*	*	40.33	34.33	0	37.33	0
4 - 5	*	*	*	*	*	40.5	32.33	0	35.6	0
5 - 6	*	*	*	*	*	40.5	37	0	38.4	0
6 - 7	*	*	*	*	*	32.25	28	0	31.4	0
7 - 8	*	*	*	*	*	34	38.5	0	36.25	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	42	0	0	42	0
10 - 11	*	*	*	*	*	40.33	39.62	0	40.05	0
11 - 12	*	*	*	*	*	38.94	39.84	0	39.43	0
12 - 13	*	*	*	*	*	42.03	42.7	0	42.29	0
13 - 14	*	*	*	*	*	40.79	43.24	0	41.82	0
14 - 15	*	*	*	*	*	41.17	40.52	0	40.88	0
15 - 16	*	*	*	*	43.64	40.74	41.02	43.64	40.89	46
16 - 17	*	*	*	*	40.53	40.2	41.16	40.53	40.73	44.7
17 - 18	*	*	*	*	41.38	39.76	41.15	41.38	40.71	46.8
18 - 19	*	*	*	*	39.77	41.02	40.53	39.77	40.75	44.6
19 - 20	*	*	*	*	42.04	40.23	40.83	42.04	40.62	46.8
20 - 21	*	*	*	*	40.72	42.13	41.46	40.72	41.75	44.8
21 - 22	*	*	*	*	42.39	39.7	42.42	42.39	40.9	49
22 - 23	*	*	*	*	42	40.64	42.03	42	41.36	45.7
23 - 24	*	*	*	*	39.63	40.24	41.5	39.63	40.92	43.3
Totals	0	0	0	0	41.1	40.4	41			
% of Total	0%	0%	0%	0%	33.55%	32.98%	33.47%			

Outgoing Weekly Speeds

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	41.75	40.62	40	42.05	*	*	*	41.02	*	45.58
1 - 2	39.6	38	37	39.38	*	*	*	38.46	*	43.25
2 - 3	39.8	37.75	45.33	35.8	*	*	*	39.12	*	46.5
3 - 4	47	0	32	34	*	*	*	38.8	*	42.33
4 - 5	33.5	36.5	40.5	33.5	*	*	*	35.5	*	43.25
5 - 6	43.5	30.5	33	40.5	*	*	*	37.43	*	38.5
6 - 7	0	36.5	37	0	*	*	*	36.67	*	37.5
7 - 8	0	28.5	38	0	*	*	*	34.2	*	38
8 - 9	41	0	0	0	*	*	*	41	*	41
9 - 10	42.44	42.25	46.1	41.67	*	*	*	43.02	*	46.38
10 - 11	40.53	41.32	41.89	41.9	*	*	*	41.36	*	44.78
11 - 12	39.48	41.1	40.04	39.51	*	*	*	40.06	*	44.08
12 - 13	39.11	39.79	38.81	39.02	*	*	*	39.17	*	44.32
13 - 14	41.21	38.62	40.47	37.77	*	*	*	39.57	*	44.08
14 - 15	39.62	40.16	41.9	*	*	*	*	40.46	*	45.9
15 - 16	40.48	39.75	41.55	*	*	*	*	40.62	*	44.77
16 - 17	40.56	39.79	38.38	*	*	*	*	39.69	*	43.7
17 - 18	40.83	40.47	40.17	*	*	*	*	40.44	*	44.93
18 - 19	39.91	41.4	39.05	*	*	*	*	40	*	45.77
19 - 20	39.96	41.98	40.36	*	*	*	*	40.71	*	44.93
20 - 21	39.59	39.1	38.85	*	*	*	*	39.16	*	44.67
21 - 22	40.73	40.77	41.87	*	*	*	*	41.18	*	45.57
22 - 23	43.03	41.41	41.89	*	*	*	*	41.92	*	46.47
23 - 24	39.32	41.3	42.18	*	*	*	*	41.14	*	45.33
Totals	40.4	40.5	40.5	39.3	0	0	0			
% of Total	25.14%	25.2%	25.2%	24.46%	0%	0%	0%			

Outgoing Monthly Speeds

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	41.75	40.62	40	42.05	*	39.27	40	41.02	39.6	45.58
1 - 2	39.6	38	37	39.38	*	39.45	48.5	38.46	40.85	43.25
2 - 3	39.8	37.75	45.33	35.8	*	36.43	38.17	39.12	37.23	46.5
3 - 4	47	0	32	34	*	40.33	34.33	38.8	37.33	42.33
4 - 5	33.5	36.5	40.5	33.5	*	40.5	32.33	35.5	35.6	43.25
5 - 6	43.5	30.5	33	40.5	*	40.5	37	37.43	38.4	38.5
6 - 7	0	36.5	37	0	*	32.25	28	36.67	31.4	37.5
7 - 8	0	28.5	38	0	*	34	38.5	34.2	36.25	38
8 - 9	41	0	0	0	*	0	0	41	0	41
9 - 10	42.44	42.25	46.1	41.67	*	42	0	43.02	42	46.38
10 - 11	40.53	41.32	41.89	41.9	*	40.33	39.62	41.36	40.05	44.78
11 - 12	39.48	41.1	40.04	39.51	*	38.94	39.84	40.06	39.43	44.08
12 - 13	39.11	39.79	38.81	39.02	*	42.03	42.7	39.17	42.29	44.32
13 - 14	41.21	38.62	40.47	37.77	*	40.79	43.24	39.57	41.82	44.08
14 - 15	39.62	40.16	41.9	*	*	41.17	40.52	40.46	40.88	45.9
15 - 16	40.48	39.75	41.55	*	43.64	40.74	41.02	40.86	40.89	45.08
16 - 17	40.56	39.79	38.38	*	40.53	40.2	41.16	39.97	40.73	43.95
17 - 18	40.83	40.47	40.17	*	41.38	39.76	41.15	40.64	40.71	45.4
18 - 19	39.91	41.4	39.05	*	39.77	41.02	40.53	39.94	40.75	45.48
19 - 20	39.96	41.98	40.36	*	42.04	40.23	40.83	41.04	40.62	45.4
20 - 21	39.59	39.1	38.85	*	40.72	42.13	41.46	39.53	41.75	44.7
21 - 22	40.73	40.77	41.87	*	42.39	39.7	42.42	41.48	40.9	46.42
22 - 23	43.03	41.41	41.89	*	42	40.64	42.03	41.94	41.36	46.28
23 - 24	39.32	41.3	42.18	*	39.63	40.24	41.5	40.85	40.92	44.82
Totals	40.4	40.5	40.5	39.3	41.1	40.4	41			
% of Total	14.27%	14.3%	14.3%	13.88%	14.51%	14.27%	14.48%			

Outgoing Weekly EightyFifthSpeeds
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	44	48	0	46	0
1 - 2	*	*	*	*	*	44	49	0	46.5	0
2 - 3	*	*	*	*	*	38	38	0	38	0
3 - 4	*	*	*	*	*	46	40	0	43	0
4 - 5	*	*	*	*	*	40	36	0	38	0
5 - 6	*	*	*	*	*	45	42	0	43.5	0
6 - 7	*	*	*	*	*	32	28	0	30	0
7 - 8	*	*	*	*	*	35	41	0	38	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	42	0	0	42	0
10 - 11	*	*	*	*	*	44	43	0	43.5	0
11 - 12	*	*	*	*	*	45	44.5	0	44.75	0
12 - 13	*	*	*	*	*	45.8	47	0	46.4	0
13 - 14	*	*	*	*	*	46	47	0	46.5	0
14 - 15	*	*	*	*	*	46	44.7	0	45.35	0
15 - 16	*	*	*	*	46	45	45.3	46	45.15	46
16 - 17	*	*	*	*	44.7	44	44.5	44.7	44.25	44.7
17 - 18	*	*	*	*	46.8	47	46.5	46.8	46.75	46.8
18 - 19	*	*	*	*	44.6	45	44.4	44.6	44.7	44.6
19 - 20	*	*	*	*	46.8	43.5	45.5	46.8	44.5	46.8
20 - 21	*	*	*	*	44.8	46	45.8	44.8	45.9	44.8
21 - 22	*	*	*	*	49	46.5	46.5	49	46.5	49
22 - 23	*	*	*	*	45.7	46	47.5	45.7	46.75	45.7
23 - 24	*	*	*	*	43.3	46	46	43.3	46	43.3
Totals	0	0	0	0	411.7	1001.8	960.2			
% of Total	0%	0%	0%	0%	17.34%	42.2%	40.45%			

Outgoing Weekly EightyFifthSpeeds
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	46	43.5	44.8	48	*	*	*	45.58	0	45.58
1 - 2	40	44	45	44	*	*	*	43.25	0	43.25
2 - 3	42	38	71	35	*	*	*	46.5	0	46.5
3 - 4	57	0	32	38	*	*	*	42.33	0	42.33
4 - 5	36	48	55	34	*	*	*	43.25	0	43.25
5 - 6	45	33	33	43	*	*	*	38.5	0	38.5
6 - 7	0	38	37	0	*	*	*	37.5	0	37.5
7 - 8	0	33	43	0	*	*	*	38	0	38
8 - 9	41	0	0	0	*	*	*	41	0	41
9 - 10	46.5	46	46	47	*	*	*	46.38	0	46.38
10 - 11	43.3	44.8	46	45	*	*	*	44.78	0	44.78
11 - 12	42	46	44.5	43.8	*	*	*	44.08	0	44.08
12 - 13	44	44	45.3	44	*	*	*	44.32	0	44.32
13 - 14	45.5	43.5	45.3	42	*	*	*	44.08	0	44.08
14 - 15	45.5	44.2	48	*	*	*	*	45.9	0	45.9
15 - 16	44	44	46.3	*	*	*	*	44.77	0	44.77
16 - 17	44.3	43.8	43	*	*	*	*	43.7	0	43.7
17 - 18	45.5	44.5	44.8	*	*	*	*	44.93	0	44.93
18 - 19	43.8	48.5	45	*	*	*	*	45.77	0	45.77
19 - 20	44.3	46	44.5	*	*	*	*	44.93	0	44.93
20 - 21	44.5	44.5	45	*	*	*	*	44.67	0	44.67
21 - 22	45.7	45	46	*	*	*	*	45.57	0	45.57
22 - 23	48	45.7	45.7	*	*	*	*	46.47	0	46.47
23 - 24	43.7	45.3	47	*	*	*	*	45.33	0	45.33
Totals	977.6	953.3	1043.2	463.8	0	0	0			
% of Total	28.44%	27.73%	30.34%	13.49%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	46	43.5	44.8	48	*	44	48	45.58	46	45.58
1 - 2	40	44	45	44	*	44	49	43.25	46.5	43.25
2 - 3	42	38	71	35	*	38	38	46.5	38	46.5
3 - 4	57	0	32	38	*	46	40	42.33	43	42.33
4 - 5	36	48	55	34	*	40	36	43.25	38	43.25
5 - 6	45	33	33	43	*	45	42	38.5	43.5	38.5
6 - 7	0	38	37	0	*	32	28	37.5	30	37.5
7 - 8	0	33	43	0	*	35	41	38	38	38
8 - 9	41	0	0	0	*	0	0	41	0	41
9 - 10	46.5	46	46	47	*	42	0	46.38	42	46.38
10 - 11	43.3	44.8	46	45	*	44	43	44.78	43.5	44.78
11 - 12	42	46	44.5	43.8	*	45	44.5	44.08	44.75	44.08
12 - 13	44	44	45.3	44	*	45.8	47	44.32	46.4	44.32
13 - 14	45.5	43.5	45.3	42	*	46	47	44.08	46.5	44.08
14 - 15	45.5	44.2	48	*	*	46	44.7	45.9	45.35	45.9
15 - 16	44	44	46.3	*	46	45	45.3	45.08	45.15	45.08
16 - 17	44.3	43.8	43	*	44.7	44	44.5	43.95	44.25	43.95
17 - 18	45.5	44.5	44.8	*	46.8	47	46.5	45.4	46.75	45.4
18 - 19	43.8	48.5	45	*	44.6	45	44.4	45.48	44.7	45.48
19 - 20	44.3	46	44.5	*	46.8	43.5	45.5	45.4	44.5	45.4
20 - 21	44.5	44.5	45	*	44.8	46	45.8	44.7	45.9	44.7
21 - 22	45.7	45	46	*	49	46.5	46.5	46.42	46.5	46.42
22 - 23	48	45.7	45.7	*	45.7	46	47.5	46.28	46.75	46.28
23 - 24	43.7	45.3	47	*	43.3	46	46	44.82	46	44.82

Summary of Violators

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	104	40.8	16	48.3
01:00:00	41	39.2	5	48.2
02:00:00	30	38.3	3	56.0
03:00:00	11	38.0	2	51.5
04:00:00	20	35.6	3	49.7
05:00:00	12	37.8	0	0.0
06:00:00	8	33.4	0	0.0
07:00:00	9	35.1	0	0.0
08:00:00	1	41.0	0	0.0
09:00:00	48	43.0	13	49.2
10:00:00	94	41.1	13	47.6
11:00:00	194	39.9	21	49.0
12:00:00	199	40.1	30	49.0
13:00:00	191	40.2	27	48.7
14:00:00	183	40.6	28	48.4
15:00:00	233	40.9	38	48.4
16:00:00	286	40.2	31	49.1
17:00:00	288	40.7	59	48.8
18:00:00	334	40.2	46	48.7
19:00:00	267	40.9	46	47.8
20:00:00	236	40.2	38	47.7
21:00:00	246	41.3	56	48.5
22:00:00	251	41.8	51	49.2
23:00:00	136	40.9	22	49.0

Outgoing Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/23/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	15:00	0	0	0	0	0	2	5	4	0	0	0	0	0	0	0	0	0	0	0	11	43.6	46	38 to 48	100.0	3	27.3	0	11	0
8/23/2024	16:00	0	0	0	1	7	20	28	6	5	1	0	0	0	0	0	0	0	0	0	68	40.5	44.7	33 to 43	75.0	9	13.2	1	62	5
8/23/2024	17:00	0	0	0	1	1	14	12	9	3	0	0	0	0	0	0	0	0	0	40	41.4	46.8	35 to 45	70.0	10	25.0	0	34	6	
8/23/2024	18:00	0	0	0	1	12	14	21	11	1	0	0	0	0	0	0	0	0	0	60	39.8	44.6	37 to 47	73.3	7	11.7	0	55	5	
8/23/2024	19:00	0	0	0	0	3	15	13	16	3	0	0	0	0	0	0	0	0	0	50	42	46.8	37 to 47	70.0	16	32.0	0	46	4	
8/23/2024	20:00	0	0	0	2	3	8	17	10	0	0	0	0	0	0	0	0	0	0	40	40.7	44.8	36 to 46	80.0	5	12.5	0	38	2	
8/23/2024	21:00	0	0	0	1	7	6	12	13	7	0	0	0	0	0	0	0	0	0	46	42.4	49	38 to 48	60.9	15	32.6	0	42	4	
8/23/2024	22:00	0	0	0	0	4	4	14	10	3	0	0	0	0	0	0	0	0	0	35	42	45.7	37 to 47	80.0	7	20.0	0	34	1	
8/23/2024	23:00	0	0	0	0	3	6	9	1	0	0	0	0	0	0	0	0	0	0	19	39.6	43.3	34 to 44	84.2	*	*	0	18	1	
24 Hr Summary		0	0	0	6	40	89	131	80	22	1	0	0	0	0	0	0	0	0	369	41.1	46	37 to 47	72.4	72	19.5	1	340	28	

Outgoing Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG		
8/24/2024	00:00	0	0	0	1	1	3	4	2	0	0	0	0	0	0	0	0	0	0	11	39.3	44	37 to 47	81.8	1	9.1	0	11	0		
8/24/2024	01:00	0	0	0	0	3	1	5	2	0	0	0	0	0	0	0	0	0	0	11	39.5	44	32 to 42	63.6	2	18.2	0	11	0		
8/24/2024	02:00	0	0	0	1	3	2	0	0	1	0	0	0	0	0	0	0	0	0	7	36.4	38	28 to 38	71.4	1	14.3	0	7	0		
8/24/2024	03:00	0	0	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	3	40.3	46	36 to 46	66.7	1	33.3	0	3	0		
8/24/2024	04:00	0	0	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	4	40.5	40	36 to 46	100.0	1	25.0	0	4	0		
8/24/2024	05:00	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	40.5	45	35 to 45	100.0	*	*	0	2	0		
8/24/2024	06:00	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	4	32.3	32	22 to 32	75.0	*	*	0	4	0		
8/24/2024	07:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	35	25 to 35	100.0	*	*	0	2	0		
8/24/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	09:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	42	42	32 to 42	100.0	*	*	0	1	0		
8/24/2024	10:00	0	0	0	0	1	5	4	2	0	0	0	0	0	0	0	0	0	0	12	40.3	44	34 to 44	83.3	2	16.7	0	11	1		
8/24/2024	11:00	0	0	0	1	0	10	2	3	0	0	0	0	0	0	0	0	0	0	16	38.9	45	35 to 45	81.3	2	12.5	0	16	0		
8/24/2024	12:00	0	0	0	1	1	7	12	13	1	0	0	0	0	0	0	0	0	0	35	42	45.8	36 to 46	80.0	8	22.9	0	35	0		
8/24/2024	13:00	0	0	0	2	1	10	9	5	2	0	0	0	0	0	0	0	0	0	29	40.8	46	36 to 46	75.9	5	17.2	0	28	1		
8/24/2024	14:00	0	0	0	1	2	12	18	7	2	0	0	0	0	0	0	0	0	0	42	41.2	46	35 to 45	76.2	7	16.7	0	41	1		
8/24/2024	15:00	0	0	0	0	5	8	22	6	2	0	0	0	0	0	0	0	0	0	43	40.7	45	37 to 47	79.1	6	14.0	0	41	2		
8/24/2024	16:00	0	0	0	0	3	12	15	4	1	0	0	0	0	0	0	0	0	0	35	40.2	44	35 to 45	80.0	4	11.4	0	32	3		
8/24/2024	17:00	0	0	2	1	5	6	9	8	2	0	0	0	0	0	0	0	0	0	33	39.8	47	39 to 49	60.6	9	27.3	0	31	2		
8/24/2024	18:00	0	0	0	0	1	17	17	11	1	0	0	0	0	0	0	0	0	0	47	41	45	35 to 45	83.0	7	14.9	0	47	0		
8/24/2024	19:00	0	0	0	0	1	7	12	2	0	0	0	0	0	0	0	0	0	0	22	40.2	43.5	36 to 46	90.9	2	9.1	0	22	0		
8/24/2024	20:00	0	0	0	0	3	5	9	13	0	0	0	0	0	0	0	0	0	0	30	42.1	46	36 to 46	76.7	7	23.3	0	28	2		
8/24/2024	21:00	0	0	2	0	6	7	12	5	1	0	0	0	0	0	0	0	0	0	33	39.7	46.5	34 to 44	60.6	6	18.2	0	31	2		
8/24/2024	22:00	0	0	0	2	2	6	9	8	1	0	0	0	0	0	0	0	0	0	28	40.6	46	35 to 45	67.9	5	17.9	0	27	1		
8/24/2024	23:00	0	0	0	2	1	4	5	3	2	0	0	0	0	0	0	0	0	0	17	40.2	46	36 to 46	64.7	4	23.5	0	16	1		
24 Hr Summary		0	0	4	13	43	126	168	97	16	0	0	0	0	0	0	0	0	0	467	40.5	46	36 to 46	71.9	80	17.1	0	451	16		

Outgoing Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/25/2024	00:00	0	0	0	0	1	4	2	1	1	0	0	0	0	0	0	0	0	0	9	40	48	33 to 43	66.7	2	22.2	0	9	0	
8/25/2024	01:00	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	48.5	49	39 to 49	100.0	2	100.0	0	2	0	
8/25/2024	02:00	0	0	0	0	0	5	0	1	0	0	0	0	0	0	0	0	0	0	6	38.2	38	28 to 38	83.3	1	16.7	0	6	0	
8/25/2024	03:00	0	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	3	34.3	40	26 to 36	66.7	*	*	0	3	0	
8/25/2024	04:00	0	0	1	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	6	32.3	36	28 to 38	83.3	*	*	0	6	0	
8/25/2024	05:00	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	3	37	42	32 to 42	100.0	*	*	0	3	0	
8/25/2024	06:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0	
8/25/2024	07:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	38.5	41	31 to 41	100.0	*	*	0	2	0	
8/25/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	10:00	0	0	0	0	1	2	4	1	0	0	0	0	0	0	0	0	0	0	8	39.6	43	33 to 43	75.0	1	12.5	0	8	0	
8/25/2024	11:00	0	0	0	0	3	5	7	3	1	0	0	0	0	0	0	0	0	0	19	39.8	44.5	35 to 45	73.7	2	10.5	0	19	0	
8/25/2024	12:00	0	0	0	0	1	7	7	6	0	2	0	0	0	0	0	0	0	0	23	42.7	47	35 to 45	78.3	4	17.4	0	23	0	
8/25/2024	13:00	0	0	0	0	0	2	12	6	1	0	0	0	0	0	0	0	0	0	21	43.2	47	39 to 49	90.5	4	19.0	0	21	0	
8/25/2024	14:00	0	0	0	1	2	11	12	6	1	0	0	0	0	0	0	0	0	0	33	40.5	44.7	37 to 47	84.8	4	12.1	0	30	3	
8/25/2024	15:00	0	0	0	0	3	15	21	11	1	0	0	0	0	0	0	0	0	0	51	41	45.3	37 to 47	82.4	9	17.6	0	49	2	
8/25/2024	16:00	0	0	0	0	1	15	19	9	0	0	0	0	0	0	0	0	0	0	44	41.2	44.5	36 to 46	90.9	5	11.4	0	42	2	
8/25/2024	17:00	0	1	0	1	6	19	21	21	2	1	0	0	0	0	0	0	0	0	72	41.2	46.5	37 to 47	68.1	19	26.4	0	72	0	
8/25/2024	18:00	0	0	0	0	5	16	27	10	1	0	0	0	0	0	0	0	0	0	59	40.5	44.4	35 to 45	81.4	6	10.2	0	58	1	
8/25/2024	19:00	0	0	0	0	5	14	13	7	2	0	0	0	0	0	0	0	0	0	41	40.8	45.5	34 to 44	73.2	7	17.1	0	39	2	
8/25/2024	20:00	0	0	0	1	1	12	13	10	2	0	0	0	0	0	0	0	0	0	39	41.5	45.8	36 to 46	76.9	9	23.1	0	38	1	
8/25/2024	21:00	0	0	0	0	2	4	9	10	1	0	0	0	0	0	0	0	0	0	26	42.4	46.5	39 to 49	73.1	6	23.1	0	26	0	
8/25/2024	22:00	0	0	0	1	2	6	12	6	3	0	0	0	0	0	0	0	0	0	30	42	47.5	36 to 46	70.0	8	26.7	0	30	0	
8/25/2024	23:00	0	0	0	0	2	6	7	3	2	0	0	0	0	0	0	0	0	0	20	41.5	46	35 to 45	70.0	4	20.0	0	20	0	
24 Hr Summary		0	1	1	6	39	148	189	113	18	3	0	0	0	0	0	0	0	0	518	41	46	36 to 46	74.1	93	18.0	0	507	11	

Outgoing Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/26/2024	00:00	0	0	0	0	2	3	5	4	2	0	0	0	0	0	0	0	0	0	16	41.8	46	36 to 46	75.0	3	18.8	0	15	1	
8/26/2024	01:00	0	0	0	0	0	3	1	1	0	0	0	0	0	0	0	0	0	0	5	39.6	40	35 to 45	100.0	*	*	0	4	1	
8/26/2024	02:00	0	0	0	0	1	0	4	0	0	0	0	0	0	0	0	0	0	0	5	39.8	42	32 to 42	80.0	*	*	0	5	0	
8/26/2024	03:00	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2	47	57	27 to 37	50.0	1	50.0	0	2	0	
8/26/2024	04:00	0	0	1	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	4	33.5	36	32 to 42	75.0	*	*	0	4	0	
8/26/2024	05:00	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	43.5	45	35 to 45	100.0	*	*	0	2	0	
8/26/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	08:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	41	41	31 to 41	100.0	*	*	0	1	0	
8/26/2024	09:00	0	0	0	0	0	1	11	4	0	0	0	0	0	0	0	0	0	0	16	42.4	46.5	37 to 47	93.8	4	25.0	0	16	0	
8/26/2024	10:00	0	0	0	0	1	3	12	1	0	0	0	0	0	0	0	0	0	0	17	40.5	43.3	34 to 44	94.1	1	5.9	0	17	0	
8/26/2024	11:00	0	0	0	0	2	12	12	1	0	0	0	0	0	0	0	0	0	0	27	39.5	42	35 to 45	92.6	*	*	0	27	0	
8/26/2024	12:00	0	0	0	3	3	8	10	3	1	0	0	0	0	0	0	0	0	0	28	39.1	44	37 to 47	71.4	3	10.7	0	20	8	
8/26/2024	13:00	0	0	0	1	3	11	16	10	1	0	1	0	0	0	0	0	0	0	43	41.2	45.5	36 to 46	76.7	7	16.3	0	38	5	
8/26/2024	14:00	0	0	2	0	6	10	12	9	0	0	0	0	0	0	0	0	0	0	39	39.6	45.5	36 to 46	66.7	7	17.9	0	35	4	
8/26/2024	15:00	0	0	0	0	6	13	18	5	1	1	0	0	0	0	0	0	0	0	44	40.5	44	36 to 46	75.0	6	13.6	0	38	6	
8/26/2024	16:00	0	0	0	3	1	18	19	4	4	1	0	0	0	0	0	0	0	0	50	40.6	44.3	35 to 45	80.0	6	12.0	0	45	5	
8/26/2024	17:00	0	0	0	1	2	14	15	7	2	0	0	0	0	0	0	0	0	0	41	40.8	45.5	35 to 45	75.6	7	17.1	0	38	3	
8/26/2024	18:00	0	0	0	0	5	34	22	7	1	1	0	0	0	0	0	0	0	0	70	39.9	43.8	35 to 45	81.4	8	11.4	0	65	5	
8/26/2024	19:00	0	0	0	2	5	12	18	7	1	0	0	0	0	0	0	0	0	0	45	40	44.3	36 to 46	75.6	5	11.1	0	42	3	
8/26/2024	20:00	0	0	1	0	4	15	12	6	1	0	0	0	0	0	0	0	0	0	39	39.6	44.5	33 to 43	79.5	5	12.8	0	38	1	
8/26/2024	21:00	0	0	0	1	5	7	5	10	2	0	0	0	0	0	0	0	0	0	30	40.7	45.7	36 to 46	66.7	6	20.0	0	30	0	
8/26/2024	22:00	0	0	0	0	2	8	9	11	3	1	0	0	0	0	0	0	0	0	34	43	48	36 to 46	70.6	9	26.5	0	33	1	
8/26/2024	23:00	0	0	0	0	2	9	6	2	0	0	0	0	0	0	0	0	0	0	19	39.3	43.7	36 to 46	89.5	2	10.5	0	17	2	
24 Hr Summary		0	0	4	11	51	183	210	93	19	5	1	0	0	0	0	0	0	0	577	40.4	45	36 to 46	76.1	80	13.9	0	532	45	

Outgoing Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/27/2024	00:00	0	0	0	0	2	8	11	3	0	0	0	0	0	0	0	0	0	0	24	40.6	43.5	36 to 46	91.7	3	12.5	0	24	0	
8/27/2024	01:00	0	0	0	0	3	2	3	1	0	0	0	0	0	0	0	0	0	0	9	38	44	32 to 42	66.7	*	*	0	9	0	
8/27/2024	02:00	0	0	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4	37.8	38	30 to 40	100.0	*	*	0	4	0	
8/27/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	04:00	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	2	36.5	48	15 to 25	50.0	1	50.0	0	2	0	
8/27/2024	05:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30.5	33	23 to 33	100.0	*	*	0	2	0	
8/27/2024	06:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	36.5	38	28 to 38	100.0	*	*	0	2	0	
8/27/2024	07:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28.5	33	23 to 33	100.0	*	*	0	2	0	
8/27/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	09:00	0	0	0	0	1	2	5	4	0	0	0	0	0	0	0	0	0	0	12	42.3	46	37 to 47	83.3	3	25.0	0	12	0	
8/27/2024	10:00	0	0	0	0	2	7	12	6	1	0	0	0	0	0	0	0	0	0	28	41.3	44.8	35 to 45	82.1	3	10.7	0	26	2	
8/27/2024	11:00	0	0	1	1	4	6	15	9	2	2	0	0	0	0	0	0	0	0	40	41.1	46	37 to 47	75.0	7	17.5	0	40	0	
8/27/2024	12:00	0	0	0	1	6	6	16	3	2	0	0	0	0	0	0	0	0	0	34	39.8	44	36 to 46	70.6	4	11.8	0	30	4	
8/27/2024	13:00	0	1	0	1	5	4	15	3	0	0	0	0	0	0	0	0	0	0	29	38.6	43.5	34 to 44	72.4	2	6.9	0	27	2	
8/27/2024	14:00	0	0	0	1	2	12	16	6	1	0	0	0	0	0	0	0	0	0	38	40.2	44.2	35 to 45	84.2	3	7.9	0	32	6	
8/27/2024	15:00	0	0	0	2	4	12	16	4	0	2	0	0	0	0	0	0	0	0	40	39.8	44	33 to 43	75.0	4	10.0	0	38	2	
8/27/2024	16:00	0	0	0	0	4	21	20	7	0	0	0	0	0	0	0	0	0	0	52	39.8	43.8	35 to 45	84.6	4	7.7	0	48	4	
8/27/2024	17:00	0	0	0	0	5	13	13	4	3	0	0	0	0	0	0	0	0	0	38	40.5	44.5	33 to 43	76.3	5	13.2	0	36	2	
8/27/2024	18:00	0	0	0	1	3	15	12	6	3	2	0	0	0	0	0	0	0	0	42	41.4	48.5	34 to 44	66.7	10	23.8	0	33	9	
8/27/2024	19:00	0	0	0	0	2	9	22	11	1	0	0	0	0	0	0	0	0	0	45	42	46	37 to 47	77.8	9	20.0	0	45	0	
8/27/2024	20:00	0	0	2	1	4	9	17	6	1	0	0	0	0	0	0	0	0	0	40	39.1	44.5	35 to 45	70.0	5	12.5	0	38	2	
8/27/2024	21:00	0	0	2	1	4	11	25	11	2	1	0	0	0	0	0	0	0	0	57	40.8	45	36 to 46	73.7	9	15.8	0	55	2	
8/27/2024	22:00	0	1	0	1	3	16	33	12	3	1	0	0	0	0	0	0	0	0	70	41.4	45.7	36 to 46	80.0	12	17.1	0	69	1	
8/27/2024	23:00	0	0	0	0	1	11	13	6	2	0	0	0	0	0	0	0	0	0	33	41.3	45.3	36 to 46	84.8	6	18.2	0	32	1	
24 Hr Summary		0	2	6	12	57	169	265	103	21	8	0	0	0	0	0	0	0	0	643	40.5	45	35 to 45	74.0	90	14.0	0	606	37	

Outgoing Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/28/2024	00:00	0	0	0	0	3	8	6	5	1	0	0	0	0	0	0	0	0	0	23	40	44.8	35 to 45	78.3	2	8.7	0	21	2	
8/28/2024	01:00	0	0	1	0	2	0	1	1	1	0	0	0	0	0	0	0	0	0	6	37	45	22 to 32	50.0	1	16.7	0	6	0	
8/28/2024	02:00	0	0	0	1	0	0	1	0	0	0	0	0	1	0	0	0	0	0	3	45.3	71	15 to 25	33.3	1	33.3	0	3	0	
8/28/2024	03:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	32	32	22 to 32	100.0	*	*	0	0	1	
8/28/2024	04:00	0	0	0	1	0	0	0	0	0	1	0	0	0	0	0	0	0	0	2	40.5	55	16 to 26	50.0	1	50.0	0	2	0	
8/28/2024	05:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	33	33	23 to 33	100.0	*	*	0	0	1	
8/28/2024	06:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	37	37	27 to 37	100.0	*	*	0	1	0	
8/28/2024	07:00	0	0	0	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	3	38	43	33 to 43	66.7	*	*	0	3	0	
8/28/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	09:00	0	0	0	0	0	1	5	3	0	0	0	1	0	0	0	0	0	0	10	46.1	46	36 to 46	80.0	4	40.0	0	10	0	
8/28/2024	10:00	0	0	0	0	1	5	7	6	0	0	0	0	0	0	0	0	0	0	19	41.9	46	37 to 47	89.5	4	21.1	0	18	1	
8/28/2024	11:00	0	0	0	2	4	12	19	6	2	0	0	0	0	0	0	0	0	0	45	40	44.5	34 to 44	73.3	6	13.3	0	44	1	
8/28/2024	12:00	0	0	1	0	8	10	10	5	2	0	0	0	0	0	0	0	0	0	36	38.8	45.3	36 to 46	66.7	6	16.7	0	32	4	
8/28/2024	13:00	0	0	0	0	4	9	10	7	0	0	0	0	0	0	0	0	0	0	30	40.5	45.3	36 to 46	80.0	5	16.7	0	30	0	
8/28/2024	14:00	0	0	0	0	3	9	8	9	2	0	0	0	0	0	0	0	0	0	31	41.9	48	35 to 45	67.7	7	22.6	0	27	4	
8/28/2024	15:00	0	0	0	1	2	13	15	12	1	0	0	0	0	0	0	0	0	0	44	41.5	46.3	38 to 48	77.3	10	22.7	0	40	4	
8/28/2024	16:00	0	0	0	1	10	10	12	3	1	0	0	0	0	0	0	0	0	0	37	38.4	43	33 to 43	75.7	3	8.1	0	31	6	
8/28/2024	17:00	0	0	0	3	8	14	26	11	2	0	0	0	0	0	0	0	0	0	64	40.2	44.8	34 to 44	68.8	9	14.1	0	55	9	
8/28/2024	18:00	0	0	0	3	9	19	15	8	1	1	0	0	0	0	0	0	0	0	56	39.1	45	35 to 45	64.3	8	14.3	0	49	7	
8/28/2024	19:00	0	0	0	2	5	19	25	12	1	0	0	0	0	0	0	0	0	0	64	40.4	44.5	35 to 45	78.1	7	10.9	0	55	9	
8/28/2024	20:00	0	0	0	3	8	15	13	7	2	0	0	0	0	0	0	0	0	0	48	38.9	45	35 to 45	62.5	7	14.6	0	43	5	
8/28/2024	21:00	0	0	0	3	2	6	27	14	2	0	0	0	0	0	0	0	0	0	54	41.9	46	38 to 48	85.2	14	25.9	0	52	2	
8/28/2024	22:00	0	0	1	0	2	11	25	12	1	1	0	1	0	0	0	0	0	0	54	41.9	45.7	35 to 45	75.9	10	18.5	0	52	2	
8/28/2024	23:00	0	0	0	1	2	6	8	9	1	0	0	1	0	0	0	0	0	0	28	42.2	47	38 to 48	75.0	6	21.4	0	28	0	
24 Hr Summary		0	0	3	22	75	168	235	130	20	3	0	3	1	0	0	0	0	0	660	40.5	46	36 to 46	69.8	111	16.8	0	602	58	

Outgoing Histogram

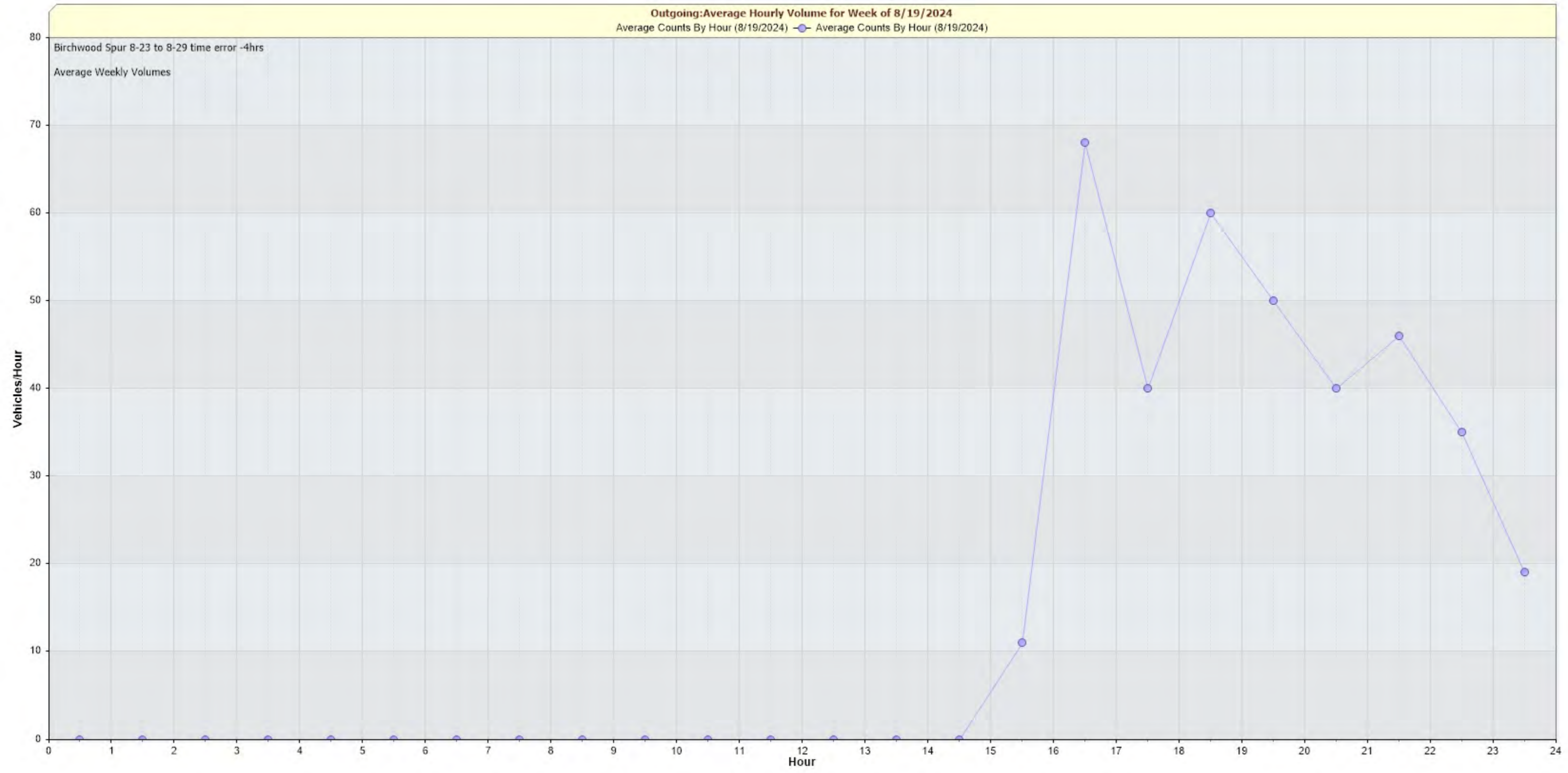
Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	0	0	0	0	1	7	5	6	2	0	0	0	0	0	0	0	0	0	21	42	48	36 to 46	76.2	5	23.8	0	20	1	
8/29/2024	01:00	0	0	0	0	2	2	3	1	0	0	0	0	0	0	0	0	0	0	8	39.4	44	34 to 44	75.0	*	*	0	8	0	
8/29/2024	02:00	0	0	0	0	2	2	1	0	0	0	0	0	0	0	0	0	0	0	5	35.8	35	25 to 35	80.0	*	*	0	4	1	
8/29/2024	03:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	38	28 to 38	100.0	*	*	0	2	0	
8/29/2024	04:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	33.5	34	24 to 34	100.0	*	*	0	1	1	
8/29/2024	05:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	40.5	43	33 to 43	100.0	*	*	0	2	0	
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	0	0	0	0	1	3	3	1	0	1	0	0	0	0	0	0	0	0	9	41.7	47	31 to 41	77.8	2	22.2	0	9	0	
8/29/2024	10:00	0	0	0	0	1	2	4	2	1	0	0	0	0	0	0	0	0	0	10	41.9	45	33 to 43	70.0	2	20.0	0	9	1	
8/29/2024	11:00	0	0	0	2	6	14	19	5	1	0	0	0	0	0	0	0	0	0	47	39.5	43.8	34 to 44	74.5	4	8.5	0	45	2	
8/29/2024	12:00	0	0	1	2	7	9	18	5	0	1	0	0	0	0	0	0	0	0	43	39	44	35 to 45	65.1	5	11.6	0	38	5	
8/29/2024	13:00	0	2	0	2	6	12	13	2	2	0	0	0	0	0	0	0	0	0	39	37.8	42	32 to 42	66.7	4	10.3	0	34	5	
8/29/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	2	1	6	29	53	67	22	6	2	0	0	0	0	0	0	0	0	188	39.3	45	35 to 45	68.1	22	11.7	0	172	16	

Outgoing: Average Hourly Volume for Week of 8/19/2024
Average Counts By Hour (8/19/2024)

Birchwood Spur 8-23 to 8-29 time error -4hrs
Average Weekly Volumes



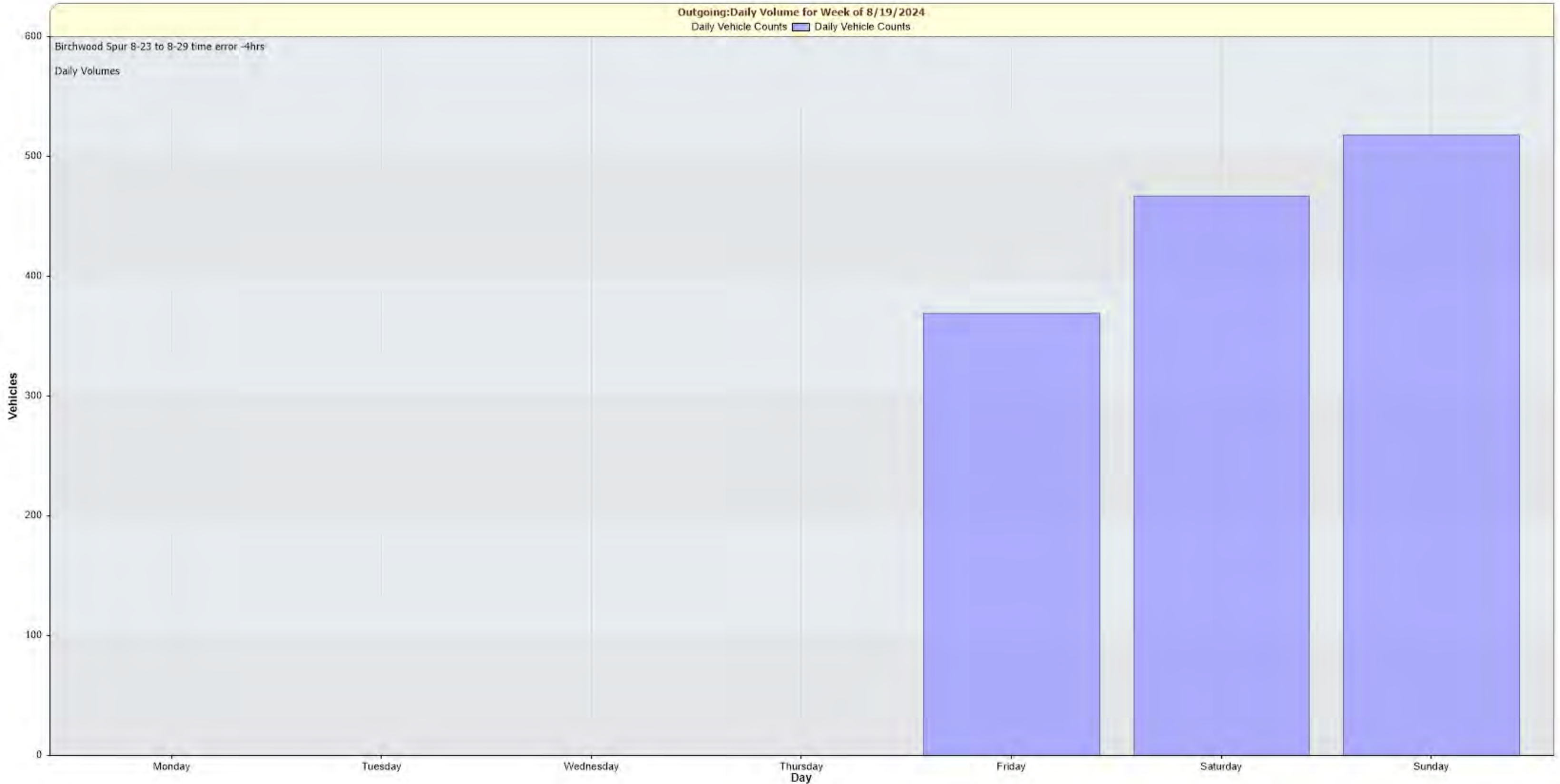
Outgoing:Average Hourly WEEKDAY Speeds for Week of 8/19/2024
Average Hourly WEEKDAY Speeds By Hour (8/19/2024) — Average Hourly WEEKDAY Speeds By Hour (8/19/2024)

Birchwood Spur 8-23 to 8-29 time error -4hrs
Average Weekly Speeds



Outgoing:Daily Volume for Week of 8/19/2024

Daily Vehicle Counts



For Project: Pilots Rd Southeast 8-29 to 9-4 time error -1hr
 Project Notes:
 Location/Name: Incoming
 Report Generated: 10/3/2024 3:30:59 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 8/29/2024 11:00:00 AM through 9/4/2024 10:59:59 AM
 85th Percentile Speed: 34 MPH
 85th Percentile Vehicles: 1642
 Max Speed: 52 MPH on 8/30/2024 7:36:08 AM
 Total Vehicles: 1932
 AADT: 322

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	264	276
AM Peak	8:00 AM 23	20
PM Peak	3:00 PM 27	26

Speed

Speed Limit: 45
 85th Percentile Speed: 34
 50th Percentile Speed: 30
 10 MPH Pace Interval: 24.0 MPH to 34.0 MPH
 Average Speed: 29.62

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	0	0	2	0	0
% over limit	0.0	0.0	0.0	0.0	0.5	0.0	0.0
Avg Speeder	0.0	0.0	0.0	0.0	49.0	0.0	0.0
Avg Speed	29.4	30.0	30.4	29.6	29.9	29.2	29.2

Class Counts

	Number	%
VEH_SM	3	0.2
VEH_MED	1824	94.4
VEH_LG	105	5.4
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/29/2024 12:00:00 PM	27.0	1	1	27	0.0	0.0%	-1.0
8/29/2024 1:00:00 PM	36.0	16	19	38	0.0	0.0%	31.9
8/29/2024 2:00:00 PM	36.0	14	16	37	0.0	0.0%	30.8
8/29/2024 3:00:00 PM	31.0	17	20	33	0.0	0.0%	28.1
8/29/2024 4:00:00 PM	33.0	19	22	35	0.0	0.0%	29.0
8/29/2024 5:00:00 PM	34.0	27	32	42	0.0	0.0%	29.0
8/29/2024 6:00:00 PM	35.0	19	22	41	0.0	0.0%	31.1
8/29/2024 7:00:00 PM	33.0	15	18	38	0.0	0.0%	30.8
8/29/2024 8:00:00 PM	32.0	16	19	40	0.0	0.0%	28.6
8/29/2024 9:00:00 PM	33.0	11	13	36	0.0	0.0%	30.0
8/29/2024 10:00:00 PM	34.0	14	16	36	0.0	0.0%	29.1
8/29/2024 11:00:00 PM	33.0	3	3	33	0.0	0.0%	27.0
8/30/2024 12:00:00 AM	31.0	5	6	35	0.0	0.0%	26.7
8/30/2024 1:00:00 AM	30.0	3	4	32	0.0	0.0%	27.8
8/30/2024 2:00:00 AM	31.0	1	1	31	0.0	0.0%	31.0
8/30/2024 3:00:00 AM	28.0	3	3	28	0.0	0.0%	26.3
8/30/2024 4:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
8/30/2024 5:00:00 AM	31.0	2	2	31	0.0	0.0%	28.0
8/30/2024 6:00:00 AM	32.0	2	2	32	0.0	0.0%	30.0
8/30/2024 7:00:00 AM	43.0	7	8	45	0.0	0.0%	33.8
8/30/2024 8:00:00 AM	32.0	12	14	52	52.0	7.1%	30.6
8/30/2024 9:00:00 AM	33.0	17	20	46	46.0	5.0%	31.1
8/30/2024 10:00:00 AM	32.0	22	26	38	0.0	0.0%	29.3
8/30/2024 11:00:00 AM	35.0	24	28	38	0.0	0.0%	30.2
8/30/2024 12:00:00 PM	36.0	17	20	39	0.0	0.0%	30.1
8/30/2024 1:00:00 PM	31.0	19	22	38	0.0	0.0%	29.0
8/30/2024 2:00:00 PM	33.0	15	18	40	0.0	0.0%	31.1
8/30/2024 3:00:00 PM	38.0	11	13	39	0.0	0.0%	33.0
8/30/2024 4:00:00 PM	31.0	19	22	40	0.0	0.0%	28.1
8/30/2024 5:00:00 PM	34.0	31	36	37	0.0	0.0%	29.3
8/30/2024 6:00:00 PM	35.0	24	28	40	0.0	0.0%	30.6
8/30/2024 7:00:00 PM	32.0	26	30	36	0.0	0.0%	29.4
8/30/2024 8:00:00 PM	33.0	21	25	38	0.0	0.0%	28.8
8/30/2024 9:00:00 PM	35.0	14	17	37	0.0	0.0%	32.2
8/30/2024 10:00:00 PM	33.0	11	13	38	0.0	0.0%	30.5
8/30/2024 11:00:00 PM	31.0	10	12	33	0.0	0.0%	26.7
8/31/2024 12:00:00 AM	26.0	3	3	26	0.0	0.0%	23.3
8/31/2024 1:00:00 AM	27.0	4	5	31	0.0	0.0%	26.4
8/31/2024 2:00:00 AM	19.0	2	2	19	0.0	0.0%	18.5
8/31/2024 3:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
8/31/2024 4:00:00 AM	40.0	1	1	40	0.0	0.0%	40.0
8/31/2024 5:00:00 AM	**No Data**						
8/31/2024 6:00:00 AM	34.0	1	1	34	0.0	0.0%	34.0
8/31/2024 7:00:00 AM	**No Data**						
8/31/2024 8:00:00 AM	28.0	3	4	29	0.0	0.0%	27.5
8/31/2024 9:00:00 AM	36.0	6	7	42	0.0	0.0%	32.1
8/31/2024 10:00:00 AM	32.0	18	21	36	0.0	0.0%	28.9

Incoming Summary
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

8/31/2024 11:00:00 AM	34.0	15	18	40	0.0	0.0%	31.3
8/31/2024 12:00:00 PM	32.0	20	23	34	0.0	0.0%	28.5
8/31/2024 1:00:00 PM	33.0	25	29	34	0.0	0.0%	29.0
8/31/2024 2:00:00 PM	33.0	19	22	38	0.0	0.0%	29.4
8/31/2024 3:00:00 PM	32.0	19	22	34	0.0	0.0%	29.3
8/31/2024 4:00:00 PM	31.0	26	31	38	0.0	0.0%	27.7
8/31/2024 5:00:00 PM	34.0	15	18	39	0.0	0.0%	31.2
8/31/2024 6:00:00 PM	34.0	16	19	36	0.0	0.0%	29.9
8/31/2024 7:00:00 PM	34.0	20	23	37	0.0	0.0%	29.6
8/31/2024 8:00:00 PM	35.0	11	13	40	0.0	0.0%	31.9
8/31/2024 9:00:00 PM	31.0	8	9	34	0.0	0.0%	29.0
8/31/2024 10:00:00 PM	33.0	8	9	33	0.0	0.0%	28.4
8/31/2024 11:00:00 PM	32.0	10	12	36	0.0	0.0%	28.0
9/1/2024 12:00:00 AM	29.0	9	11	32	0.0	0.0%	26.3
9/1/2024 1:00:00 AM	31.0	2	2	31	0.0	0.0%	27.0
9/1/2024 2:00:00 AM	35.0	3	3	35	0.0	0.0%	30.3
9/1/2024 3:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
9/1/2024 4:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
9/1/2024 5:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
9/1/2024 6:00:00 AM	**No Data**						
9/1/2024 7:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
9/1/2024 8:00:00 AM	31.0	3	4	32	0.0	0.0%	29.3
9/1/2024 9:00:00 AM	32.0	8	10	39	0.0	0.0%	29.7
9/1/2024 10:00:00 AM	33.0	9	11	37	0.0	0.0%	28.7
9/1/2024 11:00:00 AM	34.0	17	20	42	0.0	0.0%	31.5
9/1/2024 12:00:00 PM	34.0	25	29	36	0.0	0.0%	29.6
9/1/2024 1:00:00 PM	34.0	23	27	41	0.0	0.0%	28.9
9/1/2024 2:00:00 PM	33.0	20	23	36	0.0	0.0%	28.5
9/1/2024 3:00:00 PM	32.0	20	23	39	0.0	0.0%	29.3
9/1/2024 4:00:00 PM	34.0	15	18	36	0.0	0.0%	28.1
9/1/2024 5:00:00 PM	33.0	18	21	36	0.0	0.0%	28.5
9/1/2024 6:00:00 PM	34.0	24	28	36	0.0	0.0%	29.9
9/1/2024 7:00:00 PM	32.0	21	25	35	0.0	0.0%	29.0
9/1/2024 8:00:00 PM	33.0	14	17	36	0.0	0.0%	29.5
9/1/2024 9:00:00 PM	34.0	11	13	39	0.0	0.0%	31.5
9/1/2024 10:00:00 PM	32.0	16	19	36	0.0	0.0%	27.8
9/1/2024 11:00:00 PM	34.0	5	6	37	0.0	0.0%	29.5
9/2/2024 12:00:00 AM	24.0	3	4	36	0.0	0.0%	24.3
9/2/2024 1:00:00 AM	**No Data**						
9/2/2024 2:00:00 AM	36.0	2	2	36	0.0	0.0%	30.5
9/2/2024 3:00:00 AM	**No Data**						
9/2/2024 4:00:00 AM	**No Data**						
9/2/2024 5:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
9/2/2024 6:00:00 AM	**No Data**						
9/2/2024 7:00:00 AM	35.0	3	4	37	0.0	0.0%	33.5
9/2/2024 8:00:00 AM	31.0	3	4	39	0.0	0.0%	31.0
9/2/2024 9:00:00 AM	31.0	3	3	31	0.0	0.0%	30.3
9/2/2024 10:00:00 AM	34.0	7	8	35	0.0	0.0%	30.6

Incoming Summary
 Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

9/2/2024 11:00:00 AM	32.0	8	9	35	0.0	0.0%	29.8
9/2/2024 12:00:00 PM	33.0	16	19	41	0.0	0.0%	30.1
9/2/2024 1:00:00 PM	32.0	26	31	39	0.0	0.0%	28.5
9/2/2024 2:00:00 PM	35.0	20	24	37	0.0	0.0%	28.6
9/2/2024 3:00:00 PM	34.0	19	22	37	0.0	0.0%	29.1
9/2/2024 4:00:00 PM	32.0	30	35	38	0.0	0.0%	29.0
9/2/2024 5:00:00 PM	35.0	17	20	41	0.0	0.0%	31.5
9/2/2024 6:00:00 PM	33.0	12	14	35	0.0	0.0%	30.4
9/2/2024 7:00:00 PM	32.0	10	12	37	0.0	0.0%	27.7
9/2/2024 8:00:00 PM	34.0	16	19	37	0.0	0.0%	29.7
9/2/2024 9:00:00 PM	33.0	14	16	39	0.0	0.0%	30.5
9/2/2024 10:00:00 PM	31.0	14	16	36	0.0	0.0%	27.5
9/2/2024 11:00:00 PM	28.0	5	6	31	0.0	0.0%	26.8
9/3/2024 12:00:00 AM	37.0	2	2	37	0.0	0.0%	34.0
9/3/2024 1:00:00 AM	32.0	3	4	34	0.0	0.0%	30.0
9/3/2024 2:00:00 AM	**No Data**						
9/3/2024 3:00:00 AM	23.0	1	1	23	0.0	0.0%	23.0
9/3/2024 4:00:00 AM	30.0	1	1	30	0.0	0.0%	30.0
9/3/2024 5:00:00 AM	30.0	2	2	30	0.0	0.0%	29.0
9/3/2024 6:00:00 AM	37.0	2	2	37	0.0	0.0%	32.0
9/3/2024 7:00:00 AM	34.0	11	13	44	0.0	0.0%	32.2
9/3/2024 8:00:00 AM	36.0	11	13	36	0.0	0.0%	31.1
9/3/2024 9:00:00 AM	33.0	26	31	36	0.0	0.0%	29.6
9/3/2024 10:00:00 AM	35.0	29	34	38	0.0	0.0%	31.3
9/3/2024 11:00:00 AM	33.0	19	22	40	0.0	0.0%	31.0
9/3/2024 12:00:00 PM	33.0	23	27	35	0.0	0.0%	29.7
9/3/2024 1:00:00 PM	32.0	22	26	38	0.0	0.0%	29.9
9/3/2024 2:00:00 PM	33.0	13	15	41	0.0	0.0%	29.5
9/3/2024 3:00:00 PM	34.0	21	25	39	0.0	0.0%	29.3
9/3/2024 4:00:00 PM	33.0	25	29	36	0.0	0.0%	29.1
9/3/2024 5:00:00 PM	33.0	19	22	36	0.0	0.0%	28.8
9/3/2024 6:00:00 PM	34.0	19	22	38	0.0	0.0%	29.5
9/3/2024 7:00:00 PM	34.0	25	29	37	0.0	0.0%	29.6
9/3/2024 8:00:00 PM	34.0	19	22	40	0.0	0.0%	30.9
9/3/2024 9:00:00 PM	33.0	15	18	39	0.0	0.0%	29.6
9/3/2024 10:00:00 PM	33.0	7	8	34	0.0	0.0%	31.0
9/3/2024 11:00:00 PM	34.0	5	6	36	0.0	0.0%	30.2
9/4/2024 12:00:00 AM	30.0	2	2	30	0.0	0.0%	28.0
9/4/2024 1:00:00 AM	**No Data**						
9/4/2024 2:00:00 AM	43.0	2	2	43	0.0	0.0%	37.5
9/4/2024 3:00:00 AM	30.0	2	2	30	0.0	0.0%	25.0
9/4/2024 4:00:00 AM	**No Data**						
9/4/2024 5:00:00 AM	33.0	3	3	33	0.0	0.0%	30.0
9/4/2024 6:00:00 AM	31.0	2	2	31	0.0	0.0%	28.5
9/4/2024 7:00:00 AM	34.0	12	14	44	0.0	0.0%	31.3
9/4/2024 8:00:00 AM	35.0	9	11	38	0.0	0.0%	31.5
9/4/2024 9:00:00 AM	34.0	35	41	42	0.0	0.0%	29.9
9/4/2024 10:00:00 AM	35.0	20	24	39	0.0	0.0%	31.0

Incoming Summary
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

9/4/2024 11:00:00 AM	34.0	8	9	39	0.0	0.0%	28.8
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Incoming Summary
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/30/2024 12:00:00 AM	34.0	176	207	42	0.0	0.0%	29.6
8/31/2024 12:00:00 AM	34.0	313	368	52	49.0	0.5%	29.9
9/1/2024 12:00:00 AM	33.0	256	301	42	0.0	0.0%	29.2
9/2/2024 12:00:00 AM	34.0	261	307	42	0.0	0.0%	29.2
9/3/2024 12:00:00 AM	34.0	227	267	41	0.0	0.0%	29.4
9/4/2024 12:00:00 AM	33.0	318	374	44	0.0	0.0%	30.0
9/4/2024 10:59:59 AM	35.0	92	108	44	0.0	0.0%	30.4

Incoming Weekly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	4	5	2	4	3.5	30
1 - 2	*	*	*	*	1	2	3	1	2.5	31
2 - 3	*	*	*	*	3	1	1	3	1	28
3 - 4	*	*	*	*	1	1	1	1	1	28
4 - 5	*	*	*	*	2	0	1	2	0.5	31
5 - 6	*	*	*	*	2	1	0	2	0.5	32
6 - 7	*	*	*	*	8	0	1	8	0.5	43
7 - 8	*	*	*	*	14	4	4	14	4	32
8 - 9	*	*	*	*	20	7	10	20	8.5	33
9 - 10	*	*	*	*	26	21	11	26	16	32
10 - 11	*	*	*	*	28	18	20	28	19	34.7
11 - 12	*	*	*	1	20	23	29	10.5	26	31.25
12 - 13	*	*	*	19	22	29	27	20.5	28	33.15
13 - 14	*	*	*	16	18	22	23	17	22.5	34.25
14 - 15	*	*	*	20	13	22	23	16.5	22.5	34.1
15 - 16	*	*	*	22	22	31	18	22	24.5	32
16 - 17	*	*	*	32	36	18	21	34	19.5	33.75
17 - 18	*	*	*	22	28	19	28	25	23.5	34.75
18 - 19	*	*	*	18	30	23	25	24	24	32.35
19 - 20	*	*	*	19	25	13	17	22	15	32.25
20 - 21	*	*	*	13	17	9	13	15	11	33.75
21 - 22	*	*	*	16	13	9	19	14.5	14	33.5
22 - 23	*	*	*	3	12	12	6	7.5	9	32
23 - 24	*	*	*	6	3	11	4	4.5	7.5	28.5
Totals	0	0	0	207	368	301	307			
% of Total	0%	0%	0%	17.5%	31.11%	25.44%	25.95%			

Incoming Weekly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	0	4	0	*	*	*	*	1.33	0	32
1 - 2	2	0	2	*	*	*	*	1.33	0	39.5
2 - 3	0	1	2	*	*	*	*	1	0	26.5
3 - 4	0	1	0	*	*	*	*	0.33	0	30
4 - 5	1	2	3	*	*	*	*	2	0	30.67
5 - 6	0	2	2	*	*	*	*	1.33	0	34
6 - 7	4	13	14	*	*	*	*	10.33	0	34.33
7 - 8	4	13	11	*	*	*	*	9.33	0	33.77
8 - 9	3	31	41	*	*	*	*	25	0	32.53
9 - 10	8	34	24	*	*	*	*	22	0	34.5
10 - 11	9	22	9	*	*	*	*	13.33	0	33
11 - 12	19	27	*	*	*	*	*	23	0	32.65
12 - 13	31	26	*	*	*	*	*	28.5	0	32
13 - 14	24	15	*	*	*	*	*	19.5	0	33.65
14 - 15	22	25	*	*	*	*	*	23.5	0	34
15 - 16	35	29	*	*	*	*	*	32	0	32.1
16 - 17	20	22	*	*	*	*	*	21	0	33.85
17 - 18	14	22	*	*	*	*	*	18	0	33.5
18 - 19	12	29	*	*	*	*	*	20.5	0	33
19 - 20	19	22	*	*	*	*	*	20.5	0	33.75
20 - 21	16	18	*	*	*	*	*	17	0	32.75
21 - 22	16	8	*	*	*	*	*	12	0	32
22 - 23	6	6	*	*	*	*	*	6	0	31
23 - 24	2	2	*	*	*	*	*	2	0	33.5
Totals	267	374	108	0	0	0	0			
% of Total	35.65%	49.93%	14.42%	0%	0%	0%	0%			

Incoming Monthly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	4	5	*	4	5	30
1 - 2	*	*	*	*	1	2	*	1	2	31
2 - 3	*	*	*	*	3	1	*	3	1	28
3 - 4	*	*	*	*	1	1	*	1	1	28
4 - 5	*	*	*	*	2	0	*	2	0	31
5 - 6	*	*	*	*	2	1	*	2	1	32
6 - 7	*	*	*	*	8	0	*	8	0	43
7 - 8	*	*	*	*	14	4	*	14	4	32
8 - 9	*	*	*	*	20	7	*	20	7	33
9 - 10	*	*	*	*	26	21	*	26	21	32
10 - 11	*	*	*	*	28	18	*	28	18	34.7
11 - 12	*	*	*	1	20	23	*	10.5	23	31.25
12 - 13	*	*	*	19	22	29	*	20.5	29	33.15
13 - 14	*	*	*	16	18	22	*	17	22	34.25
14 - 15	*	*	*	20	13	22	*	16.5	22	34.1
15 - 16	*	*	*	22	22	31	*	22	31	32
16 - 17	*	*	*	32	36	18	*	34	18	33.75
17 - 18	*	*	*	22	28	19	*	25	19	34.75
18 - 19	*	*	*	18	30	23	*	24	23	32.35
19 - 20	*	*	*	19	25	13	*	22	13	32.25
20 - 21	*	*	*	13	17	9	*	15	9	33.75
21 - 22	*	*	*	16	13	9	*	14.5	9	33.5
22 - 23	*	*	*	3	12	12	*	7.5	12	32
23 - 24	*	*	*	6	3	11	*	4.5	11	28.5
Totals	0	0	0	207	368	301	0			
% of Total	0%	0%	0%	23.63%	42.01%	34.36%	0%			

Incoming Monthly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	0	4	0	*	*	*	2	1.33	2	32
1 - 2	2	0	2	*	*	*	3	1.33	3	39.5
2 - 3	0	1	2	*	*	*	1	1	1	26.5
3 - 4	0	1	0	*	*	*	1	0.33	1	30
4 - 5	1	2	3	*	*	*	1	2	1	30.67
5 - 6	0	2	2	*	*	*	0	1.33	0	34
6 - 7	4	13	14	*	*	*	1	10.33	1	34.33
7 - 8	4	13	11	*	*	*	4	9.33	4	33.77
8 - 9	3	31	41	*	*	*	10	25	10	32.53
9 - 10	8	34	24	*	*	*	11	22	11	34.5
10 - 11	9	22	9	*	*	*	20	13.33	20	33
11 - 12	19	27	*	*	*	*	29	23	29	32.65
12 - 13	31	26	*	*	*	*	27	28.5	27	32
13 - 14	24	15	*	*	*	*	23	19.5	23	33.65
14 - 15	22	25	*	*	*	*	23	23.5	23	34
15 - 16	35	29	*	*	*	*	18	32	18	32.1
16 - 17	20	22	*	*	*	*	21	21	21	33.85
17 - 18	14	22	*	*	*	*	28	18	28	33.5
18 - 19	12	29	*	*	*	*	25	20.5	25	33
19 - 20	19	22	*	*	*	*	17	20.5	17	33.75
20 - 21	16	18	*	*	*	*	13	17	13	32.75
21 - 22	16	8	*	*	*	*	19	12	19	32
22 - 23	6	6	*	*	*	*	6	6	6	31
23 - 24	2	2	*	*	*	*	4	2	4	33.5
Totals	267	374	108	0	0	0	307			
% of Total	25.28%	35.42%	10.23%	0%	0%	0%	29.07%			

Incoming Weekly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	27.75	26.4	27	27.75	26.57	30
1 - 2	*	*	*	*	31	18.5	30.33	31	25.6	31
2 - 3	*	*	*	*	26.33	29	28	26.33	28.5	28
3 - 4	*	*	*	*	28	40	26	28	33	28
4 - 5	*	*	*	*	28	0	28	28	28	31
5 - 6	*	*	*	*	30	34	0	30	34	32
6 - 7	*	*	*	*	33.75	0	26	33.75	26	43
7 - 8	*	*	*	*	30.57	27.5	29.25	30.57	28.38	32
8 - 9	*	*	*	*	31.15	32.14	29.7	31.15	30.71	33
9 - 10	*	*	*	*	29.35	28.9	28.73	29.35	28.84	32
10 - 11	*	*	*	*	30.21	31.28	31.45	30.21	31.37	34.7
11 - 12	*	*	*	27	30.15	28.52	29.55	30	29.1	31.25
12 - 13	*	*	*	31.95	29.05	28.97	28.89	30.39	28.93	33.15
13 - 14	*	*	*	30.75	31.06	29.36	28.48	30.91	28.91	34.25
14 - 15	*	*	*	28.1	33	29.27	29.35	30.03	29.31	34.1
15 - 16	*	*	*	29.05	28.09	27.68	28.11	28.57	27.84	32
16 - 17	*	*	*	29.03	29.33	31.17	28.48	29.19	29.72	33.75
17 - 18	*	*	*	31.09	30.64	29.89	29.89	30.84	29.89	34.75
18 - 19	*	*	*	30.83	29.43	29.57	29.04	29.96	29.29	32.35
19 - 20	*	*	*	28.58	28.84	31.92	29.53	28.73	30.57	32.25
20 - 21	*	*	*	30	32.18	29	31.54	31.23	30.5	33.75
21 - 22	*	*	*	29.06	30.46	28.44	27.84	29.69	28.04	33.5
22 - 23	*	*	*	27	26.67	28	29.5	26.73	28.5	32
23 - 24	*	*	*	26.67	23.33	26.27	24.25	25.56	25.73	28.5
Totals	0	0	0	29.6	29.9	29.2	29.2			
% of Total	0%	0%	0%	25.11%	25.36%	24.77%	24.77%			

Incoming Weekly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	0	30	0	*	*	*	*	30	*	32
1 - 2	30.5	0	37.5	*	*	*	*	34	*	39.5
2 - 3	0	23	25	*	*	*	*	24.33	*	26.5
3 - 4	0	30	0	*	*	*	*	30	*	30
4 - 5	29	29	30	*	*	*	*	29.5	*	30.67
5 - 6	0	32	28.5	*	*	*	*	30.25	*	34
6 - 7	33.5	32.23	31.29	*	*	*	*	31.97	*	34.33
7 - 8	31	31.08	31.55	*	*	*	*	31.25	*	33.77
8 - 9	30.33	29.58	29.85	*	*	*	*	29.76	*	32.53
9 - 10	30.62	31.26	31.04	*	*	*	*	31.11	*	34.5
10 - 11	29.78	31	28.78	*	*	*	*	30.23	*	33
11 - 12	30.05	29.74	*	*	*	*	*	29.87	*	32.65
12 - 13	28.48	29.92	*	*	*	*	*	29.14	*	32
13 - 14	28.62	29.47	*	*	*	*	*	28.95	*	33.65
14 - 15	29.09	29.32	*	*	*	*	*	29.21	*	34
15 - 16	29.03	29.1	*	*	*	*	*	29.06	*	32.1
16 - 17	31.45	28.82	*	*	*	*	*	30.07	*	33.85
17 - 18	30.36	29.45	*	*	*	*	*	29.81	*	33.5
18 - 19	27.67	29.55	*	*	*	*	*	29	*	33
19 - 20	29.68	30.86	*	*	*	*	*	30.32	*	33.75
20 - 21	30.5	29.56	*	*	*	*	*	30	*	32.75
21 - 22	27.5	31	*	*	*	*	*	28.67	*	32
22 - 23	26.83	30.17	*	*	*	*	*	28.5	*	31
23 - 24	34	28	*	*	*	*	*	31	*	33.5
Totals	29.4	30	30.4	0	0	0	0			
% of Total	32.74%	33.41%	33.85%	0%	0%	0%	0%			

Incoming Monthly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	27.75	26.4	*	27.75	26.4	30
1 - 2	*	*	*	*	31	18.5	*	31	18.5	31
2 - 3	*	*	*	*	26.33	29	*	26.33	29	28
3 - 4	*	*	*	*	28	40	*	28	40	28
4 - 5	*	*	*	*	28	0	*	28	*	31
5 - 6	*	*	*	*	30	34	*	30	34	32
6 - 7	*	*	*	*	33.75	0	*	33.75	*	43
7 - 8	*	*	*	*	30.57	27.5	*	30.57	27.5	32
8 - 9	*	*	*	*	31.15	32.14	*	31.15	32.14	33
9 - 10	*	*	*	*	29.35	28.9	*	29.35	28.9	32
10 - 11	*	*	*	*	30.21	31.28	*	30.21	31.28	34.7
11 - 12	*	*	*	27	30.15	28.52	*	30	28.52	31.25
12 - 13	*	*	*	31.95	29.05	28.97	*	30.39	28.97	33.15
13 - 14	*	*	*	30.75	31.06	29.36	*	30.91	29.36	34.25
14 - 15	*	*	*	28.1	33	29.27	*	30.03	29.27	34.1
15 - 16	*	*	*	29.05	28.09	27.68	*	28.57	27.68	32
16 - 17	*	*	*	29.03	29.33	31.17	*	29.19	31.17	33.75
17 - 18	*	*	*	31.09	30.64	29.89	*	30.84	29.89	34.75
18 - 19	*	*	*	30.83	29.43	29.57	*	29.96	29.57	32.35
19 - 20	*	*	*	28.58	28.84	31.92	*	28.73	31.92	32.25
20 - 21	*	*	*	30	32.18	29	*	31.23	29	33.75
21 - 22	*	*	*	29.06	30.46	28.44	*	29.69	28.44	33.5
22 - 23	*	*	*	27	26.67	28	*	26.73	28	32
23 - 24	*	*	*	26.67	23.33	26.27	*	25.56	26.27	28.5
Totals	0	0	0	29.6	29.9	29.2	0			
% of Total	0%	0%	0%	33.37%	33.71%	32.92%	0%			

Incoming Monthly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	Sep 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	0	30	0	*	*	*	27	30	27	32
1 - 2	30.5	0	37.5	*	*	*	30.33	34	30.33	39.5
2 - 3	0	23	25	*	*	*	28	24.33	28	26.5
3 - 4	0	30	0	*	*	*	26	30	26	30
4 - 5	29	29	30	*	*	*	28	29.5	28	30.67
5 - 6	0	32	28.5	*	*	*	0	30.25	0	34
6 - 7	33.5	32.23	31.29	*	*	*	26	31.97	26	34.33
7 - 8	31	31.08	31.55	*	*	*	29.25	31.25	29.25	33.77
8 - 9	30.33	29.58	29.85	*	*	*	29.7	29.76	29.7	32.53
9 - 10	30.62	31.26	31.04	*	*	*	28.73	31.11	28.73	34.5
10 - 11	29.78	31	28.78	*	*	*	31.45	30.23	31.45	33
11 - 12	30.05	29.74	*	*	*	*	29.55	29.87	29.55	32.65
12 - 13	28.48	29.92	*	*	*	*	28.89	29.14	28.89	32
13 - 14	28.62	29.47	*	*	*	*	28.48	28.95	28.48	33.65
14 - 15	29.09	29.32	*	*	*	*	29.35	29.21	29.35	34
15 - 16	29.03	29.1	*	*	*	*	28.11	29.06	28.11	32.1
16 - 17	31.45	28.82	*	*	*	*	28.48	30.07	28.48	33.85
17 - 18	30.36	29.45	*	*	*	*	29.89	29.81	29.89	33.5
18 - 19	27.67	29.55	*	*	*	*	29.04	29	29.04	33
19 - 20	29.68	30.86	*	*	*	*	29.53	30.32	29.53	33.75
20 - 21	30.5	29.56	*	*	*	*	31.54	30	31.54	32.75
21 - 22	27.5	31	*	*	*	*	27.84	28.67	27.84	32
22 - 23	26.83	30.17	*	*	*	*	29.5	28.5	29.5	31
23 - 24	34	28	*	*	*	*	24.25	31	24.25	33.5
Totals	29.4	30	30.4	0	0	0	29.2			
% of Total	24.71%	25.21%	25.55%	0%	0%	0%	24.54%			

Incoming Weekly EightyFifthSpeeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	30	27	31	30	29	30
1 - 2	*	*	*	*	31	19	35	31	27	31
2 - 3	*	*	*	*	28	29	28	28	28.5	28
3 - 4	*	*	*	*	28	40	26	28	33	28
4 - 5	*	*	*	*	31	0	28	31	28	31
5 - 6	*	*	*	*	32	34	0	32	34	32
6 - 7	*	*	*	*	43	0	26	43	26	43
7 - 8	*	*	*	*	32	28	31	32	29.5	32
8 - 9	*	*	*	*	33	36	32	33	34	33
9 - 10	*	*	*	*	32	32	33	32	32.5	32
10 - 11	*	*	*	*	34.7	34	34	34.7	34	34.7
11 - 12	*	*	*	27	35.5	31.5	33.5	31.25	32.5	31.25
12 - 13	*	*	*	35.5	30.8	32.3	33.7	33.15	33	33.15
13 - 14	*	*	*	35.5	33	33	33	34.25	33	34.25
14 - 15	*	*	*	30.7	37.5	32	31.5	34.1	31.75	34.1
15 - 16	*	*	*	33	31	30.8	33.5	32	32.15	32
16 - 17	*	*	*	34	33.5	33.7	33	33.75	33.35	33.75
17 - 18	*	*	*	35	34.5	33.3	34	34.75	33.65	34.75
18 - 19	*	*	*	32.7	32	33.7	32	32.35	32.85	32.35
19 - 20	*	*	*	32	32.5	35	32.5	32.25	33.75	32.25
20 - 21	*	*	*	32.5	35	31	34	33.75	32.5	33.75
21 - 22	*	*	*	34	33	32.7	32	33.5	32.35	33.5
22 - 23	*	*	*	33	31	31.5	34	32	32.75	32
23 - 24	*	*	*	31	26	28.8	24	28.5	26.4	28.5
Totals	0	0	0	425.9	780	698.3	724.7			
% of Total	0%	0%	0%	16.2%	29.67%	26.56%	27.57%			

Incoming Weekly EightyFifthSpeeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	0	32	0	*	*	*	*	32	0	32
1 - 2	36	0	43	*	*	*	*	39.5	0	39.5
2 - 3	0	23	30	*	*	*	*	26.5	0	26.5
3 - 4	0	30	0	*	*	*	*	30	0	30
4 - 5	29	30	33	*	*	*	*	30.67	0	30.67
5 - 6	0	37	31	*	*	*	*	34	0	34
6 - 7	35	34	34	*	*	*	*	34.33	0	34.33
7 - 8	31	35.3	35	*	*	*	*	33.77	0	33.77
8 - 9	31	32.6	34	*	*	*	*	32.53	0	32.53
9 - 10	34	35	34.5	*	*	*	*	34.5	0	34.5
10 - 11	32	33	34	*	*	*	*	33	0	33
11 - 12	33	32.3	*	*	*	*	*	32.65	0	32.65
12 - 13	32	32	*	*	*	*	*	32	0	32
13 - 14	34.3	33	*	*	*	*	*	33.65	0	33.65
14 - 15	34	34	*	*	*	*	*	34	0	34
15 - 16	32	32.2	*	*	*	*	*	32.1	0	32.1
16 - 17	34.7	33	*	*	*	*	*	33.85	0	33.85
17 - 18	33	34	*	*	*	*	*	33.5	0	33.5
18 - 19	32	34	*	*	*	*	*	33	0	33
19 - 20	33.5	34	*	*	*	*	*	33.75	0	33.75
20 - 21	33	32.5	*	*	*	*	*	32.75	0	32.75
21 - 22	31	33	*	*	*	*	*	32	0	32
22 - 23	28	34	*	*	*	*	*	31	0	31
23 - 24	37	30	*	*	*	*	*	33.5	0	33.5
Totals	655.5	749.9	308.5	0	0	0	0			
% of Total	38.25%	43.75%	18%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	30	27	*	30	27	30
1 - 2	*	*	*	*	31	19	*	31	19	31
2 - 3	*	*	*	*	28	29	*	28	29	28
3 - 4	*	*	*	*	28	40	*	28	40	28
4 - 5	*	*	*	*	31	0	*	31	0	31
5 - 6	*	*	*	*	32	34	*	32	34	32
6 - 7	*	*	*	*	43	0	*	43	0	43
7 - 8	*	*	*	*	32	28	*	32	28	32
8 - 9	*	*	*	*	33	36	*	33	36	33
9 - 10	*	*	*	*	32	32	*	32	32	32
10 - 11	*	*	*	*	34.7	34	*	34.7	34	34.7
11 - 12	*	*	*	27	35.5	31.5	*	31.25	31.5	31.25
12 - 13	*	*	*	35.5	30.8	32.3	*	33.15	32.3	33.15
13 - 14	*	*	*	35.5	33	33	*	34.25	33	34.25
14 - 15	*	*	*	30.7	37.5	32	*	34.1	32	34.1
15 - 16	*	*	*	33	31	30.8	*	32	30.8	32
16 - 17	*	*	*	34	33.5	33.7	*	33.75	33.7	33.75
17 - 18	*	*	*	35	34.5	33.3	*	34.75	33.3	34.75
18 - 19	*	*	*	32.7	32	33.7	*	32.35	33.7	32.35
19 - 20	*	*	*	32	32.5	35	*	32.25	35	32.25
20 - 21	*	*	*	32.5	35	31	*	33.75	31	33.75
21 - 22	*	*	*	34	33	32.7	*	33.5	32.7	33.5
22 - 23	*	*	*	33	31	31.5	*	32	31.5	32
23 - 24	*	*	*	31	26	28.8	*	28.5	28.8	28.5

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	0	32	0	*	*	*	31	32	31	32
1 - 2	36	0	43	*	*	*	35	39.5	35	39.5
2 - 3	0	23	30	*	*	*	28	26.5	28	26.5
3 - 4	0	30	0	*	*	*	26	30	26	30
4 - 5	29	30	33	*	*	*	28	30.67	28	30.67
5 - 6	0	37	31	*	*	*	0	34	0	34
6 - 7	35	34	34	*	*	*	26	34.33	26	34.33
7 - 8	31	35.3	35	*	*	*	31	33.77	31	33.77
8 - 9	31	32.6	34	*	*	*	32	32.53	32	32.53
9 - 10	34	35	34.5	*	*	*	33	34.5	33	34.5
10 - 11	32	33	34	*	*	*	34	33	34	33
11 - 12	33	32.3	*	*	*	*	33.5	32.65	33.5	32.65
12 - 13	32	32	*	*	*	*	33.7	32	33.7	32
13 - 14	34.3	33	*	*	*	*	33	33.65	33	33.65
14 - 15	34	34	*	*	*	*	31.5	34	31.5	34
15 - 16	32	32.2	*	*	*	*	33.5	32.1	33.5	32.1
16 - 17	34.7	33	*	*	*	*	33	33.85	33	33.85
17 - 18	33	34	*	*	*	*	34	33.5	34	33.5
18 - 19	32	34	*	*	*	*	32	33	32	33
19 - 20	33.5	34	*	*	*	*	32.5	33.75	32.5	33.75
20 - 21	33	32.5	*	*	*	*	34	32.75	34	32.75
21 - 22	31	33	*	*	*	*	32	32	32	32
22 - 23	28	34	*	*	*	*	34	31	34	31
23 - 24	37	30	*	*	*	*	24	33.5	24	33.5

Summary of Violators

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	15	27.8	0	0.0
01:00:00	10	29.5	0	0.0
02:00:00	8	26.1	0	0.0
03:00:00	4	31.0	0	0.0
04:00:00	9	29.0	0	0.0
05:00:00	7	30.7	0	0.0
06:00:00	40	32.2	0	0.0
07:00:00	50	30.6	1	52.0
08:00:00	112	30.2	1	46.0
09:00:00	124	30.2	0	0.0
10:00:00	106	30.6	0	0.0
11:00:00	119	29.6	0	0.0
12:00:00	154	29.4	0	0.0
13:00:00	118	29.5	0	0.0
14:00:00	125	29.5	0	0.0
15:00:00	157	28.5	0	0.0
16:00:00	149	29.6	0	0.0
17:00:00	133	30.2	0	0.0
18:00:00	137	29.4	0	0.0
19:00:00	115	29.8	0	0.0
20:00:00	86	30.6	0	0.0
21:00:00	81	28.8	0	0.0
22:00:00	45	27.9	0	0.0
23:00:00	28	26.4	0	0.0

Incoming Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	11:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	27	27	17 to 27	100.0	*	*	0	1	0
8/29/2024	12:00	0	0	1	4	8	6	0	0	0	0	0	0	0	0	0	0	0	0	0	19	31.9	35.5	26 to 36	84.2	*	*	3	16	0
8/29/2024	13:00	0	0	2	3	8	3	0	0	0	0	0	0	0	0	0	0	0	0	0	16	30.8	35.5	27 to 37	81.3	*	*	0	15	1
8/29/2024	14:00	0	0	1	11	8	0	0	0	0	0	0	0	0	0	0	0	0	0	0	20	28.1	30.7	23 to 33	100.0	*	*	0	19	1
8/29/2024	15:00	0	0	1	11	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	22	29	33	25 to 35	95.5	*	*	0	22	0
8/29/2024	16:00	1	0	3	17	6	3	2	0	0	0	0	0	0	0	0	0	0	0	0	32	29	34	24 to 34	75.0	*	*	0	28	4
8/29/2024	17:00	0	0	2	7	8	4	1	0	0	0	0	0	0	0	0	0	0	0	0	22	31.1	35	26 to 36	81.8	*	*	0	21	1
8/29/2024	18:00	0	0	0	5	12	1	0	0	0	0	0	0	0	0	0	0	0	0	0	18	30.8	32.7	24 to 34	94.4	*	*	0	17	1
8/29/2024	19:00	0	0	4	8	5	1	1	0	0	0	0	0	0	0	0	0	0	0	0	19	28.6	32	23 to 33	78.9	*	*	0	18	1
8/29/2024	20:00	0	0	1	4	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	13	30	32.5	23 to 33	92.3	*	*	0	13	0
8/29/2024	21:00	0	0	1	9	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	16	29.1	34	26 to 36	87.5	*	*	0	14	2
8/29/2024	22:00	0	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	27	33	23 to 33	100.0	*	*	0	3	0
8/29/2024	23:00	0	0	3	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	6	26.7	31	18 to 28	66.7	*	*	0	6	0
24 Hr Summary		1	0	21	81	77	23	4	0	0	0	0	0	0	0	0	0	0	0	0	207	29.6	34	24 to 34	81.2	*	*	3	193	11

Incoming Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/30/2024	00:00	0	1	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	4	27.8	30	22 to 32	75.0	*	*	0	4	0
8/30/2024	01:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	31	31	21 to 31	100.0	*	*	0	0	1
8/30/2024	02:00	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26.3	28	18 to 28	100.0	*	*	0	2	1
8/30/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0
8/30/2024	04:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28	31	21 to 31	100.0	*	*	0	2	0
8/30/2024	05:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30	32	22 to 32	100.0	*	*	0	2	0
8/30/2024	06:00	0	0	0	2	3	1	1	1	0	0	0	0	0	0	0	0	0	0	8	33.8	43	25 to 35	75.0	*	*	0	8	0
8/30/2024	07:00	0	0	0	8	4	1	0	0	1	0	0	0	0	0	0	0	0	0	14	30.6	32	22 to 32	85.7	1	7.1	0	12	2
8/30/2024	08:00	0	0	0	9	9	1	0	1	0	0	0	0	0	0	0	0	0	0	20	31.1	33	24 to 34	90.0	1	5.0	0	20	0
8/30/2024	09:00	0	0	1	12	11	2	0	0	0	0	0	0	0	0	0	0	0	0	26	29.3	32	25 to 35	92.3	*	*	0	25	1
8/30/2024	10:00	0	0	3	9	10	6	0	0	0	0	0	0	0	0	0	0	0	0	28	30.2	34.7	26 to 36	85.7	*	*	0	26	2
8/30/2024	11:00	0	0	0	12	4	4	0	0	0	0	0	0	0	0	0	0	0	0	20	30.1	35.5	26 to 36	85.0	*	*	0	17	3
8/30/2024	12:00	0	0	2	10	8	2	0	0	0	0	0	0	0	0	0	0	0	0	22	29	30.8	21 to 31	86.4	*	*	0	20	2
8/30/2024	13:00	0	0	2	4	9	2	1	0	0	0	0	0	0	0	0	0	0	0	18	31.1	33	23 to 33	83.3	*	*	0	17	1
8/30/2024	14:00	0	0	0	4	4	5	0	0	0	0	0	0	0	0	0	0	0	0	13	33	37.5	29 to 39	92.3	*	*	0	13	0
8/30/2024	15:00	0	0	4	12	4	1	1	0	0	0	0	0	0	0	0	0	0	0	22	28.1	31	22 to 32	90.9	*	*	0	20	2
8/30/2024	16:00	0	0	6	13	13	4	0	0	0	0	0	0	0	0	0	0	0	0	36	29.3	33.5	27 to 37	80.6	*	*	0	32	4
8/30/2024	17:00	0	0	4	7	12	4	1	0	0	0	0	0	0	0	0	0	0	0	28	30.6	34.5	24 to 34	75.0	*	*	0	24	4
8/30/2024	18:00	0	0	1	15	12	2	0	0	0	0	0	0	0	0	0	0	0	0	30	29.4	32	25 to 35	93.3	*	*	0	30	0
8/30/2024	19:00	0	0	2	15	6	2	0	0	0	0	0	0	0	0	0	0	0	0	25	28.8	32.5	24 to 34	88.0	*	*	0	25	0
8/30/2024	20:00	0	0	1	2	7	7	0	0	0	0	0	0	0	0	0	0	0	0	17	32.2	35	27 to 37	88.2	*	*	0	17	0
8/30/2024	21:00	0	0	2	1	9	1	0	0	0	0	0	0	0	0	0	0	0	0	13	30.5	33	24 to 34	84.6	*	*	0	12	1
8/30/2024	22:00	0	0	3	6	3	0	0	0	0	0	0	0	0	0	0	0	0	0	12	26.7	31	23 to 33	83.3	*	*	0	12	0
8/30/2024	23:00	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	23.3	26	16 to 26	100.0	*	*	0	3	0
24 Hr Summary		0	2	31	149	134	45	4	2	1	0	0	0	0	0	0	0	0	0	368	29.9	34	25 to 35	81.0	2	0.5	0	344	24

Incoming Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/31/2024	00:00	0	0	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	5	26.4	27	21 to 31	100.0	*	*	0	5	0	
8/31/2024	01:00	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	18.5	19	9 to 19	100.0	*	*	0	2	0	
8/31/2024	02:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0	
8/31/2024	03:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	40	40	30 to 40	100.0	*	*	0	1	0	
8/31/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/31/2024	05:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	34	34	24 to 34	100.0	*	*	0	1	0	
8/31/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/31/2024	07:00	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	27.5	28	19 to 29	100.0	*	*	0	3	1	
8/31/2024	08:00	0	0	0	2	3	1	1	0	0	0	0	0	0	0	0	0	0	0	7	32.1	36	26 to 36	85.7	*	*	0	6	1	
8/31/2024	09:00	0	0	4	8	8	1	0	0	0	0	0	0	0	0	0	0	0	0	21	28.9	32	24 to 34	95.2	*	*	0	19	2	
8/31/2024	10:00	0	0	0	5	10	2	1	0	0	0	0	0	0	0	0	0	0	0	18	31.3	34	25 to 35	88.9	*	*	0	17	1	
8/31/2024	11:00	0	0	2	15	6	0	0	0	0	0	0	0	0	0	0	0	0	0	23	28.5	31.5	22 to 32	91.3	*	*	0	22	1	
8/31/2024	12:00	0	1	3	9	16	0	0	0	0	0	0	0	0	0	0	0	0	0	29	29	32.3	24 to 34	93.1	*	*	0	29	0	
8/31/2024	13:00	0	0	3	6	11	2	0	0	0	0	0	0	0	0	0	0	0	0	22	29.4	33	23 to 33	86.4	*	*	0	21	1	
8/31/2024	14:00	0	0	2	8	12	0	0	0	0	0	0	0	0	0	0	0	0	0	22	29.3	32	24 to 34	95.5	*	*	0	20	2	
8/31/2024	15:00	0	2	5	14	8	2	0	0	0	0	0	0	0	0	0	0	0	0	31	27.7	30.8	22 to 32	83.9	*	*	0	28	3	
8/31/2024	16:00	0	0	0	5	11	2	0	0	0	0	0	0	0	0	0	0	0	0	18	31.2	33.7	24 to 34	88.9	*	*	0	17	1	
8/31/2024	17:00	0	0	1	7	10	1	0	0	0	0	0	0	0	0	0	0	0	0	19	29.9	33.3	24 to 34	94.7	*	*	0	18	1	
8/31/2024	18:00	0	0	3	7	11	2	0	0	0	0	0	0	0	0	0	0	0	0	23	29.6	33.7	24 to 34	78.3	*	*	0	22	1	
8/31/2024	19:00	0	0	0	3	7	2	1	0	0	0	0	0	0	0	0	0	0	0	13	31.9	35	25 to 35	84.6	*	*	0	13	0	
8/31/2024	20:00	0	0	1	2	6	0	0	0	0	0	0	0	0	0	0	0	0	0	9	29	31	24 to 34	100.0	*	*	0	8	1	
8/31/2024	21:00	0	1	1	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	9	28.4	32.7	23 to 33	77.8	*	*	0	9	0	
8/31/2024	22:00	0	0	4	2	5	1	0	0	0	0	0	0	0	0	0	0	0	0	12	28	31.5	22 to 32	83.3	*	*	0	12	0	
8/31/2024	23:00	0	1	2	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	11	26.3	28.8	22 to 32	90.9	*	*	0	11	0	
24 Hr Summary		0	7	33	109	132	16	4	0	0	0	0	0	0	0	0	0	0	0	301	29.2	33	24 to 34	85.4	*	*	0	285	16	

Incoming Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/1/2024	00:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27	31	21 to 31	100.0	*	*	0	2	0	
9/1/2024	01:00	0	0	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	30.3	35	25 to 35	100.0	*	*	0	3	0	
9/1/2024	02:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0	
9/1/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	1	0	
9/1/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0	
9/1/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/1/2024	06:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	0	1	
9/1/2024	07:00	0	0	1	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	4	29.3	31	22 to 32	100.0	*	*	0	4	0	
9/1/2024	08:00	0	0	1	3	5	1	0	0	0	0	0	0	0	0	0	0	0	0	10	29.7	32	24 to 34	80.0	*	*	0	10	0	
9/1/2024	09:00	0	0	3	3	4	1	0	0	0	0	0	0	0	0	0	0	0	0	11	28.7	33	21 to 31	72.7	*	*	0	10	1	
9/1/2024	10:00	0	0	3	3	11	2	1	0	0	0	0	0	0	0	0	0	0	0	20	31.5	34	24 to 34	80.0	*	*	0	20	0	
9/1/2024	11:00	0	0	3	11	13	2	0	0	0	0	0	0	0	0	0	0	0	0	29	29.6	33.5	26 to 36	89.7	*	*	0	28	1	
9/1/2024	12:00	1	1	2	7	13	2	1	0	0	0	0	0	0	0	0	0	0	0	27	28.9	33.7	25 to 35	77.8	*	*	0	25	2	
9/1/2024	13:00	0	0	4	8	8	3	0	0	0	0	0	0	0	0	0	0	0	0	23	28.5	33	25 to 35	78.3	*	*	0	22	1	
9/1/2024	14:00	0	0	1	13	7	2	0	0	0	0	0	0	0	0	0	0	0	0	23	29.3	31.5	22 to 32	91.3	*	*	0	22	1	
9/1/2024	15:00	0	2	3	5	6	2	0	0	0	0	0	0	0	0	0	0	0	0	18	28.1	33.5	23 to 33	66.7	*	*	0	18	0	
9/1/2024	16:00	0	0	5	5	9	2	0	0	0	0	0	0	0	0	0	0	0	0	21	28.5	33	24 to 34	76.2	*	*	0	20	1	
9/1/2024	17:00	0	0	2	11	11	4	0	0	0	0	0	0	0	0	0	0	0	0	28	29.9	34	25 to 35	89.3	*	*	0	27	1	
9/1/2024	18:00	0	0	5	9	10	1	0	0	0	0	0	0	0	0	0	0	0	0	25	29	32	23 to 33	84.0	*	*	0	24	1	
9/1/2024	19:00	0	0	1	8	6	2	0	0	0	0	0	0	0	0	0	0	0	0	17	29.5	32.5	23 to 33	88.2	*	*	0	17	0	
9/1/2024	20:00	0	0	0	4	7	2	0	0	0	0	0	0	0	0	0	0	0	0	13	31.5	34	25 to 35	92.3	*	*	0	13	0	
9/1/2024	21:00	0	1	5	5	7	1	0	0	0	0	0	0	0	0	0	0	0	0	19	27.8	32	22 to 32	78.9	*	*	0	19	0	
9/1/2024	22:00	0	0	1	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	6	29.5	34	24 to 34	83.3	*	*	0	6	0	
9/1/2024	23:00	0	1	2	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	4	24.3	24	14 to 24	75.0	*	*	0	4	0	
24 Hr Summary		1	5	43	103	123	30	2	0	0	0	0	0	0	0	0	0	0	0	307	29.2	34	24 to 34	78.8	*	*	0	297	10	

Incoming Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/2/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/2/2024	01:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30.5	36	15 to 25	50.0	*	*	0	2	0
9/2/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/2/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/2/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0
9/2/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/2/2024	06:00	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	33.5	35	27 to 37	100.0	*	*	0	4	0
9/2/2024	07:00	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	31	31	21 to 31	75.0	*	*	0	4	0
9/2/2024	08:00	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	30.3	31	21 to 31	100.0	*	*	0	3	0
9/2/2024	09:00	0	0	0	3	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	8	30.6	34	25 to 35	100.0	*	*	0	7	1
9/2/2024	10:00	0	0	0	5	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	9	29.8	32	25 to 35	100.0	*	*	0	9	0
9/2/2024	11:00	0	0	4	4	8	2	1	0	0	0	0	0	0	0	0	0	0	0	0	19	30.1	33	23 to 33	84.2	*	*	0	19	0
9/2/2024	12:00	0	0	4	16	10	1	0	0	0	0	0	0	0	0	0	0	0	0	0	31	28.5	32	24 to 34	87.1	*	*	0	29	2
9/2/2024	13:00	1	0	4	9	5	5	0	0	0	0	0	0	0	0	0	0	0	0	0	24	28.6	34.3	22 to 32	70.8	*	*	0	21	3
9/2/2024	14:00	0	1	2	8	8	3	0	0	0	0	0	0	0	0	0	0	0	0	0	22	29.1	34	24 to 34	77.3	*	*	0	21	1
9/2/2024	15:00	0	0	4	15	15	1	0	0	0	0	0	0	0	0	0	0	0	0	0	35	29	32	24 to 34	88.6	*	*	0	31	4
9/2/2024	16:00	0	0	0	9	6	4	1	0	0	0	0	0	0	0	0	0	0	0	0	20	31.5	34.7	25 to 35	90.0	*	*	0	20	0
9/2/2024	17:00	0	0	1	4	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	14	30.4	33	24 to 34	92.9	*	*	0	13	1
9/2/2024	18:00	1	0	3	1	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	12	27.7	32	21 to 31	66.7	*	*	0	12	0
9/2/2024	19:00	0	1	1	6	9	2	0	0	0	0	0	0	0	0	0	0	0	0	0	19	29.7	33.5	24 to 34	84.2	*	*	0	18	1
9/2/2024	20:00	0	0	0	6	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	30.5	33	24 to 34	93.8	*	*	0	15	1
9/2/2024	21:00	0	0	4	8	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	27.5	31	22 to 32	93.8	*	*	0	16	0
9/2/2024	22:00	0	0	1	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	26.8	28	18 to 28	83.3	*	*	0	6	0
9/2/2024	23:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	34	37	27 to 37	100.0	*	*	0	2	0
24 Hr Summary		2	2	29	101	102	29	2	0	0	0	0	0	0	0	0	0	0	0	0	267	29.4	34	24 to 34	82.0	*	*	0	253	14

Incoming Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/3/2024	00:00	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	30	32	24 to 34	100.0	*	*	0	4	0	
9/3/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	02:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	23	23	13 to 23	100.0	*	*	0	1	0	
9/3/2024	03:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	30	30	20 to 30	100.0	*	*	0	1	0	
9/3/2024	04:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	29	30	20 to 30	100.0	*	*	0	2	0	
9/3/2024	05:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	32	37	27 to 37	100.0	*	*	0	1	1	
9/3/2024	06:00	0	0	1	2	8	0	2	0	0	0	0	0	0	0	0	0	0	0	13	32.2	34	23 to 33	76.9	*	*	0	13	0	
9/3/2024	07:00	0	0	0	5	5	3	0	0	0	0	0	0	0	0	0	0	0	0	13	31.1	35.3	26 to 36	100.0	*	*	0	13	0	
9/3/2024	08:00	0	0	2	12	14	3	0	0	0	0	0	0	0	0	0	0	0	0	31	29.6	32.6	25 to 35	90.3	*	*	0	29	2	
9/3/2024	09:00	0	0	0	11	17	6	0	0	0	0	0	0	0	0	0	0	0	0	34	31.3	35	27 to 37	94.1	*	*	0	32	2	
9/3/2024	10:00	0	0	1	5	13	2	1	0	0	0	0	0	0	0	0	0	0	0	22	31	33	23 to 33	86.4	*	*	0	22	0	
9/3/2024	11:00	0	0	2	11	12	2	0	0	0	0	0	0	0	0	0	0	0	0	27	29.7	32.3	23 to 33	92.6	*	*	0	23	4	
9/3/2024	12:00	0	0	0	13	11	2	0	0	0	0	0	0	0	0	0	0	0	0	26	29.9	32	25 to 35	96.2	*	*	0	24	2	
9/3/2024	13:00	0	0	2	6	6	0	1	0	0	0	0	0	0	0	0	0	0	0	15	29.5	33	24 to 34	86.7	*	*	0	13	2	
9/3/2024	14:00	0	1	2	11	7	4	0	0	0	0	0	0	0	0	0	0	0	0	25	29.3	34	25 to 35	80.0	*	*	0	23	2	
9/3/2024	15:00	0	0	3	12	13	1	0	0	0	0	0	0	0	0	0	0	0	0	29	29.1	32.2	23 to 33	93.1	*	*	0	27	2	
9/3/2024	16:00	0	1	3	7	10	1	0	0	0	0	0	0	0	0	0	0	0	0	22	28.8	33	24 to 34	81.8	*	*	0	17	5	
9/3/2024	17:00	0	0	1	10	8	3	0	0	0	0	0	0	0	0	0	0	0	0	22	29.5	34	25 to 35	86.4	*	*	0	21	1	
9/3/2024	18:00	0	0	2	12	11	4	0	0	0	0	0	0	0	0	0	0	0	0	29	29.6	34	25 to 35	82.8	*	*	0	29	0	
9/3/2024	19:00	0	0	0	8	11	2	1	0	0	0	0	0	0	0	0	0	0	0	22	30.9	34	25 to 35	90.9	*	*	0	21	1	
9/3/2024	20:00	0	0	2	9	5	2	0	0	0	0	0	0	0	0	0	0	0	0	18	29.6	32.5	23 to 33	83.3	*	*	0	16	2	
9/3/2024	21:00	0	0	0	2	6	0	0	0	0	0	0	0	0	0	0	0	0	0	8	31	33	24 to 34	100.0	*	*	0	8	0	
9/3/2024	22:00	0	0	1	1	3	1	0	0	0	0	0	0	0	0	0	0	0	0	6	30.2	34	24 to 34	83.3	*	*	0	6	0	
9/3/2024	23:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28	30	20 to 30	100.0	*	*	0	2	0	
24 Hr Summary		0	2	23	142	165	37	5	0	0	0	0	0	0	0	0	0	0	0	374	30	33	25 to 35	85.0	*	*	0	348	26	

Incoming Histogram

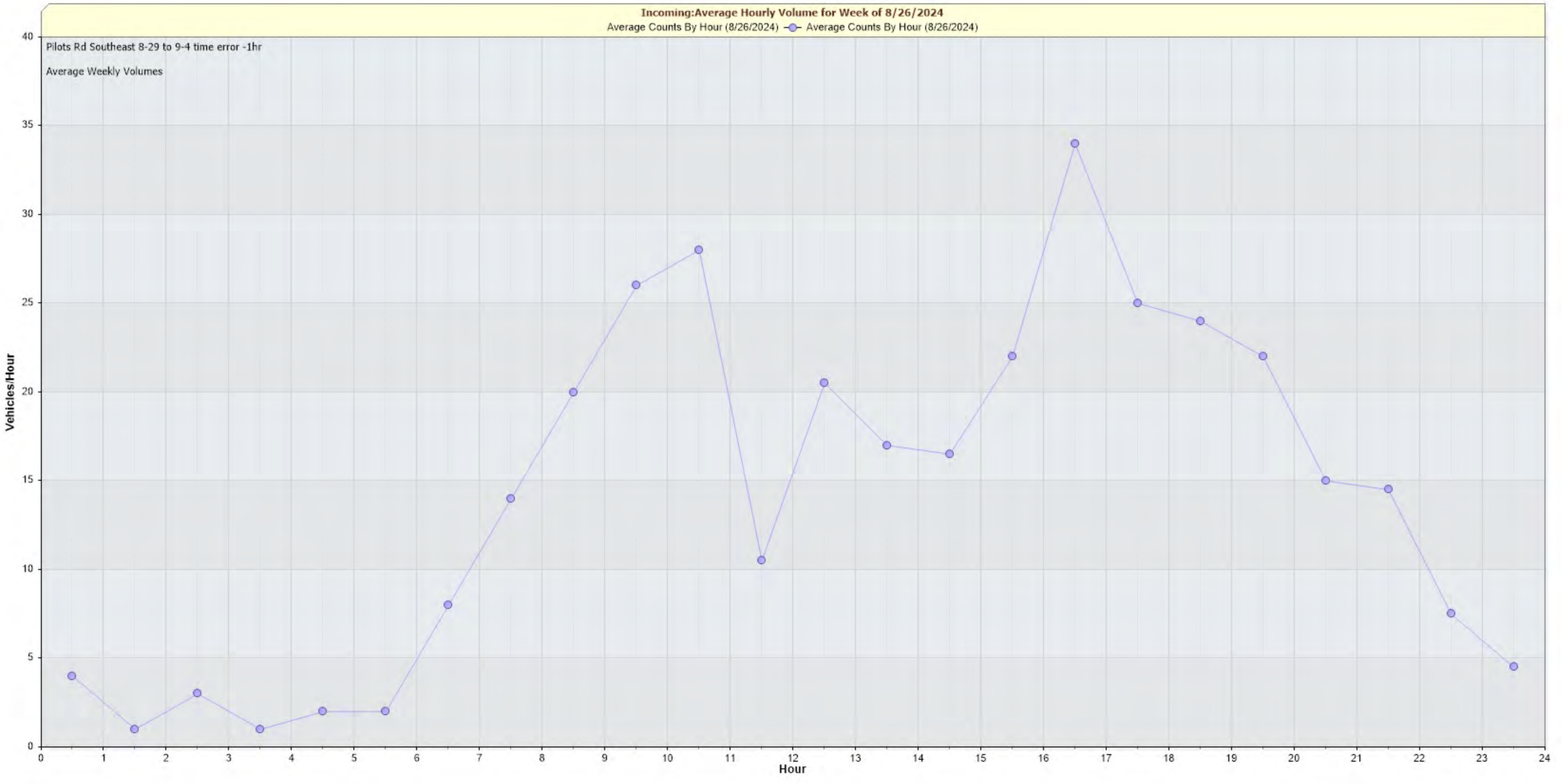
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	01:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	37.5	43	22 to 32	50.0	*	*	0	2	0
9/4/2024	02:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25	30	20 to 30	100.0	*	*	0	2	0
9/4/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	04:00	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	30	33	23 to 33	100.0	*	*	0	3	0
9/4/2024	05:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28.5	31	21 to 31	100.0	*	*	0	2	0
9/4/2024	06:00	0	0	1	5	6	1	1	0	0	0	0	0	0	0	0	0	0	0	0	14	31.3	34	27 to 37	85.7	*	*	0	14	0
9/4/2024	07:00	0	0	0	4	3	4	0	0	0	0	0	0	0	0	0	0	0	0	0	11	31.5	35	27 to 37	90.9	*	*	0	11	0
9/4/2024	08:00	0	0	2	22	11	5	1	0	0	0	0	0	0	0	0	0	0	0	0	41	29.9	34	25 to 35	87.8	*	*	0	39	2
9/4/2024	09:00	0	0	0	7	12	5	0	0	0	0	0	0	0	0	0	0	0	0	0	24	31	34.5	25 to 35	87.5	*	*	0	23	1
9/4/2024	10:00	0	0	1	6	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	9	28.8	34	24 to 34	88.9	*	*	0	8	1
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	5	47	37	16	3	0	0	0	0	0	0	0	0	0	0	0	0	108	30.4	35	25 to 35	84.3	*	*	0	104	4

Incoming: Average Hourly Volume for Week of 8/26/2024
Average Counts By Hour (8/26/2024)

Pilots Rd Southeast 8-29 to 9-4 time error -1hr
Average Weekly Volumes



Incoming:Average Hourly WEEKDAY Speeds for Week of 8/26/2024
Average Hourly WEEKDAY Speeds By Hour (8/26/2024) — Average Hourly WEEKDAY Speeds By Hour (8/26/2024)

Pilots Rd Southeast 8-29 to 9-4 time error -1hr
Average Weekly Speeds



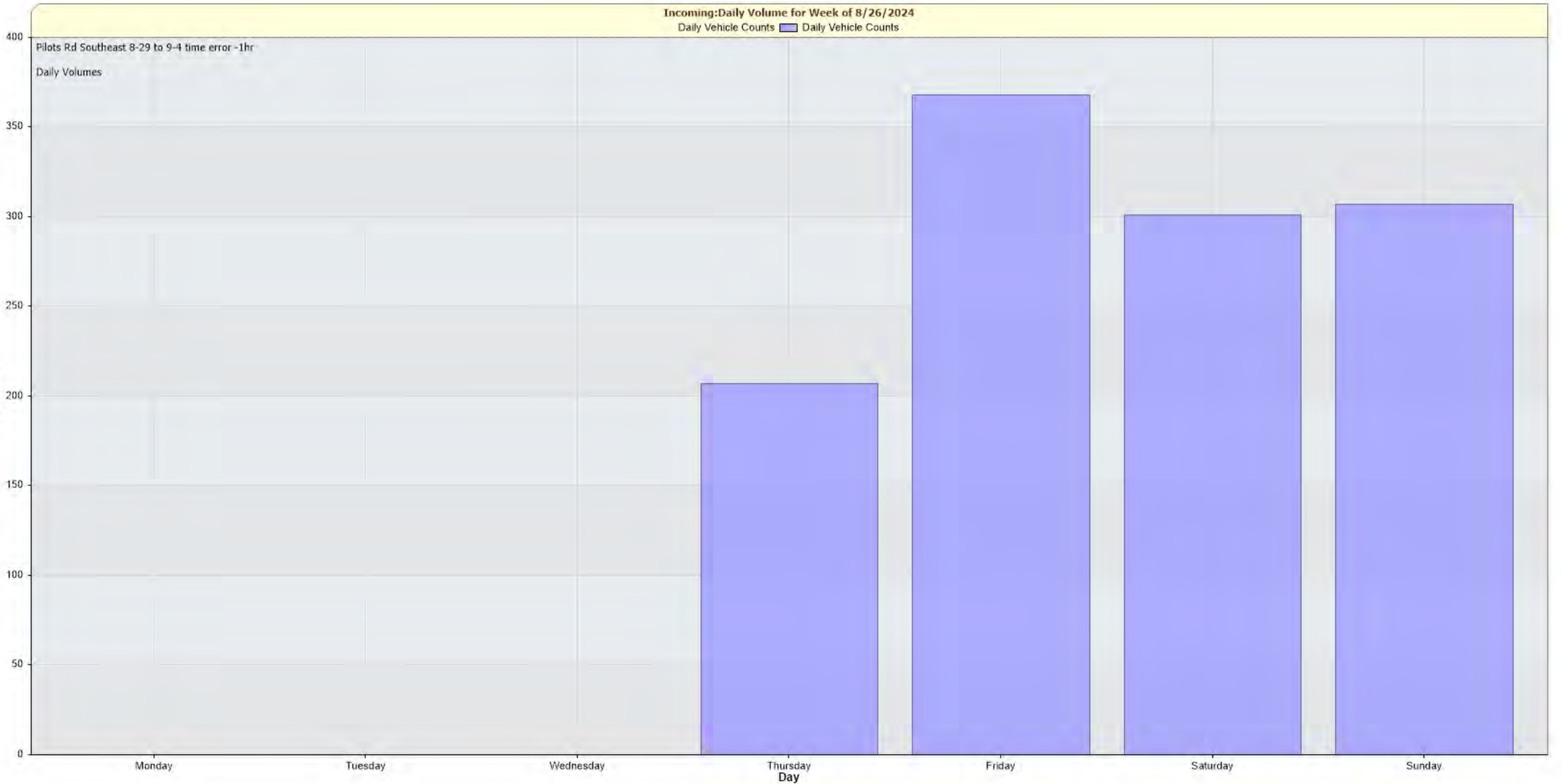
Incoming:Daily Volume for Week of 8/26/2024

Daily Vehicle Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

Daily Volumes

Vehicles



Monday

Tuesday

Wednesday

Thursday
Day

Friday

Saturday

Sunday

For Project: Pilots Rd Southeast 8-29 to 9-4 time error -1hr
 Project Notes:
 Location/Name: Outgoing
 Report Generated: 10/3/2024 3:30:59 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 8/29/2024 12:00:00 PM through 9/4/2024 10:59:59 AM
 85th Percentile Speed: 33 MPH
 85th Percentile Vehicles: 1935
 Max Speed: 51 MPH on 8/30/2024 10:04:02 PM
 Total Vehicles: 2276
 AADT: 381

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	324	325
AM Peak 11:00 AM	15	16
PM Peak 6:00 PM	41	36

Speed

Speed Limit: 45
 85th Percentile Speed: 33
 50th Percentile Speed: 29
 10 MPH Pace Interval: 23.0 MPH to 33.0 MPH
 Average Speed: 28.73

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	1	0	3	3	0	0
% over limit	0.0	0.2	0.0	0.8	0.7	0.0	0.0
Avg Speeder	0.0	50.0	0.0	47.7	47.7	0.0	0.0
Avg Speed	28.5	29.0	29.1	28.7	29.0	28.3	28.6

Class Counts

	Number	%
VEH_SM	1	0
VEH_MED	2176	95.6
VEH_LG	99	4.3
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/29/2024 1:00:00 PM	30.0	15	18	36	0.0	0.0%	27.9
8/29/2024 2:00:00 PM	31.0	20	23	33	0.0	0.0%	28.0
8/29/2024 3:00:00 PM	30.0	20	24	32	0.0	0.0%	27.1
8/29/2024 4:00:00 PM	30.0	23	27	34	0.0	0.0%	26.7
8/29/2024 5:00:00 PM	33.0	34	40	38	0.0	0.0%	28.6
8/29/2024 6:00:00 PM	33.0	37	43	35	0.0	0.0%	28.6
8/29/2024 7:00:00 PM	33.0	37	43	37	0.0	0.0%	29.2
8/29/2024 8:00:00 PM	34.0	41	48	44	0.0	0.0%	29.1
8/29/2024 9:00:00 PM	39.0	25	29	48	48.0	3.4%	32.4
8/29/2024 10:00:00 PM	33.0	26	30	47	47.0	3.3%	28.8
8/29/2024 11:00:00 PM	32.0	19	22	48	48.0	4.5%	29.0
8/30/2024 12:00:00 AM	30.0	8	10	33	0.0	0.0%	26.7
8/30/2024 1:00:00 AM	35.0	5	6	37	0.0	0.0%	29.8
8/30/2024 2:00:00 AM	26.0	2	2	26	0.0	0.0%	24.5
8/30/2024 3:00:00 AM	23.0	1	1	23	0.0	0.0%	23.0
8/30/2024 4:00:00 AM	28.0	2	2	28	0.0	0.0%	20.5
8/30/2024 5:00:00 AM	**No Data**						
8/30/2024 6:00:00 AM	30.0	2	2	30	0.0	0.0%	26.5
8/30/2024 7:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
8/30/2024 8:00:00 AM	34.0	3	4	39	0.0	0.0%	32.0
8/30/2024 9:00:00 AM	33.0	14	16	35	0.0	0.0%	28.9
8/30/2024 10:00:00 AM	30.0	8	9	35	0.0	0.0%	28.7
8/30/2024 11:00:00 AM	31.0	18	21	35	0.0	0.0%	27.9
8/30/2024 12:00:00 PM	31.0	11	13	36	0.0	0.0%	27.9
8/30/2024 1:00:00 PM	35.0	20	23	37	0.0	0.0%	28.6
8/30/2024 2:00:00 PM	30.0	20	24	35	0.0	0.0%	27.4
8/30/2024 3:00:00 PM	33.0	28	33	41	0.0	0.0%	29.5
8/30/2024 4:00:00 PM	32.0	29	34	40	0.0	0.0%	29.2
8/30/2024 5:00:00 PM	34.0	37	43	45	0.0	0.0%	29.1
8/30/2024 6:00:00 PM	33.0	42	49	38	0.0	0.0%	28.8
8/30/2024 7:00:00 PM	33.0	38	45	46	46.0	2.2%	29.9
8/30/2024 8:00:00 PM	34.0	26	30	36	0.0	0.0%	28.8
8/30/2024 9:00:00 PM	35.0	33	39	46	46.0	2.6%	31.0
8/30/2024 10:00:00 PM	32.0	23	27	35	0.0	0.0%	28.6
8/30/2024 11:00:00 PM	33.0	15	18	51	51.0	5.6%	28.6
8/31/2024 12:00:00 AM	29.0	11	13	34	0.0	0.0%	26.9
8/31/2024 1:00:00 AM	27.0	3	3	27	0.0	0.0%	24.3
8/31/2024 2:00:00 AM	28.0	2	2	28	0.0	0.0%	25.5
8/31/2024 3:00:00 AM	22.0	1	1	22	0.0	0.0%	22.0
8/31/2024 4:00:00 AM	28.0	3	4	32	0.0	0.0%	26.0
8/31/2024 5:00:00 AM	**No Data**						
8/31/2024 6:00:00 AM	**No Data**						
8/31/2024 9:00:00 AM	3	4	35	0.0	0.0%	28.3
8/31/2024 10:00:00 AM	32.0	13	15	33	0.0	0.0%	28.2
8/31/2024 11:00:00 AM	31.0	8	9	35	0.0	0.0%	28.4
8/31/2024 12:00:00 PM	30.0	14	17	33	0.0	0.0%	26.9

Outgoing Summary
 Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

8/31/2024 1:00:00 PM	31.0	13	15	33	0.0	0.0%	27.9
8/31/2024 2:00:00 PM	32.0	20	23	36	0.0	0.0%	28.7
8/31/2024 3:00:00 PM	32.0	21	25	40	0.0	0.0%	28.7
8/31/2024 4:00:00 PM	31.0	27	32	34	0.0	0.0%	27.6
8/31/2024 5:00:00 PM	33.0	18	21	38	0.0	0.0%	28.7
8/31/2024 6:00:00 PM	32.0	20	24	36	0.0	0.0%	28.9
8/31/2024 7:00:00 PM	32.0	20	23	35	0.0	0.0%	27.9
8/31/2024 8:00:00 PM	35.0	25	29	39	0.0	0.0%	29.5
8/31/2024 9:00:00 PM	33.0	25	29	36	0.0	0.0%	29.3
8/31/2024 10:00:00 PM	32.0	20	23	34	0.0	0.0%	28.8
8/31/2024 11:00:00 PM	31.0	14	17	32	0.0	0.0%	27.2
9/1/2024 12:00:00 AM	32.0	14	16	36	0.0	0.0%	27.6
9/1/2024 1:00:00 AM	29.0	3	3	29	0.0	0.0%	27.0
9/1/2024 2:00:00 AM	33.0	2	2	33	0.0	0.0%	28.5
9/1/2024 3:00:00 AM	30.0	3	3	30	0.0	0.0%	26.7
9/1/2024 4:00:00 AM	27.0	1	1	27	0.0	0.0%	27.0
9/1/2024 5:00:00 AM	28.0	3	3	28	0.0	0.0%	26.7
9/1/2024 6:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
9/1/2024 7:00:00 AM	32.0	2	2	32	0.0	0.0%	31.5
9/1/2024 8:00:00 AM	36.0	1	1	36	0.0	0.0%	36.0
9/1/2024 9:00:00 AM	27.0	3	4	28	0.0	0.0%	26.3
9/1/2024 10:00:00 AM	33.0	8	9	37	0.0	0.0%	28.3
9/1/2024 11:00:00 AM	29.0	6	7	40	0.0	0.0%	28.1
9/1/2024 12:00:00 PM	30.0	15	18	30	0.0	0.0%	26.7
9/1/2024 1:00:00 PM	34.0	17	20	38	0.0	0.0%	30.6
9/1/2024 2:00:00 PM	33.0	18	21	39	0.0	0.0%	29.7
9/1/2024 3:00:00 PM	32.0	14	17	34	0.0	0.0%	29.1
9/1/2024 4:00:00 PM	33.0	21	25	40	0.0	0.0%	29.8
9/1/2024 5:00:00 PM	30.0	16	19	35	0.0	0.0%	27.1
9/1/2024 6:00:00 PM	31.0	24	28	37	0.0	0.0%	28.3
9/1/2024 7:00:00 PM	30.0	23	27	34	0.0	0.0%	27.8
9/1/2024 8:00:00 PM	34.0	21	25	39	0.0	0.0%	29.6
9/1/2024 9:00:00 PM	32.0	18	21	38	0.0	0.0%	29.0
9/1/2024 10:00:00 PM	32.0	31	36	35	0.0	0.0%	29.0
9/1/2024 11:00:00 PM	33.0	11	13	35	0.0	0.0%	28.5
9/2/2024 12:00:00 AM	30.0	14	17	34	0.0	0.0%	27.6
9/2/2024 1:00:00 AM	32.0	5	6	40	0.0	0.0%	30.5
9/2/2024 2:00:00 AM	32.0	3	4	37	0.0	0.0%	29.0
9/2/2024 3:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
9/2/2024 4:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
9/2/2024 5:00:00 AM	31.0	1	1	31	0.0	0.0%	31.0
9/2/2024 6:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
9/2/2024 7:00:00 AM	**No Data**						
9/2/2024 8:00:00 AM	24.0	1	1	24	0.0	0.0%	24.0
9/2/2024 9:00:00 AM	29.0	2	2	29	0.0	0.0%	27.0
9/2/2024 10:00:00 AM	32.0	3	3	32	0.0	0.0%	30.0
9/2/2024 11:00:00 AM	34.0	5	6	34	0.0	0.0%	30.5
9/2/2024 12:00:00 PM	30.0	13	15	32	0.0	0.0%	26.4

Outgoing Summary
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

9/2/2024 1:00:00 PM	32.0	17	20	38	0.0	0.0%	28.1
9/2/2024 2:00:00 PM	32.0	21	25	35	0.0	0.0%	28.0
9/2/2024 3:00:00 PM	31.0	23	27	33	0.0	0.0%	26.4
9/2/2024 4:00:00 PM	32.0	27	32	35	0.0	0.0%	27.7
9/2/2024 5:00:00 PM	32.0	26	30	42	0.0	0.0%	28.4
9/2/2024 6:00:00 PM	33.0	23	27	40	0.0	0.0%	29.1
9/2/2024 7:00:00 PM	31.0	22	26	38	0.0	0.0%	28.7
9/2/2024 8:00:00 PM	32.0	25	29	41	0.0	0.0%	28.6
9/2/2024 9:00:00 PM	33.0	20	24	35	0.0	0.0%	30.3
9/2/2024 10:00:00 PM	35.0	14	16	37	0.0	0.0%	30.9
9/2/2024 11:00:00 PM	32.0	16	19	36	0.0	0.0%	28.2
9/3/2024 12:00:00 AM	36.0	10	12	42	0.0	0.0%	30.6
9/3/2024 1:00:00 AM	32.0	4	5	34	0.0	0.0%	29.0
9/3/2024 2:00:00 AM	30.0	3	4	31	0.0	0.0%	27.8
9/3/2024 3:00:00 AM	36.0	3	3	36	0.0	0.0%	27.3
9/3/2024 4:00:00 AM	27.0	1	1	27	0.0	0.0%	27.0
9/3/2024 5:00:00 AM	31.0	2	2	31	0.0	0.0%	27.5
9/3/2024 6:00:00 AM	**No Data**						
9/3/2024 7:00:00 AM	32.0	1	1	32	0.0	0.0%	32.0
9/3/2024 8:00:00 AM	33.0	3	3	33	0.0	0.0%	31.7
9/3/2024 9:00:00 AM	34.0	13	15	39	0.0	0.0%	29.9
9/3/2024 10:00:00 AM	33.0	10	12	43	0.0	0.0%	31.2
9/3/2024 11:00:00 AM	32.0	15	18	36	0.0	0.0%	29.2
9/3/2024 12:00:00 PM	31.0	16	19	33	0.0	0.0%	28.3
9/3/2024 1:00:00 PM	31.0	16	19	33	0.0	0.0%	29.4
9/3/2024 2:00:00 PM	32.0	21	25	43	0.0	0.0%	30.1
9/3/2024 3:00:00 PM	32.0	24	28	35	0.0	0.0%	28.3
9/3/2024 4:00:00 PM	32.0	20	24	38	0.0	0.0%	28.3
9/3/2024 5:00:00 PM	32.0	38	45	35	0.0	0.0%	27.7
9/3/2024 6:00:00 PM	31.0	31	37	41	0.0	0.0%	28.5
9/3/2024 7:00:00 PM	33.0	44	52	38	0.0	0.0%	29.8
9/3/2024 8:00:00 PM	32.0	35	41	36	0.0	0.0%	28.8
9/3/2024 9:00:00 PM	32.0	25	29	50	50.0	3.4%	29.3
9/3/2024 10:00:00 PM	34.0	18	21	37	0.0	0.0%	29.5
9/3/2024 11:00:00 PM	32.0	15	18	35	0.0	0.0%	30.0
9/4/2024 12:00:00 AM	30.0	9	11	32	0.0	0.0%	27.5
9/4/2024 1:00:00 AM	36.0	6	7	38	0.0	0.0%	30.3
9/4/2024 2:00:00 AM	29.0	3	4	34	0.0	0.0%	28.8
9/4/2024 3:00:00 AM	**No Data**						
9/4/2024 4:00:00 AM	32.0	2	2	32	0.0	0.0%	30.0
9/4/2024 5:00:00 AM	**No Data**						
9/4/2024 6:00:00 AM	**No Data**						
9/4/2024 7:00:00 AM	29.0	2	2	29	0.0	0.0%	24.0
9/4/2024 8:00:00 AM	35.0	3	4	35	0.0	0.0%	32.8
9/4/2024 9:00:00 AM	33.0	11	13	35	0.0	0.0%	28.5
9/4/2024 10:00:00 AM	33.0	10	12	34	0.0	0.0%	29.8
9/4/2024 11:00:00 AM	27.0	3	4	30	0.0	0.0%	26.0

Outgoing Summary
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/30/2024 12:00:00 AM	33.0	303	357	48	47.7	0.8%	28.7
8/31/2024 12:00:00 AM	33.0	387	455	51	47.7	0.7%	29.0
9/1/2024 12:00:00 AM	32.0	282	332	40	0.0	0.0%	28.3
9/2/2024 12:00:00 AM	32.0	275	323	40	0.0	0.0%	28.6
9/3/2024 12:00:00 AM	32.0	279	328	42	0.0	0.0%	28.5
9/4/2024 12:00:00 AM	33.0	368	433	50	50.0	0.2%	29.0
9/4/2024 10:59:59 AM	34.0	41	48	38	0.0	0.0%	29.1

Outgoing Weekly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	6	3	3	6	3	35
1 - 2	*	*	*	*	2	2	2	2	2	26
2 - 3	*	*	*	*	1	1	3	1	2	23
3 - 4	*	*	*	*	2	4	1	2	2.5	28
4 - 5	*	*	*	*	0	0	3	0	1.5	0
5 - 6	*	*	*	*	2	0	1	2	0.5	30
6 - 7	*	*	*	*	1	0	2	1	1	28
7 - 8	*	*	*	*	4	0	1	4	0.5	34
8 - 9	*	*	*	*	16	4	4	16	4	32.7
9 - 10	*	*	*	*	9	15	9	9	12	32
10 - 11	*	*	*	*	21	9	7	21	8	31
11 - 12	*	*	*	*	13	17	18	13	17.5	30.8
12 - 13	*	*	*	18	23	15	20	20.5	17.5	32.15
13 - 14	*	*	*	23	24	23	21	23.5	22	30.35
14 - 15	*	*	*	24	33	25	17	28.5	21	31.4
15 - 16	*	*	*	27	34	32	25	30.5	28.5	30.85
16 - 17	*	*	*	40	43	21	19	41.5	20	33
17 - 18	*	*	*	43	49	24	28	46	26	32.55
18 - 19	*	*	*	43	45	23	27	44	25	32.9
19 - 20	*	*	*	48	30	29	25	39	27	33.4
20 - 21	*	*	*	29	39	29	21	34	25	36.6
21 - 22	*	*	*	30	27	23	36	28.5	29.5	32.25
22 - 23	*	*	*	22	18	17	13	20	15	32.5
23 - 24	*	*	*	10	13	16	17	11.5	16.5	29.25
Totals	0	0	0	357	455	332	323			
% of Total	0%	0%	0%	24.34%	31.02%	22.63%	22.02%			

Outgoing Weekly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	6	5	7	*	*	*	*	6	0	33.33
1 - 2	4	4	4	*	*	*	*	4	0	30.33
2 - 3	1	3	0	*	*	*	*	1.33	0	31
3 - 4	1	1	2	*	*	*	*	1.33	0	28
4 - 5	1	2	0	*	*	*	*	1	0	31
5 - 6	1	0	0	*	*	*	*	0.33	0	28
6 - 7	0	1	2	*	*	*	*	1	0	30.5
7 - 8	1	3	4	*	*	*	*	2.67	0	30.5
8 - 9	2	15	13	*	*	*	*	10	0	31.83
9 - 10	3	12	12	*	*	*	*	9	0	32.67
10 - 11	6	18	4	*	*	*	*	9.33	0	30.77
11 - 12	15	19	*	*	*	*	*	17	0	30.5
12 - 13	20	19	*	*	*	*	*	19.5	0	31.5
13 - 14	25	25	*	*	*	*	*	25	0	32
14 - 15	27	28	*	*	*	*	*	27.5	0	31.5
15 - 16	32	24	*	*	*	*	*	28	0	31.55
16 - 17	30	45	*	*	*	*	*	37.5	0	32
17 - 18	27	37	*	*	*	*	*	32	0	32
18 - 19	26	52	*	*	*	*	*	39	0	31.75
19 - 20	29	41	*	*	*	*	*	35	0	31.85
20 - 21	24	29	*	*	*	*	*	26.5	0	32.25
21 - 22	16	21	*	*	*	*	*	18.5	0	34.25
22 - 23	19	18	*	*	*	*	*	18.5	0	31.85
23 - 24	12	11	*	*	*	*	*	11.5	0	33
Totals	328	433	48	0	0	0	0			
% of Total	40.54%	53.52%	5.93%	0%	0%	0%	0%			

Outgoing Monthly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	6	3	*	6	3	35
1 - 2	*	*	*	*	2	2	*	2	2	26
2 - 3	*	*	*	*	1	1	*	1	1	23
3 - 4	*	*	*	*	2	4	*	2	4	28
4 - 5	*	*	*	*	0	0	*	0	0	0
5 - 6	*	*	*	*	2	0	*	2	0	30
6 - 7	*	*	*	*	1	0	*	1	0	28
7 - 8	*	*	*	*	4	0	*	4	0	34
8 - 9	*	*	*	*	16	4	*	16	4	32.7
9 - 10	*	*	*	*	9	15	*	9	15	32
10 - 11	*	*	*	*	21	9	*	21	9	31
11 - 12	*	*	*	*	13	17	*	13	17	30.8
12 - 13	*	*	*	18	23	15	*	20.5	15	32.15
13 - 14	*	*	*	23	24	23	*	23.5	23	30.35
14 - 15	*	*	*	24	33	25	*	28.5	25	31.4
15 - 16	*	*	*	27	34	32	*	30.5	32	30.85
16 - 17	*	*	*	40	43	21	*	41.5	21	33
17 - 18	*	*	*	43	49	24	*	46	24	32.55
18 - 19	*	*	*	43	45	23	*	44	23	32.9
19 - 20	*	*	*	48	30	29	*	39	29	33.4
20 - 21	*	*	*	29	39	29	*	34	29	36.6
21 - 22	*	*	*	30	27	23	*	28.5	23	32.25
22 - 23	*	*	*	22	18	17	*	20	17	32.5
23 - 24	*	*	*	10	13	16	*	11.5	16	29.25
Totals	0	0	0	357	455	332	0			
% of Total	0%	0%	0%	31.21%	39.77%	29.02%	0%			

Outgoing Monthly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	6	5	7	*	*	*	3	6	3	33.33
1 - 2	4	4	4	*	*	*	2	4	2	30.33
2 - 3	1	3	0	*	*	*	3	1.33	3	31
3 - 4	1	1	2	*	*	*	1	1.33	1	28
4 - 5	1	2	0	*	*	*	3	1	3	31
5 - 6	1	0	0	*	*	*	1	0.33	1	28
6 - 7	0	1	2	*	*	*	2	1	2	30.5
7 - 8	1	3	4	*	*	*	1	2.67	1	30.5
8 - 9	2	15	13	*	*	*	4	10	4	31.83
9 - 10	3	12	12	*	*	*	9	9	9	32.67
10 - 11	6	18	4	*	*	*	7	9.33	7	30.77
11 - 12	15	19	*	*	*	*	18	17	18	30.5
12 - 13	20	19	*	*	*	*	20	19.5	20	31.5
13 - 14	25	25	*	*	*	*	21	25	21	32
14 - 15	27	28	*	*	*	*	17	27.5	17	31.5
15 - 16	32	24	*	*	*	*	25	28	25	31.55
16 - 17	30	45	*	*	*	*	19	37.5	19	32
17 - 18	27	37	*	*	*	*	28	32	28	32
18 - 19	26	52	*	*	*	*	27	39	27	31.75
19 - 20	29	41	*	*	*	*	25	35	25	31.85
20 - 21	24	29	*	*	*	*	21	26.5	21	32.25
21 - 22	16	21	*	*	*	*	36	18.5	36	34.25
22 - 23	19	18	*	*	*	*	13	18.5	13	31.85
23 - 24	12	11	*	*	*	*	17	11.5	17	33
Totals	328	433	48	0	0	0	323			
% of Total	28.98%	38.25%	4.24%	0%	0%	0%	28.53%			

Outgoing Weekly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	29.83	24.33	27	29.83	25.67	35
1 - 2	*	*	*	*	24.5	25.5	28.5	24.5	27	26
2 - 3	*	*	*	*	23	22	26.67	23	25.5	23
3 - 4	*	*	*	*	20.5	26	27	20.5	26.2	28
4 - 5	*	*	*	*	0	0	26.67	0	26.67	0
5 - 6	*	*	*	*	26.5	0	25	26.5	25	30
6 - 7	*	*	*	*	28	0	31.5	28	31.5	28
7 - 8	*	*	*	*	32	0	36	32	36	34
8 - 9	*	*	*	*	28.94	28.25	26.25	28.94	27.25	32.7
9 - 10	*	*	*	*	28.67	28.2	28.33	28.67	28.25	32
10 - 11	*	*	*	*	27.9	28.44	28.14	27.9	28.31	31
11 - 12	*	*	*	*	27.92	26.94	26.72	27.92	26.83	30.8
12 - 13	*	*	*	27.89	28.57	27.93	30.65	28.27	29.49	32.15
13 - 14	*	*	*	28.04	27.38	28.7	29.71	27.7	29.18	30.35
14 - 15	*	*	*	27.12	29.55	28.72	29.06	28.53	28.86	31.4
15 - 16	*	*	*	26.67	29.21	27.56	29.8	28.08	28.54	30.85
16 - 17	*	*	*	28.62	29.12	28.67	27.05	28.88	27.9	33
17 - 18	*	*	*	28.6	28.82	28.92	28.32	28.72	28.6	32.55
18 - 19	*	*	*	29.19	29.89	27.91	27.81	29.55	27.86	32.9
19 - 20	*	*	*	29.15	28.83	29.48	29.6	29.03	29.54	33.4
20 - 21	*	*	*	32.45	31.03	29.34	29	31.63	29.2	36.6
21 - 22	*	*	*	28.77	28.56	28.83	29	28.67	28.93	32.25
22 - 23	*	*	*	28.95	28.61	27.18	28.46	28.8	27.73	32.5
23 - 24	*	*	*	26.7	26.92	27.62	27.59	26.83	27.61	29.25
Totals	0	0	0	28.7	28.9	28.3	28.6			
% of Total	0%	0%	0%	25.07%	25.24%	24.72%	24.98%			

Outgoing Weekly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	30.5	29	30.29	*	*	*	*	30	*	33.33
1 - 2	29	27.75	28.75	*	*	*	*	28.5	*	30.33
2 - 3	26	27.33	0	*	*	*	*	27	*	31
3 - 4	25	27	30	*	*	*	*	28	*	28
4 - 5	31	27.5	0	*	*	*	*	28.67	*	31
5 - 6	28	0	0	*	*	*	*	28	*	28
6 - 7	0	32	24	*	*	*	*	26.67	*	30.5
7 - 8	24	31.67	32.75	*	*	*	*	31.25	*	30.5
8 - 9	27	29.87	28.46	*	*	*	*	29.07	*	31.83
9 - 10	30	31.17	29.83	*	*	*	*	30.44	*	32.67
10 - 11	30.5	29.22	26	*	*	*	*	29.04	*	30.77
11 - 12	26.4	28.32	*	*	*	*	*	27.47	*	30.5
12 - 13	28.15	29.42	*	*	*	*	*	28.77	*	31.5
13 - 14	27.96	30.08	*	*	*	*	*	29.02	*	32
14 - 15	26.37	28.32	*	*	*	*	*	27.36	*	31.5
15 - 16	27.69	28.33	*	*	*	*	*	27.96	*	31.55
16 - 17	28.37	27.73	*	*	*	*	*	27.99	*	32
17 - 18	29.07	28.49	*	*	*	*	*	28.73	*	32
18 - 19	28.65	29.85	*	*	*	*	*	29.45	*	31.75
19 - 20	28.62	28.78	*	*	*	*	*	28.71	*	31.85
20 - 21	30.29	29.34	*	*	*	*	*	29.77	*	32.25
21 - 22	30.94	29.48	*	*	*	*	*	30.11	*	34.25
22 - 23	28.16	30	*	*	*	*	*	29.05	*	31.85
23 - 24	30.58	27.45	*	*	*	*	*	29.09	*	33
Totals	28.5	29	29.1	0	0	0	0			
% of Total	32.91%	33.49%	33.6%	0%	0%	0%	0%			

Outgoing Monthly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	29.83	24.33	*	29.83	24.33	35
1 - 2	*	*	*	*	24.5	25.5	*	24.5	25.5	26
2 - 3	*	*	*	*	23	22	*	23	22	23
3 - 4	*	*	*	*	20.5	26	*	20.5	26	28
4 - 5	*	*	*	*	0	0	*	*	*	0
5 - 6	*	*	*	*	26.5	0	*	26.5	*	30
6 - 7	*	*	*	*	28	0	*	28	*	28
7 - 8	*	*	*	*	32	0	*	32	*	34
8 - 9	*	*	*	*	28.94	28.25	*	28.94	28.25	32.7
9 - 10	*	*	*	*	28.67	28.2	*	28.67	28.2	32
10 - 11	*	*	*	*	27.9	28.44	*	27.9	28.44	31
11 - 12	*	*	*	*	27.92	26.94	*	27.92	26.94	30.8
12 - 13	*	*	*	27.89	28.57	27.93	*	28.27	27.93	32.15
13 - 14	*	*	*	28.04	27.38	28.7	*	27.7	28.7	30.35
14 - 15	*	*	*	27.12	29.55	28.72	*	28.53	28.72	31.4
15 - 16	*	*	*	26.67	29.21	27.56	*	28.08	27.56	30.85
16 - 17	*	*	*	28.62	29.12	28.67	*	28.88	28.67	33
17 - 18	*	*	*	28.6	28.82	28.92	*	28.72	28.92	32.55
18 - 19	*	*	*	29.19	29.89	27.91	*	29.55	27.91	32.9
19 - 20	*	*	*	29.15	28.83	29.48	*	29.03	29.48	33.4
20 - 21	*	*	*	32.45	31.03	29.34	*	31.63	29.34	36.6
21 - 22	*	*	*	28.77	28.56	28.83	*	28.67	28.83	32.25
22 - 23	*	*	*	28.95	28.61	27.18	*	28.8	27.18	32.5
23 - 24	*	*	*	26.7	26.92	27.62	*	26.83	27.62	29.25
Totals	0	0	0	28.7	28.9	28.3	0			
% of Total	0%	0%	0%	33.41%	33.64%	32.95%	0%			

Outgoing Monthly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	30.5	29	30.29	*	*	*	27	30	27	33.33
1 - 2	29	27.75	28.75	*	*	*	28.5	28.5	28.5	30.33
2 - 3	26	27.33	0	*	*	*	26.67	27	26.67	31
3 - 4	25	27	30	*	*	*	27	28	27	28
4 - 5	31	27.5	0	*	*	*	26.67	28.67	26.67	31
5 - 6	28	0	0	*	*	*	25	28	25	28
6 - 7	0	32	24	*	*	*	31.5	26.67	31.5	30.5
7 - 8	24	31.67	32.75	*	*	*	36	31.25	36	30.5
8 - 9	27	29.87	28.46	*	*	*	26.25	29.07	26.25	31.83
9 - 10	30	31.17	29.83	*	*	*	28.33	30.44	28.33	32.67
10 - 11	30.5	29.22	26	*	*	*	28.14	29.04	28.14	30.77
11 - 12	26.4	28.32	*	*	*	*	26.72	27.47	26.72	30.5
12 - 13	28.15	29.42	*	*	*	*	30.65	28.77	30.65	31.5
13 - 14	27.96	30.08	*	*	*	*	29.71	29.02	29.71	32
14 - 15	26.37	28.32	*	*	*	*	29.06	27.36	29.06	31.5
15 - 16	27.69	28.33	*	*	*	*	29.8	27.96	29.8	31.55
16 - 17	28.37	27.73	*	*	*	*	27.05	27.99	27.05	32
17 - 18	29.07	28.49	*	*	*	*	28.32	28.73	28.32	32
18 - 19	28.65	29.85	*	*	*	*	27.81	29.45	27.81	31.75
19 - 20	28.62	28.78	*	*	*	*	29.6	28.71	29.6	31.85
20 - 21	30.29	29.34	*	*	*	*	29	29.77	29	32.25
21 - 22	30.94	29.48	*	*	*	*	29	30.11	29	34.25
22 - 23	28.16	30	*	*	*	*	28.46	29.05	28.46	31.85
23 - 24	30.58	27.45	*	*	*	*	27.59	29.09	27.59	33
Totals	28.5	29	29.1	0	0	0	28.6			
% of Total	24.74%	25.17%	25.26%	0%	0%	0%	24.83%			

Outgoing Weekly EightyFifthSpeeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	35	27	29	35	28	35
1 - 2	*	*	*	*	26	28	33	26	30.5	26
2 - 3	*	*	*	*	23	22	30	23	26	23
3 - 4	*	*	*	*	28	28	27	28	27.5	28
4 - 5	*	*	*	*	0	0	28	0	28	0
5 - 6	*	*	*	*	30	0	25	30	25	30
6 - 7	*	*	*	*	28	0	32	28	32	28
7 - 8	*	*	*	*	34	0	36	34	36	34
8 - 9	*	*	*	*	32.7	33	27	32.7	30	32.7
9 - 10	*	*	*	*	32	31.5	33	32	32.25	32
10 - 11	*	*	*	*	31	31	29	31	30	31
11 - 12	*	*	*	*	30.8	30	29.2	30.8	29.6	30.8
12 - 13	*	*	*	30	34.3	31	34	32.15	32.5	32.15
13 - 14	*	*	*	30.7	30	32	33	30.35	32.5	30.35
14 - 15	*	*	*	29.8	33	31.7	32	31.4	31.85	31.4
15 - 16	*	*	*	29.7	32	30.7	32.5	30.85	31.6	30.85
16 - 17	*	*	*	32.5	33.5	33	29.7	33	31.35	33
17 - 18	*	*	*	32.3	32.8	32	31	32.55	31.5	32.55
18 - 19	*	*	*	32.8	33	31.7	30	32.9	30.85	32.9
19 - 20	*	*	*	33.5	33.3	35	34	33.4	34.5	33.4
20 - 21	*	*	*	38.7	34.5	32.5	32	36.6	32.25	36.6
21 - 22	*	*	*	33	31.5	31.6	32	32.25	31.8	32.25
22 - 23	*	*	*	32	33	30.7	33	32.5	31.85	32.5
23 - 24	*	*	*	29.8	28.7	32	30	29.25	31	29.25
Totals	0	0	0	384.8	720.1	614.4	741.4			
% of Total	0%	0%	0%	15.64%	29.26%	24.97%	30.13%			

Outgoing Weekly EightyFifthSpeeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	32	32	36	*	*	*	*	33.33	0	33.33
1 - 2	32	30	29	*	*	*	*	30.33	0	30.33
2 - 3	26	36	0	*	*	*	*	31	0	31
3 - 4	25	27	32	*	*	*	*	28	0	28
4 - 5	31	31	0	*	*	*	*	31	0	31
5 - 6	28	0	0	*	*	*	*	28	0	28
6 - 7	0	32	29	*	*	*	*	30.5	0	30.5
7 - 8	24	33	34.5	*	*	*	*	30.5	0	30.5
8 - 9	29	33.5	33	*	*	*	*	31.83	0	31.83
9 - 10	32	33	33	*	*	*	*	32.67	0	32.67
10 - 11	33.5	31.8	27	*	*	*	*	30.77	0	30.77
11 - 12	30	31	*	*	*	*	*	30.5	0	30.5
12 - 13	32	31	*	*	*	*	*	31.5	0	31.5
13 - 14	32	32	*	*	*	*	*	32	0	32
14 - 15	31	32	*	*	*	*	*	31.5	0	31.5
15 - 16	31.5	31.6	*	*	*	*	*	31.55	0	31.55
16 - 17	32	32	*	*	*	*	*	32	0	32
17 - 18	33	31	*	*	*	*	*	32	0	32
18 - 19	30.8	32.7	*	*	*	*	*	31.75	0	31.75
19 - 20	31.7	32	*	*	*	*	*	31.85	0	31.85
20 - 21	33	31.5	*	*	*	*	*	32.25	0	32.25
21 - 22	35	33.5	*	*	*	*	*	34.25	0	34.25
22 - 23	31.7	32	*	*	*	*	*	31.85	0	31.85
23 - 24	36	30	*	*	*	*	*	33	0	33
Totals	712.2	731.6	253.5	0	0	0	0			
% of Total	41.96%	43.1%	14.94%	0%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	35	27	*	35	27	35
1 - 2	*	*	*	*	26	28	*	26	28	26
2 - 3	*	*	*	*	23	22	*	23	22	23
3 - 4	*	*	*	*	28	28	*	28	28	28
4 - 5	*	*	*	*	0	0	*	0	0	0
5 - 6	*	*	*	*	30	0	*	30	0	30
6 - 7	*	*	*	*	28	0	*	28	0	28
7 - 8	*	*	*	*	34	0	*	34	0	34
8 - 9	*	*	*	*	32.7	33	*	32.7	33	32.7
9 - 10	*	*	*	*	32	31.5	*	32	31.5	32
10 - 11	*	*	*	*	31	31	*	31	31	31
11 - 12	*	*	*	*	30.8	30	*	30.8	30	30.8
12 - 13	*	*	*	30	34.3	31	*	32.15	31	32.15
13 - 14	*	*	*	30.7	30	32	*	30.35	32	30.35
14 - 15	*	*	*	29.8	33	31.7	*	31.4	31.7	31.4
15 - 16	*	*	*	29.7	32	30.7	*	30.85	30.7	30.85
16 - 17	*	*	*	32.5	33.5	33	*	33	33	33
17 - 18	*	*	*	32.3	32.8	32	*	32.55	32	32.55
18 - 19	*	*	*	32.8	33	31.7	*	32.9	31.7	32.9
19 - 20	*	*	*	33.5	33.3	35	*	33.4	35	33.4
20 - 21	*	*	*	38.7	34.5	32.5	*	36.6	32.5	36.6
21 - 22	*	*	*	33	31.5	31.6	*	32.25	31.6	32.25
22 - 23	*	*	*	32	33	30.7	*	32.5	30.7	32.5
23 - 24	*	*	*	29.8	28.7	32	*	29.25	32	29.25

Outgoing Monthly EightyFifthSpeeds
 Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	32	32	36	*	*	*	29	33.33	29	33.33
1 - 2	32	30	29	*	*	*	33	30.33	33	30.33
2 - 3	26	36	0	*	*	*	30	31	30	31
3 - 4	25	27	32	*	*	*	27	28	27	28
4 - 5	31	31	0	*	*	*	28	31	28	31
5 - 6	28	0	0	*	*	*	25	28	25	28
6 - 7	0	32	29	*	*	*	32	30.5	32	30.5
7 - 8	24	33	34.5	*	*	*	36	30.5	36	30.5
8 - 9	29	33.5	33	*	*	*	27	31.83	27	31.83
9 - 10	32	33	33	*	*	*	33	32.67	33	32.67
10 - 11	33.5	31.8	27	*	*	*	29	30.77	29	30.77
11 - 12	30	31	*	*	*	*	29.2	30.5	29.2	30.5
12 - 13	32	31	*	*	*	*	34	31.5	34	31.5
13 - 14	32	32	*	*	*	*	33	32	33	32
14 - 15	31	32	*	*	*	*	32	31.5	32	31.5
15 - 16	31.5	31.6	*	*	*	*	32.5	31.55	32.5	31.55
16 - 17	32	32	*	*	*	*	29.7	32	29.7	32
17 - 18	33	31	*	*	*	*	31	32	31	32
18 - 19	30.8	32.7	*	*	*	*	30	31.75	30	31.75
19 - 20	31.7	32	*	*	*	*	34	31.85	34	31.85
20 - 21	33	31.5	*	*	*	*	32	32.25	32	32.25
21 - 22	35	33.5	*	*	*	*	32	34.25	32	34.25
22 - 23	31.7	32	*	*	*	*	33	31.85	33	31.85
23 - 24	36	30	*	*	*	*	30	33	30	33

Summary of Violators

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	30	29.1	0	0.0
01:00:00	18	27.7	0	0.0
02:00:00	9	25.9	0	0.0
03:00:00	11	25.8	0	0.0
04:00:00	6	27.7	0	0.0
05:00:00	4	26.5	0	0.0
06:00:00	6	28.5	0	0.0
07:00:00	13	31.8	0	0.0
08:00:00	54	28.8	0	0.0
09:00:00	60	29.3	0	0.0
10:00:00	65	28.5	0	0.0
11:00:00	82	27.3	0	0.0
12:00:00	115	28.8	0	0.0
13:00:00	141	28.6	0	0.0
14:00:00	154	28.2	0	0.0
15:00:00	174	28.2	0	0.0
16:00:00	198	28.3	0	0.0
17:00:00	208	28.7	0	0.0
18:00:00	216	29.1	1	46.0
19:00:00	202	29.1	0	0.0
20:00:00	171	30.3	3	48.0
21:00:00	153	29.1	1	47.0
22:00:00	107	28.6	2	49.5
23:00:00	79	27.8	0	0.0

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	0	0	2	11	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	18	27.9	30	21 to 31	88.9	*	*	1	17	0
8/29/2024	13:00	0	0	2	16	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	23	28	30.7	22 to 32	95.7	*	*	0	21	2
8/29/2024	14:00	0	0	5	12	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	24	27.1	29.8	22 to 32	95.8	*	*	0	23	1
8/29/2024	15:00	0	1	5	15	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	27	26.7	29.7	21 to 31	88.9	*	*	0	25	2
8/29/2024	16:00	0	1	5	17	15	2	0	0	0	0	0	0	0	0	0	0	0	0	0	40	28.6	32.5	23 to 33	82.5	*	*	0	36	4
8/29/2024	17:00	0	1	3	22	16	1	0	0	0	0	0	0	0	0	0	0	0	0	0	43	28.6	32.3	24 to 34	93.0	*	*	0	41	2
8/29/2024	18:00	0	1	3	19	15	5	0	0	0	0	0	0	0	0	0	0	0	0	0	43	29.2	32.8	25 to 35	83.7	*	*	0	42	1
8/29/2024	19:00	0	0	9	18	16	4	1	0	0	0	0	0	0	0	0	0	0	0	0	48	29.1	33.5	26 to 36	79.2	*	*	0	46	2
8/29/2024	20:00	0	0	0	12	8	6	2	1	0	0	0	0	0	0	0	0	0	0	0	29	32.4	38.7	26 to 36	79.3	1	3.4	0	29	0
8/29/2024	21:00	0	0	4	17	6	2	0	1	0	0	0	0	0	0	0	0	0	0	0	30	28.8	33	22 to 32	80.0	1	3.3	0	27	3
8/29/2024	22:00	0	1	3	8	8	1	0	1	0	0	0	0	0	0	0	0	0	0	0	22	29	32	24 to 34	77.3	1	4.5	0	21	1
8/29/2024	23:00	0	1	2	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	26.7	29.8	20 to 30	80.0	*	*	0	10	0
24 Hr Summary		0	6	43	169	110	23	3	3	0	0	0	0	0	0	0	0	0	0	357	28.7	33	23 to 33	81.2	3	0.8	1	338	18	

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/30/2024	00:00	0	0	1	2	1	2	0	0	0	0	0	0	0	0	0	0	0	0	6	29.8	35	22 to 32	66.7	*	*	0	6	0	
8/30/2024	01:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24.5	26	16 to 26	100.0	*	*	0	2	0	
8/30/2024	02:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	23	23	13 to 23	100.0	*	*	0	1	0	
8/30/2024	03:00	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	20.5	28	3 to 13	50.0	*	*	0	2	0	
8/30/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/30/2024	05:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	26.5	30	20 to 30	100.0	*	*	0	2	0	
8/30/2024	06:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0	
8/30/2024	07:00	0	0	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	4	32	34	24 to 34	75.0	*	*	0	3	1	
8/30/2024	08:00	0	0	4	5	6	1	0	0	0	0	0	0	0	0	0	0	0	0	16	28.9	32.7	23 to 33	93.8	*	*	0	14	2	
8/30/2024	09:00	0	0	1	5	2	1	0	0	0	0	0	0	0	0	0	0	0	0	9	28.7	32	22 to 32	88.9	*	*	0	9	0	
8/30/2024	10:00	0	0	4	11	5	1	0	0	0	0	0	0	0	0	0	0	0	0	21	27.9	31	22 to 32	95.2	*	*	0	20	1	
8/30/2024	11:00	0	0	3	5	4	1	0	0	0	0	0	0	0	0	0	0	0	0	13	27.9	30.8	21 to 31	92.3	*	*	0	12	1	
8/30/2024	12:00	0	1	3	10	5	4	0	0	0	0	0	0	0	0	0	0	0	0	23	28.6	34.3	21 to 31	78.3	*	*	0	22	1	
8/30/2024	13:00	0	1	5	10	7	1	0	0	0	0	0	0	0	0	0	0	0	0	24	27.4	30	20 to 30	79.2	*	*	0	22	2	
8/30/2024	14:00	0	0	1	17	13	1	1	0	0	0	0	0	0	0	0	0	0	0	33	29.5	33	24 to 34	90.9	*	*	0	31	2	
8/30/2024	15:00	0	0	6	11	15	1	1	0	0	0	0	0	0	0	0	0	0	0	34	29.2	32	23 to 33	91.2	*	*	0	32	2	
8/30/2024	16:00	0	1	5	22	10	4	0	1	0	0	0	0	0	0	0	0	0	0	43	29.1	33.5	25 to 35	79.1	*	*	0	39	4	
8/30/2024	17:00	0	1	6	18	21	3	0	0	0	0	0	0	0	0	0	0	0	0	49	28.8	32.8	23 to 33	83.7	*	*	0	48	1	
8/30/2024	18:00	0	0	5	15	19	5	0	1	0	0	0	0	0	0	0	0	0	0	45	29.9	33	23 to 33	80.0	1	2.2	0	43	2	
8/30/2024	19:00	0	1	0	17	10	2	0	0	0	0	0	0	0	0	0	0	0	0	30	28.8	33.3	25 to 35	93.3	*	*	0	29	1	
8/30/2024	20:00	0	0	3	9	20	4	2	1	0	0	0	0	0	0	0	0	0	0	39	31	34.5	26 to 36	82.1	1	2.6	0	39	0	
8/30/2024	21:00	0	0	3	13	10	1	0	0	0	0	0	0	0	0	0	0	0	0	27	28.6	31.5	23 to 33	92.6	*	*	0	27	0	
8/30/2024	22:00	0	0	5	7	5	0	0	0	1	0	0	0	0	0	0	0	0	0	18	28.6	33	24 to 34	83.3	1	5.6	0	18	0	
8/30/2024	23:00	0	0	3	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	13	26.9	28.7	19 to 29	92.3	*	*	0	13	0	
24 Hr Summary		1	5	61	191	156	33	4	3	1	0	0	0	0	0	0	0	0	0	455	29	33	23 to 33	82.2	3	0.7	0	435	20	

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/31/2024	00:00	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	24.3	27	17 to 27	100.0	*	*	0	3	0	
8/31/2024	01:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25.5	28	18 to 28	100.0	*	*	0	2	0	
8/31/2024	02:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	22	22	12 to 22	100.0	*	*	0	1	0	
8/31/2024	03:00	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26	28	22 to 32	75.0	*	*	0	4	0	
8/31/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/31/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/31/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/31/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/31/2024	08:00	0	0	2	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	4	28.3	33	13 to 23	50.0	*	*	0	3	1	
8/31/2024	09:00	0	0	3	5	7	0	0	0	0	0	0	0	0	0	0	0	0	0	15	28.2	31.5	22 to 32	93.3	*	*	0	14	1	
8/31/2024	10:00	0	0	1	4	3	1	0	0	0	0	0	0	0	0	0	0	0	0	9	28.4	31	21 to 31	88.9	*	*	0	9	0	
8/31/2024	11:00	0	1	2	10	4	0	0	0	0	0	0	0	0	0	0	0	0	0	17	26.9	30	23 to 33	88.2	*	*	0	17	0	
8/31/2024	12:00	0	0	1	9	5	0	0	0	0	0	0	0	0	0	0	0	0	0	15	27.9	31	23 to 33	100.0	*	*	0	15	0	
8/31/2024	13:00	0	0	2	11	8	2	0	0	0	0	0	0	0	0	0	0	0	0	23	28.7	32	22 to 32	87.0	*	*	0	22	1	
8/31/2024	14:00	0	0	4	14	5	1	1	0	0	0	0	0	0	0	0	0	0	0	25	28.7	31.7	22 to 32	88.0	*	*	0	24	1	
8/31/2024	15:00	0	0	7	15	10	0	0	0	0	0	0	0	0	0	0	0	0	0	32	27.6	30.7	22 to 32	90.6	*	*	0	30	2	
8/31/2024	16:00	0	1	3	10	5	2	0	0	0	0	0	0	0	0	0	0	0	0	21	28.7	33	23 to 33	81.0	*	*	0	21	0	
8/31/2024	17:00	0	0	3	11	9	1	0	0	0	0	0	0	0	0	0	0	0	0	24	28.9	32	23 to 33	91.7	*	*	0	24	0	
8/31/2024	18:00	0	1	4	10	7	1	0	0	0	0	0	0	0	0	0	0	0	0	23	27.9	31.7	22 to 32	87.0	*	*	0	22	1	
8/31/2024	19:00	0	0	2	18	4	5	0	0	0	0	0	0	0	0	0	0	0	0	29	29.5	35	23 to 33	82.8	*	*	0	28	1	
8/31/2024	20:00	0	0	1	14	12	2	0	0	0	0	0	0	0	0	0	0	0	0	29	29.3	32.5	25 to 35	93.1	*	*	0	28	1	
8/31/2024	21:00	0	0	1	11	11	0	0	0	0	0	0	0	0	0	0	0	0	0	23	28.8	31.6	24 to 34	95.7	*	*	0	22	1	
8/31/2024	22:00	0	2	2	7	6	0	0	0	0	0	0	0	0	0	0	0	0	0	17	27.2	30.7	22 to 32	88.2	*	*	0	17	0	
8/31/2024	23:00	0	0	5	6	4	1	0	0	0	0	0	0	0	0	0	0	0	0	16	27.6	32	22 to 32	87.5	*	*	0	16	0	
24 Hr Summary		0	6	46	160	102	17	1	0	0	0	0	0	0	0	0	0	0	0	332	28.3	32	23 to 33	87.3	*	*	0	322	10	

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/1/2024	00:00	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	27	29	19 to 29	100.0	*	*	0	3	0
9/1/2024	01:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28.5	33	23 to 33	100.0	*	*	0	2	0
9/1/2024	02:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26.7	30	20 to 30	100.0	*	*	0	2	1
9/1/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	27	27	17 to 27	100.0	*	*	0	1	0
9/1/2024	04:00	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26.7	28	18 to 28	100.0	*	*	0	3	0
9/1/2024	05:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0
9/1/2024	06:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	31.5	32	22 to 32	100.0	*	*	0	2	0
9/1/2024	07:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	36	36	26 to 36	100.0	*	*	0	1	0
9/1/2024	08:00	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26.3	27	18 to 28	100.0	*	*	0	3	1
9/1/2024	09:00	0	0	2	3	3	1	0	0	0	0	0	0	0	0	0	0	0	0	9	28.3	33	20 to 30	66.7	*	*	0	9	0
9/1/2024	10:00	0	0	2	4	0	0	1	0	0	0	0	0	0	0	0	0	0	0	7	28.1	29	19 to 29	85.7	*	*	0	7	0
9/1/2024	11:00	0	1	1	12	4	0	0	0	0	0	0	0	0	0	0	0	0	0	18	26.7	29.2	20 to 30	94.4	*	*	0	18	0
9/1/2024	12:00	0	0	1	7	9	3	0	0	0	0	0	0	0	0	0	0	0	0	20	30.6	34	26 to 36	90.0	*	*	0	20	0
9/1/2024	13:00	0	0	2	9	7	3	0	0	0	0	0	0	0	0	0	0	0	0	21	29.7	33	23 to 33	81.0	*	*	0	20	1
9/1/2024	14:00	0	1	0	7	9	0	0	0	0	0	0	0	0	0	0	0	0	0	17	29.1	32	24 to 34	94.1	*	*	0	16	1
9/1/2024	15:00	0	0	2	9	12	1	1	0	0	0	0	0	0	0	0	0	0	0	25	29.8	32.5	23 to 33	88.0	*	*	0	24	1
9/1/2024	16:00	0	1	4	9	4	1	0	0	0	0	0	0	0	0	0	0	0	0	19	27.1	29.7	21 to 31	89.5	*	*	0	19	0
9/1/2024	17:00	0	0	4	15	8	1	0	0	0	0	0	0	0	0	0	0	0	0	28	28.3	31	23 to 33	89.3	*	*	0	27	1
9/1/2024	18:00	0	0	3	15	9	0	0	0	0	0	0	0	0	0	0	0	0	0	27	27.8	30	22 to 32	96.3	*	*	0	26	1
9/1/2024	19:00	0	0	2	11	8	4	0	0	0	0	0	0	0	0	0	0	0	0	25	29.6	34	25 to 35	84.0	*	*	0	24	1
9/1/2024	20:00	0	0	2	12	5	2	0	0	0	0	0	0	0	0	0	0	0	0	21	29	32	22 to 32	85.7	*	*	0	20	1
9/1/2024	21:00	0	0	6	14	15	1	0	0	0	0	0	0	0	0	0	0	0	0	36	29	32	24 to 34	88.9	*	*	0	35	1
9/1/2024	22:00	0	1	2	3	5	2	0	0	0	0	0	0	0	0	0	0	0	0	13	28.5	33	25 to 35	76.9	*	*	0	13	0
9/1/2024	23:00	0	1	2	9	5	0	0	0	0	0	0	0	0	0	0	0	0	0	17	27.6	30	21 to 31	88.2	*	*	0	17	0
24 Hr Summary		0	5	39	150	107	20	2	0	0	0	0	0	0	0	0	0	0	0	323	28.6	32	24 to 34	85.1	*	*	0	313	10

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/2/2024	00:00	0	0	1	2	2	0	1	0	0	0	0	0	0	0	0	0	0	0	6	30.5	32	22 to 32	83.3	*	*	0	6	0	
9/2/2024	01:00	0	0	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	4	29	32	16 to 26	50.0	*	*	0	4	0	
9/2/2024	02:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	1	0	
9/2/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0	
9/2/2024	04:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	31	31	21 to 31	100.0	*	*	0	1	0	
9/2/2024	05:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0	
9/2/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/2/2024	07:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	24	24	14 to 24	100.0	*	*	0	1	0	
9/2/2024	08:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27	29	19 to 29	100.0	*	*	0	2	0	
9/2/2024	09:00	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	30	32	22 to 32	100.0	*	*	0	3	0	
9/2/2024	10:00	0	0	0	2	4	0	0	0	0	0	0	0	0	0	0	0	0	0	6	30.5	33.5	24 to 34	100.0	*	*	0	6	0	
9/2/2024	11:00	0	1	2	9	3	0	0	0	0	0	0	0	0	0	0	0	0	0	15	26.4	30	22 to 32	86.7	*	*	0	15	0	
9/2/2024	12:00	1	1	3	4	10	1	0	0	0	0	0	0	0	0	0	0	0	0	20	28.1	32	23 to 33	80.0	*	*	0	18	2	
9/2/2024	13:00	0	0	4	14	5	2	0	0	0	0	0	0	0	0	0	0	0	0	25	28	32	25 to 35	84.0	*	*	0	24	1	
9/2/2024	14:00	0	2	6	12	7	0	0	0	0	0	0	0	0	0	0	0	0	0	27	26.4	31	23 to 33	85.2	*	*	0	26	1	
9/2/2024	15:00	0	3	5	11	11	2	0	0	0	0	0	0	0	0	0	0	0	0	32	27.7	31.5	22 to 32	78.1	*	*	0	29	3	
9/2/2024	16:00	0	0	6	13	9	1	1	0	0	0	0	0	0	0	0	0	0	0	30	28.4	32	23 to 33	90.0	*	*	0	29	1	
9/2/2024	17:00	0	0	3	11	11	1	1	0	0	0	0	0	0	0	0	0	0	0	27	29.1	33	24 to 34	88.9	*	*	0	26	1	
9/2/2024	18:00	0	0	4	10	11	1	0	0	0	0	0	0	0	0	0	0	0	0	26	28.7	30.8	23 to 33	92.3	*	*	0	25	1	
9/2/2024	19:00	0	0	4	15	8	1	1	0	0	0	0	0	0	0	0	0	0	0	29	28.6	31.7	22 to 32	89.7	*	*	0	28	1	
9/2/2024	20:00	0	0	2	6	14	2	0	0	0	0	0	0	0	0	0	0	0	0	24	30.3	33	24 to 34	91.7	*	*	0	23	1	
9/2/2024	21:00	0	0	1	4	8	3	0	0	0	0	0	0	0	0	0	0	0	0	16	30.9	35	27 to 37	87.5	*	*	0	15	1	
9/2/2024	22:00	0	1	1	10	5	2	0	0	0	0	0	0	0	0	0	0	0	0	19	28.2	31.7	22 to 32	84.2	*	*	0	18	1	
9/2/2024	23:00	0	1	0	5	3	1	2	0	0	0	0	0	0	0	0	0	0	0	12	30.6	36	21 to 31	66.7	*	*	0	11	1	
24 Hr Summary		1	9	44	135	115	18	6	0	0	0	0	0	0	0	0	0	0	0	328	28.5	32	23 to 33	82.6	*	*	0	313	15	

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/3/2024	00:00	0	0	1	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	5	29	32	24 to 34	80.0	*	*	0	4	1	
9/3/2024	01:00	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	27.8	30	21 to 31	100.0	*	*	0	4	0	
9/3/2024	02:00	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	27.3	36	15 to 25	66.7	*	*	0	3	0	
9/3/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	27	27	17 to 27	100.0	*	*	0	1	0	
9/3/2024	04:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27.5	31	21 to 31	100.0	*	*	0	2	0	
9/3/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	06:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	32	32	22 to 32	100.0	*	*	0	1	0	
9/3/2024	07:00	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3	31.7	33	23 to 33	100.0	*	*	0	3	0	
9/3/2024	08:00	0	0	0	10	4	1	0	0	0	0	0	0	0	0	0	0	0	0	15	29.9	33.5	24 to 34	93.3	*	*	0	13	2	
9/3/2024	09:00	0	0	1	4	5	0	2	0	0	0	0	0	0	0	0	0	0	0	12	31.2	33	23 to 33	83.3	*	*	0	11	1	
9/3/2024	10:00	0	0	3	4	9	2	0	0	0	0	0	0	0	0	0	0	0	0	18	29.2	31.8	22 to 32	88.9	*	*	0	18	0	
9/3/2024	11:00	0	0	3	10	6	0	0	0	0	0	0	0	0	0	0	0	0	0	19	28.3	31	23 to 33	100.0	*	*	0	18	1	
9/3/2024	12:00	0	0	0	9	10	0	0	0	0	0	0	0	0	0	0	0	0	0	19	29.4	31	23 to 33	100.0	*	*	0	19	0	
9/3/2024	13:00	0	0	3	7	13	1	1	0	0	0	0	0	0	0	0	0	0	0	25	30.1	32	22 to 32	84.0	*	*	0	23	2	
9/3/2024	14:00	0	0	5	15	7	1	0	0	0	0	0	0	0	0	0	0	0	0	28	28.3	32	23 to 33	89.3	*	*	0	27	1	
9/3/2024	15:00	0	0	5	7	11	1	0	0	0	0	0	0	0	0	0	0	0	0	24	28.3	31.6	23 to 33	87.5	*	*	0	21	3	
9/3/2024	16:00	0	0	12	18	14	1	0	0	0	0	0	0	0	0	0	0	0	0	45	27.7	32	23 to 33	82.2	*	*	0	42	3	
9/3/2024	17:00	0	1	3	19	12	1	1	0	0	0	0	0	0	0	0	0	0	0	37	28.5	31	24 to 34	91.9	*	*	0	37	0	
9/3/2024	18:00	0	0	3	18	27	4	0	0	0	0	0	0	0	0	0	0	0	0	52	29.8	32.7	24 to 34	88.5	*	*	0	50	2	
9/3/2024	19:00	0	0	6	17	15	3	0	0	0	0	0	0	0	0	0	0	0	0	41	28.8	32	23 to 33	87.8	*	*	0	38	3	
9/3/2024	20:00	0	0	2	17	9	0	0	0	1	0	0	0	0	0	0	0	0	0	29	29.3	31.5	24 to 34	96.6	1	3.4	0	29	0	
9/3/2024	21:00	0	0	0	14	5	2	0	0	0	0	0	0	0	0	0	0	0	0	21	29.5	33.5	25 to 35	95.2	*	*	0	20	1	
9/3/2024	22:00	0	0	2	5	8	3	0	0	0	0	0	0	0	0	0	0	0	0	18	30	32	25 to 35	88.9	*	*	0	17	1	
9/3/2024	23:00	0	1	1	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	11	27.5	30	22 to 32	90.9	*	*	0	9	2	
24 Hr Summary		0	2	53	183	169	21	4	0	1	0	0	0	0	0	0	0	0	0	433	29	33	24 to 34	86.8	1	0.2	0	410	23	

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	0	0	0	5	0	2	0	0	0	0	0	0	0	0	0	0	0	0	7	30.3	36	26 to 36	85.7	*	*	0	6	1	
9/4/2024	01:00	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28.8	29	24 to 34	100.0	*	*	0	4	0	
9/4/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	03:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30	32	22 to 32	100.0	*	*	0	2	0	
9/4/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	06:00	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24	29	19 to 29	100.0	*	*	0	2	0	
9/4/2024	07:00	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	4	32.8	34.5	25 to 35	100.0	*	*	0	4	0	
9/4/2024	08:00	0	0	1	7	4	1	0	0	0	0	0	0	0	0	0	0	0	0	13	28.5	33	25 to 35	92.3	*	*	0	11	2	
9/4/2024	09:00	0	0	1	6	5	0	0	0	0	0	0	0	0	0	0	0	0	0	12	29.8	33	24 to 34	100.0	*	*	0	12	0	
9/4/2024	10:00	0	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26	27	20 to 30	100.0	*	*	0	4	0	
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	1	4	24	14	5	0	0	0	0	0	0	0	0	0	0	0	0	48	29.1	34	25 to 35	85.4	*	*	0	45	3	

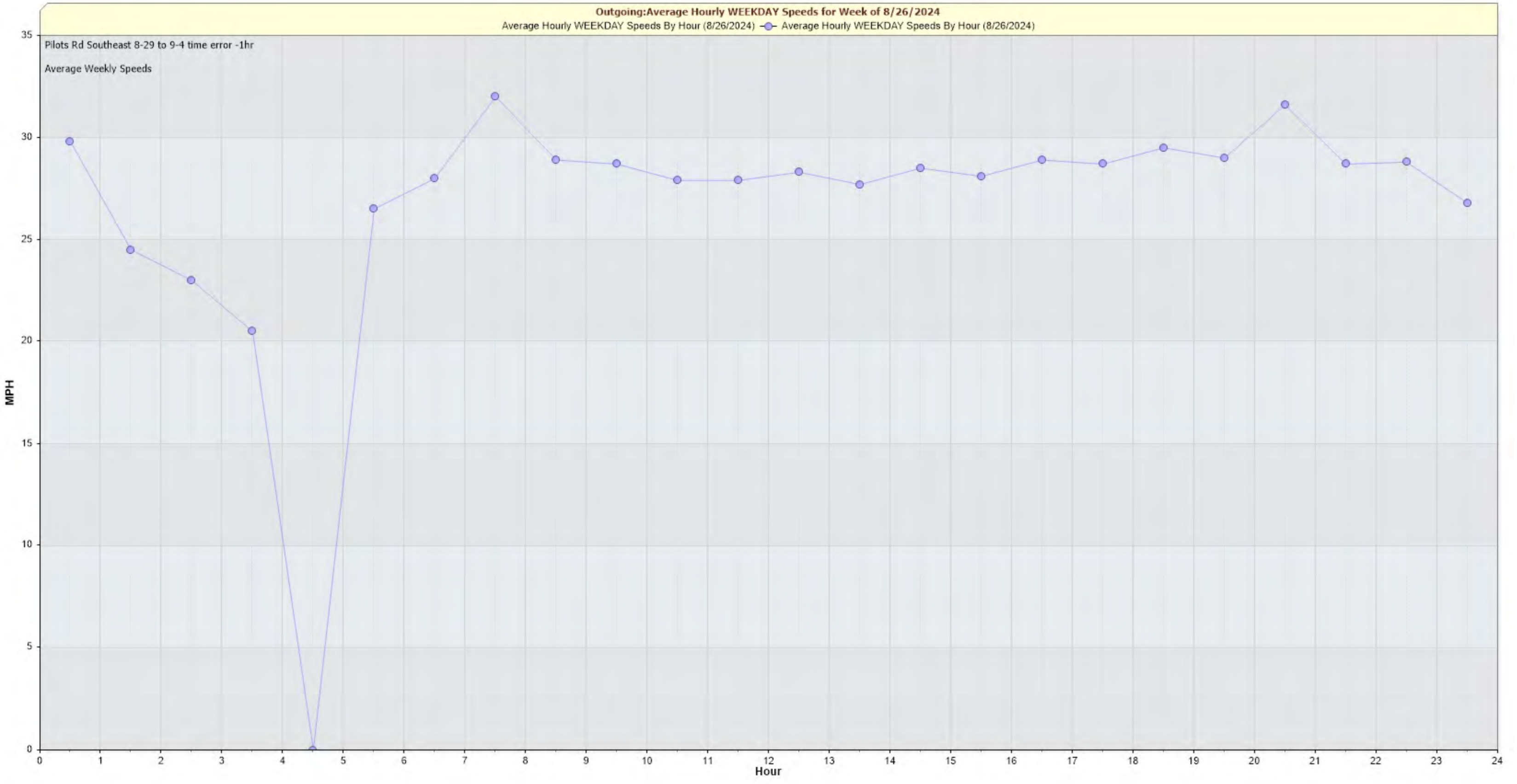
Outgoing: Average Hourly Volume for Week of 8/26/2024
Average Counts By Hour (8/26/2024)

Pilots Rd Southeast 8-29 to 9-4 time error -1hr
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 8/26/2024
Average Hourly WEEKDAY Speeds By Hour (8/26/2024) — Average Hourly WEEKDAY Speeds By Hour (8/26/2024)

Pilots Rd Southeast 8-29 to 9-4 time error -1hr
Average Weekly Speeds



Outgoing:Daily Volume for Week of 8/26/2024

Daily Vehicle Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

Daily Volumes

Vehicles

500
450
400
350
300
250
200
150
100
50
0

Monday

Tuesday

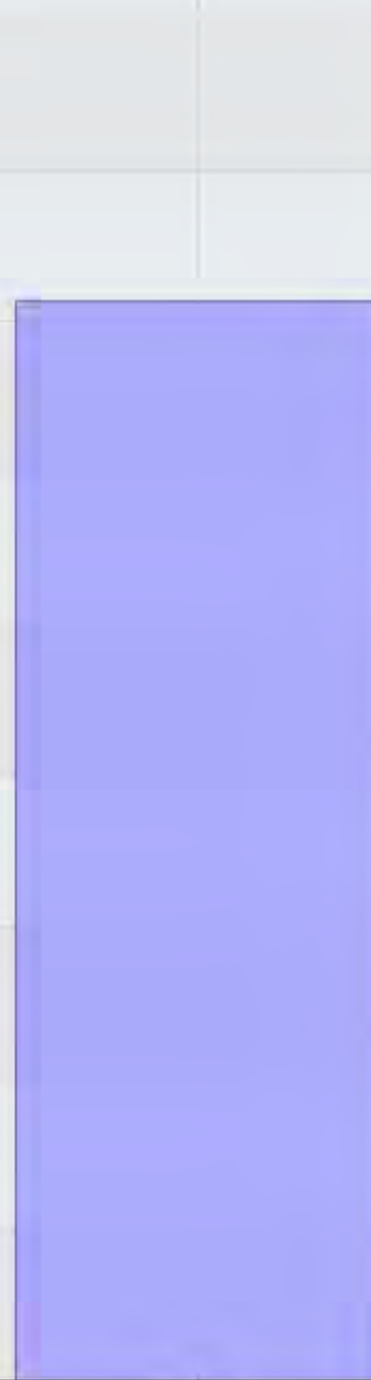
Wednesday

Thursday
Day

Friday

Saturday

Sunday



For Project: Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs
 Project Notes:
 Location/Name: Incoming
 Report Generated: 10/3/2024 3:33:12 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 8/29/2024 1:00:00 PM through 9/3/2024 1:59:59 PM
 85th Percentile Speed: 44 MPH
 85th Percentile Vehicles: 5182
 Max Speed: 98 MPH on 8/29/2024 3:06:29 PM
 Total Vehicles: 6096
 AADT: 1209

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	992	1016
AM Peak 11:00 AM	70	64
PM Peak 8:00 PM	111	103

Speed

Speed Limit: 45
 85th Percentile Speed: 44
 50th Percentile Speed: 39
 10 MPH Pace Interval: 34.0 MPH to 44.0 MPH
 Average Speed: 38.54

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	73	34	N/A	59	123	64	83
% over limit	7.1	7.3	N/A	6.1	8.2	6.0	7.8
Avg Speeder	48.2	47.9	N/A	48.8	48.3	48.2	48.1
Avg Speed	38.5	38.5	N/A	38.6	38.5	38.6	38.6

Class Counts

	Number	%
VEH_SM	14	0.2
VEH_MED	5758	94.5
VEH_LG	324	5.3
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Incoming Summary
Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/29/2024 2:00:00 PM	43.0	24	28	46	46.0	3.6%	39.0
8/29/2024 3:00:00 PM	42.0	79	93	51	48.3	4.3%	37.2
8/29/2024 4:00:00 PM	43.0	93	109	98	64.0	2.8%	37.8
8/29/2024 5:00:00 PM	43.0	87	102	51	48.0	3.9%	38.7
8/29/2024 6:00:00 PM	43.0	87	102	49	47.6	4.9%	37.5
8/29/2024 7:00:00 PM	41.0	84	99	49	47.4	5.1%	37.7
8/29/2024 8:00:00 PM	43.0	93	109	52	48.4	7.3%	39.5
8/29/2024 9:00:00 PM	45.0	88	103	51	47.9	12.6%	40.0
8/29/2024 10:00:00 PM	44.0	76	89	54	49.3	9.0%	39.9
8/29/2024 11:00:00 PM	44.0	59	69	49	47.6	7.2%	38.8
8/30/2024 12:00:00 AM	41.0	58	68	47	47.0	4.4%	38.4
8/30/2024 1:00:00 AM	44.0	39	46	58	49.2	13.0%	39.2
8/30/2024 2:00:00 AM	43.0	17	20	48	48.0	5.0%	38.5
8/30/2024 3:00:00 AM	47.0	8	9	47	47.0	22.2%	38.3
8/30/2024 4:00:00 AM	40.0	3	4	42	0.0	0.0%	40.0
8/30/2024 5:00:00 AM	31.0	1	1	31	0.0	0.0%	31.0
8/30/2024 6:00:00 AM	52.0	2	2	52	51.0	100.0%	51.0
8/30/2024 7:00:00 AM	38.0	1	1	38	0.0	0.0%	38.0
8/30/2024 8:00:00 AM	40.0	4	5	40	0.0	0.0%	39.2
8/30/2024 9:00:00 AM	44.0	12	14	47	47.0	7.1%	40.7
8/30/2024 10:00:00 AM	44.0	36	42	52	48.8	9.5%	39.0
8/30/2024 11:00:00 AM	45.0	71	84	57	49.8	15.5%	40.4
8/30/2024 12:00:00 PM	44.0	71	83	49	46.7	13.1%	38.7
8/30/2024 1:00:00 PM	44.0	87	102	62	50.4	7.8%	38.1
8/30/2024 2:00:00 PM	43.0	77	91	51	49.0	2.2%	37.9
8/30/2024 3:00:00 PM	42.0	85	100	51	47.8	4.0%	38.1
8/30/2024 4:00:00 PM	44.0	82	96	50	47.0	10.4%	38.6
8/30/2024 5:00:00 PM	43.0	82	97	49	47.6	5.2%	37.9
8/30/2024 6:00:00 PM	43.0	101	119	50	47.7	5.0%	37.1
8/30/2024 7:00:00 PM	42.0	118	139	52	48.4	5.0%	37.5
8/30/2024 8:00:00 PM	43.0	100	118	58	49.8	6.8%	38.7
8/30/2024 9:00:00 PM	45.0	123	145	55	47.9	10.3%	39.1
8/30/2024 10:00:00 PM	42.0	74	87	50	47.0	5.7%	37.0
8/30/2024 11:00:00 PM	44.0	48	56	52	48.6	8.9%	39.5
8/31/2024 12:00:00 AM	47.0	39	46	50	47.8	17.4%	40.6
8/31/2024 1:00:00 AM	42.0	42	50	45	0.0	0.0%	37.6
8/31/2024 2:00:00 AM	44.0	19	22	45	0.0	0.0%	39.0
8/31/2024 3:00:00 AM	44.0	8	9	47	47.0	11.1%	40.7
8/31/2024 4:00:00 AM	36.0	1	1	36	0.0	0.0%	36.0
8/31/2024 5:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
8/31/2024 6:00:00 AM	42.0	3	3	42	0.0	0.0%	36.7
8/31/2024 7:00:00 AM	37.0	1	1	37	0.0	0.0%	37.0
8/31/2024 8:00:00 AM	43.0	2	2	43	0.0	0.0%	42.0
8/31/2024 9:00:00 AM	51.0	2	2	51	51.0	50.0%	43.0
8/31/2024 10:00:00 AM	43.0	13	15	50	48.5	13.3%	38.9
8/31/2024 11:00:00 AM	41.0	20	24	53	53.0	4.2%	36.8
8/31/2024 12:00:00 PM	43.0	43	51	48	47.0	5.9%	38.3

Incoming Summary
 Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

8/31/2024 1:00:00 PM	43.0	43	51	54	50.3	7.8%	39.1
8/31/2024 2:00:00 PM	44.0	59	69	50	48.0	4.3%	39.4
8/31/2024 3:00:00 PM	43.0	86	101	48	46.9	9.9%	37.9
8/31/2024 4:00:00 PM	43.0	75	88	48	46.8	4.5%	38.2
8/31/2024 5:00:00 PM	43.0	69	81	49	48.0	2.5%	38.6
8/31/2024 6:00:00 PM	43.0	72	85	51	48.8	4.7%	39.3
8/31/2024 7:00:00 PM	44.0	68	80	64	50.1	11.3%	39.5
8/31/2024 8:00:00 PM	43.0	74	87	53	48.4	5.7%	38.4
8/31/2024 9:00:00 PM	44.0	60	71	48	46.7	4.2%	39.1
8/31/2024 10:00:00 PM	43.0	54	63	54	48.2	7.9%	38.4
8/31/2024 11:00:00 PM	42.0	44	52	47	47.0	1.9%	37.9
9/1/2024 12:00:00 AM	44.0	43	51	48	47.0	11.8%	38.8
9/1/2024 1:00:00 AM	43.0	26	31	47	47.0	3.2%	38.6
9/1/2024 2:00:00 AM	40.0	31	37	52	48.0	10.8%	37.6
9/1/2024 3:00:00 AM	40.0	5	6	40	0.0	0.0%	30.3
9/1/2024 4:00:00 AM	41.0	4	5	43	0.0	0.0%	38.6
9/1/2024 5:00:00 AM	37.0	6	7	40	0.0	0.0%	33.3
9/1/2024 6:00:00 AM	49.0	3	4	49	49.0	50.0%	42.0
9/1/2024 7:00:00 AM	43.0	3	4	47	47.0	25.0%	41.3
9/1/2024 8:00:00 AM	41.0	2	2	41	0.0	0.0%	38.0
9/1/2024 9:00:00 AM	53.0	3	3	53	53.0	33.3%	41.0
9/1/2024 10:00:00 AM	43.0	15	18	46	46.0	5.6%	34.1
9/1/2024 11:00:00 AM	44.0	13	15	51	50.0	13.3%	39.6
9/1/2024 12:00:00 PM	44.0	51	60	47	46.3	6.7%	39.8
9/1/2024 1:00:00 PM	42.0	54	63	51	49.0	3.2%	38.6
9/1/2024 2:00:00 PM	44.0	54	63	48	47.5	6.3%	38.4
9/1/2024 3:00:00 PM	44.0	51	60	49	47.4	15.0%	39.3
9/1/2024 4:00:00 PM	44.0	65	76	50	48.1	11.8%	38.9
9/1/2024 5:00:00 PM	44.0	61	72	52	49.0	5.6%	40.0
9/1/2024 6:00:00 PM	43.0	77	91	56	49.0	9.9%	39.6
9/1/2024 7:00:00 PM	43.0	92	108	53	48.3	7.4%	38.7
9/1/2024 8:00:00 PM	42.0	78	92	55	48.8	6.5%	37.8
9/1/2024 9:00:00 PM	44.0	79	93	49	47.0	6.5%	38.8
9/1/2024 10:00:00 PM	43.0	52	61	50	47.5	6.6%	37.6
9/1/2024 11:00:00 PM	44.0	49	58	51	48.8	6.9%	38.5
9/2/2024 12:00:00 AM	43.0	31	37	46	46.0	5.4%	37.5
9/2/2024 1:00:00 AM	42.0	32	38	54	50.0	7.9%	38.0
9/2/2024 2:00:00 AM	45.0	14	17	47	46.7	17.6%	40.9
9/2/2024 3:00:00 AM	48.0	5	6	48	48.0	33.3%	41.3
9/2/2024 4:00:00 AM	45.0	3	3	45	0.0	0.0%	41.7
9/2/2024 5:00:00 AM	41.0	6	7	41	0.0	0.0%	31.7
9/2/2024 6:00:00 AM	38.0	5	6	47	47.0	16.7%	36.3
9/2/2024 7:00:00 AM	32.0	1	1	32	0.0	0.0%	32.0
9/2/2024 8:00:00 AM	41.0	2	2	41	0.0	0.0%	31.0
9/2/2024 9:00:00 AM	42.0	2	2	42	0.0	0.0%	38.0
9/2/2024 10:00:00 AM	37.0	6	7	46	46.0	14.3%	34.7
9/2/2024 11:00:00 AM	38.0	11	13	40	0.0	0.0%	34.6
9/2/2024 12:00:00 PM	42.0	34	40	54	50.0	7.5%	38.8

Incoming Summary
 Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

9/2/2024 1:00:00 PM	44.0	48	56	48	46.6	8.9%	39.4
9/2/2024 2:00:00 PM	42.0	69	81	49	47.0	4.9%	38.1
9/2/2024 3:00:00 PM	42.0	63	74	63	56.0	2.7%	38.1
9/2/2024 4:00:00 PM	44.0	60	70	49	47.0	5.7%	38.4
9/2/2024 5:00:00 PM	42.0	67	79	47	46.5	2.5%	37.5
9/2/2024 6:00:00 PM	44.0	67	79	55	49.1	8.9%	39.3
9/2/2024 7:00:00 PM	43.0	78	92	59	49.4	8.7%	38.2
9/2/2024 8:00:00 PM	44.0	59	69	50	47.0	7.2%	39.6
9/2/2024 9:00:00 PM	45.0	73	86	51	48.6	8.1%	39.9
9/2/2024 10:00:00 PM	45.0	74	87	50	47.2	11.5%	38.9
9/2/2024 11:00:00 PM	43.0	48	56	59	49.0	8.9%	38.1
9/3/2024 12:00:00 AM	43.0	46	54	47	47.0	1.9%	38.1
9/3/2024 1:00:00 AM	42.0	27	32	47	47.0	3.1%	37.9
9/3/2024 2:00:00 AM	39.0	9	11	41	0.0	0.0%	34.9
9/3/2024 3:00:00 AM	55.0	3	3	55	55.0	33.3%	44.0
9/3/2024 4:00:00 AM	46.0	3	3	46	46.0	33.3%	38.3
9/3/2024 5:00:00 AM	37.0	3	3	37	0.0	0.0%	33.3
9/3/2024 6:00:00 AM	39.0	2	2	39	0.0	0.0%	38.5
9/3/2024 7:00:00 AM	39.0	3	3	39	0.0	0.0%	31.3
9/3/2024 8:00:00 AM	32.0	4	5	40	0.0	0.0%	29.6
9/3/2024 9:00:00 AM	45.0	25	29	49	48.5	6.9%	39.6
9/3/2024 10:00:00 AM	43.0	45	53	47	46.5	3.8%	36.6
9/3/2024 11:00:00 AM	45.0	87	102	51	47.9	10.8%	40.0
9/3/2024 12:00:00 PM	44.0	72	85	53	48.6	9.3%	39.2
9/3/2024 1:00:00 PM	44.0	85	100	49	47.0	6.0%	37.9
9/3/2024 2:00:00 PM	42.0	31	36	46	46.0	5.6%	39.0

Incoming Summary

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/30/2024 12:00:00 AM	43.0	825	971	98	48.8	6.1%	38.6
8/31/2024 12:00:00 AM	44.0	1281	1507	62	48.3	8.2%	38.5
9/1/2024 12:00:00 AM	43.0	901	1060	64	48.2	6.0%	38.6
9/2/2024 12:00:00 AM	44.0	906	1066	56	48.1	7.8%	38.6
9/3/2024 12:00:00 AM	44.0	871	1025	63	48.2	7.1%	38.5
9/3/2024 1:59:59 PM	44.0	397	467	55	47.9	7.3%	38.5

Incoming Weekly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	46	50	31	46	40.5	44
1 - 2	*	*	*	*	20	22	37	20	29.5	43
2 - 3	*	*	*	*	9	9	6	9	7.5	46.5
3 - 4	*	*	*	*	4	1	5	4	3	40
4 - 5	*	*	*	*	1	1	7	1	4	31
5 - 6	*	*	*	*	2	3	4	2	3.5	52
6 - 7	*	*	*	*	1	1	4	1	2.5	38
7 - 8	*	*	*	*	5	2	2	5	2	39.7
8 - 9	*	*	*	*	14	2	3	14	2.5	44
9 - 10	*	*	*	*	42	15	18	42	16.5	43.5
10 - 11	*	*	*	*	84	24	15	84	19.5	45
11 - 12	*	*	*	*	83	51	60	83	55.5	44.5
12 - 13	*	*	*	*	102	51	63	102	57	43.6
13 - 14	*	*	*	28	91	69	63	59.5	66	42.7
14 - 15	*	*	*	93	100	101	60	96.5	80.5	41.95
15 - 16	*	*	*	109	96	88	76	102.5	82	43.3
16 - 17	*	*	*	102	97	81	72	99.5	76.5	42.7
17 - 18	*	*	*	102	119	85	91	110.5	88	42.6
18 - 19	*	*	*	99	139	80	108	119	94	41.25
19 - 20	*	*	*	109	118	87	92	113.5	89.5	42.6
20 - 21	*	*	*	103	145	71	93	124	82	44.4
21 - 22	*	*	*	89	87	63	61	88	62	42.3
22 - 23	*	*	*	69	56	52	58	62.5	55	43.4
23 - 24	*	*	*	68	46	51	37	57	44	43.6
Totals	0	0	0	971	1507	1060	1066			
% of Total	0%	0%	0%	21.09%	32.73%	23.02%	23.15%			

Incoming Weekly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	38	32	*	*	*	*	*	35	0	42
1 - 2	17	11	*	*	*	*	*	14	0	42
2 - 3	6	3	*	*	*	*	*	4.5	0	51.25
3 - 4	3	3	*	*	*	*	*	3	0	45.5
4 - 5	7	3	*	*	*	*	*	5	0	38.75
5 - 6	6	2	*	*	*	*	*	4	0	38.5
6 - 7	1	3	*	*	*	*	*	2	0	35.5
7 - 8	2	5	*	*	*	*	*	3.5	0	36.5
8 - 9	2	29	*	*	*	*	*	15.5	0	43.15
9 - 10	7	53	*	*	*	*	*	30	0	39.75
10 - 11	13	102	*	*	*	*	*	57.5	0	41.1
11 - 12	40	85	*	*	*	*	*	62.5	0	42.75
12 - 13	56	100	*	*	*	*	*	78	0	43.6
13 - 14	81	36	*	*	*	*	*	58.5	0	41.75
14 - 15	74	*	*	*	*	*	*	74	0	42
15 - 16	70	*	*	*	*	*	*	70	0	43.2
16 - 17	79	*	*	*	*	*	*	79	0	42
17 - 18	79	*	*	*	*	*	*	79	0	43.8
18 - 19	92	*	*	*	*	*	*	92	0	42.8
19 - 20	69	*	*	*	*	*	*	69	0	44
20 - 21	86	*	*	*	*	*	*	86	0	44.1
21 - 22	87	*	*	*	*	*	*	87	0	44.4
22 - 23	56	*	*	*	*	*	*	56	0	42.7
23 - 24	54	*	*	*	*	*	*	54	0	42.5
Totals	1025	467	0	0	0	0	0			
% of Total	68.7%	31.3%	0%	0%	0%	0%	0%			

Incoming Monthly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	46	50	*	46	50	44
1 - 2	*	*	*	*	20	22	*	20	22	43
2 - 3	*	*	*	*	9	9	*	9	9	46.5
3 - 4	*	*	*	*	4	1	*	4	1	40
4 - 5	*	*	*	*	1	1	*	1	1	31
5 - 6	*	*	*	*	2	3	*	2	3	52
6 - 7	*	*	*	*	1	1	*	1	1	38
7 - 8	*	*	*	*	5	2	*	5	2	39.7
8 - 9	*	*	*	*	14	2	*	14	2	44
9 - 10	*	*	*	*	42	15	*	42	15	43.5
10 - 11	*	*	*	*	84	24	*	84	24	45
11 - 12	*	*	*	*	83	51	*	83	51	44.5
12 - 13	*	*	*	*	102	51	*	102	51	43.6
13 - 14	*	*	*	28	91	69	*	59.5	69	42.7
14 - 15	*	*	*	93	100	101	*	96.5	101	41.95
15 - 16	*	*	*	109	96	88	*	102.5	88	43.3
16 - 17	*	*	*	102	97	81	*	99.5	81	42.7
17 - 18	*	*	*	102	119	85	*	110.5	85	42.6
18 - 19	*	*	*	99	139	80	*	119	80	41.25
19 - 20	*	*	*	109	118	87	*	113.5	87	42.6
20 - 21	*	*	*	103	145	71	*	124	71	44.4
21 - 22	*	*	*	89	87	63	*	88	63	42.3
22 - 23	*	*	*	69	56	52	*	62.5	52	43.4
23 - 24	*	*	*	68	46	51	*	57	51	43.6
Totals	0	0	0	971	1507	1060	0			
% of Total	0%	0%	0%	27.44%	42.59%	29.96%	0%			

Incoming Monthly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	38	32	*	*	*	*	31	35	31	42
1 - 2	17	11	*	*	*	*	37	14	37	42
2 - 3	6	3	*	*	*	*	6	4.5	6	51.25
3 - 4	3	3	*	*	*	*	5	3	5	45.5
4 - 5	7	3	*	*	*	*	7	5	7	38.75
5 - 6	6	2	*	*	*	*	4	4	4	38.5
6 - 7	1	3	*	*	*	*	4	2	4	35.5
7 - 8	2	5	*	*	*	*	2	3.5	2	36.5
8 - 9	2	29	*	*	*	*	3	15.5	3	43.15
9 - 10	7	53	*	*	*	*	18	30	18	39.75
10 - 11	13	102	*	*	*	*	15	57.5	15	41.1
11 - 12	40	85	*	*	*	*	60	62.5	60	42.75
12 - 13	56	100	*	*	*	*	63	78	63	43.6
13 - 14	81	36	*	*	*	*	63	58.5	63	41.75
14 - 15	74	*	*	*	*	*	60	74	60	42
15 - 16	70	*	*	*	*	*	76	70	76	43.2
16 - 17	79	*	*	*	*	*	72	79	72	42
17 - 18	79	*	*	*	*	*	91	79	91	43.8
18 - 19	92	*	*	*	*	*	108	92	108	42.8
19 - 20	69	*	*	*	*	*	92	69	92	44
20 - 21	86	*	*	*	*	*	93	86	93	44.1
21 - 22	87	*	*	*	*	*	61	87	61	44.4
22 - 23	56	*	*	*	*	*	58	56	58	42.7
23 - 24	54	*	*	*	*	*	37	54	37	42.5
Totals	1025	467	0	0	0	0	1066			
% of Total	40.07%	18.26%	0%	0%	0%	0%	41.67%			

Incoming Weekly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	39.17	37.64	38.65	39.17	38.02	44
1 - 2	*	*	*	*	38.55	39	37.57	38.55	38.1	43
2 - 3	*	*	*	*	38.33	40.67	30.33	38.33	36.53	46.5
3 - 4	*	*	*	*	40	36	38.6	40	38.17	40
4 - 5	*	*	*	*	31	29	33.29	31	32.75	31
5 - 6	*	*	*	*	51	36.67	42	51	39.71	52
6 - 7	*	*	*	*	38	37	41.25	38	40.4	38
7 - 8	*	*	*	*	39.2	42	38	39.2	40	39.7
8 - 9	*	*	*	*	40.71	43	41	40.71	41.8	44
9 - 10	*	*	*	*	39.05	38.87	34.06	39.05	36.24	43.5
10 - 11	*	*	*	*	40.42	36.83	39.6	40.42	37.9	45
11 - 12	*	*	*	*	38.75	38.25	39.77	38.75	39.07	44.5
12 - 13	*	*	*	*	38.13	39.1	38.63	38.13	38.84	43.6
13 - 14	*	*	*	39.11	37.95	39.41	38.38	38.22	38.92	42.7
14 - 15	*	*	*	37.2	38.14	37.87	39.27	37.69	38.39	41.95
15 - 16	*	*	*	37.78	38.58	38.17	38.88	38.16	38.5	43.3
16 - 17	*	*	*	38.7	37.92	38.58	39.96	38.32	39.23	42.7
17 - 18	*	*	*	37.51	37.07	39.28	39.62	37.27	39.45	42.6
18 - 19	*	*	*	37.71	37.51	39.53	38.71	37.59	39.06	41.25
19 - 20	*	*	*	39.49	38.72	38.41	37.75	39.09	38.07	42.6
20 - 21	*	*	*	40.03	39.09	39.06	38.84	39.48	38.93	44.4
21 - 22	*	*	*	39.91	37	38.44	37.62	38.47	38.04	42.3
22 - 23	*	*	*	38.8	39.48	37.94	38.53	39.1	38.25	43.4
23 - 24	*	*	*	38.38	40.61	38.84	37.51	39.28	38.28	43.6
Totals	0	0	0	38.6	38.5	38.6	38.6			
% of Total	0%	0%	0%	25.02%	24.95%	25.02%	25.02%			

Incoming Weekly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	38.03	37.94	*	*	*	*	*	37.99	*	42
1 - 2	40.88	34.91	*	*	*	*	*	38.54	*	42
2 - 3	41.33	44	*	*	*	*	*	42.22	*	51.25
3 - 4	41.67	38.33	*	*	*	*	*	40	*	45.5
4 - 5	31.71	33.33	*	*	*	*	*	32.2	*	38.75
5 - 6	36.33	38.5	*	*	*	*	*	36.87	*	38.5
6 - 7	32	31.33	*	*	*	*	*	31.5	*	35.5
7 - 8	31	29.6	*	*	*	*	*	30	*	36.5
8 - 9	38	39.62	*	*	*	*	*	39.52	*	43.15
9 - 10	34.71	36.62	*	*	*	*	*	36.4	*	39.75
10 - 11	34.62	40.03	*	*	*	*	*	39.42	*	41.1
11 - 12	38.78	39.2	*	*	*	*	*	39.06	*	42.75
12 - 13	39.39	37.94	*	*	*	*	*	38.46	*	43.6
13 - 14	38.12	39	*	*	*	*	*	38.39	*	41.75
14 - 15	38.11	*	*	*	*	*	*	38.11	*	42
15 - 16	38.39	*	*	*	*	*	*	38.39	*	43.2
16 - 17	37.53	*	*	*	*	*	*	37.53	*	42
17 - 18	39.32	*	*	*	*	*	*	39.32	*	43.8
18 - 19	38.2	*	*	*	*	*	*	38.2	*	42.8
19 - 20	39.64	*	*	*	*	*	*	39.64	*	44
20 - 21	39.88	*	*	*	*	*	*	39.88	*	44.1
21 - 22	38.93	*	*	*	*	*	*	38.93	*	44.4
22 - 23	38.12	*	*	*	*	*	*	38.12	*	42.7
23 - 24	38.15	*	*	*	*	*	*	38.15	*	42.5
Totals	38.5	38.5	0	0	0	0	0			
% of Total	50%	50%	0%	0%	0%	0%	0%			

Incoming Monthly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	39.17	37.64	*	39.17	37.64	44
1 - 2	*	*	*	*	38.55	39	*	38.55	39	43
2 - 3	*	*	*	*	38.33	40.67	*	38.33	40.67	46.5
3 - 4	*	*	*	*	40	36	*	40	36	40
4 - 5	*	*	*	*	31	29	*	31	29	31
5 - 6	*	*	*	*	51	36.67	*	51	36.67	52
6 - 7	*	*	*	*	38	37	*	38	37	38
7 - 8	*	*	*	*	39.2	42	*	39.2	42	39.7
8 - 9	*	*	*	*	40.71	43	*	40.71	43	44
9 - 10	*	*	*	*	39.05	38.87	*	39.05	38.87	43.5
10 - 11	*	*	*	*	40.42	36.83	*	40.42	36.83	45
11 - 12	*	*	*	*	38.75	38.25	*	38.75	38.25	44.5
12 - 13	*	*	*	*	38.13	39.1	*	38.13	39.1	43.6
13 - 14	*	*	*	39.11	37.95	39.41	*	38.22	39.41	42.7
14 - 15	*	*	*	37.2	38.14	37.87	*	37.69	37.87	41.95
15 - 16	*	*	*	37.78	38.58	38.17	*	38.16	38.17	43.3
16 - 17	*	*	*	38.7	37.92	38.58	*	38.32	38.58	42.7
17 - 18	*	*	*	37.51	37.07	39.28	*	37.27	39.28	42.6
18 - 19	*	*	*	37.71	37.51	39.53	*	37.59	39.53	41.25
19 - 20	*	*	*	39.49	38.72	38.41	*	39.09	38.41	42.6
20 - 21	*	*	*	40.03	39.09	39.06	*	39.48	39.06	44.4
21 - 22	*	*	*	39.91	37	38.44	*	38.47	38.44	42.3
22 - 23	*	*	*	38.8	39.48	37.94	*	39.1	37.94	43.4
23 - 24	*	*	*	38.38	40.61	38.84	*	39.28	38.84	43.6
Totals	0	0	0	38.6	38.5	38.6	0			
% of Total	0%	0%	0%	33.36%	33.28%	33.36%	0%			

Incoming Monthly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	38.03	37.94	*	*	*	*	38.65	37.99	38.65	42
1 - 2	40.88	34.91	*	*	*	*	37.57	38.54	37.57	42
2 - 3	41.33	44	*	*	*	*	30.33	42.22	30.33	51.25
3 - 4	41.67	38.33	*	*	*	*	38.6	40	38.6	45.5
4 - 5	31.71	33.33	*	*	*	*	33.29	32.2	33.29	38.75
5 - 6	36.33	38.5	*	*	*	*	42	36.87	42	38.5
6 - 7	32	31.33	*	*	*	*	41.25	31.5	41.25	35.5
7 - 8	31	29.6	*	*	*	*	38	30	38	36.5
8 - 9	38	39.62	*	*	*	*	41	39.52	41	43.15
9 - 10	34.71	36.62	*	*	*	*	34.06	36.4	34.06	39.75
10 - 11	34.62	40.03	*	*	*	*	39.6	39.42	39.6	41.1
11 - 12	38.78	39.2	*	*	*	*	39.77	39.06	39.77	42.75
12 - 13	39.39	37.94	*	*	*	*	38.63	38.46	38.63	43.6
13 - 14	38.12	39	*	*	*	*	38.38	38.39	38.38	41.75
14 - 15	38.11	*	*	*	*	*	39.27	38.11	39.27	42
15 - 16	38.39	*	*	*	*	*	38.88	38.39	38.88	43.2
16 - 17	37.53	*	*	*	*	*	39.96	37.53	39.96	42
17 - 18	39.32	*	*	*	*	*	39.62	39.32	39.62	43.8
18 - 19	38.2	*	*	*	*	*	38.71	38.2	38.71	42.8
19 - 20	39.64	*	*	*	*	*	37.75	39.64	37.75	44
20 - 21	39.88	*	*	*	*	*	38.84	39.88	38.84	44.1
21 - 22	38.93	*	*	*	*	*	37.62	38.93	37.62	44.4
22 - 23	38.12	*	*	*	*	*	38.53	38.12	38.53	42.7
23 - 24	38.15	*	*	*	*	*	37.51	38.15	37.51	42.5
Totals	38.5	38.5	0	0	0	0	38.6			
% of Total	33.3%	33.3%	0%	0%	0%	0%	33.39%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	44	41.3	42.3	44	41.8	44
1 - 2	*	*	*	*	43	44	40	43	42	43
2 - 3	*	*	*	*	46.5	44	39.5	46.5	41.75	46.5
3 - 4	*	*	*	*	40	36	41	40	38.5	40
4 - 5	*	*	*	*	31	29	37	31	33	31
5 - 6	*	*	*	*	52	42	48.5	52	45.25	52
6 - 7	*	*	*	*	38	37	43	38	40	38
7 - 8	*	*	*	*	39.7	43	41	39.7	42	39.7
8 - 9	*	*	*	*	44	51	53	44	52	44
9 - 10	*	*	*	*	43.5	43	42.5	43.5	42.75	43.5
10 - 11	*	*	*	*	45	41	44	45	42.5	45
11 - 12	*	*	*	*	44.5	43	43.5	44.5	43.25	44.5
12 - 13	*	*	*	*	43.6	42.7	41.8	43.6	42.25	43.6
13 - 14	*	*	*	43	42.4	43.7	43.2	42.7	43.45	42.7
14 - 15	*	*	*	41.9	42	43	44	41.95	43.5	41.95
15 - 16	*	*	*	42.8	43.8	42.8	43.8	43.3	43.3	43.3
16 - 17	*	*	*	42.7	42.7	42.5	43.2	42.7	42.85	42.7
17 - 18	*	*	*	42.7	42.5	43	43	42.6	43	42.6
18 - 19	*	*	*	40.9	41.6	43.5	43	41.25	43.25	41.25
19 - 20	*	*	*	42.7	42.5	42.3	41.6	42.6	41.95	42.6
20 - 21	*	*	*	44.7	44.1	43.3	43.3	44.4	43.3	44.4
21 - 22	*	*	*	43.3	41.3	43	42.6	42.3	42.8	42.3
22 - 23	*	*	*	43.5	43.3	41.2	43.7	43.4	42.45	43.4
23 - 24	*	*	*	41	46.2	43.7	42.3	43.6	43	43.6
Totals	0	0	0	469.2	1027.2	1009	1030.8			
% of Total	0%	0%	0%	13.27%	29.05%	28.53%	29.15%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	42	42	*	*	*	*	*	42	0	42
1 - 2	45	39	*	*	*	*	*	42	0	42
2 - 3	47.5	55	*	*	*	*	*	51.25	0	51.25
3 - 4	45	46	*	*	*	*	*	45.5	0	45.5
4 - 5	40.5	37	*	*	*	*	*	38.75	0	38.75
5 - 6	38	39	*	*	*	*	*	38.5	0	38.5
6 - 7	32	39	*	*	*	*	*	35.5	0	35.5
7 - 8	41	32	*	*	*	*	*	36.5	0	36.5
8 - 9	42	44.3	*	*	*	*	*	43.15	0	43.15
9 - 10	37	42.5	*	*	*	*	*	39.75	0	39.75
10 - 11	38	44.2	*	*	*	*	*	41.1	0	41.1
11 - 12	42	43.5	*	*	*	*	*	42.75	0	42.75
12 - 13	44	43.2	*	*	*	*	*	43.6	0	43.6
13 - 14	41.5	42	*	*	*	*	*	41.75	0	41.75
14 - 15	42	*	*	*	*	*	*	42	0	42
15 - 16	43.2	*	*	*	*	*	*	43.2	0	43.2
16 - 17	42	*	*	*	*	*	*	42	0	42
17 - 18	43.8	*	*	*	*	*	*	43.8	0	43.8
18 - 19	42.8	*	*	*	*	*	*	42.8	0	42.8
19 - 20	44	*	*	*	*	*	*	44	0	44
20 - 21	44.1	*	*	*	*	*	*	44.1	0	44.1
21 - 22	44.4	*	*	*	*	*	*	44.4	0	44.4
22 - 23	42.7	*	*	*	*	*	*	42.7	0	42.7
23 - 24	42.5	*	*	*	*	*	*	42.5	0	42.5
Totals	1007	588.7	0	0	0	0	0			
% of Total	63.11%	36.89%	0%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	44	41.3	*	44	41.3	44
1 - 2	*	*	*	*	43	44	*	43	44	43
2 - 3	*	*	*	*	46.5	44	*	46.5	44	46.5
3 - 4	*	*	*	*	40	36	*	40	36	40
4 - 5	*	*	*	*	31	29	*	31	29	31
5 - 6	*	*	*	*	52	42	*	52	42	52
6 - 7	*	*	*	*	38	37	*	38	37	38
7 - 8	*	*	*	*	39.7	43	*	39.7	43	39.7
8 - 9	*	*	*	*	44	51	*	44	51	44
9 - 10	*	*	*	*	43.5	43	*	43.5	43	43.5
10 - 11	*	*	*	*	45	41	*	45	41	45
11 - 12	*	*	*	*	44.5	43	*	44.5	43	44.5
12 - 13	*	*	*	*	43.6	42.7	*	43.6	42.7	43.6
13 - 14	*	*	*	43	42.4	43.7	*	42.7	43.7	42.7
14 - 15	*	*	*	41.9	42	43	*	41.95	43	41.95
15 - 16	*	*	*	42.8	43.8	42.8	*	43.3	42.8	43.3
16 - 17	*	*	*	42.7	42.7	42.5	*	42.7	42.5	42.7
17 - 18	*	*	*	42.7	42.5	43	*	42.6	43	42.6
18 - 19	*	*	*	40.9	41.6	43.5	*	41.25	43.5	41.25
19 - 20	*	*	*	42.7	42.5	42.3	*	42.6	42.3	42.6
20 - 21	*	*	*	44.7	44.1	43.3	*	44.4	43.3	44.4
21 - 22	*	*	*	43.3	41.3	43	*	42.3	43	42.3
22 - 23	*	*	*	43.5	43.3	41.2	*	43.4	41.2	43.4
23 - 24	*	*	*	41	46.2	43.7	*	43.6	43.7	43.6

Incoming Monthly EightyFifthSpeeds

Birchwood Loop northwest norh of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Sep 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	42	42	*	*	*	*	42.3	42	42.3	42
1 - 2	45	39	*	*	*	*	40	42	40	42
2 - 3	47.5	55	*	*	*	*	39.5	51.25	39.5	51.25
3 - 4	45	46	*	*	*	*	41	45.5	41	45.5
4 - 5	40.5	37	*	*	*	*	37	38.75	37	38.75
5 - 6	38	39	*	*	*	*	48.5	38.5	48.5	38.5
6 - 7	32	39	*	*	*	*	43	35.5	43	35.5
7 - 8	41	32	*	*	*	*	41	36.5	41	36.5
8 - 9	42	44.3	*	*	*	*	53	43.15	53	43.15
9 - 10	37	42.5	*	*	*	*	42.5	39.75	42.5	39.75
10 - 11	38	44.2	*	*	*	*	44	41.1	44	41.1
11 - 12	42	43.5	*	*	*	*	43.5	42.75	43.5	42.75
12 - 13	44	43.2	*	*	*	*	41.8	43.6	41.8	43.6
13 - 14	41.5	42	*	*	*	*	43.2	41.75	43.2	41.75
14 - 15	42	*	*	*	*	*	44	42	44	42
15 - 16	43.2	*	*	*	*	*	43.8	43.2	43.8	43.2
16 - 17	42	*	*	*	*	*	43.2	42	43.2	42
17 - 18	43.8	*	*	*	*	*	43	43.8	43	43.8
18 - 19	42.8	*	*	*	*	*	43	42.8	43	42.8
19 - 20	44	*	*	*	*	*	41.6	44	41.6	44
20 - 21	44.1	*	*	*	*	*	43.3	44.1	43.3	44.1
21 - 22	44.4	*	*	*	*	*	42.6	44.4	42.6	44.4
22 - 23	42.7	*	*	*	*	*	43.7	42.7	43.7	42.7
23 - 24	42.5	*	*	*	*	*	42.3	42.5	42.3	42.5

Summary of Violators

Birchwood Loop northwest north of 3bears 8-29 to 9-4 from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM
time error -3hrs

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	197	38.3	11	49.0
01:00:00	107	38.3	8	47.5
02:00:00	33	38.6	6	48.7
03:00:00	16	39.3	1	46.0
04:00:00	19	32.4	0	0.0
05:00:00	17	39.7	5	49.4
06:00:00	10	36.6	1	47.0
07:00:00	16	35.4	0	0.0
08:00:00	50	40.1	5	49.6
09:00:00	135	37.2	10	47.7
10:00:00	238	39.5	27	49.2
11:00:00	319	39.0	29	47.6
12:00:00	372	38.5	25	48.7
13:00:00	368	38.5	16	47.4
14:00:00	428	38.0	29	48.0
15:00:00	439	38.3	30	49.0
16:00:00	431	38.5	17	47.9
17:00:00	476	38.4	31	48.5
18:00:00	518	38.2	37	48.9
19:00:00	475	38.8	32	48.6
20:00:00	498	39.4	44	47.8
21:00:00	387	38.4	32	47.9
22:00:00	291	38.6	20	48.4
23:00:00	256	38.7	20	47.2

Incoming Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	13:00	0	1	0	0	2	12	11	2	0	0	0	0	0	0	0	0	0	0	0	28	39.1	43	36 to 46	89.3	1	3.6	1	24	3
8/29/2024	14:00	2	0	1	3	15	42	24	5	1	0	0	0	0	0	0	0	0	0	93	37.2	41.9	34 to 44	76.3	4	4.3	1	80	12	
8/29/2024	15:00	0	0	3	7	22	34	35	7	0	0	0	0	0	0	0	0	0	1	109	37.8	42.8	35 to 45	67.9	3	2.8	0	97	12	
8/29/2024	16:00	0	0	0	1	17	42	33	8	1	0	0	0	0	0	0	0	0	0	102	38.7	42.7	33 to 43	81.4	4	3.9	0	90	12	
8/29/2024	17:00	0	2	0	6	16	39	32	7	0	0	0	0	0	0	0	0	0	0	102	37.5	42.7	34 to 44	74.5	5	4.9	0	94	8	
8/29/2024	18:00	0	1	1	2	16	47	24	8	0	0	0	0	0	0	0	0	0	0	99	37.7	40.9	31 to 41	80.8	5	5.1	0	89	10	
8/29/2024	19:00	0	0	0	2	7	47	42	8	3	0	0	0	0	0	0	0	0	0	109	39.5	42.7	34 to 44	84.4	8	7.3	0	97	12	
8/29/2024	20:00	0	1	1	0	13	29	40	16	3	0	0	0	0	0	0	0	0	0	103	40	44.7	35 to 45	72.8	13	12.6	1	97	5	
8/29/2024	21:00	0	0	0	1	8	34	37	5	4	0	0	0	0	0	0	0	0	0	89	39.9	43.3	34 to 44	82.0	8	9.0	0	82	7	
8/29/2024	22:00	0	0	0	3	8	31	19	8	0	0	0	0	0	0	0	0	0	0	69	38.8	43.5	35 to 45	76.8	5	7.2	0	68	1	
8/29/2024	23:00	0	0	0	2	4	38	20	4	0	0	0	0	0	0	0	0	0	0	68	38.4	41	34 to 44	86.8	3	4.4	0	66	2	
24 Hr Summary		2	5	6	27	128	395	317	78	12	0	0	0	0	0	0	0	0	1	971	38.6	43	34 to 44	77.1	59	6.1	3	884	84	

Incoming Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/30/2024	00:00	0	0	1	1	8	12	17	6	0	1	0	0	0	0	0	0	0	0	46	39.2	44	34 to 44	69.6	6	13.0	0	46	0
8/30/2024	01:00	0	0	0	1	0	12	6	1	0	0	0	0	0	0	0	0	0	0	20	38.5	43	34 to 44	90.0	1	5.0	0	20	0
8/30/2024	02:00	0	0	0	2	0	3	2	2	0	0	0	0	0	0	0	0	0	0	9	38.3	46.5	37 to 47	66.7	2	22.2	0	9	0
8/30/2024	03:00	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4	40	40	32 to 42	100.0	*	*	0	4	0
8/30/2024	04:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	31	31	21 to 31	100.0	*	*	0	1	0
8/30/2024	05:00	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	51	52	42 to 52	100.0	2	100.0	0	2	0
8/30/2024	06:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	38	38	28 to 38	100.0	*	*	0	1	0
8/30/2024	07:00	0	0	0	0	0	2	3	0	0	0	0	0	0	0	0	0	0	0	5	39.2	39.7	30 to 40	100.0	*	*	0	5	0
8/30/2024	08:00	0	0	0	0	1	4	7	2	0	0	0	0	0	0	0	0	0	0	14	40.7	44	35 to 45	85.7	1	7.1	0	14	0
8/30/2024	09:00	0	0	0	1	7	16	13	4	1	0	0	0	0	0	0	0	0	0	42	39	43.5	32 to 42	78.6	4	9.5	0	41	1
8/30/2024	10:00	0	0	0	0	11	28	26	13	5	1	0	0	0	0	0	0	0	0	84	40.4	45	35 to 45	71.4	13	15.5	0	83	1
8/30/2024	11:00	0	0	1	2	14	28	25	13	0	0	0	0	0	0	0	0	0	0	83	38.7	44.5	34 to 44	68.7	11	13.3	0	76	7
8/30/2024	12:00	1	0	4	6	7	47	24	9	3	0	1	0	0	0	0	0	0	0	102	38.1	43.6	35 to 45	74.5	8	7.8	0	89	13
8/30/2024	13:00	0	0	2	3	16	31	33	5	1	0	0	0	0	0	0	0	0	0	91	37.9	42.4	33 to 43	78.0	2	2.2	1	81	9
8/30/2024	14:00	0	0	0	2	20	42	30	5	1	0	0	0	0	0	0	0	0	0	100	38.1	42	32 to 42	81.0	4	4.0	0	90	10
8/30/2024	15:00	2	0	1	2	11	39	29	11	1	0	0	0	0	0	0	0	0	0	96	38.6	43.8	34 to 44	76.0	10	10.4	4	83	9
8/30/2024	16:00	0	0	2	4	15	38	32	6	0	0	0	0	0	0	0	0	0	0	97	37.9	42.7	34 to 44	78.4	5	5.2	0	86	11
8/30/2024	17:00	0	4	3	6	13	53	33	5	2	0	0	0	0	0	0	0	0	0	119	37.1	42.5	34 to 44	76.5	6	5.0	1	109	9
8/30/2024	18:00	1	0	1	4	19	76	30	6	2	0	0	0	0	0	0	0	0	0	139	37.5	41.6	33 to 43	78.4	7	5.0	0	125	14
8/30/2024	19:00	0	0	2	1	13	56	35	9	1	1	0	0	0	0	0	0	0	0	118	38.7	42.5	34 to 44	79.7	8	6.8	1	107	10
8/30/2024	20:00	0	0	1	3	19	59	40	20	2	1	0	0	0	0	0	0	0	0	145	39.1	44.1	34 to 44	73.8	15	10.3	1	132	12
8/30/2024	21:00	1	0	1	2	15	46	17	4	1	0	0	0	0	0	0	0	0	0	87	37	41.3	32 to 42	83.9	5	5.7	0	83	4
8/30/2024	22:00	0	0	0	1	6	22	21	4	2	0	0	0	0	0	0	0	0	0	56	39.5	43.3	34 to 44	80.4	5	8.9	0	56	0
8/30/2024	23:00	0	0	0	0	5	14	19	7	1	0	0	0	0	0	0	0	0	0	46	40.6	46.2	34 to 44	78.3	8	17.4	0	44	2
24 Hr Summary		5	4	19	41	201	631	444	132	25	4	1	0	0	0	0	0	0	0	1507	38.5	44	34 to 44	76.0	123	8.2	8	1387	112

Incoming Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/31/2024	00:00	0	0	0	1	9	25	13	2	0	0	0	0	0	0	0	0	0	0	50	37.6	41.3	32 to 42	82.0	*	*	0	50	0
8/31/2024	01:00	0	0	0	0	5	8	6	3	0	0	0	0	0	0	0	0	0	0	22	39	44	35 to 45	77.3	*	*	0	22	0
8/31/2024	02:00	0	0	0	0	0	3	5	1	0	0	0	0	0	0	0	0	0	0	9	40.7	44	34 to 44	88.9	1	11.1	0	8	1
8/31/2024	03:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	36	36	26 to 36	100.0	*	*	0	1	0
8/31/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0
8/31/2024	05:00	0	0	0	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	3	36.7	42	32 to 42	100.0	*	*	0	3	0
8/31/2024	06:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	37	37	27 to 37	100.0	*	*	0	1	0
8/31/2024	07:00	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	42	43	33 to 43	100.0	*	*	0	2	0
8/31/2024	08:00	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2	43	51	25 to 35	50.0	1	50.0	0	1	1
8/31/2024	09:00	0	0	0	0	3	5	5	1	1	0	0	0	0	0	0	0	0	0	15	38.9	43	33 to 43	80.0	2	13.3	0	14	1
8/31/2024	10:00	0	1	0	2	3	9	8	0	1	0	0	0	0	0	0	0	0	0	24	36.8	41	33 to 43	70.8	1	4.2	0	24	0
8/31/2024	11:00	0	0	0	1	8	26	11	5	0	0	0	0	0	0	0	0	0	0	51	38.3	43	33 to 43	78.4	3	5.9	0	49	2
8/31/2024	12:00	0	0	0	1	6	20	20	2	2	0	0	0	0	0	0	0	0	0	51	39.1	42.7	32 to 42	78.4	4	7.8	0	49	2
8/31/2024	13:00	0	0	0	0	10	23	27	8	1	0	0	0	0	0	0	0	0	0	69	39.4	43.7	35 to 45	81.2	3	4.3	0	67	2
8/31/2024	14:00	0	2	1	2	18	36	30	12	0	0	0	0	0	0	0	0	0	0	101	37.9	43	33 to 43	70.3	10	9.9	0	97	4
8/31/2024	15:00	0	0	0	3	11	44	23	7	0	0	0	0	0	0	0	0	0	0	88	38.2	42.8	34 to 44	80.7	4	4.5	0	85	3
8/31/2024	16:00	0	0	0	1	17	25	34	4	0	0	0	0	0	0	0	0	0	0	81	38.6	42.5	32 to 42	81.5	2	2.5	0	79	2
8/31/2024	17:00	0	0	1	1	9	34	32	6	2	0	0	0	0	0	0	0	0	0	85	39.3	43	34 to 44	83.5	4	4.7	0	82	3
8/31/2024	18:00	0	0	1	1	13	23	32	8	0	0	2	0	0	0	0	0	0	0	80	39.5	43.5	32 to 42	73.8	9	11.3	0	78	2
8/31/2024	19:00	0	0	0	2	11	40	27	6	1	0	0	0	0	0	0	0	0	0	87	38.4	42.3	34 to 44	81.6	5	5.7	0	84	3
8/31/2024	20:00	0	0	0	0	10	29	25	7	0	0	0	0	0	0	0	0	0	0	71	39.1	43.3	34 to 44	83.1	3	4.2	1	67	3
8/31/2024	21:00	0	0	2	2	5	29	17	7	1	0	0	0	0	0	0	0	0	0	63	38.4	43	33 to 43	77.8	5	7.9	0	61	2
8/31/2024	22:00	0	0	0	0	10	24	16	2	0	0	0	0	0	0	0	0	0	0	52	37.9	41.2	32 to 42	86.5	1	1.9	0	52	0
8/31/2024	23:00	0	0	0	2	8	16	18	7	0	0	0	0	0	0	0	0	0	0	51	38.8	43.7	33 to 43	72.5	6	11.8	0	49	2
24 Hr Summary		0	3	5	20	158	422	352	88	10	0	2	0	0	0	0	0	0	0	1060	38.6	43	34 to 44	77.9	64	6.0	1	1026	33

Incoming Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/1/2024	00:00	0	0	0	0	7	11	10	3	0	0	0	0	0	0	0	0	0	0	31	38.6	42.3	33 to 43	83.9	1	3.2	0	31	0
9/1/2024	01:00	0	0	0	3	6	15	9	3	1	0	0	0	0	0	0	0	0	0	37	37.6	40	30 to 40	75.7	4	10.8	0	35	2
9/1/2024	02:00	1	0	0	2	0	1	2	0	0	0	0	0	0	0	0	0	0	0	6	30.3	39.5	25 to 35	50.0	*	*	0	6	0
9/1/2024	03:00	0	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5	38.6	41	33 to 43	100.0	*	*	0	5	0
9/1/2024	04:00	0	0	0	2	2	2	1	0	0	0	0	0	0	0	0	0	0	0	7	33.3	37	27 to 37	85.7	*	*	0	7	0
9/1/2024	05:00	0	0	0	0	1	1	0	2	0	0	0	0	0	0	0	0	0	0	4	42	48.5	26 to 36	50.0	2	50.0	0	4	0
9/1/2024	06:00	0	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	4	41.3	43	33 to 43	75.0	1	25.0	0	4	0
9/1/2024	07:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	38	41	31 to 41	100.0	*	*	0	2	0
9/1/2024	08:00	0	0	0	0	1	1	0	0	1	0	0	0	0	0	0	0	0	0	3	41	53	26 to 36	66.7	1	33.3	0	3	0
9/1/2024	09:00	0	1	0	2	9	2	3	1	0	0	0	0	0	0	0	0	0	0	18	34.1	42.5	24 to 34	61.1	1	5.6	0	18	0
9/1/2024	10:00	0	0	0	0	3	4	6	1	1	0	0	0	0	0	0	0	0	0	15	39.6	44	34 to 44	66.7	2	13.3	0	14	1
9/1/2024	11:00	0	0	0	1	7	14	32	6	0	0	0	0	0	0	0	0	0	0	60	39.8	43.5	36 to 46	81.7	4	6.7	0	59	1
9/1/2024	12:00	0	0	0	1	7	26	26	2	1	0	0	0	0	0	0	0	0	0	63	38.6	41.8	34 to 44	85.7	2	3.2	0	63	0
9/1/2024	13:00	0	0	1	1	10	26	20	5	0	0	0	0	0	0	0	0	0	0	63	38.4	43.2	34 to 44	77.8	4	6.3	0	62	1
9/1/2024	14:00	0	0	1	0	7	26	17	9	0	0	0	0	0	0	0	0	0	0	60	39.3	44	34 to 44	75.0	9	15.0	0	59	1
9/1/2024	15:00	1	0	0	2	12	24	27	8	2	0	0	0	0	0	0	0	0	0	76	38.9	43.8	33 to 43	72.4	9	11.8	0	74	2
9/1/2024	16:00	1	0	0	1	2	25	36	5	2	0	0	0	0	0	0	0	0	0	72	40	43.2	35 to 45	88.9	4	5.6	0	70	2
9/1/2024	17:00	0	0	0	0	8	41	31	8	2	1	0	0	0	0	0	0	0	0	91	39.6	43	34 to 44	81.3	9	9.9	0	88	3
9/1/2024	18:00	0	0	0	5	16	40	35	10	2	0	0	0	0	0	0	0	0	0	108	38.7	43	33 to 43	73.1	8	7.4	0	101	7
9/1/2024	19:00	0	0	0	5	18	33	29	5	1	1	0	0	0	0	0	0	0	0	92	37.8	41.6	32 to 42	77.2	6	6.5	0	92	0
9/1/2024	20:00	1	0	0	3	9	34	37	9	0	0	0	0	0	0	0	0	0	0	93	38.8	43.3	35 to 45	79.6	6	6.5	0	88	5
9/1/2024	21:00	0	1	1	2	11	25	15	5	1	0	0	0	0	0	0	0	0	0	61	37.6	42.6	33 to 43	73.8	4	6.6	0	59	2
9/1/2024	22:00	0	0	0	1	9	29	11	6	2	0	0	0	0	0	0	0	0	0	58	38.5	43.7	34 to 44	75.9	4	6.9	0	57	1
9/1/2024	23:00	1	0	1	1	4	13	14	3	0	0	0	0	0	0	0	0	0	0	37	37.5	42.3	34 to 44	75.7	2	5.4	0	36	1
24 Hr Summary		5	2	4	32	150	397	366	92	16	2	0	0	0	0	0	0	0	0	1066	38.6	44	34 to 44	75.2	83	7.8	0	1037	29

Incoming Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/2/2024	00:00	0	0	0	1	8	15	10	3	1	0	0	0	0	0	0	0	0	0	38	38	42	31 to 41	78.9	3	7.9	0	37	1
9/2/2024	01:00	0	0	0	0	0	8	4	5	0	0	0	0	0	0	0	0	0	0	17	40.9	45	36 to 46	88.2	3	17.6	0	17	0
9/2/2024	02:00	0	0	0	0	0	3	1	2	0	0	0	0	0	0	0	0	0	0	6	41.3	47.5	31 to 41	66.7	2	33.3	0	6	0
9/2/2024	03:00	0	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	3	41.7	45	35 to 45	100.0	*	*	0	3	0
9/2/2024	04:00	0	0	2	1	1	1	2	0	0	0	0	0	0	0	0	0	0	0	7	31.7	40.5	31 to 41	57.1	*	*	0	7	0
9/2/2024	05:00	0	0	0	0	3	2	0	1	0	0	0	0	0	0	0	0	0	0	6	36.3	38	28 to 38	83.3	1	16.7	0	5	1
9/2/2024	06:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	32	32	22 to 32	100.0	*	*	0	1	0
9/2/2024	07:00	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	2	31	41	11 to 21	50.0	*	*	0	2	0
9/2/2024	08:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	38	42	32 to 42	100.0	*	*	0	2	0
9/2/2024	09:00	0	0	0	2	2	2	0	1	0	0	0	0	0	0	0	0	0	0	7	34.7	37	27 to 37	85.7	1	14.3	0	7	0
9/2/2024	10:00	0	0	0	0	5	6	2	0	0	0	0	0	0	0	0	0	0	0	13	34.6	38	30 to 40	100.0	*	*	0	11	2
9/2/2024	11:00	0	0	1	0	5	17	12	3	2	0	0	0	0	0	0	0	0	0	40	38.8	42	32 to 42	77.5	3	7.5	0	38	2
9/2/2024	12:00	0	0	0	0	7	22	19	8	0	0	0	0	0	0	0	0	0	0	56	39.4	44	35 to 45	78.6	5	8.9	0	55	1
9/2/2024	13:00	0	0	0	0	16	38	23	4	0	0	0	0	0	0	0	0	0	0	81	38.1	41.5	31 to 41	84.0	4	4.9	0	75	6
9/2/2024	14:00	0	1	0	2	8	36	24	2	0	0	1	0	0	0	0	0	0	0	74	38.1	42	34 to 44	82.4	2	2.7	0	70	4
9/2/2024	15:00	0	0	0	3	9	31	22	5	0	0	0	0	0	0	0	0	0	0	70	38.4	43.2	34 to 44	78.6	4	5.7	0	67	3
9/2/2024	16:00	1	0	0	5	17	26	25	5	0	0	0	0	0	0	0	0	0	0	79	37.5	42	33 to 43	77.2	2	2.5	2	71	6
9/2/2024	17:00	0	0	0	0	9	35	24	8	2	1	0	0	0	0	0	0	0	0	79	39.3	43.8	35 to 45	79.7	7	8.9	0	76	3
9/2/2024	18:00	0	1	1	2	18	33	28	6	2	1	0	0	0	0	0	0	0	0	92	38.2	42.8	34 to 44	75.0	8	8.7	0	90	2
9/2/2024	19:00	1	0	0	0	7	29	22	9	1	0	0	0	0	0	0	0	0	0	69	39.6	44	36 to 46	84.1	5	7.2	0	68	1
9/2/2024	20:00	0	0	1	1	8	29	33	11	3	0	0	0	0	0	0	0	0	0	86	39.9	44.1	35 to 45	80.2	7	8.1	0	83	3
9/2/2024	21:00	0	0	0	2	14	33	23	14	1	0	0	0	0	0	0	0	0	0	87	38.9	44.4	32 to 42	72.4	10	11.5	0	86	1
9/2/2024	22:00	0	0	0	1	14	20	15	5	0	1	0	0	0	0	0	0	0	0	56	38.1	42.7	32 to 42	75.0	5	8.9	0	54	2
9/2/2024	23:00	0	0	0	1	10	22	17	4	0	0	0	0	0	0	0	0	0	0	54	38.1	42.5	35 to 45	77.8	1	1.9	0	52	2
24 Hr Summary		2	2	6	21	163	409	309	97	12	3	1	0	0	0	0	0	0	0	1025	38.5	44	34 to 44	74.8	73	7.1	2	983	40

Incoming Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/3/2024	00:00	1	0	0	2	2	12	12	3	0	0	0	0	0	0	0	0	0	0	32	37.9	42	35 to 45	81.3	1	3.1	0	31	1	
9/3/2024	01:00	0	0	0	3	2	4	2	0	0	0	0	0	0	0	0	0	0	0	11	34.9	39	29 to 39	72.7	*	*	0	10	1	
9/3/2024	02:00	0	0	0	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	3	44	55	29 to 39	66.7	1	33.3	0	3	0	
9/3/2024	03:00	0	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	3	38.3	46	27 to 37	66.7	1	33.3	0	3	0	
9/3/2024	04:00	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	3	33.3	37	27 to 37	100.0	*	*	0	3	0	
9/3/2024	05:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	38.5	39	29 to 39	100.0	*	*	0	2	0	
9/3/2024	06:00	0	0	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	3	31.3	39	29 to 39	66.7	*	*	0	3	0	
9/3/2024	07:00	0	0	2	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	5	29.6	32	22 to 32	80.0	*	*	0	5	0	
9/3/2024	08:00	0	0	0	0	4	10	10	5	0	0	0	0	0	0	0	0	0	0	29	39.6	44.3	35 to 45	79.3	2	6.9	0	29	0	
9/3/2024	09:00	2	0	0	0	17	16	14	4	0	0	0	0	0	0	0	0	0	0	53	36.6	42.5	31 to 41	67.9	2	3.8	0	50	3	
9/3/2024	10:00	0	0	0	0	12	33	41	13	3	0	0	0	0	0	0	0	0	0	102	40	44.2	35 to 45	77.5	11	10.8	0	99	3	
9/3/2024	11:00	0	1	0	3	9	28	33	8	3	0	0	0	0	0	0	0	0	0	85	39.2	43.5	34 to 44	75.3	8	9.4	0	81	4	
9/3/2024	12:00	0	1	1	6	14	40	28	10	0	0	0	0	0	0	0	0	0	0	100	37.9	43.2	34 to 44	73.0	6	6.0	0	90	10	
9/3/2024	13:00	0	0	0	0	5	15	13	3	0	0	0	0	0	0	0	0	0	0	36	39	42	32 to 42	86.1	2	5.6	0	32	4	
9/3/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/3/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/3/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/3/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/3/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/3/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/3/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/3/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/3/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		3	2	4	14	70	166	154	47	6	1	0	0	0	0	0	0	0	0	467	38.5	44	35 to 45	72.8	34	7.3	0	441	26	

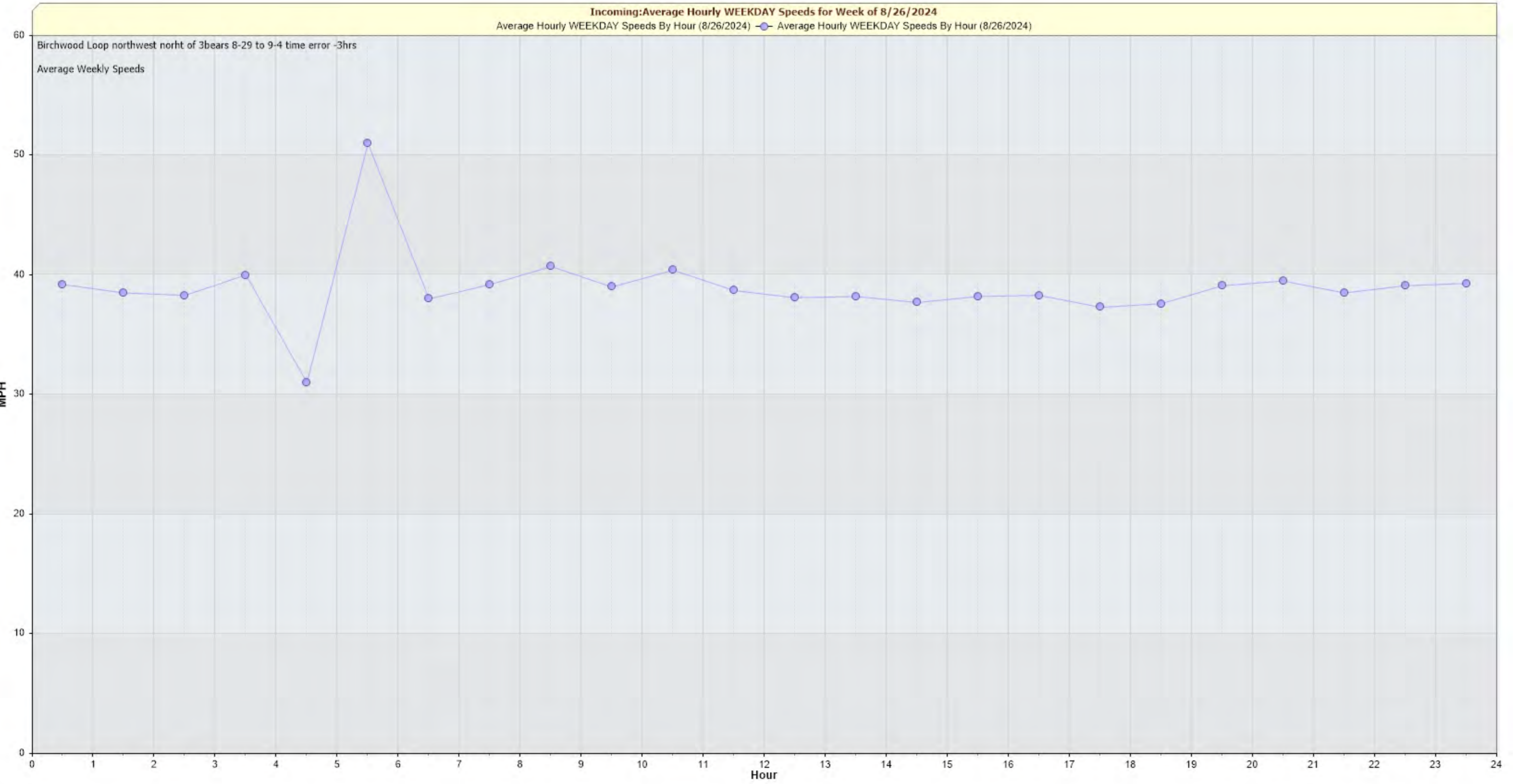
Incoming: Average Hourly Volume for Week of 8/26/2024
Average Counts By Hour (8/26/2024) — Average Counts By Hour (8/26/2024)

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs
Average Weekly Volumes



Incoming: Average Hourly WEEKDAY Speeds for Week of 8/26/2024
Average Hourly WEEKDAY Speeds By Hour (8/26/2024) — Average Hourly WEEKDAY Speeds By Hour (8/26/2024)

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs
Average Weekly Speeds

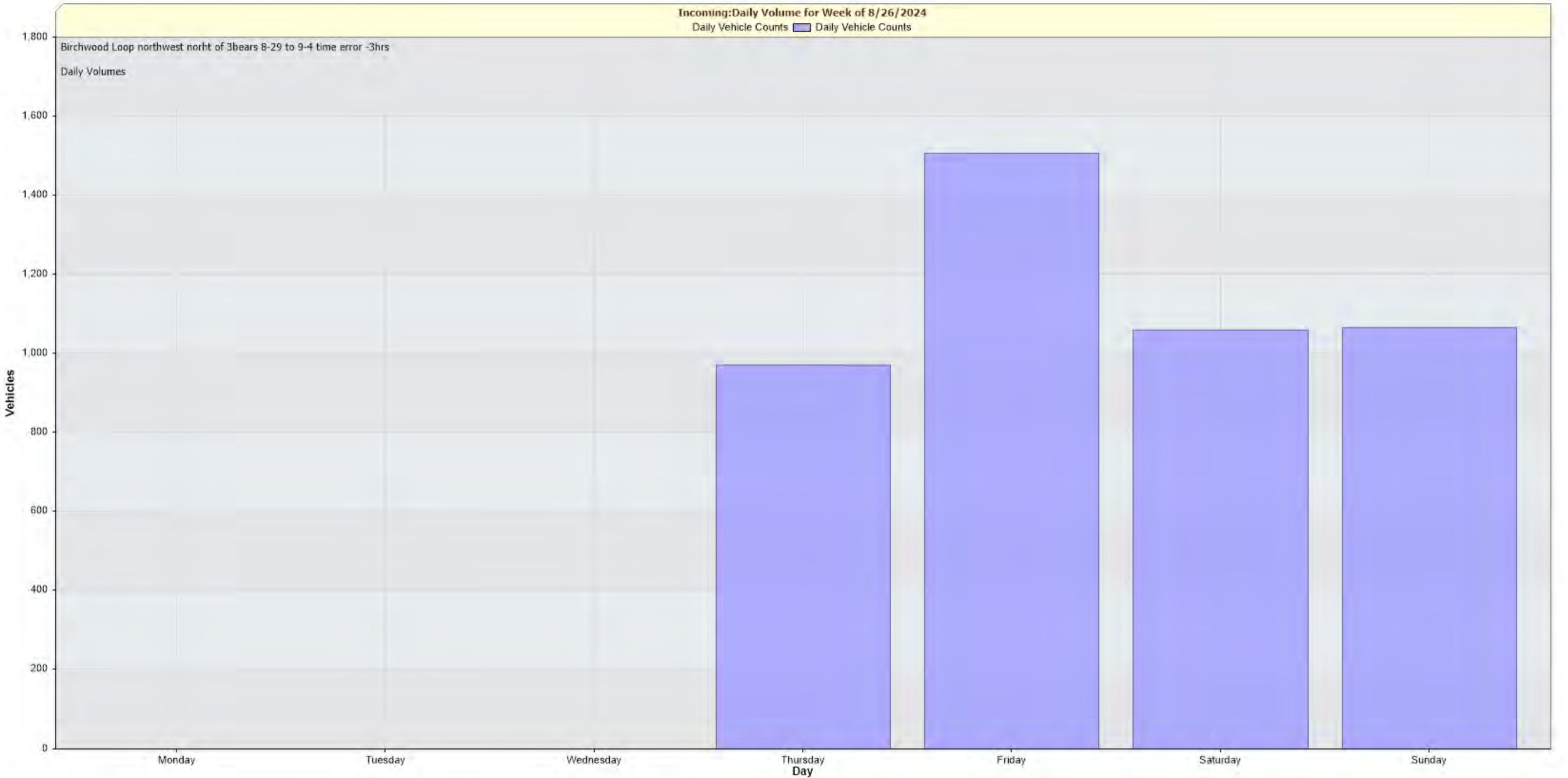


Incoming:Daily Volume for Week of 8/26/2024

Daily Vehicle Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

Daily Volumes



For Project: Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs
 Project Notes:
 Location/Name: Outgoing
 Report Generated: 10/3/2024 3:33:12 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 8/29/2024 1:00:00 PM through 9/3/2024 1:59:59 PM
 85th Percentile Speed: 44 MPH
 85th Percentile Vehicles: 5199
 Max Speed: 72 MPH on 8/30/2024 2:20:39 AM
 Total Vehicles: 6117
 AADT: 1213

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	993	1019
AM Peak 11:00 AM	62	50
PM Peak 7:00 PM	119	103

Speed

Speed Limit: 45
 85th Percentile Speed: 44
 50th Percentile Speed: 39
 10 MPH Pace Interval: 35.0 MPH to 45.0 MPH
 Average Speed: 39.12

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	87	26	N/A	90	146	80	98
% over limit	8.2	7.6	N/A	8.6	9.6	7.4	9.2
Avg Speeder	48.5	47.5	N/A	47.7	48.1	48.5	48.3
Avg Speed	38.9	38.5	N/A	39.2	39.4	39.0	39.1

Class Counts

	Number	%
VEH_SM	12	0.2
VEH_MED	5805	94.9
VEH_LG	300	4.9
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Outgoing Summary
Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/29/2024 2:00:00 PM	44.0	20	23	46	46.0	4.3%	39.6
8/29/2024 3:00:00 PM	41.0	74	87	48	47.0	3.4%	37.3
8/29/2024 4:00:00 PM	43.0	89	105	50	47.3	3.8%	38.2
8/29/2024 5:00:00 PM	44.0	97	114	50	46.9	11.4%	39.5
8/29/2024 6:00:00 PM	44.0	78	92	48	47.2	5.4%	39.0
8/29/2024 7:00:00 PM	44.0	84	99	50	47.4	8.1%	38.9
8/29/2024 8:00:00 PM	44.0	117	138	58	48.6	10.1%	39.6
8/29/2024 9:00:00 PM	45.0	111	131	51	47.6	13.0%	40.0
8/29/2024 10:00:00 PM	43.0	88	104	53	48.3	6.7%	39.8
8/29/2024 11:00:00 PM	45.0	68	80	53	47.9	15.0%	40.1
8/30/2024 12:00:00 AM	44.0	58	68	53	47.5	8.8%	39.2
8/30/2024 1:00:00 AM	44.0	42	49	48	46.7	6.1%	39.8
8/30/2024 2:00:00 AM	45.0	26	30	47	46.3	13.3%	39.7
8/30/2024 3:00:00 AM	45.0	11	13	72	59.5	15.4%	41.5
8/30/2024 4:00:00 AM	43.0	6	7	48	48.0	14.3%	39.3
8/30/2024 5:00:00 AM	38.0	2	2	38	0.0	0.0%	35.0
8/30/2024 6:00:00 AM	50.0	1	1	50	50.0	100.0%	50.0
8/30/2024 7:00:00 AM	39.0	1	1	39	0.0	0.0%	39.0
8/30/2024 8:00:00 AM	39.0	2	2	39	0.0	0.0%	38.0
8/30/2024 9:00:00 AM	44.0	13	15	46	46.0	6.7%	39.4
8/30/2024 10:00:00 AM	43.0	30	35	46	46.0	2.9%	38.0
8/30/2024 11:00:00 AM	46.0	34	40	68	49.1	20.0%	41.5
8/30/2024 12:00:00 PM	43.0	63	74	49	47.7	4.1%	39.0
8/30/2024 1:00:00 PM	44.0	83	98	50	48.0	9.2%	39.3
8/30/2024 2:00:00 PM	43.0	62	73	50	49.3	4.1%	38.7
8/30/2024 3:00:00 PM	43.0	76	89	51	49.4	5.6%	38.4
8/30/2024 4:00:00 PM	43.0	82	97	51	47.5	8.2%	39.1
8/30/2024 5:00:00 PM	43.0	95	112	49	47.4	6.2%	38.9
8/30/2024 6:00:00 PM	44.0	101	119	50	47.2	10.1%	39.6
8/30/2024 7:00:00 PM	45.0	124	146	51	47.8	14.4%	39.7
8/30/2024 8:00:00 PM	45.0	121	142	53	47.4	9.9%	39.5
8/30/2024 9:00:00 PM	45.0	122	143	52	48.4	12.6%	40.2
8/30/2024 10:00:00 PM	45.0	76	90	56	47.8	14.4%	39.8
8/30/2024 11:00:00 PM	42.0	59	69	46	46.0	2.9%	38.8
8/31/2024 12:00:00 AM	45.0	64	75	56	49.7	13.3%	39.8
8/31/2024 1:00:00 AM	44.0	45	53	59	48.8	11.3%	38.3
8/31/2024 2:00:00 AM	43.0	22	26	50	48.5	7.7%	38.9
8/31/2024 3:00:00 AM	42.0	14	17	49	48.5	11.8%	39.6
8/31/2024 4:00:00 AM	42.0	9	11	62	62.0	9.1%	39.1
8/31/2024 5:00:00 AM	48.0	2	2	48	48.0	50.0%	45.0
8/31/2024 6:00:00 AM	42.0	3	4	44	0.0	0.0%	41.3
8/31/2024 7:00:00 AM	38.0	2	2	38	0.0	0.0%	37.0
8/31/2024 8:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
8/31/2024 9:00:00 AM	40.0	4	5	44	0.0	0.0%	38.8
8/31/2024 10:00:00 AM	41.0	10	12	45	0.0	0.0%	38.9
8/31/2024 11:00:00 AM	44.0	19	22	53	53.0	9.1%	40.1
8/31/2024 12:00:00 PM	44.0	29	34	46	46.0	5.9%	38.2

Outgoing Summary
 Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

8/31/2024 1:00:00 PM	43.0	37	43	46	46.0	7.0%	38.1
8/31/2024 2:00:00 PM	44.0	52	61	50	48.0	8.2%	39.8
8/31/2024 3:00:00 PM	44.0	76	89	59	48.8	6.7%	39.5
8/31/2024 4:00:00 PM	44.0	71	84	51	47.8	7.1%	39.8
8/31/2024 5:00:00 PM	43.0	77	91	52	47.6	5.5%	38.1
8/31/2024 6:00:00 PM	44.0	68	80	50	47.9	10.0%	39.2
8/31/2024 7:00:00 PM	43.0	69	81	49	47.8	4.9%	37.9
8/31/2024 8:00:00 PM	42.0	72	85	52	48.0	3.5%	38.5
8/31/2024 9:00:00 PM	43.0	69	81	49	46.7	7.4%	39.2
8/31/2024 10:00:00 PM	44.0	53	62	54	47.7	9.7%	39.7
8/31/2024 11:00:00 PM	43.0	60	71	59	51.5	8.5%	39.7
9/1/2024 12:00:00 AM	44.0	54	64	52	48.8	9.4%	38.8
9/1/2024 1:00:00 AM	43.0	34	40	48	47.0	7.5%	38.6
9/1/2024 2:00:00 AM	43.0	25	29	50	48.5	6.9%	38.1
9/1/2024 3:00:00 AM	41.0	8	10	43	0.0	0.0%	36.7
9/1/2024 4:00:00 AM	44.0	8	10	53	53.0	10.0%	39.3
9/1/2024 5:00:00 AM	42.0	5	6	43	0.0	0.0%	40.2
9/1/2024 6:00:00 AM	43.0	4	5	46	46.0	20.0%	38.4
9/1/2024 7:00:00 AM	45.0	5	6	56	56.0	16.7%	43.7
9/1/2024 8:00:00 AM	42.0	3	3	42	0.0	0.0%	33.7
9/1/2024 9:00:00 AM	42.0	4	5	47	47.0	20.0%	37.4
9/1/2024 10:00:00 AM	46.0	22	26	61	53.4	19.2%	41.0
9/1/2024 11:00:00 AM	46.0	22	26	51	48.2	19.2%	40.6
9/1/2024 12:00:00 PM	44.0	27	32	47	46.3	9.4%	39.7
9/1/2024 1:00:00 PM	44.0	31	36	50	48.0	11.1%	38.3
9/1/2024 2:00:00 PM	44.0	47	55	47	46.7	5.5%	38.7
9/1/2024 3:00:00 PM	44.0	63	74	50	47.2	12.2%	39.8
9/1/2024 4:00:00 PM	45.0	83	98	55	48.8	8.1%	39.2
9/1/2024 5:00:00 PM	43.0	103	121	53	47.5	8.3%	38.7
9/1/2024 6:00:00 PM	44.0	73	86	47	46.3	4.7%	38.8
9/1/2024 7:00:00 PM	45.0	63	74	50	47.6	13.5%	39.5
9/1/2024 8:00:00 PM	43.0	62	73	56	49.1	9.6%	39.1
9/1/2024 9:00:00 PM	45.0	60	70	54	50.5	8.6%	39.5
9/1/2024 10:00:00 PM	44.0	65	76	50	49.0	3.9%	38.9
9/1/2024 11:00:00 PM	45.0	51	60	50	47.1	13.3%	39.5
9/2/2024 12:00:00 AM	45.0	37	43	49	47.5	9.3%	38.4
9/2/2024 1:00:00 AM	44.0	23	27	52	49.0	14.8%	39.4
9/2/2024 2:00:00 AM	41.0	20	24	44	0.0	0.0%	38.3
9/2/2024 3:00:00 AM	46.0	11	13	60	51.0	23.1%	41.5
9/2/2024 4:00:00 AM	45.0	7	8	48	48.0	12.5%	38.1
9/2/2024 5:00:00 AM	40.0	3	3	40	0.0	0.0%	38.0
9/2/2024 6:00:00 AM	36.0	5	6	39	0.0	0.0%	33.5
9/2/2024 7:00:00 AM	38.0	3	3	38	0.0	0.0%	36.0
9/2/2024 8:00:00 AM	37.0	2	2	37	0.0	0.0%	36.5
9/2/2024 9:00:00 AM	**No Data**						
9/2/2024 10:00:00 AM	40.0	9	11	45	0.0	0.0%	38.1
9/2/2024 11:00:00 AM	39.0	10	12	45	0.0	0.0%	35.7
9/2/2024 12:00:00 PM	42.0	33	39	45	0.0	0.0%	37.8

Outgoing Summary
Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

9/2/2024 1:00:00 PM	44.0	47	55	50	48.1	12.7%	38.6
9/2/2024 2:00:00 PM	41.0	58	68	48	47.0	2.9%	38.0
9/2/2024 3:00:00 PM	44.0	75	88	57	49.0	4.5%	39.2
9/2/2024 4:00:00 PM	44.0	58	68	58	49.8	7.4%	39.6
9/2/2024 5:00:00 PM	43.0	64	75	49	47.0	4.0%	38.2
9/2/2024 6:00:00 PM	42.0	80	94	51	47.9	7.4%	38.4
9/2/2024 7:00:00 PM	44.0	78	92	69	50.1	8.7%	39.9
9/2/2024 8:00:00 PM	44.0	66	78	58	47.8	12.8%	39.6
9/2/2024 9:00:00 PM	46.0	65	77	51	48.4	16.9%	40.8
9/2/2024 10:00:00 PM	42.0	82	96	59	49.2	6.3%	38.5
9/2/2024 11:00:00 PM	44.0	56	66	48	46.7	10.6%	38.9
9/3/2024 12:00:00 AM	45.0	52	61	56	48.9	11.5%	39.1
9/3/2024 1:00:00 AM	43.0	14	16	44	0.0	0.0%	38.4
9/3/2024 2:00:00 AM	42.0	16	19	46	46.0	5.3%	35.2
9/3/2024 3:00:00 AM	45.0	6	7	46	46.0	14.3%	38.6
9/3/2024 4:00:00 AM	43.0	6	7	49	49.0	14.3%	40.7
9/3/2024 5:00:00 AM	43.0	3	4	44	0.0	0.0%	39.3
9/3/2024 6:00:00 AM	40.0	2	2	40	0.0	0.0%	39.0
9/3/2024 7:00:00 AM	37.0	2	2	37	0.0	0.0%	35.5
9/3/2024 8:00:00 AM	54.0	3	3	54	54.0	33.3%	43.0
9/3/2024 9:00:00 AM	48.0	14	16	49	48.3	18.8%	42.3
9/3/2024 10:00:00 AM	43.0	23	27	46	46.0	7.4%	38.6
9/3/2024 11:00:00 AM	43.0	37	43	49	47.2	11.6%	38.1
9/3/2024 12:00:00 PM	45.0	63	74	52	47.3	9.5%	39.4
9/3/2024 1:00:00 PM	42.0	66	78	49	47.3	5.1%	37.7
9/3/2024 2:00:00 PM	41.0	38	45	46	46.0	2.2%	38.3

Outgoing Summary

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/30/2024 12:00:00 AM	44.0	885	1041	58	47.7	8.6%	39.2
8/31/2024 12:00:00 AM	44.0	1294	1522	72	48.1	9.6%	39.4
9/1/2024 12:00:00 AM	44.0	919	1081	62	48.5	7.4%	39.0
9/2/2024 12:00:00 AM	44.0	904	1064	61	48.3	9.2%	39.1
9/3/2024 12:00:00 AM	44.0	906	1066	69	48.5	8.2%	38.9
9/3/2024 1:59:59 PM	44.0	292	343	54	47.5	7.6%	38.5

Outgoing Weekly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	49	53	40	49	46.5	43.3
1 - 2	*	*	*	*	30	26	29	30	27.5	45
2 - 3	*	*	*	*	13	17	10	13	13.5	45
3 - 4	*	*	*	*	7	11	10	7	10.5	43
4 - 5	*	*	*	*	2	2	6	2	4	38
5 - 6	*	*	*	*	1	4	5	1	4.5	50
6 - 7	*	*	*	*	1	2	6	1	4	39
7 - 8	*	*	*	*	2	1	3	2	2	39
8 - 9	*	*	*	*	15	5	5	15	5	44
9 - 10	*	*	*	*	35	12	26	35	19	42.5
10 - 11	*	*	*	*	40	22	26	40	24	45.4
11 - 12	*	*	*	*	74	34	32	74	33	42.5
12 - 13	*	*	*	*	98	43	36	98	39.5	44
13 - 14	*	*	*	23	73	61	55	48	58	42.95
14 - 15	*	*	*	87	89	89	74	88	81.5	41.55
15 - 16	*	*	*	105	97	84	98	101	91	42.8
16 - 17	*	*	*	114	112	91	121	113	106	43.15
17 - 18	*	*	*	92	119	80	86	105.5	83	43.55
18 - 19	*	*	*	99	146	81	74	122.5	77.5	43.95
19 - 20	*	*	*	138	142	85	73	140	79	44.2
20 - 21	*	*	*	131	143	81	70	137	75.5	44.6
21 - 22	*	*	*	104	90	62	76	97	69	43.85
22 - 23	*	*	*	80	69	71	60	74.5	65.5	43.5
23 - 24	*	*	*	68	75	64	43	71.5	53.5	44
Totals	0	0	0	1041	1522	1081	1064			
% of Total	0%	0%	0%	22.11%	32.33%	22.96%	22.6%			

Outgoing Weekly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	27	16	*	*	*	*	*	21.5	0	43.5
1 - 2	24	19	*	*	*	*	*	21.5	0	41
2 - 3	13	7	*	*	*	*	*	10	0	45.5
3 - 4	8	7	*	*	*	*	*	7.5	0	44
4 - 5	3	4	*	*	*	*	*	3.5	0	41.5
5 - 6	6	2	*	*	*	*	*	4	0	38
6 - 7	3	2	*	*	*	*	*	2.5	0	37.5
7 - 8	2	3	*	*	*	*	*	2.5	0	45.5
8 - 9	0	16	*	*	*	*	*	8	0	47.5
9 - 10	11	27	*	*	*	*	*	19	0	41.4
10 - 11	12	43	*	*	*	*	*	27.5	0	41
11 - 12	39	74	*	*	*	*	*	56.5	0	42.95
12 - 13	55	78	*	*	*	*	*	66.5	0	42.9
13 - 14	68	45	*	*	*	*	*	56.5	0	40.95
14 - 15	88	*	*	*	*	*	*	88	0	43.5
15 - 16	68	*	*	*	*	*	*	68	0	44
16 - 17	75	*	*	*	*	*	*	75	0	42.6
17 - 18	94	*	*	*	*	*	*	94	0	42
18 - 19	92	*	*	*	*	*	*	92	0	43.5
19 - 20	78	*	*	*	*	*	*	78	0	44
20 - 21	77	*	*	*	*	*	*	77	0	45.3
21 - 22	96	*	*	*	*	*	*	96	0	41.9
22 - 23	66	*	*	*	*	*	*	66	0	44
23 - 24	61	*	*	*	*	*	*	61	0	44.5
Totals	1066	343	0	0	0	0	0			
% of Total	75.66%	24.34%	0%	0%	0%	0%	0%			

Outgoing Monthly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	49	53	*	49	53	43.3
1 - 2	*	*	*	*	30	26	*	30	26	45
2 - 3	*	*	*	*	13	17	*	13	17	45
3 - 4	*	*	*	*	7	11	*	7	11	43
4 - 5	*	*	*	*	2	2	*	2	2	38
5 - 6	*	*	*	*	1	4	*	1	4	50
6 - 7	*	*	*	*	1	2	*	1	2	39
7 - 8	*	*	*	*	2	1	*	2	1	39
8 - 9	*	*	*	*	15	5	*	15	5	44
9 - 10	*	*	*	*	35	12	*	35	12	42.5
10 - 11	*	*	*	*	40	22	*	40	22	45.4
11 - 12	*	*	*	*	74	34	*	74	34	42.5
12 - 13	*	*	*	*	98	43	*	98	43	44
13 - 14	*	*	*	23	73	61	*	48	61	42.95
14 - 15	*	*	*	87	89	89	*	88	89	41.55
15 - 16	*	*	*	105	97	84	*	101	84	42.8
16 - 17	*	*	*	114	112	91	*	113	91	43.15
17 - 18	*	*	*	92	119	80	*	105.5	80	43.55
18 - 19	*	*	*	99	146	81	*	122.5	81	43.95
19 - 20	*	*	*	138	142	85	*	140	85	44.2
20 - 21	*	*	*	131	143	81	*	137	81	44.6
21 - 22	*	*	*	104	90	62	*	97	62	43.85
22 - 23	*	*	*	80	69	71	*	74.5	71	43.5
23 - 24	*	*	*	68	75	64	*	71.5	64	44
Totals	0	0	0	1041	1522	1081	0			
% of Total	0%	0%	0%	28.57%	41.77%	29.67%	0%			

Outgoing Monthly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	27	16	*	*	*	*	40	21.5	40	43.5
1 - 2	24	19	*	*	*	*	29	21.5	29	41
2 - 3	13	7	*	*	*	*	10	10	10	45.5
3 - 4	8	7	*	*	*	*	10	7.5	10	44
4 - 5	3	4	*	*	*	*	6	3.5	6	41.5
5 - 6	6	2	*	*	*	*	5	4	5	38
6 - 7	3	2	*	*	*	*	6	2.5	6	37.5
7 - 8	2	3	*	*	*	*	3	2.5	3	45.5
8 - 9	0	16	*	*	*	*	5	8	5	47.5
9 - 10	11	27	*	*	*	*	26	19	26	41.4
10 - 11	12	43	*	*	*	*	26	27.5	26	41
11 - 12	39	74	*	*	*	*	32	56.5	32	42.95
12 - 13	55	78	*	*	*	*	36	66.5	36	42.9
13 - 14	68	45	*	*	*	*	55	56.5	55	40.95
14 - 15	88	*	*	*	*	*	74	88	74	43.5
15 - 16	68	*	*	*	*	*	98	68	98	44
16 - 17	75	*	*	*	*	*	121	75	121	42.6
17 - 18	94	*	*	*	*	*	86	94	86	42
18 - 19	92	*	*	*	*	*	74	92	74	43.5
19 - 20	78	*	*	*	*	*	73	78	73	44
20 - 21	77	*	*	*	*	*	70	77	70	45.3
21 - 22	96	*	*	*	*	*	76	96	76	41.9
22 - 23	66	*	*	*	*	*	60	66	60	44
23 - 24	61	*	*	*	*	*	43	61	43	44.5
Totals	1066	343	0	0	0	0	1064			
% of Total	43.11%	13.87%	0%	0%	0%	0%	43.02%			

Outgoing Weekly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	39.8	38.3	38.62	39.8	38.44	43.3
1 - 2	*	*	*	*	39.67	38.88	38.1	39.67	38.47	45
2 - 3	*	*	*	*	41.46	39.59	36.7	41.46	38.52	45
3 - 4	*	*	*	*	39.29	39.09	39.3	39.29	39.19	43
4 - 5	*	*	*	*	35	45	40.17	35	41.38	38
5 - 6	*	*	*	*	50	41.25	38.4	50	39.67	50
6 - 7	*	*	*	*	39	37	43.67	39	42	39
7 - 8	*	*	*	*	38	35	33.67	38	34	39
8 - 9	*	*	*	*	39.4	38.8	37.4	39.4	38.1	44
9 - 10	*	*	*	*	38	38.92	40.96	38	40.32	42.5
10 - 11	*	*	*	*	41.45	40.14	40.58	41.45	40.38	45.4
11 - 12	*	*	*	*	39.04	38.21	39.66	39.04	38.91	42.5
12 - 13	*	*	*	*	39.35	38.07	38.25	39.35	38.15	44
13 - 14	*	*	*	39.78	38.68	39.75	38.71	38.95	39.26	42.95
14 - 15	*	*	*	37.28	38.45	39.49	39.77	37.87	39.62	41.55
15 - 16	*	*	*	38.18	39.09	39.79	39.2	38.62	39.47	42.8
16 - 17	*	*	*	39.5	38.88	38.05	38.74	39.19	38.44	43.15
17 - 18	*	*	*	38.97	39.56	39.16	38.76	39.3	38.95	43.55
18 - 19	*	*	*	38.88	39.66	37.89	39.51	39.35	38.66	43.95
19 - 20	*	*	*	39.6	39.54	38.51	39.05	39.57	38.76	44.2
20 - 21	*	*	*	40.02	40.15	39.16	39.5	40.09	39.32	44.6
21 - 22	*	*	*	39.75	39.77	39.68	38.89	39.76	39.25	43.85
22 - 23	*	*	*	40.06	38.81	39.72	39.47	39.48	39.6	43.5
23 - 24	*	*	*	39.21	39.76	38.83	38.4	39.5	38.65	44
Totals	0	0	0	39.2	39.4	39	39.1			
% of Total	0%	0%	0%	25.02%	25.14%	24.89%	24.95%			

Outgoing Weekly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	39.41	38.38	*	*	*	*	*	39.02	*	43.5
1 - 2	38.25	35.21	*	*	*	*	*	36.91	*	41
2 - 3	41.54	38.57	*	*	*	*	*	40.5	*	45.5
3 - 4	38.12	40.71	*	*	*	*	*	39.33	*	44
4 - 5	38	39.25	*	*	*	*	*	38.71	*	41.5
5 - 6	33.5	39	*	*	*	*	*	34.88	*	38
6 - 7	36	35.5	*	*	*	*	*	35.8	*	37.5
7 - 8	36.5	43	*	*	*	*	*	40.4	*	45.5
8 - 9	0	42.31	*	*	*	*	*	42.31	*	47.5
9 - 10	38.09	38.63	*	*	*	*	*	38.47	*	41.4
10 - 11	35.67	38.09	*	*	*	*	*	37.56	*	41
11 - 12	37.82	39.42	*	*	*	*	*	38.87	*	42.95
12 - 13	38.62	37.68	*	*	*	*	*	38.07	*	42.9
13 - 14	37.99	38.27	*	*	*	*	*	38.1	*	40.95
14 - 15	39.24	*	*	*	*	*	*	39.24	*	43.5
15 - 16	39.63	*	*	*	*	*	*	39.63	*	44
16 - 17	38.17	*	*	*	*	*	*	38.17	*	42.6
17 - 18	38.39	*	*	*	*	*	*	38.39	*	42
18 - 19	39.88	*	*	*	*	*	*	39.88	*	43.5
19 - 20	39.6	*	*	*	*	*	*	39.6	*	44
20 - 21	40.77	*	*	*	*	*	*	40.77	*	45.3
21 - 22	38.47	*	*	*	*	*	*	38.47	*	41.9
22 - 23	38.94	*	*	*	*	*	*	38.94	*	44
23 - 24	39.07	*	*	*	*	*	*	39.07	*	44.5
Totals	38.9	38.5	0	0	0	0	0			
% of Total	50.26%	49.74%	0%	0%	0%	0%	0%			

Outgoing Monthly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	39.8	38.3	*	39.8	38.3	43.3
1 - 2	*	*	*	*	39.67	38.88	*	39.67	38.88	45
2 - 3	*	*	*	*	41.46	39.59	*	41.46	39.59	45
3 - 4	*	*	*	*	39.29	39.09	*	39.29	39.09	43
4 - 5	*	*	*	*	35	45	*	35	45	38
5 - 6	*	*	*	*	50	41.25	*	50	41.25	50
6 - 7	*	*	*	*	39	37	*	39	37	39
7 - 8	*	*	*	*	38	35	*	38	35	39
8 - 9	*	*	*	*	39.4	38.8	*	39.4	38.8	44
9 - 10	*	*	*	*	38	38.92	*	38	38.92	42.5
10 - 11	*	*	*	*	41.45	40.14	*	41.45	40.14	45.4
11 - 12	*	*	*	*	39.04	38.21	*	39.04	38.21	42.5
12 - 13	*	*	*	*	39.35	38.07	*	39.35	38.07	44
13 - 14	*	*	*	39.78	38.68	39.75	*	38.95	39.75	42.95
14 - 15	*	*	*	37.28	38.45	39.49	*	37.87	39.49	41.55
15 - 16	*	*	*	38.18	39.09	39.79	*	38.62	39.79	42.8
16 - 17	*	*	*	39.5	38.88	38.05	*	39.19	38.05	43.15
17 - 18	*	*	*	38.97	39.56	39.16	*	39.3	39.16	43.55
18 - 19	*	*	*	38.88	39.66	37.89	*	39.35	37.89	43.95
19 - 20	*	*	*	39.6	39.54	38.51	*	39.57	38.51	44.2
20 - 21	*	*	*	40.02	40.15	39.16	*	40.09	39.16	44.6
21 - 22	*	*	*	39.75	39.77	39.68	*	39.76	39.68	43.85
22 - 23	*	*	*	40.06	38.81	39.72	*	39.48	39.72	43.5
23 - 24	*	*	*	39.21	39.76	38.83	*	39.5	38.83	44
Totals	0	0	0	39.2	39.4	39	0			
% of Total	0%	0%	0%	33.33%	33.5%	33.16%	0%			

Outgoing Monthly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Sep 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	39.41	38.38	*	*	*	*	38.62	39.02	38.62	43.5
1 - 2	38.25	35.21	*	*	*	*	38.1	36.91	38.1	41
2 - 3	41.54	38.57	*	*	*	*	36.7	40.5	36.7	45.5
3 - 4	38.12	40.71	*	*	*	*	39.3	39.33	39.3	44
4 - 5	38	39.25	*	*	*	*	40.17	38.71	40.17	41.5
5 - 6	33.5	39	*	*	*	*	38.4	34.88	38.4	38
6 - 7	36	35.5	*	*	*	*	43.67	35.8	43.67	37.5
7 - 8	36.5	43	*	*	*	*	33.67	40.4	33.67	45.5
8 - 9	0	42.31	*	*	*	*	37.4	42.31	37.4	47.5
9 - 10	38.09	38.63	*	*	*	*	40.96	38.47	40.96	41.4
10 - 11	35.67	38.09	*	*	*	*	40.58	37.56	40.58	41
11 - 12	37.82	39.42	*	*	*	*	39.66	38.87	39.66	42.95
12 - 13	38.62	37.68	*	*	*	*	38.25	38.07	38.25	42.9
13 - 14	37.99	38.27	*	*	*	*	38.71	38.1	38.71	40.95
14 - 15	39.24	*	*	*	*	*	39.77	39.24	39.77	43.5
15 - 16	39.63	*	*	*	*	*	39.2	39.63	39.2	44
16 - 17	38.17	*	*	*	*	*	38.74	38.17	38.74	42.6
17 - 18	38.39	*	*	*	*	*	38.76	38.39	38.76	42
18 - 19	39.88	*	*	*	*	*	39.51	39.88	39.51	43.5
19 - 20	39.6	*	*	*	*	*	39.05	39.6	39.05	44
20 - 21	40.77	*	*	*	*	*	39.5	40.77	39.5	45.3
21 - 22	38.47	*	*	*	*	*	38.89	38.47	38.89	41.9
22 - 23	38.94	*	*	*	*	*	39.47	38.94	39.47	44
23 - 24	39.07	*	*	*	*	*	38.4	39.07	38.4	44.5
Totals	38.9	38.5	0	0	0	0	39.1			
% of Total	33.39%	33.05%	0%	0%	0%	0%	33.56%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	43.3	44	43	43.3	43.5	43.3
1 - 2	*	*	*	*	45	42.5	43	45	42.75	45
2 - 3	*	*	*	*	45	42	41	45	41.5	45
3 - 4	*	*	*	*	43	41.5	44	43	42.75	43
4 - 5	*	*	*	*	38	48	42	38	45	38
5 - 6	*	*	*	*	50	42	43	50	42.5	50
6 - 7	*	*	*	*	39	38	45	39	41.5	39
7 - 8	*	*	*	*	39	35	42	39	38.5	39
8 - 9	*	*	*	*	44	40	42	44	41	44
9 - 10	*	*	*	*	42.5	41	46	42.5	43.5	42.5
10 - 11	*	*	*	*	45.4	43.5	46	45.4	44.75	45.4
11 - 12	*	*	*	*	42.5	43.3	44	42.5	43.65	42.5
12 - 13	*	*	*	*	44	43	44	44	43.5	44
13 - 14	*	*	*	43.5	42.4	43.5	43.2	42.95	43.35	42.95
14 - 15	*	*	*	40.9	42.2	43.1	44	41.55	43.55	41.55
15 - 16	*	*	*	43	42.6	43.4	44.2	42.8	43.8	42.8
16 - 17	*	*	*	43.7	42.6	42.6	43	43.15	42.8	43.15
17 - 18	*	*	*	43.3	43.8	44	43.5	43.55	43.75	43.55
18 - 19	*	*	*	43.2	44.7	43	44.8	43.95	43.9	43.95
19 - 20	*	*	*	43.9	44.5	41.8	43	44.2	42.4	44.2
20 - 21	*	*	*	44.7	44.5	42.5	44.3	44.6	43.4	44.6
21 - 22	*	*	*	43	44.7	43.8	43.2	43.85	43.5	43.85
22 - 23	*	*	*	45	42	43	44.8	43.5	43.9	43.5
23 - 24	*	*	*	43.3	44.7	43.7	44.3	44	44	44
Totals	0	0	0	477.5	1039.4	1018.2	1047.3			
% of Total	0%	0%	0%	13.33%	29.01%	28.42%	29.23%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	44	43	*	*	*	*	*	43.5	0	43.5
1 - 2	40.5	41.5	*	*	*	*	*	41	0	41
2 - 3	46	45	*	*	*	*	*	45.5	0	45.5
3 - 4	45	43	*	*	*	*	*	44	0	44
4 - 5	40	43	*	*	*	*	*	41.5	0	41.5
5 - 6	36	40	*	*	*	*	*	38	0	38
6 - 7	38	37	*	*	*	*	*	37.5	0	37.5
7 - 8	37	54	*	*	*	*	*	45.5	0	45.5
8 - 9	0	47.5	*	*	*	*	*	47.5	0	47.5
9 - 10	40	42.8	*	*	*	*	*	41.4	0	41.4
10 - 11	39	43	*	*	*	*	*	41	0	41
11 - 12	41.7	44.2	*	*	*	*	*	42.95	0	42.95
12 - 13	44	41.8	*	*	*	*	*	42.9	0	42.9
13 - 14	40.9	41	*	*	*	*	*	40.95	0	40.95
14 - 15	43.5	*	*	*	*	*	*	43.5	0	43.5
15 - 16	44	*	*	*	*	*	*	44	0	44
16 - 17	42.6	*	*	*	*	*	*	42.6	0	42.6
17 - 18	42	*	*	*	*	*	*	42	0	42
18 - 19	43.5	*	*	*	*	*	*	43.5	0	43.5
19 - 20	44	*	*	*	*	*	*	44	0	44
20 - 21	45.3	*	*	*	*	*	*	45.3	0	45.3
21 - 22	41.9	*	*	*	*	*	*	41.9	0	41.9
22 - 23	44	*	*	*	*	*	*	44	0	44
23 - 24	44.5	*	*	*	*	*	*	44.5	0	44.5
Totals	967.4	606.8	0	0	0	0	0			
% of Total	61.45%	38.55%	0%	0%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	43.3	44	*	43.3	44	43.3
1 - 2	*	*	*	*	45	42.5	*	45	42.5	45
2 - 3	*	*	*	*	45	42	*	45	42	45
3 - 4	*	*	*	*	43	41.5	*	43	41.5	43
4 - 5	*	*	*	*	38	48	*	38	48	38
5 - 6	*	*	*	*	50	42	*	50	42	50
6 - 7	*	*	*	*	39	38	*	39	38	39
7 - 8	*	*	*	*	39	35	*	39	35	39
8 - 9	*	*	*	*	44	40	*	44	40	44
9 - 10	*	*	*	*	42.5	41	*	42.5	41	42.5
10 - 11	*	*	*	*	45.4	43.5	*	45.4	43.5	45.4
11 - 12	*	*	*	*	42.5	43.3	*	42.5	43.3	42.5
12 - 13	*	*	*	*	44	43	*	44	43	44
13 - 14	*	*	*	43.5	42.4	43.5	*	42.95	43.5	42.95
14 - 15	*	*	*	40.9	42.2	43.1	*	41.55	43.1	41.55
15 - 16	*	*	*	43	42.6	43.4	*	42.8	43.4	42.8
16 - 17	*	*	*	43.7	42.6	42.6	*	43.15	42.6	43.15
17 - 18	*	*	*	43.3	43.8	44	*	43.55	44	43.55
18 - 19	*	*	*	43.2	44.7	43	*	43.95	43	43.95
19 - 20	*	*	*	43.9	44.5	41.8	*	44.2	41.8	44.2
20 - 21	*	*	*	44.7	44.5	42.5	*	44.6	42.5	44.6
21 - 22	*	*	*	43	44.7	43.8	*	43.85	43.8	43.85
22 - 23	*	*	*	45	42	43	*	43.5	43	43.5
23 - 24	*	*	*	43.3	44.7	43.7	*	44	43.7	44

Outgoing Monthly EightyFifthSpeeds

Birchwood Loop northwest norh of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	44	43	*	*	*	*	43	43.5	43	43.5
1 - 2	40.5	41.5	*	*	*	*	43	41	43	41
2 - 3	46	45	*	*	*	*	41	45.5	41	45.5
3 - 4	45	43	*	*	*	*	44	44	44	44
4 - 5	40	43	*	*	*	*	42	41.5	42	41.5
5 - 6	36	40	*	*	*	*	43	38	43	38
6 - 7	38	37	*	*	*	*	45	37.5	45	37.5
7 - 8	37	54	*	*	*	*	42	45.5	42	45.5
8 - 9	0	47.5	*	*	*	*	42	47.5	42	47.5
9 - 10	40	42.8	*	*	*	*	46	41.4	46	41.4
10 - 11	39	43	*	*	*	*	46	41	46	41
11 - 12	41.7	44.2	*	*	*	*	44	42.95	44	42.95
12 - 13	44	41.8	*	*	*	*	44	42.9	44	42.9
13 - 14	40.9	41	*	*	*	*	43.2	40.95	43.2	40.95
14 - 15	43.5	*	*	*	*	*	44	43.5	44	43.5
15 - 16	44	*	*	*	*	*	44.2	44	44.2	44
16 - 17	42.6	*	*	*	*	*	43	42.6	43	42.6
17 - 18	42	*	*	*	*	*	43.5	42	43.5	42
18 - 19	43.5	*	*	*	*	*	44.8	43.5	44.8	43.5
19 - 20	44	*	*	*	*	*	43	44	43	44
20 - 21	45.3	*	*	*	*	*	44.3	45.3	44.3	45.3
21 - 22	41.9	*	*	*	*	*	43.2	41.9	43.2	41.9
22 - 23	44	*	*	*	*	*	44.8	44	44.8	44
23 - 24	44.5	*	*	*	*	*	44.3	44.5	44.3	44.5

Summary of Violators

Birchwood Loop northwest north of 3bears 8-29 to 9-4 from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM
time error -3hrs

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	185	38.9	16	48.1
01:00:00	128	38.2	9	47.2
02:00:00	60	39.8	8	51.9
03:00:00	43	39.3	5	52.0
04:00:00	17	39.5	1	48.0
05:00:00	18	38.1	2	48.0
06:00:00	14	39.6	1	56.0
07:00:00	11	37.6	1	54.0
08:00:00	41	40.2	5	47.6
09:00:00	111	39.0	8	50.6
10:00:00	143	39.6	20	48.8
11:00:00	253	38.9	15	47.0
12:00:00	310	38.5	27	47.7
13:00:00	325	38.8	15	47.6
14:00:00	427	38.8	27	48.2
15:00:00	452	39.1	31	48.2
16:00:00	513	38.7	38	47.3
17:00:00	471	39.0	36	47.4
18:00:00	492	39.2	51	48.1
19:00:00	516	39.3	48	48.1
20:00:00	502	40.0	60	48.2
21:00:00	428	39.3	35	48.2
22:00:00	346	39.4	35	48.0
23:00:00	311	39.1	33	48.7

Outgoing Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	13:00	0	0	0	0	3	8	10	2	0	0	0	0	0	0	0	0	0	0	23	39.8	43.5	34 to 44	87.0	1	4.3	0	20	3	
8/29/2024	14:00	0	0	1	5	16	34	27	4	0	0	0	0	0	0	0	0	0	87	37.3	40.9	31 to 41	74.7	3	3.4	1	76	10		
8/29/2024	15:00	0	0	1	4	19	38	35	7	1	0	0	0	0	0	0	0	0	105	38.2	43	34 to 44	73.3	4	3.8	1	92	12		
8/29/2024	16:00	0	0	0	6	9	36	48	14	1	0	0	0	0	0	0	0	0	114	39.5	43.7	34 to 44	78.1	13	11.4	0	107	7		
8/29/2024	17:00	0	0	0	1	9	41	34	7	0	0	0	0	0	0	0	0	0	92	39	43.3	34 to 44	87.0	5	5.4	1	87	4		
8/29/2024	18:00	0	0	0	1	16	38	32	11	1	0	0	0	0	0	0	0	0	99	38.9	43.2	35 to 45	74.7	8	8.1	0	97	2		
8/29/2024	19:00	0	0	0	3	19	44	52	16	3	1	0	0	0	0	0	0	0	138	39.6	43.9	36 to 46	75.4	14	10.1	0	129	9		
8/29/2024	20:00	0	0	0	3	10	48	42	24	4	0	0	0	0	0	0	0	0	131	40	44.7	35 to 45	77.1	17	13.0	0	125	6		
8/29/2024	21:00	0	0	0	3	11	31	48	9	2	0	0	0	0	0	0	0	0	104	39.8	43	33 to 43	80.8	7	6.7	0	99	5		
8/29/2024	22:00	0	0	0	2	5	30	26	15	2	0	0	0	0	0	0	0	0	80	40.1	45	35 to 45	76.3	12	15.0	0	78	2		
8/29/2024	23:00	0	1	0	1	5	29	24	7	1	0	0	0	0	0	0	0	0	68	39.2	43.3	34 to 44	80.9	6	8.8	0	66	2		
24 Hr Summary		0	1	2	29	122	377	378	116	15	1	0	0	0	0	0	0	0	1041	39.2	44	35 to 45	76.6	90	8.6	3	976	62		

Outgoing Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/30/2024	00:00	0	0	0	1	2	18	23	5	0	0	0	0	0	0	0	0	0	0	49	39.8	43.3	35 to 45	87.8	3	6.1	0	46	3
8/30/2024	01:00	0	0	0	1	2	13	7	7	0	0	0	0	0	0	0	0	0	0	30	39.7	45	36 to 46	83.3	4	13.3	0	30	0
8/30/2024	02:00	0	0	0	1	1	5	3	2	0	0	0	0	1	0	0	0	0	0	13	41.5	45	34 to 44	69.2	2	15.4	0	13	0
8/30/2024	03:00	0	0	0	0	2	2	2	1	0	0	0	0	0	0	0	0	0	0	7	39.3	43	33 to 43	85.7	1	14.3	0	7	0
8/30/2024	04:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	35	38	28 to 38	100.0	*	*	0	2	0
8/30/2024	05:00	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	50	50	40 to 50	100.0	1	100.0	0	1	0
8/30/2024	06:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	39	39	29 to 39	100.0	*	*	0	1	0
8/30/2024	07:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	38	39	29 to 39	100.0	*	*	0	2	0
8/30/2024	08:00	0	0	0	0	4	3	6	2	0	0	0	0	0	0	0	0	0	0	15	39.4	44	34 to 44	80.0	1	6.7	0	13	2
8/30/2024	09:00	0	1	1	2	2	11	15	3	0	0	0	0	0	0	0	0	0	0	35	38	42.5	35 to 45	80.0	1	2.9	0	32	3
8/30/2024	10:00	0	0	0	2	3	6	19	9	0	0	0	1	0	0	0	0	0	0	40	41.5	45.4	36 to 46	77.5	8	20.0	0	39	1
8/30/2024	11:00	0	0	0	1	12	26	29	6	0	0	0	0	0	0	0	0	0	0	74	39	42.5	34 to 44	83.8	3	4.1	0	66	8
8/30/2024	12:00	0	0	0	0	13	41	29	13	2	0	0	0	0	0	0	0	0	0	98	39.3	44	35 to 45	77.6	9	9.2	0	87	11
8/30/2024	13:00	0	0	0	2	11	24	31	3	2	0	0	0	0	0	0	0	0	0	73	38.7	42.4	35 to 45	78.1	3	4.1	0	65	8
8/30/2024	14:00	0	0	1	1	13	33	33	6	2	0	0	0	0	0	0	0	0	0	89	38.4	42.2	35 to 45	77.5	5	5.6	0	78	11
8/30/2024	15:00	0	0	0	3	11	33	38	11	1	0	0	0	0	0	0	0	0	0	97	39.1	42.6	33 to 43	78.4	8	8.2	0	91	6
8/30/2024	16:00	0	0	0	1	17	38	47	9	0	0	0	0	0	0	0	0	0	0	112	38.9	42.6	34 to 44	80.4	7	6.3	1	99	12
8/30/2024	17:00	0	1	0	1	14	34	53	15	1	0	0	0	0	0	0	0	0	0	119	39.6	43.8	34 to 44	76.5	12	10.1	1	114	4
8/30/2024	18:00	0	0	0	1	20	57	44	20	4	0	0	0	0	0	0	0	0	0	146	39.7	44.7	34 to 44	75.3	21	14.4	2	138	6
8/30/2024	19:00	0	0	1	2	18	51	43	26	1	0	0	0	0	0	0	0	0	0	142	39.5	44.5	35 to 45	75.4	14	9.9	1	131	10
8/30/2024	20:00	0	0	0	2	14	43	60	19	5	0	0	0	0	0	0	0	0	0	143	40.2	44.5	35 to 45	76.2	18	12.6	0	137	6
8/30/2024	21:00	0	0	0	2	8	35	29	14	1	1	0	0	0	0	0	0	0	0	90	39.8	44.7	36 to 46	75.6	13	14.4	0	89	1
8/30/2024	22:00	0	0	0	0	10	30	20	9	0	0	0	0	0	0	0	0	0	0	69	38.8	42	33 to 43	87.0	2	2.9	0	68	1
8/30/2024	23:00	0	0	0	2	9	29	22	8	4	1	0	0	0	0	0	0	0	0	75	39.8	44.7	34 to 44	74.7	10	13.3	0	73	2
24 Hr Summary		0	2	3	25	187	536	553	188	24	2	0	1	1	0	0	0	0	0	1522	39.4	44	35 to 45	76.1	146	9.6	5	1422	95

Outgoing Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/31/2024	00:00	0	1	2	1	11	13	17	7	0	1	0	0	0	0	0	0	0	0	53	38.3	44	32 to 42	66.0	6	11.3	0	51	2
8/31/2024	01:00	0	0	0	0	6	5	12	2	1	0	0	0	0	0	0	0	0	0	26	38.9	42.5	32 to 42	73.1	2	7.7	0	25	1
8/31/2024	02:00	0	0	0	0	1	9	4	3	0	0	0	0	0	0	0	0	0	0	17	39.6	42	32 to 42	82.4	2	11.8	0	17	0
8/31/2024	03:00	0	0	0	1	2	3	4	0	0	0	1	0	0	0	0	0	0	0	11	39.1	41.5	32 to 42	81.8	1	9.1	0	11	0
8/31/2024	04:00	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	45	48	38 to 48	100.0	1	50.0	0	2	0
8/31/2024	05:00	0	0	0	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	4	41.3	42	34 to 44	100.0	*	*	0	4	0
8/31/2024	06:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	37	38	28 to 38	100.0	*	*	0	2	0
8/31/2024	07:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	1	0
8/31/2024	08:00	0	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5	38.8	40	34 to 44	100.0	*	*	0	5	0
8/31/2024	09:00	0	0	0	0	1	5	5	1	0	0	0	0	0	0	0	0	0	0	12	38.9	41	32 to 42	91.7	*	*	0	11	1
8/31/2024	10:00	0	0	0	0	3	9	8	0	2	0	0	0	0	0	0	0	0	0	22	40.1	43.5	34 to 44	77.3	2	9.1	0	22	0
8/31/2024	11:00	0	0	0	2	6	10	13	3	0	0	0	0	0	0	0	0	0	0	34	38.2	43.3	34 to 44	73.5	2	5.9	0	33	1
8/31/2024	12:00	0	0	0	3	6	17	13	4	0	0	0	0	0	0	0	0	0	0	43	38.1	43	36 to 46	76.7	3	7.0	0	42	1
8/31/2024	13:00	0	0	0	1	9	15	28	7	1	0	0	0	0	0	0	0	0	0	61	39.8	43.5	33 to 43	78.7	5	8.2	0	60	1
8/31/2024	14:00	0	0	0	1	13	27	41	6	0	1	0	0	0	0	0	0	0	0	89	39.5	43.1	34 to 44	83.1	6	6.7	0	87	2
8/31/2024	15:00	0	0	0	1	11	29	35	7	1	0	0	0	0	0	0	0	0	0	84	39.8	43.4	34 to 44	84.5	6	7.1	0	81	3
8/31/2024	16:00	0	0	1	5	16	31	29	8	1	0	0	0	0	0	0	0	0	0	91	38.1	42.6	33 to 43	70.3	5	5.5	0	89	2
8/31/2024	17:00	0	0	0	3	9	33	23	11	1	0	0	0	0	0	0	0	0	0	80	39.2	44	34 to 44	75.0	8	10.0	0	78	2
8/31/2024	18:00	0	0	0	3	14	35	21	8	0	0	0	0	0	0	0	0	0	0	81	37.9	43	34 to 44	74.1	4	4.9	0	78	3
8/31/2024	19:00	0	0	0	3	10	36	30	5	1	0	0	0	0	0	0	0	0	0	85	38.5	41.8	35 to 45	81.2	3	3.5	1	77	7
8/31/2024	20:00	0	0	0	1	10	28	34	8	0	0	0	0	0	0	0	0	0	0	81	39.2	42.5	36 to 46	81.5	6	7.4	0	77	4
8/31/2024	21:00	0	0	0	1	8	22	23	7	1	0	0	0	0	0	0	0	0	0	62	39.7	43.8	34 to 44	77.4	6	9.7	0	59	3
8/31/2024	22:00	0	0	0	2	9	24	26	6	3	1	0	0	0	0	0	0	0	0	71	39.7	43	33 to 43	76.1	6	8.5	0	71	0
8/31/2024	23:00	0	0	0	2	14	20	19	6	3	0	0	0	0	0	0	0	0	0	64	38.8	43.7	32 to 42	71.9	6	9.4	0	62	2
24 Hr Summary		0	1	3	30	159	378	391	100	15	3	1	0	0	0	0	0	0	0	1081	39	44	34 to 44	75.9	80	7.4	1	1045	35

Outgoing Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/1/2024	00:00	0	0	0	0	9	11	17	3	0	0	0	0	0	0	0	0	0	0	40	38.6	43	32 to 42	77.5	3	7.5	0	39	1
9/1/2024	01:00	0	0	0	1	5	12	7	3	1	0	0	0	0	0	0	0	0	0	29	38.1	43	31 to 41	79.3	2	6.9	1	26	2
9/1/2024	02:00	0	0	0	0	4	3	3	0	0	0	0	0	0	0	0	0	0	0	10	36.7	41	32 to 42	90.0	*	*	0	10	0
9/1/2024	03:00	0	0	0	1	0	5	2	1	1	0	0	0	0	0	0	0	0	0	10	39.3	44	35 to 45	80.0	1	10.0	0	10	0
9/1/2024	04:00	0	0	0	0	0	1	5	0	0	0	0	0	0	0	0	0	0	0	6	40.2	42	33 to 43	100.0	*	*	0	6	0
9/1/2024	05:00	0	0	0	0	1	2	1	1	0	0	0	0	0	0	0	0	0	0	5	38.4	43	36 to 46	80.0	1	20.0	0	5	0
9/1/2024	06:00	0	0	0	0	0	2	2	1	0	1	0	0	0	0	0	0	0	0	6	43.7	45	35 to 45	83.3	1	16.7	0	6	0
9/1/2024	07:00	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	3	33.7	42	22 to 32	66.7	*	*	0	3	0
9/1/2024	08:00	0	0	0	1	1	1	1	1	0	0	0	0	0	0	0	0	0	0	5	37.4	42	27 to 37	60.0	1	20.0	0	5	0
9/1/2024	09:00	0	0	1	0	1	11	8	2	1	0	2	0	0	0	0	0	0	0	26	41	46	34 to 44	76.9	5	19.2	0	25	1
9/1/2024	10:00	0	0	0	0	3	7	10	5	1	0	0	0	0	0	0	0	0	0	26	40.6	46	33 to 43	73.1	5	19.2	0	26	0
9/1/2024	11:00	0	0	1	0	3	11	12	5	0	0	0	0	0	0	0	0	0	0	32	39.7	44	36 to 46	81.3	3	9.4	0	31	1
9/1/2024	12:00	0	0	1	2	5	13	10	3	2	0	0	0	0	0	0	0	0	0	36	38.3	44	32 to 42	72.2	4	11.1	0	35	1
9/1/2024	13:00	0	0	0	1	9	20	21	4	0	0	0	0	0	0	0	0	0	0	55	38.7	43.2	34 to 44	78.2	3	5.5	0	53	2
9/1/2024	14:00	0	0	0	1	8	27	27	9	2	0	0	0	0	0	0	0	0	0	74	39.8	44	36 to 46	82.4	9	12.2	0	73	1
9/1/2024	15:00	0	0	1	1	16	37	26	14	2	1	0	0	0	0	0	0	0	0	98	39.2	44.2	35 to 45	73.5	8	8.2	0	95	3
9/1/2024	16:00	0	0	1	3	15	50	38	13	1	0	0	0	0	0	0	0	0	0	121	38.7	43	34 to 44	76.9	10	8.3	0	115	6
9/1/2024	17:00	0	0	0	4	13	25	34	10	0	0	0	0	0	0	0	0	0	0	86	38.8	43.5	34 to 44	76.7	4	4.7	0	86	0
9/1/2024	18:00	0	0	0	0	14	25	19	14	2	0	0	0	0	0	0	0	0	0	74	39.5	44.8	35 to 45	67.6	10	13.5	0	69	5
9/1/2024	19:00	0	0	0	3	12	20	30	5	2	1	0	0	0	0	0	0	0	0	73	39.1	43	34 to 44	75.3	7	9.6	0	71	2
9/1/2024	20:00	0	0	0	1	12	26	19	9	3	0	0	0	0	0	0	0	0	0	70	39.5	44.3	35 to 45	72.9	6	8.6	0	66	4
9/1/2024	21:00	0	0	0	1	8	35	25	6	1	0	0	0	0	0	0	0	0	0	76	38.9	43.2	35 to 45	84.2	3	3.9	0	75	1
9/1/2024	22:00	0	0	1	0	8	22	17	11	1	0	0	0	0	0	0	0	0	0	60	39.5	44.8	32 to 42	71.7	8	13.3	0	59	1
9/1/2024	23:00	0	0	1	0	7	18	10	7	0	0	0	0	0	0	0	0	0	0	43	38.4	44.3	32 to 42	81.4	4	9.3	0	41	2
24 Hr Summary		0	0	7	21	155	384	345	127	20	3	2	0	0	0	0	0	0	0	1064	39.1	44	34 to 44	73.6	98	9.2	1	1030	33

Outgoing Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG		
9/2/2024	00:00	0	0	0	1	4	10	8	3	1	0	0	0	0	0	0	0	0	0	27	39.4	44	33 to 43	74.1	4	14.8	0	27	0		
9/2/2024	01:00	0	0	0	1	1	13	9	0	0	0	0	0	0	0	0	0	0	0	24	38.3	40.5	34 to 44	95.8	*	*	0	23	1		
9/2/2024	02:00	0	0	0	0	3	2	5	2	0	0	1	0	0	0	0	0	0	0	13	41.5	46	33 to 43	69.2	3	23.1	0	13	0		
9/2/2024	03:00	0	0	1	0	1	2	1	3	0	0	0	0	0	0	0	0	0	0	8	38.1	45	35 to 45	62.5	1	12.5	0	8	0		
9/2/2024	04:00	0	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3	38	40	30 to 40	100.0	*	*	0	3	0		
9/2/2024	05:00	0	0	0	1	3	2	0	0	0	0	0	0	0	0	0	0	0	0	6	33.5	36	29 to 39	100.0	*	*	0	6	0		
9/2/2024	06:00	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3	36	38	28 to 38	100.0	*	*	0	3	0		
9/2/2024	07:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	36.5	37	27 to 37	100.0	*	*	0	2	0		
9/2/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/2/2024	09:00	0	0	0	0	1	6	3	1	0	0	0	0	0	0	0	0	0	0	11	38.1	40	31 to 41	90.9	*	*	0	11	0		
9/2/2024	10:00	0	1	0	1	1	7	1	1	0	0	0	0	0	0	0	0	0	0	12	35.7	39	29 to 39	75.0	*	*	0	11	1		
9/2/2024	11:00	0	0	0	1	7	14	16	1	0	0	0	0	0	0	0	0	0	0	39	37.8	41.7	32 to 42	79.5	*	*	0	38	1		
9/2/2024	12:00	0	0	0	3	8	22	14	7	1	0	0	0	0	0	0	0	0	0	55	38.6	44	33 to 43	74.5	7	12.7	0	55	0		
9/2/2024	13:00	0	0	0	2	9	29	25	3	0	0	0	0	0	0	0	0	0	0	68	38	40.9	31 to 41	83.8	2	2.9	0	66	2		
9/2/2024	14:00	0	0	0	3	8	37	29	10	0	1	0	0	0	0	0	0	0	0	88	39.2	43.5	35 to 45	83.0	4	4.5	0	83	5		
9/2/2024	15:00	0	0	0	1	8	27	22	8	1	1	0	0	0	0	0	0	0	0	68	39.6	44	35 to 45	79.4	5	7.4	0	67	1		
9/2/2024	16:00	0	0	0	3	14	27	25	6	0	0	0	0	0	0	0	0	0	0	75	38.2	42.6	33 to 43	77.3	3	4.0	0	65	10		
9/2/2024	17:00	0	0	0	1	17	38	29	8	1	0	0	0	0	0	0	0	0	0	94	38.4	42	32 to 42	77.7	7	7.4	0	91	3		
9/2/2024	18:00	0	0	0	2	8	33	37	10	1	0	0	1	0	0	0	0	0	0	92	39.9	43.5	34 to 44	81.5	8	8.7	0	90	2		
9/2/2024	19:00	0	0	1	0	11	24	30	10	1	1	0	0	0	0	0	0	0	0	78	39.6	44	36 to 46	79.5	10	12.8	0	73	5		
9/2/2024	20:00	0	0	1	2	5	17	32	16	4	0	0	0	0	0	0	0	0	0	77	40.8	45.3	35 to 45	72.7	13	16.9	0	74	3		
9/2/2024	21:00	0	0	0	2	16	39	30	7	1	1	0	0	0	0	0	0	0	0	96	38.5	41.9	32 to 42	78.1	6	6.3	0	90	6		
9/2/2024	22:00	0	2	0	1	8	20	25	10	0	0	0	0	0	0	0	0	0	0	66	38.9	44	34 to 44	72.7	7	10.6	1	64	1		
9/2/2024	23:00	0	0	0	4	8	20	18	9	1	1	0	0	0	0	0	0	0	0	61	39.1	44.5	36 to 46	70.5	7	11.5	0	60	1		
24 Hr Summary		0	3	3	29	142	395	360	115	12	5	1	1	0	0	0	0	0	0	1066	38.9	44	35 to 45	75.2	87	8.2	1	1023	42		

Outgoing Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/3/2024	00:00	0	0	0	0	1	10	5	0	0	0	0	0	0	0	0	0	0	0	16	38.4	43	34 to 44	93.8	*	*	0	16	0	
9/3/2024	01:00	0	0	2	3	3	5	4	2	0	0	0	0	0	0	0	0	0	0	19	35.2	41.5	29 to 39	57.9	1	5.3	0	18	1	
9/3/2024	02:00	0	0	1	0	1	0	3	2	0	0	0	0	0	0	0	0	0	0	7	38.6	45	36 to 46	71.4	1	14.3	0	6	1	
9/3/2024	03:00	0	0	0	0	1	1	4	1	0	0	0	0	0	0	0	0	0	0	7	40.7	43	33 to 43	71.4	1	14.3	0	7	0	
9/3/2024	04:00	0	0	0	1	0	0	3	0	0	0	0	0	0	0	0	0	0	0	4	39.3	43	34 to 44	75.0	*	*	0	4	0	
9/3/2024	05:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	39	40	30 to 40	100.0	*	*	0	2	0	
9/3/2024	06:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	35.5	37	27 to 37	100.0	*	*	0	2	0	
9/3/2024	07:00	0	0	0	0	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3	43	54	28 to 38	66.7	1	33.3	0	2	1	
9/3/2024	08:00	0	0	0	0	2	2	7	5	0	0	0	0	0	0	0	0	0	0	16	42.3	47.5	39 to 49	87.5	3	18.8	0	16	0	
9/3/2024	09:00	0	0	0	1	2	13	8	3	0	0	0	0	0	0	0	0	0	0	27	38.6	42.8	33 to 43	85.2	2	7.4	0	25	2	
9/3/2024	10:00	0	0	0	2	8	17	10	6	0	0	0	0	0	0	0	0	0	0	43	38.1	43	32 to 42	67.4	5	11.6	0	40	3	
9/3/2024	11:00	0	0	0	3	10	20	29	11	1	0	0	0	0	0	0	0	0	0	74	39.4	44.2	36 to 46	74.3	7	9.5	0	66	8	
9/3/2024	12:00	0	0	1	3	13	30	25	6	0	0	0	0	0	0	0	0	0	0	78	37.7	41.8	33 to 43	75.6	4	5.1	0	68	10	
9/3/2024	13:00	0	0	0	1	6	18	18	2	0	0	0	0	0	0	0	0	0	0	45	38.3	41	34 to 44	88.9	1	2.2	1	37	7	
9/3/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	4	14	48	120	117	38	2	0	0	0	0	0	0	0	0	0	343	38.5	44	35 to 45	73.2	26	7.6	1	309	33	

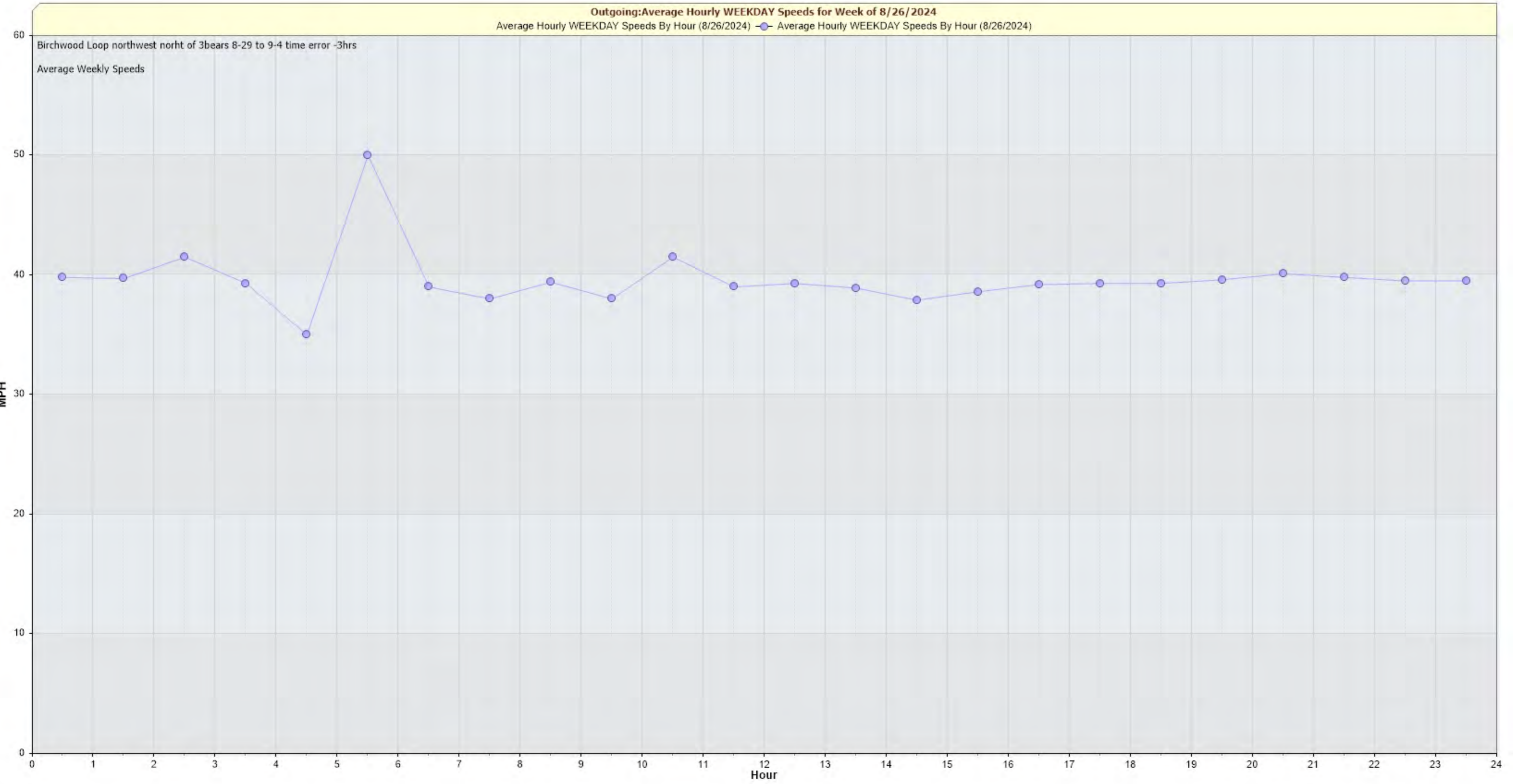
Outgoing: Average Hourly Volume for Week of 8/26/2024
Average Counts By Hour (8/26/2024) — Average Counts By Hour (8/26/2024)

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 8/26/2024
Average Hourly WEEKDAY Speeds By Hour (8/26/2024) — Average Hourly WEEKDAY Speeds By Hour (8/26/2024)

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs
Average Weekly Speeds

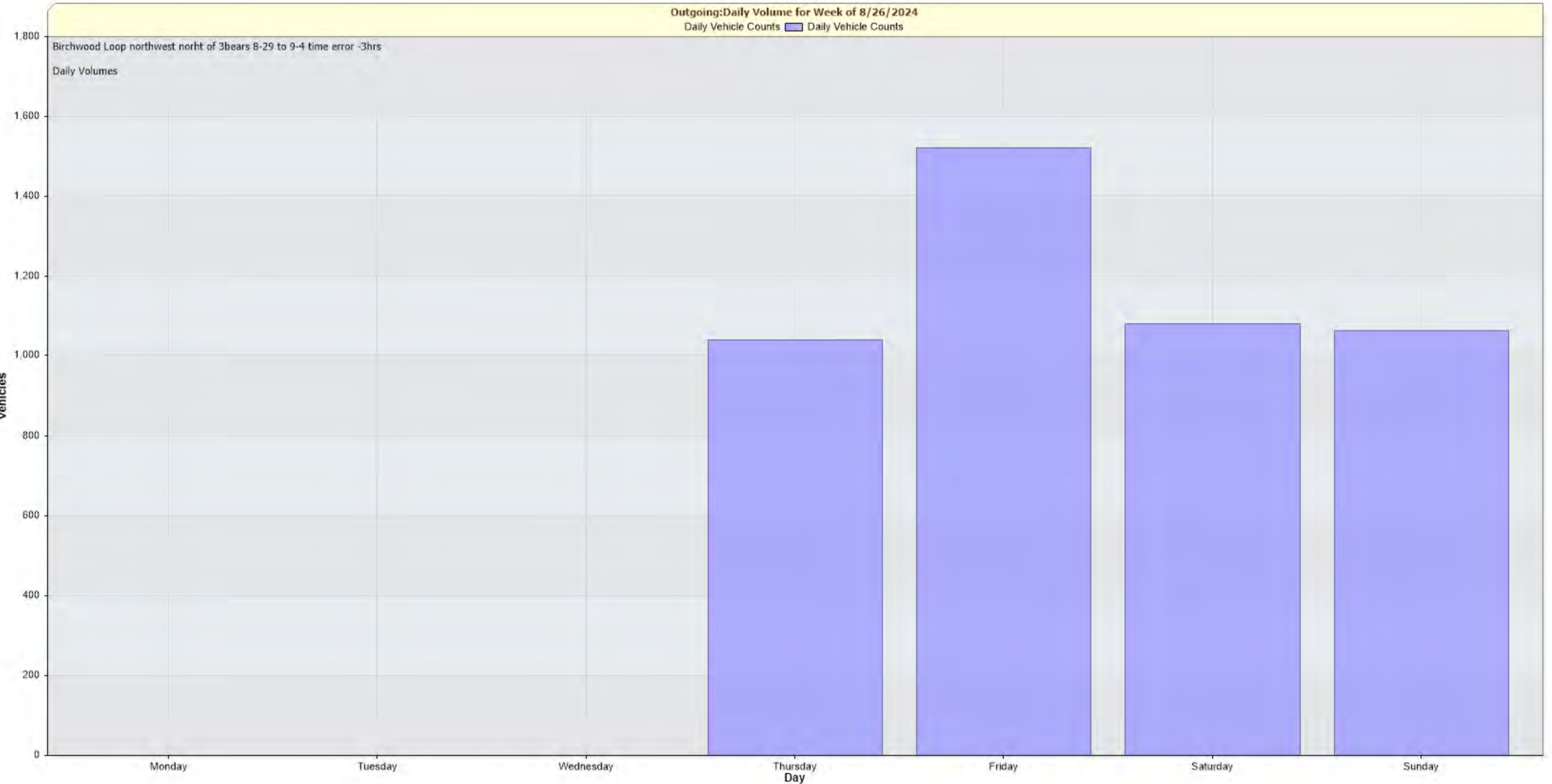


Outgoing: Daily Volume for Week of 8/26/2024

Daily Vehicle Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

Daily Volumes



For Project: N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:38:47 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 8/29/2024 2:00:00 PM through 9/4/2024 1:59:59 PM

85th Percentile Speed 37 MPH

85th Percentile Vehicles 10926

Max Speed 62 MPH on 9/1/2024 10:45:40 AM

Total Vehicles 12854

AADT: 2142

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1791	1836
AM Peak 11:00 AM	115	100
PM Peak 9:00 PM	212	190

Speed

Speed Limit: 45

85th Percentile Speed: 37

50th Percentile Speed: 31

10 MPH Pace Interval: 29.0 MPH to 39.0 MPH

Average Speed: 29.17

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	3	10	3	10	9	4	9
% over limit	0.2	0.4	0.5	0.6	0.4	0.2	0.5
Avg Speeder	47.7	47.8	49.0	46.5	48.1	48.0	49.9
Avg Speed	28.3	29.7	28.0	29.8	29.7	29.0	28.7

Class Counts

	Number	%
VEH_SM	1	0
VEH_MED	12090	94.1
VEH_LG	763	5.9
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Incoming Summary
 N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/29/2024 3:00:00 PM	37.0	15	18	40	0.0	0.0%	28.1
8/29/2024 4:00:00 PM	36.0	116	137	46	46.0	0.7%	28.4
8/29/2024 5:00:00 PM	38.0	134	158	46	46.0	0.6%	30.1
8/29/2024 6:00:00 PM	37.0	129	152	46	46.0	0.7%	30.6
8/29/2024 7:00:00 PM	36.0	156	183	45	0.0	0.0%	29.5
8/29/2024 8:00:00 PM	37.0	144	169	44	0.0	0.0%	29.8
8/29/2024 9:00:00 PM	37.0	184	216	46	46.0	0.5%	29.8
8/29/2024 10:00:00 PM	37.0	176	207	49	47.5	1.0%	29.8
8/29/2024 11:00:00 PM	38.0	156	183	44	0.0	0.0%	29.5
8/30/2024 12:00:00 AM	39.0	116	136	48	46.5	2.9%	30.5
8/30/2024 1:00:00 AM	36.0	88	103	42	0.0	0.0%	28.0
8/30/2024 2:00:00 AM	37.0	54	64	40	0.0	0.0%	28.5
8/30/2024 3:00:00 AM	36.0	37	44	45	0.0	0.0%	27.9
8/30/2024 4:00:00 AM	34.0	20	24	41	0.0	0.0%	27.0
8/30/2024 5:00:00 AM	36.0	8	10	49	49.0	10.0%	29.3
8/30/2024 6:00:00 AM	19.0	2	2	19	0.0	0.0%	18.5
8/30/2024 7:00:00 AM	27.0	3	4	28	0.0	0.0%	23.3
8/30/2024 8:00:00 AM	26.0	4	5	34	0.0	0.0%	23.8
8/30/2024 9:00:00 AM	34.0	8	9	37	0.0	0.0%	23.1
8/30/2024 10:00:00 AM	37.0	16	19	41	0.0	0.0%	26.0
8/30/2024 11:00:00 AM	36.0	41	48	44	0.0	0.0%	27.4
8/30/2024 12:00:00 PM	37.0	118	139	45	0.0	0.0%	28.5
8/30/2024 1:00:00 PM	37.0	103	121	42	0.0	0.0%	29.4
8/30/2024 2:00:00 PM	39.0	109	128	46	46.0	0.8%	30.2
8/30/2024 3:00:00 PM	37.0	109	128	45	0.0	0.0%	30.0
8/30/2024 4:00:00 PM	38.0	130	153	46	46.0	0.7%	31.0
8/30/2024 5:00:00 PM	37.0	130	153	43	0.0	0.0%	29.3
8/30/2024 6:00:00 PM	36.0	179	211	45	0.0	0.0%	29.9
8/30/2024 7:00:00 PM	37.0	162	190	41	0.0	0.0%	30.4
8/30/2024 8:00:00 PM	37.0	190	223	44	0.0	0.0%	30.1
8/30/2024 9:00:00 PM	37.0	205	241	47	47.0	0.4%	30.1
8/30/2024 10:00:00 PM	38.0	206	242	53	49.3	1.7%	31.3
8/30/2024 11:00:00 PM	37.0	139	163	48	48.0	0.6%	29.3
8/31/2024 12:00:00 AM	37.0	105	123	44	0.0	0.0%	28.9
8/31/2024 1:00:00 AM	36.0	99	116	45	0.0	0.0%	28.0
8/31/2024 2:00:00 AM	35.0	69	81	43	0.0	0.0%	27.0
8/31/2024 3:00:00 AM	36.0	37	43	41	0.0	0.0%	27.3
8/31/2024 4:00:00 AM	36.0	20	24	45	0.0	0.0%	28.0
8/31/2024 5:00:00 AM	38.0	11	13	39	0.0	0.0%	29.5
8/31/2024 6:00:00 AM	33.0	4	5	35	0.0	0.0%	27.0
8/31/2024 7:00:00 AM	36.0	8	9	40	0.0	0.0%	27.0
8/31/2024 8:00:00 AM	32.0	5	6	33	0.0	0.0%	26.3
8/31/2024 9:00:00 AM	28.0	5	6	32	0.0	0.0%	24.3
8/31/2024 10:00:00 AM	32.0	5	6	37	0.0	0.0%	30.5
8/31/2024 11:00:00 AM	35.0	16	19	39	0.0	0.0%	29.9
8/31/2024 12:00:00 PM	36.0	32	38	42	0.0	0.0%	28.7
8/31/2024 1:00:00 PM	36.0	48	57	49	49.0	1.8%	29.1

Incoming Summary
 N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

8/31/2024 2:00:00 PM	36.0	82	96	42	0.0	0.0%	29.2
8/31/2024 3:00:00 PM	37.0	96	113	47	47.0	0.9%	29.4
8/31/2024 4:00:00 PM	38.0	121	142	45	0.0	0.0%	30.1
8/31/2024 5:00:00 PM	37.0	128	151	44	0.0	0.0%	29.4
8/31/2024 6:00:00 PM	37.0	135	159	48	48.0	0.6%	30.0
8/31/2024 7:00:00 PM	37.0	133	156	44	0.0	0.0%	29.4
8/31/2024 8:00:00 PM	37.0	117	138	43	0.0	0.0%	29.2
8/31/2024 9:00:00 PM	36.0	131	154	44	0.0	0.0%	28.2
8/31/2024 10:00:00 PM	37.0	125	147	45	0.0	0.0%	28.6
8/31/2024 11:00:00 PM	37.0	111	131	45	0.0	0.0%	29.9
9/1/2024 12:00:00 AM	36.0	109	128	48	48.0	0.8%	28.3
9/1/2024 1:00:00 AM	37.0	78	92	49	48.0	2.2%	28.5
9/1/2024 2:00:00 AM	36.0	48	56	43	0.0	0.0%	28.3
9/1/2024 3:00:00 AM	35.0	31	37	45	0.0	0.0%	28.1
9/1/2024 4:00:00 AM	35.0	17	20	41	0.0	0.0%	26.3
9/1/2024 5:00:00 AM	34.0	8	10	36	0.0	0.0%	27.7
9/1/2024 6:00:00 AM	34.0	8	9	37	0.0	0.0%	25.2
9/1/2024 7:00:00 AM	33.0	3	3	33	0.0	0.0%	28.3
9/1/2024 8:00:00 AM	38.0	9	11	41	0.0	0.0%	32.5
9/1/2024 9:00:00 AM	33.0	8	10	36	0.0	0.0%	26.3
9/1/2024 10:00:00 AM	33.0	11	13	42	0.0	0.0%	25.8
9/1/2024 11:00:00 AM	41.0	17	20	62	58.0	10.0%	34.4
9/1/2024 12:00:00 PM	37.0	46	54	48	48.0	1.9%	28.8
9/1/2024 1:00:00 PM	38.0	79	93	44	0.0	0.0%	28.6
9/1/2024 2:00:00 PM	36.0	130	153	49	49.0	0.7%	28.4
9/1/2024 3:00:00 PM	36.0	121	142	44	0.0	0.0%	28.6
9/1/2024 4:00:00 PM	36.0	127	149	42	0.0	0.0%	28.9
9/1/2024 5:00:00 PM	37.0	119	140	47	47.0	0.7%	28.7
9/1/2024 6:00:00 PM	37.0	150	176	44	0.0	0.0%	30.0
9/1/2024 7:00:00 PM	37.0	113	133	45	0.0	0.0%	29.9
9/1/2024 8:00:00 PM	38.0	103	121	46	46.0	0.8%	29.8
9/1/2024 9:00:00 PM	37.0	122	144	47	47.0	0.7%	28.7
9/1/2024 10:00:00 PM	36.0	122	143	44	0.0	0.0%	27.8
9/1/2024 11:00:00 PM	36.0	109	128	42	0.0	0.0%	27.7
9/2/2024 12:00:00 AM	35.0	85	100	45	0.0	0.0%	27.1
9/2/2024 1:00:00 AM	36.0	81	95	44	0.0	0.0%	27.1
9/2/2024 2:00:00 AM	36.0	37	44	45	0.0	0.0%	25.7
9/2/2024 3:00:00 AM	31.0	37	44	36	0.0	0.0%	23.3
9/2/2024 4:00:00 AM	36.0	24	28	40	0.0	0.0%	27.3
9/2/2024 5:00:00 AM	32.0	9	11	45	0.0	0.0%	26.1
9/2/2024 6:00:00 AM	38.0	2	2	38	0.0	0.0%	35.0
9/2/2024 7:00:00 AM	35.0	3	3	35	0.0	0.0%	28.7
9/2/2024 8:00:00 AM	28.0	3	4	29	0.0	0.0%	27.0
9/2/2024 9:00:00 AM	28.0	4	5	32	0.0	0.0%	23.4
9/2/2024 10:00:00 AM	34.0	6	7	35	0.0	0.0%	23.0
9/2/2024 11:00:00 AM	34.0	20	24	42	0.0	0.0%	28.2
9/2/2024 12:00:00 PM	39.0	17	20	42	0.0	0.0%	29.4
9/2/2024 1:00:00 PM	38.0	38	45	43	0.0	0.0%	30.4

Incoming Summary
 N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

9/2/2024 2:00:00 PM	37.0	60	70	45	0.0	0.0%	28.4
9/2/2024 3:00:00 PM	36.0	96	113	44	0.0	0.0%	29.0
9/2/2024 4:00:00 PM	36.0	108	127	44	0.0	0.0%	29.6
9/2/2024 5:00:00 PM	38.0	118	139	47	47.0	0.7%	31.4
9/2/2024 6:00:00 PM	35.0	133	157	43	0.0	0.0%	28.0
9/2/2024 7:00:00 PM	37.0	134	158	48	48.0	0.6%	28.6
9/2/2024 8:00:00 PM	36.0	144	169	44	0.0	0.0%	28.6
9/2/2024 9:00:00 PM	36.0	133	157	43	0.0	0.0%	28.0
9/2/2024 10:00:00 PM	37.0	118	139	48	48.0	0.7%	27.3
9/2/2024 11:00:00 PM	37.0	114	134	45	0.0	0.0%	27.4
9/3/2024 12:00:00 AM	36.0	88	103	45	0.0	0.0%	27.7
9/3/2024 1:00:00 AM	36.0	68	80	47	47.0	1.3%	27.6
9/3/2024 2:00:00 AM	34.0	31	37	45	0.0	0.0%	27.6
9/3/2024 3:00:00 AM	33.0	30	35	40	0.0	0.0%	26.3
9/3/2024 4:00:00 AM	36.0	14	16	38	0.0	0.0%	27.1
9/3/2024 5:00:00 AM	38.0	8	10	40	0.0	0.0%	30.9
9/3/2024 6:00:00 AM	29.0	3	4	30	0.0	0.0%	26.5
9/3/2024 7:00:00 AM	33.0	5	6	35	0.0	0.0%	27.7
9/3/2024 8:00:00 AM	30.0	5	6	35	0.0	0.0%	24.3
9/3/2024 9:00:00 AM	32.0	8	10	36	0.0	0.0%	27.4
9/3/2024 10:00:00 AM	36.0	25	29	43	0.0	0.0%	26.5
9/3/2024 11:00:00 AM	37.0	43	51	46	46.0	2.0%	30.1
9/3/2024 12:00:00 PM	37.0	130	153	45	0.0	0.0%	28.6
9/3/2024 1:00:00 PM	37.0	99	117	46	46.0	0.9%	29.6
9/3/2024 2:00:00 PM	36.0	118	139	43	0.0	0.0%	29.4
9/3/2024 3:00:00 PM	36.0	110	130	44	0.0	0.0%	28.8
9/3/2024 4:00:00 PM	38.0	92	108	49	49.0	0.9%	31.2
9/3/2024 5:00:00 PM	37.0	130	153	43	0.0	0.0%	29.4
9/3/2024 6:00:00 PM	37.0	138	162	45	0.0	0.0%	30.3
9/3/2024 7:00:00 PM	37.0	167	197	45	0.0	0.0%	30.2
9/3/2024 8:00:00 PM	37.0	177	208	48	47.0	1.0%	31.0
9/3/2024 9:00:00 PM	37.0	194	228	46	46.0	0.4%	30.1
9/3/2024 10:00:00 PM	38.0	222	261	55	50.5	0.8%	30.7
9/3/2024 11:00:00 PM	37.0	157	185	49	49.0	0.5%	31.1
9/4/2024 12:00:00 AM	35.0	110	130	40	0.0	0.0%	28.3
9/4/2024 1:00:00 AM	37.0	98	115	44	0.0	0.0%	29.8
9/4/2024 2:00:00 AM	33.0	44	52	50	50.0	1.9%	27.8
9/4/2024 3:00:00 AM	38.0	25	29	50	50.0	3.4%	30.8
9/4/2024 4:00:00 AM	37.0	14	16	41	0.0	0.0%	29.7
9/4/2024 5:00:00 AM	36.0	6	7	41	0.0	0.0%	28.3
9/4/2024 6:00:00 AM	16.0	1	1	16	0.0	0.0%	16.0
9/4/2024 7:00:00 AM	30.0	3	3	30	0.0	0.0%	28.0
9/4/2024 8:00:00 AM	32.0	2	2	32	0.0	0.0%	24.0
9/4/2024 9:00:00 AM	35.0	9	11	36	0.0	0.0%	26.4
9/4/2024 10:00:00 AM	34.0	22	26	42	0.0	0.0%	24.8
9/4/2024 11:00:00 AM	36.0	40	47	47	47.0	2.1%	27.0
9/4/2024 12:00:00 PM	35.0	127	149	43	0.0	0.0%	27.6
9/4/2024 1:00:00 PM	35.0	94	111	45	0.0	0.0%	27.4

Incoming Summary

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

9/4/2024 2:00:00 PM	35.0	26	31	42	0.0	0.0%	27.1
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Incoming Summary

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/30/2024 12:00:00 AM	37.0	1325	1559	49	46.5	0.6%	29.8
8/31/2024 12:00:00 AM	37.0	2165	2547	53	48.1	0.4%	29.7
9/1/2024 12:00:00 AM	37.0	1647	1938	49	48.0	0.2%	29.0
9/2/2024 12:00:00 AM	37.0	1663	1957	62	49.9	0.5%	28.7
9/3/2024 12:00:00 AM	37.0	1528	1798	48	47.7	0.2%	28.3
9/4/2024 12:00:00 AM	37.0	2087	2455	55	47.8	0.4%	29.7
9/4/2024 1:59:59 PM	36.0	510	600	50	49.0	0.5%	28.0

Incoming Weekly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	103	116	92	103	104	35.8
1 - 2	*	*	*	*	64	81	56	64	68.5	36.7
2 - 3	*	*	*	*	44	43	37	44	40	35.8
3 - 4	*	*	*	*	24	24	20	24	22	34
4 - 5	*	*	*	*	10	13	10	10	11.5	36
5 - 6	*	*	*	*	2	5	9	2	7	19
6 - 7	*	*	*	*	4	9	3	4	6	27
7 - 8	*	*	*	*	5	6	11	5	8.5	26
8 - 9	*	*	*	*	9	6	10	9	8	34
9 - 10	*	*	*	*	19	6	13	19	9.5	36.5
10 - 11	*	*	*	*	48	19	20	48	19.5	36
11 - 12	*	*	*	*	139	38	54	139	46	36.2
12 - 13	*	*	*	*	121	57	93	121	75	36.8
13 - 14	*	*	*	*	128	96	153	128	124.5	38.2
14 - 15	*	*	*	18	128	113	142	73	127.5	36.6
15 - 16	*	*	*	137	153	142	149	145	145.5	37
16 - 17	*	*	*	158	153	151	140	155.5	145.5	36.85
17 - 18	*	*	*	152	211	159	176	181.5	167.5	36.5
18 - 19	*	*	*	183	190	156	133	186.5	144.5	36.1
19 - 20	*	*	*	169	223	138	121	196	129.5	36.8
20 - 21	*	*	*	216	241	154	144	228.5	149	36.8
21 - 22	*	*	*	207	242	147	143	224.5	145	37.2
22 - 23	*	*	*	183	163	131	128	173	129.5	37
23 - 24	*	*	*	136	123	128	100	129.5	114	37.9
Totals	0	0	0	1559	2547	1938	1957			
% of Total	0%	0%	0%	19.49%	31.83%	24.22%	24.46%			

Incoming Weekly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	95	80	115	*	*	*	*	96.67	0	36.23
1 - 2	44	37	52	*	*	*	*	44.33	0	34.17
2 - 3	44	35	29	*	*	*	*	36	0	33.5
3 - 4	28	16	16	*	*	*	*	20	0	36.17
4 - 5	11	10	7	*	*	*	*	9.33	0	35.33
5 - 6	2	4	1	*	*	*	*	2.33	0	27.67
6 - 7	3	6	3	*	*	*	*	4	0	32.67
7 - 8	4	6	2	*	*	*	*	4	0	30
8 - 9	5	10	11	*	*	*	*	8.67	0	31.67
9 - 10	7	29	26	*	*	*	*	20.67	0	34.67
10 - 11	24	51	47	*	*	*	*	40.67	0	35.5
11 - 12	20	153	149	*	*	*	*	107.33	0	36.8
12 - 13	45	117	111	*	*	*	*	91	0	36.57
13 - 14	70	139	31	*	*	*	*	80	0	35.73
14 - 15	113	130	*	*	*	*	*	121.5	0	35.75
15 - 16	127	108	*	*	*	*	*	117.5	0	36.85
16 - 17	139	153	*	*	*	*	*	146	0	36.75
17 - 18	157	162	*	*	*	*	*	159.5	0	35.9
18 - 19	158	197	*	*	*	*	*	177.5	0	36.2
19 - 20	169	208	*	*	*	*	*	188.5	0	36.4
20 - 21	157	228	*	*	*	*	*	192.5	0	35.95
21 - 22	139	261	*	*	*	*	*	200	0	36.85
22 - 23	134	185	*	*	*	*	*	159.5	0	36.6
23 - 24	103	130	*	*	*	*	*	116.5	0	35.45
Totals	1798	2455	600	0	0	0	0			
% of Total	37.05%	50.59%	12.36%	0%	0%	0%	0%			

Incoming Monthly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	103	116	*	103	116	35.8
1 - 2	*	*	*	*	64	81	*	64	81	36.7
2 - 3	*	*	*	*	44	43	*	44	43	35.8
3 - 4	*	*	*	*	24	24	*	24	24	34
4 - 5	*	*	*	*	10	13	*	10	13	36
5 - 6	*	*	*	*	2	5	*	2	5	19
6 - 7	*	*	*	*	4	9	*	4	9	27
7 - 8	*	*	*	*	5	6	*	5	6	26
8 - 9	*	*	*	*	9	6	*	9	6	34
9 - 10	*	*	*	*	19	6	*	19	6	36.5
10 - 11	*	*	*	*	48	19	*	48	19	36
11 - 12	*	*	*	*	139	38	*	139	38	36.2
12 - 13	*	*	*	*	121	57	*	121	57	36.8
13 - 14	*	*	*	*	128	96	*	128	96	38.2
14 - 15	*	*	*	18	128	113	*	73	113	36.6
15 - 16	*	*	*	137	153	142	*	145	142	37
16 - 17	*	*	*	158	153	151	*	155.5	151	36.85
17 - 18	*	*	*	152	211	159	*	181.5	159	36.5
18 - 19	*	*	*	183	190	156	*	186.5	156	36.1
19 - 20	*	*	*	169	223	138	*	196	138	36.8
20 - 21	*	*	*	216	241	154	*	228.5	154	36.8
21 - 22	*	*	*	207	242	147	*	224.5	147	37.2
22 - 23	*	*	*	183	163	131	*	173	131	37
23 - 24	*	*	*	136	123	128	*	129.5	128	37.9
Totals	0	0	0	1559	2547	1938	0			
% of Total	0%	0%	0%	25.79%	42.14%	32.06%	0%			

Incoming Monthly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	95	80	115	*	*	*	92	96.67	92	36.23
1 - 2	44	37	52	*	*	*	56	44.33	56	34.17
2 - 3	44	35	29	*	*	*	37	36	37	33.5
3 - 4	28	16	16	*	*	*	20	20	20	36.17
4 - 5	11	10	7	*	*	*	10	9.33	10	35.33
5 - 6	2	4	1	*	*	*	9	2.33	9	27.67
6 - 7	3	6	3	*	*	*	3	4	3	32.67
7 - 8	4	6	2	*	*	*	11	4	11	30
8 - 9	5	10	11	*	*	*	10	8.67	10	31.67
9 - 10	7	29	26	*	*	*	13	20.67	13	34.67
10 - 11	24	51	47	*	*	*	20	40.67	20	35.5
11 - 12	20	153	149	*	*	*	54	107.33	54	36.8
12 - 13	45	117	111	*	*	*	93	91	93	36.57
13 - 14	70	139	31	*	*	*	153	80	153	35.73
14 - 15	113	130	*	*	*	*	142	121.5	142	35.75
15 - 16	127	108	*	*	*	*	149	117.5	149	36.85
16 - 17	139	153	*	*	*	*	140	146	140	36.75
17 - 18	157	162	*	*	*	*	176	159.5	176	35.9
18 - 19	158	197	*	*	*	*	133	177.5	133	36.2
19 - 20	169	208	*	*	*	*	121	188.5	121	36.4
20 - 21	157	228	*	*	*	*	144	192.5	144	35.95
21 - 22	139	261	*	*	*	*	143	200	143	36.85
22 - 23	134	185	*	*	*	*	128	159.5	128	36.6
23 - 24	103	130	*	*	*	*	100	116.5	100	35.45
Totals	1798	2455	600	0	0	0	1957			
% of Total	26.4%	36.05%	8.81%	0%	0%	0%	28.74%			

Incoming Weekly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	28	28.04	28.48	28	28.24	35.8
1 - 2	*	*	*	*	28.47	27.05	28.32	28.47	27.57	36.7
2 - 3	*	*	*	*	27.91	27.26	28.14	27.91	27.66	35.8
3 - 4	*	*	*	*	27	28	26.3	27	27.23	34
4 - 5	*	*	*	*	29.3	29.54	27.7	29.3	28.74	36
5 - 6	*	*	*	*	18.5	27	25.22	18.5	25.86	19
6 - 7	*	*	*	*	23.25	27	28.33	23.25	27.33	27
7 - 8	*	*	*	*	23.8	26.33	32.45	23.8	30.29	26
8 - 9	*	*	*	*	23.11	24.33	26.3	23.11	25.56	34
9 - 10	*	*	*	*	26	30.5	25.85	26	27.32	36.5
10 - 11	*	*	*	*	27.42	29.89	34.4	27.42	32.21	36
11 - 12	*	*	*	*	28.49	28.71	28.76	28.49	28.74	36.2
12 - 13	*	*	*	*	29.4	29.05	28.65	29.4	28.8	36.8
13 - 14	*	*	*	*	30.21	29.19	28.39	30.21	28.7	38.2
14 - 15	*	*	*	28.17	30.01	29.4	28.56	29.78	28.93	36.6
15 - 16	*	*	*	28.42	31.03	30.11	28.91	29.8	29.49	37
16 - 17	*	*	*	30.15	29.28	29.4	28.74	29.72	29.08	36.85
17 - 18	*	*	*	30.61	29.92	30.01	29.99	30.21	30	36.5
18 - 19	*	*	*	29.55	30.42	29.36	29.89	29.99	29.61	36.1
19 - 20	*	*	*	29.75	30.06	29.21	29.82	29.93	29.49	36.8
20 - 21	*	*	*	29.81	30.05	28.21	28.65	29.94	28.42	36.8
21 - 22	*	*	*	29.83	31.31	28.59	27.84	30.63	28.22	37.2
22 - 23	*	*	*	29.51	29.33	29.92	27.74	29.42	28.85	37
23 - 24	*	*	*	30.49	28.92	28.34	27.1	29.74	27.8	37.9
Totals	0	0	0	29.8	29.7	29	28.7			
% of Total	0%	0%	0%	25.43%	25.34%	24.74%	24.49%			

Incoming Weekly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	27.13	27.62	29.83	*	*	*	*	28.33	*	36.23
1 - 2	25.66	27.62	27.81	*	*	*	*	27.05	*	34.17
2 - 3	23.25	26.34	30.76	*	*	*	*	26.27	*	33.5
3 - 4	27.29	27.06	29.69	*	*	*	*	27.87	*	36.17
4 - 5	26.09	30.9	28.29	*	*	*	*	28.36	*	35.33
5 - 6	35	26.5	16	*	*	*	*	27.43	*	27.67
6 - 7	28.67	27.67	28	*	*	*	*	28	*	32.67
7 - 8	27	24.33	24	*	*	*	*	25.17	*	30
8 - 9	23.4	27.4	26.36	*	*	*	*	26.19	*	31.67
9 - 10	23	26.52	24.81	*	*	*	*	25.4	*	34.67
10 - 11	28.21	30.1	26.98	*	*	*	*	28.52	*	35.5
11 - 12	29.4	28.63	27.6	*	*	*	*	28.2	*	36.8
12 - 13	30.36	29.6	27.39	*	*	*	*	28.82	*	36.57
13 - 14	28.36	29.39	27.06	*	*	*	*	28.79	*	35.73
14 - 15	29.01	28.81	*	*	*	*	*	28.9	*	35.75
15 - 16	29.56	31.19	*	*	*	*	*	30.31	*	36.85
16 - 17	31.38	29.44	*	*	*	*	*	30.36	*	36.75
17 - 18	28.03	30.3	*	*	*	*	*	29.18	*	35.9
18 - 19	28.56	30.19	*	*	*	*	*	29.46	*	36.2
19 - 20	28.6	30.96	*	*	*	*	*	29.9	*	36.4
20 - 21	28	30.12	*	*	*	*	*	29.26	*	35.95
21 - 22	27.32	30.65	*	*	*	*	*	29.49	*	36.85
22 - 23	27.44	31.08	*	*	*	*	*	29.55	*	36.6
23 - 24	27.73	28.27	*	*	*	*	*	28.03	*	35.45
Totals	28.3	29.8	28	0	0	0	0			
% of Total	32.87%	34.61%	32.52%	0%	0%	0%	0%			

Incoming Monthly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	28	28.04	*	28	28.04	35.8
1 - 2	*	*	*	*	28.47	27.05	*	28.47	27.05	36.7
2 - 3	*	*	*	*	27.91	27.26	*	27.91	27.26	35.8
3 - 4	*	*	*	*	27	28	*	27	28	34
4 - 5	*	*	*	*	29.3	29.54	*	29.3	29.54	36
5 - 6	*	*	*	*	18.5	27	*	18.5	27	19
6 - 7	*	*	*	*	23.25	27	*	23.25	27	27
7 - 8	*	*	*	*	23.8	26.33	*	23.8	26.33	26
8 - 9	*	*	*	*	23.11	24.33	*	23.11	24.33	34
9 - 10	*	*	*	*	26	30.5	*	26	30.5	36.5
10 - 11	*	*	*	*	27.42	29.89	*	27.42	29.89	36
11 - 12	*	*	*	*	28.49	28.71	*	28.49	28.71	36.2
12 - 13	*	*	*	*	29.4	29.05	*	29.4	29.05	36.8
13 - 14	*	*	*	*	30.21	29.19	*	30.21	29.19	38.2
14 - 15	*	*	*	28.17	30.01	29.4	*	29.78	29.4	36.6
15 - 16	*	*	*	28.42	31.03	30.11	*	29.8	30.11	37
16 - 17	*	*	*	30.15	29.28	29.4	*	29.72	29.4	36.85
17 - 18	*	*	*	30.61	29.92	30.01	*	30.21	30.01	36.5
18 - 19	*	*	*	29.55	30.42	29.36	*	29.99	29.36	36.1
19 - 20	*	*	*	29.75	30.06	29.21	*	29.93	29.21	36.8
20 - 21	*	*	*	29.81	30.05	28.21	*	29.94	28.21	36.8
21 - 22	*	*	*	29.83	31.31	28.59	*	30.63	28.59	37.2
22 - 23	*	*	*	29.51	29.33	29.92	*	29.42	29.92	37
23 - 24	*	*	*	30.49	28.92	28.34	*	29.74	28.34	37.9
Totals	0	0	0	29.8	29.7	29	0			
% of Total	0%	0%	0%	33.67%	33.56%	32.77%	0%			

Incoming Monthly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	27.13	27.62	29.83	*	*	*	28.48	28.33	28.48	36.23
1 - 2	25.66	27.62	27.81	*	*	*	28.32	27.05	28.32	34.17
2 - 3	23.25	26.34	30.76	*	*	*	28.14	26.27	28.14	33.5
3 - 4	27.29	27.06	29.69	*	*	*	26.3	27.87	26.3	36.17
4 - 5	26.09	30.9	28.29	*	*	*	27.7	28.36	27.7	35.33
5 - 6	35	26.5	16	*	*	*	25.22	27.43	25.22	27.67
6 - 7	28.67	27.67	28	*	*	*	28.33	28	28.33	32.67
7 - 8	27	24.33	24	*	*	*	32.45	25.17	32.45	30
8 - 9	23.4	27.4	26.36	*	*	*	26.3	26.19	26.3	31.67
9 - 10	23	26.52	24.81	*	*	*	25.85	25.4	25.85	34.67
10 - 11	28.21	30.1	26.98	*	*	*	34.4	28.52	34.4	35.5
11 - 12	29.4	28.63	27.6	*	*	*	28.76	28.2	28.76	36.8
12 - 13	30.36	29.6	27.39	*	*	*	28.65	28.82	28.65	36.57
13 - 14	28.36	29.39	27.06	*	*	*	28.39	28.79	28.39	35.73
14 - 15	29.01	28.81	*	*	*	*	28.56	28.9	28.56	35.75
15 - 16	29.56	31.19	*	*	*	*	28.91	30.31	28.91	36.85
16 - 17	31.38	29.44	*	*	*	*	28.74	30.36	28.74	36.75
17 - 18	28.03	30.3	*	*	*	*	29.99	29.18	29.99	35.9
18 - 19	28.56	30.19	*	*	*	*	29.89	29.46	29.89	36.2
19 - 20	28.6	30.96	*	*	*	*	29.82	29.9	29.82	36.4
20 - 21	28	30.12	*	*	*	*	28.65	29.26	28.65	35.95
21 - 22	27.32	30.65	*	*	*	*	27.84	29.49	27.84	36.85
22 - 23	27.44	31.08	*	*	*	*	27.74	29.55	27.74	36.6
23 - 24	27.73	28.27	*	*	*	*	27.1	28.03	27.1	35.45
Totals	28.3	29.8	28	0	0	0	28.7			
% of Total	24.65%	25.96%	24.39%	0%	0%	0%	25%			

Incoming Weekly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	35.8	36	37	35.8	36.5	35.8
1 - 2	*	*	*	*	36.7	35	36	36.7	35.5	36.7
2 - 3	*	*	*	*	35.8	35.7	35	35.8	35.35	35.8
3 - 4	*	*	*	*	34	36	35	34	35.5	34
4 - 5	*	*	*	*	36	37.5	34	36	35.75	36
5 - 6	*	*	*	*	19	33	34	19	33.5	19
6 - 7	*	*	*	*	27	36	33	27	34.5	27
7 - 8	*	*	*	*	26	32	38	26	35	26
8 - 9	*	*	*	*	34	28	33	34	30.5	34
9 - 10	*	*	*	*	36.5	32	33	36.5	32.5	36.5
10 - 11	*	*	*	*	36	35	41	36	38	36
11 - 12	*	*	*	*	36.2	35.7	36.7	36.2	36.2	36.2
12 - 13	*	*	*	*	36.8	36	37.3	36.8	36.65	36.8
13 - 14	*	*	*	*	38.2	36	35.3	38.2	35.65	38.2
14 - 15	*	*	*	36.5	36.7	36.7	36	36.6	36.35	36.6
15 - 16	*	*	*	36	38	37.4	36	37	36.7	37
16 - 17	*	*	*	37.2	36.5	36.8	36.4	36.85	36.6	36.85
17 - 18	*	*	*	37	36	36.8	36.6	36.5	36.7	36.5
18 - 19	*	*	*	35.8	36.4	36.4	37	36.1	36.7	36.1
19 - 20	*	*	*	37	36.6	36.3	37.5	36.8	36.9	36.8
20 - 21	*	*	*	36.7	36.9	35.8	36.7	36.8	36.25	36.8
21 - 22	*	*	*	36.8	37.6	36	35.9	37.2	35.95	37.2
22 - 23	*	*	*	37.2	36.8	37	35.3	37	36.15	37
23 - 24	*	*	*	39	36.8	35.8	35	37.9	35.4	37.9
Totals	0	0	0	369.2	836.3	848.9	860.7			
% of Total	0%	0%	0%	12.67%	28.69%	29.12%	29.53%			

Incoming Weekly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	36	36	36.7	*	*	*	*	36.23	0	36.23
1 - 2	35.5	34	33	*	*	*	*	34.17	0	34.17
2 - 3	30	33	37.5	*	*	*	*	33.5	0	33.5
3 - 4	36	36	36.5	*	*	*	*	36.17	0	36.17
4 - 5	32	38	36	*	*	*	*	35.33	0	35.33
5 - 6	38	29	16	*	*	*	*	27.67	0	27.67
6 - 7	35	33	30	*	*	*	*	32.67	0	32.67
7 - 8	28	30	32	*	*	*	*	30	0	30
8 - 9	28	32	35	*	*	*	*	31.67	0	31.67
9 - 10	34	36	34	*	*	*	*	34.67	0	34.67
10 - 11	34	37	35.5	*	*	*	*	35.5	0	35.5
11 - 12	39	36.7	34.7	*	*	*	*	36.8	0	36.8
12 - 13	37.7	37	35	*	*	*	*	36.57	0	36.57
13 - 14	36.6	35.9	34.7	*	*	*	*	35.73	0	35.73
14 - 15	35.9	35.6	*	*	*	*	*	35.75	0	35.75
15 - 16	36	37.7	*	*	*	*	*	36.85	0	36.85
16 - 17	37.2	36.3	*	*	*	*	*	36.75	0	36.75
17 - 18	34.9	36.9	*	*	*	*	*	35.9	0	35.9
18 - 19	36.2	36.2	*	*	*	*	*	36.2	0	36.2
19 - 20	35.9	36.9	*	*	*	*	*	36.4	0	36.4
20 - 21	35.6	36.3	*	*	*	*	*	35.95	0	35.95
21 - 22	36.4	37.3	*	*	*	*	*	36.85	0	36.85
22 - 23	36.2	37	*	*	*	*	*	36.6	0	36.6
23 - 24	36	34.9	*	*	*	*	*	35.45	0	35.45
Totals	840.1	848.7	466.6	0	0	0	0			
% of Total	38.98%	39.38%	21.65%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	35.8	36	*	35.8	36	35.8
1 - 2	*	*	*	*	36.7	35	*	36.7	35	36.7
2 - 3	*	*	*	*	35.8	35.7	*	35.8	35.7	35.8
3 - 4	*	*	*	*	34	36	*	34	36	34
4 - 5	*	*	*	*	36	37.5	*	36	37.5	36
5 - 6	*	*	*	*	19	33	*	19	33	19
6 - 7	*	*	*	*	27	36	*	27	36	27
7 - 8	*	*	*	*	26	32	*	26	32	26
8 - 9	*	*	*	*	34	28	*	34	28	34
9 - 10	*	*	*	*	36.5	32	*	36.5	32	36.5
10 - 11	*	*	*	*	36	35	*	36	35	36
11 - 12	*	*	*	*	36.2	35.7	*	36.2	35.7	36.2
12 - 13	*	*	*	*	36.8	36	*	36.8	36	36.8
13 - 14	*	*	*	*	38.2	36	*	38.2	36	38.2
14 - 15	*	*	*	36.5	36.7	36.7	*	36.6	36.7	36.6
15 - 16	*	*	*	36	38	37.4	*	37	37.4	37
16 - 17	*	*	*	37.2	36.5	36.8	*	36.85	36.8	36.85
17 - 18	*	*	*	37	36	36.8	*	36.5	36.8	36.5
18 - 19	*	*	*	35.8	36.4	36.4	*	36.1	36.4	36.1
19 - 20	*	*	*	37	36.6	36.3	*	36.8	36.3	36.8
20 - 21	*	*	*	36.7	36.9	35.8	*	36.8	35.8	36.8
21 - 22	*	*	*	36.8	37.6	36	*	37.2	36	37.2
22 - 23	*	*	*	37.2	36.8	37	*	37	37	37
23 - 24	*	*	*	39	36.8	35.8	*	37.9	35.8	37.9

Incoming Monthly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	36	36	36.7	*	*	*	37	36.23	37	36.23
1 - 2	35.5	34	33	*	*	*	36	34.17	36	34.17
2 - 3	30	33	37.5	*	*	*	35	33.5	35	33.5
3 - 4	36	36	36.5	*	*	*	35	36.17	35	36.17
4 - 5	32	38	36	*	*	*	34	35.33	34	35.33
5 - 6	38	29	16	*	*	*	34	27.67	34	27.67
6 - 7	35	33	30	*	*	*	33	32.67	33	32.67
7 - 8	28	30	32	*	*	*	38	30	38	30
8 - 9	28	32	35	*	*	*	33	31.67	33	31.67
9 - 10	34	36	34	*	*	*	33	34.67	33	34.67
10 - 11	34	37	35.5	*	*	*	41	35.5	41	35.5
11 - 12	39	36.7	34.7	*	*	*	36.7	36.8	36.7	36.8
12 - 13	37.7	37	35	*	*	*	37.3	36.57	37.3	36.57
13 - 14	36.6	35.9	34.7	*	*	*	35.3	35.73	35.3	35.73
14 - 15	35.9	35.6	*	*	*	*	36	35.75	36	35.75
15 - 16	36	37.7	*	*	*	*	36	36.85	36	36.85
16 - 17	37.2	36.3	*	*	*	*	36.4	36.75	36.4	36.75
17 - 18	34.9	36.9	*	*	*	*	36.6	35.9	36.6	35.9
18 - 19	36.2	36.2	*	*	*	*	37	36.2	37	36.2
19 - 20	35.9	36.9	*	*	*	*	37.5	36.4	37.5	36.4
20 - 21	35.6	36.3	*	*	*	*	36.7	35.95	36.7	35.95
21 - 22	36.4	37.3	*	*	*	*	35.9	36.85	35.9	36.85
22 - 23	36.2	37	*	*	*	*	35.3	36.6	35.3	36.6
23 - 24	36	34.9	*	*	*	*	35	35.45	35	35.45

Summary of Violators

N Birchwood southeast pilots to on ramp 8-29 to 9-4 from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM
 time error -4hrs

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	601	28.2	3	47.7
01:00:00	334	27.5	1	50.0
02:00:00	232	27.1	1	50.0
03:00:00	128	27.5	0	0.0
04:00:00	61	28.7	1	49.0
05:00:00	23	25.7	0	0.0
06:00:00	28	27.0	0	0.0
07:00:00	34	27.5	0	0.0
08:00:00	51	25.5	0	0.0
09:00:00	100	25.9	0	0.0
10:00:00	209	29.0	4	52.3
11:00:00	553	28.4	1	48.0
12:00:00	544	28.9	2	47.5
13:00:00	617	29.0	2	47.5
14:00:00	644	29.1	1	47.0
15:00:00	816	29.8	3	47.0
16:00:00	894	29.7	3	46.7
17:00:00	1017	29.8	2	47.0
18:00:00	1017	29.7	1	48.0
19:00:00	1028	29.8	3	46.7
20:00:00	1140	29.3	4	46.5
21:00:00	1139	29.6	9	49.0
22:00:00	924	29.3	2	48.5
23:00:00	720	28.6	5	46.8

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	14:00	0	4	4	1	3	5	1	0	0	0	0	0	0	0	0	0	0	0	18	28.2	36.5	15 to 25	50.0	*	*	0	18	0	
8/29/2024	15:00	2	24	30	9	32	31	8	1	0	0	0	0	0	0	0	0	0	0	137	28.4	36	29 to 39	49.6	1	0.7	1	124	12	
8/29/2024	16:00	4	19	19	16	45	46	6	3	0	0	0	0	0	0	0	0	0	0	158	30.1	37.2	29 to 39	59.5	1	0.6	0	145	13	
8/29/2024	17:00	0	16	24	16	43	44	8	1	0	0	0	0	0	0	0	0	0	0	152	30.6	37	29 to 39	60.5	1	0.7	0	139	13	
8/29/2024	18:00	1	19	31	22	62	41	6	1	0	0	0	0	0	0	0	0	0	0	183	29.5	35.8	28 to 38	62.3	*	*	0	172	11	
8/29/2024	19:00	1	25	17	28	51	38	9	0	0	0	0	0	0	0	0	0	0	0	169	29.8	37	27 to 37	58.6	*	*	0	157	12	
8/29/2024	20:00	1	31	32	18	71	50	12	1	0	0	0	0	0	0	0	0	0	0	216	29.8	36.7	28 to 38	59.3	1	0.5	0	206	10	
8/29/2024	21:00	2	25	33	25	58	46	16	2	0	0	0	0	0	0	0	0	0	0	207	29.8	36.8	29 to 39	56.0	2	1.0	0	196	11	
8/29/2024	22:00	1	27	35	12	43	55	10	0	0	0	0	0	0	0	0	0	0	0	183	29.5	37.2	30 to 40	54.6	*	*	0	173	10	
8/29/2024	23:00	0	20	20	13	34	29	16	4	0	0	0	0	0	0	0	0	0	0	136	30.5	39	31 to 41	53.7	4	2.9	0	130	6	
24 Hr Summary		12	210	245	160	442	385	92	13	0	0	0	0	0	0	0	0	0	0	1559	29.8	37	29 to 39	56.7	10	0.6	1	1460	98	

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/30/2024	00:00	1	19	20	10	25	23	5	0	0	0	0	0	0	0	0	0	0	0	103	28	35.8	28 to 38	51.5	*	*	0	100	3
8/30/2024	01:00	0	8	16	11	11	17	1	0	0	0	0	0	0	0	0	0	0	0	64	28.5	36.7	19 to 29	51.6	*	*	0	59	5
8/30/2024	02:00	1	9	8	4	8	12	1	1	0	0	0	0	0	0	0	0	0	0	44	27.9	35.8	27 to 37	47.7	*	*	0	44	0
8/30/2024	03:00	0	5	6	3	6	3	1	0	0	0	0	0	0	0	0	0	0	0	24	27	34	15 to 25	50.0	*	*	0	23	1
8/30/2024	04:00	0	3	1	1	1	3	0	1	0	0	0	0	0	0	0	0	0	0	10	29.3	36	28 to 38	50.0	1	10.0	0	10	0
8/30/2024	05:00	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	18.5	19	9 to 19	100.0	*	*	0	2	0
8/30/2024	06:00	1	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	23.3	27	18 to 28	75.0	*	*	0	4	0
8/30/2024	07:00	0	1	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	5	23.8	26	16 to 26	80.0	*	*	0	5	0
8/30/2024	08:00	0	3	4	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	9	23.1	34	13 to 23	77.8	*	*	0	9	0
8/30/2024	09:00	0	6	5	2	1	4	1	0	0	0	0	0	0	0	0	0	0	0	19	26	36.5	14 to 24	57.9	*	*	0	19	0
8/30/2024	10:00	1	7	15	2	11	9	3	0	0	0	0	0	0	0	0	0	0	0	48	27.4	36	14 to 24	47.9	*	*	0	44	4
8/30/2024	11:00	0	19	36	15	30	29	8	2	0	0	0	0	0	0	0	0	0	0	139	28.5	36.2	27 to 37	48.2	*	*	0	130	9
8/30/2024	12:00	1	18	21	10	29	34	8	0	0	0	0	0	0	0	0	0	0	0	121	29.4	36.8	30 to 40	55.4	*	*	0	112	9
8/30/2024	13:00	1	15	24	10	30	32	15	1	0	0	0	0	0	0	0	0	0	0	128	30.2	38.2	31 to 41	53.9	1	0.8	0	118	10
8/30/2024	14:00	3	12	22	12	38	33	6	2	0	0	0	0	0	0	0	0	0	0	128	30	36.7	28 to 38	57.8	*	*	0	115	13
8/30/2024	15:00	0	17	19	16	40	44	16	1	0	0	0	0	0	0	0	0	0	0	153	31	38	30 to 40	59.5	1	0.7	0	142	11
8/30/2024	16:00	1	23	27	11	44	39	8	0	0	0	0	0	0	0	0	0	0	0	153	29.3	36.5	29 to 39	57.5	*	*	0	143	10
8/30/2024	17:00	1	27	27	27	61	59	8	1	0	0	0	0	0	0	0	0	0	0	211	29.9	36	29 to 39	60.7	*	*	0	192	19
8/30/2024	18:00	0	16	29	23	65	50	7	0	0	0	0	0	0	0	0	0	0	0	190	30.4	36.4	27 to 37	66.3	*	*	0	180	10
8/30/2024	19:00	0	18	45	22	75	46	17	0	0	0	0	0	0	0	0	0	0	0	223	30.1	36.6	28 to 38	56.5	*	*	0	209	14
8/30/2024	20:00	1	33	38	19	70	61	17	2	0	0	0	0	0	0	0	0	0	0	241	30.1	36.9	29 to 39	56.4	1	0.4	0	225	16
8/30/2024	21:00	0	27	30	15	74	69	21	5	1	0	0	0	0	0	0	0	0	0	242	31.3	37.6	30 to 40	63.2	4	1.7	0	228	14
8/30/2024	22:00	2	25	29	12	41	46	6	2	0	0	0	0	0	0	0	0	0	0	163	29.3	36.8	29 to 39	55.8	1	0.6	0	157	6
8/30/2024	23:00	0	21	24	9	34	27	8	0	0	0	0	0	0	0	0	0	0	0	123	28.9	36.8	30 to 40	52.0	*	*	0	114	9
24 Hr Summary		14	334	448	238	696	641	157	18	1	0	0	0	0	0	0	0	0	0	2547	29.7	37	29 to 39	55.0	9	0.4	0	2384	163

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/31/2024	00:00	0	22	25	10	35	14	9	1	0	0	0	0	0	0	0	0	0	0	116	28	36	30 to 40	46.6	*	*	0	110	6
8/31/2024	01:00	1	15	21	10	20	8	6	0	0	0	0	0	0	0	0	0	0	0	81	27	35	16 to 26	49.4	*	*	0	77	4
8/31/2024	02:00	0	6	13	4	11	8	1	0	0	0	0	0	0	0	0	0	0	0	43	27.3	35.7	18 to 28	48.8	*	*	0	42	1
8/31/2024	03:00	0	6	4	3	5	3	2	1	0	0	0	0	0	0	0	0	0	0	24	28	36	17 to 27	50.0	*	*	0	24	0
8/31/2024	04:00	1	1	2	1	3	5	0	0	0	0	0	0	0	0	0	0	0	0	13	29.5	37.5	28 to 38	61.5	*	*	0	12	1
8/31/2024	05:00	0	2	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	5	27	33	25 to 35	60.0	*	*	0	5	0
8/31/2024	06:00	0	3	1	0	3	1	1	0	0	0	0	0	0	0	0	0	0	0	9	27	36	30 to 40	55.6	*	*	0	9	0
8/31/2024	07:00	0	2	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	6	26.3	32	23 to 33	66.7	*	*	0	6	0
8/31/2024	08:00	0	2	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	6	24.3	28	18 to 28	83.3	*	*	0	5	1
8/31/2024	09:00	0	0	1	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	6	30.5	32	27 to 37	83.3	*	*	0	5	1
8/31/2024	10:00	0	1	4	3	7	4	0	0	0	0	0	0	0	0	0	0	0	0	19	29.9	35	29 to 39	73.7	*	*	0	16	3
8/31/2024	11:00	1	7	6	2	9	12	1	0	0	0	0	0	0	0	0	0	0	0	38	28.7	35.7	28 to 38	57.9	*	*	0	34	4
8/31/2024	12:00	0	10	11	2	22	6	4	2	0	0	0	0	0	0	0	0	0	0	57	29.1	36	30 to 40	54.4	1	1.8	0	54	3
8/31/2024	13:00	0	11	21	6	30	22	6	0	0	0	0	0	0	0	0	0	0	0	96	29.2	36	30 to 40	58.3	*	*	0	92	4
8/31/2024	14:00	1	17	20	8	35	25	6	1	0	0	0	0	0	0	0	0	0	0	113	29.4	36.7	29 to 39	55.8	1	0.9	0	106	7
8/31/2024	15:00	0	19	25	10	39	38	10	1	0	0	0	0	0	0	0	0	0	0	142	30.1	37.4	30 to 40	58.5	*	*	0	132	10
8/31/2024	16:00	0	20	30	11	43	38	9	0	0	0	0	0	0	0	0	0	0	0	151	29.4	36.8	30 to 40	57.6	*	*	0	144	7
8/31/2024	17:00	0	22	22	21	43	39	11	1	0	0	0	0	0	0	0	0	0	0	159	30	36.8	27 to 37	57.2	1	0.6	0	153	6
8/31/2024	18:00	0	24	26	14	49	33	10	0	0	0	0	0	0	0	0	0	0	0	156	29.4	36.4	30 to 40	55.8	*	*	0	148	8
8/31/2024	19:00	2	20	19	21	36	36	4	0	0	0	0	0	0	0	0	0	0	0	138	29.2	36.3	29 to 39	55.8	*	*	0	132	6
8/31/2024	20:00	0	28	34	18	28	37	9	0	0	0	0	0	0	0	0	0	0	0	154	28.2	35.8	27 to 37	46.8	*	*	0	142	12
8/31/2024	21:00	0	26	31	13	38	31	7	1	0	0	0	0	0	0	0	0	0	0	147	28.6	36	30 to 40	48.3	*	*	0	138	9
8/31/2024	22:00	0	17	23	9	38	30	13	1	0	0	0	0	0	0	0	0	0	0	131	29.9	37	30 to 40	58.8	*	*	0	125	6
8/31/2024	23:00	2	20	26	9	36	29	5	1	0	0	0	0	0	0	0	0	0	0	128	28.3	35.8	30 to 40	53.1	1	0.8	0	123	5
24 Hr Summary		8	301	366	178	540	421	114	10	0	0	0	0	0	0	0	0	0	0	1938	29	37	30 to 40	52.6	4	0.2	0	1834	104

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/1/2024	00:00	0	20	20	6	20	16	7	3	0	0	0	0	0	0	0	0	0	0	92	28.5	37	17 to 27	44.6	2	2.2	0	88	4
9/1/2024	01:00	0	10	12	7	12	10	5	0	0	0	0	0	0	0	0	0	0	0	56	28.3	36	28 to 38	46.4	*	*	0	54	2
9/1/2024	02:00	0	8	6	6	8	6	2	1	0	0	0	0	0	0	0	0	0	0	37	28.1	35	27 to 37	48.6	*	*	0	36	1
9/1/2024	03:00	0	4	5	4	3	3	1	0	0	0	0	0	0	0	0	0	0	0	20	26.3	35	17 to 27	60.0	*	*	0	20	0
9/1/2024	04:00	0	2	2	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	10	27.7	34	26 to 36	60.0	*	*	0	10	0
9/1/2024	05:00	0	4	0	3	1	1	0	0	0	0	0	0	0	0	0	0	0	0	9	25.2	34	15 to 25	55.6	*	*	0	8	1
9/1/2024	06:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	28.3	33	23 to 33	100.0	*	*	0	3	0
9/1/2024	07:00	0	0	0	4	2	4	1	0	0	0	0	0	0	0	0	0	0	0	11	32.5	38	25 to 35	63.6	*	*	0	11	0
9/1/2024	08:00	0	2	2	3	1	2	0	0	0	0	0	0	0	0	0	0	0	0	10	26.3	33	16 to 26	60.0	*	*	0	10	0
9/1/2024	09:00	0	5	2	1	3	0	2	0	0	0	0	0	0	0	0	0	0	0	13	25.8	33	16 to 26	61.5	*	*	0	13	0
9/1/2024	10:00	0	0	5	1	4	5	3	0	1	0	1	0	0	0	0	0	0	0	20	34.4	41	31 to 41	55.0	2	10.0	0	19	1
9/1/2024	11:00	0	6	13	7	13	11	3	1	0	0	0	0	0	0	0	0	0	0	54	28.8	36.7	20 to 30	46.3	1	1.9	0	52	2
9/1/2024	12:00	0	15	20	9	27	15	7	0	0	0	0	0	0	0	0	0	0	0	93	28.6	37.3	29 to 39	47.3	*	*	0	88	5
9/1/2024	13:00	0	23	26	28	49	22	4	1	0	0	0	0	0	0	0	0	0	0	153	28.4	35.3	27 to 37	57.5	1	0.7	0	148	5
9/1/2024	14:00	1	27	19	21	41	26	7	0	0	0	0	0	0	0	0	0	0	0	142	28.6	36	28 to 38	57.0	*	*	0	137	5
9/1/2024	15:00	0	24	27	11	46	34	7	0	0	0	0	0	0	0	0	0	0	0	149	28.9	36	30 to 40	56.4	*	*	0	144	5
9/1/2024	16:00	1	24	32	6	34	34	8	1	0	0	0	0	0	0	0	0	0	0	140	28.7	36.4	30 to 40	50.7	1	0.7	0	134	6
9/1/2024	17:00	1	21	28	13	52	50	11	0	0	0	0	0	0	0	0	0	0	0	176	30	36.6	30 to 40	61.4	*	*	0	170	6
9/1/2024	18:00	1	17	20	15	28	41	10	1	0	0	0	0	0	0	0	0	0	0	133	29.9	37	30 to 40	55.6	*	*	0	127	6
9/1/2024	19:00	1	21	16	7	37	24	14	1	0	0	0	0	0	0	0	0	0	0	121	29.8	37.5	31 to 41	55.4	1	0.8	0	116	5
9/1/2024	20:00	0	23	34	9	40	31	6	1	0	0	0	0	0	0	0	0	0	0	144	28.7	36.7	29 to 39	52.1	1	0.7	0	137	7
9/1/2024	21:00	1	22	36	15	33	29	7	0	0	0	0	0	0	0	0	0	0	0	143	27.8	35.9	27 to 37	47.6	*	*	0	136	7
9/1/2024	22:00	0	22	35	8	34	26	3	0	0	0	0	0	0	0	0	0	0	0	128	27.7	35.3	29 to 39	48.4	*	*	0	123	5
9/1/2024	23:00	0	18	29	13	21	14	4	1	0	0	0	0	0	0	0	0	0	0	100	27.1	35	18 to 28	52.0	*	*	0	96	4
24 Hr Summary		6	318	390	198	514	406	112	11	1	0	1	0	0	0	0	0	0	0	1957	28.7	37	28 to 38	50.1	9	0.5	0	1880	77

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/2/2024	00:00	2	19	23	8	26	12	5	0	0	0	0	0	0	0	0	0	0	0	95	27.1	36	14 to 24	46.3	*	*	0	91	4
9/2/2024	01:00	1	11	14	5	4	6	2	1	0	0	0	0	0	0	0	0	0	0	44	25.7	35.5	14 to 24	59.1	*	*	0	44	0
9/2/2024	02:00	0	17	12	4	8	3	0	0	0	0	0	0	0	0	0	0	0	0	44	23.3	30	15 to 25	68.2	*	*	0	43	1
9/2/2024	03:00	0	3	9	5	6	3	2	0	0	0	0	0	0	0	0	0	0	0	28	27.3	36	22 to 32	57.1	*	*	0	28	0
9/2/2024	04:00	0	1	6	1	1	0	1	1	0	0	0	0	0	0	0	0	0	0	11	26.1	32	15 to 25	72.7	*	*	0	10	1
9/2/2024	05:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	35	38	28 to 38	100.0	*	*	0	2	0
9/2/2024	06:00	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	28.7	35	17 to 27	66.7	*	*	0	3	0
9/2/2024	07:00	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	27	28	19 to 29	100.0	*	*	0	4	0
9/2/2024	08:00	0	2	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	5	23.4	28	18 to 28	80.0	*	*	0	5	0
9/2/2024	09:00	0	4	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	7	23	34	10 to 20	71.4	*	*	0	6	1
9/2/2024	10:00	0	5	3	4	8	3	1	0	0	0	0	0	0	0	0	0	0	0	24	28.2	34	27 to 37	62.5	*	*	0	24	0
9/2/2024	11:00	0	4	3	2	4	4	3	0	0	0	0	0	0	0	0	0	0	0	20	29.4	39	30 to 40	50.0	*	*	0	18	2
9/2/2024	12:00	1	7	5	2	12	13	5	0	0	0	0	0	0	0	0	0	0	0	45	30.4	37.7	30 to 40	57.8	*	*	0	43	2
9/2/2024	13:00	0	12	17	4	15	18	3	1	0	0	0	0	0	0	0	0	0	0	70	28.4	36.6	28 to 38	50.0	*	*	0	65	5
9/2/2024	14:00	0	13	28	4	39	22	7	0	0	0	0	0	0	0	0	0	0	0	113	29	35.9	30 to 40	57.5	*	*	0	107	6
9/2/2024	15:00	2	16	23	11	31	35	9	0	0	0	0	0	0	0	0	0	0	0	127	29.6	36	28 to 38	56.7	*	*	0	118	9
9/2/2024	16:00	0	13	12	15	47	42	9	1	0	0	0	0	0	0	0	0	0	0	139	31.4	37.2	30 to 40	69.1	1	0.7	0	131	8
9/2/2024	17:00	0	22	39	17	40	33	6	0	0	0	0	0	0	0	0	0	0	0	157	28	34.9	28 to 38	51.0	*	*	0	148	9
9/2/2024	18:00	1	26	31	14	45	34	6	1	0	0	0	0	0	0	0	0	0	0	158	28.6	36.2	29 to 39	52.5	1	0.6	0	150	8
9/2/2024	19:00	0	28	34	18	42	40	7	0	0	0	0	0	0	0	0	0	0	0	169	28.6	35.9	29 to 39	52.1	*	*	0	164	5
9/2/2024	20:00	0	29	31	20	43	27	7	0	0	0	0	0	0	0	0	0	0	0	157	28	35.6	30 to 40	47.1	*	*	0	145	12
9/2/2024	21:00	1	33	33	10	28	23	9	2	0	0	0	0	0	0	0	0	0	0	139	27.3	36.4	15 to 25	48.9	1	0.7	0	133	6
9/2/2024	22:00	1	27	35	9	26	28	7	1	0	0	0	0	0	0	0	0	0	0	134	27.4	36.2	18 to 28	50.0	*	*	0	126	8
9/2/2024	23:00	0	18	26	12	23	18	5	1	0	0	0	0	0	0	0	0	0	0	103	27.7	36	17 to 27	44.7	*	*	0	100	3
24 Hr Summary		9	310	387	171	451	367	94	9	0	0	0	0	0	0	0	0	0	0	1798	28.3	37	30 to 40	48.1	3	0.2	0	1708	90

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/3/2024	00:00	1	15	20	6	20	12	4	2	0	0	0	0	0	0	0	0	0	0	80	27.6	36	17 to 27	51.3	1	1.3	0	76	4
9/3/2024	01:00	0	4	11	5	11	4	1	1	0	0	0	0	0	0	0	0	0	0	37	27.6	34	18 to 28	48.6	*	*	0	36	1
9/3/2024	02:00	0	6	8	11	5	4	1	0	0	0	0	0	0	0	0	0	0	0	35	26.3	33	18 to 28	57.1	*	*	0	33	2
9/3/2024	03:00	0	4	3	1	4	4	0	0	0	0	0	0	0	0	0	0	0	0	16	27.1	36	28 to 38	56.3	*	*	0	15	1
9/3/2024	04:00	0	1	1	2	3	2	1	0	0	0	0	0	0	0	0	0	0	0	10	30.9	38	23 to 33	60.0	*	*	0	10	0
9/3/2024	05:00	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26.5	29	19 to 29	75.0	*	*	0	4	0
9/3/2024	06:00	0	0	2	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	6	27.7	33	25 to 35	66.7	*	*	0	6	0
9/3/2024	07:00	0	3	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	6	24.3	30	16 to 26	66.7	*	*	0	6	0
9/3/2024	08:00	1	1	2	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	10	27.4	32	26 to 36	60.0	*	*	0	8	2
9/3/2024	09:00	0	5	10	5	2	4	3	0	0	0	0	0	0	0	0	0	0	0	29	26.5	36	17 to 27	58.6	*	*	0	28	1
9/3/2024	10:00	0	5	12	1	16	12	4	1	0	0	0	0	0	0	0	0	0	0	51	30.1	37	30 to 40	60.8	1	2.0	0	47	4
9/3/2024	11:00	1	15	43	14	38	36	5	1	0	0	0	0	0	0	0	0	0	0	153	28.6	36.7	30 to 40	51.0	*	*	0	145	8
9/3/2024	12:00	1	16	18	13	28	34	6	1	0	0	0	0	0	0	0	0	0	0	117	29.6	37	29 to 39	55.6	1	0.9	0	103	14
9/3/2024	13:00	1	11	29	18	37	39	4	0	0	0	0	0	0	0	0	0	0	0	139	29.4	35.9	28 to 38	58.3	*	*	0	127	12
9/3/2024	14:00	1	15	30	13	40	23	8	0	0	0	0	0	0	0	0	0	0	0	130	28.8	35.6	28 to 38	52.3	*	*	0	114	16
9/3/2024	15:00	1	7	16	13	29	31	10	1	0	0	0	0	0	0	0	0	0	0	108	31.2	37.7	30 to 40	61.1	1	0.9	0	98	10
9/3/2024	16:00	0	18	31	15	45	37	7	0	0	0	0	0	0	0	0	0	0	0	153	29.4	36.3	28 to 38	56.2	*	*	0	139	14
9/3/2024	17:00	0	11	31	18	48	44	9	1	0	0	0	0	0	0	0	0	0	0	162	30.3	36.9	28 to 38	59.3	*	*	0	153	9
9/3/2024	18:00	0	21	26	32	57	51	9	1	0	0	0	0	0	0	0	0	0	0	197	30.2	36.2	28 to 38	62.9	*	*	0	182	15
9/3/2024	19:00	0	19	25	22	68	60	12	2	0	0	0	0	0	0	0	0	0	0	208	31	36.9	29 to 39	65.9	2	1.0	0	191	17
9/3/2024	20:00	2	25	33	23	74	56	14	1	0	0	0	0	0	0	0	0	0	0	228	30.1	36.3	27 to 37	59.6	1	0.4	0	206	22
9/3/2024	21:00	0	20	48	29	80	62	19	2	0	1	0	0	0	0	0	0	0	0	261	30.7	37.3	29 to 39	57.9	2	0.8	0	247	14
9/3/2024	22:00	0	18	21	22	50	60	13	1	0	0	0	0	0	0	0	0	0	0	185	31.1	37	28 to 38	65.4	1	0.5	0	169	16
9/3/2024	23:00	0	18	28	17	40	25	2	0	0	0	0	0	0	0	0	0	0	0	130	28.3	34.9	29 to 39	53.8	*	*	0	121	9
24 Hr Summary		9	259	448	284	703	604	132	15	0	1	0	0	0	0	0	0	0	0	2455	29.7	37	29 to 39	56.3	10	0.4	0	2264	191

Incoming Histogram

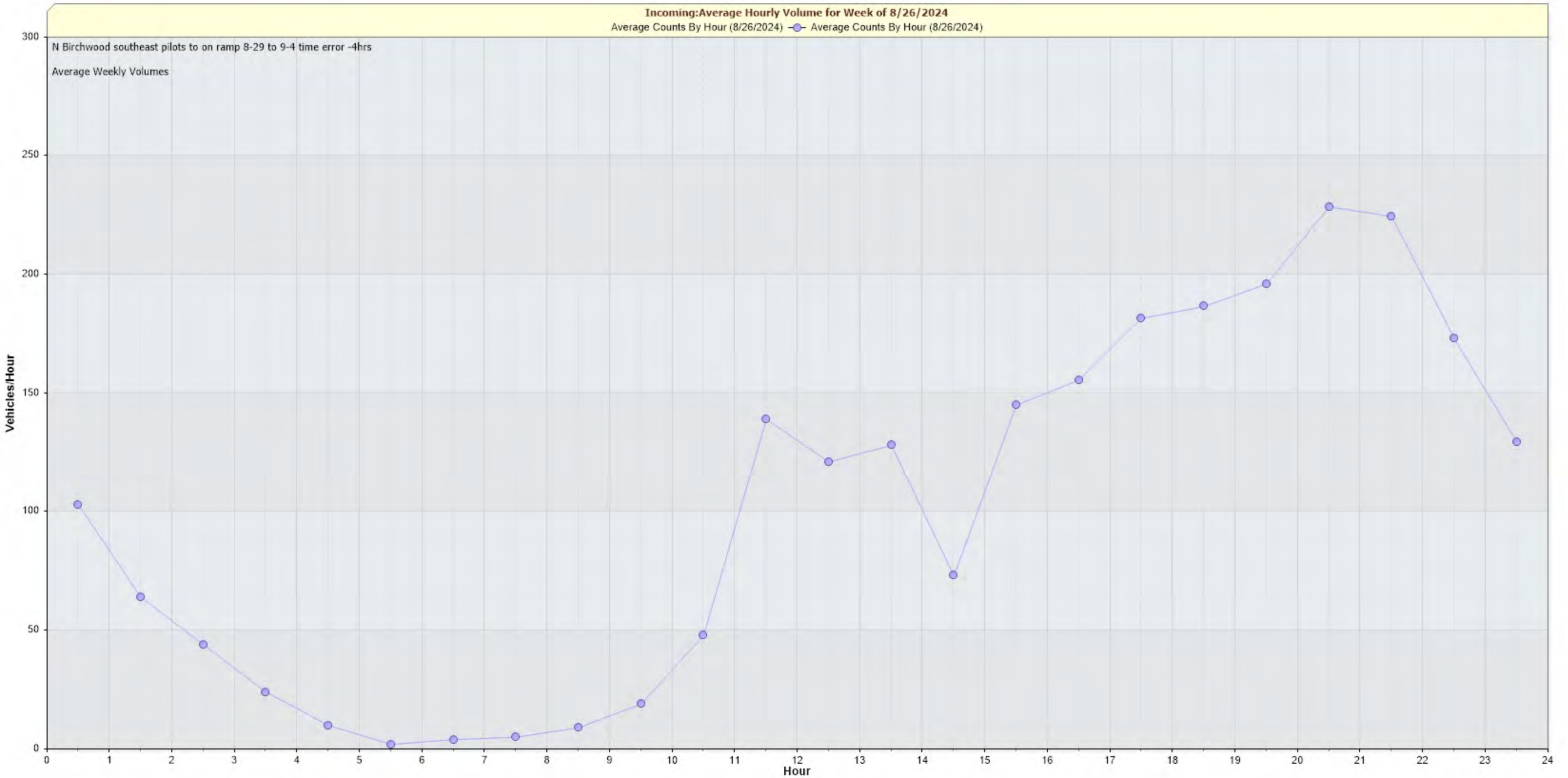
N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG		
9/4/2024	00:00	2	12	17	13	36	30	5	0	0	0	0	0	0	0	0	0	0	0	115	29.8	36.7	29 to 39	60.0	*	*	0	108	7		
9/4/2024	01:00	0	10	10	7	18	5	1	0	1	0	0	0	0	0	0	0	0	0	52	27.8	33	23 to 33	53.8	1	1.9	0	50	2		
9/4/2024	02:00	0	2	5	6	6	7	2	0	1	0	0	0	0	0	0	0	0	0	29	30.8	37.5	28 to 38	58.6	1	3.4	0	26	3		
9/4/2024	03:00	0	1	4	1	5	4	1	0	0	0	0	0	0	0	0	0	0	0	16	29.7	36.5	27 to 37	56.3	*	*	0	16	0		
9/4/2024	04:00	0	2	0	2	1	1	1	0	0	0	0	0	0	0	0	0	0	0	7	28.3	36	16 to 26	57.1	*	*	0	6	1		
9/4/2024	05:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	16	16	6 to 16	100.0	*	*	0	1	0		
9/4/2024	06:00	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	28	30	20 to 30	100.0	*	*	0	2	1		
9/4/2024	07:00	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24	32	6 to 16	50.0	*	*	0	2	0		
9/4/2024	08:00	0	3	2	0	3	3	0	0	0	0	0	0	0	0	0	0	0	0	11	26.4	35	26 to 36	54.5	*	*	0	11	0		
9/4/2024	09:00	0	5	11	5	1	3	1	0	0	0	0	0	0	0	0	0	0	0	26	24.8	34	17 to 27	76.9	*	*	0	26	0		
9/4/2024	10:00	1	7	16	3	11	6	1	2	0	0	0	0	0	0	0	0	0	0	47	27	35.5	14 to 24	48.9	1	2.1	0	45	2		
9/4/2024	11:00	0	27	34	13	49	22	4	0	0	0	0	0	0	0	0	0	0	0	149	27.6	34.7	29 to 39	50.3	*	*	0	139	10		
9/4/2024	12:00	0	19	29	18	24	14	6	1	0	0	0	0	0	0	0	0	0	0	111	27.4	35	17 to 27	48.6	*	*	0	99	12		
9/4/2024	13:00	0	5	10	3	6	5	2	0	0	0	0	0	0	0	0	0	0	0	31	27.1	34.7	13 to 23	48.4	*	*	0	29	2		
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		3	95	138	73	162	100	24	3	2	0	0	0	0	0	0	0	0	0	600	28	36	27 to 37	47.3	3	0.5	0	560	40		

Incoming: Average Hourly Volume for Week of 8/26/2024
Average Counts By Hour (8/26/2024) ● Average Counts By Hour (8/26/2024)

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs
Average Weekly Volumes



Incoming:Average Hourly WEEKDAY Speeds for Week of 8/26/2024
Average Hourly WEEKDAY Speeds By Hour (8/26/2024) — Average Hourly WEEKDAY Speeds By Hour (8/26/2024)

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs
Average Weekly Speeds

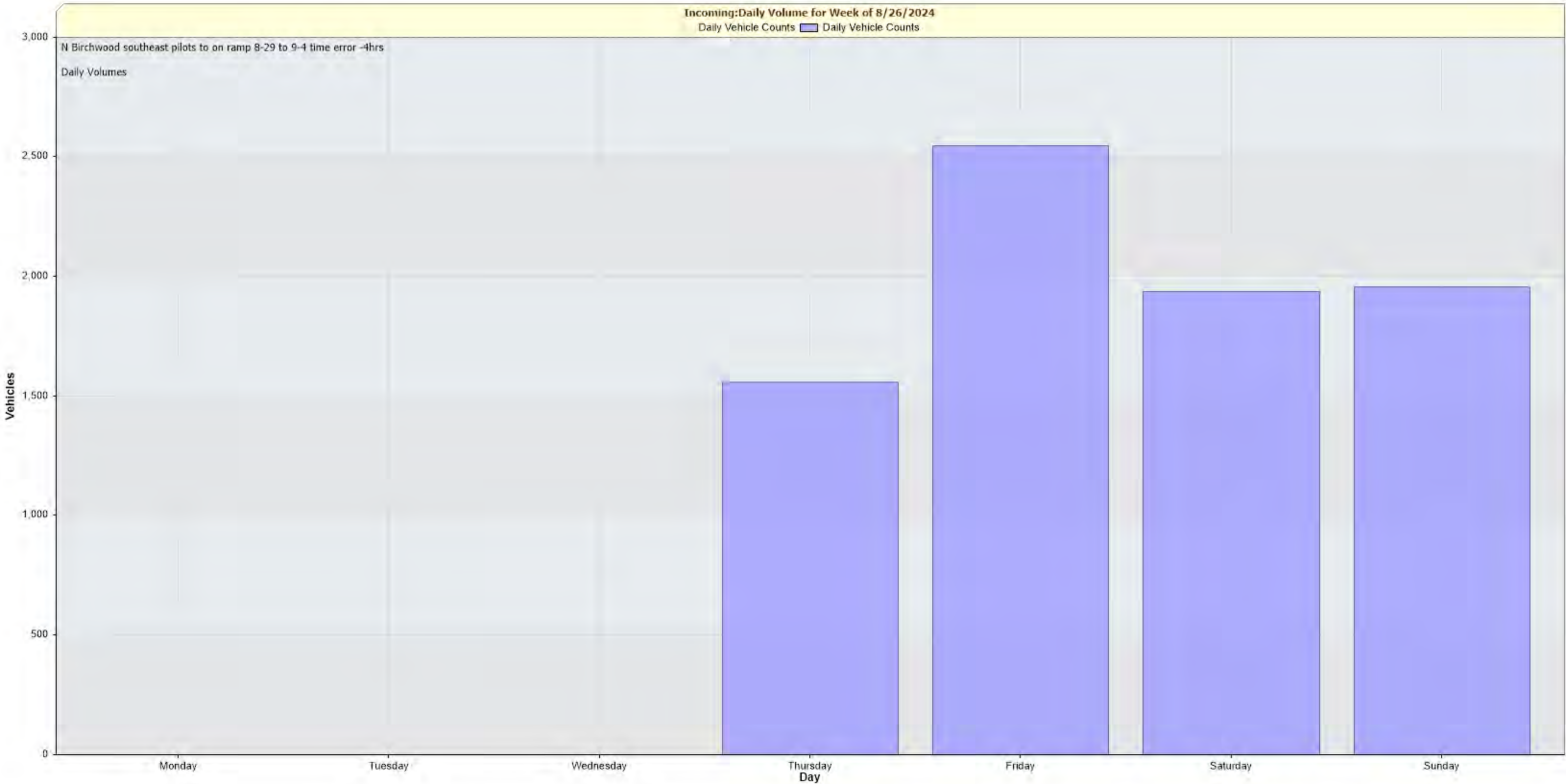


Incoming: Daily Volume for Week of 8/26/2024

Daily Vehicle Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

Daily Volumes



For Project: N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:38:47 PM

Speed Intervals: 1 MPH

Time Intervals: Instant

Traffic Report From: 8/29/2024 2:00:00 PM through 9/4/2024 1:59:59 PM

85th Percentile Speed: 37 MPH

85th Percentile Vehicles: 15024

Max Speed: 70 MPH on 9/3/2024 1:43:06 PM

Total Vehicles: 17675

AADT: 2945

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	2460	2525
AM Peak 11:00 AM	177	132
PM Peak 7:00 PM	285	259

Speed

Speed Limit: 45

85th Percentile Speed: 37

50th Percentile Speed: 31

10 MPH Pace Interval: 26.0 MPH to 36.0 MPH

Average Speed: 31.29

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	30	31	12	29	37	33	33
% over limit	1.2	0.9	1.3	1.4	1.1	1.3	1.2
Avg Speeder	48.0	49.5	48.1	48.1	47.7	48.6	47.5
Avg Speed	31.5	31.2	30.2	31.7	31.3	31.4	31.1

Class Counts

	Number	%
VEH_SM	83	0.5
VEH_MED	16868	95.4
VEH_LG	724	4.1
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Outgoing Summary
 N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/29/2024 3:00:00 PM	40.0	23	27	43	0.0	0.0%	33.3
8/29/2024 4:00:00 PM	38.0	151	178	52	49.7	1.7%	32.0
8/29/2024 5:00:00 PM	37.0	190	224	46	46.0	0.9%	31.2
8/29/2024 6:00:00 PM	39.0	161	189	50	47.2	2.6%	32.1
8/29/2024 7:00:00 PM	36.0	230	271	53	50.7	1.1%	29.9
8/29/2024 8:00:00 PM	36.0	240	282	49	46.8	1.4%	31.2
8/29/2024 9:00:00 PM	39.0	212	250	52	48.4	2.0%	32.3
8/29/2024 10:00:00 PM	39.0	221	260	52	48.0	1.5%	33.0
8/29/2024 11:00:00 PM	38.0	191	225	50	48.0	0.9%	31.9
8/30/2024 12:00:00 AM	38.0	156	184	48	48.0	0.5%	32.3
8/30/2024 1:00:00 AM	37.0	131	154	46	46.0	0.6%	30.8
8/30/2024 2:00:00 AM	38.0	66	78	45	0.0	0.0%	31.7
8/30/2024 3:00:00 AM	38.0	39	46	46	46.0	2.2%	29.7
8/30/2024 4:00:00 AM	35.0	18	21	44	0.0	0.0%	29.2
8/30/2024 5:00:00 AM	33.0	6	7	43	0.0	0.0%	31.0
8/30/2024 6:00:00 AM	32.0	8	9	37	0.0	0.0%	25.3
8/30/2024 7:00:00 AM	34.0	8	9	34	0.0	0.0%	25.1
8/30/2024 8:00:00 AM	31.0	7	8	33	0.0	0.0%	27.9
8/30/2024 9:00:00 AM	33.0	8	9	35	0.0	0.0%	27.2
8/30/2024 10:00:00 AM	36.0	25	29	38	0.0	0.0%	29.5
8/30/2024 11:00:00 AM	40.0	71	83	46	46.0	1.2%	31.5
8/30/2024 12:00:00 PM	35.0	183	215	48	47.5	0.9%	29.3
8/30/2024 1:00:00 PM	38.0	169	199	45	0.0	0.0%	31.1
8/30/2024 2:00:00 PM	38.0	151	178	46	46.0	2.2%	31.6
8/30/2024 3:00:00 PM	38.0	154	181	53	48.8	2.2%	31.0
8/30/2024 4:00:00 PM	38.0	188	221	46	46.0	0.5%	31.9
8/30/2024 5:00:00 PM	38.0	185	218	49	48.7	1.4%	31.3
8/30/2024 6:00:00 PM	37.0	206	242	51	51.0	0.4%	30.9
8/30/2024 7:00:00 PM	36.0	249	293	48	47.3	1.0%	31.3
8/30/2024 8:00:00 PM	38.0	267	314	56	51.7	1.0%	31.8
8/30/2024 9:00:00 PM	38.0	241	284	49	47.5	0.7%	32.2
8/30/2024 10:00:00 PM	38.0	273	321	48	46.8	1.6%	32.6
8/30/2024 11:00:00 PM	36.0	212	249	48	47.0	0.8%	30.9
8/31/2024 12:00:00 AM	37.0	129	152	48	47.5	2.6%	31.7
8/31/2024 1:00:00 AM	37.0	102	120	45	0.0	0.0%	31.7
8/31/2024 2:00:00 AM	37.0	85	100	48	47.0	2.0%	30.3
8/31/2024 3:00:00 AM	37.0	43	51	46	46.0	3.9%	29.0
8/31/2024 4:00:00 AM	37.0	22	26	47	47.0	7.7%	29.3
8/31/2024 5:00:00 AM	30.0	8	10	39	0.0	0.0%	27.8
8/31/2024 6:00:00 AM	28.0	9	11	37	0.0	0.0%	25.0
8/31/2024 7:00:00 AM	25.0	4	5	37	0.0	0.0%	25.8
8/31/2024 8:00:00 AM	33.0	3	4	36	0.0	0.0%	30.8
8/31/2024 9:00:00 AM	34.0	6	7	44	0.0	0.0%	31.3
8/31/2024 10:00:00 AM	29.0	6	7	37	0.0	0.0%	25.4
8/31/2024 11:00:00 AM	36.0	22	26	50	50.0	3.8%	30.5
8/31/2024 12:00:00 PM	36.0	38	45	45	0.0	0.0%	29.8
8/31/2024 1:00:00 PM	38.0	81	95	50	50.0	1.1%	31.2

Outgoing Summary
 N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

8/31/2024 2:00:00 PM	36.0	120	141	49	49.0	0.7%	31.0
8/31/2024 3:00:00 PM	38.0	156	183	49	47.5	1.1%	31.7
8/31/2024 4:00:00 PM	38.0	186	219	51	48.0	1.4%	31.8
8/31/2024 5:00:00 PM	37.0	175	206	52	48.3	1.5%	31.4
8/31/2024 6:00:00 PM	38.0	167	197	55	52.3	1.5%	31.4
8/31/2024 7:00:00 PM	37.0	207	244	42	0.0	0.0%	31.0
8/31/2024 8:00:00 PM	39.0	166	195	50	47.8	2.1%	32.2
8/31/2024 9:00:00 PM	38.0	180	212	51	49.0	1.4%	31.9
8/31/2024 10:00:00 PM	38.0	171	201	53	49.3	2.0%	31.9
8/31/2024 11:00:00 PM	38.0	145	171	51	49.5	1.2%	32.3
9/1/2024 12:00:00 AM	37.0	121	142	45	0.0	0.0%	31.4
9/1/2024 1:00:00 AM	38.0	106	125	48	47.3	2.4%	31.8
9/1/2024 2:00:00 AM	38.0	54	63	45	0.0	0.0%	31.2
9/1/2024 3:00:00 AM	35.0	58	68	45	0.0	0.0%	28.4
9/1/2024 4:00:00 AM	37.0	20	24	42	0.0	0.0%	27.4
9/1/2024 5:00:00 AM	32.0	11	13	45	0.0	0.0%	29.3
9/1/2024 6:00:00 AM	30.0	10	12	42	0.0	0.0%	25.8
9/1/2024 7:00:00 AM	37.0	5	6	38	0.0	0.0%	27.3
9/1/2024 8:00:00 AM	43.0	8	9	45	0.0	0.0%	35.3
9/1/2024 9:00:00 AM	27.0	3	4	32	0.0	0.0%	28.0
9/1/2024 10:00:00 AM	34.0	6	7	35	0.0	0.0%	28.3
9/1/2024 11:00:00 AM	36.0	20	23	42	0.0	0.0%	30.3
9/1/2024 12:00:00 PM	35.0	31	37	42	0.0	0.0%	29.6
9/1/2024 1:00:00 PM	40.0	80	94	47	47.0	1.1%	32.4
9/1/2024 2:00:00 PM	38.0	102	120	48	47.0	1.7%	31.9
9/1/2024 3:00:00 PM	36.0	207	244	49	49.0	0.4%	29.7
9/1/2024 4:00:00 PM	36.0	154	181	46	46.0	2.2%	30.6
9/1/2024 5:00:00 PM	36.0	269	317	47	47.0	0.6%	30.2
9/1/2024 6:00:00 PM	37.0	191	225	48	48.0	0.4%	31.7
9/1/2024 7:00:00 PM	39.0	172	202	51	47.5	2.0%	32.7
9/1/2024 8:00:00 PM	37.0	187	220	53	49.0	2.3%	31.5
9/1/2024 9:00:00 PM	37.0	205	241	51	49.7	1.2%	31.6
9/1/2024 10:00:00 PM	37.0	173	203	47	47.0	0.5%	31.7
9/1/2024 11:00:00 PM	37.0	156	184	48	47.0	1.1%	31.4
9/2/2024 12:00:00 AM	38.0	115	135	47	46.3	3.0%	31.8
9/2/2024 1:00:00 AM	36.0	118	139	45	0.0	0.0%	29.5
9/2/2024 2:00:00 AM	36.0	62	73	50	49.0	2.7%	30.4
9/2/2024 3:00:00 AM	38.0	33	39	49	49.0	2.6%	29.5
9/2/2024 4:00:00 AM	39.0	20	24	46	46.0	4.2%	31.0
9/2/2024 5:00:00 AM	35.0	7	8	37	0.0	0.0%	26.6
9/2/2024 6:00:00 AM	32.0	9	11	47	46.5	18.2%	29.7
9/2/2024 7:00:00 AM	36.0	5	6	37	0.0	0.0%	30.2
9/2/2024 8:00:00 AM	25.0	2	2	25	0.0	0.0%	24.0
9/2/2024 9:00:00 AM	31.0	3	4	33	0.0	0.0%	28.0
9/2/2024 10:00:00 AM	29.0	8	10	42	0.0	0.0%	25.8
9/2/2024 11:00:00 AM	36.0	19	22	40	0.0	0.0%	29.1
9/2/2024 12:00:00 PM	37.0	31	36	44	0.0	0.0%	30.2
9/2/2024 1:00:00 PM	39.0	53	62	43	0.0	0.0%	32.1

Outgoing Summary
 N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

9/2/2024 2:00:00 PM	39.0	85	100	47	46.3	3.0%	32.5
9/2/2024 3:00:00 PM	37.0	143	168	46	46.0	0.6%	31.7
9/2/2024 4:00:00 PM	37.0	145	171	47	47.0	0.6%	31.7
9/2/2024 5:00:00 PM	37.0	158	186	45	0.0	0.0%	31.8
9/2/2024 6:00:00 PM	38.0	163	192	46	46.0	0.5%	31.9
9/2/2024 7:00:00 PM	36.0	200	235	54	54.0	0.4%	31.0
9/2/2024 8:00:00 PM	39.0	209	246	54	51.0	1.2%	32.3
9/2/2024 9:00:00 PM	38.0	180	212	52	49.5	0.9%	31.8
9/2/2024 10:00:00 PM	39.0	163	192	48	46.8	2.1%	31.8
9/2/2024 11:00:00 PM	40.0	146	172	50	48.3	4.1%	32.5
9/3/2024 12:00:00 AM	37.0	140	165	46	46.0	0.6%	31.7
9/3/2024 1:00:00 AM	36.0	99	117	50	49.0	2.6%	31.2
9/3/2024 2:00:00 AM	36.0	58	68	45	0.0	0.0%	29.4
9/3/2024 3:00:00 AM	34.0	20	23	38	0.0	0.0%	28.8
9/3/2024 4:00:00 AM	33.0	12	14	66	66.0	7.1%	29.5
9/3/2024 5:00:00 AM	30.0	4	5	31	0.0	0.0%	28.0
9/3/2024 6:00:00 AM	37.0	3	4	43	0.0	0.0%	33.0
9/3/2024 7:00:00 AM	35.0	8	9	42	0.0	0.0%	29.2
9/3/2024 8:00:00 AM	33.0	4	5	37	0.0	0.0%	28.6
9/3/2024 9:00:00 AM	32.0	9	11	35	0.0	0.0%	26.8
9/3/2024 10:00:00 AM	34.0	50	59	41	0.0	0.0%	28.0
9/3/2024 11:00:00 AM	35.0	75	88	46	46.0	1.1%	29.7
9/3/2024 12:00:00 PM	36.0	198	233	45	0.0	0.0%	29.5
9/3/2024 1:00:00 PM	36.0	162	191	48	48.0	0.5%	30.8
9/3/2024 2:00:00 PM	37.0	161	189	70	70.0	0.5%	30.8
9/3/2024 3:00:00 PM	37.0	150	177	46	46.0	0.6%	31.0
9/3/2024 4:00:00 PM	38.0	169	199	46	46.0	1.0%	31.3
9/3/2024 5:00:00 PM	37.0	146	172	47	47.0	0.6%	31.2
9/3/2024 6:00:00 PM	37.0	158	186	43	0.0	0.0%	30.7
9/3/2024 7:00:00 PM	36.0	222	261	46	46.0	0.4%	30.3
9/3/2024 8:00:00 PM	37.0	255	300	47	46.7	1.0%	31.9
9/3/2024 9:00:00 PM	38.0	254	299	49	47.5	0.7%	32.0
9/3/2024 10:00:00 PM	38.0	244	287	48	47.3	1.0%	32.6
9/3/2024 11:00:00 PM	39.0	192	226	52	48.8	3.5%	32.8
9/4/2024 12:00:00 AM	38.0	156	184	56	53.7	1.6%	32.3
9/4/2024 1:00:00 AM	39.0	115	135	55	49.7	2.2%	32.1
9/4/2024 2:00:00 AM	39.0	82	96	49	48.0	2.1%	33.1
9/4/2024 3:00:00 AM	39.0	24	28	48	48.0	3.6%	31.6
9/4/2024 4:00:00 AM	36.0	10	12	46	46.0	16.7%	34.2
9/4/2024 5:00:00 AM	30.0	4	5	32	0.0	0.0%	28.0
9/4/2024 6:00:00 AM	33.0	6	7	36	0.0	0.0%	28.7
9/4/2024 7:00:00 AM	35.0	3	3	35	0.0	0.0%	27.7
9/4/2024 8:00:00 AM	34.0	6	7	37	0.0	0.0%	27.4
9/4/2024 9:00:00 AM	29.0	10	12	48	48.0	8.3%	27.7
9/4/2024 10:00:00 AM	34.0	36	42	42	0.0	0.0%	28.3
9/4/2024 11:00:00 AM	39.0	82	96	49	48.0	3.1%	30.8
9/4/2024 12:00:00 PM	34.0	192	226	44	0.0	0.0%	28.9
9/4/2024 1:00:00 PM	35.0	167	197	44	0.0	0.0%	29.6

Outgoing Summary

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

9/4/2024 2:00:00 PM	35.0	36	42	44	0.0	0.0%	28.3
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Outgoing Summary

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/30/2024 12:00:00 AM	38.0	1776	2090	53	48.1	1.4%	31.7
8/31/2024 12:00:00 AM	37.0	2992	3520	56	47.7	1.1%	31.3
9/1/2024 12:00:00 AM	38.0	2225	2618	55	48.6	1.3%	31.4
9/2/2024 12:00:00 AM	37.0	2343	2757	53	47.5	1.2%	31.1
9/3/2024 12:00:00 AM	38.0	2104	2475	54	48.0	1.2%	31.5
9/4/2024 12:00:00 AM	37.0	2811	3307	70	49.5	0.9%	31.2
9/4/2024 1:59:59 PM	37.0	772	908	55	48.1	1.3%	30.2

Outgoing Weekly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	154	120	125	154	122.5	36.7
1 - 2	*	*	*	*	78	100	63	78	81.5	38
2 - 3	*	*	*	*	46	51	68	46	59.5	37.3
3 - 4	*	*	*	*	21	26	24	21	25	35
4 - 5	*	*	*	*	7	10	13	7	11.5	33
5 - 6	*	*	*	*	9	11	12	9	11.5	32
6 - 7	*	*	*	*	9	5	6	9	5.5	33.5
7 - 8	*	*	*	*	8	4	9	8	6.5	31
8 - 9	*	*	*	*	9	7	4	9	5.5	33
9 - 10	*	*	*	*	29	7	7	29	7	36
10 - 11	*	*	*	*	83	26	23	83	24.5	39.3
11 - 12	*	*	*	*	215	45	37	215	41	34.5
12 - 13	*	*	*	*	199	95	94	199	94.5	37.3
13 - 14	*	*	*	*	178	141	120	178	130.5	37.2
14 - 15	*	*	*	27	181	183	244	104	213.5	39
15 - 16	*	*	*	178	221	219	181	199.5	200	37.55
16 - 17	*	*	*	224	218	206	317	221	261.5	36.95
17 - 18	*	*	*	189	242	197	225	215.5	211	37.65
18 - 19	*	*	*	271	293	244	202	282	223	35.75
19 - 20	*	*	*	282	314	195	220	298	207.5	36.45
20 - 21	*	*	*	250	284	212	241	267	226.5	37.75
21 - 22	*	*	*	260	321	201	203	290.5	202	37.9
22 - 23	*	*	*	225	249	171	184	237	177.5	36.85
23 - 24	*	*	*	184	152	142	135	168	138.5	37.1
Totals	0	0	0	2090	3520	2618	2757			
% of Total	0%	0%	0%	19.03%	32.04%	23.83%	25.1%			

Outgoing Weekly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	139	117	135	*	*	*	*	130.33	0	36.5
1 - 2	73	68	96	*	*	*	*	79	0	36.67
2 - 3	39	23	28	*	*	*	*	30	0	36.83
3 - 4	24	14	12	*	*	*	*	16.67	0	36
4 - 5	8	5	5	*	*	*	*	6	0	31.67
5 - 6	11	4	7	*	*	*	*	7.33	0	34
6 - 7	6	9	3	*	*	*	*	6	0	35.33
7 - 8	2	5	7	*	*	*	*	4.67	0	30.67
8 - 9	4	11	12	*	*	*	*	9	0	30.67
9 - 10	10	59	42	*	*	*	*	37	0	32.23
10 - 11	22	88	96	*	*	*	*	68.67	0	36.47
11 - 12	36	233	226	*	*	*	*	165	0	35.17
12 - 13	62	191	197	*	*	*	*	150	0	36.4
13 - 14	100	189	42	*	*	*	*	110.33	0	36.4
14 - 15	168	177	*	*	*	*	*	172.5	0	36.9
15 - 16	171	199	*	*	*	*	*	185	0	37.25
16 - 17	186	172	*	*	*	*	*	179	0	36.5
17 - 18	192	186	*	*	*	*	*	189	0	36.85
18 - 19	235	261	*	*	*	*	*	248	0	35.75
19 - 20	246	300	*	*	*	*	*	273	0	37.55
20 - 21	212	299	*	*	*	*	*	255.5	0	37.45
21 - 22	192	287	*	*	*	*	*	239.5	0	37.95
22 - 23	172	226	*	*	*	*	*	199	0	38.9
23 - 24	165	184	*	*	*	*	*	174.5	0	37.2
Totals	2475	3307	908	0	0	0	0			
% of Total	37%	49.43%	13.57%	0%	0%	0%	0%			

Outgoing Monthly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	154	120	*	154	120	36.7
1 - 2	*	*	*	*	78	100	*	78	100	38
2 - 3	*	*	*	*	46	51	*	46	51	37.3
3 - 4	*	*	*	*	21	26	*	21	26	35
4 - 5	*	*	*	*	7	10	*	7	10	33
5 - 6	*	*	*	*	9	11	*	9	11	32
6 - 7	*	*	*	*	9	5	*	9	5	33.5
7 - 8	*	*	*	*	8	4	*	8	4	31
8 - 9	*	*	*	*	9	7	*	9	7	33
9 - 10	*	*	*	*	29	7	*	29	7	36
10 - 11	*	*	*	*	83	26	*	83	26	39.3
11 - 12	*	*	*	*	215	45	*	215	45	34.5
12 - 13	*	*	*	*	199	95	*	199	95	37.3
13 - 14	*	*	*	*	178	141	*	178	141	37.2
14 - 15	*	*	*	27	181	183	*	104	183	39
15 - 16	*	*	*	178	221	219	*	199.5	219	37.55
16 - 17	*	*	*	224	218	206	*	221	206	36.95
17 - 18	*	*	*	189	242	197	*	215.5	197	37.65
18 - 19	*	*	*	271	293	244	*	282	244	35.75
19 - 20	*	*	*	282	314	195	*	298	195	36.45
20 - 21	*	*	*	250	284	212	*	267	212	37.75
21 - 22	*	*	*	260	321	201	*	290.5	201	37.9
22 - 23	*	*	*	225	249	171	*	237	171	36.85
23 - 24	*	*	*	184	152	142	*	168	142	37.1
Totals	0	0	0	2090	3520	2618	0			
% of Total	0%	0%	0%	25.4%	42.78%	31.82%	0%			

Outgoing Monthly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	139	117	135	*	*	*	125	130.33	125	36.5
1 - 2	73	68	96	*	*	*	63	79	63	36.67
2 - 3	39	23	28	*	*	*	68	30	68	36.83
3 - 4	24	14	12	*	*	*	24	16.67	24	36
4 - 5	8	5	5	*	*	*	13	6	13	31.67
5 - 6	11	4	7	*	*	*	12	7.33	12	34
6 - 7	6	9	3	*	*	*	6	6	6	35.33
7 - 8	2	5	7	*	*	*	9	4.67	9	30.67
8 - 9	4	11	12	*	*	*	4	9	4	30.67
9 - 10	10	59	42	*	*	*	7	37	7	32.23
10 - 11	22	88	96	*	*	*	23	68.67	23	36.47
11 - 12	36	233	226	*	*	*	37	165	37	35.17
12 - 13	62	191	197	*	*	*	94	150	94	36.4
13 - 14	100	189	42	*	*	*	120	110.33	120	36.4
14 - 15	168	177	*	*	*	*	244	172.5	244	36.9
15 - 16	171	199	*	*	*	*	181	185	181	37.25
16 - 17	186	172	*	*	*	*	317	179	317	36.5
17 - 18	192	186	*	*	*	*	225	189	225	36.85
18 - 19	235	261	*	*	*	*	202	248	202	35.75
19 - 20	246	300	*	*	*	*	220	273	220	37.55
20 - 21	212	299	*	*	*	*	241	255.5	241	37.45
21 - 22	192	287	*	*	*	*	203	239.5	203	37.95
22 - 23	172	226	*	*	*	*	184	199	184	38.9
23 - 24	165	184	*	*	*	*	135	174.5	135	37.2
Totals	2475	3307	908	0	0	0	2757			
% of Total	26.2%	35.01%	9.61%	0%	0%	0%	29.18%			

Outgoing Weekly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	30.83	31.73	31.82	30.83	31.77	36.7
1 - 2	*	*	*	*	31.68	30.27	31.19	31.68	30.63	38
2 - 3	*	*	*	*	29.67	29	28.44	29.67	28.68	37.3
3 - 4	*	*	*	*	29.24	29.27	27.42	29.24	28.38	35
4 - 5	*	*	*	*	31	27.8	29.31	31	28.65	33
5 - 6	*	*	*	*	25.33	25	25.75	25.33	25.39	32
6 - 7	*	*	*	*	25.11	25.8	27.33	25.11	26.64	33.5
7 - 8	*	*	*	*	27.88	30.75	35.33	27.88	33.92	31
8 - 9	*	*	*	*	27.22	31.29	28	27.22	30.09	33
9 - 10	*	*	*	*	29.48	25.43	28.29	29.48	26.86	36
10 - 11	*	*	*	*	31.52	30.54	30.35	31.52	30.45	39.3
11 - 12	*	*	*	*	29.31	29.76	29.65	29.31	29.71	34.5
12 - 13	*	*	*	*	31.09	31.23	32.43	31.09	31.83	37.3
13 - 14	*	*	*	*	31.61	30.96	31.88	31.61	31.38	37.2
14 - 15	*	*	*	33.04	30.99	31.73	29.74	31.26	30.59	39
15 - 16	*	*	*	32.01	31.89	31.78	30.58	31.94	31.24	37.55
16 - 17	*	*	*	31.16	31.34	31.39	30.22	31.25	30.68	36.95
17 - 18	*	*	*	32.13	30.86	31.41	31.7	31.42	31.56	37.65
18 - 19	*	*	*	29.93	31.29	31	32.69	30.63	31.76	35.75
19 - 20	*	*	*	31.23	31.84	32.24	31.53	31.55	31.87	36.45
20 - 21	*	*	*	32.32	32.24	31.92	31.6	32.28	31.75	37.75
21 - 22	*	*	*	32.99	32.63	31.87	31.75	32.79	31.81	37.9
22 - 23	*	*	*	31.88	30.9	32.26	31.38	31.36	31.8	36.85
23 - 24	*	*	*	32.31	31.66	31.39	31.83	32.01	31.61	37.1
Totals	0	0	0	31.7	31.3	31.4	31.1			
% of Total	0%	0%	0%	25.26%	24.94%	25.02%	24.78%			

Outgoing Weekly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	29.47	31.21	32.07	*	*	*	*	30.89	*	36.5
1 - 2	30.38	29.43	33.09	*	*	*	*	31.21	*	36.67
2 - 3	29.46	28.83	31.61	*	*	*	*	29.97	*	36.83
3 - 4	31.04	29.5	34.17	*	*	*	*	31.36	*	36
4 - 5	26.62	28	28	*	*	*	*	27.39	*	31.67
5 - 6	29.73	33	28.71	*	*	*	*	30	*	34
6 - 7	30.17	29.22	27.67	*	*	*	*	29.28	*	35.33
7 - 8	24	28.6	27.43	*	*	*	*	27.36	*	30.67
8 - 9	28	26.82	27.67	*	*	*	*	27.37	*	30.67
9 - 10	25.8	28.03	28.31	*	*	*	*	27.94	*	32.23
10 - 11	29.09	29.66	30.79	*	*	*	*	30.13	*	36.47
11 - 12	30.17	29.55	28.93	*	*	*	*	29.31	*	35.17
12 - 13	32.11	30.79	29.65	*	*	*	*	30.47	*	36.4
13 - 14	32.47	30.83	28.31	*	*	*	*	31	*	36.4
14 - 15	31.73	31.01	*	*	*	*	*	31.36	*	36.9
15 - 16	31.73	31.31	*	*	*	*	*	31.5	*	37.25
16 - 17	31.76	31.2	*	*	*	*	*	31.49	*	36.5
17 - 18	31.94	30.67	*	*	*	*	*	31.31	*	36.85
18 - 19	30.99	30.29	*	*	*	*	*	30.62	*	35.75
19 - 20	32.26	31.85	*	*	*	*	*	32.03	*	37.55
20 - 21	31.8	32.05	*	*	*	*	*	31.95	*	37.45
21 - 22	31.82	32.55	*	*	*	*	*	32.26	*	37.95
22 - 23	32.53	32.84	*	*	*	*	*	32.71	*	38.9
23 - 24	31.7	32.3	*	*	*	*	*	32.02	*	37.2
Totals	31.5	31.2	30.2	0	0	0	0			
% of Total	33.91%	33.58%	32.51%	0%	0%	0%	0%			

Outgoing Monthly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	30.83	31.73	*	30.83	31.73	36.7
1 - 2	*	*	*	*	31.68	30.27	*	31.68	30.27	38
2 - 3	*	*	*	*	29.67	29	*	29.67	29	37.3
3 - 4	*	*	*	*	29.24	29.27	*	29.24	29.27	35
4 - 5	*	*	*	*	31	27.8	*	31	27.8	33
5 - 6	*	*	*	*	25.33	25	*	25.33	25	32
6 - 7	*	*	*	*	25.11	25.8	*	25.11	25.8	33.5
7 - 8	*	*	*	*	27.88	30.75	*	27.88	30.75	31
8 - 9	*	*	*	*	27.22	31.29	*	27.22	31.29	33
9 - 10	*	*	*	*	29.48	25.43	*	29.48	25.43	36
10 - 11	*	*	*	*	31.52	30.54	*	31.52	30.54	39.3
11 - 12	*	*	*	*	29.31	29.76	*	29.31	29.76	34.5
12 - 13	*	*	*	*	31.09	31.23	*	31.09	31.23	37.3
13 - 14	*	*	*	*	31.61	30.96	*	31.61	30.96	37.2
14 - 15	*	*	*	33.04	30.99	31.73	*	31.26	31.73	39
15 - 16	*	*	*	32.01	31.89	31.78	*	31.94	31.78	37.55
16 - 17	*	*	*	31.16	31.34	31.39	*	31.25	31.39	36.95
17 - 18	*	*	*	32.13	30.86	31.41	*	31.42	31.41	37.65
18 - 19	*	*	*	29.93	31.29	31	*	30.63	31	35.75
19 - 20	*	*	*	31.23	31.84	32.24	*	31.55	32.24	36.45
20 - 21	*	*	*	32.32	32.24	31.92	*	32.28	31.92	37.75
21 - 22	*	*	*	32.99	32.63	31.87	*	32.79	31.87	37.9
22 - 23	*	*	*	31.88	30.9	32.26	*	31.36	32.26	36.85
23 - 24	*	*	*	32.31	31.66	31.39	*	32.01	31.39	37.1
Totals	0	0	0	31.7	31.3	31.4	0			
% of Total	0%	0%	0%	33.58%	33.16%	33.26%	0%			

Outgoing Monthly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	29.47	31.21	32.07	*	*	*	31.82	30.89	31.82	36.5
1 - 2	30.38	29.43	33.09	*	*	*	31.19	31.21	31.19	36.67
2 - 3	29.46	28.83	31.61	*	*	*	28.44	29.97	28.44	36.83
3 - 4	31.04	29.5	34.17	*	*	*	27.42	31.36	27.42	36
4 - 5	26.62	28	28	*	*	*	29.31	27.39	29.31	31.67
5 - 6	29.73	33	28.71	*	*	*	25.75	30	25.75	34
6 - 7	30.17	29.22	27.67	*	*	*	27.33	29.28	27.33	35.33
7 - 8	24	28.6	27.43	*	*	*	35.33	27.36	35.33	30.67
8 - 9	28	26.82	27.67	*	*	*	28	27.37	28	30.67
9 - 10	25.8	28.03	28.31	*	*	*	28.29	27.94	28.29	32.23
10 - 11	29.09	29.66	30.79	*	*	*	30.35	30.13	30.35	36.47
11 - 12	30.17	29.55	28.93	*	*	*	29.65	29.31	29.65	35.17
12 - 13	32.11	30.79	29.65	*	*	*	32.43	30.47	32.43	36.4
13 - 14	32.47	30.83	28.31	*	*	*	31.88	31	31.88	36.4
14 - 15	31.73	31.01	*	*	*	*	29.74	31.36	29.74	36.9
15 - 16	31.73	31.31	*	*	*	*	30.58	31.5	30.58	37.25
16 - 17	31.76	31.2	*	*	*	*	30.22	31.49	30.22	36.5
17 - 18	31.94	30.67	*	*	*	*	31.7	31.31	31.7	36.85
18 - 19	30.99	30.29	*	*	*	*	32.69	30.62	32.69	35.75
19 - 20	32.26	31.85	*	*	*	*	31.53	32.03	31.53	37.55
20 - 21	31.8	32.05	*	*	*	*	31.6	31.95	31.6	37.45
21 - 22	31.82	32.55	*	*	*	*	31.75	32.26	31.75	37.95
22 - 23	32.53	32.84	*	*	*	*	31.38	32.71	31.38	38.9
23 - 24	31.7	32.3	*	*	*	*	31.83	32.02	31.83	37.2
Totals	31.5	31.2	30.2	0	0	0	31.1			
% of Total	25.4%	25.16%	24.35%	0%	0%	0%	25.08%			

Outgoing Weekly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	36.7	37	37.8	36.7	37.4	36.7
1 - 2	*	*	*	*	38	36.4	37.7	38	37.05	38
2 - 3	*	*	*	*	37.3	36.3	34.2	37.3	35.25	37.3
3 - 4	*	*	*	*	35	36.5	37	35	36.75	35
4 - 5	*	*	*	*	33	30	32	33	31	33
5 - 6	*	*	*	*	32	28	30	32	29	32
6 - 7	*	*	*	*	33.5	25	37	33.5	31	33.5
7 - 8	*	*	*	*	31	33	43	31	38	31
8 - 9	*	*	*	*	33	34	27	33	30.5	33
9 - 10	*	*	*	*	36	29	34	36	31.5	36
10 - 11	*	*	*	*	39.3	36	36	39.3	36	39.3
11 - 12	*	*	*	*	34.5	35.5	35	34.5	35.25	34.5
12 - 13	*	*	*	*	37.3	38	39.5	37.3	38.75	37.3
13 - 14	*	*	*	*	37.2	35.9	37.7	37.2	36.8	37.2
14 - 15	*	*	*	40	38	38	35.5	39	36.75	39
15 - 16	*	*	*	37.9	37.2	37.7	35.8	37.55	36.75	37.55
16 - 17	*	*	*	36.8	37.1	36.9	35.8	36.95	36.35	36.95
17 - 18	*	*	*	39	36.3	37.6	36.9	37.65	37.25	37.65
18 - 19	*	*	*	35.9	35.6	36.2	39	35.75	37.6	35.75
19 - 20	*	*	*	35.8	37.1	38.2	36.2	36.45	37.2	36.45
20 - 21	*	*	*	38.4	37.1	37.1	36.7	37.75	36.9	37.75
21 - 22	*	*	*	38.6	37.2	37.7	36.9	37.9	37.3	37.9
22 - 23	*	*	*	38	35.7	37.2	36.5	36.85	36.85	36.85
23 - 24	*	*	*	37.6	36.6	37	38	37.1	37.5	37.1
Totals	0	0	0	378	861.7	844.2	865.2			
% of Total	0%	0%	0%	12.82%	29.22%	28.63%	29.34%			

Outgoing Weekly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	35.3	35.7	38.5	*	*	*	*	36.5	0	36.5
1 - 2	35.6	35.7	38.7	*	*	*	*	36.67	0	36.67
2 - 3	37.5	34	39	*	*	*	*	36.83	0	36.83
3 - 4	39	33	36	*	*	*	*	36	0	36
4 - 5	35	30	30	*	*	*	*	31.67	0	31.67
5 - 6	32	37	33	*	*	*	*	34	0	34
6 - 7	36	35	35	*	*	*	*	35.33	0	35.33
7 - 8	25	33	34	*	*	*	*	30.67	0	30.67
8 - 9	31	32	29	*	*	*	*	30.67	0	30.67
9 - 10	29	33.7	34	*	*	*	*	32.23	0	32.23
10 - 11	36	34.8	38.6	*	*	*	*	36.47	0	36.47
11 - 12	36.5	35.2	33.8	*	*	*	*	35.17	0	35.17
12 - 13	39	35.8	34.4	*	*	*	*	36.4	0	36.4
13 - 14	38.6	36.3	34.3	*	*	*	*	36.4	0	36.4
14 - 15	36.8	37	*	*	*	*	*	36.9	0	36.9
15 - 16	36.9	37.6	*	*	*	*	*	37.25	0	37.25
16 - 17	36.3	36.7	*	*	*	*	*	36.5	0	36.5
17 - 18	37.2	36.5	*	*	*	*	*	36.85	0	36.85
18 - 19	35.8	35.7	*	*	*	*	*	35.75	0	35.75
19 - 20	38.4	36.7	*	*	*	*	*	37.55	0	37.55
20 - 21	37.7	37.2	*	*	*	*	*	37.45	0	37.45
21 - 22	38.2	37.7	*	*	*	*	*	37.95	0	37.95
22 - 23	39.6	38.2	*	*	*	*	*	38.9	0	38.9
23 - 24	36.9	37.5	*	*	*	*	*	37.2	0	37.2
Totals	859.3	852	488.3	0	0	0	0			
% of Total	39.07%	38.73%	22.2%	0%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	36.7	37	*	36.7	37	36.7
1 - 2	*	*	*	*	38	36.4	*	38	36.4	38
2 - 3	*	*	*	*	37.3	36.3	*	37.3	36.3	37.3
3 - 4	*	*	*	*	35	36.5	*	35	36.5	35
4 - 5	*	*	*	*	33	30	*	33	30	33
5 - 6	*	*	*	*	32	28	*	32	28	32
6 - 7	*	*	*	*	33.5	25	*	33.5	25	33.5
7 - 8	*	*	*	*	31	33	*	31	33	31
8 - 9	*	*	*	*	33	34	*	33	34	33
9 - 10	*	*	*	*	36	29	*	36	29	36
10 - 11	*	*	*	*	39.3	36	*	39.3	36	39.3
11 - 12	*	*	*	*	34.5	35.5	*	34.5	35.5	34.5
12 - 13	*	*	*	*	37.3	38	*	37.3	38	37.3
13 - 14	*	*	*	*	37.2	35.9	*	37.2	35.9	37.2
14 - 15	*	*	*	40	38	38	*	39	38	39
15 - 16	*	*	*	37.9	37.2	37.7	*	37.55	37.7	37.55
16 - 17	*	*	*	36.8	37.1	36.9	*	36.95	36.9	36.95
17 - 18	*	*	*	39	36.3	37.6	*	37.65	37.6	37.65
18 - 19	*	*	*	35.9	35.6	36.2	*	35.75	36.2	35.75
19 - 20	*	*	*	35.8	37.1	38.2	*	36.45	38.2	36.45
20 - 21	*	*	*	38.4	37.1	37.1	*	37.75	37.1	37.75
21 - 22	*	*	*	38.6	37.2	37.7	*	37.9	37.7	37.9
22 - 23	*	*	*	38	35.7	37.2	*	36.85	37.2	36.85
23 - 24	*	*	*	37.6	36.6	37	*	37.1	37	37.1

Outgoing Monthly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	35.3	35.7	38.5	*	*	*	37.8	36.5	37.8	36.5
1 - 2	35.6	35.7	38.7	*	*	*	37.7	36.67	37.7	36.67
2 - 3	37.5	34	39	*	*	*	34.2	36.83	34.2	36.83
3 - 4	39	33	36	*	*	*	37	36	37	36
4 - 5	35	30	30	*	*	*	32	31.67	32	31.67
5 - 6	32	37	33	*	*	*	30	34	30	34
6 - 7	36	35	35	*	*	*	37	35.33	37	35.33
7 - 8	25	33	34	*	*	*	43	30.67	43	30.67
8 - 9	31	32	29	*	*	*	27	30.67	27	30.67
9 - 10	29	33.7	34	*	*	*	34	32.23	34	32.23
10 - 11	36	34.8	38.6	*	*	*	36	36.47	36	36.47
11 - 12	36.5	35.2	33.8	*	*	*	35	35.17	35	35.17
12 - 13	39	35.8	34.4	*	*	*	39.5	36.4	39.5	36.4
13 - 14	38.6	36.3	34.3	*	*	*	37.7	36.4	37.7	36.4
14 - 15	36.8	37	*	*	*	*	35.5	36.9	35.5	36.9
15 - 16	36.9	37.6	*	*	*	*	35.8	37.25	35.8	37.25
16 - 17	36.3	36.7	*	*	*	*	35.8	36.5	35.8	36.5
17 - 18	37.2	36.5	*	*	*	*	36.9	36.85	36.9	36.85
18 - 19	35.8	35.7	*	*	*	*	39	35.75	39	35.75
19 - 20	38.4	36.7	*	*	*	*	36.2	37.55	36.2	37.55
20 - 21	37.7	37.2	*	*	*	*	36.7	37.45	36.7	37.45
21 - 22	38.2	37.7	*	*	*	*	36.9	37.95	36.9	37.95
22 - 23	39.6	38.2	*	*	*	*	36.5	38.9	36.5	38.9
23 - 24	36.9	37.5	*	*	*	*	38	37.2	38	37.2

Summary of Violators

N Birchwood southeast pilots to on ramp 8-29 to 9-4 from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM
 time error -4hrs

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	790	31.2	10	48.4
01:00:00	478	31.1	6	48.0
02:00:00	255	29.3	5	47.0
03:00:00	121	29.8	6	49.7
04:00:00	48	28.5	0	0.0
05:00:00	54	27.3	2	46.5
06:00:00	38	27.5	0	0.0
07:00:00	35	29.9	0	0.0
08:00:00	47	28.0	1	48.0
09:00:00	154	28.1	0	0.0
10:00:00	338	30.5	6	47.7
11:00:00	792	29.4	2	47.5
12:00:00	838	30.9	3	48.3
13:00:00	770	31.3	11	48.7
14:00:00	980	31.0	9	47.9
15:00:00	1169	31.6	14	47.3
16:00:00	1323	31.1	11	47.6
17:00:00	1231	31.4	11	48.9
18:00:00	1506	31.0	12	48.7
19:00:00	1557	31.8	22	48.7
20:00:00	1498	32.0	17	48.6
21:00:00	1464	32.3	21	47.6
22:00:00	1227	31.9	23	48.3
23:00:00	962	31.9	13	48.5

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	14:00	0	0	3	7	3	9	5	0	0	0	0	0	0	0	0	0	0	0	27	33	40	27 to 37	59.3	*	*	1	22	4	
8/29/2024	15:00	0	4	18	38	54	41	18	3	2	0	0	0	0	0	0	0	0	0	178	32	37.9	26 to 36	60.7	3	1.7	1	162	15	
8/29/2024	16:00	0	2	30	52	79	38	19	4	0	0	0	0	0	0	0	0	0	0	224	31.2	36.8	25 to 35	65.2	2	0.9	1	206	17	
8/29/2024	17:00	0	2	19	49	58	33	22	5	1	0	0	0	0	0	0	0	0	0	189	32.1	39	27 to 37	65.6	5	2.6	1	178	10	
8/29/2024	18:00	0	1	58	83	68	40	18	1	2	0	0	0	0	0	0	0	0	0	271	29.9	35.9	23 to 33	66.8	3	1.1	0	257	14	
8/29/2024	19:00	0	6	22	75	107	56	10	6	0	0	0	0	0	0	0	0	0	0	282	31.2	35.8	26 to 36	72.3	4	1.4	1	269	12	
8/29/2024	20:00	1	3	28	50	74	61	28	4	1	0	0	0	0	0	0	0	0	0	250	32.3	38.4	27 to 37	62.0	5	2.0	0	238	12	
8/29/2024	21:00	0	0	18	61	82	63	29	6	1	0	0	0	0	0	0	0	0	0	260	33	38.6	27 to 37	69.6	4	1.5	1	247	12	
8/29/2024	22:00	0	4	20	59	69	49	21	2	1	0	0	0	0	0	0	0	0	0	225	31.9	38	26 to 36	64.0	2	0.9	2	215	8	
8/29/2024	23:00	0	2	12	50	50	50	18	2	0	0	0	0	0	0	0	0	0	0	184	32.3	37.6	27 to 37	64.7	1	0.5	2	175	7	
24 Hr Summary		1	24	228	524	644	440	188	33	8	0	0	0	0	0	0	0	0	0	2090	31.7	38	27 to 37	63.9	29	1.4	10	1969	111	

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/30/2024	00:00	0	1	23	37	55	25	11	2	0	0	0	0	0	0	0	0	0	0	154	30.8	36.7	24 to 34	62.3	1	0.6	1	149	4
8/30/2024	01:00	0	1	13	14	28	11	10	1	0	0	0	0	0	0	0	0	0	0	78	31.7	38	27 to 37	62.8	*	*	0	77	1
8/30/2024	02:00	0	1	14	8	11	7	3	2	0	0	0	0	0	0	0	0	0	0	46	29.7	37.3	20 to 30	54.3	1	2.2	0	46	0
8/30/2024	03:00	0	2	4	6	5	2	2	0	0	0	0	0	0	0	0	0	0	0	21	29.2	35	24 to 34	66.7	*	*	0	21	0
8/30/2024	04:00	0	0	0	4	2	0	1	0	0	0	0	0	0	0	0	0	0	0	7	31	33	23 to 33	85.7	*	*	0	7	0
8/30/2024	05:00	1	0	4	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	9	25.3	32	22 to 32	66.7	*	*	0	9	0
8/30/2024	06:00	1	0	4	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	9	25.1	33.5	15 to 25	55.6	*	*	0	9	0
8/30/2024	07:00	0	0	2	3	3	0	0	0	0	0	0	0	0	0	0	0	0	0	8	27.9	31	21 to 31	87.5	*	*	0	8	0
8/30/2024	08:00	0	1	2	3	2	1	0	0	0	0	0	0	0	0	0	0	0	0	9	27.2	33	23 to 33	66.7	*	*	0	9	0
8/30/2024	09:00	0	1	3	11	8	6	0	0	0	0	0	0	0	0	0	0	0	0	29	29.5	36	23 to 33	69.0	*	*	0	29	0
8/30/2024	10:00	0	4	12	19	17	18	10	3	0	0	0	0	0	0	0	0	0	0	83	31.5	39.3	26 to 36	56.6	1	1.2	1	79	3
8/30/2024	11:00	0	2	41	80	58	23	8	3	0	0	0	0	0	0	0	0	0	0	215	29.3	34.5	23 to 33	72.1	2	0.9	0	213	2
8/30/2024	12:00	0	5	17	57	70	27	21	2	0	0	0	0	0	0	0	0	0	0	199	31.1	37.3	24 to 34	66.8	*	*	2	184	13
8/30/2024	13:00	0	3	15	56	47	40	10	7	0	0	0	0	0	0	0	0	0	0	178	31.6	37.2	27 to 37	65.2	4	2.2	2	161	15
8/30/2024	14:00	0	7	29	43	47	37	12	5	1	0	0	0	0	0	0	0	0	0	181	31	38	25 to 35	55.8	4	2.2	1	163	17
8/30/2024	15:00	0	0	25	45	81	49	19	2	0	0	0	0	0	0	0	0	0	0	221	31.9	37.2	27 to 37	67.0	1	0.5	1	203	17
8/30/2024	16:00	0	3	27	57	66	45	15	5	0	0	0	0	0	0	0	0	0	0	218	31.3	37.1	26 to 36	62.4	3	1.4	2	202	14
8/30/2024	17:00	0	5	28	66	77	48	16	1	1	0	0	0	0	0	0	0	0	0	242	30.9	36.3	26 to 36	65.7	1	0.4	2	223	17
8/30/2024	18:00	0	3	27	82	100	63	14	4	0	0	0	0	0	0	0	0	0	0	293	31.3	35.6	26 to 36	75.1	3	1.0	3	281	9
8/30/2024	19:00	0	3	26	84	97	74	26	2	1	1	0	0	0	0	0	0	0	0	314	31.8	37.1	27 to 37	66.2	3	1.0	3	292	19
8/30/2024	20:00	0	2	26	58	100	75	19	4	0	0	0	0	0	0	0	0	0	0	284	32.2	37.1	28 to 38	71.8	2	0.7	1	268	15
8/30/2024	21:00	0	0	20	77	108	87	21	8	0	0	0	0	0	0	0	0	0	0	321	32.6	37.2	27 to 37	71.3	5	1.6	3	302	16
8/30/2024	22:00	0	3	22	81	83	46	11	3	0	0	0	0	0	0	0	0	0	0	249	30.9	35.7	26 to 36	71.9	2	0.8	1	239	9
8/30/2024	23:00	0	1	21	32	52	36	6	4	0	0	0	0	0	0	0	0	0	0	152	31.7	36.6	27 to 37	67.1	4	2.6	0	149	3
24 Hr Summary		2	48	405	925	1122	721	235	58	3	1	0	0	0	0	0	0	0	0	3520	31.3	37	27 to 37	65.1	37	1.1	23	3323	174

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/31/2024	00:00	0	1	16	21	46	27	6	3	0	0	0	0	0	0	0	0	0	0	120	31.7	37	29 to 39	68.3	*	*	0	117	3
8/31/2024	01:00	0	2	14	39	20	16	6	3	0	0	0	0	0	0	0	0	0	0	100	30.3	36.4	23 to 33	62.0	2	2.0	0	99	1
8/31/2024	02:00	0	4	10	17	9	6	3	2	0	0	0	0	0	0	0	0	0	0	51	29	36.3	21 to 31	56.9	2	3.9	0	51	0
8/31/2024	03:00	1	3	4	6	5	5	0	2	0	0	0	0	0	0	0	0	0	0	26	29.3	36.5	25 to 35	50.0	2	7.7	0	25	1
8/31/2024	04:00	0	1	1	4	3	1	0	0	0	0	0	0	0	0	0	0	0	0	10	27.8	30	20 to 30	70.0	*	*	0	10	0
8/31/2024	05:00	0	1	6	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	11	25	28	18 to 28	81.8	*	*	0	11	0
8/31/2024	06:00	0	0	3	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	5	25.8	25	15 to 25	80.0	*	*	0	5	0
8/31/2024	07:00	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	4	30.8	33	23 to 33	75.0	*	*	0	4	0
8/31/2024	08:00	0	0	1	2	3	0	1	0	0	0	0	0	0	0	0	0	0	0	7	31.3	34	24 to 34	71.4	*	*	0	7	0
8/31/2024	09:00	0	1	2	3	0	1	0	0	0	0	0	0	0	0	0	0	0	0	7	25.4	29	17 to 27	71.4	*	*	0	7	0
8/31/2024	10:00	0	1	7	3	7	6	1	0	1	0	0	0	0	0	0	0	0	0	26	30.5	36	27 to 37	57.7	1	3.8	0	21	5
8/31/2024	11:00	0	2	6	19	8	6	2	2	0	0	0	0	0	0	0	0	0	0	45	29.8	35.5	22 to 32	68.9	*	*	1	42	2
8/31/2024	12:00	0	0	16	24	27	16	7	4	1	0	0	0	0	0	0	0	0	0	95	31.2	38	27 to 37	58.9	1	1.1	1	89	5
8/31/2024	13:00	0	2	12	50	41	30	4	2	0	0	0	0	0	0	0	0	0	0	141	31	35.9	26 to 36	73.8	1	0.7	1	136	4
8/31/2024	14:00	0	2	15	60	52	32	18	4	0	0	0	0	0	0	0	0	0	0	183	31.7	38	26 to 36	65.6	2	1.1	0	175	8
8/31/2024	15:00	0	6	20	56	65	45	23	3	1	0	0	0	0	0	0	0	0	0	219	31.8	37.7	27 to 37	63.0	3	1.4	1	204	14
8/31/2024	16:00	0	0	29	53	63	40	18	2	1	0	0	0	0	0	0	0	0	0	206	31.4	36.9	23 to 33	64.1	3	1.5	0	196	10
8/31/2024	17:00	0	0	29	53	61	31	20	1	1	1	0	0	0	0	0	0	0	0	197	31.4	37.6	24 to 34	64.0	3	1.5	2	187	8
8/31/2024	18:00	0	2	24	74	82	45	17	0	0	0	0	0	0	0	0	0	0	0	244	31	36.2	25 to 35	68.4	*	*	2	233	9
8/31/2024	19:00	0	2	11	59	62	36	17	7	1	0	0	0	0	0	0	0	0	0	195	32.2	38.2	26 to 36	64.6	4	2.1	1	188	6
8/31/2024	20:00	0	0	16	59	73	45	14	4	1	0	0	0	0	0	0	0	0	0	212	31.9	37.1	25 to 35	69.3	3	1.4	0	206	6
8/31/2024	21:00	0	1	17	61	60	41	16	4	1	0	0	0	0	0	0	0	0	0	201	31.9	37.7	27 to 37	65.7	4	2.0	0	193	8
8/31/2024	22:00	0	1	15	37	63	40	13	1	1	0	0	0	0	0	0	0	0	0	171	32.3	37.2	27 to 37	71.3	2	1.2	1	166	4
8/31/2024	23:00	0	1	14	37	47	35	7	1	0	0	0	0	0	0	0	0	0	0	142	31.4	37	27 to 37	64.8	*	*	0	140	2
24 Hr Summary		1	33	289	740	800	507	193	45	9	1	0	0	0	0	0	0	0	0	2618	31.4	38	27 to 37	63.6	33	1.3	10	2512	96

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/1/2024	00:00	0	0	13	29	50	22	7	4	0	0	0	0	0	0	0	0	0	0	125	31.8	37.8	24 to 34	69.6	3	2.4	0	123	2
9/1/2024	01:00	0	1	13	10	17	16	5	1	0	0	0	0	0	0	0	0	0	0	63	31.2	37.7	30 to 40	54.0	*	*	0	63	0
9/1/2024	02:00	0	8	12	18	19	8	2	1	0	0	0	0	0	0	0	0	0	0	68	28.4	34.2	22 to 32	61.8	*	*	0	65	3
9/1/2024	03:00	0	1	10	6	2	4	1	0	0	0	0	0	0	0	0	0	0	0	24	27.4	37	18 to 28	66.7	*	*	0	24	0
9/1/2024	04:00	0	0	3	4	4	0	1	1	0	0	0	0	0	0	0	0	0	0	13	29.3	32	22 to 32	76.9	*	*	0	13	0
9/1/2024	05:00	0	3	2	4	2	0	1	0	0	0	0	0	0	0	0	0	0	0	12	25.8	30	17 to 27	75.0	*	*	0	12	0
9/1/2024	06:00	0	2	1	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	6	27.3	37	11 to 21	50.0	*	*	0	6	0
9/1/2024	07:00	0	0	0	2	2	2	2	1	0	0	0	0	0	0	0	0	0	0	9	35.3	43	25 to 35	55.6	*	*	0	9	0
9/1/2024	08:00	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28	27	22 to 32	100.0	*	*	0	4	0
9/1/2024	09:00	0	0	3	0	3	1	0	0	0	0	0	0	0	0	0	0	0	0	7	28.3	34	24 to 34	57.1	*	*	0	6	1
9/1/2024	10:00	0	0	5	5	8	3	2	0	0	0	0	0	0	0	0	0	0	0	23	30.3	36	23 to 33	69.6	*	*	0	23	0
9/1/2024	11:00	0	1	8	10	11	4	3	0	0	0	0	0	0	0	0	0	0	0	37	29.6	35	23 to 33	64.9	*	*	0	35	2
9/1/2024	12:00	0	7	6	16	34	16	11	4	0	0	0	0	0	0	0	0	0	0	94	32.4	39.5	28 to 38	64.9	1	1.1	0	92	2
9/1/2024	13:00	0	2	15	25	34	30	11	3	0	0	0	0	0	0	0	0	0	0	120	31.9	37.7	27 to 37	60.8	2	1.7	0	119	1
9/1/2024	14:00	0	8	42	71	73	41	7	2	0	0	0	0	0	0	0	0	0	0	244	29.7	35.5	24 to 34	63.5	1	0.4	1	238	5
9/1/2024	15:00	0	2	24	49	68	28	6	4	0	0	0	0	0	0	0	0	0	0	181	30.6	35.8	24 to 34	70.2	4	2.2	0	177	4
9/1/2024	16:00	0	3	52	99	92	54	14	3	0	0	0	0	0	0	0	0	0	0	317	30.2	35.8	24 to 34	66.6	2	0.6	1	307	9
9/1/2024	17:00	0	3	15	59	81	52	14	1	0	0	0	0	0	0	0	0	0	0	225	31.7	36.9	26 to 36	69.3	1	0.4	0	220	5
9/1/2024	18:00	0	3	17	43	63	46	23	6	1	0	0	0	0	0	0	0	0	0	202	32.7	39	27 to 37	59.4	4	2.0	0	201	1
9/1/2024	19:00	0	3	16	64	76	42	13	4	2	0	0	0	0	0	0	0	0	0	220	31.5	36.2	25 to 35	70.9	5	2.3	0	214	6
9/1/2024	20:00	0	4	17	67	88	46	15	2	2	0	0	0	0	0	0	0	0	0	241	31.6	36.7	26 to 36	70.1	3	1.2	0	236	5
9/1/2024	21:00	0	0	22	56	62	45	16	2	0	0	0	0	0	0	0	0	0	0	203	31.7	36.9	27 to 37	70.4	1	0.5	0	201	2
9/1/2024	22:00	0	0	25	41	61	45	7	5	0	0	0	0	0	0	0	0	0	0	184	31.4	36.5	27 to 37	66.3	2	1.1	0	180	4
9/1/2024	23:00	0	3	16	34	35	29	14	4	0	0	0	0	0	0	0	0	0	0	135	31.8	38	25 to 35	58.5	4	3.0	0	130	5
24 Hr Summary		0	54	337	715	887	536	175	48	5	0	0	0	0	0	0	0	0	0	2757	31.1	37	26 to 36	63.7	33	1.2	2	2698	57

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/2/2024	00:00	0	6	23	41	43	18	6	2	0	0	0	0	0	0	0	0	0	0	139	29.5	35.3	24 to 34	65.5	*	*	0	139	0
9/2/2024	01:00	0	2	12	22	17	17	1	1	1	0	0	0	0	0	0	0	0	0	73	30.4	35.6	26 to 36	67.1	2	2.7	2	68	3
9/2/2024	02:00	0	5	6	10	5	10	2	1	0	0	0	0	0	0	0	0	0	0	39	29.5	37.5	26 to 36	48.7	1	2.6	0	38	1
9/2/2024	03:00	0	4	2	4	5	5	3	1	0	0	0	0	0	0	0	0	0	0	24	31	39	26 to 36	45.8	1	4.2	0	24	0
9/2/2024	04:00	0	2	1	1	2	2	0	0	0	0	0	0	0	0	0	0	0	0	8	26.6	35	15 to 25	50.0	*	*	0	8	0
9/2/2024	05:00	0	1	1	6	1	0	0	2	0	0	0	0	0	0	0	0	0	0	11	29.7	32	18 to 28	72.7	2	18.2	0	11	0
9/2/2024	06:00	0	0	2	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	6	30.2	36	27 to 37	66.7	*	*	0	6	0
9/2/2024	07:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24	25	15 to 25	100.0	*	*	0	2	0
9/2/2024	08:00	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28	31	23 to 33	100.0	*	*	0	4	0
9/2/2024	09:00	0	1	5	2	1	0	1	0	0	0	0	0	0	0	0	0	0	0	10	25.8	29	19 to 29	80.0	*	*	0	10	0
9/2/2024	10:00	0	1	2	9	6	3	1	0	0	0	0	0	0	0	0	0	0	0	22	29.1	36	22 to 32	72.7	*	*	0	20	2
9/2/2024	11:00	0	0	5	13	10	6	2	0	0	0	0	0	0	0	0	0	0	0	36	30.2	36.5	24 to 34	69.4	*	*	0	34	2
9/2/2024	12:00	0	1	5	15	20	12	9	0	0	0	0	0	0	0	0	0	0	0	62	32.1	39	27 to 37	66.1	*	*	0	60	2
9/2/2024	13:00	0	2	7	20	39	19	9	4	0	0	0	0	0	0	0	0	0	0	100	32.5	38.6	25 to 35	66.0	3	3.0	0	95	5
9/2/2024	14:00	0	0	19	46	50	37	15	1	0	0	0	0	0	0	0	0	0	0	168	31.7	36.8	26 to 36	67.9	1	0.6	0	160	8
9/2/2024	15:00	0	1	18	40	58	39	12	3	0	0	0	0	0	0	0	0	0	0	171	31.7	36.9	27 to 37	66.1	1	0.6	1	163	7
9/2/2024	16:00	0	1	16	43	73	40	11	2	0	0	0	0	0	0	0	0	0	0	186	31.8	36.3	27 to 37	73.1	*	*	0	181	5
9/2/2024	17:00	0	2	19	41	61	57	11	1	0	0	0	0	0	0	0	0	0	0	192	31.9	37.2	27 to 37	67.7	1	0.5	1	181	10
9/2/2024	18:00	0	2	20	70	87	47	8	0	1	0	0	0	0	0	0	0	0	0	235	31	35.8	26 to 36	71.1	1	0.4	0	227	8
9/2/2024	19:00	0	1	22	61	81	47	27	5	2	0	0	0	0	0	0	0	0	0	246	32.3	38.4	27 to 37	62.6	3	1.2	0	243	3
9/2/2024	20:00	0	3	20	60	59	44	23	2	1	0	0	0	0	0	0	0	0	0	212	31.8	37.7	26 to 36	63.7	2	0.9	0	208	4
9/2/2024	21:00	0	6	17	47	61	37	18	6	0	0	0	0	0	0	0	0	0	0	192	31.8	38.2	25 to 35	60.9	4	2.1	1	186	5
9/2/2024	22:00	0	4	16	39	50	33	23	5	2	0	0	0	0	0	0	0	0	0	172	32.5	39.6	27 to 37	61.6	7	4.1	1	165	6
9/2/2024	23:00	0	2	19	31	57	46	9	1	0	0	0	0	0	0	0	0	0	0	165	31.7	36.9	29 to 39	67.9	1	0.6	0	159	6
24 Hr Summary		0	47	260	623	789	521	191	36	7	0	0	0	0	0	0	0	0	0	2475	31.5	38	27 to 37	64.1	29	1.2	6	2392	77

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/3/2024	00:00	0	2	13	28	44	22	5	1	2	0	0	0	0	0	0	0	0	0	117	31.2	35.7	26 to 36	71.8	3	2.6	1	115	1
9/3/2024	01:00	1	4	16	11	20	10	5	1	0	0	0	0	0	0	0	0	0	0	68	29.4	35.7	27 to 37	55.9	*	*	0	65	3
9/3/2024	02:00	0	1	6	4	9	3	0	0	0	0	0	0	0	0	0	0	0	0	23	28.8	34	23 to 33	65.2	*	*	0	22	1
9/3/2024	03:00	0	2	2	6	2	0	1	0	0	0	0	1	0	0	0	0	0	0	14	29.5	33	18 to 28	64.3	1	7.1	0	13	1
9/3/2024	04:00	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	5	28	30	21 to 31	100.0	*	*	0	5	0
9/3/2024	05:00	0	0	1	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	4	33	37	27 to 37	50.0	*	*	0	3	1
9/3/2024	06:00	1	0	1	2	3	1	1	0	0	0	0	0	0	0	0	0	0	0	9	29.2	35	22 to 32	66.7	*	*	0	9	0
9/3/2024	07:00	0	1	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	5	28.6	33	18 to 28	60.0	*	*	0	5	0
9/3/2024	08:00	0	2	0	6	1	2	0	0	0	0	0	0	0	0	0	0	0	0	11	26.8	32	25 to 35	81.8	*	*	0	10	1
9/3/2024	09:00	0	3	21	11	16	7	1	0	0	0	0	0	0	0	0	0	0	0	59	28	33.7	23 to 33	62.7	*	*	0	58	1
9/3/2024	10:00	0	2	16	27	27	10	5	1	0	0	0	0	0	0	0	0	0	0	88	29.7	34.8	23 to 33	68.2	1	1.1	0	84	4
9/3/2024	11:00	0	6	48	63	77	30	8	1	0	0	0	0	0	0	0	0	0	0	233	29.5	35.2	24 to 34	67.8	*	*	4	225	4
9/3/2024	12:00	0	1	22	57	65	27	17	2	0	0	0	0	0	0	0	0	0	0	191	30.8	35.8	25 to 35	69.6	1	0.5	0	182	9
9/3/2024	13:00	0	6	29	43	62	31	17	0	0	0	0	0	1	0	0	0	0	0	189	30.8	36.3	27 to 37	63.5	1	0.5	4	169	16
9/3/2024	14:00	0	3	27	49	44	35	16	3	0	0	0	0	0	0	0	0	0	0	177	31	37	26 to 36	59.3	1	0.6	1	164	12
9/3/2024	15:00	0	3	29	51	55	36	20	5	0	0	0	0	0	0	0	0	0	0	199	31.3	37.6	23 to 33	58.8	2	1.0	3	179	17
9/3/2024	16:00	0	5	13	52	55	28	17	2	0	0	0	0	0	0	0	0	0	0	172	31.2	36.7	26 to 36	67.4	1	0.6	2	159	11
9/3/2024	17:00	0	2	25	56	60	28	15	0	0	0	0	0	0	0	0	0	0	0	186	30.7	36.5	24 to 34	68.8	*	*	0	176	10
9/3/2024	18:00	0	4	35	79	80	52	10	1	0	0	0	0	0	0	0	0	0	0	261	30.3	35.7	27 to 37	67.0	1	0.4	2	246	13
9/3/2024	19:00	1	2	20	73	110	73	17	4	0	0	0	0	0	0	0	0	0	0	300	31.9	36.7	26 to 36	71.3	3	1.0	0	279	21
9/3/2024	20:00	0	3	24	65	115	67	21	4	0	0	0	0	0	0	0	0	0	0	299	32	37.2	26 to 36	69.2	2	0.7	3	279	17
9/3/2024	21:00	0	2	26	64	86	82	21	6	0	0	0	0	0	0	0	0	0	0	287	32.6	37.7	27 to 37	67.9	3	1.0	1	269	17
9/3/2024	22:00	0	0	15	58	72	50	22	6	3	0	0	0	0	0	0	0	0	0	226	32.8	38.2	26 to 36	67.7	8	3.5	0	216	10
9/3/2024	23:00	0	2	14	44	67	37	12	6	0	2	0	0	0	0	0	0	0	0	184	32.3	37.5	28 to 38	69.6	3	1.6	2	177	5
24 Hr Summary		3	56	403	854	1074	633	232	43	5	2	0	1	1	0	0	0	0	0	3307	31.2	37	26 to 36	64.9	31	0.9	23	3109	175

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG		
9/4/2024	00:00	0	2	16	33	33	34	11	5	0	1	0	0	0	0	0	0	0	0	135	32.1	38.5	26 to 36	60.7	3	2.2	1	127	7		
9/4/2024	01:00	0	1	6	19	29	28	11	2	0	0	0	0	0	0	0	0	0	0	96	33.1	38.7	29 to 39	65.6	2	2.1	0	95	1		
9/4/2024	02:00	0	0	7	4	7	6	2	2	0	0	0	0	0	0	0	0	0	0	28	31.6	39	23 to 33	53.6	1	3.6	0	27	1		
9/4/2024	03:00	0	0	0	3	5	2	0	2	0	0	0	0	0	0	0	0	0	0	12	34.2	36	26 to 36	83.3	2	16.7	0	12	0		
9/4/2024	04:00	0	0	1	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	5	28	30	22 to 32	100.0	*	*	0	3	2		
9/4/2024	05:00	0	0	2	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	7	28.7	33	23 to 33	85.7	*	*	0	7	0		
9/4/2024	06:00	1	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	27.7	35	25 to 35	66.7	*	*	0	3	0		
9/4/2024	07:00	0	0	3	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	7	27.4	34	16 to 26	71.4	*	*	0	7	0		
9/4/2024	08:00	0	1	3	6	1	0	0	1	0	0	0	0	0	0	0	0	0	0	12	27.7	29	22 to 32	83.3	1	8.3	0	12	0		
9/4/2024	09:00	0	0	14	12	10	3	3	0	0	0	0	0	0	0	0	0	0	0	42	28.3	34	20 to 30	69.0	*	*	0	42	0		
9/4/2024	10:00	0	3	17	23	25	16	8	4	0	0	0	0	0	0	0	0	0	0	96	30.8	38.6	24 to 34	54.2	3	3.1	1	93	2		
9/4/2024	11:00	1	4	45	84	60	19	13	0	0	0	0	0	0	0	0	0	0	0	226	28.9	33.8	24 to 34	72.6	*	*	2	217	7		
9/4/2024	12:00	0	3	28	68	65	26	7	0	0	0	0	0	0	0	0	0	0	0	197	29.6	34.4	23 to 33	73.1	*	*	4	181	12		
9/4/2024	13:00	1	2	7	17	8	6	1	0	0	0	0	0	0	0	0	0	0	0	42	28.3	34.3	23 to 33	69.0	*	*	1	39	2		
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		3	16	149	275	249	143	56	16	0	1	0	0	0	0	0	0	0	0	908	30.2	37	23 to 33	63.4	12	1.3	9	865	34		

Outgoing: Average Hourly Volume for Week of 8/26/2024

Average Counts By Hour (8/26/2024) — Average Counts By Hour (8/26/2024)

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 8/26/2024
Average Hourly WEEKDAY Speeds By Hour (8/26/2024) — Average Hourly WEEKDAY Speeds By Hour (8/26/2024)

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs
Average Weekly Speeds



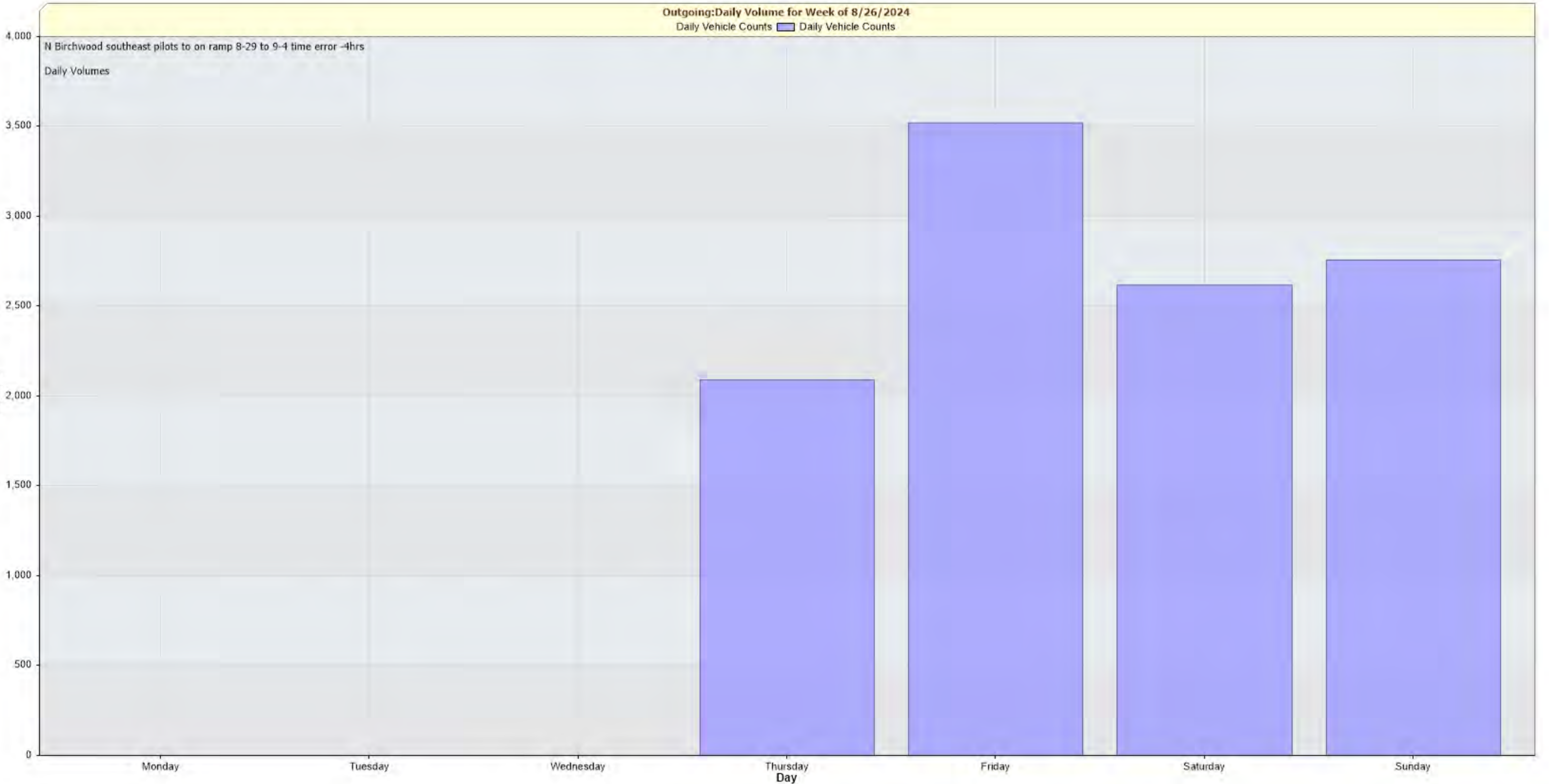
Outgoing:Daily Volume for Week of 8/26/2024

Daily Vehicle Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

Daily Volumes

Vehicles



Monday

Tuesday

Wednesday

Thursday
Day

Friday

Saturday

Sunday

For Project: Birchwood Loop SE on overpass 9-4 to 9-10 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:40:57 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/4/2024 6:00:00 PM through 9/10/2024 3:59:59 PM

85th Percentile Speed 37 MPH

85th Percentile Vehicles 9285

Max Speed 70 MPH on 9/6/2024 11:59:11 PM

Total Vehicles 10924

AADT: 1846

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1545	1560
AM Peak 8:00 AM	146	111
PM Peak 5:00 PM	201	169

Speed

Speed Limit: 45

85th Percentile Speed: 37

50th Percentile Speed: 32

10 MPH Pace Interval: 27.0 MPH to 37.0 MPH

Average Speed: 31.29

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	1	3	1	7	7	19	11
% over limit	0.1	0.3	0.2	0.4	0.3	1.1	0.8
Avg Speeder	49.0	46.7	49.0	47.0	51.3	47.8	47.3
Avg Speed	31.1	31.1	31.5	31.0	31.0	31.7	31.7

Class Counts

	Number	%
VEH_SM	3	0
VEH_MED	10380	95
VEH_LG	541	5
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Incoming Summary
Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/4/2024 7:00:00 PM	37.0	148	174	44	0.0	0.0%	31.0
9/4/2024 8:00:00 PM	37.0	139	163	41	0.0	0.0%	31.6
9/4/2024 9:00:00 PM	38.0	95	112	45	0.0	0.0%	31.8
9/4/2024 10:00:00 PM	38.0	78	92	45	0.0	0.0%	32.5
9/4/2024 11:00:00 PM	35.0	30	35	42	0.0	0.0%	29.5
9/5/2024 12:00:00 AM	40.0	17	20	49	49.0	5.0%	32.9
9/5/2024 1:00:00 AM	37.0	10	12	44	0.0	0.0%	31.8
9/5/2024 2:00:00 AM	31.0	6	7	31	0.0	0.0%	25.4
9/5/2024 3:00:00 AM	32.0	2	2	32	0.0	0.0%	30.0
9/5/2024 4:00:00 AM	46.0	1	1	46	46.0	100.0%	46.0
9/5/2024 5:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
9/5/2024 6:00:00 AM	33.0	10	12	37	0.0	0.0%	27.1
9/5/2024 7:00:00 AM	32.0	24	28	33	0.0	0.0%	27.8
9/5/2024 8:00:00 AM	35.0	57	67	42	0.0	0.0%	29.8
9/5/2024 9:00:00 AM	34.0	137	161	42	0.0	0.0%	29.0
9/5/2024 10:00:00 AM	34.0	94	110	42	0.0	0.0%	28.8
9/5/2024 11:00:00 AM	38.0	99	117	49	47.0	2.6%	31.8
9/5/2024 12:00:00 PM	37.0	72	85	44	0.0	0.0%	31.5
9/5/2024 1:00:00 PM	37.0	75	88	42	0.0	0.0%	31.2
9/5/2024 2:00:00 PM	37.0	88	104	41	0.0	0.0%	31.2
9/5/2024 3:00:00 PM	37.0	93	109	42	0.0	0.0%	31.8
9/5/2024 4:00:00 PM	34.0	130	153	48	48.0	0.7%	29.0
9/5/2024 5:00:00 PM	36.0	144	169	44	0.0	0.0%	31.2
9/5/2024 6:00:00 PM	37.0	178	210	45	0.0	0.0%	31.9
9/5/2024 7:00:00 PM	38.0	159	187	45	0.0	0.0%	32.8
9/5/2024 8:00:00 PM	37.0	118	139	47	47.0	1.4%	32.0
9/5/2024 9:00:00 PM	37.0	68	80	45	0.0	0.0%	32.5
9/5/2024 10:00:00 PM	36.0	52	61	41	0.0	0.0%	31.1
9/5/2024 11:00:00 PM	36.0	30	35	40	0.0	0.0%	30.6
9/6/2024 12:00:00 AM	38.0	19	22	44	0.0	0.0%	31.5
9/6/2024 1:00:00 AM	40.0	13	15	43	0.0	0.0%	31.6
9/6/2024 2:00:00 AM	33.0	5	6	43	0.0	0.0%	30.5
9/6/2024 3:00:00 AM	32.0	7	8	33	0.0	0.0%	28.8
9/6/2024 4:00:00 AM	33.0	2	2	33	0.0	0.0%	31.5
9/6/2024 5:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
9/6/2024 6:00:00 AM	32.0	4	5	47	47.0	20.0%	31.2
9/6/2024 7:00:00 AM	33.0	17	20	38	0.0	0.0%	27.1
9/6/2024 8:00:00 AM	33.0	62	73	44	0.0	0.0%	28.0
9/6/2024 9:00:00 AM	33.0	112	132	40	0.0	0.0%	28.4
9/6/2024 10:00:00 AM	35.0	99	116	44	0.0	0.0%	29.7
9/6/2024 11:00:00 AM	36.0	62	73	41	0.0	0.0%	30.9
9/6/2024 12:00:00 PM	37.0	100	118	48	48.0	0.8%	30.5
9/6/2024 1:00:00 PM	37.0	112	132	42	0.0	0.0%	31.5
9/6/2024 2:00:00 PM	36.0	109	128	42	0.0	0.0%	30.7
9/6/2024 3:00:00 PM	37.0	177	208	44	0.0	0.0%	31.8
9/6/2024 4:00:00 PM	35.0	157	185	40	0.0	0.0%	30.2
9/6/2024 5:00:00 PM	36.0	164	193	43	0.0	0.0%	31.4

Incoming Summary
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

9/6/2024 6:00:00 PM	37.0	175	206	43	0.0	0.0%	32.5
9/6/2024 7:00:00 PM	36.0	169	199	41	0.0	0.0%	32.2
9/6/2024 8:00:00 PM	37.0	145	171	45	0.0	0.0%	32.2
9/6/2024 9:00:00 PM	38.0	93	109	47	47.0	2.8%	30.8
9/6/2024 10:00:00 PM	36.0	47	55	43	0.0	0.0%	31.7
9/6/2024 11:00:00 PM	35.0	30	35	42	0.0	0.0%	31.0
9/7/2024 12:00:00 AM	37.0	30	35	70	61.5	5.7%	31.3
9/7/2024 1:00:00 AM	32.0	12	14	39	0.0	0.0%	29.4
9/7/2024 2:00:00 AM	35.0	8	10	42	0.0	0.0%	31.6
9/7/2024 3:00:00 AM	34.0	11	13	48	48.0	7.7%	30.2
9/7/2024 4:00:00 AM	30.0	3	3	30	0.0	0.0%	28.7
9/7/2024 5:00:00 AM	29.0	2	2	29	0.0	0.0%	28.5
9/7/2024 6:00:00 AM	38.0	6	7	44	0.0	0.0%	33.1
9/7/2024 7:00:00 AM	36.0	12	14	43	0.0	0.0%	29.6
9/7/2024 8:00:00 AM	38.0	31	36	44	0.0	0.0%	31.2
9/7/2024 9:00:00 AM	38.0	37	43	44	0.0	0.0%	32.1
9/7/2024 10:00:00 AM	37.0	43	51	47	47.0	3.9%	32.3
9/7/2024 11:00:00 AM	35.0	84	99	44	0.0	0.0%	30.2
9/7/2024 12:00:00 PM	37.0	110	129	50	48.0	1.6%	31.3
9/7/2024 1:00:00 PM	36.0	110	129	45	0.0	0.0%	31.7
9/7/2024 2:00:00 PM	37.0	132	155	47	46.5	1.3%	31.9
9/7/2024 3:00:00 PM	37.0	125	147	43	0.0	0.0%	31.2
9/7/2024 4:00:00 PM	37.0	116	136	46	46.0	0.7%	31.4
9/7/2024 5:00:00 PM	37.0	119	140	42	0.0	0.0%	31.5
9/7/2024 6:00:00 PM	38.0	121	142	57	57.0	0.7%	32.5
9/7/2024 7:00:00 PM	39.0	132	155	48	47.0	1.3%	33.0
9/7/2024 8:00:00 PM	37.0	94	111	46	46.0	0.9%	32.3
9/7/2024 9:00:00 PM	37.0	94	111	49	49.0	0.9%	31.4
9/7/2024 10:00:00 PM	38.0	64	75	48	48.0	6.7%	33.0
9/7/2024 11:00:00 PM	39.0	36	42	46	46.0	2.4%	31.5
9/8/2024 12:00:00 AM	36.0	37	43	45	0.0	0.0%	32.4
9/8/2024 1:00:00 AM	37.0	15	18	46	46.0	5.6%	31.4
9/8/2024 2:00:00 AM	34.0	8	9	36	0.0	0.0%	30.9
9/8/2024 3:00:00 AM	31.0	3	3	31	0.0	0.0%	29.3
9/8/2024 4:00:00 AM	31.0	3	3	31	0.0	0.0%	26.0
9/8/2024 5:00:00 AM	42.0	2	2	42	0.0	0.0%	32.5
9/8/2024 6:00:00 AM	38.0	2	2	38	0.0	0.0%	31.5
9/8/2024 7:00:00 AM	31.0	4	5	31	0.0	0.0%	27.6
9/8/2024 8:00:00 AM	32.0	16	19	39	0.0	0.0%	29.2
9/8/2024 9:00:00 AM	37.0	34	40	44	0.0	0.0%	30.6
9/8/2024 10:00:00 AM	36.0	55	65	44	0.0	0.0%	31.6
9/8/2024 11:00:00 AM	36.0	90	106	42	0.0	0.0%	30.9
9/8/2024 12:00:00 PM	37.0	89	105	49	48.0	1.9%	31.8
9/8/2024 1:00:00 PM	36.0	99	117	45	0.0	0.0%	31.8
9/8/2024 2:00:00 PM	38.0	84	99	44	0.0	0.0%	32.8
9/8/2024 3:00:00 PM	38.0	93	109	47	47.0	0.9%	32.9
9/8/2024 4:00:00 PM	36.0	99	116	46	46.0	0.9%	31.6
9/8/2024 5:00:00 PM	37.0	84	99	43	0.0	0.0%	31.2

Incoming Summary
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

9/8/2024 6:00:00 PM	38.0	87	102	42	0.0	0.0%	31.9
9/8/2024 7:00:00 PM	39.0	82	96	51	49.0	3.1%	32.8
9/8/2024 8:00:00 PM	38.0	69	81	46	46.0	3.7%	32.2
9/8/2024 9:00:00 PM	35.0	65	77	43	0.0	0.0%	31.9
9/8/2024 10:00:00 PM	36.0	50	59	45	0.0	0.0%	30.2
9/8/2024 11:00:00 PM	36.0	30	35	42	0.0	0.0%	30.8
9/9/2024 12:00:00 AM	35.0	18	21	44	0.0	0.0%	32.9
9/9/2024 1:00:00 AM	37.0	15	18	39	0.0	0.0%	30.3
9/9/2024 2:00:00 AM	32.0	4	5	32	0.0	0.0%	30.0
9/9/2024 3:00:00 AM	42.0	12	14	44	0.0	0.0%	32.1
9/9/2024 4:00:00 AM	30.0	7	8	36	0.0	0.0%	26.8
9/9/2024 5:00:00 AM	35.0	3	4	39	0.0	0.0%	33.0
9/9/2024 6:00:00 AM	37.0	8	9	45	0.0	0.0%	29.6
9/9/2024 7:00:00 AM	31.0	20	24	34	0.0	0.0%	25.4
9/9/2024 8:00:00 AM	34.0	48	57	40	0.0	0.0%	29.9
9/9/2024 9:00:00 AM	34.0	110	130	44	0.0	0.0%	29.5
9/9/2024 10:00:00 AM	34.0	95	112	45	0.0	0.0%	29.8
9/9/2024 11:00:00 AM	36.0	99	116	43	0.0	0.0%	30.0
9/9/2024 12:00:00 PM	36.0	84	99	42	0.0	0.0%	31.4
9/9/2024 1:00:00 PM	37.0	88	104	45	0.0	0.0%	31.5
9/9/2024 2:00:00 PM	38.0	94	111	49	49.0	0.9%	32.2
9/9/2024 3:00:00 PM	39.0	85	100	43	0.0	0.0%	31.7
9/9/2024 4:00:00 PM	35.0	107	126	41	0.0	0.0%	30.4
9/9/2024 5:00:00 PM	36.0	106	125	43	0.0	0.0%	31.5
9/9/2024 6:00:00 PM	36.0	161	189	44	0.0	0.0%	31.9
9/9/2024 7:00:00 PM	38.0	161	189	45	0.0	0.0%	32.7
9/9/2024 8:00:00 PM	37.0	113	133	42	0.0	0.0%	32.4
9/9/2024 9:00:00 PM	37.0	70	82	43	0.0	0.0%	31.6
9/9/2024 10:00:00 PM	37.0	65	77	42	0.0	0.0%	31.2
9/9/2024 11:00:00 PM	38.0	30	35	40	0.0	0.0%	29.9
9/10/2024 12:00:00 AM	35.0	16	19	38	0.0	0.0%	28.2
9/10/2024 1:00:00 AM	37.0	13	15	44	0.0	0.0%	31.9
9/10/2024 2:00:00 AM	45.0	7	8	46	46.0	12.5%	33.6
9/10/2024 3:00:00 AM	31.0	3	3	31	0.0	0.0%	31.0
9/10/2024 4:00:00 AM	28.0	2	2	28	0.0	0.0%	26.5
9/10/2024 5:00:00 AM	30.0	2	2	30	0.0	0.0%	25.5
9/10/2024 6:00:00 AM	35.0	8	10	36	0.0	0.0%	31.3
9/10/2024 7:00:00 AM	35.0	13	15	40	0.0	0.0%	31.1
9/10/2024 8:00:00 AM	35.0	56	66	42	0.0	0.0%	29.7
9/10/2024 9:00:00 AM	34.0	139	163	43	0.0	0.0%	28.9
9/10/2024 10:00:00 AM	36.0	132	155	43	0.0	0.0%	30.1
9/10/2024 11:00:00 AM	35.0	104	122	40	0.0	0.0%	30.3
9/10/2024 12:00:00 PM	37.0	90	106	44	0.0	0.0%	32.3
9/10/2024 1:00:00 PM	37.0	97	114	44	0.0	0.0%	32.0
9/10/2024 2:00:00 PM	38.0	111	131	48	48.0	0.8%	33.4
9/10/2024 3:00:00 PM	39.0	125	147	46	46.0	0.7%	32.4
9/10/2024 4:00:00 PM	40.0	3	3	40	0.0	0.0%	33.0

Incoming Summary
Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/5/2024 12:00:00 AM	37.0	507	596	49	49.0	0.2%	31.5
9/6/2024 12:00:00 AM	36.0	1666	1960	49	47.0	0.4%	31.0
9/7/2024 12:00:00 AM	36.0	1891	2225	70	51.3	0.3%	31.0
9/8/2024 12:00:00 AM	37.0	1536	1807	57	47.8	1.1%	31.7
9/9/2024 12:00:00 AM	37.0	1180	1388	51	47.3	0.8%	31.7
9/10/2024 12:00:00 AM	37.0	1603	1886	49	49.0	0.1%	31.1
9/10/2024 3:59:59 PM	36.0	903	1062	48	46.7	0.3%	31.1

Incoming Weekly Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	12	15	14	18	13.5	16	38.5
1 - 2	*	*	*	7	6	10	9	6.5	9.5	31.75
2 - 3	*	*	*	2	8	13	3	5	8	32
3 - 4	*	*	*	1	2	3	3	1.5	3	39.5
4 - 5	*	*	*	1	1	2	2	1	2	31.5
5 - 6	*	*	*	12	5	7	2	8.5	4.5	32.5
6 - 7	*	*	*	28	20	14	5	24	9.5	32.35
7 - 8	*	*	*	67	73	36	19	70	27.5	34
8 - 9	*	*	*	161	132	43	40	146.5	41.5	33.15
9 - 10	*	*	*	110	116	51	65	113	58	34.2
10 - 11	*	*	*	117	73	99	106	95	102.5	36.65
11 - 12	*	*	*	85	118	129	105	101.5	117	36.3
12 - 13	*	*	*	88	132	129	117	110	123	36.8
13 - 14	*	*	*	104	128	155	99	116	127	35.9
14 - 15	*	*	*	109	208	147	109	158.5	128	36.65
15 - 16	*	*	*	153	185	136	116	169	126	33.9
16 - 17	*	*	*	169	193	140	99	181	119.5	35.6
17 - 18	*	*	*	210	206	142	102	208	122	36.5
18 - 19	*	*	174	187	199	155	96	186.67	125.5	36.47
19 - 20	*	*	163	139	171	111	81	157.67	96	36.7
20 - 21	*	*	112	80	109	111	77	100.33	94	37.4
21 - 22	*	*	92	61	55	75	59	69.33	67	36.3
22 - 23	*	*	35	35	35	42	35	35	38.5	34.9
23 - 24	*	*	20	22	35	43	21	25.67	32	38.33
Totals	0	0	596	1960	2225	1807	1388			
% of Total	0%	0%	7.47%	24.57%	27.9%	22.66%	17.4%			

Incoming Weekly Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	18	15	*	*	*	*	*	16.5	0	37
1 - 2	5	8	*	*	*	*	*	6.5	0	38.25
2 - 3	14	3	*	*	*	*	*	8.5	0	36.5
3 - 4	8	2	*	*	*	*	*	5	0	29
4 - 5	4	2	*	*	*	*	*	3	0	32.5
5 - 6	9	10	*	*	*	*	*	9.5	0	36
6 - 7	24	15	*	*	*	*	*	19.5	0	32.75
7 - 8	57	66	*	*	*	*	*	61.5	0	34.5
8 - 9	130	163	*	*	*	*	*	146.5	0	33.55
9 - 10	112	155	*	*	*	*	*	133.5	0	34.5
10 - 11	116	122	*	*	*	*	*	119	0	35.2
11 - 12	99	106	*	*	*	*	*	102.5	0	36.05
12 - 13	104	114	*	*	*	*	*	109	0	36.7
13 - 14	111	131	*	*	*	*	*	121	0	37.3
14 - 15	100	147	*	*	*	*	*	123.5	0	38.25
15 - 16	126	3	*	*	*	*	*	64.5	0	37.5
16 - 17	125	*	*	*	*	*	*	125	0	36
17 - 18	189	*	*	*	*	*	*	189	0	35.5
18 - 19	189	*	*	*	*	*	*	189	0	37.3
19 - 20	133	*	*	*	*	*	*	133	0	36.4
20 - 21	82	*	*	*	*	*	*	82	0	36.7
21 - 22	77	*	*	*	*	*	*	77	0	36.6
22 - 23	35	*	*	*	*	*	*	35	0	37.3
23 - 24	19	*	*	*	*	*	*	19	0	35
Totals	1886	1062	0	0	0	0	0			
% of Total	63.98%	36.02%	0%	0%	0%	0%	0%			

Incoming Monthly Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	18	15	*	12	15	14	18	15	16	37.75
1 - 2	5	8	*	7	6	10	9	6.5	9.5	35
2 - 3	14	3	*	2	8	13	3	6.75	8	34.25
3 - 4	8	2	*	1	2	3	3	3.25	3	34.25
4 - 5	4	2	*	1	1	2	2	2	2	32
5 - 6	9	10	*	12	5	7	2	9	4.5	34.25
6 - 7	24	15	*	28	20	14	5	21.75	9.5	32.55
7 - 8	57	66	*	67	73	36	19	65.75	27.5	34.25
8 - 9	130	163	*	161	132	43	40	146.5	41.5	33.35
9 - 10	112	155	*	110	116	51	65	123.25	58	34.35
10 - 11	116	122	*	117	73	99	106	107	102.5	35.92
11 - 12	99	106	*	85	118	129	105	102	117	36.17
12 - 13	104	114	*	88	132	129	117	109.5	123	36.75
13 - 14	111	131	*	104	128	155	99	118.5	127	36.6
14 - 15	100	147	*	109	208	147	109	141	128	37.45
15 - 16	126	3	*	153	185	136	116	116.75	126	35.7
16 - 17	125	*	*	169	193	140	99	162.33	119.5	35.73
17 - 18	189	*	*	210	206	142	102	201.67	122	36.17
18 - 19	189	*	174	187	199	155	96	187.25	125.5	36.67
19 - 20	133	*	163	139	171	111	81	151.5	96	36.62
20 - 21	82	*	112	80	109	111	77	95.75	94	37.22
21 - 22	77	*	92	61	55	75	59	71.25	67	36.38
22 - 23	35	*	35	35	35	42	35	35	38.5	35.5
23 - 24	19	*	20	22	35	43	21	24	32	37.5
Totals	1886	1062	596	1960	2225	1807	1388			
% of Total	17.26%	9.72%	5.46%	17.94%	20.37%	16.54%	12.71%			

Incoming Weekly Speeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	31.75	31.6	29.43	31.44	31.67	30.56	38.5
1 - 2	*	*	*	25.43	30.5	31.6	30.89	27.77	31.26	31.75
2 - 3	*	*	*	30	28.75	30.23	29.33	29	30.06	32
3 - 4	*	*	*	46	31.5	28.67	26	36.33	27.33	39.5
4 - 5	*	*	*	35	28	28.5	32.5	31.5	30.5	31.5
5 - 6	*	*	*	27.08	31.2	33.14	31.5	28.29	32.78	32.5
6 - 7	*	*	*	27.82	27.1	29.64	27.6	27.52	29.11	32.35
7 - 8	*	*	*	29.81	27.97	31.22	29.16	28.85	30.51	34
8 - 9	*	*	*	29.05	28.42	32.09	30.65	28.77	31.4	33.15
9 - 10	*	*	*	28.83	29.73	32.27	31.57	29.29	31.88	34.2
10 - 11	*	*	*	31.82	30.9	30.17	30.92	31.47	30.56	36.65
11 - 12	*	*	*	31.53	30.5	31.34	31.8	30.93	31.55	36.3
12 - 13	*	*	*	31.22	31.52	31.74	31.78	31.4	31.76	36.8
13 - 14	*	*	*	31.17	30.73	31.86	32.8	30.93	32.23	35.9
14 - 15	*	*	*	31.77	31.79	31.22	32.94	31.78	31.95	36.65
15 - 16	*	*	*	28.98	30.22	31.35	31.65	29.66	31.49	33.9
16 - 17	*	*	*	31.18	31.43	31.49	31.21	31.31	31.37	35.6
17 - 18	*	*	*	31.86	32.46	32.48	31.9	32.16	32.24	36.5
18 - 19	*	*	30.95	32.84	32.22	32.97	32.83	32.03	32.92	36.47
19 - 20	*	*	31.64	31.99	32.18	32.31	32.17	31.94	32.25	36.7
20 - 21	*	*	31.82	32.5	30.75	31.37	31.94	31.61	31.6	37.4
21 - 22	*	*	32.48	31.15	31.73	33	30.17	31.89	31.75	36.3
22 - 23	*	*	29.51	30.57	30.97	31.48	30.83	30.35	31.18	34.9
23 - 24	*	*	32.9	31.55	31.29	32.37	32.86	31.78	32.53	38.33
Totals	0	0	31.5	31	31	31.8	31.7			
% of Total	0%	0%	20.06%	19.75%	19.75%	20.25%	20.19%			

Incoming Weekly Speeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	30.33	31.87	*	*	*	*	*	31.03	*	37
1 - 2	30	33.62	*	*	*	*	*	32.23	*	38.25
2 - 3	32.14	31	*	*	*	*	*	31.94	*	36.5
3 - 4	26.75	26.5	*	*	*	*	*	26.7	*	29
4 - 5	33	25.5	*	*	*	*	*	30.5	*	32.5
5 - 6	29.56	31.3	*	*	*	*	*	30.47	*	36
6 - 7	25.42	31.07	*	*	*	*	*	27.59	*	32.75
7 - 8	29.95	29.73	*	*	*	*	*	29.83	*	34.5
8 - 9	29.54	28.88	*	*	*	*	*	29.17	*	33.55
9 - 10	29.75	30.15	*	*	*	*	*	29.98	*	34.5
10 - 11	29.97	30.28	*	*	*	*	*	30.13	*	35.2
11 - 12	31.35	32.33	*	*	*	*	*	31.86	*	36.05
12 - 13	31.5	32.04	*	*	*	*	*	31.78	*	36.7
13 - 14	32.23	33.43	*	*	*	*	*	32.88	*	37.3
14 - 15	31.68	32.38	*	*	*	*	*	32.1	*	38.25
15 - 16	30.37	33	*	*	*	*	*	30.43	*	37.5
16 - 17	31.53	*	*	*	*	*	*	31.53	*	36
17 - 18	31.9	*	*	*	*	*	*	31.9	*	35.5
18 - 19	32.69	*	*	*	*	*	*	32.69	*	37.3
19 - 20	32.39	*	*	*	*	*	*	32.39	*	36.4
20 - 21	31.62	*	*	*	*	*	*	31.62	*	36.7
21 - 22	31.22	*	*	*	*	*	*	31.22	*	36.6
22 - 23	29.89	*	*	*	*	*	*	29.89	*	37.3
23 - 24	28.21	*	*	*	*	*	*	28.21	*	35
Totals	31.1	31.1	0	0	0	0	0			
% of Total	50%	50%	0%	0%	0%	0%	0%			

Incoming Monthly Speeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	30.33	31.87	*	31.75	31.6	29.43	31.44	31.32	30.56	37.75
1 - 2	30	33.62	*	25.43	30.5	31.6	30.89	30	31.26	35
2 - 3	32.14	31	*	30	28.75	30.23	29.33	30.85	30.06	34.25
3 - 4	26.75	26.5	*	46	31.5	28.67	26	28.92	27.33	34.25
4 - 5	33	25.5	*	35	28	28.5	32.5	30.75	30.5	32
5 - 6	29.56	31.3	*	27.08	31.2	33.14	31.5	29.44	32.78	34.25
6 - 7	25.42	31.07	*	27.82	27.1	29.64	27.6	27.55	29.11	32.55
7 - 8	29.95	29.73	*	29.81	27.97	31.22	29.16	29.31	30.51	34.25
8 - 9	29.54	28.88	*	29.05	28.42	32.09	30.65	28.97	31.4	33.35
9 - 10	29.75	30.15	*	28.83	29.73	32.27	31.57	29.67	31.88	34.35
10 - 11	29.97	30.28	*	31.82	30.9	30.17	30.92	30.72	30.56	35.92
11 - 12	31.35	32.33	*	31.53	30.5	31.34	31.8	31.4	31.55	36.17
12 - 13	31.5	32.04	*	31.22	31.52	31.74	31.78	31.59	31.76	36.75
13 - 14	32.23	33.43	*	31.17	30.73	31.86	32.8	31.93	32.23	36.6
14 - 15	31.68	32.38	*	31.77	31.79	31.22	32.94	31.92	31.95	37.45
15 - 16	30.37	33	*	28.98	30.22	31.35	31.65	29.87	31.49	35.7
16 - 17	31.53	*	*	31.18	31.43	31.49	31.21	31.37	31.37	35.73
17 - 18	31.9	*	*	31.86	32.46	32.48	31.9	32.08	32.24	36.17
18 - 19	32.69	*	30.95	32.84	32.22	32.97	32.83	32.2	32.92	36.67
19 - 20	32.39	*	31.64	31.99	32.18	32.31	32.17	32.04	32.25	36.62
20 - 21	31.62	*	31.82	32.5	30.75	31.37	31.94	31.62	31.6	37.22
21 - 22	31.22	*	32.48	31.15	31.73	33	30.17	31.71	31.75	36.38
22 - 23	29.89	*	29.51	30.57	30.97	31.48	30.83	30.24	31.18	35.5
23 - 24	28.21	*	32.9	31.55	31.29	32.37	32.86	31.07	32.53	37.5
Totals	31.1	31.1	31.5	31	31	31.8	31.7			
% of Total	14.19%	14.19%	14.37%	14.14%	14.14%	14.51%	14.46%			

Incoming Weekly EightyFifthSpeeds
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	37	40	32	37	38.5	34.5	38.5
1 - 2	*	*	*	30.5	33	35	34	31.75	34.5	31.75
2 - 3	*	*	*	32	32	34	31	32	32.5	32
3 - 4	*	*	*	46	33	30	31	39.5	30.5	39.5
4 - 5	*	*	*	35	28	29	42	31.5	35.5	31.5
5 - 6	*	*	*	33	32	38	38	32.5	38	32.5
6 - 7	*	*	*	31.7	33	36	30.5	32.35	33.25	32.35
7 - 8	*	*	*	35	33	37.7	32	34	34.85	34
8 - 9	*	*	*	33.4	32.9	38	36.3	33.15	37.15	33.15
9 - 10	*	*	*	34	34.4	37	35.3	34.2	36.15	34.2
10 - 11	*	*	*	37.6	35.7	34.2	36	36.65	35.1	36.65
11 - 12	*	*	*	36.5	36.1	36.3	36.5	36.3	36.4	36.3
12 - 13	*	*	*	37	36.6	35.8	36	36.8	35.9	36.8
13 - 14	*	*	*	36.3	35.5	36.8	37.6	35.9	37.2	35.9
14 - 15	*	*	*	36.8	36.5	36.1	37.5	36.65	36.8	36.65
15 - 16	*	*	*	33.6	34.2	36.3	36	33.9	36.15	33.9
16 - 17	*	*	*	35.7	35.5	37	37	35.6	37	35.6
17 - 18	*	*	*	36.1	36.9	37.2	37.2	36.5	37.2	36.5
18 - 19	*	*	36.2	37.2	36	38.2	38.4	36.47	38.3	36.47
19 - 20	*	*	36.8	36.5	36.8	37	38	36.7	37.5	36.7
20 - 21	*	*	38	37	37.2	36.5	35	37.4	35.75	37.4
21 - 22	*	*	37.4	35.9	35.6	37.7	36	36.3	36.85	36.3
22 - 23	*	*	34.5	35.7	34.5	38.5	36	34.9	37.25	34.9
23 - 24	*	*	40	38	37	36	35	38.33	35.5	38.33
Totals	0	0	222.9	857.5	835.4	860.3	859.3			
% of Total	0%	0%	6.13%	23.59%	22.98%	23.66%	23.64%			

Incoming Weekly EightyFifthSpeeds
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	37	37	*	*	*	*	*	37	0	37
1 - 2	31.5	45	*	*	*	*	*	38.25	0	38.25
2 - 3	42	31	*	*	*	*	*	36.5	0	36.5
3 - 4	30	28	*	*	*	*	*	29	0	29
4 - 5	35	30	*	*	*	*	*	32.5	0	32.5
5 - 6	37	35	*	*	*	*	*	36	0	36
6 - 7	30.5	35	*	*	*	*	*	32.75	0	32.75
7 - 8	34	35	*	*	*	*	*	34.5	0	34.5
8 - 9	33.6	33.5	*	*	*	*	*	33.55	0	33.55
9 - 10	33.8	35.2	*	*	*	*	*	34.5	0	34.5
10 - 11	35.4	35	*	*	*	*	*	35.2	0	35.2
11 - 12	35.8	36.3	*	*	*	*	*	36.05	0	36.05
12 - 13	36.6	36.8	*	*	*	*	*	36.7	0	36.7
13 - 14	37.3	37.3	*	*	*	*	*	37.3	0	37.3
14 - 15	38.4	38.1	*	*	*	*	*	38.25	0	38.25
15 - 16	35	40	*	*	*	*	*	37.5	0	37.5
16 - 17	36	*	*	*	*	*	*	36	0	36
17 - 18	35.5	*	*	*	*	*	*	35.5	0	35.5
18 - 19	37.3	*	*	*	*	*	*	37.3	0	37.3
19 - 20	36.4	*	*	*	*	*	*	36.4	0	36.4
20 - 21	36.7	*	*	*	*	*	*	36.7	0	36.7
21 - 22	36.6	*	*	*	*	*	*	36.6	0	36.6
22 - 23	37.3	*	*	*	*	*	*	37.3	0	37.3
23 - 24	35	*	*	*	*	*	*	35	0	35
Totals	853.7	568.2	0	0	0	0	0			
% of Total	60.04%	39.96%	0%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	37	37	*	37	40	32	37	37.75	34.5	37.75
1 - 2	31.5	45	*	30.5	33	35	34	35	34.5	35
2 - 3	42	31	*	32	32	34	31	34.25	32.5	34.25
3 - 4	30	28	*	46	33	30	31	34.25	30.5	34.25
4 - 5	35	30	*	35	28	29	42	32	35.5	32
5 - 6	37	35	*	33	32	38	38	34.25	38	34.25
6 - 7	30.5	35	*	31.7	33	36	30.5	32.55	33.25	32.55
7 - 8	34	35	*	35	33	37.7	32	34.25	34.85	34.25
8 - 9	33.6	33.5	*	33.4	32.9	38	36.3	33.35	37.15	33.35
9 - 10	33.8	35.2	*	34	34.4	37	35.3	34.35	36.15	34.35
10 - 11	35.4	35	*	37.6	35.7	34.2	36	35.92	35.1	35.92
11 - 12	35.8	36.3	*	36.5	36.1	36.3	36.5	36.17	36.4	36.17
12 - 13	36.6	36.8	*	37	36.6	35.8	36	36.75	35.9	36.75
13 - 14	37.3	37.3	*	36.3	35.5	36.8	37.6	36.6	37.2	36.6
14 - 15	38.4	38.1	*	36.8	36.5	36.1	37.5	37.45	36.8	37.45
15 - 16	35	40	*	33.6	34.2	36.3	36	35.7	36.15	35.7
16 - 17	36	*	*	35.7	35.5	37	37	35.73	37	35.73
17 - 18	35.5	*	*	36.1	36.9	37.2	37.2	36.17	37.2	36.17
18 - 19	37.3	*	36.2	37.2	36	38.2	38.4	36.67	38.3	36.67
19 - 20	36.4	*	36.8	36.5	36.8	37	38	36.62	37.5	36.62
20 - 21	36.7	*	38	37	37.2	36.5	35	37.22	35.75	37.22
21 - 22	36.6	*	37.4	35.9	35.6	37.7	36	36.38	36.85	36.38
22 - 23	37.3	*	34.5	35.7	34.5	38.5	36	35.5	37.25	35.5
23 - 24	35	*	40	38	37	36	35	37.5	35.5	37.5

Summary of Violators

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	92	31.1	1	46.0
01:00:00	45	30.5	1	46.0
02:00:00	43	30.6	1	48.0
03:00:00	19	28.4	1	46.0
04:00:00	12	30.7	0	0.0
05:00:00	45	30.1	1	47.0
06:00:00	106	27.8	0	0.0
07:00:00	318	29.5	0	0.0
08:00:00	669	29.3	0	0.0
09:00:00	609	30.1	2	47.0
10:00:00	633	30.7	3	47.0
11:00:00	642	31.5	5	48.0
12:00:00	684	31.7	0	0.0
13:00:00	728	32.0	4	47.5
14:00:00	820	31.9	2	46.5
15:00:00	719	30.4	3	46.7
16:00:00	726	31.4	0	0.0
17:00:00	849	32.1	1	57.0
18:00:00	1000	32.4	5	48.2
19:00:00	798	32.1	6	46.3
20:00:00	571	31.6	4	47.5
21:00:00	419	31.7	5	48.0
22:00:00	217	30.6	1	46.0
23:00:00	160	31.7	3	57.3

Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	6	7	7	33	63	47	11	0	0	0	0	0	0	0	0	0	0	0	174	30.9	36.2	27 to 37	72.4	*	*	2	166	6	
9/4/2024	19:00	3	2	6	35	70	35	12	0	0	0	0	0	0	0	0	0	0	0	163	31.6	36.8	28 to 38	76.1	*	*	0	157	6	
9/4/2024	20:00	4	0	4	25	48	18	12	1	0	0	0	0	0	0	0	0	0	0	112	31.8	38	26 to 36	70.5	*	*	0	110	2	
9/4/2024	21:00	1	1	0	21	41	22	5	1	0	0	0	0	0	0	0	0	0	0	92	32.5	37.4	26 to 36	73.9	*	*	0	91	1	
9/4/2024	22:00	2	1	2	12	12	4	2	0	0	0	0	0	0	0	0	0	0	0	35	29.5	34.5	24 to 34	74.3	*	*	0	35	0	
9/4/2024	23:00	0	0	0	7	7	2	2	2	0	0	0	0	0	0	0	0	0	0	20	32.9	40	23 to 33	70.0	1	5.0	0	19	1	
24 Hr Summary		16	11	19	133	241	128	44	4	0	0	0	0	0	0	0	0	0	0	596	31.5	37	26 to 36	72.0	1	0.2	2	578	16	

Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/5/2024	00:00	0	1	0	4	1	4	2	0	0	0	0	0	0	0	0	0	0	0	12	31.8	37	25 to 35	66.7	*	*	0	12	0
9/5/2024	01:00	0	2	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	7	25.4	30.5	21 to 31	71.4	*	*	0	7	0
9/5/2024	02:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30	32	22 to 32	100.0	*	*	0	2	0
9/5/2024	03:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	46	46	36 to 46	100.0	1	100.0	0	1	0
9/5/2024	04:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	1	0
9/5/2024	05:00	2	0	2	0	6	2	0	0	0	0	0	0	0	0	0	0	0	0	12	27.1	33	27 to 37	66.7	*	*	0	12	0
9/5/2024	06:00	2	0	2	8	16	0	0	0	0	0	0	0	0	0	0	0	0	0	28	27.8	31.7	23 to 33	85.7	*	*	0	27	1
9/5/2024	07:00	1	4	3	23	22	12	2	0	0	0	0	0	0	0	0	0	0	0	67	29.8	35	26 to 36	76.1	*	*	0	65	2
9/5/2024	08:00	5	3	14	50	68	19	2	0	0	0	0	0	0	0	0	0	0	0	161	29	33.4	25 to 35	78.9	*	*	0	158	3
9/5/2024	09:00	4	2	13	42	33	13	3	0	0	0	0	0	0	0	0	0	0	0	110	28.8	34	24 to 34	72.7	*	*	0	108	2
9/5/2024	10:00	0	2	7	36	42	19	8	3	0	0	0	0	0	0	0	0	0	0	117	31.8	37.6	24 to 34	68.4	3	2.6	0	108	9
9/5/2024	11:00	0	0	9	20	27	23	6	0	0	0	0	0	0	0	0	0	0	0	85	31.5	36.5	27 to 37	68.2	*	*	0	81	4
9/5/2024	12:00	1	2	3	23	36	18	5	0	0	0	0	0	0	0	0	0	0	0	88	31.2	37	25 to 35	70.5	*	*	0	79	9
9/5/2024	13:00	3	1	6	25	39	25	5	0	0	0	0	0	0	0	0	0	0	0	104	31.2	36.3	25 to 35	70.2	*	*	0	96	8
9/5/2024	14:00	1	3	2	25	45	27	6	0	0	0	0	0	0	0	0	0	0	0	109	31.8	36.8	28 to 38	75.2	*	*	0	103	6
9/5/2024	15:00	9	2	11	55	58	10	7	1	0	0	0	0	0	0	0	0	0	0	153	29	33.6	24 to 34	77.8	1	0.7	0	150	3
9/5/2024	16:00	5	4	5	40	74	32	9	0	0	0	0	0	0	0	0	0	0	0	169	31.2	35.7	26 to 36	77.5	*	*	0	158	11
9/5/2024	17:00	5	1	6	39	98	49	11	1	0	0	0	0	0	0	0	0	0	0	210	31.9	36.1	28 to 38	80.5	*	*	0	194	16
9/5/2024	18:00	0	1	5	39	79	43	19	1	0	0	0	0	0	0	0	0	0	0	187	32.8	37.2	27 to 37	78.6	*	*	0	180	7
9/5/2024	19:00	3	0	3	33	59	33	5	3	0	0	0	0	0	0	0	0	0	0	139	32	36.5	26 to 36	78.4	2	1.4	0	134	5
9/5/2024	20:00	1	1	2	16	32	25	2	1	0	0	0	0	0	0	0	0	0	0	80	32.5	37	27 to 37	77.5	*	*	0	77	3
9/5/2024	21:00	2	2	3	8	28	12	6	0	0	0	0	0	0	0	0	0	0	0	61	31.1	35.9	27 to 37	75.4	*	*	0	59	2
9/5/2024	22:00	1	0	2	9	16	6	1	0	0	0	0	0	0	0	0	0	0	0	35	30.6	35.7	26 to 36	80.0	*	*	0	34	1
9/5/2024	23:00	1	1	0	5	7	5	3	0	0	0	0	0	0	0	0	0	0	0	22	31.5	38	26 to 36	68.2	*	*	0	21	1
24 Hr Summary		46	32	98	503	790	378	102	11	0	0	0	0	0	0	0	0	0	0	1960	31	36	26 to 36	73.5	7	0.4	0	1867	93

Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/6/2024	00:00	0	0	1	8	1	2	3	0	0	0	0	0	0	0	0	0	0	0	15	31.6	40	18 to 28	60.0	*	*	0	14	1
9/6/2024	01:00	0	1	0	1	3	0	1	0	0	0	0	0	0	0	0	0	0	0	6	30.5	33	23 to 33	66.7	*	*	0	6	0
9/6/2024	02:00	0	0	1	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	8	28.8	32	22 to 32	87.5	*	*	0	8	0
9/6/2024	03:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	31.5	33	23 to 33	100.0	*	*	0	2	0
9/6/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0
9/6/2024	05:00	0	0	1	2	1	0	0	1	0	0	0	0	0	0	0	0	0	0	5	31.2	32	22 to 32	80.0	1	20.0	0	5	0
9/6/2024	06:00	1	1	5	5	6	2	0	0	0	0	0	0	0	0	0	0	0	0	20	27.1	33	21 to 31	60.0	*	*	0	19	1
9/6/2024	07:00	5	3	9	25	21	5	5	0	0	0	0	0	0	0	0	0	0	0	73	28	33	23 to 33	74.0	*	*	0	70	3
9/6/2024	08:00	2	5	21	41	49	11	3	0	0	0	0	0	0	0	0	0	0	0	132	28.4	32.9	24 to 34	74.2	*	*	0	128	4
9/6/2024	09:00	2	1	9	48	35	18	3	0	0	0	0	0	0	0	0	0	0	0	116	29.7	34.4	25 to 35	80.2	*	*	0	110	6
9/6/2024	10:00	1	1	4	21	32	12	2	0	0	0	0	0	0	0	0	0	0	0	73	30.9	35.7	26 to 36	78.1	*	*	0	67	6
9/6/2024	11:00	3	5	8	28	46	22	4	2	0	0	0	0	0	0	0	0	0	0	118	30.5	36.1	27 to 37	68.6	1	0.8	0	108	10
9/6/2024	12:00	1	3	5	34	52	31	6	0	0	0	0	0	0	0	0	0	0	0	132	31.5	36.6	28 to 38	76.5	*	*	0	128	4
9/6/2024	13:00	3	1	5	33	56	28	2	0	0	0	0	0	0	0	0	0	0	0	128	30.7	35.5	27 to 37	81.3	*	*	0	117	11
9/6/2024	14:00	6	3	5	41	95	46	12	0	0	0	0	0	0	0	0	0	0	0	208	31.8	36.5	27 to 37	76.9	*	*	0	195	13
9/6/2024	15:00	3	5	10	52	83	29	3	0	0	0	0	0	0	0	0	0	0	0	185	30.2	34.2	26 to 36	83.8	*	*	0	174	11
9/6/2024	16:00	2	4	7	40	95	35	10	0	0	0	0	0	0	0	0	0	0	0	193	31.4	35.5	27 to 37	79.3	*	*	0	180	13
9/6/2024	17:00	1	2	6	35	97	48	17	0	0	0	0	0	0	0	0	0	0	0	206	32.5	36.9	27 to 37	76.2	*	*	0	189	17
9/6/2024	18:00	2	3	6	31	93	57	7	0	0	0	0	0	0	0	0	0	0	0	199	32.2	36	27 to 37	81.9	*	*	0	189	10
9/6/2024	19:00	2	2	6	33	76	36	14	2	0	0	0	0	0	0	0	0	0	0	171	32.2	36.8	26 to 36	76.6	*	*	0	164	7
9/6/2024	20:00	5	3	9	23	35	25	6	3	0	0	0	0	0	0	0	0	0	0	109	30.8	37.2	28 to 38	64.2	3	2.8	0	104	5
9/6/2024	21:00	0	1	1	14	26	10	3	0	0	0	0	0	0	0	0	0	0	0	55	31.7	35.6	26 to 36	83.6	*	*	0	54	1
9/6/2024	22:00	0	0	3	9	17	5	1	0	0	0	0	0	0	0	0	0	0	0	35	31	34.5	24 to 34	80.0	*	*	0	34	1
9/6/2024	23:00	1	1	3	11	12	4	1	0	1	0	0	0	1	0	0	0	0	0	35	31.3	37	24 to 34	68.6	2	5.7	0	35	0
24 Hr Summary		40	45	125	538	938	426	103	8	1	0	0	0	1	0	0	0	0	0	2225	31	36	26 to 36	74.6	7	0.3	0	2101	124

Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/7/2024	00:00	0	0	2	6	5	1	0	0	0	0	0	0	0	0	0	0	0	0	14	29.4	32	23 to 33	85.7	*	*	0	12	2
9/7/2024	01:00	0	0	1	3	3	2	1	0	0	0	0	0	0	0	0	0	0	0	10	31.6	35	28 to 38	80.0	*	*	0	10	0
9/7/2024	02:00	1	0	0	6	4	0	1	1	0	0	0	0	0	0	0	0	0	0	13	30.2	34	24 to 34	76.9	1	7.7	0	12	1
9/7/2024	03:00	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	28.7	30	20 to 30	100.0	*	*	0	3	0
9/7/2024	04:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28.5	29	19 to 29	100.0	*	*	0	2	0
9/7/2024	05:00	0	0	0	3	1	2	1	0	0	0	0	0	0	0	0	0	0	0	7	33.1	38	21 to 31	57.1	*	*	0	7	0
9/7/2024	06:00	1	0	2	3	5	2	1	0	0	0	0	0	0	0	0	0	0	0	14	29.6	36	24 to 34	64.3	*	*	0	13	1
9/7/2024	07:00	0	1	3	8	16	5	3	0	0	0	0	0	0	0	0	0	0	0	36	31.2	37.7	23 to 33	72.2	*	*	0	33	3
9/7/2024	08:00	1	2	1	6	19	8	6	0	0	0	0	0	0	0	0	0	0	0	43	32.1	38	27 to 37	69.8	*	*	0	42	1
9/7/2024	09:00	0	2	3	10	18	12	4	2	0	0	0	0	0	0	0	0	0	0	51	32.3	37	26 to 36	70.6	2	3.9	0	45	6
9/7/2024	10:00	3	2	6	26	46	13	3	0	0	0	0	0	0	0	0	0	0	0	99	30.2	34.2	26 to 36	79.8	*	*	0	97	2
9/7/2024	11:00	2	3	6	32	53	27	4	1	1	0	0	0	0	0	0	0	0	0	129	31.3	36.3	28 to 38	76.0	2	1.6	0	122	7
9/7/2024	12:00	1	0	7	28	64	23	5	1	0	0	0	0	0	0	0	0	0	0	129	31.7	35.8	27 to 37	82.2	*	*	0	124	5
9/7/2024	13:00	2	3	5	35	67	31	10	2	0	0	0	0	0	0	0	0	0	0	155	31.9	36.8	28 to 38	76.8	2	1.3	0	150	5
9/7/2024	14:00	6	2	6	26	64	34	9	0	0	0	0	0	0	0	0	0	0	0	147	31.2	36.1	27 to 37	75.5	*	*	0	138	9
9/7/2024	15:00	4	1	6	32	50	34	8	1	0	0	0	0	0	0	0	0	0	0	136	31.4	36.3	27 to 37	73.5	1	0.7	0	127	9
9/7/2024	16:00	7	1	5	25	55	39	8	0	0	0	0	0	0	0	0	0	0	0	140	31.5	37	27 to 37	72.9	*	*	0	135	5
9/7/2024	17:00	2	1	7	25	63	29	13	1	0	1	0	0	0	0	0	0	0	0	142	32.5	37.2	27 to 37	73.2	1	0.7	0	135	7
9/7/2024	18:00	3	1	2	25	66	39	17	2	0	0	0	0	0	0	0	0	0	0	155	33	38.2	27 to 37	76.1	2	1.3	0	151	4
9/7/2024	19:00	3	3	2	12	54	29	6	2	0	0	0	0	0	0	0	0	0	0	111	32.3	37	29 to 39	77.5	1	0.9	0	106	5
9/7/2024	20:00	2	2	8	25	39	23	11	1	0	0	0	0	0	0	0	0	0	0	111	31.4	36.5	26 to 36	71.2	1	0.9	0	107	4
9/7/2024	21:00	0	0	2	17	31	17	3	5	0	0	0	0	0	0	0	0	0	0	75	33	37.7	25 to 35	70.7	5	6.7	0	74	1
9/7/2024	22:00	2	0	4	7	15	9	4	1	0	0	0	0	0	0	0	0	0	0	42	31.5	38.5	25 to 35	64.3	1	2.4	0	40	2
9/7/2024	23:00	0	1	1	13	14	10	3	1	0	0	0	0	0	0	0	0	0	0	43	32.4	36	26 to 36	81.4	*	*	0	43	0
24 Hr Summary		40	25	79	376	754	389	121	21	1	1	0	0	0	0	0	0	0	0	1807	31.7	37	27 to 37	74.0	19	1.1	0	1728	79

Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/8/2024	00:00	0	0	4	2	7	2	1	2	0	0	0	0	0	0	0	0	0	0	18	31.4	37	22 to 32	72.2	1	5.6	0	18	0
9/8/2024	01:00	0	0	1	2	5	1	0	0	0	0	0	0	0	0	0	0	0	0	9	30.9	34	24 to 34	88.9	*	*	0	9	0
9/8/2024	02:00	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	29.3	31	21 to 31	100.0	*	*	0	3	0
9/8/2024	03:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26	31	21 to 31	100.0	*	*	0	3	0
9/8/2024	04:00	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	2	32.5	42	13 to 23	50.0	*	*	0	2	0
9/8/2024	05:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	31.5	38	15 to 25	50.0	*	*	0	2	0
9/8/2024	06:00	0	0	1	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	5	27.6	30.5	21 to 31	100.0	*	*	0	5	0
9/8/2024	07:00	1	0	1	7	8	2	0	0	0	0	0	0	0	0	0	0	0	0	19	29.2	32	24 to 34	78.9	*	*	0	17	2
9/8/2024	08:00	2	0	2	11	17	5	3	0	0	0	0	0	0	0	0	0	0	0	40	30.6	36.3	24 to 34	75.0	*	*	0	35	5
9/8/2024	09:00	0	0	4	16	32	10	3	0	0	0	0	0	0	0	0	0	0	0	65	31.6	35.3	27 to 37	83.1	*	*	0	62	3
9/8/2024	10:00	2	1	5	25	53	15	5	0	0	0	0	0	0	0	0	0	0	0	106	30.9	36	27 to 37	76.4	*	*	0	102	4
9/8/2024	11:00	1	1	5	29	39	25	2	3	0	0	0	0	0	0	0	0	0	0	105	31.8	36.5	27 to 37	76.2	2	1.9	0	102	3
9/8/2024	12:00	2	3	2	29	47	22	10	2	0	0	0	0	0	0	0	0	0	0	117	31.8	36	25 to 35	75.2	*	*	0	113	4
9/8/2024	13:00	1	1	2	21	37	28	9	0	0	0	0	0	0	0	0	0	0	0	99	32.8	37.6	28 to 38	76.8	*	*	0	93	6
9/8/2024	14:00	0	1	3	24	40	33	6	2	0	0	0	0	0	0	0	0	0	0	109	32.9	37.5	27 to 37	77.1	1	0.9	0	104	5
9/8/2024	15:00	1	2	1	36	47	21	7	1	0	0	0	0	0	0	0	0	0	0	116	31.6	36	26 to 36	80.2	1	0.9	0	112	4
9/8/2024	16:00	4	0	4	23	42	18	8	0	0	0	0	0	0	0	0	0	0	0	99	31.2	37	27 to 37	73.7	*	*	0	97	2
9/8/2024	17:00	3	0	3	24	38	26	8	0	0	0	0	0	0	0	0	0	0	0	102	31.9	37.2	28 to 38	74.5	*	*	0	100	2
9/8/2024	18:00	2	1	1	21	38	22	8	2	1	0	0	0	0	0	0	0	0	0	96	32.8	38.4	28 to 38	75.0	3	3.1	0	94	2
9/8/2024	19:00	2	1	5	16	35	13	5	4	0	0	0	0	0	0	0	0	0	0	81	32.2	38	26 to 36	70.4	3	3.7	0	77	4
9/8/2024	20:00	2	1	0	13	41	14	6	0	0	0	0	0	0	0	0	0	0	0	77	31.9	35	26 to 36	81.8	*	*	0	75	2
9/8/2024	21:00	4	0	1	21	19	11	2	1	0	0	0	0	0	0	0	0	0	0	59	30.2	36	26 to 36	74.6	*	*	0	56	3
9/8/2024	22:00	1	0	1	9	18	4	2	0	0	0	0	0	0	0	0	0	0	0	35	30.8	36	27 to 37	82.9	*	*	0	35	0
9/8/2024	23:00	0	0	0	4	13	3	1	0	0	0	0	0	0	0	0	0	0	0	21	32.9	35	25 to 35	85.7	*	*	0	20	1
24 Hr Summary		28	12	48	338	581	276	87	17	1	0	0	0	0	0	0	0	0	0	1388	31.7	37	27 to 37	74.6	11	0.8	0	1336	52

Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/9/2024	00:00	0	1	1	7	4	5	0	0	0	0	0	0	0	0	0	0	0	0	18	30.3	37	23 to 33	66.7	*	*	0	17	1
9/9/2024	01:00	0	0	0	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	5	30	31.5	22 to 32	100.0	*	*	0	5	0
9/9/2024	02:00	1	0	3	2	3	0	5	0	0	0	0	0	0	0	0	0	0	0	14	32.1	42	33 to 43	50.0	*	*	0	12	2
9/9/2024	03:00	1	0	1	4	1	1	0	0	0	0	0	0	0	0	0	0	0	0	8	26.8	30	20 to 30	75.0	*	*	0	8	0
9/9/2024	04:00	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	4	33	35	25 to 35	75.0	*	*	0	4	0
9/9/2024	05:00	0	1	2	2	2	1	0	1	0	0	0	0	0	0	0	0	0	0	9	29.6	37	19 to 29	55.6	*	*	0	8	1
9/9/2024	06:00	2	2	5	8	7	0	0	0	0	0	0	0	0	0	0	0	0	0	24	25.4	30.5	24 to 34	70.8	*	*	0	23	1
9/9/2024	07:00	1	0	3	24	20	8	1	0	0	0	0	0	0	0	0	0	0	0	57	29.9	34	25 to 35	87.7	*	*	0	56	1
9/9/2024	08:00	4	4	4	46	56	11	5	0	0	0	0	0	0	0	0	0	0	0	130	29.5	33.6	25 to 35	80.0	*	*	0	124	6
9/9/2024	09:00	3	2	7	35	49	12	3	1	0	0	0	0	0	0	0	0	0	0	112	29.8	33.8	25 to 35	79.5	*	*	0	106	6
9/9/2024	10:00	3	2	16	31	40	18	6	0	0	0	0	0	0	0	0	0	0	0	116	30	35.4	24 to 34	68.1	*	*	0	109	7
9/9/2024	11:00	2	2	5	15	50	22	3	0	0	0	0	0	0	0	0	0	0	0	99	31.4	35.8	28 to 38	78.8	*	*	0	91	8
9/9/2024	12:00	4	1	3	27	34	26	7	2	0	0	0	0	0	0	0	0	0	0	104	31.5	36.6	26 to 36	72.1	*	*	0	98	6
9/9/2024	13:00	4	0	1	21	41	34	9	1	0	0	0	0	0	0	0	0	0	0	111	32.2	37.3	28 to 38	73.9	1	0.9	0	103	8
9/9/2024	14:00	4	1	8	16	38	22	11	0	0	0	0	0	0	0	0	0	0	0	100	31.7	38.4	29 to 39	65.0	*	*	0	92	8
9/9/2024	15:00	2	1	10	41	46	23	3	0	0	0	0	0	0	0	0	0	0	0	126	30.4	35	25 to 35	74.6	*	*	0	121	5
9/9/2024	16:00	3	0	4	30	53	29	6	0	0	0	0	0	0	0	0	0	0	0	125	31.5	36	25 to 35	76.0	*	*	0	117	8
9/9/2024	17:00	2	1	5	40	90	38	13	0	0	0	0	0	0	0	0	0	0	0	189	31.9	35.5	26 to 36	82.0	*	*	0	179	10
9/9/2024	18:00	3	4	4	23	81	61	12	1	0	0	0	0	0	0	0	0	0	0	189	32.7	37.3	29 to 39	82.0	*	*	0	184	5
9/9/2024	19:00	0	2	1	25	68	27	10	0	0	0	0	0	0	0	0	0	0	0	133	32.4	36.4	27 to 37	81.2	*	*	0	125	8
9/9/2024	20:00	3	1	1	18	30	23	6	0	0	0	0	0	0	0	0	0	0	0	82	31.6	36.7	27 to 37	75.6	*	*	0	79	3
9/9/2024	21:00	2	2	4	17	29	18	5	0	0	0	0	0	0	0	0	0	0	0	77	31.2	36.6	28 to 38	74.0	*	*	0	76	1
9/9/2024	22:00	1	1	5	10	9	7	2	0	0	0	0	0	0	0	0	0	0	0	35	29.9	37.3	28 to 38	62.9	*	*	0	32	3
9/9/2024	23:00	2	0	2	5	6	4	0	0	0	0	0	0	0	0	0	0	0	0	19	28.2	35	23 to 33	68.4	*	*	0	17	2
24 Hr Summary		47	28	95	449	762	392	107	6	0	0	0	0	0	0	0	0	0	0	1886	31.1	37	26 to 36	73.0	1	0.1	0	1786	100

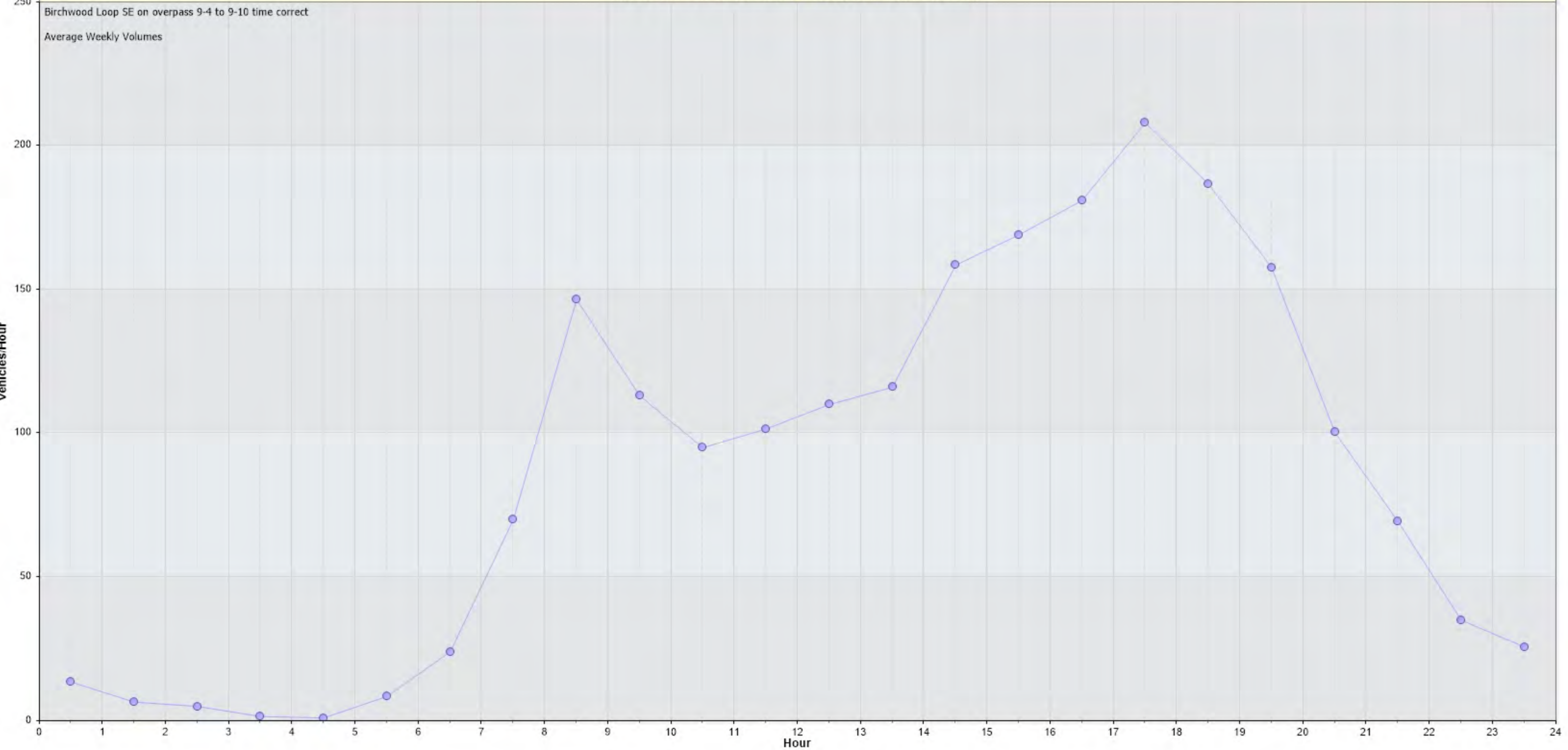
Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	1	0	1	3	5	3	2	0	0	0	0	0	0	0	0	0	0	0	15	31.9	37	27 to 37	73.3	*	*	0	14	1	
9/10/2024	01:00	0	0	1	3	0	2	0	2	0	0	0	0	0	0	0	0	0	0	8	33.6	45	18 to 28	50.0	1	12.5	0	8	0	
9/10/2024	02:00	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3	31	31	21 to 31	100.0	*	*	0	3	0	
9/10/2024	03:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	26.5	28	18 to 28	100.0	*	*	0	2	0	
9/10/2024	04:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25.5	30	20 to 30	100.0	*	*	0	2	0	
9/10/2024	05:00	0	0	1	2	4	3	0	0	0	0	0	0	0	0	0	0	0	0	10	31.3	35	26 to 36	90.0	*	*	0	7	3	
9/10/2024	06:00	0	0	1	6	5	1	2	0	0	0	0	0	0	0	0	0	0	0	15	31.1	35	22 to 32	80.0	*	*	0	15	0	
9/10/2024	07:00	1	1	6	23	23	8	4	0	0	0	0	0	0	0	0	0	0	0	66	29.7	35	24 to 34	74.2	*	*	0	66	0	
9/10/2024	08:00	5	2	20	53	64	16	3	0	0	0	0	0	0	0	0	0	0	0	163	28.9	33.5	25 to 35	76.7	*	*	1	150	12	
9/10/2024	09:00	6	1	11	48	60	23	6	0	0	0	0	0	0	0	0	0	0	0	155	30.1	35.2	25 to 35	72.9	*	*	0	144	11	
9/10/2024	10:00	6	0	10	26	57	21	2	0	0	0	0	0	0	0	0	0	0	0	122	30.3	35	28 to 38	74.6	*	*	0	111	11	
9/10/2024	11:00	0	1	3	23	43	29	7	0	0	0	0	0	0	0	0	0	0	0	106	32.3	36.3	27 to 37	80.2	*	*	0	94	12	
9/10/2024	12:00	0	2	5	27	44	26	10	0	0	0	0	0	0	0	0	0	0	0	114	32	36.8	26 to 36	73.7	*	*	0	106	8	
9/10/2024	13:00	1	0	4	22	48	42	13	1	0	0	0	0	0	0	0	0	0	0	131	33.4	37.3	27 to 37	76.3	1	0.8	0	124	7	
9/10/2024	14:00	4	3	4	29	45	46	14	2	0	0	0	0	0	0	0	0	0	0	147	32.4	38.1	27 to 37	70.1	1	0.7	0	135	12	
9/10/2024	15:00	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	3	33	40	21 to 31	66.7	*	*	0	3	0	
9/10/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		24	10	68	268	403	220	64	5	0	0	0	0	0	0	0	0	0	0	1062	31.1	36	26 to 36	72.8	3	0.3	1	984	77	

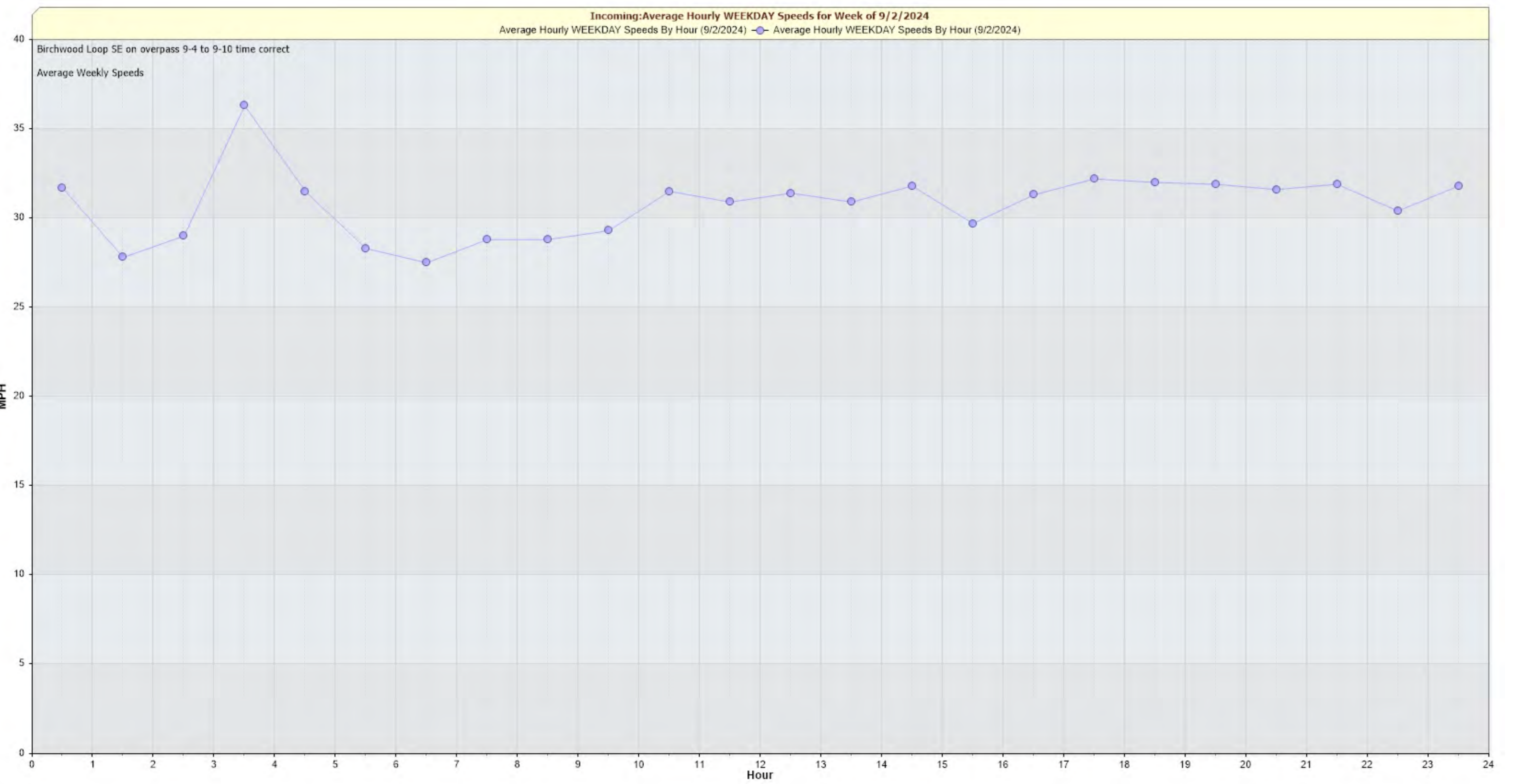
Incoming: Average Hourly Volume for Week of 9/2/2024
Average Counts By Hour (9/2/2024) — Average Counts By Hour (9/2/2024)



Birchwood Loop SE on overpass 9-4 to 9-10 time correct
Average Weekly Volumes

Incoming: Average Hourly WEEKDAY Speeds for Week of 9/2/2024
Average Hourly WEEKDAY Speeds By Hour (9/2/2024) — Average Hourly WEEKDAY Speeds By Hour (9/2/2024)

Birchwood Loop SE on overpass 9-4 to 9-10 time correct
Average Weekly Speeds



Incoming:Daily Volume for Week of 9/2/2024

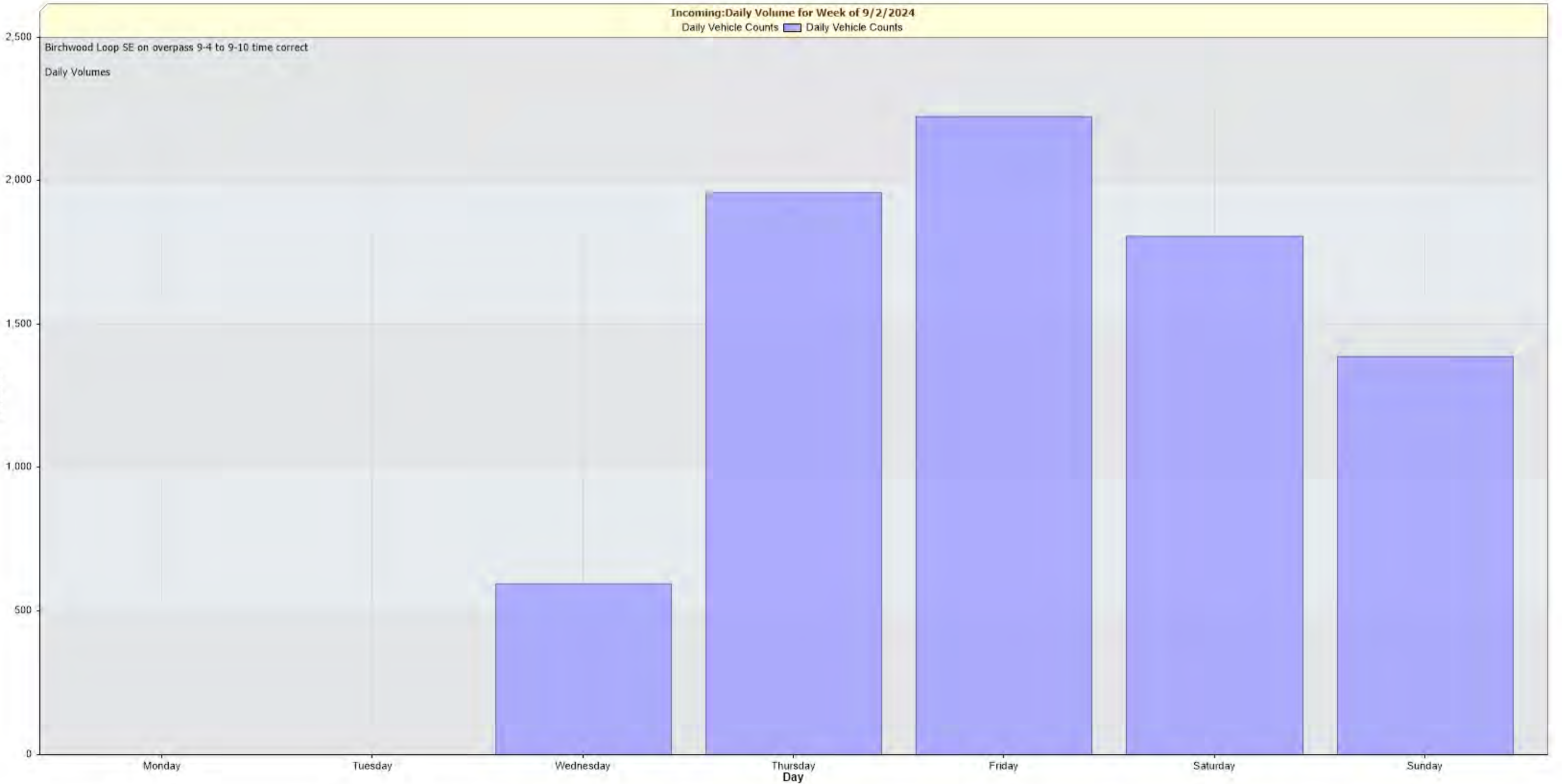
Daily Vehicle Counts

Daily Vehicle Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

Daily Volumes

Vehicles



Monday

Tuesday

Wednesday

Thursday
Day

Friday

Saturday

Sunday

For Project: Birchwood Loop SE on overpass 9-4 to 9-10 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:40:57 PM

Speed Intervals: 1 MPH

Time Intervals: Instant

Traffic Report From: 9/4/2024 6:00:00 PM through 9/10/2024 3:59:59 PM

85th Percentile Speed: 37 MPH

85th Percentile Vehicles: 11006

Max Speed: 63 MPH on 9/5/2024 2:16:14 PM

Total Vehicles: 12948

AADT: 2188

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1816	1849
AM Peak 10:00 AM	135	130
PM Peak 5:00 PM	237	211

Speed

Speed Limit: 45

85th Percentile Speed: 37

50th Percentile Speed: 31

10 MPH Pace Interval: 26.0 MPH to 36.0 MPH

Average Speed: 31.57

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	25	8	8	29	24	32	22
% over limit	1.1	0.7	1.1	1.2	0.9	1.5	1.3
Avg Speeder	49.3	48.6	47.1	48.1	47.5	48.8	47.6
Avg Speed	31.5	31.3	32.2	31.1	31.5	31.8	32.0

Class Counts

	Number	%
VEH_SM	102	0.8
VEH_MED	11992	92.6
VEH_LG	854	6.6
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Outgoing Summary
Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/4/2024 7:00:00 PM	37.0	147	173	50	48.0	1.2%	32.2
9/4/2024 8:00:00 PM	38.0	197	232	45	0.0	0.0%	32.2
9/4/2024 9:00:00 PM	38.0	96	113	44	0.0	0.0%	32.8
9/4/2024 10:00:00 PM	37.0	100	118	49	48.0	1.7%	31.6
9/4/2024 11:00:00 PM	39.0	49	58	47	46.3	5.2%	31.9
9/5/2024 12:00:00 AM	41.0	20	24	46	46.0	4.2%	32.3
9/5/2024 1:00:00 AM	44.0	13	15	51	49.5	13.3%	33.6
9/5/2024 2:00:00 AM	41.0	5	6	44	0.0	0.0%	31.2
9/5/2024 3:00:00 AM	31.0	3	4	44	0.0	0.0%	33.3
9/5/2024 4:00:00 AM	30.0	3	3	30	0.0	0.0%	28.7
9/5/2024 5:00:00 AM	29.0	3	4	32	0.0	0.0%	26.0
9/5/2024 6:00:00 AM	25.0	3	4	28	0.0	0.0%	25.5
9/5/2024 7:00:00 AM	33.0	13	15	43	0.0	0.0%	29.4
9/5/2024 8:00:00 AM	38.0	59	69	47	47.0	4.3%	31.1
9/5/2024 9:00:00 AM	36.0	110	129	44	0.0	0.0%	30.1
9/5/2024 10:00:00 AM	38.0	98	115	46	46.0	0.9%	31.4
9/5/2024 11:00:00 AM	37.0	128	151	46	46.0	1.3%	30.0
9/5/2024 12:00:00 PM	37.0	127	149	47	47.0	0.7%	31.2
9/5/2024 1:00:00 PM	37.0	135	159	49	47.8	3.8%	31.1
9/5/2024 2:00:00 PM	37.0	122	143	55	55.0	0.7%	31.6
9/5/2024 3:00:00 PM	36.0	143	168	63	63.0	0.6%	30.6
9/5/2024 4:00:00 PM	34.0	173	204	48	48.0	0.5%	29.5
9/5/2024 5:00:00 PM	36.0	198	233	47	47.0	0.4%	30.8
9/5/2024 6:00:00 PM	37.0	209	246	46	46.0	1.2%	31.9
9/5/2024 7:00:00 PM	36.0	201	236	49	48.0	0.8%	31.9
9/5/2024 8:00:00 PM	37.0	145	171	50	48.0	1.2%	32.2
9/5/2024 9:00:00 PM	36.0	88	103	48	46.7	2.9%	31.7
9/5/2024 10:00:00 PM	37.0	81	95	45	0.0	0.0%	32.5
9/5/2024 11:00:00 PM	35.0	53	62	44	0.0	0.0%	30.5
9/6/2024 12:00:00 AM	36.0	15	18	44	0.0	0.0%	32.8
9/6/2024 1:00:00 AM	36.0	10	12	44	0.0	0.0%	30.1
9/6/2024 2:00:00 AM	39.0	4	5	43	0.0	0.0%	33.0
9/6/2024 3:00:00 AM	30.0	2	2	30	0.0	0.0%	24.0
9/6/2024 4:00:00 AM	44.0	1	1	44	0.0	0.0%	44.0
9/6/2024 5:00:00 AM	33.0	5	6	34	0.0	0.0%	30.7
9/6/2024 6:00:00 AM	34.0	4	5	38	0.0	0.0%	30.8
9/6/2024 7:00:00 AM	32.0	14	17	34	0.0	0.0%	29.2
9/6/2024 8:00:00 AM	36.0	49	58	46	46.0	1.7%	31.3
9/6/2024 9:00:00 AM	34.0	102	120	45	0.0	0.0%	30.0
9/6/2024 10:00:00 AM	37.0	93	109	49	48.0	1.8%	31.2
9/6/2024 11:00:00 AM	36.0	119	140	43	0.0	0.0%	30.0
9/6/2024 12:00:00 PM	37.0	94	110	46	46.0	0.9%	30.4
9/6/2024 1:00:00 PM	38.0	137	161	47	46.3	1.9%	31.4
9/6/2024 2:00:00 PM	38.0	131	154	49	48.0	1.3%	31.9
9/6/2024 3:00:00 PM	37.0	168	198	42	0.0	0.0%	31.5
9/6/2024 4:00:00 PM	36.0	161	189	49	47.0	2.6%	31.4
9/6/2024 5:00:00 PM	37.0	197	232	49	48.0	0.9%	32.2

Outgoing Summary
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

9/6/2024 6:00:00 PM	37.0	209	246	52	52.0	0.4%	31.9
9/6/2024 7:00:00 PM	37.0	207	244	51	49.0	0.8%	32.5
9/6/2024 8:00:00 PM	37.0	171	201	46	46.0	0.5%	31.3
9/6/2024 9:00:00 PM	37.0	110	130	49	49.0	0.8%	32.3
9/6/2024 10:00:00 PM	37.0	76	89	46	46.0	2.2%	31.1
9/6/2024 11:00:00 PM	35.0	56	66	48	48.0	1.5%	31.2
9/7/2024 12:00:00 AM	36.0	27	32	45	0.0	0.0%	30.6
9/7/2024 1:00:00 AM	34.0	15	18	41	0.0	0.0%	27.5
9/7/2024 2:00:00 AM	36.0	8	10	44	0.0	0.0%	31.8
9/7/2024 3:00:00 AM	32.0	8	9	38	0.0	0.0%	28.1
9/7/2024 4:00:00 AM	37.0	3	3	37	0.0	0.0%	30.0
9/7/2024 5:00:00 AM	39.0	2	2	39	0.0	0.0%	32.0
9/7/2024 6:00:00 AM	42.0	6	7	43	0.0	0.0%	37.6
9/7/2024 7:00:00 AM	33.0	12	14	44	0.0	0.0%	30.4
9/7/2024 8:00:00 AM	36.0	22	26	45	0.0	0.0%	30.7
9/7/2024 9:00:00 AM	39.0	26	31	47	47.0	3.2%	33.3
9/7/2024 10:00:00 AM	38.0	48	57	47	47.0	1.8%	32.4
9/7/2024 11:00:00 AM	36.0	76	90	62	62.0	1.1%	31.8
9/7/2024 12:00:00 PM	38.0	114	134	46	46.0	0.7%	32.1
9/7/2024 1:00:00 PM	38.0	139	164	50	48.7	1.8%	32.3
9/7/2024 2:00:00 PM	36.0	171	201	58	52.3	1.5%	30.8
9/7/2024 3:00:00 PM	37.0	144	170	48	46.7	1.8%	31.9
9/7/2024 4:00:00 PM	38.0	139	164	48	48.0	0.6%	32.7
9/7/2024 5:00:00 PM	38.0	147	173	46	46.0	1.2%	31.9
9/7/2024 6:00:00 PM	37.0	160	188	50	48.0	2.7%	31.6
9/7/2024 7:00:00 PM	38.0	171	201	44	0.0	0.0%	32.0
9/7/2024 8:00:00 PM	37.0	137	161	46	46.0	0.6%	32.1
9/7/2024 9:00:00 PM	36.0	108	127	49	48.0	1.6%	31.7
9/7/2024 10:00:00 PM	37.0	79	93	57	49.4	5.4%	31.9
9/7/2024 11:00:00 PM	38.0	39	46	47	47.0	2.2%	31.4
9/8/2024 12:00:00 AM	40.0	30	35	51	50.5	5.7%	31.7
9/8/2024 1:00:00 AM	34.0	19	22	43	0.0	0.0%	29.5
9/8/2024 2:00:00 AM	34.0	9	11	49	49.0	9.1%	32.8
9/8/2024 3:00:00 AM	33.0	8	9	36	0.0	0.0%	27.1
9/8/2024 4:00:00 AM	30.0	2	2	30	0.0	0.0%	28.0
9/8/2024 5:00:00 AM	30.0	3	4	33	0.0	0.0%	28.5
9/8/2024 6:00:00 AM	42.0	2	2	42	0.0	0.0%	37.0
9/8/2024 7:00:00 AM	34.0	3	4	43	0.0	0.0%	32.3
9/8/2024 8:00:00 AM	34.0	9	11	36	0.0	0.0%	30.6
9/8/2024 9:00:00 AM	35.0	23	27	51	51.0	3.7%	30.0
9/8/2024 10:00:00 AM	38.0	39	46	44	0.0	0.0%	32.1
9/8/2024 11:00:00 AM	38.0	68	80	47	47.0	2.5%	31.9
9/8/2024 12:00:00 PM	37.0	120	141	51	48.7	2.1%	32.0
9/8/2024 1:00:00 PM	40.0	106	125	46	46.0	1.6%	32.9
9/8/2024 2:00:00 PM	37.0	173	203	48	47.5	1.0%	31.9
9/8/2024 3:00:00 PM	37.0	124	146	49	49.0	0.7%	31.6
9/8/2024 4:00:00 PM	38.0	116	136	47	47.0	0.7%	32.0
9/8/2024 5:00:00 PM	37.0	104	122	46	46.0	0.8%	31.8

Outgoing Summary
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

9/8/2024 6:00:00 PM	39.0	133	156	47	46.3	1.9%	33.0
9/8/2024 7:00:00 PM	37.0	132	155	47	47.0	0.6%	32.1
9/8/2024 8:00:00 PM	37.0	94	110	48	47.5	1.8%	32.1
9/8/2024 9:00:00 PM	39.0	78	92	51	48.5	2.2%	33.1
9/8/2024 10:00:00 PM	37.0	69	81	45	0.0	0.0%	31.3
9/8/2024 11:00:00 PM	36.0	31	37	42	0.0	0.0%	31.7
9/9/2024 12:00:00 AM	37.0	15	18	41	0.0	0.0%	32.1
9/9/2024 1:00:00 AM	30.0	8	10	36	0.0	0.0%	27.6
9/9/2024 2:00:00 AM	31.0	6	7	32	0.0	0.0%	28.9
9/9/2024 3:00:00 AM	39.0	3	3	39	0.0	0.0%	31.0
9/9/2024 4:00:00 AM	33.0	3	4	35	0.0	0.0%	32.0
9/9/2024 5:00:00 AM	38.0	6	7	50	50.0	14.3%	28.0
9/9/2024 6:00:00 AM	30.0	3	4	30	0.0	0.0%	28.3
9/9/2024 7:00:00 AM	42.0	11	13	47	47.0	7.7%	33.2
9/9/2024 8:00:00 AM	34.0	41	48	38	0.0	0.0%	29.8
9/9/2024 9:00:00 AM	38.0	111	131	52	50.0	1.5%	32.1
9/9/2024 10:00:00 AM	38.0	110	130	51	49.3	2.3%	31.6
9/9/2024 11:00:00 AM	38.0	100	118	45	0.0	0.0%	31.8
9/9/2024 12:00:00 PM	36.0	110	129	43	0.0	0.0%	29.7
9/9/2024 1:00:00 PM	38.0	97	114	47	46.5	1.8%	32.8
9/9/2024 2:00:00 PM	38.0	121	142	47	46.5	1.4%	32.5
9/9/2024 3:00:00 PM	38.0	115	135	49	47.7	2.2%	31.9
9/9/2024 4:00:00 PM	36.0	165	194	45	0.0	0.0%	30.3
9/9/2024 5:00:00 PM	36.0	187	220	58	58.0	0.5%	30.9
9/9/2024 6:00:00 PM	36.0	188	221	44	0.0	0.0%	31.8
9/9/2024 7:00:00 PM	36.0	163	192	55	52.0	1.0%	31.8
9/9/2024 8:00:00 PM	38.0	162	191	49	47.8	2.6%	31.9
9/9/2024 9:00:00 PM	38.0	93	109	50	50.0	0.9%	32.6
9/9/2024 10:00:00 PM	37.0	65	76	45	0.0	0.0%	32.0
9/9/2024 11:00:00 PM	35.0	43	51	43	0.0	0.0%	29.9
9/10/2024 12:00:00 AM	39.0	16	19	55	54.0	10.5%	32.1
9/10/2024 1:00:00 AM	36.0	10	12	42	0.0	0.0%	29.4
9/10/2024 2:00:00 AM	35.0	3	3	35	0.0	0.0%	29.3
9/10/2024 3:00:00 AM	30.0	4	5	34	0.0	0.0%	26.0
9/10/2024 4:00:00 AM	32.0	5	6	39	0.0	0.0%	29.7
9/10/2024 5:00:00 AM	30.0	7	8	36	0.0	0.0%	25.8
9/10/2024 6:00:00 AM	39.0	3	3	39	0.0	0.0%	30.3
9/10/2024 7:00:00 AM	33.0	16	19	44	0.0	0.0%	28.7
9/10/2024 8:00:00 AM	38.0	50	59	48	48.0	1.7%	31.8
9/10/2024 9:00:00 AM	37.0	102	120	44	0.0	0.0%	31.7
9/10/2024 10:00:00 AM	37.0	123	145	53	49.5	1.4%	31.3
9/10/2024 11:00:00 AM	37.0	112	132	53	49.5	1.5%	30.8
9/10/2024 12:00:00 PM	37.0	101	119	43	0.0	0.0%	30.8
9/10/2024 1:00:00 PM	37.0	120	141	48	48.0	0.7%	31.3
9/10/2024 2:00:00 PM	38.0	116	137	46	46.0	0.7%	32.5
9/10/2024 3:00:00 PM	37.0	134	158	49	49.0	0.6%	31.4
9/10/2024 4:00:00 PM	29.0	2	2	29	0.0	0.0%	27.0

Outgoing Summary
Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/5/2024 12:00:00 AM	38.0	610	718	50	47.1	1.1%	32.2
9/6/2024 12:00:00 AM	37.0	2127	2502	63	48.1	1.2%	31.1
9/7/2024 12:00:00 AM	37.0	2148	2527	52	47.5	0.9%	31.5
9/8/2024 12:00:00 AM	38.0	1805	2124	62	48.8	1.5%	31.8
9/9/2024 12:00:00 AM	38.0	1479	1740	51	47.6	1.3%	32.0
9/10/2024 12:00:00 AM	37.0	1928	2268	58	49.3	1.1%	31.5
9/10/2024 3:59:59 PM	37.0	909	1069	53	48.6	0.7%	31.3

Outgoing Weekly Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	15	12	18	22	13.5	20	40
1 - 2	*	*	*	6	5	10	11	5.5	10.5	40
2 - 3	*	*	*	4	2	9	9	3	9	30.5
3 - 4	*	*	*	3	1	3	2	2	2.5	37
4 - 5	*	*	*	4	6	2	4	5	3	31
5 - 6	*	*	*	4	5	7	2	4.5	4.5	29.5
6 - 7	*	*	*	15	17	14	4	16	9	32.25
7 - 8	*	*	*	69	58	26	11	63.5	18.5	36.65
8 - 9	*	*	*	129	120	31	27	124.5	29	34.8
9 - 10	*	*	*	115	109	57	46	112	51.5	36.9
10 - 11	*	*	*	151	140	90	80	145.5	85	35.7
11 - 12	*	*	*	149	110	134	141	129.5	137.5	36.25
12 - 13	*	*	*	159	161	164	125	160	144.5	37.25
13 - 14	*	*	*	143	154	201	203	148.5	202	37.05
14 - 15	*	*	*	168	198	170	146	183	158	36
15 - 16	*	*	*	204	189	164	136	196.5	150	34.8
16 - 17	*	*	*	233	232	173	122	232.5	147.5	35.75
17 - 18	*	*	*	246	246	188	156	246	172	36.6
18 - 19	*	*	173	236	244	201	155	217.67	178	36.3
19 - 20	*	*	232	171	201	161	110	201.33	135.5	36.7
20 - 21	*	*	113	103	130	127	92	115.33	109.5	36.77
21 - 22	*	*	118	95	89	93	81	100.67	87	36.57
22 - 23	*	*	58	62	66	46	37	62	41.5	36.17
23 - 24	*	*	24	18	32	35	18	24.67	26.5	37.33
Totals	0	0	718	2502	2527	2124	1740			
% of Total	0%	0%	7.47%	26.03%	26.29%	22.1%	18.1%			

Outgoing Weekly Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	10	12	*	*	*	*	*	11	0	33
1 - 2	7	3	*	*	*	*	*	5	0	33
2 - 3	3	5	*	*	*	*	*	4	0	34.5
3 - 4	4	6	*	*	*	*	*	5	0	32.5
4 - 5	7	8	*	*	*	*	*	7.5	0	34
5 - 6	4	3	*	*	*	*	*	3.5	0	34.25
6 - 7	13	19	*	*	*	*	*	16	0	37.5
7 - 8	48	59	*	*	*	*	*	53.5	0	35.65
8 - 9	131	120	*	*	*	*	*	125.5	0	37.05
9 - 10	130	145	*	*	*	*	*	137.5	0	37.05
10 - 11	118	132	*	*	*	*	*	125	0	36.95
11 - 12	129	119	*	*	*	*	*	124	0	35.75
12 - 13	114	141	*	*	*	*	*	127.5	0	37.5
13 - 14	142	137	*	*	*	*	*	139.5	0	37.45
14 - 15	135	158	*	*	*	*	*	146.5	0	37
15 - 16	194	2	*	*	*	*	*	98	0	32.2
16 - 17	220	*	*	*	*	*	*	220	0	35.5
17 - 18	221	*	*	*	*	*	*	221	0	35.9
18 - 19	192	*	*	*	*	*	*	192	0	35.7
19 - 20	191	*	*	*	*	*	*	191	0	37.1
20 - 21	109	*	*	*	*	*	*	109	0	37.7
21 - 22	76	*	*	*	*	*	*	76	0	37
22 - 23	51	*	*	*	*	*	*	51	0	35
23 - 24	19	*	*	*	*	*	*	19	0	38.5
Totals	2268	1069	0	0	0	0	0			
% of Total	67.97%	32.03%	0%	0%	0%	0%	0%			

Outgoing Monthly Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	10	12	*	15	12	18	22	12.25	20	36.5
1 - 2	7	3	*	6	5	10	11	5.25	10.5	36.5
2 - 3	3	5	*	4	2	9	9	3.5	9	32.5
3 - 4	4	6	*	3	1	3	2	3.5	2.5	34.75
4 - 5	7	8	*	4	6	2	4	6.25	3	32.5
5 - 6	4	3	*	4	5	7	2	4	4.5	31.88
6 - 7	13	19	*	15	17	14	4	16	9	34.88
7 - 8	48	59	*	69	58	26	11	58.5	18.5	36.15
8 - 9	131	120	*	129	120	31	27	125	29	35.92
9 - 10	130	145	*	115	109	57	46	124.75	51.5	36.98
10 - 11	118	132	*	151	140	90	80	135.25	85	36.33
11 - 12	129	119	*	149	110	134	141	126.75	137.5	36
12 - 13	114	141	*	159	161	164	125	143.75	144.5	37.38
13 - 14	142	137	*	143	154	201	203	144	202	37.25
14 - 15	135	158	*	168	198	170	146	164.75	158	36.5
15 - 16	194	2	*	204	189	164	136	147.25	150	33.5
16 - 17	220	*	*	233	232	173	122	228.33	147.5	35.67
17 - 18	221	*	*	246	246	188	156	237.67	172	36.37
18 - 19	192	*	173	236	244	201	155	211.25	178	36.15
19 - 20	191	*	232	171	201	161	110	198.75	135.5	36.8
20 - 21	109	*	113	103	130	127	92	113.75	109.5	37
21 - 22	76	*	118	95	89	93	81	94.5	87	36.68
22 - 23	51	*	58	62	66	46	37	59.25	41.5	35.88
23 - 24	19	*	24	18	32	35	18	23.25	26.5	37.62
Totals	2268	1069	718	2502	2527	2124	1740			
% of Total	17.52%	8.26%	5.55%	19.32%	19.52%	16.4%	13.44%			

Outgoing Weekly Speeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	33.6	30.08	27.5	29.5	32.04	28.6	40
1 - 2	*	*	*	31.17	33	31.8	32.82	32	32.33	40
2 - 3	*	*	*	33.25	24	28.11	27.11	30.17	27.61	30.5
3 - 4	*	*	*	28.67	44	30	28	32.5	29.2	37
4 - 5	*	*	*	26	30.67	32	28.5	28.8	29.67	31
5 - 6	*	*	*	25.5	30.8	37.57	37	28.44	37.44	29.5
6 - 7	*	*	*	29.4	29.18	30.36	32.25	29.28	30.78	32.25
7 - 8	*	*	*	31.07	31.28	30.69	30.64	31.17	30.68	36.65
8 - 9	*	*	*	30.14	30.02	33.32	30.04	30.08	31.79	34.8
9 - 10	*	*	*	31.4	31.17	32.37	32.07	31.29	32.23	36.9
10 - 11	*	*	*	30.03	30.04	31.8	31.9	30.03	31.85	35.7
11 - 12	*	*	*	31.17	30.43	32.1	31.99	30.86	32.04	36.25
12 - 13	*	*	*	31.11	31.38	32.33	32.9	31.24	32.57	37.25
13 - 14	*	*	*	31.59	31.88	30.78	31.9	31.74	31.34	37.05
14 - 15	*	*	*	30.62	31.52	31.89	31.64	31.11	31.78	36
15 - 16	*	*	*	29.52	31.41	32.72	31.99	30.43	32.39	34.8
16 - 17	*	*	*	30.75	32.22	31.87	31.81	31.49	31.84	35.75
17 - 18	*	*	*	31.88	31.92	31.63	32.99	31.9	32.25	36.6
18 - 19	*	*	32.22	31.88	32.51	31.98	32.12	32.21	32.04	36.3
19 - 20	*	*	32.24	32.19	31.3	32.06	32.13	31.91	32.08	36.7
20 - 21	*	*	32.76	31.71	32.28	31.7	33.14	32.27	32.31	36.77
21 - 22	*	*	31.58	32.55	31.12	31.92	31.3	31.75	31.63	36.57
22 - 23	*	*	31.88	30.48	31.15	31.37	31.68	31.16	31.51	36.17
23 - 24	*	*	32.33	32.83	30.59	31.71	32.06	31.7	31.83	37.33
Totals	0	0	32.2	31.1	31.5	31.8	32			
% of Total	0%	0%	20.3%	19.61%	19.86%	20.05%	20.18%			

Outgoing Weekly Speeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	27.6	29.42	*	*	*	*	*	28.59	*	33
1 - 2	28.86	29.33	*	*	*	*	*	29	*	33
2 - 3	31	26	*	*	*	*	*	27.88	*	34.5
3 - 4	32	29.67	*	*	*	*	*	30.6	*	32.5
4 - 5	28	25.75	*	*	*	*	*	26.8	*	34
5 - 6	28.25	30.33	*	*	*	*	*	29.14	*	34.25
6 - 7	33.15	28.74	*	*	*	*	*	30.53	*	37.5
7 - 8	29.77	31.78	*	*	*	*	*	30.88	*	35.65
8 - 9	32.05	31.67	*	*	*	*	*	31.87	*	37.05
9 - 10	31.64	31.3	*	*	*	*	*	31.46	*	37.05
10 - 11	31.77	30.77	*	*	*	*	*	31.24	*	36.95
11 - 12	29.65	30.84	*	*	*	*	*	30.22	*	35.75
12 - 13	32.76	31.26	*	*	*	*	*	31.93	*	37.5
13 - 14	32.46	32.53	*	*	*	*	*	32.5	*	37.45
14 - 15	31.93	31.39	*	*	*	*	*	31.64	*	37
15 - 16	30.32	27	*	*	*	*	*	30.29	*	32.2
16 - 17	30.93	*	*	*	*	*	*	30.93	*	35.5
17 - 18	31.76	*	*	*	*	*	*	31.76	*	35.9
18 - 19	31.81	*	*	*	*	*	*	31.81	*	35.7
19 - 20	31.86	*	*	*	*	*	*	31.86	*	37.1
20 - 21	32.63	*	*	*	*	*	*	32.63	*	37.7
21 - 22	31.97	*	*	*	*	*	*	31.97	*	37
22 - 23	29.9	*	*	*	*	*	*	29.9	*	35
23 - 24	32.05	*	*	*	*	*	*	32.05	*	38.5
Totals	31.5	31.3	0	0	0	0	0			
% of Total	50.16%	49.84%	0%	0%	0%	0%	0%			

Outgoing Monthly Speeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	27.6	29.42	*	33.6	30.08	27.5	29.5	30.49	28.6	36.5
1 - 2	28.86	29.33	*	31.17	33	31.8	32.82	30.57	32.33	36.5
2 - 3	31	26	*	33.25	24	28.11	27.11	28.86	27.61	32.5
3 - 4	32	29.67	*	28.67	44	30	28	31.14	29.2	34.75
4 - 5	28	25.75	*	26	30.67	32	28.5	27.6	29.67	32.5
5 - 6	28.25	30.33	*	25.5	30.8	37.57	37	28.75	37.44	31.88
6 - 7	33.15	28.74	*	29.4	29.18	30.36	32.25	29.91	30.78	34.88
7 - 8	29.77	31.78	*	31.07	31.28	30.69	30.64	31.03	30.68	36.15
8 - 9	32.05	31.67	*	30.14	30.02	33.32	30.04	30.98	31.79	35.92
9 - 10	31.64	31.3	*	31.4	31.17	32.37	32.07	31.38	32.23	36.98
10 - 11	31.77	30.77	*	30.03	30.04	31.8	31.9	30.59	31.85	36.33
11 - 12	29.65	30.84	*	31.17	30.43	32.1	31.99	30.55	32.04	36
12 - 13	32.76	31.26	*	31.11	31.38	32.33	32.9	31.55	32.57	37.38
13 - 14	32.46	32.53	*	31.59	31.88	30.78	31.9	32.11	31.34	37.25
14 - 15	31.93	31.39	*	30.62	31.52	31.89	31.64	31.34	31.78	36.5
15 - 16	30.32	27	*	29.52	31.41	32.72	31.99	30.38	32.39	33.5
16 - 17	30.93	*	*	30.75	32.22	31.87	31.81	31.31	31.84	35.67
17 - 18	31.76	*	*	31.88	31.92	31.63	32.99	31.86	32.25	36.37
18 - 19	31.81	*	32.22	31.88	32.51	31.98	32.12	32.12	32.04	36.15
19 - 20	31.86	*	32.24	32.19	31.3	32.06	32.13	31.9	32.08	36.8
20 - 21	32.63	*	32.76	31.71	32.28	31.7	33.14	32.36	32.31	37
21 - 22	31.97	*	31.58	32.55	31.12	31.92	31.3	31.8	31.63	36.68
22 - 23	29.9	*	31.88	30.48	31.15	31.37	31.68	30.89	31.51	35.88
23 - 24	32.05	*	32.33	32.83	30.59	31.71	32.06	31.77	31.83	37.62
Totals	31.5	31.3	32.2	31.1	31.5	31.8	32			
% of Total	14.23%	14.14%	14.54%	14.05%	14.23%	14.36%	14.45%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	44	36	34	34	40	34	40
1 - 2	*	*	*	41	39	36	34	40	35	40
2 - 3	*	*	*	31	30	32	33	30.5	32.5	30.5
3 - 4	*	*	*	30	44	37	30	37	33.5	37
4 - 5	*	*	*	29	33	39	30	31	34.5	31
5 - 6	*	*	*	25	34	42	42	29.5	42	29.5
6 - 7	*	*	*	32.5	32	33	34	32.25	33.5	32.25
7 - 8	*	*	*	38	35.3	35.7	34	36.65	34.85	36.65
8 - 9	*	*	*	35.7	33.9	38.7	35	34.8	36.85	34.8
9 - 10	*	*	*	37.2	36.6	37.8	38	36.9	37.9	36.9
10 - 11	*	*	*	36.3	35.1	36	38	35.7	37	35.7
11 - 12	*	*	*	36.2	36.3	37.6	37	36.25	37.3	36.25
12 - 13	*	*	*	36.9	37.6	37.2	39.3	37.25	38.25	37.25
13 - 14	*	*	*	36.7	37.4	35.6	36.3	37.05	35.95	37.05
14 - 15	*	*	*	35.7	36.3	37	36.3	36	36.65	36
15 - 16	*	*	*	34	35.6	37.3	37.6	34.8	37.45	34.8
16 - 17	*	*	*	35.1	36.4	37.4	36.3	35.75	36.85	35.75
17 - 18	*	*	*	36.4	36.8	36.8	38.2	36.6	37.5	36.6
18 - 19	*	*	36.8	35.4	36.7	37.2	36.8	36.3	37	36.3
19 - 20	*	*	37.1	36.5	36.5	36.4	36.3	36.7	36.35	36.7
20 - 21	*	*	37.6	35.7	37	35.7	39	36.77	37.35	36.77
21 - 22	*	*	36.4	36.9	36.4	37	37	36.57	37	36.57
22 - 23	*	*	38.5	35	35	38	36	36.17	37	36.17
23 - 24	*	*	40.3	36	35.7	40	37	37.33	38.5	37.33
Totals	0	0	226.7	846.2	862.6	884.4	865.1			
% of Total	0%	0%	6.15%	22.96%	23.41%	24%	23.48%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	30	36	*	*	*	*	*	33	0	33
1 - 2	31	35	*	*	*	*	*	33	0	33
2 - 3	39	30	*	*	*	*	*	34.5	0	34.5
3 - 4	33	32	*	*	*	*	*	32.5	0	32.5
4 - 5	38	30	*	*	*	*	*	34	0	34
5 - 6	29.5	39	*	*	*	*	*	34.25	0	34.25
6 - 7	42	33	*	*	*	*	*	37.5	0	37.5
7 - 8	33.3	38	*	*	*	*	*	35.65	0	35.65
8 - 9	37.2	36.9	*	*	*	*	*	37.05	0	37.05
9 - 10	37.4	36.7	*	*	*	*	*	37.05	0	37.05
10 - 11	37.5	36.4	*	*	*	*	*	36.95	0	36.95
11 - 12	35.2	36.3	*	*	*	*	*	35.75	0	35.75
12 - 13	38	37	*	*	*	*	*	37.5	0	37.5
13 - 14	37.8	37.1	*	*	*	*	*	37.45	0	37.45
14 - 15	37.5	36.5	*	*	*	*	*	37	0	37
15 - 16	35.4	29	*	*	*	*	*	32.2	0	32.2
16 - 17	35.5	*	*	*	*	*	*	35.5	0	35.5
17 - 18	35.9	*	*	*	*	*	*	35.9	0	35.9
18 - 19	35.7	*	*	*	*	*	*	35.7	0	35.7
19 - 20	37.1	*	*	*	*	*	*	37.1	0	37.1
20 - 21	37.7	*	*	*	*	*	*	37.7	0	37.7
21 - 22	37	*	*	*	*	*	*	37	0	37
22 - 23	35	*	*	*	*	*	*	35	0	35
23 - 24	38.5	*	*	*	*	*	*	38.5	0	38.5
Totals	864.2	558.9	0	0	0	0	0			
% of Total	60.73%	39.27%	0%	0%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	30	36	*	44	36	34	34	36.5	34	36.5
1 - 2	31	35	*	41	39	36	34	36.5	35	36.5
2 - 3	39	30	*	31	30	32	33	32.5	32.5	32.5
3 - 4	33	32	*	30	44	37	30	34.75	33.5	34.75
4 - 5	38	30	*	29	33	39	30	32.5	34.5	32.5
5 - 6	29.5	39	*	25	34	42	42	31.88	42	31.88
6 - 7	42	33	*	32.5	32	33	34	34.88	33.5	34.88
7 - 8	33.3	38	*	38	35.3	35.7	34	36.15	34.85	36.15
8 - 9	37.2	36.9	*	35.7	33.9	38.7	35	35.92	36.85	35.92
9 - 10	37.4	36.7	*	37.2	36.6	37.8	38	36.98	37.9	36.98
10 - 11	37.5	36.4	*	36.3	35.1	36	38	36.33	37	36.33
11 - 12	35.2	36.3	*	36.2	36.3	37.6	37	36	37.3	36
12 - 13	38	37	*	36.9	37.6	37.2	39.3	37.38	38.25	37.38
13 - 14	37.8	37.1	*	36.7	37.4	35.6	36.3	37.25	35.95	37.25
14 - 15	37.5	36.5	*	35.7	36.3	37	36.3	36.5	36.65	36.5
15 - 16	35.4	29	*	34	35.6	37.3	37.6	33.5	37.45	33.5
16 - 17	35.5	*	*	35.1	36.4	37.4	36.3	35.67	36.85	35.67
17 - 18	35.9	*	*	36.4	36.8	36.8	38.2	36.37	37.5	36.37
18 - 19	35.7	*	36.8	35.4	36.7	37.2	36.8	36.15	37	36.15
19 - 20	37.1	*	37.1	36.5	36.5	36.4	36.3	36.8	36.35	36.8
20 - 21	37.7	*	37.6	35.7	37	35.7	39	37	37.35	37
21 - 22	37	*	36.4	36.9	36.4	37	37	36.68	37	36.68
22 - 23	35	*	38.5	35	35	38	36	35.88	37	35.88
23 - 24	38.5	*	40.3	36	35.7	40	37	37.62	38.5	37.62

Summary of Violators

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	89	29.6	2	49.5
01:00:00	42	31.5	1	49.0
02:00:00	32	28.2	0	0.0
03:00:00	19	30.6	0	0.0
04:00:00	31	28.0	1	50.0
05:00:00	25	31.9	0	0.0
06:00:00	82	30.1	1	47.0
07:00:00	271	31.0	5	47.0
08:00:00	558	31.1	4	49.5
09:00:00	602	31.5	9	48.4
10:00:00	711	30.9	7	49.6
11:00:00	782	31.1	6	47.5
12:00:00	864	31.9	17	47.4
13:00:00	980	31.8	11	49.3
14:00:00	975	31.5	9	49.3
15:00:00	889	31.1	8	47.3
16:00:00	980	31.5	7	48.4
17:00:00	1057	32.0	12	47.4
18:00:00	1201	32.1	9	49.0
19:00:00	1066	31.9	11	47.5
20:00:00	674	32.3	9	48.0
21:00:00	552	31.7	9	48.3
22:00:00	320	31.0	5	46.8
23:00:00	146	31.8	5	51.0

Outgoing Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	0	1	7	38	78	35	11	2	1	0	0	0	0	0	0	0	0	0	173	32.2	36.8	27 to 37	75.1	2	1.2	1	168	4	
9/4/2024	19:00	0	2	12	50	102	43	21	2	0	0	0	0	0	0	0	0	0	0	232	32.2	37.1	27 to 37	71.6	*	*	2	221	9	
9/4/2024	20:00	0	0	4	25	45	27	12	0	0	0	0	0	0	0	0	0	0	0	113	32.8	37.6	27 to 37	73.5	*	*	0	109	4	
9/4/2024	21:00	0	0	10	33	45	21	7	2	0	0	0	0	0	0	0	0	0	0	118	31.6	36.4	27 to 37	74.6	2	1.7	1	114	3	
9/4/2024	22:00	0	0	5	18	21	6	5	3	0	0	0	0	0	0	0	0	0	0	58	31.9	38.5	24 to 34	72.4	3	5.2	0	57	1	
9/4/2024	23:00	0	0	4	7	4	4	4	1	0	0	0	0	0	0	0	0	0	0	24	32.3	40.3	23 to 33	58.3	1	4.2	0	23	1	
24 Hr Summary		0	3	42	171	295	136	60	10	1	0	0	0	0	0	0	0	0	0	718	32.2	38	27 to 37	72.4	8	1.1	4	692	22	

Outgoing Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/5/2024	00:00	0	0	2	1	7	2	1	1	1	0	0	0	0	0	0	0	0	0	15	33.6	44	22 to 32	60.0	2	13.3	0	14	1
9/5/2024	01:00	0	0	2	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	6	31.2	41	20 to 30	50.0	*	*	0	6	0
9/5/2024	02:00	0	0	0	1	2	0	1	0	0	0	0	0	0	0	0	0	0	0	4	33.3	31	21 to 31	75.0	*	*	0	4	0
9/5/2024	03:00	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	28.7	30	20 to 30	100.0	*	*	0	3	0
9/5/2024	04:00	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26	29	22 to 32	75.0	*	*	0	4	0
9/5/2024	05:00	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	25.5	25	18 to 28	100.0	*	*	0	4	0
9/5/2024	06:00	0	0	2	6	6	0	1	0	0	0	0	0	0	0	0	0	0	0	15	29.4	32.5	23 to 33	86.7	*	*	0	12	3
9/5/2024	07:00	1	1	9	15	27	7	4	5	0	0	0	0	0	0	0	0	0	0	69	31.1	38	23 to 33	66.7	3	4.3	1	61	7
9/5/2024	08:00	0	1	15	46	42	20	5	0	0	0	0	0	0	0	0	0	0	0	129	30.1	35.7	24 to 34	72.1	*	*	0	119	10
9/5/2024	09:00	0	1	7	37	38	26	4	2	0	0	0	0	0	0	0	0	0	0	115	31.4	37.2	25 to 35	68.7	1	0.9	3	103	9
9/5/2024	10:00	0	4	22	51	39	26	5	4	0	0	0	0	0	0	0	0	0	0	151	30	36.3	24 to 34	66.9	2	1.3	1	133	17
9/5/2024	11:00	0	0	11	50	46	31	10	1	0	0	0	0	0	0	0	0	0	0	149	31.2	36.2	25 to 35	70.5	1	0.7	1	136	12
9/5/2024	12:00	0	3	25	43	46	27	6	9	0	0	0	0	0	0	0	0	0	0	159	31.1	36.9	23 to 33	63.5	6	3.8	2	130	27
9/5/2024	13:00	0	4	9	37	47	37	6	2	0	1	0	0	0	0	0	0	0	0	143	31.6	36.7	25 to 35	67.1	1	0.7	1	126	16
9/5/2024	14:00	1	4	19	46	63	27	5	2	0	0	1	0	0	0	0	0	0	0	168	30.6	35.7	24 to 34	70.2	1	0.6	3	140	25
9/5/2024	15:00	0	3	22	86	62	26	4	1	0	0	0	0	0	0	0	0	0	0	204	29.5	34	25 to 35	77.9	1	0.5	4	183	17
9/5/2024	16:00	0	4	22	70	88	36	10	3	0	0	0	0	0	0	0	0	0	0	233	30.8	35.1	26 to 36	73.8	1	0.4	1	213	19
9/5/2024	17:00	0	0	16	68	91	50	15	6	0	0	0	0	0	0	0	0	0	0	246	31.9	36.4	25 to 35	72.8	3	1.2	4	224	18
9/5/2024	18:00	1	0	9	56	111	48	9	2	0	0	0	0	0	0	0	0	0	0	236	31.9	35.4	26 to 36	81.8	2	0.8	2	221	13
9/5/2024	19:00	0	0	5	50	63	35	15	2	1	0	0	0	0	0	0	0	0	0	171	32.2	36.5	26 to 36	73.1	2	1.2	1	166	4
9/5/2024	20:00	0	0	5	25	52	14	4	3	0	0	0	0	0	0	0	0	0	0	103	31.7	35.7	27 to 37	80.6	3	2.9	0	101	2
9/5/2024	21:00	0	0	5	20	34	30	4	2	0	0	0	0	0	0	0	0	0	0	95	32.5	36.9	28 to 38	77.9	*	*	0	91	4
9/5/2024	22:00	0	0	9	16	23	10	4	0	0	0	0	0	0	0	0	0	0	0	62	30.5	35	25 to 35	71.0	*	*	0	59	3
9/5/2024	23:00	0	0	0	3	9	3	3	0	0	0	0	0	0	0	0	0	0	0	18	32.8	36	26 to 36	77.8	*	*	0	18	0
24 Hr Summary		3	26	217	733	901	455	118	45	2	1	1	0	0	0	0	0	0	0	2502	31.1	37	25 to 35	71.0	29	1.2	24	2271	207

Outgoing Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/6/2024	00:00	0	0	1	6	2	2	1	0	0	0	0	0	0	0	0	0	0	0	12	30.1	36	22 to 32	75.0	*	*	0	12	0
9/6/2024	01:00	0	0	0	2	1	1	1	0	0	0	0	0	0	0	0	0	0	0	5	33	39	21 to 31	60.0	*	*	0	5	0
9/6/2024	02:00	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24	30	8 to 18	50.0	*	*	0	2	0
9/6/2024	03:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	44	44	34 to 44	100.0	*	*	0	1	0
9/6/2024	04:00	0	0	1	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	6	30.7	33	23 to 33	83.3	*	*	0	6	0
9/6/2024	05:00	0	0	1	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	5	30.8	34	28 to 38	80.0	*	*	0	5	0
9/6/2024	06:00	0	0	3	4	10	0	0	0	0	0	0	0	0	0	0	0	0	0	17	29.2	32	22 to 32	82.4	*	*	0	16	1
9/6/2024	07:00	0	0	5	19	20	10	3	1	0	0	0	0	0	0	0	0	0	0	58	31.3	35.3	25 to 35	74.1	1	1.7	0	55	3
9/6/2024	08:00	0	1	7	49	46	14	2	1	0	0	0	0	0	0	0	0	0	0	120	30	33.9	24 to 34	81.7	*	*	0	114	6
9/6/2024	09:00	0	1	13	35	29	22	7	2	0	0	0	0	0	0	0	0	0	0	109	31.2	36.6	27 to 37	68.8	2	1.8	0	100	9
9/6/2024	10:00	0	4	22	37	48	20	9	0	0	0	0	0	0	0	0	0	0	0	140	30	35.1	26 to 36	68.6	*	*	1	124	15
9/6/2024	11:00	0	3	7	44	36	13	5	2	0	0	0	0	0	0	0	0	0	0	110	30.4	36.3	23 to 33	75.5	1	0.9	1	100	9
9/6/2024	12:00	0	7	16	35	57	29	12	5	0	0	0	0	0	0	0	0	0	0	161	31.4	37.6	26 to 36	63.4	3	1.9	1	142	18
9/6/2024	13:00	0	2	7	47	49	35	11	3	0	0	0	0	0	0	0	0	0	0	154	31.9	37.4	25 to 35	69.5	2	1.3	1	140	13
9/6/2024	14:00	0	0	16	53	76	40	13	0	0	0	0	0	0	0	0	0	0	0	198	31.5	36.3	26 to 36	72.2	*	*	4	182	12
9/6/2024	15:00	0	1	12	55	78	30	8	5	0	0	0	0	0	0	0	0	0	0	189	31.4	35.6	26 to 36	76.7	5	2.6	0	177	12
9/6/2024	16:00	0	2	10	50	99	55	12	4	0	0	0	0	0	0	0	0	0	0	232	32.2	36.4	27 to 37	79.3	2	0.9	3	206	23
9/6/2024	17:00	0	2	11	63	104	44	20	1	1	0	0	0	0	0	0	0	0	0	246	31.9	36.8	26 to 36	74.8	1	0.4	3	226	17
9/6/2024	18:00	0	1	11	47	109	58	15	2	1	0	0	0	0	0	0	0	0	0	244	32.5	36.7	27 to 37	76.6	2	0.8	0	229	15
9/6/2024	19:00	0	1	12	67	70	38	12	1	0	0	0	0	0	0	0	0	0	0	201	31.3	36.5	27 to 37	75.1	1	0.5	3	186	12
9/6/2024	20:00	0	0	6	36	48	24	12	4	0	0	0	0	0	0	0	0	0	0	130	32.3	37	27 to 37	73.1	1	0.8	2	122	6
9/6/2024	21:00	0	0	9	23	37	15	3	2	0	0	0	0	0	0	0	0	0	0	89	31.1	36.4	23 to 33	73.0	2	2.2	2	83	4
9/6/2024	22:00	0	1	4	24	21	11	3	2	0	0	0	0	0	0	0	0	0	0	66	31.2	35	26 to 36	78.8	1	1.5	0	65	1
9/6/2024	23:00	0	0	4	13	6	7	1	1	0	0	0	0	0	0	0	0	0	0	32	30.6	35.7	23 to 33	68.8	*	*	0	29	3
24 Hr Summary		0	27	178	711	953	469	151	36	2	0	0	0	0	0	0	0	0	0	2527	31.5	37	26 to 36	72.3	24	0.9	21	2327	179

Outgoing Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/7/2024	00:00	0	2	4	6	3	2	1	0	0	0	0	0	0	0	0	0	0	0	18	27.5	34	21 to 31	66.7	*	*	0	16	2
9/7/2024	01:00	0	0	1	3	3	1	2	0	0	0	0	0	0	0	0	0	0	0	10	31.8	36	21 to 31	70.0	*	*	0	10	0
9/7/2024	02:00	0	1	1	3	3	1	0	0	0	0	0	0	0	0	0	0	0	0	9	28.1	32	22 to 32	77.8	*	*	0	9	0
9/7/2024	03:00	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	30	37	21 to 31	66.7	*	*	0	3	0
9/7/2024	04:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	32	39	15 to 25	50.0	*	*	0	2	0
9/7/2024	05:00	0	0	1	0	1	1	4	0	0	0	0	0	0	0	0	0	0	0	7	37.6	42	33 to 43	85.7	*	*	1	6	0
9/7/2024	06:00	0	0	1	6	6	0	1	0	0	0	0	0	0	0	0	0	0	0	14	30.4	33	24 to 34	85.7	*	*	0	14	0
9/7/2024	07:00	0	0	5	7	8	3	2	1	0	0	0	0	0	0	0	0	0	0	26	30.7	35.7	23 to 33	61.5	*	*	0	20	6
9/7/2024	08:00	0	0	2	4	14	7	3	1	0	0	0	0	0	0	0	0	0	0	31	33.3	38.7	29 to 39	74.2	1	3.2	0	29	2
9/7/2024	09:00	0	0	6	19	8	16	5	3	0	0	0	0	0	0	0	0	0	0	57	32.4	37.8	28 to 38	59.6	1	1.8	0	53	4
9/7/2024	10:00	0	2	2	27	38	14	6	0	0	0	1	0	0	0	0	0	0	0	90	31.8	36	25 to 35	76.7	1	1.1	2	84	4
9/7/2024	11:00	0	0	11	37	43	29	12	2	0	0	0	0	0	0	0	0	0	0	134	32.1	37.6	26 to 36	70.1	1	0.7	1	123	10
9/7/2024	12:00	0	1	10	41	59	36	13	3	1	0	0	0	0	0	0	0	0	0	164	32.3	37.2	26 to 36	73.2	3	1.8	1	155	8
9/7/2024	13:00	0	1	20	67	70	33	7	1	1	1	0	0	0	0	0	0	0	0	201	30.8	35.6	26 to 36	72.1	3	1.5	2	189	10
9/7/2024	14:00	0	0	7	54	57	40	9	3	0	0	0	0	0	0	0	0	0	0	170	31.9	37	26 to 36	72.9	3	1.8	2	160	8
9/7/2024	15:00	0	2	5	41	60	38	14	4	0	0	0	0	0	0	0	0	0	0	164	32.7	37.3	27 to 37	76.2	1	0.6	0	154	10
9/7/2024	16:00	0	2	13	44	65	35	12	2	0	0	0	0	0	0	0	0	0	0	173	31.9	37.4	28 to 38	69.4	2	1.2	4	160	9
9/7/2024	17:00	1	3	15	51	65	34	14	4	1	0	0	0	0	0	0	0	0	0	188	31.6	36.8	27 to 37	70.7	5	2.7	3	176	9
9/7/2024	18:00	1	3	10	56	66	49	16	0	0	0	0	0	0	0	0	0	0	0	201	32	37.2	28 to 38	71.1	*	*	2	192	7
9/7/2024	19:00	1	1	9	37	59	43	10	1	0	0	0	0	0	0	0	0	0	0	161	32.1	36.4	27 to 37	77.6	1	0.6	1	152	8
9/7/2024	20:00	0	0	6	37	51	25	6	2	0	0	0	0	0	0	0	0	0	0	127	31.7	35.7	26 to 36	80.3	2	1.6	0	126	1
9/7/2024	21:00	0	0	5	29	36	13	5	4	0	1	0	0	0	0	0	0	0	0	93	31.9	37	25 to 35	76.3	5	5.4	0	90	3
9/7/2024	22:00	0	1	7	11	12	9	4	2	0	0	0	0	0	0	0	0	0	0	46	31.4	38	26 to 36	60.9	1	2.2	0	45	1
9/7/2024	23:00	0	2	3	10	9	5	4	0	2	0	0	0	0	0	0	0	0	0	35	31.7	40	25 to 35	62.9	2	5.7	0	35	0
24 Hr Summary		3	21	145	591	737	436	150	33	5	2	1	0	0	0	0	0	0	0	2124	31.8	38	26 to 36	70.1	32	1.5	19	2003	102

Outgoing Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/8/2024	00:00	0	0	3	10	6	2	1	0	0	0	0	0	0	0	0	0	0	0	22	29.5	34	22 to 32	81.8	*	*	0	21	1
9/8/2024	01:00	0	0	0	4	5	0	1	1	0	0	0	0	0	0	0	0	0	0	11	32.8	34	24 to 34	81.8	1	9.1	0	11	0
9/8/2024	02:00	0	1	2	2	3	1	0	0	0	0	0	0	0	0	0	0	0	0	9	27.1	33	23 to 33	77.8	*	*	0	9	0
9/8/2024	03:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28	30	20 to 30	100.0	*	*	0	2	0
9/8/2024	04:00	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28.5	30	23 to 33	100.0	*	*	0	4	0
9/8/2024	05:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	37	42	32 to 42	100.0	*	*	0	2	0
9/8/2024	06:00	0	0	0	2	1	0	1	0	0	0	0	0	0	0	0	0	0	0	4	32.3	34	24 to 34	75.0	*	*	0	4	0
9/8/2024	07:00	0	0	1	4	4	2	0	0	0	0	0	0	0	0	0	0	0	0	11	30.6	34	26 to 36	90.9	*	*	0	11	0
9/8/2024	08:00	0	2	3	10	5	5	0	1	1	0	0	0	0	0	0	0	0	0	27	30	35	25 to 35	66.7	1	3.7	0	24	3
9/8/2024	09:00	0	0	3	12	19	7	5	0	0	0	0	0	0	0	0	0	0	0	46	32.1	38	26 to 36	73.9	*	*	0	46	0
9/8/2024	10:00	0	1	7	28	15	18	6	5	0	0	0	0	0	0	0	0	0	0	80	31.9	38	22 to 32	58.8	2	2.5	1	76	3
9/8/2024	11:00	0	0	11	39	50	27	8	5	1	0	0	0	0	0	0	0	0	0	141	32	37	26 to 36	70.2	3	2.1	0	136	5
9/8/2024	12:00	0	1	6	24	52	21	16	5	0	0	0	0	0	0	0	0	0	0	125	32.9	39.3	26 to 36	65.6	2	1.6	0	121	4
9/8/2024	13:00	0	1	8	63	78	37	14	2	0	0	0	0	0	0	0	0	0	0	203	31.9	36.3	26 to 36	77.8	2	1.0	1	193	9
9/8/2024	14:00	0	0	10	45	50	32	8	1	0	0	0	0	0	0	0	0	0	0	146	31.6	36.3	27 to 37	76.0	1	0.7	0	140	6
9/8/2024	15:00	0	3	10	33	46	30	12	2	0	0	0	0	0	0	0	0	0	0	136	32	37.6	28 to 38	69.1	1	0.7	0	131	5
9/8/2024	16:00	0	0	9	33	44	29	6	1	0	0	0	0	0	0	0	0	0	0	122	31.8	36.3	27 to 37	74.6	1	0.8	0	120	2
9/8/2024	17:00	1	1	3	35	63	33	15	5	0	0	0	0	0	0	0	0	0	0	156	33	38.2	27 to 37	75.0	3	1.9	1	151	4
9/8/2024	18:00	0	0	9	45	60	26	13	2	0	0	0	0	0	0	0	0	0	0	155	32.1	36.8	26 to 36	73.5	1	0.6	1	151	3
9/8/2024	19:00	0	0	4	29	48	19	8	2	0	0	0	0	0	0	0	0	0	0	110	32.1	36.3	26 to 36	78.2	2	1.8	0	109	1
9/8/2024	20:00	0	0	7	18	31	22	12	1	1	0	0	0	0	0	0	0	0	0	92	33.1	39	27 to 37	66.3	2	2.2	0	89	3
9/8/2024	21:00	0	0	9	23	23	23	2	1	0	0	0	0	0	0	0	0	0	0	81	31.3	37	28 to 38	66.7	*	*	0	79	2
9/8/2024	22:00	0	0	5	8	14	8	2	0	0	0	0	0	0	0	0	0	0	0	37	31.7	36	27 to 37	73.0	*	*	0	36	1
9/8/2024	23:00	0	0	1	5	6	3	3	0	0	0	0	0	0	0	0	0	0	0	18	32.1	37	25 to 35	66.7	*	*	0	17	1
24 Hr Summary		1	10	111	475	627	345	134	34	3	0	0	0	0	0	0	0	0	0	1740	32	38	26 to 36	70.9	22	1.3	4	1683	53

Outgoing Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/9/2024	00:00	0	1	2	4	2	1	0	0	0	0	0	0	0	0	0	0	0	0	10	27.6	30	23 to 33	80.0	*	*	0	9	1
9/9/2024	01:00	0	0	1	1	5	0	0	0	0	0	0	0	0	0	0	0	0	0	7	28.9	31	21 to 31	85.7	*	*	1	5	1
9/9/2024	02:00	0	0	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	31	39	18 to 28	66.7	*	*	0	3	0
9/9/2024	03:00	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	4	32	33	25 to 35	100.0	*	*	0	4	0
9/9/2024	04:00	1	1	2	0	1	1	0	0	1	0	0	0	0	0	0	0	0	0	7	28	38	13 to 23	42.9	1	14.3	0	7	0
9/9/2024	05:00	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28.3	29.5	20 to 30	100.0	*	*	0	4	0
9/9/2024	06:00	0	0	1	3	4	2	2	1	0	0	0	0	0	0	0	0	0	0	13	33.2	42	25 to 35	69.2	1	7.7	0	12	1
9/9/2024	07:00	0	0	6	14	23	5	0	0	0	0	0	0	0	0	0	0	0	0	48	29.8	33.3	23 to 33	79.2	*	*	0	45	3
9/9/2024	08:00	0	0	6	39	54	20	9	2	1	0	0	0	0	0	0	0	0	0	131	32.1	37.2	24 to 34	73.3	2	1.5	0	125	6
9/9/2024	09:00	0	1	11	39	44	22	9	2	2	0	0	0	0	0	0	0	0	0	130	31.6	37.4	24 to 34	70.0	3	2.3	3	112	15
9/9/2024	10:00	0	2	5	38	37	24	11	1	0	0	0	0	0	0	0	0	0	0	118	31.8	37.5	26 to 36	67.8	*	*	2	103	13
9/9/2024	11:00	0	0	25	43	36	19	6	0	0	0	0	0	0	0	0	0	0	0	129	29.7	35.2	23 to 33	71.3	*	*	3	111	15
9/9/2024	12:00	0	0	7	28	38	29	9	3	0	0	0	0	0	0	0	0	0	0	114	32.8	38	28 to 38	67.5	2	1.8	1	104	9
9/9/2024	13:00	0	0	13	28	49	35	15	2	0	0	0	0	0	0	0	0	0	0	142	32.5	37.8	26 to 36	67.6	2	1.4	0	131	11
9/9/2024	14:00	0	1	10	41	38	32	10	3	0	0	0	0	0	0	0	0	0	0	135	31.9	37.5	26 to 36	70.4	3	2.2	0	125	10
9/9/2024	15:00	0	1	18	71	66	33	4	1	0	0	0	0	0	0	0	0	0	0	194	30.3	35.4	23 to 33	75.8	*	*	1	180	13
9/9/2024	16:00	2	3	17	69	73	44	11	0	0	1	0	0	0	0	0	0	0	0	220	30.9	35.5	26 to 36	74.5	1	0.5	2	205	13
9/9/2024	17:00	0	0	9	53	104	46	9	0	0	0	0	0	0	0	0	0	0	0	221	31.8	35.9	27 to 37	81.4	*	*	0	209	12
9/9/2024	18:00	0	3	8	50	77	42	9	2	0	1	0	0	0	0	0	0	0	0	192	31.8	35.7	26 to 36	79.7	2	1.0	0	180	12
9/9/2024	19:00	0	3	8	56	74	32	13	5	0	0	0	0	0	0	0	0	0	0	191	31.9	37.1	25 to 35	72.3	5	2.6	4	177	10
9/9/2024	20:00	0	0	6	25	43	23	10	1	1	0	0	0	0	0	0	0	0	0	109	32.6	37.7	27 to 37	74.3	1	0.9	0	104	5
9/9/2024	21:00	0	0	4	23	29	15	4	1	0	0	0	0	0	0	0	0	0	0	76	32	37	27 to 37	76.3	*	*	0	75	1
9/9/2024	22:00	0	2	5	19	15	6	4	0	0	0	0	0	0	0	0	0	0	0	51	29.9	35	22 to 32	72.5	*	*	0	47	4
9/9/2024	23:00	0	1	5	3	2	6	0	0	1	1	0	0	0	0	0	0	0	0	19	32.1	38.5	16 to 26	47.4	2	10.5	0	18	1
24 Hr Summary		3	19	169	652	818	439	135	24	6	3	0	0	0	0	0	0	0	0	2268	31.5	37	26 to 36	71.6	25	1.1	17	2095	156

Outgoing Histogram

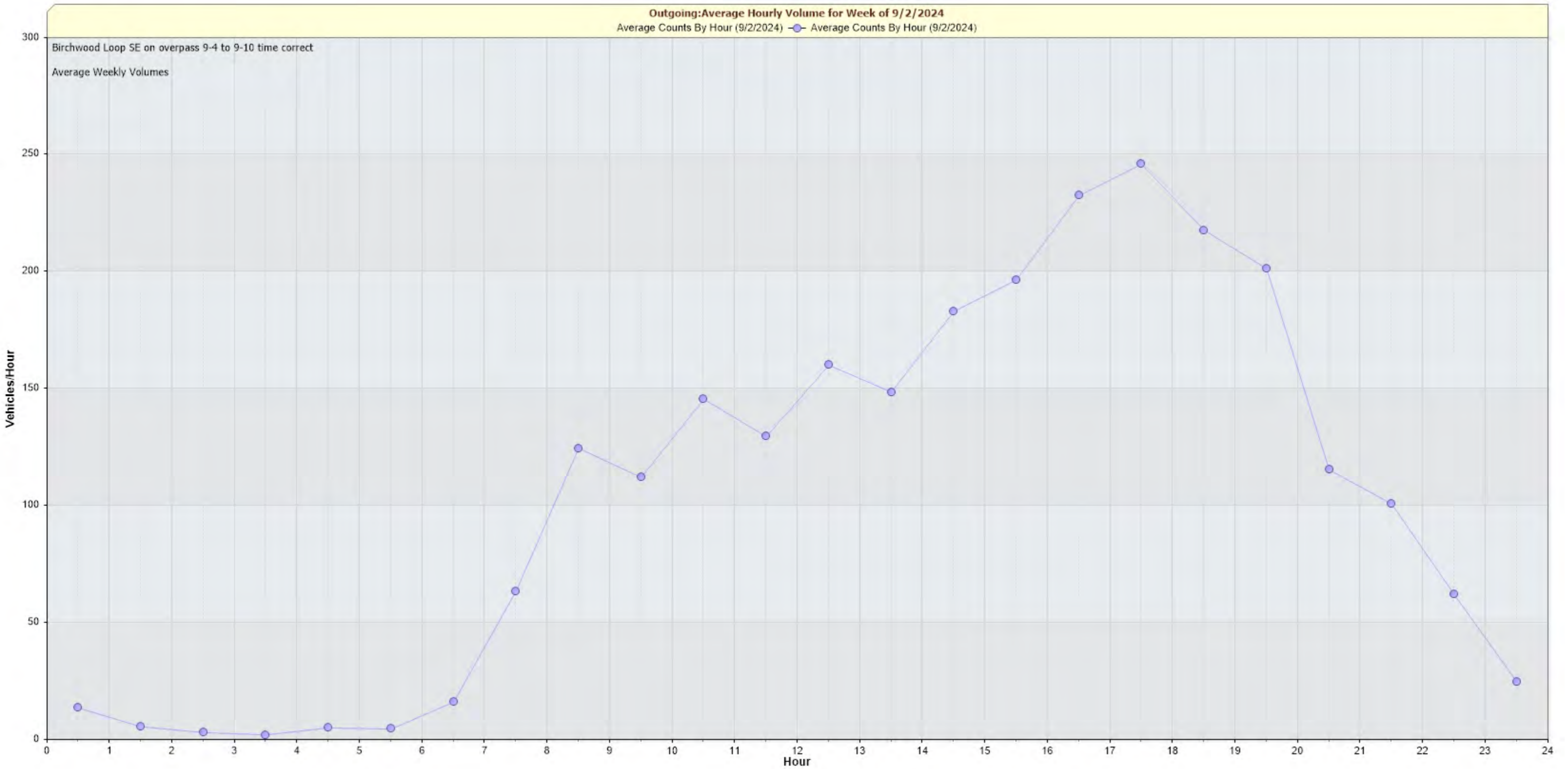
Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	0	0	3	5	0	3	1	0	0	0	0	0	0	0	0	0	0	0	12	29.4	36	18 to 28	66.7	*	*	0	11	1	
9/10/2024	01:00	0	0	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	29.3	35	25 to 35	100.0	*	*	0	3	0	
9/10/2024	02:00	0	1	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	5	26	30	20 to 30	60.0	*	*	0	5	0	
9/10/2024	03:00	0	0	1	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	6	29.7	32	22 to 32	66.7	*	*	0	6	0	
9/10/2024	04:00	0	1	2	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	8	25.8	30	20 to 30	75.0	*	*	0	8	0	
9/10/2024	05:00	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	30.3	39	19 to 29	66.7	*	*	0	3	0	
9/10/2024	06:00	0	2	3	7	4	1	2	0	0	0	0	0	0	0	0	0	0	0	19	28.7	33	23 to 33	73.7	*	*	0	16	3	
9/10/2024	07:00	0	1	4	17	21	11	4	1	0	0	0	0	0	0	0	0	0	0	59	31.8	38	24 to 34	67.8	1	1.7	0	55	4	
9/10/2024	08:00	0	0	4	41	45	20	10	0	0	0	0	0	0	0	0	0	0	0	120	31.7	36.9	25 to 35	74.2	*	*	1	111	8	
9/10/2024	09:00	1	2	12	42	48	30	8	1	1	0	0	0	0	0	0	0	0	0	145	31.3	36.7	27 to 37	73.1	2	1.4	1	125	19	
9/10/2024	10:00	0	0	16	49	32	26	7	1	1	0	0	0	0	0	0	0	0	0	132	30.8	36.4	24 to 34	68.2	2	1.5	3	109	20	
9/10/2024	11:00	0	2	15	31	35	30	6	0	0	0	0	0	0	0	0	0	0	0	119	30.8	36.3	27 to 37	66.4	*	*	1	101	17	
9/10/2024	12:00	1	5	13	34	45	31	11	1	0	0	0	0	0	0	0	0	0	0	141	31.3	37	27 to 37	67.4	1	0.7	4	113	24	
9/10/2024	13:00	0	0	10	27	53	36	9	2	0	0	0	0	0	0	0	0	0	0	137	32.5	37.1	28 to 38	73.0	1	0.7	2	119	16	
9/10/2024	14:00	0	1	13	49	47	36	10	2	0	0	0	0	0	0	0	0	0	0	158	31.4	36.5	26 to 36	69.6	1	0.6	1	134	23	
9/10/2024	15:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27	29	19 to 29	100.0	*	*	0	2	0	
9/10/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		2	15	98	312	336	228	68	8	2	0	0	0	0	0	0	0	0	0	1069	31.3	37	27 to 37	67.1	8	0.7	13	921	135	

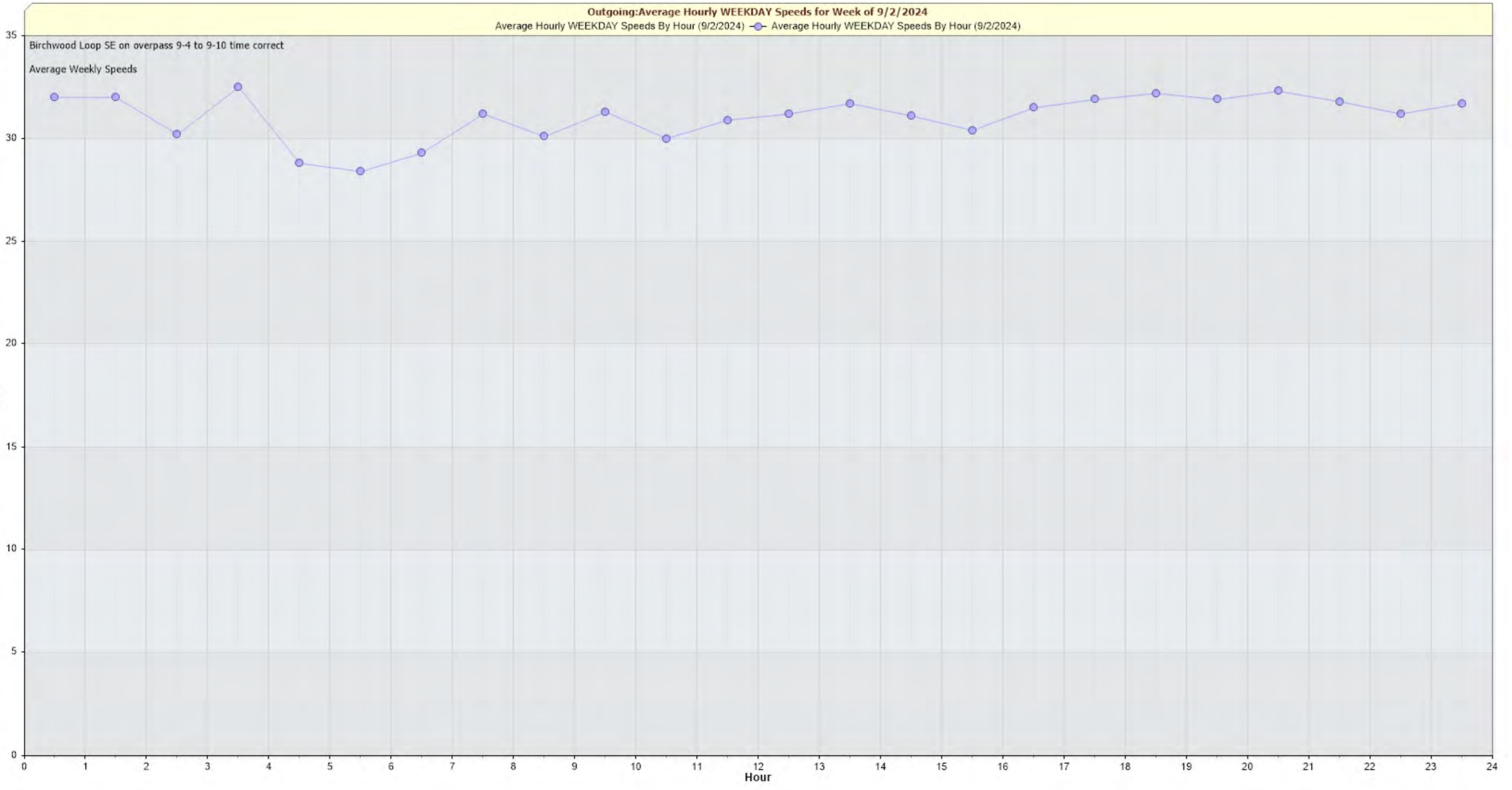
Outgoing: Average Hourly Volume for Week of 9/2/2024
Average Counts By Hour (9/2/2024) ● Average Counts By Hour (9/2/2024)

Birchwood Loop SE on overpass 9-4 to 9-10 time correct
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/2/2024
Average Hourly WEEKDAY Speeds By Hour (9/2/2024) — Average Hourly WEEKDAY Speeds By Hour (9/2/2024)

Birchwood Loop SE on overpass 9-4 to 9-10 time correct
Average Weekly Speeds

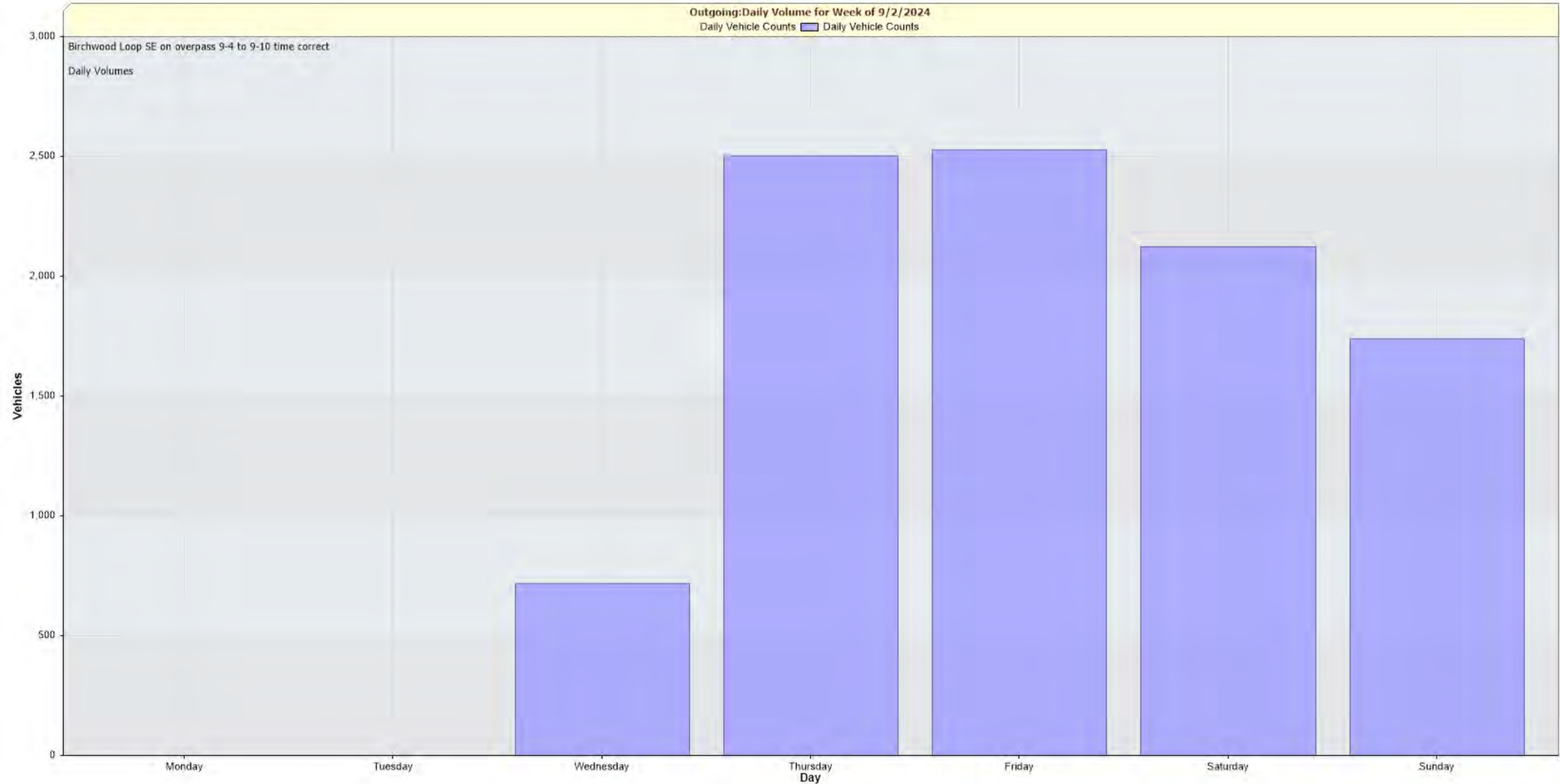


Outgoing: Daily Volume for Week of 9/2/2024

Daily Vehicle Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

Daily Volumes



Monday

Tuesday

Wednesday

Thursday
Day

Friday

Saturday

Sunday

Vehicles

3,000
2,500
2,000
1,500
1,000
500
0

For Project: Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:43:14 PM

Speed Intervals: 1 MPH

Time Intervals: Instant

Traffic Report From: 9/4/2024 7:00:00 PM through 9/10/2024 10:59:59 AM

85th Percentile Speed: 25 MPH

85th Percentile Vehicles: 8

Max Speed: 59 MPH on 9/6/2024 7:56:05 PM

Total Vehicles: 10

AADT: 1

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	2	2
AM Peak 9:00 AM	2	2
PM Peak 7:00 PM	1	1

Speed

Speed Limit: 65

85th Percentile Speed: 25

50th Percentile Speed: 21

10 MPH Pace Interval: 15.0 MPH to 25.0 MPH

Average Speed: 24.5

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	0	0	0	N/A	N/A
% over limit	0.0	0.0	0.0	0.0	0.0	N/A	N/A
Avg Speeder	0.0	0.0	0.0	0.0	0.0	N/A	N/A
Avg Speed	22.0	21.0	15.5	22.0	31.8	N/A	N/A

Class Counts

	Number	%
VEH_SM	3	30
VEH_MED	7	70
VEH_LG	0	0
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Incoming Summary
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/4/2024 8:00:00 PM	17.0	2	2	17	0.0	0.0%	15.5
9/4/2024 9:00:00 PM	**No Data**						
9/4/2024 10:00:00 PM	**No Data**						
						
9/5/2024 11:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
9/5/2024 12:00:00 PM	19.0	1	1	19	0.0	0.0%	19.0
9/5/2024 1:00:00 PM	**No Data**						
9/5/2024 2:00:00 PM	**No Data**						
9/6/2024 9:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
9/6/2024 10:00:00 AM	23.0	2	2	23	0.0	0.0%	21.5
9/6/2024 11:00:00 AM	**No Data**						
9/6/2024 12:00:00 PM	**No Data**						
9/6/2024 8:00:00 PM	59.0	1	1	59	0.0	0.0%	59.0
9/6/2024 9:00:00 PM	**No Data**						
9/6/2024 10:00:00 PM	**No Data**						
9/9/2024 11:00:00 AM	22.0	1	1	22	0.0	0.0%	22.0
9/9/2024 12:00:00 PM	**No Data**						
9/9/2024 1:00:00 PM	**No Data**						
9/10/2024 11:00:00 AM	21.0	1	1	21	0.0	0.0%	21.0

Incoming Summary
Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/5/2024 12:00:00 AM	17.0	2	2	17	0.0	0.0%	15.5
9/6/2024 12:00:00 AM	25.0	2	2	25	0.0	0.0%	22.0
9/7/2024 12:00:00 AM	25.0	3	4	59	0.0	0.0%	31.8
9/8/2024 12:00:00 AM	**No Data**						
9/9/2024 12:00:00 AM	**No Data**						
9/10/2024 12:00:00 AM	22.0	1	1	22	0.0	0.0%	22.0
9/10/2024 10:59:59 AM	21.0	1	1	21	0.0	0.0%	21.0

Incoming Weekly Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	0	0	0	0	0	0	0
1 - 2	*	*	*	0	0	0	0	0	0	0
2 - 3	*	*	*	0	0	0	0	0	0	0
3 - 4	*	*	*	0	0	0	0	0	0	0
4 - 5	*	*	*	0	0	0	0	0	0	0
5 - 6	*	*	*	0	0	0	0	0	0	0
6 - 7	*	*	*	0	0	0	0	0	0	0
7 - 8	*	*	*	0	0	0	0	0	0	0
8 - 9	*	*	*	0	1	0	0	0.5	0	25
9 - 10	*	*	*	0	2	0	0	1	0	23
10 - 11	*	*	*	1	0	0	0	0.5	0	25
11 - 12	*	*	*	1	0	0	0	0.5	0	19
12 - 13	*	*	*	0	0	0	0	0	0	0
13 - 14	*	*	*	0	0	0	0	0	0	0
14 - 15	*	*	*	0	0	0	0	0	0	0
15 - 16	*	*	*	0	0	0	0	0	0	0
16 - 17	*	*	*	0	0	0	0	0	0	0
17 - 18	*	*	*	0	0	0	0	0	0	0
18 - 19	*	*	*	0	0	0	0	0	0	0
19 - 20	*	*	2	0	1	0	0	1	0	38
20 - 21	*	*	0	0	0	0	0	0	0	0
21 - 22	*	*	0	0	0	0	0	0	0	0
22 - 23	*	*	0	0	0	0	0	0	0	0
23 - 24	*	*	0	0	0	0	0	0	0	0
Totals	0	0	2	2	4	0	0			
% of Total	0%	0%	25%	25%	50%	0%	0%			

Incoming Weekly Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	0	0	*	*	*	*	*	0	0	0
1 - 2	0	0	*	*	*	*	*	0	0	0
2 - 3	0	0	*	*	*	*	*	0	0	0
3 - 4	0	0	*	*	*	*	*	0	0	0
4 - 5	0	0	*	*	*	*	*	0	0	0
5 - 6	0	0	*	*	*	*	*	0	0	0
6 - 7	0	0	*	*	*	*	*	0	0	0
7 - 8	0	0	*	*	*	*	*	0	0	0
8 - 9	0	0	*	*	*	*	*	0	0	0
9 - 10	0	0	*	*	*	*	*	0	0	0
10 - 11	1	1	*	*	*	*	*	1	0	21.5
11 - 12	0	*	*	*	*	*	*	0	0	0
12 - 13	0	*	*	*	*	*	*	0	0	0
13 - 14	0	*	*	*	*	*	*	0	0	0
14 - 15	0	*	*	*	*	*	*	0	0	0
15 - 16	0	*	*	*	*	*	*	0	0	0
16 - 17	0	*	*	*	*	*	*	0	0	0
17 - 18	0	*	*	*	*	*	*	0	0	0
18 - 19	0	*	*	*	*	*	*	0	0	0
19 - 20	0	*	*	*	*	*	*	0	0	0
20 - 21	0	*	*	*	*	*	*	0	0	0
21 - 22	0	*	*	*	*	*	*	0	0	0
22 - 23	0	*	*	*	*	*	*	0	0	0
23 - 24	0	*	*	*	*	*	*	0	0	0
Totals	1	1	0	0	0	0	0			
% of Total	50%	50%	0%	0%	0%	0%	0%			

Incoming Monthly Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	0	0	*	0	0	0	0	0	0	0
1 - 2	0	0	*	0	0	0	0	0	0	0
2 - 3	0	0	*	0	0	0	0	0	0	0
3 - 4	0	0	*	0	0	0	0	0	0	0
4 - 5	0	0	*	0	0	0	0	0	0	0
5 - 6	0	0	*	0	0	0	0	0	0	0
6 - 7	0	0	*	0	0	0	0	0	0	0
7 - 8	0	0	*	0	0	0	0	0	0	0
8 - 9	0	0	*	0	1	0	0	0.25	0	25
9 - 10	0	0	*	0	2	0	0	0.5	0	23
10 - 11	1	1	*	1	0	0	0	0.75	0	22.67
11 - 12	0	*	*	1	0	0	0	0.33	0	19
12 - 13	0	*	*	0	0	0	0	0	0	0
13 - 14	0	*	*	0	0	0	0	0	0	0
14 - 15	0	*	*	0	0	0	0	0	0	0
15 - 16	0	*	*	0	0	0	0	0	0	0
16 - 17	0	*	*	0	0	0	0	0	0	0
17 - 18	0	*	*	0	0	0	0	0	0	0
18 - 19	0	*	*	0	0	0	0	0	0	0
19 - 20	0	*	2	0	1	0	0	0.75	0	38
20 - 21	0	*	0	0	0	0	0	0	0	0
21 - 22	0	*	0	0	0	0	0	0	0	0
22 - 23	0	*	0	0	0	0	0	0	0	0
23 - 24	0	*	0	0	0	0	0	0	0	0
Totals	1	1	2	2	4	0	0			
% of Total	10%	10%	20%	20%	40%	0%	0%			

Incoming Weekly Speeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	0	0	0	0	0	0	0
1 - 2	*	*	*	0	0	0	0	0	0	0
2 - 3	*	*	*	0	0	0	0	0	0	0
3 - 4	*	*	*	0	0	0	0	0	0	0
4 - 5	*	*	*	0	0	0	0	0	0	0
5 - 6	*	*	*	0	0	0	0	0	0	0
6 - 7	*	*	*	0	0	0	0	0	0	0
7 - 8	*	*	*	0	0	0	0	0	0	0
8 - 9	*	*	*	0	25	0	0	25	0	25
9 - 10	*	*	*	0	21.5	0	0	21.5	0	23
10 - 11	*	*	*	25	0	0	0	25	0	25
11 - 12	*	*	*	19	0	0	0	19	0	19
12 - 13	*	*	*	0	0	0	0	0	0	0
13 - 14	*	*	*	0	0	0	0	0	0	0
14 - 15	*	*	*	0	0	0	0	0	0	0
15 - 16	*	*	*	0	0	0	0	0	0	0
16 - 17	*	*	*	0	0	0	0	0	0	0
17 - 18	*	*	*	0	0	0	0	0	0	0
18 - 19	*	*	*	0	0	0	0	0	0	0
19 - 20	*	*	15.5	0	59	0	0	30	0	38
20 - 21	*	*	0	0	0	0	0	0	0	0
21 - 22	*	*	0	0	0	0	0	0	0	0
22 - 23	*	*	0	0	0	0	0	0	0	0
23 - 24	*	*	0	0	0	0	0	0	0	0
Totals	0	0	15.5	22	31.8	0	0			
% of Total	0%	0%	22.37%	31.75%	45.89%	0%	0%			

Incoming Weekly Speeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	0	0	*	*	*	*	*	*	*	0
1 - 2	0	0	*	*	*	*	*	*	*	0
2 - 3	0	0	*	*	*	*	*	*	*	0
3 - 4	0	0	*	*	*	*	*	*	*	0
4 - 5	0	0	*	*	*	*	*	*	*	0
5 - 6	0	0	*	*	*	*	*	*	*	0
6 - 7	0	0	*	*	*	*	*	*	*	0
7 - 8	0	0	*	*	*	*	*	*	*	0
8 - 9	0	0	*	*	*	*	*	*	*	0
9 - 10	0	0	*	*	*	*	*	*	*	0
10 - 11	22	21	*	*	*	*	*	21.5	*	21.5
11 - 12	0	*	*	*	*	*	*	*	*	0
12 - 13	0	*	*	*	*	*	*	*	*	0
13 - 14	0	*	*	*	*	*	*	*	*	0
14 - 15	0	*	*	*	*	*	*	*	*	0
15 - 16	0	*	*	*	*	*	*	*	*	0
16 - 17	0	*	*	*	*	*	*	*	*	0
17 - 18	0	*	*	*	*	*	*	*	*	0
18 - 19	0	*	*	*	*	*	*	*	*	0
19 - 20	0	*	*	*	*	*	*	*	*	0
20 - 21	0	*	*	*	*	*	*	*	*	0
21 - 22	0	*	*	*	*	*	*	*	*	0
22 - 23	0	*	*	*	*	*	*	*	*	0
23 - 24	0	*	*	*	*	*	*	*	*	0
Totals	22	21	0	0	0	0	0			
% of Total	51.16%	48.84%	0%	0%	0%	0%	0%			

Incoming Monthly Speeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	0	0	*	0	0	0	0	0	0	0
1 - 2	0	0	*	0	0	0	0	0	0	0
2 - 3	0	0	*	0	0	0	0	0	0	0
3 - 4	0	0	*	0	0	0	0	0	0	0
4 - 5	0	0	*	0	0	0	0	0	0	0
5 - 6	0	0	*	0	0	0	0	0	0	0
6 - 7	0	0	*	0	0	0	0	0	0	0
7 - 8	0	0	*	0	0	0	0	0	0	0
8 - 9	0	0	*	0	25	0	0	25	0	25
9 - 10	0	0	*	0	21.5	0	0	21.5	0	23
10 - 11	22	21	*	25	0	0	0	22.67	0	22.67
11 - 12	0	*	*	19	0	0	0	19	0	19
12 - 13	0	*	*	0	0	0	0	0	0	0
13 - 14	0	*	*	0	0	0	0	0	0	0
14 - 15	0	*	*	0	0	0	0	0	0	0
15 - 16	0	*	*	0	0	0	0	0	0	0
16 - 17	0	*	*	0	0	0	0	0	0	0
17 - 18	0	*	*	0	0	0	0	0	0	0
18 - 19	0	*	*	0	0	0	0	0	0	0
19 - 20	0	*	15.5	0	59	0	0	30	0	38
20 - 21	0	*	0	0	0	0	0	0	0	0
21 - 22	0	*	0	0	0	0	0	0	0	0
22 - 23	0	*	0	0	0	0	0	0	0	0
23 - 24	0	*	0	0	0	0	0	0	0	0
Totals	22	21	15.5	22	31.8	0	0			
% of Total	19.59%	18.7%	13.8%	19.59%	28.32%	0%	0%			

Incoming Weekly EightyFifthSpeeds
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	0	0	0	0	0	0	0
1 - 2	*	*	*	0	0	0	0	0	0	0
2 - 3	*	*	*	0	0	0	0	0	0	0
3 - 4	*	*	*	0	0	0	0	0	0	0
4 - 5	*	*	*	0	0	0	0	0	0	0
5 - 6	*	*	*	0	0	0	0	0	0	0
6 - 7	*	*	*	0	0	0	0	0	0	0
7 - 8	*	*	*	0	0	0	0	0	0	0
8 - 9	*	*	*	0	25	0	0	25	0	25
9 - 10	*	*	*	0	23	0	0	23	0	23
10 - 11	*	*	*	25	0	0	0	25	0	25
11 - 12	*	*	*	19	0	0	0	19	0	19
12 - 13	*	*	*	0	0	0	0	0	0	0
13 - 14	*	*	*	0	0	0	0	0	0	0
14 - 15	*	*	*	0	0	0	0	0	0	0
15 - 16	*	*	*	0	0	0	0	0	0	0
16 - 17	*	*	*	0	0	0	0	0	0	0
17 - 18	*	*	*	0	0	0	0	0	0	0
18 - 19	*	*	*	0	0	0	0	0	0	0
19 - 20	*	*	17	0	59	0	0	38	0	38
20 - 21	*	*	0	0	0	0	0	0	0	0
21 - 22	*	*	0	0	0	0	0	0	0	0
22 - 23	*	*	0	0	0	0	0	0	0	0
23 - 24	*	*	0	0	0	0	0	0	0	0
Totals	0	0	17	44	107	0	0			
% of Total	0%	0%	10.12%	26.19%	63.69%	0%	0%			

Incoming Weekly EightyFifthSpeeds
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	0	0	*	*	*	*	*	0	0	0
1 - 2	0	0	*	*	*	*	*	0	0	0
2 - 3	0	0	*	*	*	*	*	0	0	0
3 - 4	0	0	*	*	*	*	*	0	0	0
4 - 5	0	0	*	*	*	*	*	0	0	0
5 - 6	0	0	*	*	*	*	*	0	0	0
6 - 7	0	0	*	*	*	*	*	0	0	0
7 - 8	0	0	*	*	*	*	*	0	0	0
8 - 9	0	0	*	*	*	*	*	0	0	0
9 - 10	0	0	*	*	*	*	*	0	0	0
10 - 11	22	21	*	*	*	*	*	21.5	0	21.5
11 - 12	0	*	*	*	*	*	*	0	0	0
12 - 13	0	*	*	*	*	*	*	0	0	0
13 - 14	0	*	*	*	*	*	*	0	0	0
14 - 15	0	*	*	*	*	*	*	0	0	0
15 - 16	0	*	*	*	*	*	*	0	0	0
16 - 17	0	*	*	*	*	*	*	0	0	0
17 - 18	0	*	*	*	*	*	*	0	0	0
18 - 19	0	*	*	*	*	*	*	0	0	0
19 - 20	0	*	*	*	*	*	*	0	0	0
20 - 21	0	*	*	*	*	*	*	0	0	0
21 - 22	0	*	*	*	*	*	*	0	0	0
22 - 23	0	*	*	*	*	*	*	0	0	0
23 - 24	0	*	*	*	*	*	*	0	0	0
Totals	22	21	0	0	0	0	0			
% of Total	51.16%	48.84%	0%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	Sep 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	0	0	*	0	0	0	0	0	0	0
1 - 2	0	0	*	0	0	0	0	0	0	0
2 - 3	0	0	*	0	0	0	0	0	0	0
3 - 4	0	0	*	0	0	0	0	0	0	0
4 - 5	0	0	*	0	0	0	0	0	0	0
5 - 6	0	0	*	0	0	0	0	0	0	0
6 - 7	0	0	*	0	0	0	0	0	0	0
7 - 8	0	0	*	0	0	0	0	0	0	0
8 - 9	0	0	*	0	25	0	0	25	0	25
9 - 10	0	0	*	0	23	0	0	23	0	23
10 - 11	22	21	*	25	0	0	0	22.67	0	22.67
11 - 12	0	*	*	19	0	0	0	19	0	19
12 - 13	0	*	*	0	0	0	0	0	0	0
13 - 14	0	*	*	0	0	0	0	0	0	0
14 - 15	0	*	*	0	0	0	0	0	0	0
15 - 16	0	*	*	0	0	0	0	0	0	0
16 - 17	0	*	*	0	0	0	0	0	0	0
17 - 18	0	*	*	0	0	0	0	0	0	0
18 - 19	0	*	*	0	0	0	0	0	0	0
19 - 20	0	*	17	0	59	0	0	38	0	38
20 - 21	0	*	0	0	0	0	0	0	0	0
21 - 22	0	*	0	0	0	0	0	0	0	0
22 - 23	0	*	0	0	0	0	0	0	0	0
23 - 24	0	*	0	0	0	0	0	0	0	0

Summary of Violators

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	0	N/A	N/A	N/A
01:00:00	0	N/A	N/A	N/A
02:00:00	0	N/A	N/A	N/A
03:00:00	0	N/A	N/A	N/A
04:00:00	0	N/A	N/A	N/A
05:00:00	0	N/A	N/A	N/A
06:00:00	0	N/A	N/A	N/A
07:00:00	0	N/A	N/A	N/A
08:00:00	1	25.0	0	0.0
09:00:00	2	21.5	0	0.0
10:00:00	3	22.7	0	0.0
11:00:00	1	19.0	0	0.0
12:00:00	0	N/A	N/A	N/A
13:00:00	0	N/A	N/A	N/A
14:00:00	0	N/A	N/A	N/A
15:00:00	0	N/A	N/A	N/A
16:00:00	0	N/A	N/A	N/A
17:00:00	0	N/A	N/A	N/A
18:00:00	0	N/A	N/A	N/A
19:00:00	3	30.0	0	0.0
20:00:00	0	N/A	N/A	N/A
21:00:00	0	N/A	N/A	N/A
22:00:00	0	N/A	N/A	N/A
23:00:00	0	N/A	N/A	N/A

Incoming Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	19:00	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	15.5	17	7 to 17	100.0	*	*	1	1	0
9/4/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	15.5	17	7 to 17	100.0	*	*	1	1	0

Incoming Histogram
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/5/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	10:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0
9/5/2024	11:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	19	19	9 to 19	100.0	*	*	1	0	0
9/5/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	22	25	15 to 25	100.0	*	*	1	1	0	

Incoming Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/6/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	08:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0
9/6/2024	09:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	21.5	23	13 to 23	100.0	*	*	1	1	0
9/6/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	19:00	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	59	59	49 to 59	100.0	*	*	0	1	0
9/6/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	2	1	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	4	31.8	25	15 to 25	75.0	*	*	1	3	0

Incoming Histogram
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/7/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	*	*	*	*	0	0	0

Incoming Histogram
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/8/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	*	*	*	*	0	0	0

Incoming Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/9/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	10:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	22	22	12 to 22	100.0	*	*	0	1	0
9/9/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	22	22	12 to 22	100.0	*	*	0	1	0

Incoming Histogram

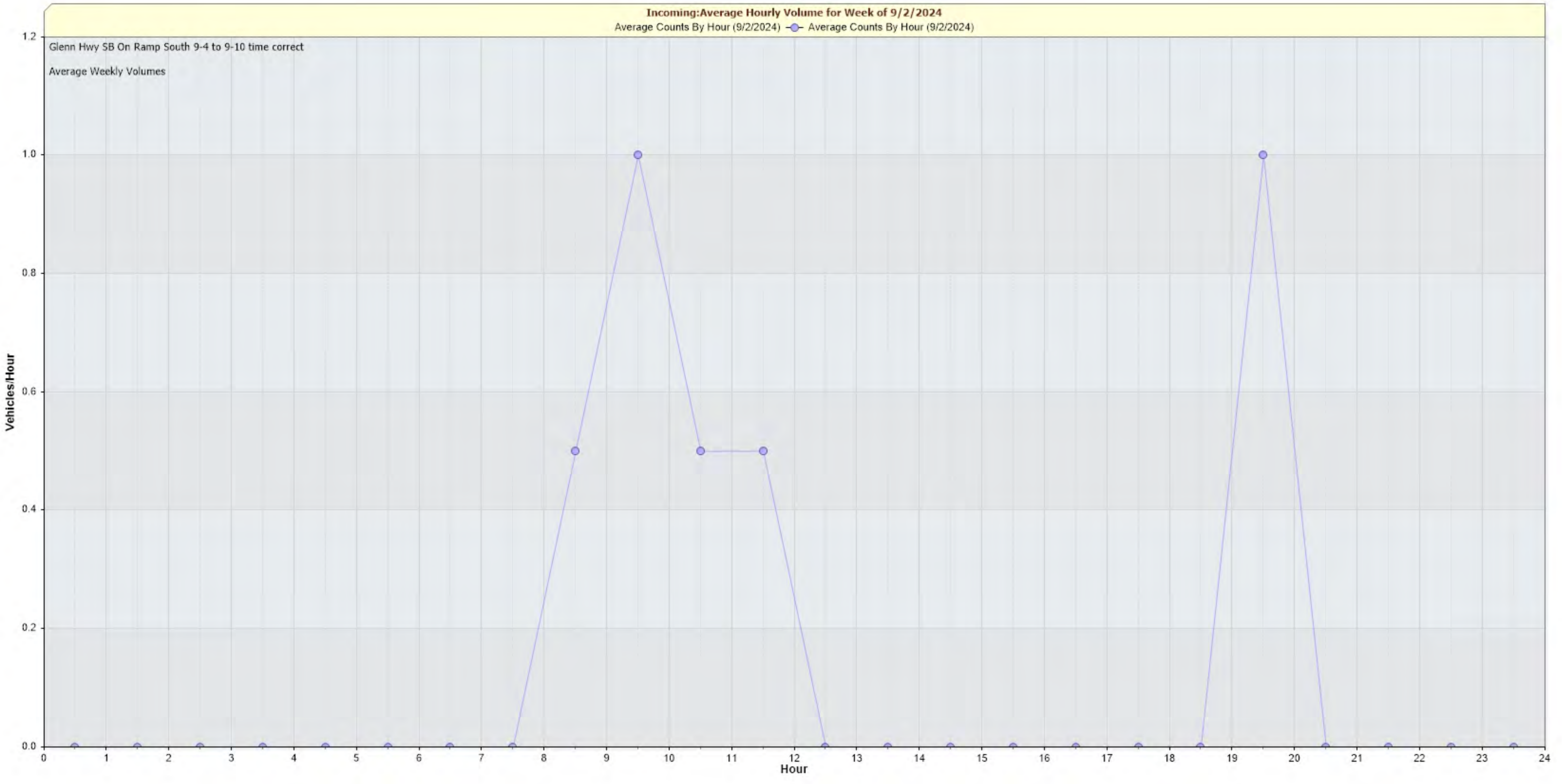
Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	10:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	21	21	11 to 21	100.0	*	*	0	1	0
9/10/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	21	21	11 to 21	100.0	*	*	0	1	0

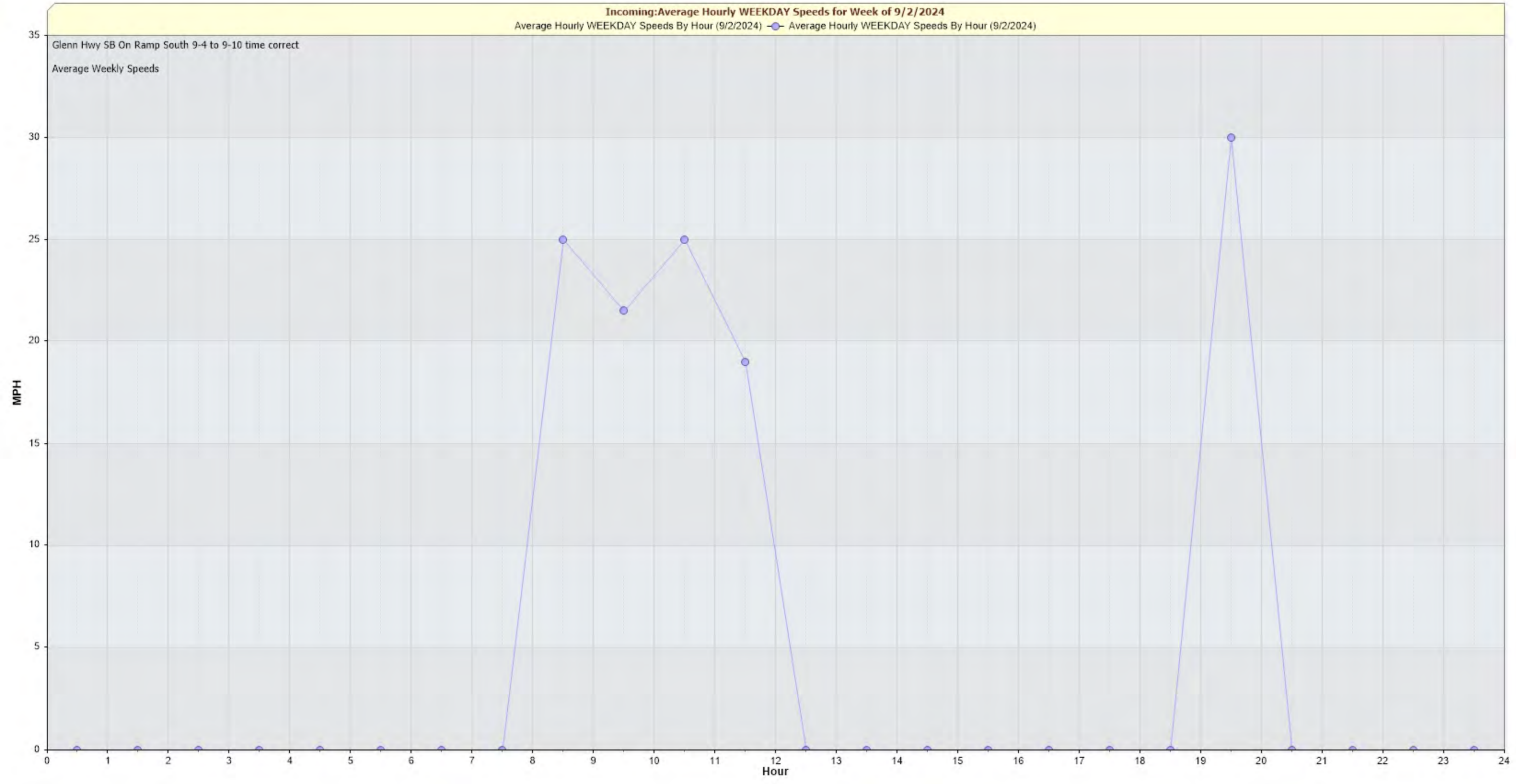
Incoming:Average Hourly Volume for Week of 9/2/2024
Average Counts By Hour (9/2/2024) ● Average Counts By Hour (9/2/2024)

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct
Average Weekly Volumes



Incoming: Average Hourly WEEKDAY Speeds for Week of 9/2/2024
Average Hourly WEEKDAY Speeds By Hour (9/2/2024) — Average Hourly WEEKDAY Speeds By Hour (9/2/2024)

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct
Average Weekly Speeds

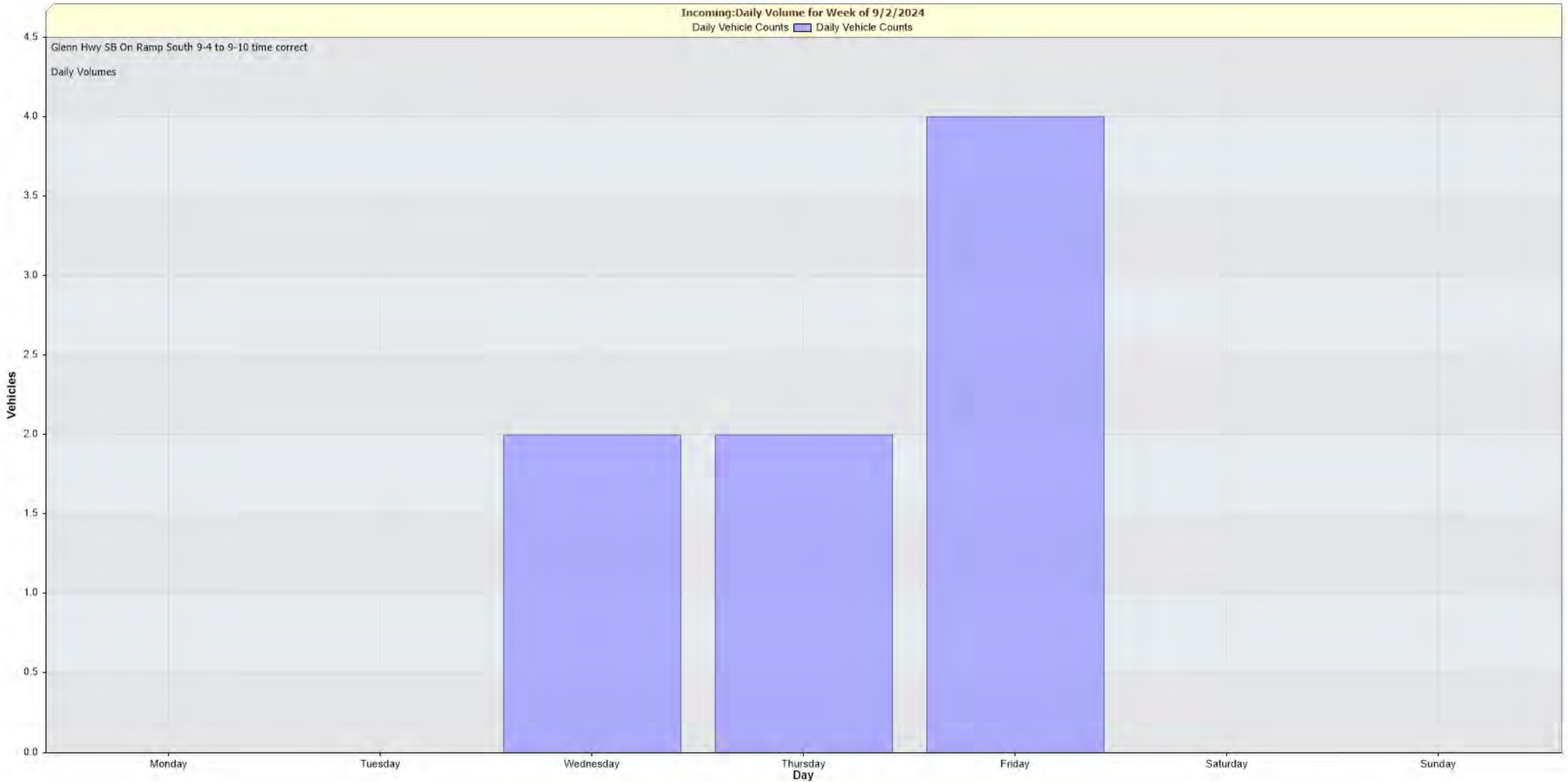


Incoming:Daily Volume for Week of 9/2/2024

Daily Vehicle Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

Daily Volumes



For Project: Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:43:14 PM

Speed Intervals: 1 MPH

Time Intervals: Instant

Traffic Report From: 9/4/2024 7:00:00 PM through 9/10/2024 1:59:59 PM

85th Percentile Speed: 37 MPH

85th Percentile Vehicles: 7862

Max Speed: 50 MPH on 9/6/2024 4:04:36 PM

Total Vehicles: 9249

AADT: 1596

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1322	1321
AM Peak 7:00 AM	215	152
PM Peak 2:00 PM	149	130

Speed

Speed Limit: 65

85th Percentile Speed: 37

50th Percentile Speed: 33

10 MPH Pace Interval: 28.0 MPH to 38.0 MPH

Average Speed: 33.01

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	0	0	0	0	0
% over limit	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speeder	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speed	32.7	33.0	32.8	33.2	33.1	33.2	32.9

Class Counts

	Number	%
VEH_SM	51	0.6
VEH_MED	9198	99.4
VEH_LG	0	0
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/4/2024 8:00:00 PM	37.0	60	70	48	0.0	0.0%	33.4
9/4/2024 9:00:00 PM	36.0	62	73	41	0.0	0.0%	32.9
9/4/2024 10:00:00 PM	36.0	27	32	41	0.0	0.0%	31.9
9/4/2024 11:00:00 PM	36.0	11	13	38	0.0	0.0%	32.3
9/5/2024 12:00:00 AM	36.0	10	12	38	0.0	0.0%	32.2
9/5/2024 1:00:00 AM	36.0	3	4	37	0.0	0.0%	35.3
9/5/2024 2:00:00 AM	34.0	3	4	35	0.0	0.0%	31.5
9/5/2024 3:00:00 AM	**No Data**						
9/5/2024 4:00:00 AM	38.0	5	6	46	0.0	0.0%	34.0
9/5/2024 5:00:00 AM	37.0	11	13	43	0.0	0.0%	32.9
9/5/2024 6:00:00 AM	38.0	51	60	49	0.0	0.0%	34.1
9/5/2024 7:00:00 AM	38.0	86	101	46	0.0	0.0%	34.1
9/5/2024 8:00:00 AM	37.0	199	234	45	0.0	0.0%	33.6
9/5/2024 9:00:00 AM	36.0	144	170	44	0.0	0.0%	33.3
9/5/2024 10:00:00 AM	37.0	90	106	43	0.0	0.0%	32.8
9/5/2024 11:00:00 AM	37.0	68	80	43	0.0	0.0%	32.8
9/5/2024 12:00:00 PM	37.0	89	105	42	0.0	0.0%	33.0
9/5/2024 1:00:00 PM	36.0	68	80	44	0.0	0.0%	33.0
9/5/2024 2:00:00 PM	37.0	70	82	43	0.0	0.0%	32.9
9/5/2024 3:00:00 PM	37.0	124	146	44	0.0	0.0%	32.8
9/5/2024 4:00:00 PM	37.0	103	121	44	0.0	0.0%	33.1
9/5/2024 5:00:00 PM	37.0	97	114	44	0.0	0.0%	33.3
9/5/2024 6:00:00 PM	37.0	88	104	43	0.0	0.0%	33.6
9/5/2024 7:00:00 PM	37.0	56	66	42	0.0	0.0%	32.3
9/5/2024 8:00:00 PM	37.0	61	72	45	0.0	0.0%	33.1
9/5/2024 9:00:00 PM	37.0	48	56	42	0.0	0.0%	33.4
9/5/2024 10:00:00 PM	36.0	36	42	39	0.0	0.0%	31.8
9/5/2024 11:00:00 PM	33.0	15	18	38	0.0	0.0%	31.2
9/6/2024 12:00:00 AM	36.0	3	4	37	0.0	0.0%	34.0
9/6/2024 1:00:00 AM	36.0	8	9	39	0.0	0.0%	32.0
9/6/2024 2:00:00 AM	30.0	4	5	35	0.0	0.0%	29.2
9/6/2024 3:00:00 AM	37.0	2	2	37	0.0	0.0%	36.0
9/6/2024 4:00:00 AM	35.0	5	6	36	0.0	0.0%	32.7
9/6/2024 5:00:00 AM	37.0	11	13	41	0.0	0.0%	33.2
9/6/2024 6:00:00 AM	37.0	31	37	43	0.0	0.0%	33.1
9/6/2024 7:00:00 AM	37.0	89	105	44	0.0	0.0%	33.3
9/6/2024 8:00:00 AM	37.0	190	224	42	0.0	0.0%	33.4
9/6/2024 9:00:00 AM	36.0	126	148	44	0.0	0.0%	32.3
9/6/2024 10:00:00 AM	36.0	82	96	44	0.0	0.0%	32.2
9/6/2024 11:00:00 AM	37.0	69	81	44	0.0	0.0%	32.8
9/6/2024 12:00:00 PM	37.0	83	98	45	0.0	0.0%	33.1
9/6/2024 1:00:00 PM	37.0	86	101	46	0.0	0.0%	32.6
9/6/2024 2:00:00 PM	37.0	110	130	46	0.0	0.0%	33.0
9/6/2024 3:00:00 PM	38.0	134	158	45	0.0	0.0%	33.8
9/6/2024 4:00:00 PM	37.0	109	128	44	0.0	0.0%	33.1
9/6/2024 5:00:00 PM	37.0	94	110	50	0.0	0.0%	33.6
9/6/2024 6:00:00 PM	37.0	109	128	48	0.0	0.0%	33.7

Outgoing Summary
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

9/6/2024 7:00:00 PM	37.0	85	100	49	0.0	0.0%	32.9
9/6/2024 8:00:00 PM	37.0	63	74	47	0.0	0.0%	33.7
9/6/2024 9:00:00 PM	37.0	52	61	43	0.0	0.0%	33.8
9/6/2024 10:00:00 PM	36.0	38	45	39	0.0	0.0%	32.2
9/6/2024 11:00:00 PM	34.0	27	32	41	0.0	0.0%	30.0
9/7/2024 12:00:00 AM	37.0	11	13	39	0.0	0.0%	30.5
9/7/2024 1:00:00 AM	34.0	7	8	47	0.0	0.0%	31.5
9/7/2024 2:00:00 AM	33.0	8	9	36	0.0	0.0%	27.3
9/7/2024 3:00:00 AM	33.0	5	6	33	0.0	0.0%	29.2
9/7/2024 4:00:00 AM	32.0	3	4	34	0.0	0.0%	28.5
9/7/2024 5:00:00 AM	32.0	4	5	37	0.0	0.0%	32.0
9/7/2024 6:00:00 AM	36.0	13	15	38	0.0	0.0%	31.1
9/7/2024 7:00:00 AM	34.0	21	25	37	0.0	0.0%	30.7
9/7/2024 8:00:00 AM	37.0	29	34	43	0.0	0.0%	33.3
9/7/2024 9:00:00 AM	38.0	47	55	44	0.0	0.0%	33.4
9/7/2024 10:00:00 AM	36.0	74	87	40	0.0	0.0%	31.9
9/7/2024 11:00:00 AM	38.0	95	112	44	0.0	0.0%	33.5
9/7/2024 12:00:00 PM	37.0	91	107	45	0.0	0.0%	33.3
9/7/2024 1:00:00 PM	37.0	98	115	42	0.0	0.0%	33.2
9/7/2024 2:00:00 PM	36.0	88	104	44	0.0	0.0%	32.4
9/7/2024 3:00:00 PM	37.0	97	114	44	0.0	0.0%	33.5
9/7/2024 4:00:00 PM	37.0	84	99	44	0.0	0.0%	34.0
9/7/2024 5:00:00 PM	37.0	82	96	45	0.0	0.0%	33.5
9/7/2024 6:00:00 PM	37.0	87	102	44	0.0	0.0%	34.1
9/7/2024 7:00:00 PM	39.0	65	77	46	0.0	0.0%	34.2
9/7/2024 8:00:00 PM	37.0	60	71	43	0.0	0.0%	33.6
9/7/2024 9:00:00 PM	37.0	51	60	45	0.0	0.0%	33.3
9/7/2024 10:00:00 PM	38.0	35	41	47	0.0	0.0%	34.1
9/7/2024 11:00:00 PM	36.0	20	24	42	0.0	0.0%	32.2
9/8/2024 12:00:00 AM	33.0	8	9	33	0.0	0.0%	29.6
9/8/2024 1:00:00 AM	36.0	10	12	41	0.0	0.0%	32.7
9/8/2024 2:00:00 AM	34.0	4	5	36	0.0	0.0%	33.4
9/8/2024 3:00:00 AM	36.0	2	2	36	0.0	0.0%	34.5
9/8/2024 4:00:00 AM	35.0	4	5	36	0.0	0.0%	31.8
9/8/2024 5:00:00 AM	36.0	2	2	36	0.0	0.0%	34.0
9/8/2024 6:00:00 AM	33.0	7	8	36	0.0	0.0%	30.4
9/8/2024 7:00:00 AM	36.0	14	16	37	0.0	0.0%	31.8
9/8/2024 8:00:00 AM	39.0	20	23	43	0.0	0.0%	34.4
9/8/2024 9:00:00 AM	37.0	57	67	42	0.0	0.0%	32.9
9/8/2024 10:00:00 AM	36.0	54	64	42	0.0	0.0%	32.9
9/8/2024 11:00:00 AM	37.0	81	95	49	0.0	0.0%	33.0
9/8/2024 12:00:00 PM	36.0	75	88	40	0.0	0.0%	32.7
9/8/2024 1:00:00 PM	36.0	144	169	42	0.0	0.0%	32.7
9/8/2024 2:00:00 PM	36.0	78	92	46	0.0	0.0%	32.5
9/8/2024 3:00:00 PM	37.0	80	94	44	0.0	0.0%	33.0
9/8/2024 4:00:00 PM	36.0	74	87	44	0.0	0.0%	32.6
9/8/2024 5:00:00 PM	37.0	86	101	42	0.0	0.0%	33.5
9/8/2024 6:00:00 PM	37.0	83	98	42	0.0	0.0%	33.2

Outgoing Summary
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

9/8/2024 7:00:00 PM	37.0	65	76	46	0.0	0.0%	33.2
9/8/2024 8:00:00 PM	36.0	38	45	43	0.0	0.0%	33.3
9/8/2024 9:00:00 PM	37.0	48	56	41	0.0	0.0%	33.9
9/8/2024 10:00:00 PM	35.0	24	28	49	0.0	0.0%	31.8
9/8/2024 11:00:00 PM	36.0	14	16	37	0.0	0.0%	32.4
9/9/2024 12:00:00 AM	36.0	7	8	38	0.0	0.0%	31.8
9/9/2024 1:00:00 AM	39.0	5	6	42	0.0	0.0%	36.0
9/9/2024 2:00:00 AM	34.0	4	5	36	0.0	0.0%	31.6
9/9/2024 3:00:00 AM	34.0	8	10	45	0.0	0.0%	32.2
9/9/2024 4:00:00 AM	34.0	7	8	35	0.0	0.0%	32.5
9/9/2024 5:00:00 AM	37.0	8	10	39	0.0	0.0%	32.8
9/9/2024 6:00:00 AM	38.0	37	43	43	0.0	0.0%	33.1
9/9/2024 7:00:00 AM	38.0	86	101	45	0.0	0.0%	33.3
9/9/2024 8:00:00 AM	37.0	138	162	46	0.0	0.0%	33.3
9/9/2024 9:00:00 AM	36.0	131	154	43	0.0	0.0%	32.2
9/9/2024 10:00:00 AM	36.0	110	130	39	0.0	0.0%	32.4
9/9/2024 11:00:00 AM	35.0	79	93	42	0.0	0.0%	31.5
9/9/2024 12:00:00 PM	36.0	75	88	40	0.0	0.0%	31.8
9/9/2024 1:00:00 PM	36.0	72	85	48	0.0	0.0%	32.5
9/9/2024 2:00:00 PM	36.0	85	100	45	0.0	0.0%	32.6
9/9/2024 3:00:00 PM	36.0	122	144	45	0.0	0.0%	32.7
9/9/2024 4:00:00 PM	36.0	104	122	43	0.0	0.0%	32.9
9/9/2024 5:00:00 PM	36.0	67	79	42	0.0	0.0%	32.2
9/9/2024 6:00:00 PM	37.0	72	85	43	0.0	0.0%	33.7
9/9/2024 7:00:00 PM	37.0	60	70	42	0.0	0.0%	33.7
9/9/2024 8:00:00 PM	36.0	41	48	44	0.0	0.0%	32.8
9/9/2024 9:00:00 PM	36.0	42	49	41	0.0	0.0%	32.8
9/9/2024 10:00:00 PM	36.0	20	24	40	0.0	0.0%	33.0
9/9/2024 11:00:00 PM	34.0	10	12	36	0.0	0.0%	31.7
9/10/2024 12:00:00 AM	34.0	7	8	36	0.0	0.0%	31.3
9/10/2024 1:00:00 AM	29.0	2	2	29	0.0	0.0%	28.5
9/10/2024 2:00:00 AM	46.0	3	3	46	0.0	0.0%	37.7
9/10/2024 3:00:00 AM	32.0	1	1	32	0.0	0.0%	32.0
9/10/2024 4:00:00 AM	35.0	5	6	38	0.0	0.0%	33.5
9/10/2024 5:00:00 AM	35.0	8	10	44	0.0	0.0%	32.1
9/10/2024 6:00:00 AM	37.0	38	45	43	0.0	0.0%	33.4
9/10/2024 7:00:00 AM	37.0	96	113	41	0.0	0.0%	33.2
9/10/2024 8:00:00 AM	37.0	204	240	46	0.0	0.0%	33.7
9/10/2024 9:00:00 AM	37.0	163	192	46	0.0	0.0%	33.2
9/10/2024 10:00:00 AM	36.0	90	106	48	0.0	0.0%	32.9
9/10/2024 11:00:00 AM	37.0	79	93	43	0.0	0.0%	32.5
9/10/2024 12:00:00 PM	37.0	76	90	40	0.0	0.0%	32.8
9/10/2024 1:00:00 PM	37.0	76	90	43	0.0	0.0%	32.8
9/10/2024 2:00:00 PM	36.0	80	94	43	0.0	0.0%	31.9

Outgoing Summary
Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/5/2024 12:00:00 AM	36.0	170	200	48	0.0	0.0%	32.8
9/6/2024 12:00:00 AM	37.0	1520	1788	49	0.0	0.0%	33.2
9/7/2024 12:00:00 AM	37.0	1618	1904	50	0.0	0.0%	33.1
9/8/2024 12:00:00 AM	37.0	1172	1379	47	0.0	0.0%	33.2
9/9/2024 12:00:00 AM	36.0	1068	1257	49	0.0	0.0%	32.9
9/10/2024 12:00:00 AM	36.0	1391	1636	48	0.0	0.0%	32.7
9/10/2024 1:59:59 PM	37.0	922	1085	48	0.0	0.0%	33.0

Outgoing Weekly Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	4	9	8	12	6.5	10	36
1 - 2	*	*	*	4	5	9	5	4.5	7	32
2 - 3	*	*	*	0	2	6	2	1	4	37
3 - 4	*	*	*	6	6	4	5	6	4.5	36.5
4 - 5	*	*	*	13	13	5	2	13	3.5	37
5 - 6	*	*	*	60	37	15	8	48.5	11.5	37.05
6 - 7	*	*	*	101	105	25	16	103	20.5	37.15
7 - 8	*	*	*	234	224	34	23	229	28.5	36.6
8 - 9	*	*	*	170	148	55	67	159	61	35.9
9 - 10	*	*	*	106	96	87	64	101	75.5	36.05
10 - 11	*	*	*	80	81	112	95	80.5	103.5	36.55
11 - 12	*	*	*	105	98	107	88	101.5	97.5	36.5
12 - 13	*	*	*	80	101	115	169	90.5	142	36.1
13 - 14	*	*	*	82	130	104	92	106	98	36.4
14 - 15	*	*	*	146	158	114	94	152	104	36.75
15 - 16	*	*	*	121	128	99	87	124.5	93	36.4
16 - 17	*	*	*	114	110	96	101	112	98.5	36.6
17 - 18	*	*	*	104	128	102	98	116	100	36.65
18 - 19	*	*	*	66	100	77	76	83	76.5	36.45
19 - 20	*	*	70	72	74	71	45	72	58	36.73
20 - 21	*	*	73	56	61	60	56	63.33	58	36.4
21 - 22	*	*	32	42	45	41	28	39.67	34.5	35.57
22 - 23	*	*	13	18	32	24	16	21	20	34.23
23 - 24	*	*	12	4	13	9	8	9.67	8.5	36.17
Totals	0	0	200	1788	1904	1379	1257			
% of Total	0%	0%	3.06%	27.39%	29.17%	21.12%	19.26%			

Outgoing Weekly Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	6	2	*	*	*	*	*	4	0	34
1 - 2	5	3	*	*	*	*	*	4	0	40
2 - 3	10	1	*	*	*	*	*	5.5	0	33
3 - 4	8	6	*	*	*	*	*	7	0	34.5
4 - 5	10	10	*	*	*	*	*	10	0	36
5 - 6	43	45	*	*	*	*	*	44	0	36.85
6 - 7	101	113	*	*	*	*	*	107	0	36.9
7 - 8	162	240	*	*	*	*	*	201	0	36.6
8 - 9	154	192	*	*	*	*	*	173	0	36.15
9 - 10	130	106	*	*	*	*	*	118	0	35.95
10 - 11	93	93	*	*	*	*	*	93	0	35.55
11 - 12	88	90	*	*	*	*	*	89	0	36.05
12 - 13	85	90	*	*	*	*	*	87.5	0	36.35
13 - 14	100	94	*	*	*	*	*	97	0	35.2
14 - 15	144	*	*	*	*	*	*	144	0	35.8
15 - 16	122	*	*	*	*	*	*	122	0	35.7
16 - 17	79	*	*	*	*	*	*	79	0	35.4
17 - 18	85	*	*	*	*	*	*	85	0	37
18 - 19	70	*	*	*	*	*	*	70	0	36.7
19 - 20	48	*	*	*	*	*	*	48	0	36
20 - 21	49	*	*	*	*	*	*	49	0	35.9
21 - 22	24	*	*	*	*	*	*	24	0	36
22 - 23	12	*	*	*	*	*	*	12	0	33.5
23 - 24	8	*	*	*	*	*	*	8	0	34
Totals	1636	1085	0	0	0	0	0			
% of Total	60.12%	39.88%	0%	0%	0%	0%	0%			

Outgoing Monthly Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	6	2	*	4	9	8	12	5.25	10	35
1 - 2	5	3	*	4	5	9	5	4.25	7	36
2 - 3	10	1	*	0	2	6	2	3.25	4	34.33
3 - 4	8	6	*	6	6	4	5	6.5	4.5	35.5
4 - 5	10	10	*	13	13	5	2	11.5	3.5	36.5
5 - 6	43	45	*	60	37	15	8	46.25	11.5	36.95
6 - 7	101	113	*	101	105	25	16	105	20.5	37.03
7 - 8	162	240	*	234	224	34	23	215	28.5	36.6
8 - 9	154	192	*	170	148	55	67	166	61	36.02
9 - 10	130	106	*	106	96	87	64	109.5	75.5	36
10 - 11	93	93	*	80	81	112	95	86.75	103.5	36.05
11 - 12	88	90	*	105	98	107	88	95.25	97.5	36.28
12 - 13	85	90	*	80	101	115	169	89	142	36.22
13 - 14	100	94	*	82	130	104	92	101.5	98	35.8
14 - 15	144	*	*	146	158	114	94	149.33	104	36.43
15 - 16	122	*	*	121	128	99	87	123.67	93	36.17
16 - 17	79	*	*	114	110	96	101	101	98.5	36.2
17 - 18	85	*	*	104	128	102	98	105.67	100	36.77
18 - 19	70	*	*	66	100	77	76	78.67	76.5	36.53
19 - 20	48	*	70	72	74	71	45	66	58	36.55
20 - 21	49	*	73	56	61	60	56	59.75	58	36.28
21 - 22	24	*	32	42	45	41	28	35.75	34.5	35.67
22 - 23	12	*	13	18	32	24	16	18.75	20	34.05
23 - 24	8	*	12	4	13	9	8	9.25	8.5	35.62
Totals	1636	1085	200	1788	1904	1379	1257			
% of Total	17.69%	11.73%	2.16%	19.33%	20.59%	14.91%	13.59%			

Outgoing Weekly Speeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	35.25	32	31.5	32.67	33	32.2	36
1 - 2	*	*	*	31.5	29.2	27.33	33.4	30.22	29.5	32
2 - 3	*	*	*	0	36	29.17	34.5	36	30.5	37
3 - 4	*	*	*	34	32.67	28.5	31.8	33.33	30.33	36.5
4 - 5	*	*	*	32.92	33.15	32	34	33.04	32.57	37
5 - 6	*	*	*	34.08	33.14	31.07	30.38	33.72	30.83	37.05
6 - 7	*	*	*	34.08	33.28	30.72	31.81	33.67	31.15	37.15
7 - 8	*	*	*	33.63	33.38	33.32	34.39	33.51	33.75	36.6
8 - 9	*	*	*	33.28	32.26	33.38	32.85	32.81	33.09	35.9
9 - 10	*	*	*	32.79	32.21	31.89	32.92	32.51	32.32	36.05
10 - 11	*	*	*	32.76	32.81	33.46	33	32.79	33.25	36.55
11 - 12	*	*	*	32.97	33.11	33.34	32.73	33.04	33.06	36.5
12 - 13	*	*	*	33.03	32.63	33.23	32.69	32.81	32.9	36.1
13 - 14	*	*	*	32.94	33.04	32.37	32.53	33	32.44	36.4
14 - 15	*	*	*	32.78	33.77	33.5	33.01	33.29	33.28	36.75
15 - 16	*	*	*	33.1	33.11	34.04	32.64	33.1	33.39	36.4
16 - 17	*	*	*	33.26	33.65	33.52	33.5	33.45	33.51	36.6
17 - 18	*	*	*	33.58	33.69	34.09	33.15	33.64	33.63	36.65
18 - 19	*	*	*	32.35	32.91	34.23	33.16	32.69	33.7	36.45
19 - 20	*	*	33.4	33.07	33.72	33.59	33.29	33.4	33.47	36.73
20 - 21	*	*	32.88	33.41	33.82	33.27	33.86	33.34	33.55	36.4
21 - 22	*	*	31.94	31.79	32.18	34.12	31.82	31.97	33.19	35.57
22 - 23	*	*	32.31	31.22	30.03	32.21	32.38	30.84	32.27	34.23
23 - 24	*	*	32.17	34	30.54	29.56	31.75	31.69	30.59	36.17
Totals	0	0	32.8	33.2	33.1	33.2	32.9			
% of Total	0%	0%	19.85%	20.1%	20.04%	20.1%	19.92%			

Outgoing Weekly Speeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	36	28.5	*	*	*	*	*	34.12	*	34
1 - 2	31.6	37.67	*	*	*	*	*	33.88	*	40
2 - 3	32.2	32	*	*	*	*	*	32.18	*	33
3 - 4	32.5	33.5	*	*	*	*	*	32.93	*	34.5
4 - 5	32.8	32.1	*	*	*	*	*	32.45	*	36
5 - 6	33.07	33.38	*	*	*	*	*	33.23	*	36.85
6 - 7	33.29	33.24	*	*	*	*	*	33.26	*	36.9
7 - 8	33.27	33.7	*	*	*	*	*	33.52	*	36.6
8 - 9	32.2	33.17	*	*	*	*	*	32.74	*	36.15
9 - 10	32.4	32.87	*	*	*	*	*	32.61	*	35.95
10 - 11	31.53	32.47	*	*	*	*	*	32	*	35.55
11 - 12	31.84	32.8	*	*	*	*	*	32.33	*	36.05
12 - 13	32.47	32.76	*	*	*	*	*	32.62	*	36.35
13 - 14	32.57	31.94	*	*	*	*	*	32.26	*	35.2
14 - 15	32.69	*	*	*	*	*	*	32.69	*	35.8
15 - 16	32.87	*	*	*	*	*	*	32.87	*	35.7
16 - 17	32.15	*	*	*	*	*	*	32.15	*	35.4
17 - 18	33.67	*	*	*	*	*	*	33.67	*	37
18 - 19	33.67	*	*	*	*	*	*	33.67	*	36.7
19 - 20	32.77	*	*	*	*	*	*	32.77	*	36
20 - 21	32.78	*	*	*	*	*	*	32.78	*	35.9
21 - 22	32.96	*	*	*	*	*	*	32.96	*	36
22 - 23	31.67	*	*	*	*	*	*	31.67	*	33.5
23 - 24	31.25	*	*	*	*	*	*	31.25	*	34
Totals	32.7	33	0	0	0	0	0			
% of Total	49.77%	50.23%	0%	0%	0%	0%	0%			

Outgoing Monthly Speeds
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	36	28.5	*	35.25	32	31.5	32.67	33.43	32.2	35
1 - 2	31.6	37.67	*	31.5	29.2	27.33	33.4	31.94	29.5	36
2 - 3	32.2	32	*	0	36	29.17	34.5	32.77	30.5	34.33
3 - 4	32.5	33.5	*	34	32.67	28.5	31.8	33.12	30.33	35.5
4 - 5	32.8	32.1	*	32.92	33.15	32	34	32.78	32.57	36.5
5 - 6	33.07	33.38	*	34.08	33.14	31.07	30.38	33.49	30.83	36.95
6 - 7	33.29	33.24	*	34.08	33.28	30.72	31.81	33.46	31.15	37.03
7 - 8	33.27	33.7	*	33.63	33.38	33.32	34.39	33.52	33.75	36.6
8 - 9	32.2	33.17	*	33.28	32.26	33.38	32.85	32.77	33.09	36.02
9 - 10	32.4	32.87	*	32.79	32.21	31.89	32.92	32.57	32.32	36
10 - 11	31.53	32.47	*	32.76	32.81	33.46	33	32.37	33.25	36.05
11 - 12	31.84	32.8	*	32.97	33.11	33.34	32.73	32.71	33.06	36.28
12 - 13	32.47	32.76	*	33.03	32.63	33.23	32.69	32.71	32.9	36.22
13 - 14	32.57	31.94	*	32.94	33.04	32.37	32.53	32.65	32.44	35.8
14 - 15	32.69	*	*	32.78	33.77	33.5	33.01	33.1	33.28	36.43
15 - 16	32.87	*	*	33.1	33.11	34.04	32.64	33.03	33.39	36.17
16 - 17	32.15	*	*	33.26	33.65	33.52	33.5	33.11	33.51	36.2
17 - 18	33.67	*	*	33.58	33.69	34.09	33.15	33.65	33.63	36.77
18 - 19	33.67	*	*	32.35	32.91	34.23	33.16	32.98	33.7	36.53
19 - 20	32.77	*	33.4	33.07	33.72	33.59	33.29	33.28	33.47	36.55
20 - 21	32.78	*	32.88	33.41	33.82	33.27	33.86	33.22	33.55	36.28
21 - 22	32.96	*	31.94	31.79	32.18	34.12	31.82	32.14	33.19	35.67
22 - 23	31.67	*	32.31	31.22	30.03	32.21	32.38	30.97	32.27	34.05
23 - 24	31.25	*	32.17	34	30.54	29.56	31.75	31.59	30.59	35.62
Totals	32.7	33	32.8	33.2	33.1	33.2	32.9			
% of Total	14.16%	14.29%	14.21%	14.38%	14.34%	14.38%	14.25%			

Outgoing Weekly EightyFifthSpeeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	36	36	34	35.5	36	34.75	36
1 - 2	*	*	*	34	30	33	34	32	33.5	32
2 - 3	*	*	*	0	37	32.5	36	37	34.25	37
3 - 4	*	*	*	38	35	32	35	36.5	33.5	36.5
4 - 5	*	*	*	37	37	32	36	37	34	37
5 - 6	*	*	*	37.3	36.8	36	33	37.05	34.5	37.05
6 - 7	*	*	*	37.6	36.7	34	36	37.15	35	37.15
7 - 8	*	*	*	36.7	36.5	36.6	39	36.6	37.8	36.6
8 - 9	*	*	*	36	35.8	37.4	36.3	35.9	36.85	35.9
9 - 10	*	*	*	36.1	36	35.4	35.7	36.05	35.55	36.05
10 - 11	*	*	*	36.5	36.6	37.1	36.4	36.55	36.75	36.55
11 - 12	*	*	*	36.3	36.7	36.6	35.5	36.5	36.05	36.5
12 - 13	*	*	*	36	36.2	36.1	35.8	36.1	35.95	36.1
13 - 14	*	*	*	36.5	36.3	35.9	35.4	36.4	35.65	36.4
14 - 15	*	*	*	36.1	37.4	37	36.8	36.75	36.9	36.75
15 - 16	*	*	*	36.7	36.1	36.6	35.5	36.4	36.05	36.4
16 - 17	*	*	*	36.2	37	36.5	36.4	36.6	36.45	36.6
17 - 18	*	*	*	36.6	36.7	36.9	36.3	36.65	36.6	36.65
18 - 19	*	*	*	36.6	36.3	38.2	36.6	36.45	37.4	36.45
19 - 20	*	*	36.5	37	36.7	36.6	36	36.73	36.3	36.73
20 - 21	*	*	35.9	36.5	36.8	36.2	36.3	36.4	36.25	36.4
21 - 22	*	*	35.3	36	35.4	38	35	35.57	36.5	35.57
22 - 23	*	*	35.7	33	34	36	36	34.23	36	34.23
23 - 24	*	*	35.5	36	37	32.5	36	36.17	34.25	36.17
Totals	0	0	178.9	834.7	866	853.1	860.5			
% of Total	0%	0%	4.98%	23.23%	24.1%	23.74%	23.95%			

Outgoing Weekly EightyFifthSpeeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	39	29	*	*	*	*	*	34	0	34
1 - 2	34	46	*	*	*	*	*	40	0	40
2 - 3	34	32	*	*	*	*	*	33	0	33
3 - 4	34	35	*	*	*	*	*	34.5	0	34.5
4 - 5	37	35	*	*	*	*	*	36	0	36
5 - 6	37.2	36.5	*	*	*	*	*	36.85	0	36.85
6 - 7	37.2	36.6	*	*	*	*	*	36.9	0	36.9
7 - 8	36.4	36.8	*	*	*	*	*	36.6	0	36.6
8 - 9	35.5	36.8	*	*	*	*	*	36.15	0	36.15
9 - 10	35.9	36	*	*	*	*	*	35.95	0	35.95
10 - 11	35	36.1	*	*	*	*	*	35.55	0	35.55
11 - 12	35.4	36.7	*	*	*	*	*	36.05	0	36.05
12 - 13	35.7	37	*	*	*	*	*	36.35	0	36.35
13 - 14	35.3	35.1	*	*	*	*	*	35.2	0	35.2
14 - 15	35.8	*	*	*	*	*	*	35.8	0	35.8
15 - 16	35.7	*	*	*	*	*	*	35.7	0	35.7
16 - 17	35.4	*	*	*	*	*	*	35.4	0	35.4
17 - 18	37	*	*	*	*	*	*	37	0	37
18 - 19	36.7	*	*	*	*	*	*	36.7	0	36.7
19 - 20	36	*	*	*	*	*	*	36	0	36
20 - 21	35.9	*	*	*	*	*	*	35.9	0	35.9
21 - 22	36	*	*	*	*	*	*	36	0	36
22 - 23	33.5	*	*	*	*	*	*	33.5	0	33.5
23 - 24	34	*	*	*	*	*	*	34	0	34
Totals	857.6	504.6	0	0	0	0	0			
% of Total	62.96%	37.04%	0%	0%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	39	29	*	36	36	34	35.5	35	34.75	35
1 - 2	34	46	*	34	30	33	34	36	33.5	36
2 - 3	34	32	*	0	37	32.5	36	34.33	34.25	34.33
3 - 4	34	35	*	38	35	32	35	35.5	33.5	35.5
4 - 5	37	35	*	37	37	32	36	36.5	34	36.5
5 - 6	37.2	36.5	*	37.3	36.8	36	33	36.95	34.5	36.95
6 - 7	37.2	36.6	*	37.6	36.7	34	36	37.03	35	37.03
7 - 8	36.4	36.8	*	36.7	36.5	36.6	39	36.6	37.8	36.6
8 - 9	35.5	36.8	*	36	35.8	37.4	36.3	36.02	36.85	36.02
9 - 10	35.9	36	*	36.1	36	35.4	35.7	36	35.55	36
10 - 11	35	36.1	*	36.5	36.6	37.1	36.4	36.05	36.75	36.05
11 - 12	35.4	36.7	*	36.3	36.7	36.6	35.5	36.28	36.05	36.28
12 - 13	35.7	37	*	36	36.2	36.1	35.8	36.22	35.95	36.22
13 - 14	35.3	35.1	*	36.5	36.3	35.9	35.4	35.8	35.65	35.8
14 - 15	35.8	*	*	36.1	37.4	37	36.8	36.43	36.9	36.43
15 - 16	35.7	*	*	36.7	36.1	36.6	35.5	36.17	36.05	36.17
16 - 17	35.4	*	*	36.2	37	36.5	36.4	36.2	36.45	36.2
17 - 18	37	*	*	36.6	36.7	36.9	36.3	36.77	36.6	36.77
18 - 19	36.7	*	*	36.6	36.3	38.2	36.6	36.53	37.4	36.53
19 - 20	36	*	36.5	37	36.7	36.6	36	36.55	36.3	36.55
20 - 21	35.9	*	35.9	36.5	36.8	36.2	36.3	36.28	36.25	36.28
21 - 22	36	*	35.3	36	35.4	38	35	35.67	36.5	35.67
22 - 23	33.5	*	35.7	33	34	36	36	34.05	36	34.05
23 - 24	34	*	35.5	36	37	32.5	36	35.62	34.25	35.62

Summary of Violators

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	41	32.8	0	0.0
01:00:00	31	30.8	0	0.0
02:00:00	21	31.9	0	0.0
03:00:00	35	32.4	0	0.0
04:00:00	53	32.8	0	0.0
05:00:00	208	33.2	0	0.0
06:00:00	461	33.3	0	0.0
07:00:00	917	33.5	0	0.0
08:00:00	786	32.8	0	0.0
09:00:00	589	32.5	0	0.0
10:00:00	554	32.7	0	0.0
11:00:00	576	32.8	0	0.0
12:00:00	640	32.8	0	0.0
13:00:00	602	32.6	0	0.0
14:00:00	656	33.2	0	0.0
15:00:00	557	33.1	0	0.0
16:00:00	500	33.3	0	0.0
17:00:00	517	33.6	0	0.0
18:00:00	389	33.3	0	0.0
19:00:00	380	33.3	0	0.0
20:00:00	355	33.3	0	0.0
21:00:00	212	32.5	0	0.0
22:00:00	115	31.4	0	0.0
23:00:00	54	31.3	0	0.0

Outgoing Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	19:00	0	0	1	7	36	22	3	1	0	0	0	0	0	0	0	0	0	0	70	33.4	36.5	29 to 39	88.6	*	*	0	70	0	
9/4/2024	20:00	0	0	0	17	34	19	3	0	0	0	0	0	0	0	0	0	0	0	73	32.9	35.9	28 to 38	87.7	*	*	0	73	0	
9/4/2024	21:00	0	0	2	6	15	8	1	0	0	0	0	0	0	0	0	0	0	0	32	31.9	35.3	27 to 37	87.5	*	*	0	32	0	
9/4/2024	22:00	0	0	1	2	5	5	0	0	0	0	0	0	0	0	0	0	0	0	13	32.3	35.7	28 to 38	92.3	*	*	0	13	0	
9/4/2024	23:00	0	0	1	1	5	5	0	0	0	0	0	0	0	0	0	0	0	0	12	32.2	35.5	26 to 36	83.3	*	*	0	12	0	
24 Hr Summary		0	0	5	33	95	59	7	1	0	0	0	0	0	0	0	0	0	0	200	32.8	36	28 to 38	86.0	*	*	0	200	0	

Outgoing Histogram
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/5/2024	00:00	0	0	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	4	35.3	36	27 to 37	100.0	*	*	0	4	0	
9/5/2024	01:00	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	4	31.5	34	25 to 35	100.0	*	*	0	4	0	
9/5/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/5/2024	03:00	1	0	0	0	1	3	0	1	0	0	0	0	0	0	0	0	0	0	6	34	38	28 to 38	66.7	*	*	0	6	0	
9/5/2024	04:00	0	0	1	1	7	2	2	0	0	0	0	0	0	0	0	0	0	0	13	32.9	37	27 to 37	76.9	*	*	0	13	0	
9/5/2024	05:00	0	0	1	6	28	20	4	1	0	0	0	0	0	0	0	0	0	0	60	34.1	37.3	28 to 38	83.3	*	*	0	60	0	
9/5/2024	06:00	0	0	1	13	43	36	7	1	0	0	0	0	0	0	0	0	0	0	101	34.1	37.6	28 to 38	86.1	*	*	1	100	0	
9/5/2024	07:00	0	0	0	25	122	77	9	1	0	0	0	0	0	0	0	0	0	0	234	33.6	36.7	29 to 39	89.7	*	*	0	234	0	
9/5/2024	08:00	0	1	1	21	84	55	8	0	0	0	0	0	0	0	0	0	0	0	170	33.3	36	28 to 38	87.6	*	*	2	168	0	
9/5/2024	09:00	0	0	2	22	44	32	6	0	0	0	0	0	0	0	0	0	0	0	106	32.8	36.1	27 to 37	84.9	*	*	0	106	0	
9/5/2024	10:00	0	0	1	16	40	19	4	0	0	0	0	0	0	0	0	0	0	0	80	32.8	36.5	28 to 38	83.8	*	*	0	80	0	
9/5/2024	11:00	0	1	1	14	54	32	3	0	0	0	0	0	0	0	0	0	0	0	105	33	36.3	29 to 39	90.5	*	*	1	104	0	
9/5/2024	12:00	0	0	2	8	44	24	2	0	0	0	0	0	0	0	0	0	0	0	80	33	36	28 to 38	92.5	*	*	0	80	0	
9/5/2024	13:00	0	0	1	16	33	29	3	0	0	0	0	0	0	0	0	0	0	0	82	32.9	36.5	28 to 38	84.1	*	*	0	82	0	
9/5/2024	14:00	0	0	4	26	68	42	6	0	0	0	0	0	0	0	0	0	0	0	146	32.8	36.1	28 to 38	86.3	*	*	2	144	0	
9/5/2024	15:00	0	1	4	12	61	36	7	0	0	0	0	0	0	0	0	0	0	0	121	33.1	36.7	29 to 39	84.3	*	*	2	119	0	
9/5/2024	16:00	0	0	1	15	53	39	6	0	0	0	0	0	0	0	0	0	0	0	114	33.3	36.2	28 to 38	88.6	*	*	0	114	0	
9/5/2024	17:00	0	0	1	10	56	31	6	0	0	0	0	0	0	0	0	0	0	0	104	33.6	36.6	29 to 39	89.4	*	*	0	104	0	
9/5/2024	18:00	0	2	2	10	28	19	5	0	0	0	0	0	0	0	0	0	0	0	66	32.3	36.6	27 to 37	77.3	*	*	1	65	0	
9/5/2024	19:00	0	0	2	12	31	20	6	1	0	0	0	0	0	0	0	0	0	0	72	33.1	37	27 to 37	77.8	*	*	0	72	0	
9/5/2024	20:00	0	0	1	9	27	16	3	0	0	0	0	0	0	0	0	0	0	0	56	33.4	36.5	28 to 38	89.3	*	*	1	55	0	
9/5/2024	21:00	0	0	2	11	17	12	0	0	0	0	0	0	0	0	0	0	0	0	42	31.8	36	27 to 37	85.7	*	*	0	42	0	
9/5/2024	22:00	0	0	0	5	11	2	0	0	0	0	0	0	0	0	0	0	0	0	18	31.2	33	25 to 35	94.4	*	*	0	18	0	
9/5/2024	23:00	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	4	34	36	27 to 37	100.0	*	*	0	4	0	
24 Hr Summary		1	5	28	254	856	552	87	5	0	0	0	0	0	0	0	0	0	0	1788	33.2	37	28 to 38	85.9	*	*	10	1778	0	

Outgoing Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/6/2024	00:00	0	0	0	2	4	3	0	0	0	0	0	0	0	0	0	0	0	0	9	32	36	29 to 39	88.9	*	*	0	9	0
9/6/2024	01:00	0	0	1	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	5	29.2	30	20 to 30	80.0	*	*	0	5	0
9/6/2024	02:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	36	37	27 to 37	100.0	*	*	0	2	0
9/6/2024	03:00	0	0	0	1	3	2	0	0	0	0	0	0	0	0	0	0	0	0	6	32.7	35	26 to 36	100.0	*	*	1	5	0
9/6/2024	04:00	0	0	1	1	5	5	1	0	0	0	0	0	0	0	0	0	0	0	13	33.2	37	28 to 38	84.6	*	*	0	13	0
9/6/2024	05:00	0	0	0	6	17	12	2	0	0	0	0	0	0	0	0	0	0	0	37	33.1	36.8	28 to 38	81.1	*	*	0	37	0
9/6/2024	06:00	0	0	0	17	51	31	6	0	0	0	0	0	0	0	0	0	0	0	105	33.3	36.7	28 to 38	87.6	*	*	1	104	0
9/6/2024	07:00	0	0	3	23	118	72	8	0	0	0	0	0	0	0	0	0	0	0	224	33.4	36.5	28 to 38	87.9	*	*	3	221	0
9/6/2024	08:00	0	2	3	27	79	30	7	0	0	0	0	0	0	0	0	0	0	0	148	32.3	35.8	28 to 38	83.8	*	*	2	146	0
9/6/2024	09:00	0	1	3	18	50	20	4	0	0	0	0	0	0	0	0	0	0	0	96	32.2	36	28 to 38	86.5	*	*	3	93	0
9/6/2024	10:00	0	0	2	14	40	20	5	0	0	0	0	0	0	0	0	0	0	0	81	32.8	36.6	27 to 37	81.5	*	*	1	80	0
9/6/2024	11:00	0	0	2	14	45	29	7	1	0	0	0	0	0	0	0	0	0	0	98	33.1	36.7	28 to 38	82.7	*	*	0	98	0
9/6/2024	12:00	0	0	4	19	41	34	2	1	0	0	0	0	0	0	0	0	0	0	101	32.6	36.2	28 to 38	85.1	*	*	1	100	0
9/6/2024	13:00	0	1	0	19	67	36	6	1	0	0	0	0	0	0	0	0	0	0	130	33	36.3	27 to 37	87.7	*	*	0	130	0
9/6/2024	14:00	0	0	4	19	64	59	11	1	0	0	0	0	0	0	0	0	0	0	158	33.8	37.4	28 to 38	85.4	*	*	1	157	0
9/6/2024	15:00	0	0	1	19	57	46	5	0	0	0	0	0	0	0	0	0	0	0	128	33.1	36.1	28 to 38	86.7	*	*	0	128	0
9/6/2024	16:00	0	0	1	9	64	27	7	0	2	0	0	0	0	0	0	0	0	0	110	33.6	37	30 to 40	86.4	*	*	1	109	0
9/6/2024	17:00	0	0	2	14	59	46	6	1	0	0	0	0	0	0	0	0	0	0	128	33.7	36.7	29 to 39	87.5	*	*	2	126	0
9/6/2024	18:00	0	0	3	16	47	31	2	1	0	0	0	0	0	0	0	0	0	0	100	32.9	36.3	28 to 38	86.0	*	*	0	100	0
9/6/2024	19:00	0	0	0	8	39	22	4	1	0	0	0	0	0	0	0	0	0	0	74	33.7	36.7	30 to 40	85.1	*	*	0	74	0
9/6/2024	20:00	0	0	1	6	27	23	4	0	0	0	0	0	0	0	0	0	0	0	61	33.8	36.8	28 to 38	90.2	*	*	0	61	0
9/6/2024	21:00	0	0	1	10	19	15	0	0	0	0	0	0	0	0	0	0	0	0	45	32.2	35.4	26 to 36	91.1	*	*	0	45	0
9/6/2024	22:00	0	0	5	9	13	4	1	0	0	0	0	0	0	0	0	0	0	0	32	30	34	26 to 36	81.3	*	*	0	32	0
9/6/2024	23:00	0	0	1	7	1	4	0	0	0	0	0	0	0	0	0	0	0	0	13	30.5	37	22 to 32	69.2	*	*	0	13	0
24 Hr Summary		0	4	38	280	911	574	88	7	2	0	0	0	0	0	0	0	0	0	1904	33.1	37	28 to 38	85.1	*	*	16	1888	0

Outgoing Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/7/2024	00:00	0	0	0	4	3	0	0	1	0	0	0	0	0	0	0	0	0	0	8	31.5	34	24 to 34	87.5	*	*	0	8	0
9/7/2024	01:00	0	0	3	4	1	1	0	0	0	0	0	0	0	0	0	0	0	0	9	27.3	33	23 to 33	88.9	*	*	0	9	0
9/7/2024	02:00	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	6	29.2	32.5	23 to 33	100.0	*	*	0	6	0
9/7/2024	03:00	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28.5	32	24 to 34	100.0	*	*	0	4	0
9/7/2024	04:00	0	0	0	1	3	1	0	0	0	0	0	0	0	0	0	0	0	0	5	32	32	27 to 37	100.0	*	*	0	5	0
9/7/2024	05:00	0	0	1	4	7	3	0	0	0	0	0	0	0	0	0	0	0	0	15	31.1	36	28 to 38	86.7	*	*	0	15	0
9/7/2024	06:00	0	0	3	7	11	4	0	0	0	0	0	0	0	0	0	0	0	0	25	30.7	34	27 to 37	88.0	*	*	0	25	0
9/7/2024	07:00	0	0	0	5	15	13	1	0	0	0	0	0	0	0	0	0	0	0	34	33.3	36.6	27 to 37	91.2	*	*	0	34	0
9/7/2024	08:00	0	0	1	8	26	16	4	0	0	0	0	0	0	0	0	0	0	0	55	33.4	37.4	28 to 38	83.6	*	*	1	54	0
9/7/2024	09:00	0	0	1	19	47	19	1	0	0	0	0	0	0	0	0	0	0	0	87	31.9	35.4	27 to 37	90.8	*	*	0	87	0
9/7/2024	10:00	0	0	1	17	50	36	8	0	0	0	0	0	0	0	0	0	0	0	112	33.5	37.1	28 to 38	83.9	*	*	0	112	0
9/7/2024	11:00	0	0	1	15	52	31	7	1	0	0	0	0	0	0	0	0	0	0	107	33.3	36.6	27 to 37	86.0	*	*	0	107	0
9/7/2024	12:00	0	0	0	13	65	34	3	0	0	0	0	0	0	0	0	0	0	0	115	33.2	36.1	28 to 38	93.0	*	*	0	115	0
9/7/2024	13:00	0	0	5	19	51	28	1	0	0	0	0	0	0	0	0	0	0	0	104	32.4	35.9	28 to 38	81.7	*	*	1	103	0
9/7/2024	14:00	0	0	2	12	56	37	7	0	0	0	0	0	0	0	0	0	0	0	114	33.5	37	29 to 39	85.1	*	*	1	113	0
9/7/2024	15:00	0	0	0	4	56	33	6	0	0	0	0	0	0	0	0	0	0	0	99	34	36.6	29 to 39	92.9	*	*	0	99	0
9/7/2024	16:00	0	0	0	10	53	31	1	1	0	0	0	0	0	0	0	0	0	0	96	33.5	36.5	29 to 39	92.7	*	*	1	95	0
9/7/2024	17:00	0	1	1	9	45	37	9	0	0	0	0	0	0	0	0	0	0	0	102	34.1	36.9	29 to 39	84.3	*	*	1	101	0
9/7/2024	18:00	0	0	1	7	32	30	6	1	0	0	0	0	0	0	0	0	0	0	77	34.2	38.2	29 to 39	81.8	*	*	0	77	0
9/7/2024	19:00	0	0	0	6	38	25	2	0	0	0	0	0	0	0	0	0	0	0	71	33.6	36.6	29 to 39	94.4	*	*	0	71	0
9/7/2024	20:00	1	0	1	6	31	19	1	1	0	0	0	0	0	0	0	0	0	0	60	33.3	36.2	27 to 37	85.0	*	*	2	58	0
9/7/2024	21:00	0	0	0	5	18	14	2	2	0	0	0	0	0	0	0	0	0	0	41	34.1	38	27 to 37	82.9	*	*	0	41	0
9/7/2024	22:00	0	0	1	6	12	3	2	0	0	0	0	0	0	0	0	0	0	0	24	32.2	36	27 to 37	83.3	*	*	0	24	0
9/7/2024	23:00	0	0	0	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	9	29.6	32.5	23 to 33	100.0	*	*	0	9	0
24 Hr Summary		1	1	25	188	681	415	61	7	0	0	0	0	0	0	0	0	0	0	1379	33.2	37	28 to 38	84.8	*	*	7	1372	0

Outgoing Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/8/2024	00:00	0	0	0	1	8	2	1	0	0	0	0	0	0	0	0	0	0	0	12	32.7	35.5	26 to 36	91.7	*	*	0	12	0
9/8/2024	01:00	0	0	0	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	5	33.4	34	26 to 36	100.0	*	*	0	5	0
9/8/2024	02:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34.5	36	26 to 36	100.0	*	*	0	2	0
9/8/2024	03:00	0	0	0	2	1	2	0	0	0	0	0	0	0	0	0	0	0	0	5	31.8	35	26 to 36	100.0	*	*	0	5	0
9/8/2024	04:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	36	26 to 36	100.0	*	*	0	2	0
9/8/2024	05:00	0	0	0	4	3	1	0	0	0	0	0	0	0	0	0	0	0	0	8	30.4	33	26 to 36	100.0	*	*	0	8	0
9/8/2024	06:00	0	0	0	5	7	4	0	0	0	0	0	0	0	0	0	0	0	0	16	31.8	36	27 to 37	93.8	*	*	0	16	0
9/8/2024	07:00	0	0	0	3	8	9	3	0	0	0	0	0	0	0	0	0	0	0	23	34.4	39	28 to 38	78.3	*	*	0	23	0
9/8/2024	08:00	0	0	1	14	31	19	2	0	0	0	0	0	0	0	0	0	0	0	67	32.9	36.3	27 to 37	91.0	*	*	0	67	0
9/8/2024	09:00	0	0	0	12	30	19	3	0	0	0	0	0	0	0	0	0	0	0	64	32.9	35.7	28 to 38	89.1	*	*	0	64	0
9/8/2024	10:00	0	0	1	18	47	21	7	1	0	0	0	0	0	0	0	0	0	0	95	33	36.4	27 to 37	85.3	*	*	0	95	0
9/8/2024	11:00	0	0	2	13	41	31	1	0	0	0	0	0	0	0	0	0	0	0	88	32.7	35.5	27 to 37	89.8	*	*	1	87	0
9/8/2024	12:00	0	0	1	28	89	47	4	0	0	0	0	0	0	0	0	0	0	0	169	32.7	35.8	26 to 36	85.8	*	*	1	168	0
9/8/2024	13:00	0	0	1	21	44	23	2	1	0	0	0	0	0	0	0	0	0	0	92	32.5	35.4	27 to 37	89.1	*	*	0	92	0
9/8/2024	14:00	0	0	0	21	42	25	6	0	0	0	0	0	0	0	0	0	0	0	94	33	36.8	28 to 38	86.2	*	*	0	94	0
9/8/2024	15:00	0	0	1	13	47	25	1	0	0	0	0	0	0	0	0	0	0	0	87	32.6	35.5	28 to 38	90.8	*	*	0	87	0
9/8/2024	16:00	0	0	1	16	43	37	4	0	0	0	0	0	0	0	0	0	0	0	101	33.5	36.4	27 to 37	88.1	*	*	0	101	0
9/8/2024	17:00	0	0	0	16	44	33	5	0	0	0	0	0	0	0	0	0	0	0	98	33.2	36.3	28 to 38	85.7	*	*	0	98	0
9/8/2024	18:00	0	0	3	12	28	29	3	1	0	0	0	0	0	0	0	0	0	0	76	33.2	36.6	28 to 38	84.2	*	*	1	75	0
9/8/2024	19:00	0	0	0	7	22	13	3	0	0	0	0	0	0	0	0	0	0	0	45	33.3	36	27 to 37	84.4	*	*	0	45	0
9/8/2024	20:00	0	0	0	3	32	19	2	0	0	0	0	0	0	0	0	0	0	0	56	33.9	36.3	29 to 39	96.4	*	*	1	55	0
9/8/2024	21:00	0	0	1	7	13	6	0	1	0	0	0	0	0	0	0	0	0	0	28	31.8	35	26 to 36	85.7	*	*	0	28	0
9/8/2024	22:00	0	0	1	2	8	5	0	0	0	0	0	0	0	0	0	0	0	0	16	32.4	36	27 to 37	93.8	*	*	0	16	0
9/8/2024	23:00	0	0	1	3	1	3	0	0	0	0	0	0	0	0	0	0	0	0	8	31.8	36	28 to 38	87.5	*	*	0	8	0
24 Hr Summary		0	0	14	221	595	376	47	4	0	0	0	0	0	0	0	0	0	0	1257	32.9	36	28 to 38	86.9	*	*	4	1253	0

Outgoing Histogram
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/9/2024	00:00	0	0	0	0	2	3	1	0	0	0	0	0	0	0	0	0	0	0	6	36	39	32 to 42	100.0	*	*	0	6	0
9/9/2024	01:00	0	0	0	1	3	1	0	0	0	0	0	0	0	0	0	0	0	0	5	31.6	34	26 to 36	100.0	*	*	0	5	0
9/9/2024	02:00	0	0	0	4	4	0	1	1	0	0	0	0	0	0	0	0	0	0	10	32.2	34	24 to 34	80.0	*	*	0	10	0
9/9/2024	03:00	0	0	0	1	6	1	0	0	0	0	0	0	0	0	0	0	0	0	8	32.5	34	25 to 35	100.0	*	*	0	8	0
9/9/2024	04:00	0	0	1	1	5	3	0	0	0	0	0	0	0	0	0	0	0	0	10	32.8	37	29 to 39	90.0	*	*	0	10	0
9/9/2024	05:00	0	0	1	6	22	11	3	0	0	0	0	0	0	0	0	0	0	0	43	33.1	37.2	28 to 38	88.4	*	*	1	42	0
9/9/2024	06:00	0	0	3	14	43	36	4	1	0	0	0	0	0	0	0	0	0	0	101	33.3	37.2	29 to 39	83.2	*	*	0	101	0
9/9/2024	07:00	0	0	4	16	77	62	2	1	0	0	0	0	0	0	0	0	0	0	162	33.3	36.4	28 to 38	89.5	*	*	0	162	0
9/9/2024	08:00	0	0	3	30	78	36	7	0	0	0	0	0	0	0	0	0	0	0	154	32.2	35.5	26 to 36	83.1	*	*	0	154	0
9/9/2024	09:00	0	0	2	29	56	43	0	0	0	0	0	0	0	0	0	0	0	0	130	32.4	35.9	28 to 38	90.0	*	*	1	129	0
9/9/2024	10:00	0	0	5	18	51	17	2	0	0	0	0	0	0	0	0	0	0	0	93	31.5	35	26 to 36	82.8	*	*	0	93	0
9/9/2024	11:00	0	0	2	18	46	21	1	0	0	0	0	0	0	0	0	0	0	0	88	31.8	35.4	27 to 37	89.8	*	*	0	88	0
9/9/2024	12:00	0	0	2	19	34	28	1	1	0	0	0	0	0	0	0	0	0	0	85	32.5	35.7	27 to 37	83.5	*	*	0	85	0
9/9/2024	13:00	0	0	2	14	54	27	2	1	0	0	0	0	0	0	0	0	0	0	100	32.6	35.3	26 to 36	87.0	*	*	0	100	0
9/9/2024	14:00	0	1	3	19	80	35	5	1	0	0	0	0	0	0	0	0	0	0	144	32.7	35.8	28 to 38	86.8	*	*	3	141	0
9/9/2024	15:00	0	0	1	19	62	38	2	0	0	0	0	0	0	0	0	0	0	0	122	32.9	35.7	28 to 38	90.2	*	*	2	120	0
9/9/2024	16:00	0	1	3	13	42	18	2	0	0	0	0	0	0	0	0	0	0	0	79	32.2	35.4	27 to 37	87.3	*	*	0	79	0
9/9/2024	17:00	0	0	1	10	40	30	4	0	0	0	0	0	0	0	0	0	0	0	85	33.7	37	28 to 38	90.6	*	*	0	85	0
9/9/2024	18:00	0	0	0	4	40	24	2	0	0	0	0	0	0	0	0	0	0	0	70	33.7	36.7	28 to 38	97.1	*	*	0	70	0
9/9/2024	19:00	0	0	2	3	31	9	3	0	0	0	0	0	0	0	0	0	0	0	48	32.8	36	29 to 39	85.4	*	*	1	47	0
9/9/2024	20:00	0	0	0	8	27	13	1	0	0	0	0	0	0	0	0	0	0	0	49	32.8	35.9	26 to 36	87.8	*	*	0	49	0
9/9/2024	21:00	0	0	0	5	11	7	1	0	0	0	0	0	0	0	0	0	0	0	24	33	36	27 to 37	83.3	*	*	0	24	0
9/9/2024	22:00	0	0	0	3	8	1	0	0	0	0	0	0	0	0	0	0	0	0	12	31.7	33.5	26 to 36	100.0	*	*	0	12	0
9/9/2024	23:00	0	0	0	2	5	1	0	0	0	0	0	0	0	0	0	0	0	0	8	31.3	34	24 to 34	87.5	*	*	0	8	0
24 Hr Summary		0	2	35	257	827	465	44	6	0	0	0	0	0	0	0	0	0	0	1636	32.7	36	28 to 38	85.9	*	*	8	1628	0

Outgoing Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

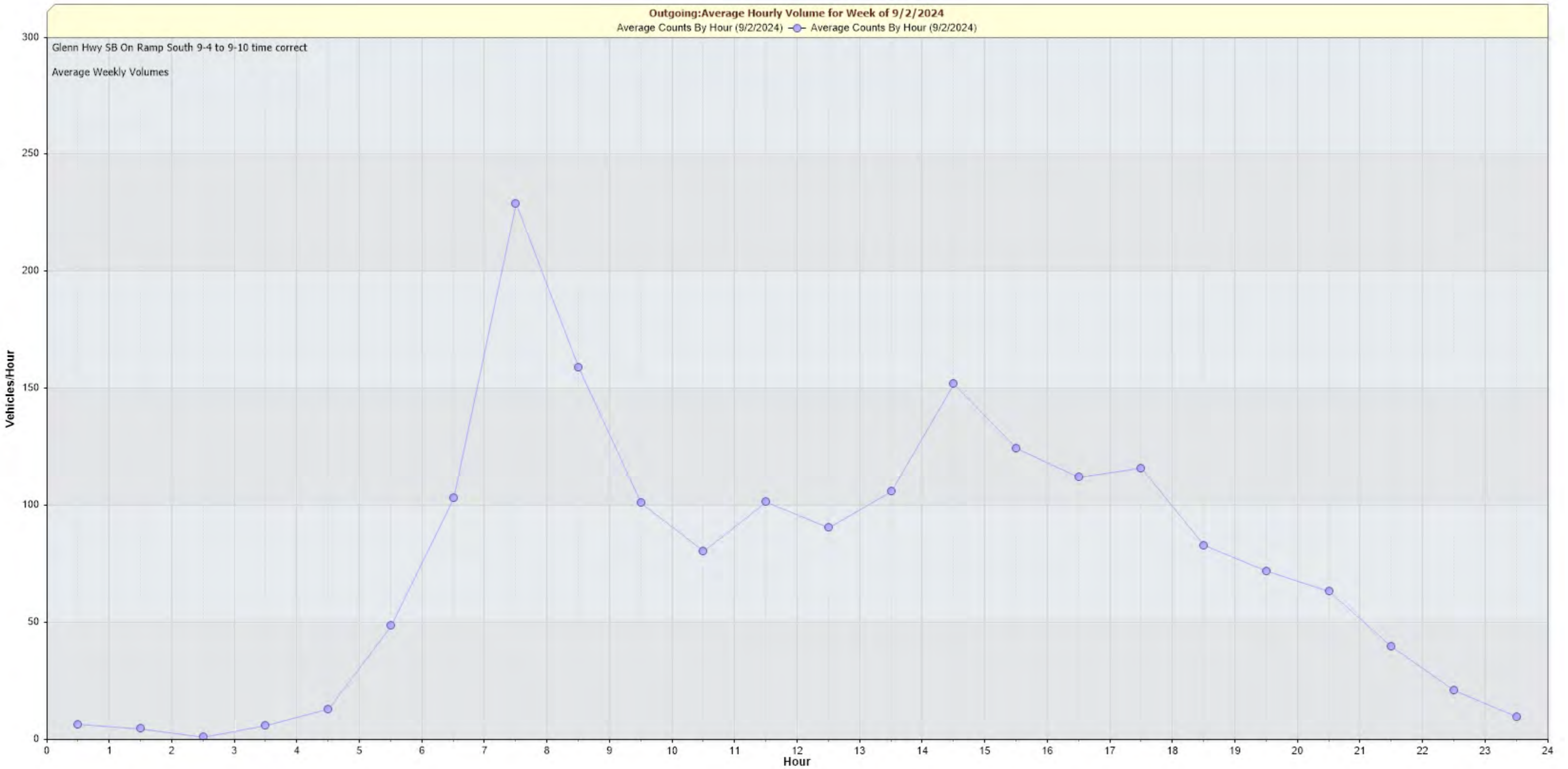
Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28.5	29	19 to 29	100.0	*	*	0	2	0	
9/10/2024	01:00	0	0	0	1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	3	37.7	46	36 to 46	66.7	*	*	0	3	0	
9/10/2024	02:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	32	32	22 to 32	100.0	*	*	0	1	0	
9/10/2024	03:00	0	0	0	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	6	33.5	35	28 to 38	100.0	*	*	0	6	0	
9/10/2024	04:00	0	0	1	1	5	2	1	0	0	0	0	0	0	0	0	0	0	0	10	32.1	35	27 to 37	80.0	*	*	0	10	0	
9/10/2024	05:00	0	0	0	8	19	15	3	0	0	0	0	0	0	0	0	0	0	0	45	33.4	36.5	27 to 37	82.2	*	*	0	45	0	
9/10/2024	06:00	0	0	1	20	46	42	4	0	0	0	0	0	0	0	0	0	0	0	113	33.2	36.6	27 to 37	83.2	*	*	1	112	0	
9/10/2024	07:00	0	0	3	22	122	81	11	1	0	0	0	0	0	0	0	0	0	0	240	33.7	36.8	29 to 39	88.8	*	*	2	238	0	
9/10/2024	08:00	0	0	4	32	84	64	7	1	0	0	0	0	0	0	0	0	0	0	192	33.2	36.8	28 to 38	83.3	*	*	0	192	0	
9/10/2024	09:00	0	0	3	12	56	33	1	1	0	0	0	0	0	0	0	0	0	0	106	32.9	36	27 to 37	88.7	*	*	2	104	0	
9/10/2024	10:00	0	0	3	19	43	26	2	0	0	0	0	0	0	0	0	0	0	0	93	32.5	36.1	27 to 37	86.0	*	*	0	93	0	
9/10/2024	11:00	0	0	1	18	41	27	3	0	0	0	0	0	0	0	0	0	0	0	90	32.8	36.7	28 to 38	82.2	*	*	0	90	0	
9/10/2024	12:00	0	0	2	16	46	22	4	0	0	0	0	0	0	0	0	0	0	0	90	32.8	37	28 to 38	82.2	*	*	1	89	0	
9/10/2024	13:00	0	0	3	17	55	16	3	0	0	0	0	0	0	0	0	0	0	0	94	31.9	35.1	27 to 37	90.4	*	*	0	94	0	
9/10/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	21	168	522	330	40	4	0	0	0	0	0	0	0	0	0	0	1085	33	37	28 to 38	84.1	*	*	6	1079	0	

Outgoing:Average Hourly Volume for Week of 9/2/2024

Average Counts By Hour (9/2/2024) — Average Counts By Hour (9/2/2024)

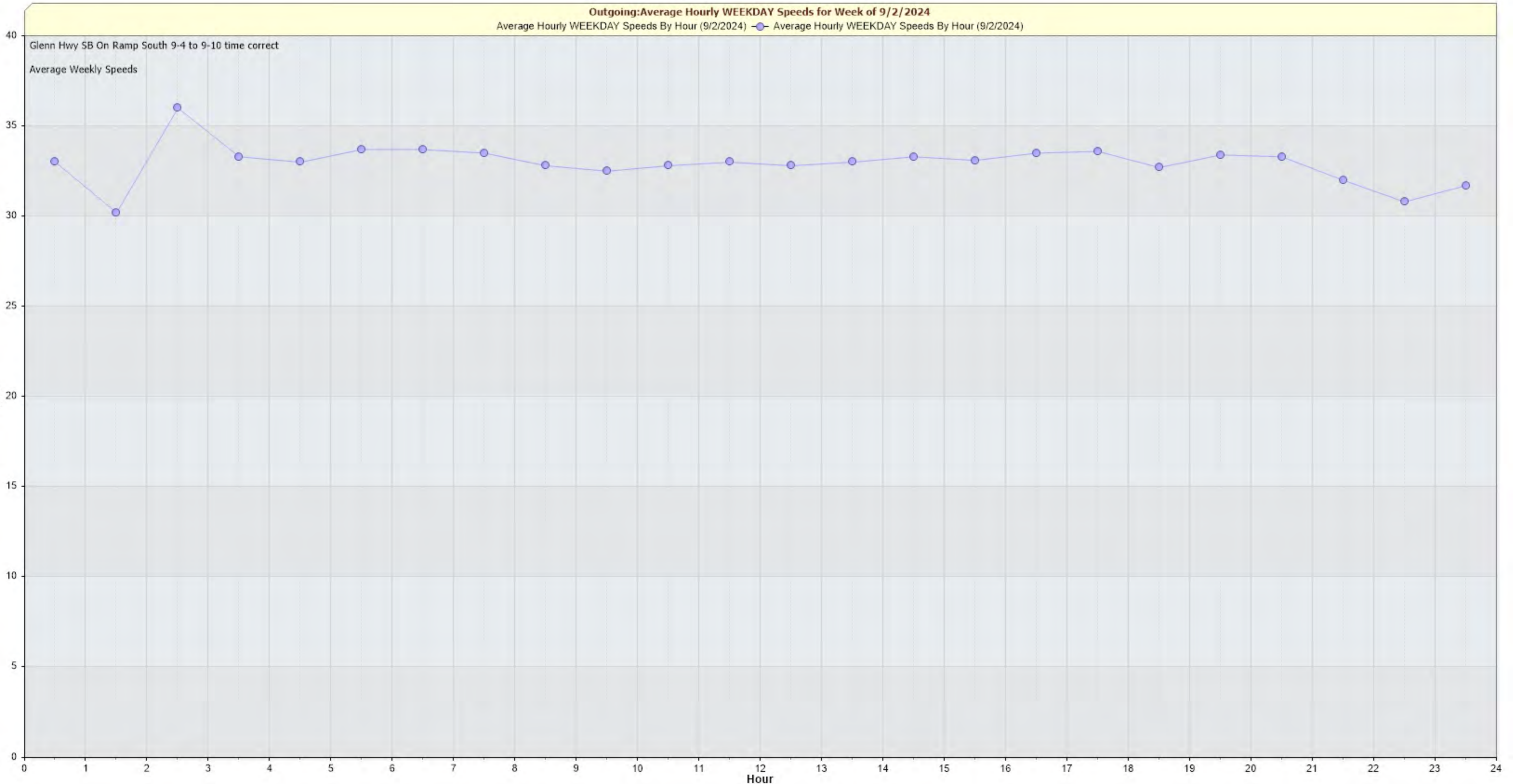
Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

Average Weekly Volumes



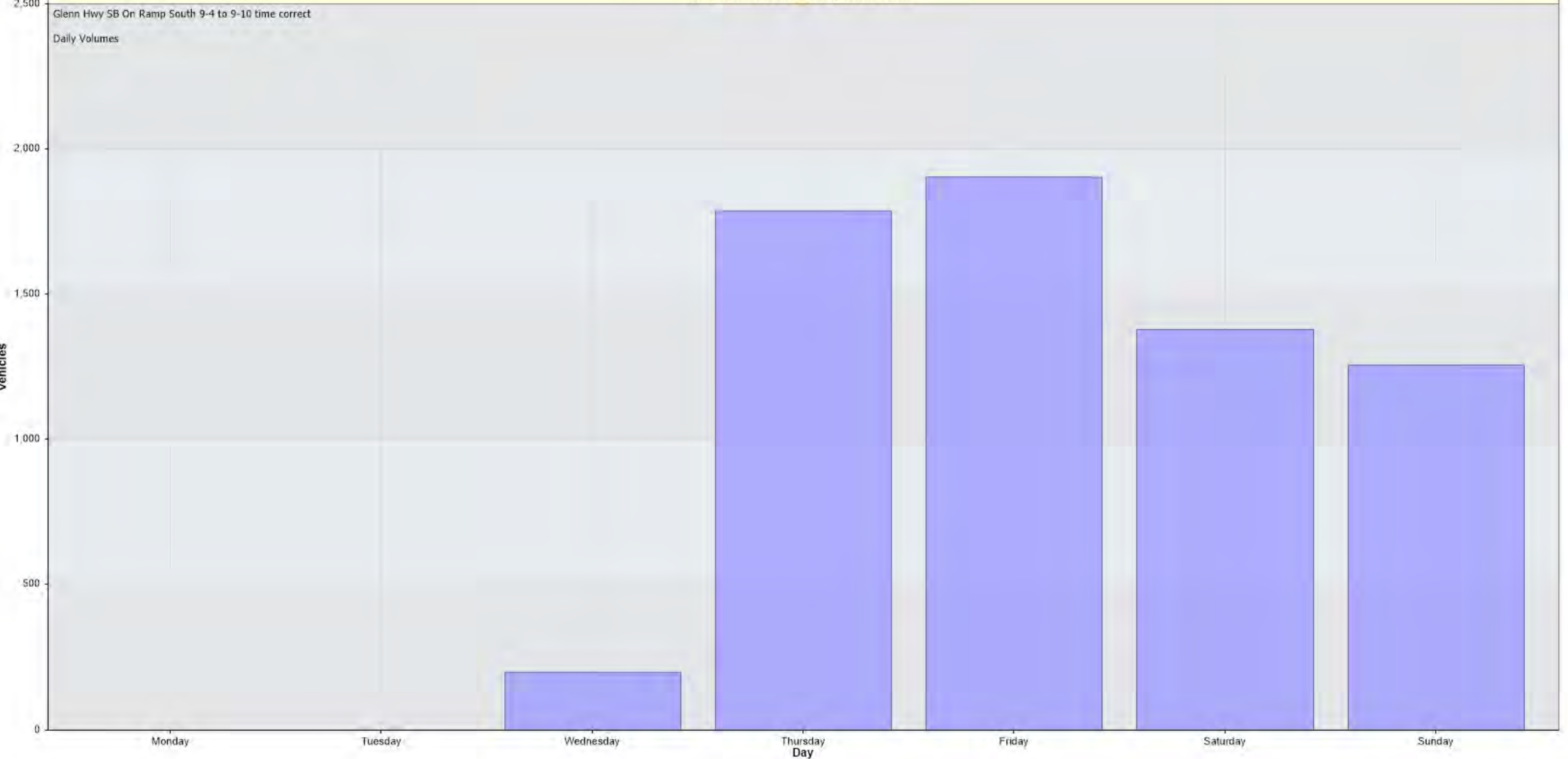
Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/2/2024
Average Hourly WEEKDAY Speeds By Hour (9/2/2024) — Average Hourly WEEKDAY Speeds By Hour (9/2/2024)

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct
Average Weekly Speeds



Outgoing:Daily Volume for Week of 9/2/2024

Daily Vehicle Counts



Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct
Daily Volumes

For Project: Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:44:56 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/4/2024 5:00:00 PM through 9/10/2024 2:59:59 PM

85th Percentile Speed 39 MPH

85th Percentile Vehicles 4834

Max Speed 54 MPH on 9/6/2024 11:00:51 AM

Total Vehicles 5687

AADT: 961

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	811	812
AM Peak 7:00 AM	81	66
PM Peak 4:00 PM	99	93

Speed

Speed Limit: 65

85th Percentile Speed: 39

50th Percentile Speed: 34

10 MPH Pace Interval: 29.0 MPH to 39.0 MPH

Average Speed: 33.76

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	0	0	0	0	0
% over limit	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speeder	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speed	32.8	33.4	34.2	34.3	34.1	34.1	32.7

Class Counts

	Number	%
VEH_SM	15	0.3
VEH_MED	5167	90.9
VEH_LG	505	8.9
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/4/2024 6:00:00 PM	39.0	60	71	44	0.0	0.0%	35.0
9/4/2024 7:00:00 PM	38.0	88	104	52	0.0	0.0%	34.3
9/4/2024 8:00:00 PM	41.0	40	47	48	0.0	0.0%	34.8
9/4/2024 9:00:00 PM	37.0	40	47	46	0.0	0.0%	32.5
9/4/2024 10:00:00 PM	40.0	17	20	45	0.0	0.0%	33.3
9/4/2024 11:00:00 PM	39.0	14	16	48	0.0	0.0%	34.9
9/5/2024 12:00:00 AM	39.0	3	3	39	0.0	0.0%	34.0
9/5/2024 1:00:00 AM	34.0	2	2	34	0.0	0.0%	30.5
9/5/2024 2:00:00 AM	43.0	3	3	43	0.0	0.0%	39.3
9/5/2024 3:00:00 AM	44.0	1	1	44	0.0	0.0%	44.0
9/5/2024 4:00:00 AM	34.0	3	4	41	0.0	0.0%	34.5
9/5/2024 5:00:00 AM	36.0	3	4	40	0.0	0.0%	34.5
9/5/2024 6:00:00 AM	41.0	13	15	42	0.0	0.0%	36.1
9/5/2024 7:00:00 AM	40.0	49	58	45	0.0	0.0%	35.1
9/5/2024 8:00:00 AM	39.0	82	97	45	0.0	0.0%	34.1
9/5/2024 9:00:00 AM	39.0	70	82	50	0.0	0.0%	34.6
9/5/2024 10:00:00 AM	39.0	81	95	46	0.0	0.0%	33.8
9/5/2024 11:00:00 AM	40.0	67	79	48	0.0	0.0%	34.8
9/5/2024 12:00:00 PM	40.0	73	86	46	0.0	0.0%	34.4
9/5/2024 1:00:00 PM	40.0	72	85	49	0.0	0.0%	34.3
9/5/2024 2:00:00 PM	39.0	74	87	48	0.0	0.0%	34.7
9/5/2024 3:00:00 PM	38.0	90	106	47	0.0	0.0%	33.0
9/5/2024 4:00:00 PM	42.0	76	89	48	0.0	0.0%	34.9
9/5/2024 5:00:00 PM	39.0	75	88	47	0.0	0.0%	34.2
9/5/2024 6:00:00 PM	39.0	66	78	47	0.0	0.0%	33.4
9/5/2024 7:00:00 PM	37.0	49	58	43	0.0	0.0%	32.6
9/5/2024 8:00:00 PM	39.0	44	52	48	0.0	0.0%	35.2
9/5/2024 9:00:00 PM	38.0	32	38	45	0.0	0.0%	34.1
9/5/2024 10:00:00 PM	38.0	19	22	51	0.0	0.0%	34.5
9/5/2024 11:00:00 PM	44.0	8	9	45	0.0	0.0%	36.6
9/6/2024 12:00:00 AM	36.0	4	5	37	0.0	0.0%	34.0
9/6/2024 1:00:00 AM	35.0	3	3	35	0.0	0.0%	32.0
9/6/2024 2:00:00 AM	35.0	2	2	35	0.0	0.0%	31.5
9/6/2024 3:00:00 AM	**No Data**						
9/6/2024 4:00:00 AM	40.0	6	7	41	0.0	0.0%	31.4
9/6/2024 5:00:00 AM	38.0	3	3	38	0.0	0.0%	32.7
9/6/2024 6:00:00 AM	39.0	15	18	45	0.0	0.0%	34.7
9/6/2024 7:00:00 AM	42.0	31	36	49	0.0	0.0%	36.0
9/6/2024 8:00:00 AM	38.0	67	79	48	0.0	0.0%	33.9
9/6/2024 9:00:00 AM	38.0	52	61	45	0.0	0.0%	33.9
9/6/2024 10:00:00 AM	37.0	75	88	44	0.0	0.0%	31.9
9/6/2024 11:00:00 AM	40.0	56	66	45	0.0	0.0%	34.6
9/6/2024 12:00:00 PM	40.0	77	91	54	0.0	0.0%	34.5
9/6/2024 1:00:00 PM	39.0	74	87	46	0.0	0.0%	33.5
9/6/2024 2:00:00 PM	41.0	67	79	53	0.0	0.0%	35.0
9/6/2024 3:00:00 PM	39.0	70	82	48	0.0	0.0%	34.1
9/6/2024 4:00:00 PM	40.0	66	78	49	0.0	0.0%	34.6

Incoming Summary
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

9/6/2024 5:00:00 PM	38.0	107	126	47	0.0	0.0%	33.8
9/6/2024 6:00:00 PM	40.0	84	99	48	0.0	0.0%	34.5
9/6/2024 7:00:00 PM	40.0	71	84	46	0.0	0.0%	35.2
9/6/2024 8:00:00 PM	40.0	46	54	47	0.0	0.0%	36.1
9/6/2024 9:00:00 PM	36.0	41	48	41	0.0	0.0%	32.4
9/6/2024 10:00:00 PM	35.0	10	12	37	0.0	0.0%	32.1
9/6/2024 11:00:00 PM	28.0	5	6	31	0.0	0.0%	26.8
9/7/2024 12:00:00 AM	38.0	3	3	38	0.0	0.0%	35.0
9/7/2024 1:00:00 AM	36.0	3	3	36	0.0	0.0%	31.0
9/7/2024 2:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
9/7/2024 3:00:00 AM	**No Data**						
9/7/2024 4:00:00 AM	42.0	2	2	42	0.0	0.0%	35.0
9/7/2024 5:00:00 AM	26.0	2	2	26	0.0	0.0%	25.5
9/7/2024 6:00:00 AM	40.0	4	5	42	0.0	0.0%	37.4
9/7/2024 7:00:00 AM	42.0	9	11	44	0.0	0.0%	37.5
9/7/2024 8:00:00 AM	40.0	20	24	46	0.0	0.0%	36.2
9/7/2024 9:00:00 AM	38.0	35	41	44	0.0	0.0%	33.9
9/7/2024 10:00:00 AM	37.0	41	48	41	0.0	0.0%	32.4
9/7/2024 11:00:00 AM	38.0	59	69	42	0.0	0.0%	33.2
9/7/2024 12:00:00 PM	39.0	67	79	44	0.0	0.0%	34.4
9/7/2024 1:00:00 PM	38.0	81	95	44	0.0	0.0%	32.1
9/7/2024 2:00:00 PM	40.0	70	82	49	0.0	0.0%	34.7
9/7/2024 3:00:00 PM	39.0	66	78	47	0.0	0.0%	34.2
9/7/2024 4:00:00 PM	38.0	76	90	46	0.0	0.0%	34.3
9/7/2024 5:00:00 PM	41.0	80	94	52	0.0	0.0%	35.0
9/7/2024 6:00:00 PM	39.0	79	93	45	0.0	0.0%	34.1
9/7/2024 7:00:00 PM	41.0	60	71	48	0.0	0.0%	34.7
9/7/2024 8:00:00 PM	39.0	43	51	45	0.0	0.0%	34.4
9/7/2024 9:00:00 PM	39.0	39	46	49	0.0	0.0%	34.8
9/7/2024 10:00:00 PM	40.0	23	27	48	0.0	0.0%	33.6
9/7/2024 11:00:00 PM	40.0	19	22	51	0.0	0.0%	35.2
9/8/2024 12:00:00 AM	37.0	17	20	50	0.0	0.0%	33.8
9/8/2024 1:00:00 AM	35.0	2	2	35	0.0	0.0%	34.0
9/8/2024 2:00:00 AM	29.0	4	5	36	0.0	0.0%	27.8
9/8/2024 3:00:00 AM	**No Data**						
9/8/2024 4:00:00 AM	40.0	3	3	40	0.0	0.0%	31.7
9/8/2024 5:00:00 AM	30.0	2	2	30	0.0	0.0%	30.0
9/8/2024 6:00:00 AM	40.0	1	1	40	0.0	0.0%	40.0
9/8/2024 7:00:00 AM	35.0	3	3	35	0.0	0.0%	32.0
9/8/2024 8:00:00 AM	35.0	3	4	38	0.0	0.0%	32.0
9/8/2024 9:00:00 AM	33.0	12	14	40	0.0	0.0%	31.1
9/8/2024 10:00:00 AM	36.0	36	42	44	0.0	0.0%	32.1
9/8/2024 11:00:00 AM	36.0	23	27	43	0.0	0.0%	31.9
9/8/2024 12:00:00 PM	36.0	25	29	41	0.0	0.0%	31.5
9/8/2024 1:00:00 PM	35.0	20	23	41	0.0	0.0%	29.7
9/8/2024 2:00:00 PM	34.0	22	26	39	0.0	0.0%	29.2
9/8/2024 3:00:00 PM	36.0	37	43	44	0.0	0.0%	29.1
9/8/2024 4:00:00 PM	38.0	39	46	53	0.0	0.0%	33.0

Incoming Summary
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

9/8/2024 5:00:00 PM	39.0	64	75	47	0.0	0.0%	34.4
9/8/2024 6:00:00 PM	38.0	60	70	48	0.0	0.0%	33.3
9/8/2024 7:00:00 PM	39.0	46	54	45	0.0	0.0%	35.7
9/8/2024 8:00:00 PM	38.0	33	39	41	0.0	0.0%	33.1
9/8/2024 9:00:00 PM	38.0	37	44	46	0.0	0.0%	33.2
9/8/2024 10:00:00 PM	39.0	10	12	45	0.0	0.0%	35.3
9/8/2024 11:00:00 PM	35.0	6	7	43	0.0	0.0%	33.0
9/9/2024 12:00:00 AM	42.0	6	7	46	0.0	0.0%	35.7
9/9/2024 1:00:00 AM	41.0	1	1	41	0.0	0.0%	41.0
9/9/2024 2:00:00 AM	47.0	2	2	47	0.0	0.0%	43.5
9/9/2024 3:00:00 AM	47.0	1	1	47	0.0	0.0%	47.0
9/9/2024 4:00:00 AM	36.0	3	4	40	0.0	0.0%	33.0
9/9/2024 5:00:00 AM	34.0	3	4	42	0.0	0.0%	34.8
9/9/2024 6:00:00 AM	39.0	12	14	43	0.0	0.0%	33.8
9/9/2024 7:00:00 AM	39.0	34	40	48	0.0	0.0%	34.8
9/9/2024 8:00:00 AM	38.0	52	61	45	0.0	0.0%	32.3
9/9/2024 9:00:00 AM	38.0	29	34	43	0.0	0.0%	33.6
9/9/2024 10:00:00 AM	36.0	20	23	43	0.0	0.0%	33.0
9/9/2024 11:00:00 AM	36.0	43	51	39	0.0	0.0%	29.8
9/9/2024 12:00:00 PM	36.0	24	28	40	0.0	0.0%	30.8
9/9/2024 1:00:00 PM	38.0	18	21	40	0.0	0.0%	31.0
9/9/2024 2:00:00 PM	37.0	26	30	40	0.0	0.0%	32.1
9/9/2024 3:00:00 PM	36.0	58	68	48	0.0	0.0%	32.2
9/9/2024 4:00:00 PM	38.0	48	56	44	0.0	0.0%	31.6
9/9/2024 5:00:00 PM	38.0	71	83	46	0.0	0.0%	32.7
9/9/2024 6:00:00 PM	39.0	58	68	44	0.0	0.0%	33.8
9/9/2024 7:00:00 PM	39.0	55	65	49	0.0	0.0%	35.4
9/9/2024 8:00:00 PM	38.0	31	36	45	0.0	0.0%	35.0
9/9/2024 9:00:00 PM	37.0	25	29	41	0.0	0.0%	32.3
9/9/2024 10:00:00 PM	36.0	14	17	37	0.0	0.0%	30.5
9/9/2024 11:00:00 PM	41.0	11	13	46	0.0	0.0%	32.9
9/10/2024 12:00:00 AM	36.0	6	7	38	0.0	0.0%	33.0
9/10/2024 1:00:00 AM	23.0	1	1	23	0.0	0.0%	23.0
9/10/2024 2:00:00 AM	39.0	3	3	39	0.0	0.0%	37.3
9/10/2024 3:00:00 AM	39.0	5	6	45	0.0	0.0%	36.3
9/10/2024 4:00:00 AM	34.0	3	4	44	0.0	0.0%	32.5
9/10/2024 5:00:00 AM	39.0	6	7	45	0.0	0.0%	36.0
9/10/2024 6:00:00 AM	42.0	15	18	52	0.0	0.0%	35.4
9/10/2024 7:00:00 AM	42.0	38	45	47	0.0	0.0%	36.2
9/10/2024 8:00:00 AM	39.0	75	88	48	0.0	0.0%	34.1
9/10/2024 9:00:00 AM	39.0	54	63	43	0.0	0.0%	32.8
9/10/2024 10:00:00 AM	39.0	88	103	46	0.0	0.0%	33.3
9/10/2024 11:00:00 AM	38.0	51	60	44	0.0	0.0%	32.8
9/10/2024 12:00:00 PM	40.0	42	49	46	0.0	0.0%	32.4
9/10/2024 1:00:00 PM	38.0	28	33	41	0.0	0.0%	31.4
9/10/2024 2:00:00 PM	38.0	42	49	50	0.0	0.0%	32.4
9/10/2024 3:00:00 PM	39.0	6	7	42	0.0	0.0%	28.7

Incoming Summary
Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/5/2024 12:00:00 AM	39.0	262	308	52	0.0	0.0%	34.2
9/6/2024 12:00:00 AM	40.0	1057	1243	51	0.0	0.0%	34.3
9/7/2024 12:00:00 AM	40.0	1030	1212	54	0.0	0.0%	34.1
9/8/2024 12:00:00 AM	39.0	896	1054	52	0.0	0.0%	34.1
9/9/2024 12:00:00 AM	38.0	491	578	53	0.0	0.0%	32.7
9/10/2024 12:00:00 AM	38.0	643	756	49	0.0	0.0%	32.8
9/10/2024 2:59:59 PM	39.0	456	536	52	0.0	0.0%	33.4

Incoming Weekly Counts

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	2	3	3	2	2.5	2.5	34.5
1 - 2	*	*	*	3	2	1	5	2.5	3	39
2 - 3	*	*	*	1	0	0	0	0.5	0	44
3 - 4	*	*	*	4	7	2	3	5.5	2.5	37
4 - 5	*	*	*	4	3	2	2	3.5	2	37
5 - 6	*	*	*	15	18	5	1	16.5	3	39.75
6 - 7	*	*	*	58	36	11	3	47	7	40.55
7 - 8	*	*	*	97	79	24	4	88	14	38
8 - 9	*	*	*	82	61	41	14	71.5	27.5	38.45
9 - 10	*	*	*	95	88	48	42	91.5	45	37.6
10 - 11	*	*	*	79	66	69	27	72.5	48	39.6
11 - 12	*	*	*	86	91	79	29	88.5	54	40
12 - 13	*	*	*	85	87	95	23	86	59	39.25
13 - 14	*	*	*	87	79	82	26	83	54	39.8
14 - 15	*	*	*	106	82	78	43	94	60.5	38.45
15 - 16	*	*	*	89	78	90	46	83.5	68	40.45
16 - 17	*	*	*	88	126	94	75	107	84.5	38.1
17 - 18	*	*	71	78	99	93	70	82.67	81.5	38.87
18 - 19	*	*	104	58	84	71	54	82	62.5	38.07
19 - 20	*	*	47	52	54	51	39	51	45	40
20 - 21	*	*	47	38	48	46	44	44.33	45	37
21 - 22	*	*	20	22	12	27	12	18	19.5	37.27
22 - 23	*	*	16	9	6	22	7	10.33	14.5	37
23 - 24	*	*	3	5	3	20	7	3.67	13.5	37.67
Totals	0	0	308	1243	1212	1054	578			
% of Total	0%	0%	7.01%	28.28%	27.58%	23.98%	13.15%			

Incoming Weekly Counts

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	1	1	*	*	*	*	*	1	0	32
1 - 2	2	3	*	*	*	*	*	2.5	0	43
2 - 3	1	6	*	*	*	*	*	3.5	0	43
3 - 4	4	4	*	*	*	*	*	4	0	35
4 - 5	4	7	*	*	*	*	*	5.5	0	36.5
5 - 6	14	18	*	*	*	*	*	16	0	40.15
6 - 7	40	45	*	*	*	*	*	42.5	0	39.9
7 - 8	61	88	*	*	*	*	*	74.5	0	38.1
8 - 9	34	63	*	*	*	*	*	48.5	0	37.95
9 - 10	23	103	*	*	*	*	*	63	0	37.1
10 - 11	51	60	*	*	*	*	*	55.5	0	36.65
11 - 12	28	49	*	*	*	*	*	38.5	0	37.4
12 - 13	21	33	*	*	*	*	*	27	0	37.5
13 - 14	30	49	*	*	*	*	*	39.5	0	36.9
14 - 15	68	7	*	*	*	*	*	37.5	0	37.15
15 - 16	56	*	*	*	*	*	*	56	0	38
16 - 17	83	*	*	*	*	*	*	83	0	37.5
17 - 18	68	*	*	*	*	*	*	68	0	38.8
18 - 19	65	*	*	*	*	*	*	65	0	39
19 - 20	36	*	*	*	*	*	*	36	0	38
20 - 21	29	*	*	*	*	*	*	29	0	37
21 - 22	17	*	*	*	*	*	*	17	0	35.7
22 - 23	13	*	*	*	*	*	*	13	0	41
23 - 24	7	*	*	*	*	*	*	7	0	36
Totals	756	536	0	0	0	0	0			
% of Total	58.51%	41.49%	0%	0%	0%	0%	0%			

Incoming Monthly Counts

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	1	1	*	2	3	3	2	1.75	2.5	33.25
1 - 2	2	3	*	3	2	1	5	2.5	3	41
2 - 3	1	6	*	1	0	0	0	2	0	43.33
3 - 4	4	4	*	4	7	2	3	4.75	2.5	36
4 - 5	4	7	*	4	3	2	2	4.5	2	36.75
5 - 6	14	18	*	15	18	5	1	16.25	3	39.95
6 - 7	40	45	*	58	36	11	3	44.75	7	40.22
7 - 8	61	88	*	97	79	24	4	81.25	14	38.05
8 - 9	34	63	*	82	61	41	14	60	27.5	38.2
9 - 10	23	103	*	95	88	48	42	77.25	45	37.35
10 - 11	51	60	*	79	66	69	27	64	48	38.12
11 - 12	28	49	*	86	91	79	29	63.5	54	38.7
12 - 13	21	33	*	85	87	95	23	56.5	59	38.38
13 - 14	30	49	*	87	79	82	26	61.25	54	38.35
14 - 15	68	7	*	106	82	78	43	65.75	60.5	37.8
15 - 16	56	*	*	89	78	90	46	74.33	68	39.63
16 - 17	83	*	*	88	126	94	75	99	84.5	37.9
17 - 18	68	*	71	78	99	93	70	79	81.5	38.85
18 - 19	65	*	104	58	84	71	54	77.75	62.5	38.3
19 - 20	36	*	47	52	54	51	39	47.25	45	39.5
20 - 21	29	*	47	38	48	46	44	40.5	45	37
21 - 22	17	*	20	22	12	27	12	17.75	19.5	36.88
22 - 23	13	*	16	9	6	22	7	11	14.5	38
23 - 24	7	*	3	5	3	20	7	4.5	13.5	37.25
Totals	756	536	308	1243	1212	1054	578			
% of Total	13.29%	9.43%	5.42%	21.86%	21.31%	18.53%	10.16%			

Incoming Weekly Speeds

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	30.5	32	31	34	31.4	32.2	34.5
1 - 2	*	*	*	39.33	31.5	26	27.8	36.2	27.5	39
2 - 3	*	*	*	44	0	0	0	44	0	44
3 - 4	*	*	*	34.5	31.43	35	31.67	32.55	33	37
4 - 5	*	*	*	34.5	32.67	25.5	30	33.71	27.75	37
5 - 6	*	*	*	36.13	34.72	37.4	40	35.36	37.83	39.75
6 - 7	*	*	*	35.14	36	37.55	32	35.47	36.36	40.55
7 - 8	*	*	*	34.13	33.9	36.17	32	34.03	35.57	38
8 - 9	*	*	*	34.65	33.93	33.9	31.07	34.34	33.18	38.45
9 - 10	*	*	*	33.81	31.92	32.4	32.1	32.9	32.26	37.6
10 - 11	*	*	*	34.77	34.62	33.19	31.93	34.7	32.83	39.6
11 - 12	*	*	*	34.42	34.48	34.35	31.52	34.45	33.59	40
12 - 13	*	*	*	34.33	33.47	32.11	29.7	33.9	31.64	39.25
13 - 14	*	*	*	34.7	34.99	34.68	29.15	34.84	33.35	39.8
14 - 15	*	*	*	33.03	34.07	34.23	29.07	33.48	32.4	38.45
15 - 16	*	*	*	34.91	34.59	34.3	33.04	34.76	33.88	40.45
16 - 17	*	*	*	34.16	33.85	35.04	34.4	33.98	34.76	38.1
17 - 18	*	*	34.97	33.45	34.47	34.13	33.33	34.29	33.79	38.87
18 - 19	*	*	34.26	32.57	35.2	34.73	35.74	34.18	35.17	38.07
19 - 20	*	*	34.83	35.15	36.11	34.41	33.1	35.39	33.84	40
20 - 21	*	*	32.53	34.13	32.38	34.76	33.16	32.93	33.98	37
21 - 22	*	*	33.3	34.45	32.08	33.56	35.33	33.5	34.1	37.27
22 - 23	*	*	34.88	36.56	26.83	35.23	33	33.81	34.69	37
23 - 24	*	*	34	34	35	33.75	35.71	34.27	34.26	37.67
Totals	0	0	34.2	34.2	34.1	34.1	32.7			
% of Total	0%	0%	20.2%	20.2%	20.14%	20.14%	19.31%			

Incoming Weekly Speeds

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	41	23	*	*	*	*	*	32	*	32
1 - 2	43.5	37.33	*	*	*	*	*	39.8	*	43
2 - 3	47	36.33	*	*	*	*	*	37.86	*	43
3 - 4	33	32.5	*	*	*	*	*	32.75	*	35
4 - 5	34.75	36	*	*	*	*	*	35.55	*	36.5
5 - 6	33.79	35.39	*	*	*	*	*	34.69	*	40.15
6 - 7	34.8	36.24	*	*	*	*	*	35.56	*	39.9
7 - 8	32.33	34.14	*	*	*	*	*	33.4	*	38.1
8 - 9	33.65	32.78	*	*	*	*	*	33.08	*	37.95
9 - 10	33.04	33.33	*	*	*	*	*	33.28	*	37.1
10 - 11	29.78	32.8	*	*	*	*	*	31.41	*	36.65
11 - 12	30.79	32.37	*	*	*	*	*	31.79	*	37.4
12 - 13	30.95	31.36	*	*	*	*	*	31.2	*	37.5
13 - 14	32.1	32.45	*	*	*	*	*	32.32	*	36.9
14 - 15	32.24	28.71	*	*	*	*	*	31.91	*	37.15
15 - 16	31.55	*	*	*	*	*	*	31.55	*	38
16 - 17	32.65	*	*	*	*	*	*	32.65	*	37.5
17 - 18	33.79	*	*	*	*	*	*	33.79	*	38.8
18 - 19	35.43	*	*	*	*	*	*	35.43	*	39
19 - 20	34.97	*	*	*	*	*	*	34.97	*	38
20 - 21	32.34	*	*	*	*	*	*	32.34	*	37
21 - 22	30.47	*	*	*	*	*	*	30.47	*	35.7
22 - 23	32.92	*	*	*	*	*	*	32.92	*	41
23 - 24	33	*	*	*	*	*	*	33	*	36
Totals	32.8	33.4	0	0	0	0	0			
% of Total	49.55%	50.45%	0%	0%	0%	0%	0%			

Incoming Monthly Speeds

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	41	23	*	30.5	32	31	34	31.57	32.2	33.25
1 - 2	43.5	37.33	*	39.33	31.5	26	27.8	38	27.5	41
2 - 3	47	36.33	*	44	0	0	0	38.62	0	43.33
3 - 4	33	32.5	*	34.5	31.43	35	31.67	32.63	33	36
4 - 5	34.75	36	*	34.5	32.67	25.5	30	34.83	27.75	36.75
5 - 6	33.79	35.39	*	36.13	34.72	37.4	40	35.03	37.83	39.95
6 - 7	34.8	36.24	*	35.14	36	37.55	32	35.51	36.36	40.22
7 - 8	32.33	34.14	*	34.13	33.9	36.17	32	33.74	35.57	38.05
8 - 9	33.65	32.78	*	34.65	33.93	33.9	31.07	33.83	33.18	38.2
9 - 10	33.04	33.33	*	33.81	31.92	32.4	32.1	33.06	32.26	37.35
10 - 11	29.78	32.8	*	34.77	34.62	33.19	31.93	33.28	32.83	38.12
11 - 12	30.79	32.37	*	34.42	34.48	34.35	31.52	33.65	33.59	38.7
12 - 13	30.95	31.36	*	34.33	33.47	32.11	29.7	33.25	31.64	38.38
13 - 14	32.1	32.45	*	34.7	34.99	34.68	29.15	34.02	33.35	38.35
14 - 15	32.24	28.71	*	33.03	34.07	34.23	29.07	33.03	32.4	37.8
15 - 16	31.55	*	*	34.91	34.59	34.3	33.04	33.96	33.88	39.63
16 - 17	32.65	*	*	34.16	33.85	35.04	34.4	33.61	34.76	37.9
17 - 18	33.79	*	34.97	33.45	34.47	34.13	33.33	34.19	33.79	38.85
18 - 19	35.43	*	34.26	32.57	35.2	34.73	35.74	34.44	35.17	38.3
19 - 20	34.97	*	34.83	35.15	36.11	34.41	33.1	35.31	33.84	39.5
20 - 21	32.34	*	32.53	34.13	32.38	34.76	33.16	32.83	33.98	37
21 - 22	30.47	*	33.3	34.45	32.08	33.56	35.33	32.77	34.1	36.88
22 - 23	32.92	*	34.88	36.56	26.83	35.23	33	33.55	34.69	38
23 - 24	33	*	34	34	35	33.75	35.71	33.78	34.26	37.25
Totals	32.8	33.4	34.2	34.2	34.1	34.1	32.7			
% of Total	13.93%	14.18%	14.52%	14.52%	14.48%	14.48%	13.89%			

Incoming Weekly EightyFifthSpeeds

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	34	35	36	35	34.5	35.5	34.5
1 - 2	*	*	*	43	35	26	29	39	27.5	39
2 - 3	*	*	*	44	0	0	0	44	0	44
3 - 4	*	*	*	34	40	42	40	37	41	37
4 - 5	*	*	*	36	38	26	30	37	28	37
5 - 6	*	*	*	40.5	39	40	40	39.75	40	39.75
6 - 7	*	*	*	39.8	41.3	42	35	40.55	38.5	40.55
7 - 8	*	*	*	38.2	37.8	39.5	35	38	37.25	38
8 - 9	*	*	*	38.9	38	37.8	33	38.45	35.4	38.45
9 - 10	*	*	*	38.9	36.3	36.8	36	37.6	36.4	37.6
10 - 11	*	*	*	39.7	39.5	37.4	36	39.6	36.7	39.6
11 - 12	*	*	*	40	40	38.7	36	40	37.35	40
12 - 13	*	*	*	39.5	39	37.3	35	39.25	36.15	39.25
13 - 14	*	*	*	38.8	40.8	39.2	34	39.8	36.6	39.8
14 - 15	*	*	*	38	38.9	38.4	36	38.45	37.2	38.45
15 - 16	*	*	*	41.2	39.7	37.7	37.5	40.45	37.6	40.45
16 - 17	*	*	*	38.3	37.9	40.5	38.3	38.1	39.4	38.1
17 - 18	*	*	39	38.4	39.2	39	37.8	38.87	38.4	38.87
18 - 19	*	*	37.6	37	39.6	40.2	39	38.07	39.6	38.07
19 - 20	*	*	41	39	40	39	37.3	40	38.15	40
20 - 21	*	*	37	38	36	39	37.5	37	38.25	37
21 - 22	*	*	39.5	37.3	35	39.3	39	37.27	39.15	37.27
22 - 23	*	*	39	44	28	39.5	35	37	37.25	37
23 - 24	*	*	39	36	38	36.3	42	37.67	39.15	37.67
Totals	0	0	272.1	932.5	872	867.6	833.4			
% of Total	0%	0%	7.2%	24.68%	23.08%	22.97%	22.06%			

Incoming Weekly EightyFifthSpeeds
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	41	23	*	*	*	*	*	32	0	32
1 - 2	47	39	*	*	*	*	*	43	0	43
2 - 3	47	39	*	*	*	*	*	43	0	43
3 - 4	36	34	*	*	*	*	*	35	0	35
4 - 5	34	39	*	*	*	*	*	36.5	0	36.5
5 - 6	39	41.3	*	*	*	*	*	40.15	0	40.15
6 - 7	38.3	41.5	*	*	*	*	*	39.9	0	39.9
7 - 8	37.8	38.4	*	*	*	*	*	38.1	0	38.1
8 - 9	37.7	38.2	*	*	*	*	*	37.95	0	37.95
9 - 10	36	38.2	*	*	*	*	*	37.1	0	37.1
10 - 11	36	37.3	*	*	*	*	*	36.65	0	36.65
11 - 12	35.5	39.3	*	*	*	*	*	37.4	0	37.4
12 - 13	37.5	37.5	*	*	*	*	*	37.5	0	37.5
13 - 14	36.5	37.3	*	*	*	*	*	36.9	0	36.9
14 - 15	35.3	39	*	*	*	*	*	37.15	0	37.15
15 - 16	38	*	*	*	*	*	*	38	0	38
16 - 17	37.5	*	*	*	*	*	*	37.5	0	37.5
17 - 18	38.8	*	*	*	*	*	*	38.8	0	38.8
18 - 19	39	*	*	*	*	*	*	39	0	39
19 - 20	38	*	*	*	*	*	*	38	0	38
20 - 21	37	*	*	*	*	*	*	37	0	37
21 - 22	35.7	*	*	*	*	*	*	35.7	0	35.7
22 - 23	41	*	*	*	*	*	*	41	0	41
23 - 24	36	*	*	*	*	*	*	36	0	36
Totals	915.6	562	0	0	0	0	0			
% of Total	61.97%	38.03%	0%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	41	23	*	34	35	36	35	33.25	35.5	33.25
1 - 2	47	39	*	43	35	26	29	41	27.5	41
2 - 3	47	39	*	44	0	0	0	43.33	0	43.33
3 - 4	36	34	*	34	40	42	40	36	41	36
4 - 5	34	39	*	36	38	26	30	36.75	28	36.75
5 - 6	39	41.3	*	40.5	39	40	40	39.95	40	39.95
6 - 7	38.3	41.5	*	39.8	41.3	42	35	40.22	38.5	40.22
7 - 8	37.8	38.4	*	38.2	37.8	39.5	35	38.05	37.25	38.05
8 - 9	37.7	38.2	*	38.9	38	37.8	33	38.2	35.4	38.2
9 - 10	36	38.2	*	38.9	36.3	36.8	36	37.35	36.4	37.35
10 - 11	36	37.3	*	39.7	39.5	37.4	36	38.12	36.7	38.12
11 - 12	35.5	39.3	*	40	40	38.7	36	38.7	37.35	38.7
12 - 13	37.5	37.5	*	39.5	39	37.3	35	38.38	36.15	38.38
13 - 14	36.5	37.3	*	38.8	40.8	39.2	34	38.35	36.6	38.35
14 - 15	35.3	39	*	38	38.9	38.4	36	37.8	37.2	37.8
15 - 16	38	*	*	41.2	39.7	37.7	37.5	39.63	37.6	39.63
16 - 17	37.5	*	*	38.3	37.9	40.5	38.3	37.9	39.4	37.9
17 - 18	38.8	*	39	38.4	39.2	39	37.8	38.85	38.4	38.85
18 - 19	39	*	37.6	37	39.6	40.2	39	38.3	39.6	38.3
19 - 20	38	*	41	39	40	39	37.3	39.5	38.15	39.5
20 - 21	37	*	37	38	36	39	37.5	37	38.25	37
21 - 22	35.7	*	39.5	37.3	35	39.3	39	36.88	39.15	36.88
22 - 23	41	*	39	44	28	39.5	35	38	37.25	38
23 - 24	36	*	39	36	38	36.3	42	37.25	39.15	37.25

Summary of Violators

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	12	31.8	0	0.0
01:00:00	16	34.1	0	0.0
02:00:00	8	38.6	0	0.0
03:00:00	24	32.7	0	0.0
04:00:00	22	33.5	0	0.0
05:00:00	71	35.3	0	0.0
06:00:00	193	35.6	0	0.0
07:00:00	353	33.9	0	0.0
08:00:00	295	33.7	0	0.0
09:00:00	399	32.9	0	0.0
10:00:00	352	33.2	0	0.0
11:00:00	362	33.6	0	0.0
12:00:00	344	32.7	0	0.0
13:00:00	353	33.8	0	0.0
14:00:00	384	32.8	0	0.0
15:00:00	359	33.9	0	0.0
16:00:00	466	34.0	0	0.0
17:00:00	479	34.1	0	0.0
18:00:00	436	34.7	0	0.0
19:00:00	279	34.8	0	0.0
20:00:00	252	33.2	0	0.0
21:00:00	110	33.2	0	0.0
22:00:00	73	34.0	0	0.0
23:00:00	45	34.1	0	0.0

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	0	0	1	5	28	26	11	0	0	0	0	0	0	0	0	0	0	0	71	35	39	30 to 40	83.1	*	*	0	70	1	
9/4/2024	18:00	2	0	1	10	38	42	6	4	1	0	0	0	0	0	0	0	0	0	104	34.3	37.6	28 to 38	79.8	*	*	1	97	6	
9/4/2024	19:00	0	0	4	3	19	11	7	3	0	0	0	0	0	0	0	0	0	0	47	34.8	41	33 to 43	70.2	*	*	0	46	1	
9/4/2024	20:00	0	2	2	10	16	12	4	1	0	0	0	0	0	0	0	0	0	0	47	32.5	37	26 to 36	72.3	*	*	0	46	1	
9/4/2024	21:00	0	0	1	3	10	2	3	1	0	0	0	0	0	0	0	0	0	0	20	33.3	39.5	27 to 37	75.0	*	*	0	20	0	
9/4/2024	22:00	0	0	0	1	7	6	1	1	0	0	0	0	0	0	0	0	0	0	16	34.9	39	29 to 39	81.3	*	*	0	16	0	
9/4/2024	23:00	0	0	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	0	3	34	39	25 to 35	66.7	*	*	0	3	0	
24 Hr Summary		2	2	9	33	118	101	32	10	1	0	0	0	0	0	0	0	0	0	308	34.2	39	30 to 40	75.3	*	*	1	298	9	

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/5/2024	00:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30.5	34	24 to 34	100.0	*	*	0	2	0
9/5/2024	01:00	0	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3	39.3	43	33 to 43	100.0	*	*	0	3	0
9/5/2024	02:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	44	44	34 to 44	100.0	*	*	0	1	0
9/5/2024	03:00	0	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	0	0	4	34.5	34	31 to 41	100.0	*	*	0	4	0
9/5/2024	04:00	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	4	34.5	36	26 to 36	75.0	*	*	0	4	0
9/5/2024	05:00	0	0	0	2	2	7	4	0	0	0	0	0	0	0	0	0	0	0	15	36.1	40.5	32 to 42	80.0	*	*	0	14	1
9/5/2024	06:00	0	1	1	4	18	22	10	2	0	0	0	0	0	0	0	0	0	0	58	35.1	39.8	30 to 40	75.9	*	*	0	52	6
9/5/2024	07:00	0	0	4	17	22	44	9	1	0	0	0	0	0	0	0	0	0	0	97	34.1	38.2	29 to 39	76.3	*	*	0	81	16
9/5/2024	08:00	1	0	0	11	28	31	8	2	1	0	0	0	0	0	0	0	0	0	82	34.6	38.9	29 to 39	74.4	*	*	0	69	13
9/5/2024	09:00	0	3	2	16	27	34	10	3	0	0	0	0	0	0	0	0	0	0	95	33.8	38.9	30 to 40	70.5	*	*	0	80	15
9/5/2024	10:00	0	2	4	6	21	32	12	2	0	0	0	0	0	0	0	0	0	0	79	34.8	39.7	31 to 41	74.7	*	*	0	64	15
9/5/2024	11:00	0	0	4	10	30	27	13	2	0	0	0	0	0	0	0	0	0	0	86	34.4	40	29 to 39	69.8	*	*	0	72	14
9/5/2024	12:00	2	0	4	10	26	29	6	8	0	0	0	0	0	0	0	0	0	0	85	34.3	39.5	29 to 39	71.8	*	*	0	68	17
9/5/2024	13:00	0	1	2	8	29	35	8	4	0	0	0	0	0	0	0	0	0	0	87	34.7	38.8	29 to 39	75.9	*	*	0	68	19
9/5/2024	14:00	1	2	5	15	41	28	12	2	0	0	0	0	0	0	0	0	0	0	106	33	38	31 to 41	69.8	*	*	1	87	18
9/5/2024	15:00	1	0	4	13	22	27	18	4	0	0	0	0	0	0	0	0	0	0	89	34.9	41.2	32 to 42	66.3	*	*	1	68	20
9/5/2024	16:00	0	1	3	7	32	34	9	2	0	0	0	0	0	0	0	0	0	0	88	34.2	38.3	29 to 39	80.7	*	*	0	80	8
9/5/2024	17:00	0	0	5	10	29	25	8	1	0	0	0	0	0	0	0	0	0	0	78	33.4	38.4	27 to 37	73.1	*	*	0	74	4
9/5/2024	18:00	0	1	0	13	24	15	5	0	0	0	0	0	0	0	0	0	0	0	58	32.6	37	26 to 36	79.3	*	*	0	56	2
9/5/2024	19:00	0	1	0	2	23	18	7	1	0	0	0	0	0	0	0	0	0	0	52	35.2	39	30 to 40	82.7	*	*	0	51	1
9/5/2024	20:00	1	0	1	5	11	15	4	1	0	0	0	0	0	0	0	0	0	0	38	34.1	38	29 to 39	76.3	*	*	0	38	0
9/5/2024	21:00	0	0	0	2	9	10	0	0	1	0	0	0	0	0	0	0	0	0	22	34.5	37.3	28 to 38	90.9	*	*	0	20	2
9/5/2024	22:00	0	0	0	2	1	3	2	1	0	0	0	0	0	0	0	0	0	0	9	36.6	44	26 to 36	66.7	*	*	0	8	1
9/5/2024	23:00	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	5	34	36	27 to 37	100.0	*	*	0	5	0
24 Hr Summary		6	12	39	155	402	441	150	36	2	0	0	0	0	0	0	0	0	0	1243	34.3	40	29 to 39	71.8	*	*	2	1069	172

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/6/2024	00:00	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	3	32	35	25 to 35	100.0	*	*	0	3	0	
9/6/2024	01:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	31.5	35	25 to 35	100.0	*	*	0	1	1	
9/6/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	03:00	0	0	1	2	2	0	2	0	0	0	0	0	0	0	0	0	0	0	7	31.4	40	21 to 31	71.4	*	*	0	7	0	
9/6/2024	04:00	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	32.7	38	28 to 38	100.0	*	*	0	3	0	
9/6/2024	05:00	0	0	0	0	11	4	2	1	0	0	0	0	0	0	0	0	0	0	18	34.7	39	30 to 40	88.9	*	*	0	18	0	
9/6/2024	06:00	0	0	1	2	10	14	8	1	0	0	0	0	0	0	0	0	0	0	36	36	41.3	30 to 40	72.2	*	*	0	34	2	
9/6/2024	07:00	0	0	3	12	29	26	7	2	0	0	0	0	0	0	0	0	0	0	79	33.9	37.8	29 to 39	75.9	*	*	0	75	4	
9/6/2024	08:00	0	0	0	13	23	17	4	4	0	0	0	0	0	0	0	0	0	0	61	33.9	38	27 to 37	77.0	*	*	0	49	12	
9/6/2024	09:00	1	0	6	21	30	22	8	0	0	0	0	0	0	0	0	0	0	0	88	31.9	36.3	26 to 36	75.0	*	*	1	66	21	
9/6/2024	10:00	0	0	2	6	27	19	11	1	0	0	0	0	0	0	0	0	0	0	66	34.6	39.5	30 to 40	75.8	*	*	1	58	7	
9/6/2024	11:00	1	3	2	7	32	28	12	5	1	0	0	0	0	0	0	0	0	0	91	34.5	40	30 to 40	70.3	*	*	1	74	16	
9/6/2024	12:00	0	2	3	14	32	23	11	2	0	0	0	0	0	0	0	0	0	0	87	33.5	39	29 to 39	71.3	*	*	0	80	7	
9/6/2024	13:00	0	1	2	7	28	25	13	1	2	0	0	0	0	0	0	0	0	0	79	35	40.8	28 to 38	70.9	*	*	0	75	4	
9/6/2024	14:00	1	1	4	9	28	28	9	2	0	0	0	0	0	0	0	0	0	0	82	34.1	38.9	29 to 39	74.4	*	*	0	73	9	
9/6/2024	15:00	3	0	2	8	24	25	12	4	0	0	0	0	0	0	0	0	0	0	78	34.6	39.7	30 to 40	70.5	*	*	1	71	6	
9/6/2024	16:00	1	2	2	17	40	49	13	2	0	0	0	0	0	0	0	0	0	0	126	33.8	37.9	28 to 38	77.0	*	*	0	113	13	
9/6/2024	17:00	0	1	5	11	33	33	12	4	0	0	0	0	0	0	0	0	0	0	99	34.5	39.2	29 to 39	72.7	*	*	0	90	9	
9/6/2024	18:00	2	0	0	6	27	32	14	3	0	0	0	0	0	0	0	0	0	0	84	35.2	39.6	31 to 41	79.8	*	*	0	83	1	
9/6/2024	19:00	0	1	0	1	19	21	10	2	0	0	0	0	0	0	0	0	0	0	54	36.1	40	30 to 40	81.5	*	*	0	52	2	
9/6/2024	20:00	1	0	0	8	24	12	3	0	0	0	0	0	0	0	0	0	0	0	48	32.4	36	26 to 36	81.3	*	*	0	47	1	
9/6/2024	21:00	0	0	1	1	6	4	0	0	0	0	0	0	0	0	0	0	0	0	12	32.1	35	27 to 37	91.7	*	*	0	12	0	
9/6/2024	22:00	0	0	1	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	6	26.8	28	21 to 31	100.0	*	*	0	6	0	
9/6/2024	23:00	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3	35	38	28 to 38	100.0	*	*	0	3	0	
24 Hr Summary		10	11	35	151	430	387	151	34	3	0	0	0	0	0	0	0	0	0	1212	34.1	40	30 to 40	71.9	*	*	4	1093	115	

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/7/2024	00:00	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	31	36	26 to 36	100.0	*	*	0	3	0	
9/7/2024	01:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	1	0	
9/7/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/7/2024	03:00	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	2	35	42	18 to 28	50.0	*	*	0	2	0	
9/7/2024	04:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25.5	26	16 to 26	100.0	*	*	0	2	0	
9/7/2024	05:00	0	0	0	0	1	2	2	0	0	0	0	0	0	0	0	0	0	0	5	37.4	40	30 to 40	80.0	*	*	0	5	0	
9/7/2024	06:00	0	0	0	0	4	3	4	0	0	0	0	0	0	0	0	0	0	0	11	37.5	42	32 to 42	81.8	*	*	0	7	4	
9/7/2024	07:00	0	0	0	0	11	8	4	1	0	0	0	0	0	0	0	0	0	0	24	36.2	39.5	30 to 40	87.5	*	*	0	22	2	
9/7/2024	08:00	0	0	2	4	15	15	5	0	0	0	0	0	0	0	0	0	0	0	41	33.9	37.8	30 to 40	82.9	*	*	0	40	1	
9/7/2024	09:00	0	2	2	10	14	18	2	0	0	0	0	0	0	0	0	0	0	0	48	32.4	36.8	28 to 38	79.2	*	*	0	47	1	
9/7/2024	10:00	0	0	6	8	22	29	4	0	0	0	0	0	0	0	0	0	0	0	69	33.2	37.4	28 to 38	76.8	*	*	0	67	2	
9/7/2024	11:00	1	1	3	9	21	34	10	0	0	0	0	0	0	0	0	0	0	0	79	34.4	38.7	29 to 39	73.4	*	*	0	75	4	
9/7/2024	12:00	2	3	6	15	34	28	7	0	0	0	0	0	0	0	0	0	0	0	95	32.1	37.3	28 to 38	72.6	*	*	0	88	7	
9/7/2024	13:00	1	1	1	5	30	31	11	2	0	0	0	0	0	0	0	0	0	0	82	34.7	39.2	30 to 40	79.3	*	*	1	78	3	
9/7/2024	14:00	0	2	0	9	31	27	7	2	0	0	0	0	0	0	0	0	0	0	78	34.2	38.4	30 to 40	76.9	*	*	0	76	2	
9/7/2024	15:00	0	1	2	9	34	32	11	1	0	0	0	0	0	0	0	0	0	0	90	34.3	37.7	28 to 38	78.9	*	*	0	86	4	
9/7/2024	16:00	1	2	1	12	26	35	11	4	2	0	0	0	0	0	0	0	0	0	94	35	40.5	29 to 39	72.3	*	*	0	92	2	
9/7/2024	17:00	0	0	6	11	26	36	12	2	0	0	0	0	0	0	0	0	0	0	93	34.1	39	30 to 40	75.3	*	*	0	88	5	
9/7/2024	18:00	0	0	4	4	24	25	12	2	0	0	0	0	0	0	0	0	0	0	71	34.7	40.2	31 to 41	74.6	*	*	0	67	4	
9/7/2024	19:00	0	0	1	7	18	17	7	1	0	0	0	0	0	0	0	0	0	0	51	34.4	39	29 to 39	74.5	*	*	0	51	0	
9/7/2024	20:00	0	0	3	4	12	20	6	1	0	0	0	0	0	0	0	0	0	0	46	34.8	39	29 to 39	73.9	*	*	0	46	0	
9/7/2024	21:00	0	1	2	3	7	9	3	2	0	0	0	0	0	0	0	0	0	0	27	33.6	39.3	30 to 40	70.4	*	*	1	26	0	
9/7/2024	22:00	0	0	1	2	6	9	3	0	1	0	0	0	0	0	0	0	0	0	22	35.2	39.5	30 to 40	77.3	*	*	0	21	1	
9/7/2024	23:00	0	0	0	4	8	7	0	0	1	0	0	0	0	0	0	0	0	0	20	33.8	36.3	27 to 37	95.0	*	*	0	20	0	
24 Hr Summary		5	13	40	121	345	386	122	18	4	0	0	0	0	0	0	0	0	0	1054	34.1	39	30 to 40	73.5	*	*	2	1010	42	

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/8/2024	00:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	35	25 to 35	100.0	*	*	0	2	0	
9/8/2024	01:00	0	0	2	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	5	27.8	29	19 to 29	80.0	*	*	0	5	0	
9/8/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/8/2024	03:00	0	0	0	2	0	0	1	0	0	0	0	0	0	0	0	0	0	0	3	31.7	40	19 to 29	66.7	*	*	0	3	0	
9/8/2024	04:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30	30	20 to 30	100.0	*	*	0	2	0	
9/8/2024	05:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	40	40	30 to 40	100.0	*	*	0	1	0	
9/8/2024	06:00	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	32	35	25 to 35	100.0	*	*	0	3	0	
9/8/2024	07:00	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	4	32	35	25 to 35	75.0	*	*	0	4	0	
9/8/2024	08:00	0	0	1	4	7	1	1	0	0	0	0	0	0	0	0	0	0	0	14	31.1	33	23 to 33	85.7	*	*	0	14	0	
9/8/2024	09:00	0	0	1	12	15	12	2	0	0	0	0	0	0	0	0	0	0	0	42	32.1	36	26 to 36	81.0	*	*	0	41	1	
9/8/2024	10:00	0	0	2	8	8	8	1	0	0	0	0	0	0	0	0	0	0	0	27	31.9	36	28 to 38	85.2	*	*	0	26	1	
9/8/2024	11:00	0	1	1	6	13	6	2	0	0	0	0	0	0	0	0	0	0	0	29	31.5	36	26 to 36	75.9	*	*	0	29	0	
9/8/2024	12:00	1	2	2	4	10	2	2	0	0	0	0	0	0	0	0	0	0	0	23	29.7	35	24 to 34	69.6	*	*	0	20	3	
9/8/2024	13:00	0	3	2	5	12	4	0	0	0	0	0	0	0	0	0	0	0	0	26	29.2	34	26 to 36	73.1	*	*	0	25	1	
9/8/2024	14:00	5	2	6	4	16	7	3	0	0	0	0	0	0	0	0	0	0	0	43	29.1	36	30 to 40	58.1	*	*	0	41	2	
9/8/2024	15:00	1	1	2	8	16	12	4	1	1	0	0	0	0	0	0	0	0	0	46	33	37.5	27 to 37	73.9	*	*	0	41	5	
9/8/2024	16:00	0	0	3	5	31	27	8	1	0	0	0	0	0	0	0	0	0	0	75	34.4	38.3	30 to 40	82.7	*	*	0	73	2	
9/8/2024	17:00	0	1	6	6	25	24	7	1	0	0	0	0	0	0	0	0	0	0	70	33.3	37.8	28 to 38	72.9	*	*	0	70	0	
9/8/2024	18:00	0	0	0	2	19	25	7	1	0	0	0	0	0	0	0	0	0	0	54	35.7	39	31 to 41	90.7	*	*	0	52	2	
9/8/2024	19:00	0	0	1	7	16	14	1	0	0	0	0	0	0	0	0	0	0	0	39	33.1	37.3	28 to 38	87.2	*	*	0	38	1	
9/8/2024	20:00	1	0	2	6	15	14	5	1	0	0	0	0	0	0	0	0	0	0	44	33.2	37.5	27 to 37	75.0	*	*	0	42	2	
9/8/2024	21:00	0	0	1	0	4	5	1	1	0	0	0	0	0	0	0	0	0	0	12	35.3	39	29 to 39	75.0	*	*	0	12	0	
9/8/2024	22:00	0	0	0	2	3	1	1	0	0	0	0	0	0	0	0	0	0	0	7	33	35	25 to 35	85.7	*	*	0	7	0	
9/8/2024	23:00	0	0	0	1	3	1	1	1	0	0	0	0	0	0	0	0	0	0	7	35.7	42	32 to 42	71.4	*	*	0	6	1	
24 Hr Summary		8	10	32	87	217	168	48	7	1	0	0	0	0	0	0	0	0	0	578	32.7	38	28 to 38	73.4	*	*	0	557	21	

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/9/2024	00:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	41	41	31 to 41	100.0	*	*	0	1	0
9/9/2024	01:00	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	43.5	47	37 to 47	100.0	*	*	0	2	0
9/9/2024	02:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	47	47	37 to 47	100.0	*	*	0	1	0
9/9/2024	03:00	0	0	0	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	4	33	36	26 to 36	75.0	*	*	0	4	0
9/9/2024	04:00	0	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	0	0	4	34.8	34	24 to 34	75.0	*	*	0	4	0
9/9/2024	05:00	0	0	0	4	3	5	2	0	0	0	0	0	0	0	0	0	0	0	14	33.8	39	26 to 36	78.6	*	*	0	14	0
9/9/2024	06:00	0	1	0	4	12	19	3	1	0	0	0	0	0	0	0	0	0	0	40	34.8	38.3	29 to 39	85.0	*	*	0	37	3
9/9/2024	07:00	2	0	2	10	26	18	2	1	0	0	0	0	0	0	0	0	0	0	61	32.3	37.8	27 to 37	72.1	*	*	0	58	3
9/9/2024	08:00	0	0	1	3	14	14	2	0	0	0	0	0	0	0	0	0	0	0	34	33.6	37.7	29 to 39	85.3	*	*	0	29	5
9/9/2024	09:00	0	0	0	3	14	4	2	0	0	0	0	0	0	0	0	0	0	0	23	33	36	26 to 36	87.0	*	*	0	21	2
9/9/2024	10:00	2	2	6	14	13	14	0	0	0	0	0	0	0	0	0	0	0	0	51	29.8	36	29 to 39	64.7	*	*	0	38	13
9/9/2024	11:00	0	1	3	7	10	6	1	0	0	0	0	0	0	0	0	0	0	0	28	30.8	35.5	26 to 36	75.0	*	*	0	22	6
9/9/2024	12:00	0	1	2	4	9	4	1	0	0	0	0	0	0	0	0	0	0	0	21	31	37.5	24 to 34	66.7	*	*	0	15	6
9/9/2024	13:00	0	1	2	4	15	7	1	0	0	0	0	0	0	0	0	0	0	0	30	32.1	36.5	27 to 37	76.7	*	*	0	26	4
9/9/2024	14:00	1	0	3	11	35	17	0	1	0	0	0	0	0	0	0	0	0	0	68	32.2	35.3	29 to 39	83.8	*	*	0	62	6
9/9/2024	15:00	0	4	2	15	16	13	6	0	0	0	0	0	0	0	0	0	0	0	56	31.6	38	28 to 38	73.2	*	*	0	51	5
9/9/2024	16:00	2	3	3	16	20	31	6	2	0	0	0	0	0	0	0	0	0	0	83	32.7	37.5	28 to 38	72.3	*	*	0	76	7
9/9/2024	17:00	0	0	3	8	28	20	9	0	0	0	0	0	0	0	0	0	0	0	68	33.8	38.8	29 to 39	73.5	*	*	0	63	5
9/9/2024	18:00	0	0	0	5	24	26	7	3	0	0	0	0	0	0	0	0	0	0	65	35.4	39	29 to 39	81.5	*	*	0	62	3
9/9/2024	19:00	0	0	0	4	13	14	3	2	0	0	0	0	0	0	0	0	0	0	36	35	38	28 to 38	83.3	*	*	0	36	0
9/9/2024	20:00	0	0	0	8	13	4	4	0	0	0	0	0	0	0	0	0	0	0	29	32.3	37	25 to 35	82.8	*	*	0	28	1
9/9/2024	21:00	1	0	1	3	7	5	0	0	0	0	0	0	0	0	0	0	0	0	17	30.5	35.7	27 to 37	82.4	*	*	0	16	1
9/9/2024	22:00	0	1	0	4	3	2	2	1	0	0	0	0	0	0	0	0	0	0	13	32.9	41	26 to 36	69.2	*	*	0	13	0
9/9/2024	23:00	0	0	0	1	3	3	0	0	0	0	0	0	0	0	0	0	0	0	7	33	36	26 to 36	85.7	*	*	0	4	3
24 Hr Summary		8	14	28	129	282	227	55	13	0	0	0	0	0	0	0	0	0	0	756	32.8	38	28 to 38	73.9	*	*	0	683	73

Incoming Histogram
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

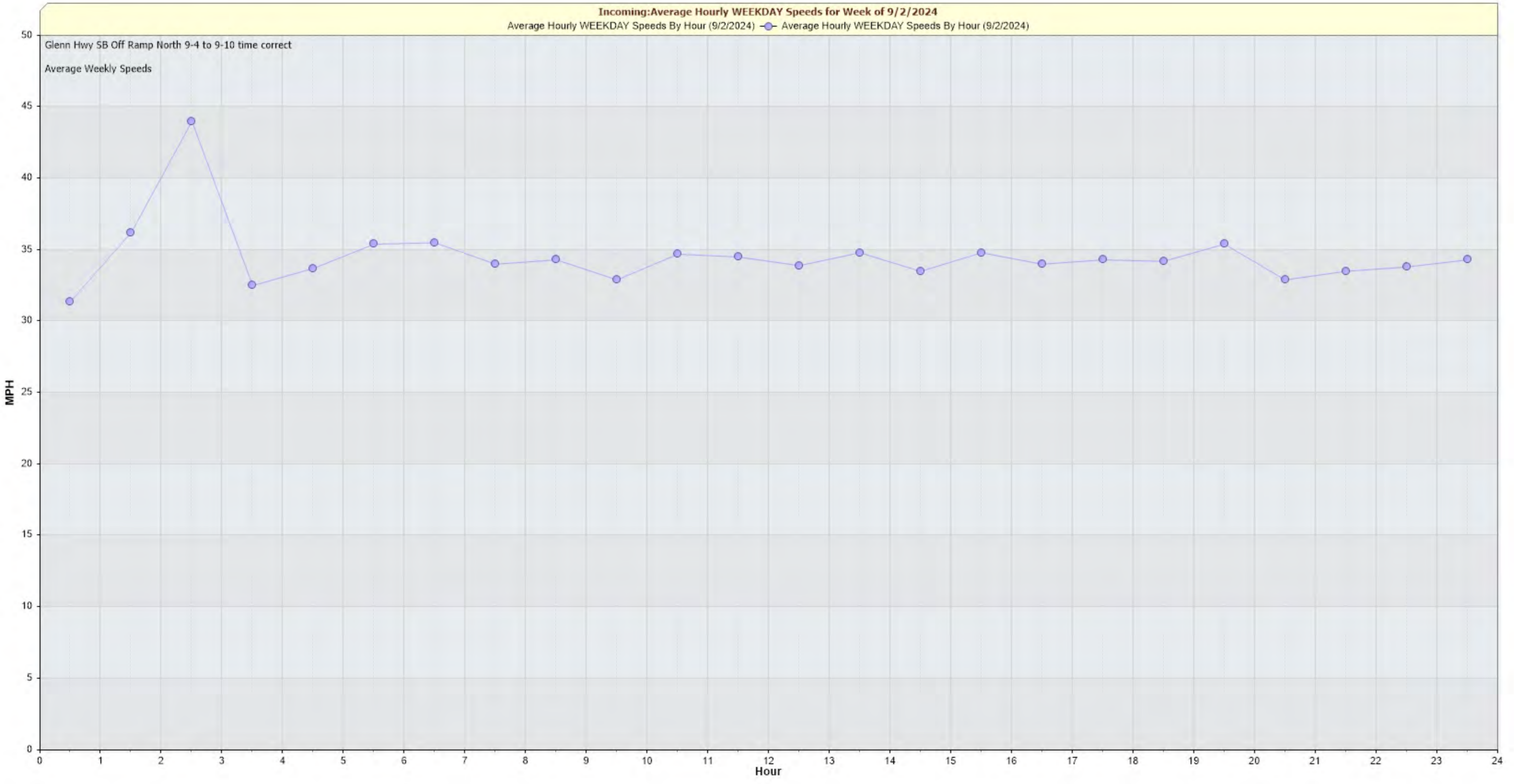
Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	23	23	13 to 23	100.0	*	*	0	1	0	
9/10/2024	01:00	0	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3	37.3	39	29 to 39	100.0	*	*	0	3	0	
9/10/2024	02:00	0	0	0	1	1	3	0	1	0	0	0	0	0	0	0	0	0	0	6	36.3	39	29 to 39	83.3	*	*	0	6	0	
9/10/2024	03:00	0	0	1	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	4	32.5	34	21 to 31	50.0	*	*	0	4	0	
9/10/2024	04:00	0	0	1	0	1	4	0	1	0	0	0	0	0	0	0	0	0	0	7	36	39	29 to 39	71.4	*	*	0	6	1	
9/10/2024	05:00	0	0	0	5	2	6	4	0	1	0	0	0	0	0	0	0	0	0	18	35.4	41.3	27 to 37	61.1	*	*	1	16	1	
9/10/2024	06:00	1	0	0	0	15	19	6	4	0	0	0	0	0	0	0	0	0	0	45	36.2	41.5	30 to 40	80.0	*	*	0	43	2	
9/10/2024	07:00	0	3	4	5	33	33	5	5	0	0	0	0	0	0	0	0	0	0	88	34.1	38.4	29 to 39	77.3	*	*	1	81	6	
9/10/2024	08:00	1	2	3	9	19	23	6	0	0	0	0	0	0	0	0	0	0	0	63	32.8	38.2	29 to 39	71.4	*	*	0	52	11	
9/10/2024	09:00	0	3	7	14	28	40	10	1	0	0	0	0	0	0	0	0	0	0	103	33.3	38.2	29 to 39	70.9	*	*	2	82	19	
9/10/2024	10:00	2	1	0	9	21	23	4	0	0	0	0	0	0	0	0	0	0	0	60	32.8	37.3	28 to 38	78.3	*	*	1	50	9	
9/10/2024	11:00	1	2	4	8	13	13	6	2	0	0	0	0	0	0	0	0	0	0	49	32.4	39.3	28 to 38	61.2	*	*	0	39	10	
9/10/2024	12:00	1	1	2	7	11	10	1	0	0	0	0	0	0	0	0	0	0	0	33	31.4	37.5	27 to 37	69.7	*	*	1	28	4	
9/10/2024	13:00	0	2	1	10	21	11	3	0	1	0	0	0	0	0	0	0	0	0	49	32.4	37.3	28 to 38	79.6	*	*	0	41	8	
9/10/2024	14:00	1	1	0	2	0	2	1	0	0	0	0	0	0	0	0	0	0	0	7	28.7	39	27 to 37	42.9	*	*	0	5	2	
9/10/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		7	15	24	70	167	190	47	14	2	0	0	0	0	0	0	0	0	0	536	33.4	39	29 to 39	70.5	*	*	6	457	73	

Incoming: Average Hourly Volume for Week of 9/2/2024
Average Counts By Hour (9/2/2024) — Average Counts By Hour (9/2/2024)



Incoming: Average Hourly WEEKDAY Speeds for Week of 9/2/2024
Average Hourly WEEKDAY Speeds By Hour (9/2/2024) — Average Hourly WEEKDAY Speeds By Hour (9/2/2024)

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct
Average Weekly Speeds

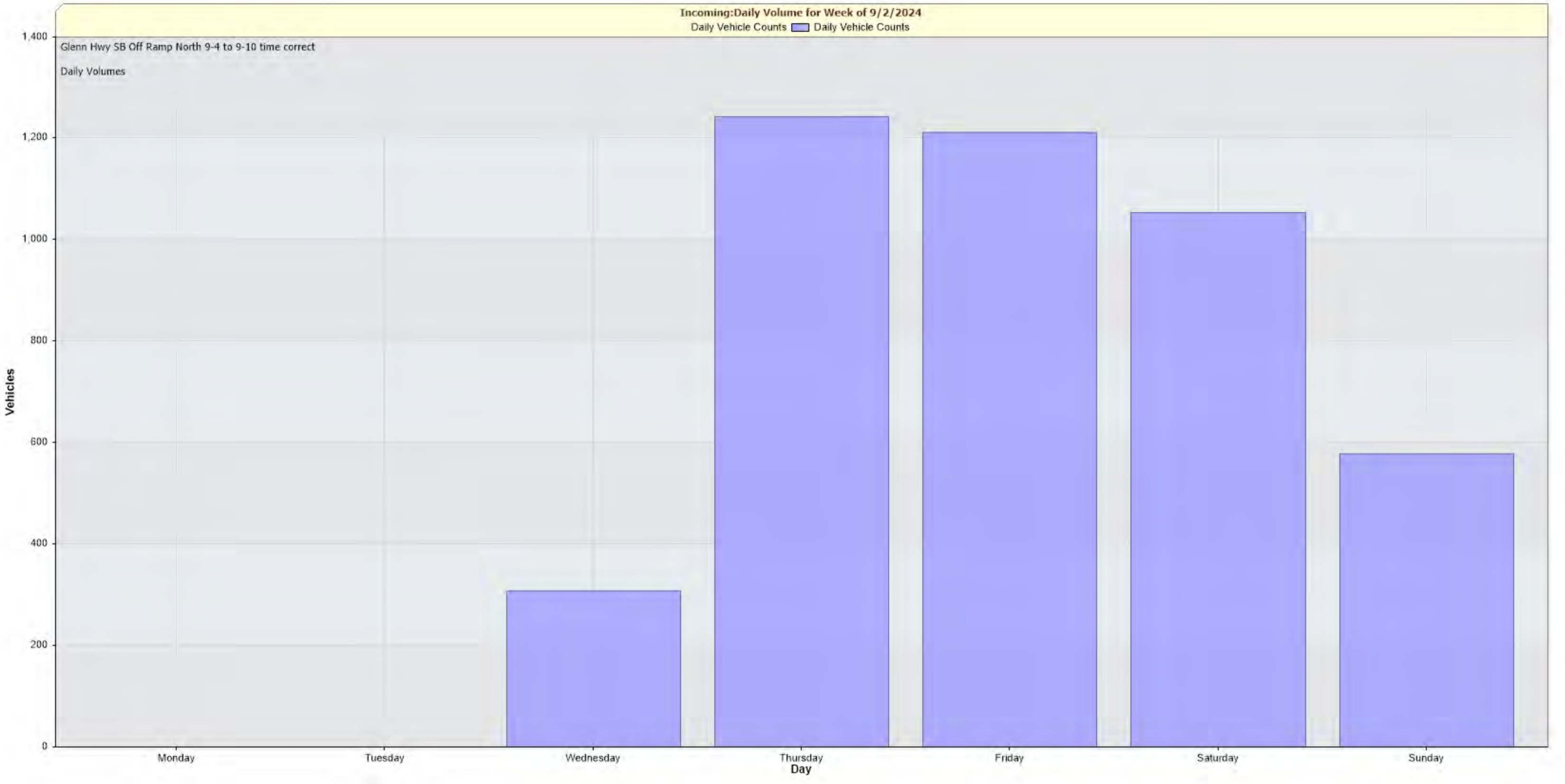


Incoming:Daily Volume for Week of 9/2/2024

Daily Vehicle Counts

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

Daily Volumes



For Project: Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:44:55 PM

Speed Intervals: 1 MPH

Time Intervals: Instant

Traffic Report From: 9/6/2024 5:00:00 PM through 9/6/2024 5:59:59 PM

85th Percentile Speed: 42 MPH

85th Percentile Vehicles: 1

Max Speed: 42 MPH on 9/6/2024 5:21:22 PM

Total Vehicles: 1

AADT: 24

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1	1
AM Peak 12:00 AM	0	0
PM Peak 5:00 PM	1	1

Speed

Speed Limit: 65

85th Percentile Speed: 42

50th Percentile Speed: 1

10 MPH Pace Interval: 32.0 MPH to 42.0 MPH

Average Speed: 42

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	N/A	N/A	N/A	N/A	0	N/A	N/A
% over limit	N/A	N/A	N/A	N/A	0.0	N/A	N/A
Avg Speeder	N/A	N/A	N/A	N/A	0.0	N/A	N/A
Avg Speed	N/A	N/A	N/A	N/A	42.0	N/A	N/A

Class Counts

	Number	%
VEH_SM	0	0
VEH_MED	1	100
VEH_LG	0	0
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Outgoing Weekly Counts

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	*	*	0	0	0
1 - 2	*	*	*	*	*	*	*	0	0	0
2 - 3	*	*	*	*	*	*	*	0	0	0
3 - 4	*	*	*	*	*	*	*	0	0	0
4 - 5	*	*	*	*	*	*	*	0	0	0
5 - 6	*	*	*	*	*	*	*	0	0	0
6 - 7	*	*	*	*	*	*	*	0	0	0
7 - 8	*	*	*	*	*	*	*	0	0	0
8 - 9	*	*	*	*	*	*	*	0	0	0
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	1	*	*	1	0	42
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	0	0	0	0	1	0	0			
% of Total	0%	0%	0%	0%	100%	0%	0%			

Outgoing Monthly Counts
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	*	*	*	0	0	0
1 - 2	*	*	*	*	*	*	*	0	0	0
2 - 3	*	*	*	*	*	*	*	0	0	0
3 - 4	*	*	*	*	*	*	*	0	0	0
4 - 5	*	*	*	*	*	*	*	0	0	0
5 - 6	*	*	*	*	*	*	*	0	0	0
6 - 7	*	*	*	*	*	*	*	0	0	0
7 - 8	*	*	*	*	*	*	*	0	0	0
8 - 9	*	*	*	*	*	*	*	0	0	0
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	1	*	*	1	0	42
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	0	0	0	0	1	0	0			
% of Total	0%	0%	0%	0%	100%	0%	0%			

Outgoing Weekly Speeds
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	*	*	*	*	0
1 - 2	*	*	*	*	*	*	*	*	*	0
2 - 3	*	*	*	*	*	*	*	*	*	0
3 - 4	*	*	*	*	*	*	*	*	*	0
4 - 5	*	*	*	*	*	*	*	*	*	0
5 - 6	*	*	*	*	*	*	*	*	*	0
6 - 7	*	*	*	*	*	*	*	*	*	0
7 - 8	*	*	*	*	*	*	*	*	*	0
8 - 9	*	*	*	*	*	*	*	*	*	0
9 - 10	*	*	*	*	*	*	*	*	*	0
10 - 11	*	*	*	*	*	*	*	*	*	0
11 - 12	*	*	*	*	*	*	*	*	*	0
12 - 13	*	*	*	*	*	*	*	*	*	0
13 - 14	*	*	*	*	*	*	*	*	*	0
14 - 15	*	*	*	*	*	*	*	*	*	0
15 - 16	*	*	*	*	*	*	*	*	*	0
16 - 17	*	*	*	*	*	*	*	*	*	0
17 - 18	*	*	*	*	42	*	*	42	*	42
18 - 19	*	*	*	*	*	*	*	*	*	0
19 - 20	*	*	*	*	*	*	*	*	*	0
20 - 21	*	*	*	*	*	*	*	*	*	0
21 - 22	*	*	*	*	*	*	*	*	*	0
22 - 23	*	*	*	*	*	*	*	*	*	0
23 - 24	*	*	*	*	*	*	*	*	*	0
Totals	0	0	0	0	42	0	0			
% of Total	0%	0%	0%	0%	100%	0%	0%			

Outgoing Monthly Speeds
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	*	*	*	*	0
1 - 2	*	*	*	*	*	*	*	*	*	0
2 - 3	*	*	*	*	*	*	*	*	*	0
3 - 4	*	*	*	*	*	*	*	*	*	0
4 - 5	*	*	*	*	*	*	*	*	*	0
5 - 6	*	*	*	*	*	*	*	*	*	0
6 - 7	*	*	*	*	*	*	*	*	*	0
7 - 8	*	*	*	*	*	*	*	*	*	0
8 - 9	*	*	*	*	*	*	*	*	*	0
9 - 10	*	*	*	*	*	*	*	*	*	0
10 - 11	*	*	*	*	*	*	*	*	*	0
11 - 12	*	*	*	*	*	*	*	*	*	0
12 - 13	*	*	*	*	*	*	*	*	*	0
13 - 14	*	*	*	*	*	*	*	*	*	0
14 - 15	*	*	*	*	*	*	*	*	*	0
15 - 16	*	*	*	*	*	*	*	*	*	0
16 - 17	*	*	*	*	*	*	*	*	*	0
17 - 18	*	*	*	*	42	*	*	42	*	42
18 - 19	*	*	*	*	*	*	*	*	*	0
19 - 20	*	*	*	*	*	*	*	*	*	0
20 - 21	*	*	*	*	*	*	*	*	*	0
21 - 22	*	*	*	*	*	*	*	*	*	0
22 - 23	*	*	*	*	*	*	*	*	*	0
23 - 24	*	*	*	*	*	*	*	*	*	0
Totals	0	0	0	0	42	0	0			
% of Total	0%	0%	0%	0%	100%	0%	0%			

Outgoing Weekly EightyFifthSpeeds
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	*	*	0	0	0
1 - 2	*	*	*	*	*	*	*	0	0	0
2 - 3	*	*	*	*	*	*	*	0	0	0
3 - 4	*	*	*	*	*	*	*	0	0	0
4 - 5	*	*	*	*	*	*	*	0	0	0
5 - 6	*	*	*	*	*	*	*	0	0	0
6 - 7	*	*	*	*	*	*	*	0	0	0
7 - 8	*	*	*	*	*	*	*	0	0	0
8 - 9	*	*	*	*	*	*	*	0	0	0
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	42	*	*	42	0	42
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	0	0	0	0	42	0	0			
% of Total	0%	0%	0%	0%	100%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	*	*	0	0	0
1 - 2	*	*	*	*	*	*	*	0	0	0
2 - 3	*	*	*	*	*	*	*	0	0	0
3 - 4	*	*	*	*	*	*	*	0	0	0
4 - 5	*	*	*	*	*	*	*	0	0	0
5 - 6	*	*	*	*	*	*	*	0	0	0
6 - 7	*	*	*	*	*	*	*	0	0	0
7 - 8	*	*	*	*	*	*	*	0	0	0
8 - 9	*	*	*	*	*	*	*	0	0	0
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	42	*	*	42	0	42
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0

Summary of Violators

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	0	N/A	N/A	N/A
01:00:00	0	N/A	N/A	N/A
02:00:00	0	N/A	N/A	N/A
03:00:00	0	N/A	N/A	N/A
04:00:00	0	N/A	N/A	N/A
05:00:00	0	N/A	N/A	N/A
06:00:00	0	N/A	N/A	N/A
07:00:00	0	N/A	N/A	N/A
08:00:00	0	N/A	N/A	N/A
09:00:00	0	N/A	N/A	N/A
10:00:00	0	N/A	N/A	N/A
11:00:00	0	N/A	N/A	N/A
12:00:00	0	N/A	N/A	N/A
13:00:00	0	N/A	N/A	N/A
14:00:00	0	N/A	N/A	N/A
15:00:00	0	N/A	N/A	N/A
16:00:00	0	N/A	N/A	N/A
17:00:00	1	42.0	0	0.0
18:00:00	0	N/A	N/A	N/A
19:00:00	0	N/A	N/A	N/A
20:00:00	0	N/A	N/A	N/A
21:00:00	0	N/A	N/A	N/A
22:00:00	0	N/A	N/A	N/A
23:00:00	0	N/A	N/A	N/A

Outgoing Histogram

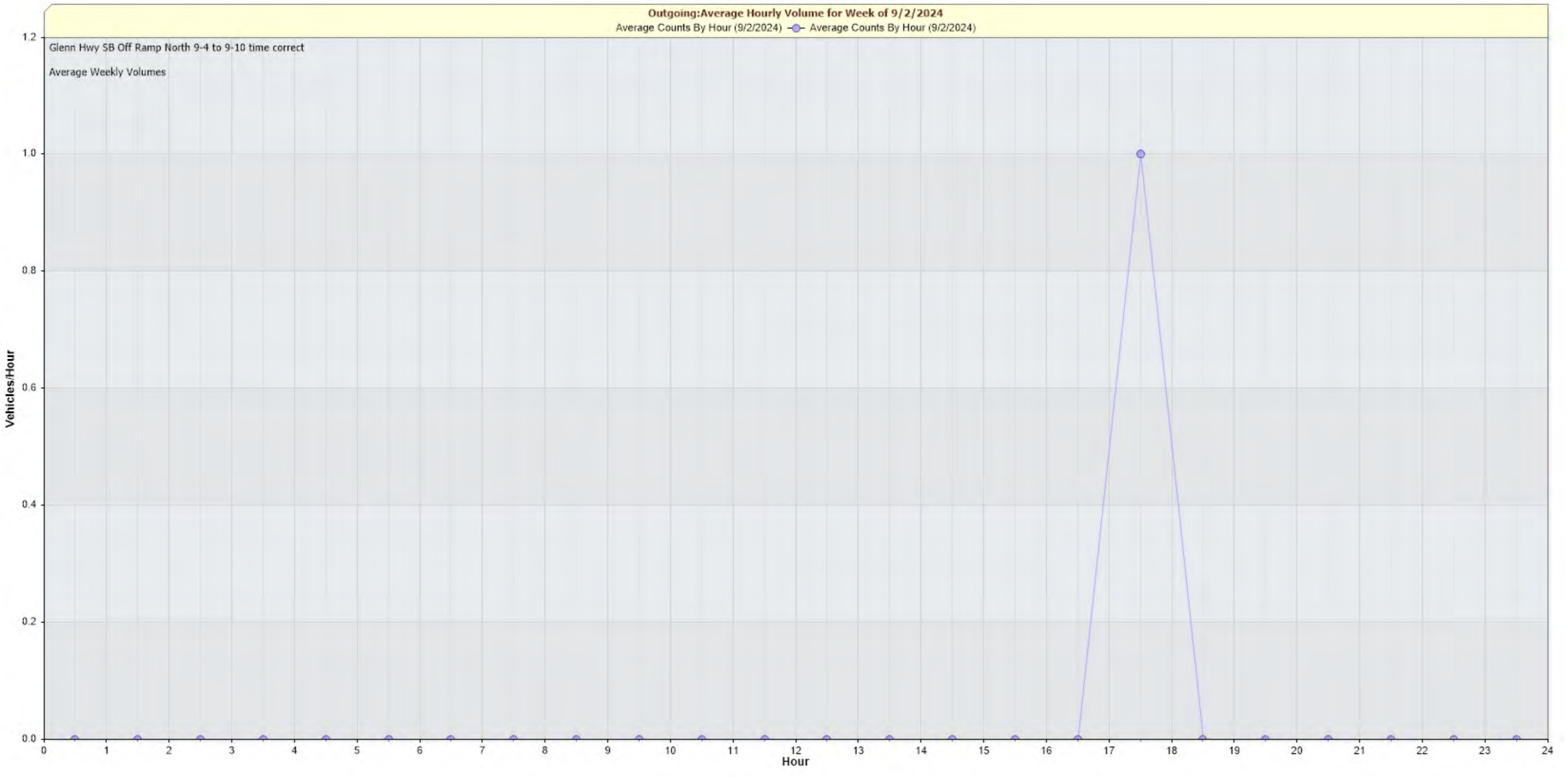
Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/6/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	17:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	42	42	32 to 42	100.0	*	*	0	1	0	
9/6/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	42	42	32 to 42	100.0	*	*	0	1	0	

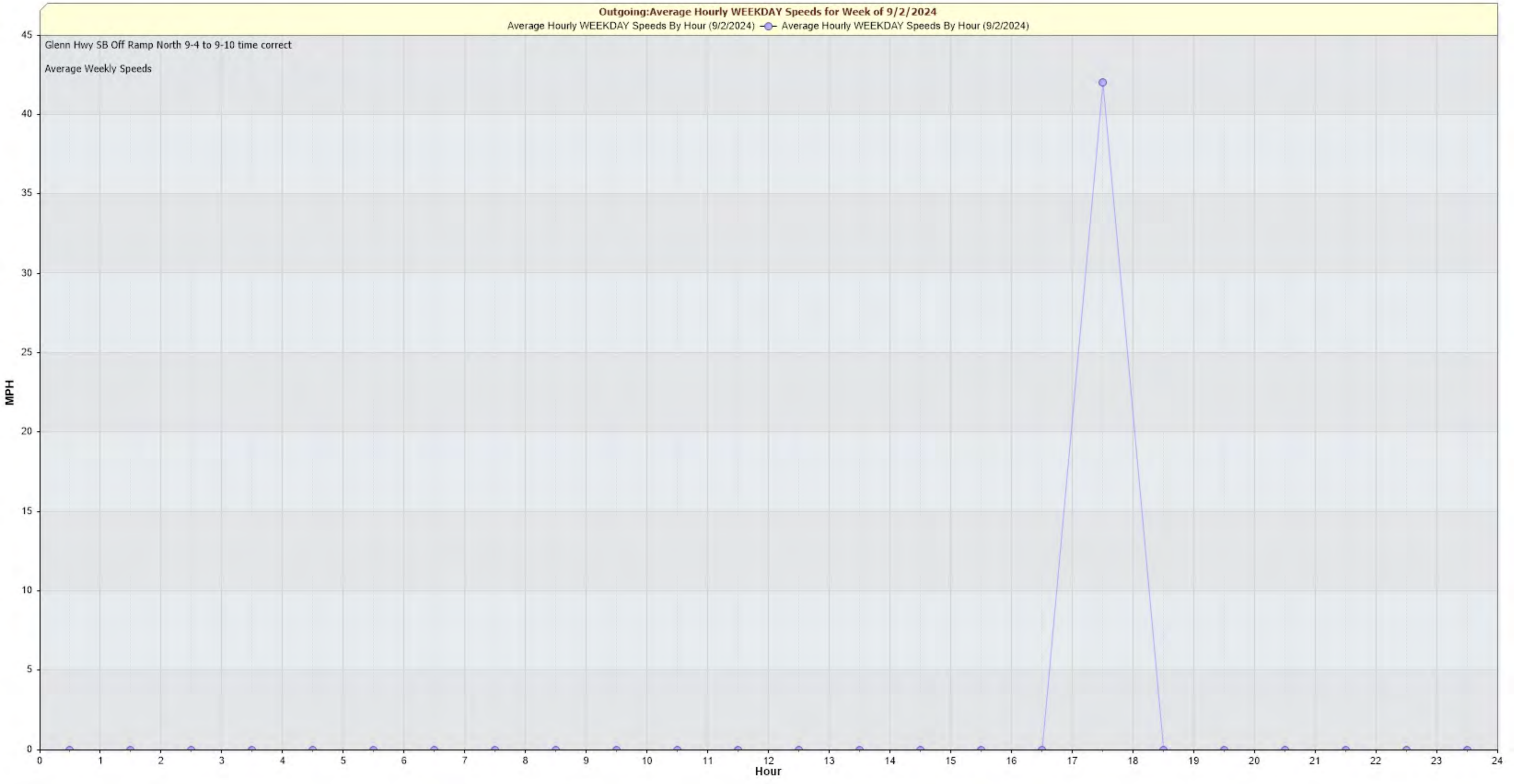
Outgoing: Average Hourly Volume for Week of 9/2/2024
Average Counts By Hour (9/2/2024) — Average Counts By Hour (9/2/2024)

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/2/2024
Average Hourly WEEKDAY Speeds By Hour (9/2/2024) — Average Hourly WEEKDAY Speeds By Hour (9/2/2024)

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct
Average Weekly Speeds

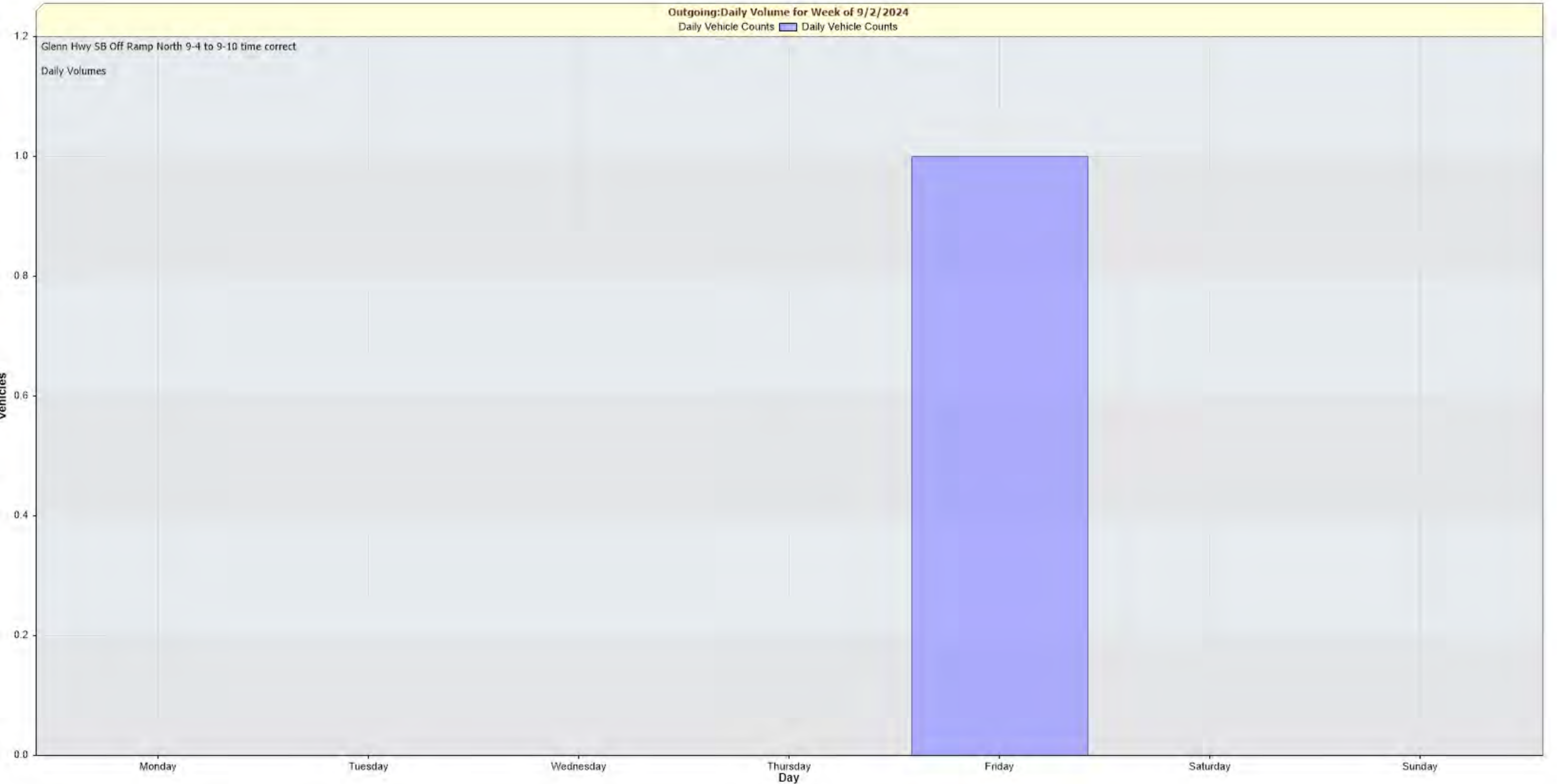


Outgoing:Daily Volume for Week of 9/2/2024

Daily Vehicle Counts

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

Daily Volumes



For Project: Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:49:26 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/10/2024 3:00:00 PM through 9/16/2024 8:59:59 AM

85th Percentile Speed 40 MPH

85th Percentile Vehicles 8926

Max Speed 53 MPH on 9/15/2024 6:30:37 PM

Total Vehicles 10501

AADT: 1826

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1500	1500
AM Peak 8:00 AM	118	89
PM Peak 6:00 PM	217	181

Speed

Speed Limit: 65

85th Percentile Speed: 40

50th Percentile Speed: 35

10 MPH Pace Interval: 31.0 MPH to 41.0 MPH

Average Speed: 33.87

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	0	0	0	0	0
% over limit	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speeder	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speed	34.1	32.7	34.1	32.9	34.8	33.3	35.0

Class Counts

	Number	%
VEH_SM	24	0.2
VEH_MED	10205	97.2
VEH_LG	272	2.6
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/10/2024 4:00:00 PM	39.0	101	119	49	0.0	0.0%	32.4
9/10/2024 5:00:00 PM	40.0	133	156	46	0.0	0.0%	32.7
9/10/2024 6:00:00 PM	39.0	183	215	45	0.0	0.0%	32.7
9/10/2024 7:00:00 PM	39.0	151	178	49	0.0	0.0%	33.3
9/10/2024 8:00:00 PM	38.0	150	177	45	0.0	0.0%	31.7
9/10/2024 9:00:00 PM	40.0	94	110	52	0.0	0.0%	33.7
9/10/2024 10:00:00 PM	38.0	54	64	44	0.0	0.0%	33.9
9/10/2024 11:00:00 PM	37.0	48	56	46	0.0	0.0%	31.3
9/11/2024 12:00:00 AM	37.0	20	23	40	0.0	0.0%	32.3
9/11/2024 1:00:00 AM	35.0	14	16	41	0.0	0.0%	28.6
9/11/2024 2:00:00 AM	36.0	3	3	36	0.0	0.0%	30.3
9/11/2024 3:00:00 AM	31.0	4	5	39	0.0	0.0%	26.8
9/11/2024 4:00:00 AM	34.0	4	5	40	0.0	0.0%	28.0
9/11/2024 5:00:00 AM	34.0	1	1	34	0.0	0.0%	34.0
9/11/2024 6:00:00 AM	35.0	10	12	41	0.0	0.0%	30.4
9/11/2024 7:00:00 AM	40.0	8	9	43	0.0	0.0%	33.6
9/11/2024 8:00:00 AM	37.0	21	25	39	0.0	0.0%	33.2
9/11/2024 9:00:00 AM	39.0	130	153	48	0.0	0.0%	31.7
9/11/2024 10:00:00 AM	38.0	61	72	52	0.0	0.0%	33.3
9/11/2024 11:00:00 AM	39.0	71	84	45	0.0	0.0%	33.7
9/11/2024 12:00:00 PM	41.0	61	72	46	0.0	0.0%	33.0
9/11/2024 1:00:00 PM	41.0	59	69	47	0.0	0.0%	35.9
9/11/2024 2:00:00 PM	40.0	96	113	46	0.0	0.0%	33.8
9/11/2024 3:00:00 PM	41.0	116	136	48	0.0	0.0%	34.1
9/11/2024 4:00:00 PM	40.0	153	180	47	0.0	0.0%	33.6
9/11/2024 5:00:00 PM	40.0	130	153	45	0.0	0.0%	34.0
9/11/2024 6:00:00 PM	42.0	166	195	49	0.0	0.0%	35.9
9/11/2024 7:00:00 PM	41.0	202	238	48	0.0	0.0%	35.7
9/11/2024 8:00:00 PM	40.0	182	214	47	0.0	0.0%	34.2
9/11/2024 9:00:00 PM	41.0	95	112	48	0.0	0.0%	36.0
9/11/2024 10:00:00 PM	40.0	83	98	46	0.0	0.0%	33.5
9/11/2024 11:00:00 PM	40.0	37	43	46	0.0	0.0%	33.1
9/12/2024 12:00:00 AM	37.0	20	24	44	0.0	0.0%	31.9
9/12/2024 1:00:00 AM	37.0	11	13	41	0.0	0.0%	34.6
9/12/2024 2:00:00 AM	34.0	5	6	35	0.0	0.0%	28.8
9/12/2024 3:00:00 AM	32.0	4	5	36	0.0	0.0%	27.8
9/12/2024 4:00:00 AM	34.0	1	1	34	0.0	0.0%	34.0
9/12/2024 5:00:00 AM	26.0	2	2	26	0.0	0.0%	18.5
9/12/2024 6:00:00 AM	38.0	3	3	38	0.0	0.0%	34.7
9/12/2024 7:00:00 AM	38.0	12	14	42	0.0	0.0%	32.6
9/12/2024 8:00:00 AM	39.0	20	23	46	0.0	0.0%	34.1
9/12/2024 9:00:00 AM	38.0	137	161	44	0.0	0.0%	32.7
9/12/2024 10:00:00 AM	39.0	82	96	46	0.0	0.0%	32.8
9/12/2024 11:00:00 AM	38.0	62	73	47	0.0	0.0%	33.2
9/12/2024 12:00:00 PM	39.0	67	79	43	0.0	0.0%	32.9
9/12/2024 1:00:00 PM	39.0	76	89	51	0.0	0.0%	32.7
9/12/2024 2:00:00 PM	38.0	89	105	44	0.0	0.0%	31.0

Incoming Summary
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

9/12/2024 3:00:00 PM	39.0	99	116	44	0.0	0.0%	32.7
9/12/2024 4:00:00 PM	39.0	141	166	49	0.0	0.0%	33.6
9/12/2024 5:00:00 PM	40.0	155	182	46	0.0	0.0%	32.7
9/12/2024 6:00:00 PM	39.0	179	211	47	0.0	0.0%	33.9
9/12/2024 7:00:00 PM	38.0	169	199	47	0.0	0.0%	32.7
9/12/2024 8:00:00 PM	39.0	150	177	49	0.0	0.0%	34.2
9/12/2024 9:00:00 PM	38.0	95	112	48	0.0	0.0%	33.2
9/12/2024 10:00:00 PM	37.0	65	76	47	0.0	0.0%	32.1
9/12/2024 11:00:00 PM	35.0	47	55	41	0.0	0.0%	30.9
9/13/2024 12:00:00 AM	36.0	23	27	39	0.0	0.0%	30.1
9/13/2024 1:00:00 AM	37.0	15	18	41	0.0	0.0%	28.3
9/13/2024 2:00:00 AM	42.0	7	8	43	0.0	0.0%	36.4
9/13/2024 3:00:00 AM	37.0	7	8	42	0.0	0.0%	31.1
9/13/2024 4:00:00 AM	34.0	3	3	34	0.0	0.0%	29.7
9/13/2024 5:00:00 AM	**No Data**						
9/13/2024 6:00:00 AM	33.0	3	3	33	0.0	0.0%	31.7
9/13/2024 7:00:00 AM	38.0	10	12	47	0.0	0.0%	31.3
9/13/2024 8:00:00 AM	40.0	27	32	45	0.0	0.0%	31.7
9/13/2024 9:00:00 AM	40.0	125	147	49	0.0	0.0%	34.8
9/13/2024 10:00:00 AM	41.0	65	77	48	0.0	0.0%	35.2
9/13/2024 11:00:00 AM	41.0	65	77	47	0.0	0.0%	35.2
9/13/2024 12:00:00 PM	40.0	94	111	49	0.0	0.0%	33.5
9/13/2024 1:00:00 PM	40.0	86	101	48	0.0	0.0%	34.8
9/13/2024 2:00:00 PM	41.0	127	149	49	0.0	0.0%	34.9
9/13/2024 3:00:00 PM	41.0	163	192	52	0.0	0.0%	35.8
9/13/2024 4:00:00 PM	41.0	163	192	49	0.0	0.0%	35.5
9/13/2024 5:00:00 PM	40.0	164	193	49	0.0	0.0%	34.8
9/13/2024 6:00:00 PM	41.0	190	224	52	0.0	0.0%	35.3
9/13/2024 7:00:00 PM	41.0	218	256	48	0.0	0.0%	35.0
9/13/2024 8:00:00 PM	41.0	150	176	50	0.0	0.0%	34.6
9/13/2024 9:00:00 PM	41.0	91	107	47	0.0	0.0%	35.1
9/13/2024 10:00:00 PM	40.0	94	110	47	0.0	0.0%	34.9
9/13/2024 11:00:00 PM	40.0	51	60	45	0.0	0.0%	33.7
9/14/2024 12:00:00 AM	38.0	26	31	45	0.0	0.0%	32.2
9/14/2024 1:00:00 AM	40.0	16	19	43	0.0	0.0%	34.4
9/14/2024 2:00:00 AM	39.0	12	14	46	0.0	0.0%	36.6
9/14/2024 3:00:00 AM	35.0	10	12	39	0.0	0.0%	30.4
9/14/2024 4:00:00 AM	34.0	2	2	34	0.0	0.0%	33.5
9/14/2024 5:00:00 AM	30.0	1	1	30	0.0	0.0%	30.0
9/14/2024 6:00:00 AM	35.0	10	12	43	0.0	0.0%	28.8
9/14/2024 7:00:00 AM	40.0	8	9	44	0.0	0.0%	32.1
9/14/2024 8:00:00 AM	42.0	19	22	47	0.0	0.0%	34.5
9/14/2024 9:00:00 AM	43.0	37	43	50	0.0	0.0%	36.3
9/14/2024 10:00:00 AM	41.0	61	72	48	0.0	0.0%	35.2
9/14/2024 11:00:00 AM	41.0	76	89	50	0.0	0.0%	35.3
9/14/2024 12:00:00 PM	38.0	70	82	47	0.0	0.0%	33.6
9/14/2024 1:00:00 PM	40.0	72	85	45	0.0	0.0%	33.9
9/14/2024 2:00:00 PM	40.0	101	119	47	0.0	0.0%	34.7

Incoming Summary
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

9/14/2024 3:00:00 PM	38.0	102	120	42	0.0	0.0%	32.3
9/14/2024 4:00:00 PM	39.0	112	132	47	0.0	0.0%	32.4
9/14/2024 5:00:00 PM	39.0	121	142	47	0.0	0.0%	31.8
9/14/2024 6:00:00 PM	38.0	98	115	41	0.0	0.0%	32.9
9/14/2024 7:00:00 PM	40.0	90	106	49	0.0	0.0%	33.1
9/14/2024 8:00:00 PM	40.0	75	88	46	0.0	0.0%	34.0
9/14/2024 9:00:00 PM	39.0	68	80	48	0.0	0.0%	34.3
9/14/2024 10:00:00 PM	38.0	65	76	48	0.0	0.0%	31.3
9/14/2024 11:00:00 PM	38.0	42	49	43	0.0	0.0%	30.1
9/15/2024 12:00:00 AM	37.0	26	31	46	0.0	0.0%	31.8
9/15/2024 1:00:00 AM	33.0	18	21	37	0.0	0.0%	27.9
9/15/2024 2:00:00 AM	36.0	7	8	45	0.0	0.0%	34.9
9/15/2024 3:00:00 AM	29.0	3	4	38	0.0	0.0%	29.8
9/15/2024 4:00:00 AM	47.0	3	3	47	0.0	0.0%	35.0
9/15/2024 5:00:00 AM	32.0	4	5	33	0.0	0.0%	26.8
9/15/2024 6:00:00 AM	36.0	6	7	42	0.0	0.0%	30.7
9/15/2024 7:00:00 AM	34.0	3	4	40	0.0	0.0%	34.3
9/15/2024 8:00:00 AM	34.0	9	11	37	0.0	0.0%	31.8
9/15/2024 9:00:00 AM	39.0	18	21	46	0.0	0.0%	34.5
9/15/2024 10:00:00 AM	39.0	48	57	45	0.0	0.0%	33.4
9/15/2024 11:00:00 AM	40.0	86	101	46	0.0	0.0%	34.2
9/15/2024 12:00:00 PM	40.0	87	102	47	0.0	0.0%	34.2
9/15/2024 1:00:00 PM	41.0	110	130	48	0.0	0.0%	34.6
9/15/2024 2:00:00 PM	40.0	99	116	51	0.0	0.0%	35.3
9/15/2024 3:00:00 PM	42.0	101	119	46	0.0	0.0%	36.2
9/15/2024 4:00:00 PM	41.0	99	117	48	0.0	0.0%	35.6
9/15/2024 5:00:00 PM	40.0	105	124	48	0.0	0.0%	34.6
9/15/2024 6:00:00 PM	42.0	110	130	48	0.0	0.0%	36.2
9/15/2024 7:00:00 PM	43.0	88	104	53	0.0	0.0%	37.2
9/15/2024 8:00:00 PM	42.0	81	95	51	0.0	0.0%	37.1
9/15/2024 9:00:00 PM	40.0	60	71	50	0.0	0.0%	33.8
9/15/2024 10:00:00 PM	40.0	65	77	45	0.0	0.0%	33.5
9/15/2024 11:00:00 PM	42.0	29	34	47	0.0	0.0%	35.7
9/16/2024 12:00:00 AM	39.0	16	19	43	0.0	0.0%	32.2
9/16/2024 1:00:00 AM	39.0	12	14	41	0.0	0.0%	36.0
9/16/2024 2:00:00 AM	40.0	9	11	43	0.0	0.0%	33.9
9/16/2024 3:00:00 AM	36.0	4	5	41	0.0	0.0%	34.4
9/16/2024 4:00:00 AM	37.0	6	7	38	0.0	0.0%	27.7
9/16/2024 5:00:00 AM	40.0	3	4	44	0.0	0.0%	33.3
9/16/2024 6:00:00 AM	40.0	3	4	40	0.0	0.0%	38.3
9/16/2024 7:00:00 AM	39.0	8	9	45	0.0	0.0%	28.7
9/16/2024 8:00:00 AM	40.0	22	26	41	0.0	0.0%	34.7
9/16/2024 9:00:00 AM	41.0	11	13	51	0.0	0.0%	37.0

Incoming Summary
Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/11/2024 12:00:00 AM	39.0	933	1098	52	0.0	0.0%	32.7
9/12/2024 12:00:00 AM	40.0	1727	2032	52	0.0	0.0%	34.1
9/13/2024 12:00:00 AM	39.0	1692	1991	51	0.0	0.0%	32.9
9/14/2024 12:00:00 AM	41.0	1944	2287	52	0.0	0.0%	34.8
9/15/2024 12:00:00 AM	39.0	1292	1520	50	0.0	0.0%	33.3
9/16/2024 12:00:00 AM	41.0	1258	1480	53	0.0	0.0%	35.0
9/16/2024 8:59:59 AM	40.0	79	93	51	0.0	0.0%	34.1

Incoming Weekly Counts

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	16	13	18	19	21	15.67	20	36.33
1 - 2	*	*	3	6	8	14	8	5.67	11	37.33
2 - 3	*	*	5	5	8	12	4	6	8	33.33
3 - 4	*	*	5	1	3	2	3	3	2.5	34
4 - 5	*	*	1	2	0	1	5	1	3	30
5 - 6	*	*	12	3	3	12	7	6	9.5	35.33
6 - 7	*	*	9	14	12	9	4	11.67	6.5	38.67
7 - 8	*	*	25	23	32	22	11	26.67	16.5	38.67
8 - 9	*	*	153	161	147	43	21	153.67	32	38.37
9 - 10	*	*	72	96	77	72	57	81.67	64.5	39.3
10 - 11	*	*	84	73	77	89	101	78	95	39.07
11 - 12	*	*	72	79	111	82	102	87.33	92	39.47
12 - 13	*	*	69	89	101	85	130	86.33	107.5	39.57
13 - 14	*	*	113	105	149	119	116	122.33	117.5	39.13
14 - 15	*	*	136	116	192	120	119	148	119.5	39.83
15 - 16	*	119	180	166	192	132	117	164.25	124.5	39.28
16 - 17	*	156	153	182	193	142	124	171	133	39.35
17 - 18	*	215	195	211	224	115	130	211.25	122.5	39.7
18 - 19	*	178	238	199	256	106	104	217.75	105	39.38
19 - 20	*	177	214	177	176	88	95	186	91.5	39.1
20 - 21	*	110	112	112	107	80	71	110.25	75.5	39.7
21 - 22	*	64	98	76	110	76	77	87	76.5	38.45
22 - 23	*	56	43	55	60	49	34	53.5	41.5	37.83
23 - 24	*	23	24	27	31	31	19	26.25	25	36.92
Totals	0	1098	2032	1991	2287	1520	1480			
% of Total	0%	10.55%	19.52%	19.13%	21.97%	14.6%	14.22%			

Incoming Weekly Counts

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	14	*	*	*	*	*	*	14	0	39
1 - 2	11	*	*	*	*	*	*	11	0	39.5
2 - 3	5	*	*	*	*	*	*	5	0	36
3 - 4	7	*	*	*	*	*	*	7	0	37
4 - 5	4	*	*	*	*	*	*	4	0	40
5 - 6	4	*	*	*	*	*	*	4	0	39.5
6 - 7	9	*	*	*	*	*	*	9	0	39
7 - 8	26	*	*	*	*	*	*	26	0	39.5
8 - 9	13	*	*	*	*	*	*	13	0	41
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	*	*	*	0	0	0
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	93	0	0	0	0	0	0			
% of Total	100%	0%	0%	0%	0%	0%	0%			

Incoming Monthly Counts

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	14	*	16	13	18	19	21	15.25	20	37
1 - 2	11	*	3	6	8	14	8	7	11	37.88
2 - 3	5	*	5	5	8	12	4	5.75	8	34
3 - 4	7	*	5	1	3	2	3	4	2.5	34.75
4 - 5	4	*	1	2	0	1	5	1.75	3	33.33
5 - 6	4	*	12	3	3	12	7	5.5	9.5	36.38
6 - 7	9	*	9	14	12	9	4	11	6.5	38.75
7 - 8	26	*	25	23	32	22	11	26.5	16.5	38.88
8 - 9	13	*	153	161	147	43	21	118.5	32	39.02
9 - 10	*	*	72	96	77	72	57	81.67	64.5	39.3
10 - 11	*	*	84	73	77	89	101	78	95	39.07
11 - 12	*	*	72	79	111	82	102	87.33	92	39.47
12 - 13	*	*	69	89	101	85	130	86.33	107.5	39.57
13 - 14	*	*	113	105	149	119	116	122.33	117.5	39.13
14 - 15	*	*	136	116	192	120	119	148	119.5	39.83
15 - 16	*	119	180	166	192	132	117	164.25	124.5	39.28
16 - 17	*	156	153	182	193	142	124	171	133	39.35
17 - 18	*	215	195	211	224	115	130	211.25	122.5	39.7
18 - 19	*	178	238	199	256	106	104	217.75	105	39.38
19 - 20	*	177	214	177	176	88	95	186	91.5	39.1
20 - 21	*	110	112	112	107	80	71	110.25	75.5	39.7
21 - 22	*	64	98	76	110	76	77	87	76.5	38.45
22 - 23	*	56	43	55	60	49	34	53.5	41.5	37.83
23 - 24	*	23	24	27	31	31	19	26.25	25	36.92
Totals	93	1098	2032	1991	2287	1520	1480			
% of Total	0.89%	10.46%	19.35%	18.96%	21.78%	14.47%	14.09%			

Incoming Weekly Speeds

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	28.56	34.62	28.33	34.42	27.9	30.15	31	36.33
1 - 2	*	*	30.33	28.83	36.38	36.57	34.88	32.65	35.95	37.33
2 - 3	*	*	26.8	27.8	31.12	30.42	29.75	29	30.25	33.33
3 - 4	*	*	28	34	29.67	33.5	35	29.22	34.4	34
4 - 5	*	*	34	18.5	0	30	26.8	23.67	27.33	30
5 - 6	*	*	30.42	34.67	31.67	28.75	30.71	31.33	29.47	35.33
6 - 7	*	*	33.56	32.57	31.25	32.11	34.25	32.37	32.77	38.67
7 - 8	*	*	33.2	34.13	31.72	34.45	31.82	32.88	33.58	38.67
8 - 9	*	*	31.68	32.73	34.8	36.35	34.48	33.04	35.73	38.37
9 - 10	*	*	33.26	32.84	35.21	35.19	33.44	33.71	34.42	39.3
10 - 11	*	*	33.69	33.22	35.17	35.3	34.21	34.03	34.72	39.07
11 - 12	*	*	32.99	32.87	33.53	33.57	34.21	33.18	33.92	39.47
12 - 13	*	*	35.88	32.73	34.83	33.94	34.55	34.39	34.31	39.57
13 - 14	*	*	33.78	31.05	34.86	34.72	35.34	33.44	35.03	39.13
14 - 15	*	*	34.14	32.72	35.76	32.25	36.24	34.47	34.24	39.83
15 - 16	*	32.39	33.56	33.63	35.46	32.44	35.62	33.92	33.94	39.28
16 - 17	*	32.73	33.97	32.67	34.83	31.82	34.56	33.58	33.09	39.35
17 - 18	*	32.66	35.86	33.89	35.27	32.87	36.25	34.4	34.66	39.7
18 - 19	*	33.3	35.69	32.74	34.95	33.1	37.24	34.31	35.15	39.38
19 - 20	*	31.7	34.17	34.23	34.56	33.97	37.05	33.69	35.57	39.1
20 - 21	*	33.65	36.03	33.23	35.06	34.26	33.77	34.49	34.03	39.7
21 - 22	*	33.88	33.52	32.13	34.89	31.3	33.47	33.72	32.39	38.45
22 - 23	*	31.27	33.14	30.91	33.68	30.14	35.71	32.23	32.42	37.83
23 - 24	*	32.26	31.88	30.11	32.16	31.84	32.21	31.59	31.98	36.92
Totals	0	32.7	34.1	32.9	34.8	33.3	35			
% of Total	0%	16.12%	16.81%	16.22%	17.16%	16.42%	17.26%			

Incoming Weekly Speeds
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	36	*	*	*	*	*	*	36	*	39
1 - 2	33.91	*	*	*	*	*	*	33.91	*	39.5
2 - 3	34.4	*	*	*	*	*	*	34.4	*	36
3 - 4	27.71	*	*	*	*	*	*	27.71	*	37
4 - 5	33.25	*	*	*	*	*	*	33.25	*	40
5 - 6	38.25	*	*	*	*	*	*	38.25	*	39.5
6 - 7	28.67	*	*	*	*	*	*	28.67	*	39
7 - 8	34.69	*	*	*	*	*	*	34.69	*	39.5
8 - 9	37	*	*	*	*	*	*	37	*	41
9 - 10	*	*	*	*	*	*	*	*	*	0
10 - 11	*	*	*	*	*	*	*	*	*	0
11 - 12	*	*	*	*	*	*	*	*	*	0
12 - 13	*	*	*	*	*	*	*	*	*	0
13 - 14	*	*	*	*	*	*	*	*	*	0
14 - 15	*	*	*	*	*	*	*	*	*	0
15 - 16	*	*	*	*	*	*	*	*	*	0
16 - 17	*	*	*	*	*	*	*	*	*	0
17 - 18	*	*	*	*	*	*	*	*	*	0
18 - 19	*	*	*	*	*	*	*	*	*	0
19 - 20	*	*	*	*	*	*	*	*	*	0
20 - 21	*	*	*	*	*	*	*	*	*	0
21 - 22	*	*	*	*	*	*	*	*	*	0
22 - 23	*	*	*	*	*	*	*	*	*	0
23 - 24	*	*	*	*	*	*	*	*	*	0
Totals	34.1	0	0	0	0	0	0			
% of Total	100%	0%	0%	0%	0%	0%	0%			

Incoming Monthly Speeds

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	36	*	28.56	34.62	28.33	34.42	27.9	31.49	31	37
1 - 2	33.91	*	30.33	28.83	36.38	36.57	34.88	33.14	35.95	37.88
2 - 3	34.4	*	26.8	27.8	31.12	30.42	29.75	30.17	30.25	34
3 - 4	27.71	*	28	34	29.67	33.5	35	28.56	34.4	34.75
4 - 5	33.25	*	34	18.5	0	30	26.8	29.14	27.33	33.33
5 - 6	38.25	*	30.42	34.67	31.67	28.75	30.71	32.59	29.47	36.38
6 - 7	28.67	*	33.56	32.57	31.25	32.11	34.25	31.61	32.77	38.75
7 - 8	34.69	*	33.2	34.13	31.72	34.45	31.82	33.32	33.58	38.88
8 - 9	37	*	31.68	32.73	34.8	36.35	34.48	33.15	35.73	39.02
9 - 10	*	*	33.26	32.84	35.21	35.19	33.44	33.71	34.42	39.3
10 - 11	*	*	33.69	33.22	35.17	35.3	34.21	34.03	34.72	39.07
11 - 12	*	*	32.99	32.87	33.53	33.57	34.21	33.18	33.92	39.47
12 - 13	*	*	35.88	32.73	34.83	33.94	34.55	34.39	34.31	39.57
13 - 14	*	*	33.78	31.05	34.86	34.72	35.34	33.44	35.03	39.13
14 - 15	*	*	34.14	32.72	35.76	32.25	36.24	34.47	34.24	39.83
15 - 16	*	32.39	33.56	33.63	35.46	32.44	35.62	33.92	33.94	39.28
16 - 17	*	32.73	33.97	32.67	34.83	31.82	34.56	33.58	33.09	39.35
17 - 18	*	32.66	35.86	33.89	35.27	32.87	36.25	34.4	34.66	39.7
18 - 19	*	33.3	35.69	32.74	34.95	33.1	37.24	34.31	35.15	39.38
19 - 20	*	31.7	34.17	34.23	34.56	33.97	37.05	33.69	35.57	39.1
20 - 21	*	33.65	36.03	33.23	35.06	34.26	33.77	34.49	34.03	39.7
21 - 22	*	33.88	33.52	32.13	34.89	31.3	33.47	33.72	32.39	38.45
22 - 23	*	31.27	33.14	30.91	33.68	30.14	35.71	32.23	32.42	37.83
23 - 24	*	32.26	31.88	30.11	32.16	31.84	32.21	31.59	31.98	36.92
Totals	34.1	32.7	34.1	32.9	34.8	33.3	35			
% of Total	14.39%	13.8%	14.39%	13.89%	14.69%	14.06%	14.77%			

Incoming Weekly EightyFifthSpeeds

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	35	37	37	40	32.7	36.33	36.35	36.33
1 - 2	*	*	36	34	42	39	36	37.33	37.5	37.33
2 - 3	*	*	31	32	37	35	29	33.33	32	33.33
3 - 4	*	*	34	34	34	34	47	34	40.5	34
4 - 5	*	*	34	26	0	30	32	30	31	30
5 - 6	*	*	35	38	33	35	36	35.33	35.5	35.33
6 - 7	*	*	40	38	38	40	34	38.67	37	38.67
7 - 8	*	*	37	39	40	42	34	38.67	38	38.67
8 - 9	*	*	38.1	37.7	39.3	42.3	38.5	38.37	40.4	38.37
9 - 10	*	*	37.9	39	41	41	38.2	39.3	39.6	39.3
10 - 11	*	*	38.8	38	40.4	41	39.5	39.07	40.25	39.07
11 - 12	*	*	40.5	38.5	39.4	38	39.7	39.47	38.85	39.47
12 - 13	*	*	40.5	38.2	40	39.7	40.9	39.57	40.3	39.57
13 - 14	*	*	39.1	37.6	40.7	39.8	39.8	39.13	39.8	39.13
14 - 15	*	*	40.1	38.5	40.9	37.4	41.2	39.83	39.3	39.83
15 - 16	*	38.5	39.5	38.5	40.6	38.4	40.9	39.28	39.65	39.28
16 - 17	*	39.3	39.1	39.1	39.9	38.1	39.6	39.35	38.85	39.35
17 - 18	*	38.2	41.3	38.6	40.7	37.7	41.4	39.7	39.55	39.7
18 - 19	*	38.9	41	37.5	40.1	39.5	42.8	39.38	41.15	39.38
19 - 20	*	37.5	39.7	38.5	40.7	39.5	41.6	39.1	40.55	39.1
20 - 21	*	39.5	40.5	38	40.8	38.2	39.6	39.7	38.9	39.7
21 - 22	*	38	39.2	37	39.6	38	39.8	38.45	38.9	38.45
22 - 23	*	37	40	34.8	39.5	38	42	37.83	40	37.83
23 - 24	*	36.7	37	36	38	37	39	36.92	38	36.92
Totals	0	343.6	914.3	883.5	902.6	918.6	925.2			
% of Total	0%	7.03%	18.71%	18.08%	18.47%	18.79%	18.93%			

Incoming Weekly EightyFifthSpeeds
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	39	*	*	*	*	*	*	39	0	39
1 - 2	39.5	*	*	*	*	*	*	39.5	0	39.5
2 - 3	36	*	*	*	*	*	*	36	0	36
3 - 4	37	*	*	*	*	*	*	37	0	37
4 - 5	40	*	*	*	*	*	*	40	0	40
5 - 6	39.5	*	*	*	*	*	*	39.5	0	39.5
6 - 7	39	*	*	*	*	*	*	39	0	39
7 - 8	39.5	*	*	*	*	*	*	39.5	0	39.5
8 - 9	41	*	*	*	*	*	*	41	0	41
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	*	*	*	0	0	0
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	350.5	0	0	0	0	0	0			
% of Total	100%	0%	0%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	39	*	35	37	37	40	32.7	37	36.35	37
1 - 2	39.5	*	36	34	42	39	36	37.88	37.5	37.88
2 - 3	36	*	31	32	37	35	29	34	32	34
3 - 4	37	*	34	34	34	34	47	34.75	40.5	34.75
4 - 5	40	*	34	26	0	30	32	33.33	31	33.33
5 - 6	39.5	*	35	38	33	35	36	36.38	35.5	36.38
6 - 7	39	*	40	38	38	40	34	38.75	37	38.75
7 - 8	39.5	*	37	39	40	42	34	38.88	38	38.88
8 - 9	41	*	38.1	37.7	39.3	42.3	38.5	39.02	40.4	39.02
9 - 10	*	*	37.9	39	41	41	38.2	39.3	39.6	39.3
10 - 11	*	*	38.8	38	40.4	41	39.5	39.07	40.25	39.07
11 - 12	*	*	40.5	38.5	39.4	38	39.7	39.47	38.85	39.47
12 - 13	*	*	40.5	38.2	40	39.7	40.9	39.57	40.3	39.57
13 - 14	*	*	39.1	37.6	40.7	39.8	39.8	39.13	39.8	39.13
14 - 15	*	*	40.1	38.5	40.9	37.4	41.2	39.83	39.3	39.83
15 - 16	*	38.5	39.5	38.5	40.6	38.4	40.9	39.28	39.65	39.28
16 - 17	*	39.3	39.1	39.1	39.9	38.1	39.6	39.35	38.85	39.35
17 - 18	*	38.2	41.3	38.6	40.7	37.7	41.4	39.7	39.55	39.7
18 - 19	*	38.9	41	37.5	40.1	39.5	42.8	39.38	41.15	39.38
19 - 20	*	37.5	39.7	38.5	40.7	39.5	41.6	39.1	40.55	39.1
20 - 21	*	39.5	40.5	38	40.8	38.2	39.6	39.7	38.9	39.7
21 - 22	*	38	39.2	37	39.6	38	39.8	38.45	38.9	38.45
22 - 23	*	37	40	34.8	39.5	38	42	37.83	40	37.83
23 - 24	*	36.7	37	36	38	37	39	36.92	38	36.92

Summary of Violators

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	101	31.3	0	0.0
01:00:00	50	34.4	0	0.0
02:00:00	39	30.2	0	0.0
03:00:00	21	30.0	0	0.0
04:00:00	13	28.3	0	0.0
05:00:00	41	31.1	0	0.0
06:00:00	57	31.9	0	0.0
07:00:00	139	33.4	0	0.0
08:00:00	538	33.5	0	0.0
09:00:00	374	34.0	0	0.0
10:00:00	424	34.3	0	0.0
11:00:00	446	33.5	0	0.0
12:00:00	474	34.4	0	0.0
13:00:00	602	34.1	0	0.0
14:00:00	683	34.4	0	0.0
15:00:00	906	33.9	0	0.0
16:00:00	950	33.4	0	0.0
17:00:00	1090	34.5	0	0.0
18:00:00	1081	34.5	0	0.0
19:00:00	927	34.1	0	0.0
20:00:00	592	34.4	0	0.0
21:00:00	501	33.3	0	0.0
22:00:00	297	32.3	0	0.0
23:00:00	155	31.7	0	0.0

Incoming Histogram

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	15:00	8	1	7	12	40	36	13	2	0	0	0	0	0	0	0	0	0	0	0	119	32.4	38.5	29 to 39	69.7	*	*	1	115	3
9/10/2024	16:00	12	4	1	16	43	53	26	1	0	0	0	0	0	0	0	0	0	0	0	156	32.7	39.3	31 to 41	70.5	*	*	0	150	6
9/10/2024	17:00	12	4	6	22	61	87	22	1	0	0	0	0	0	0	0	0	0	0	0	215	32.7	38.2	30 to 40	72.1	*	*	1	211	3
9/10/2024	18:00	11	0	1	15	60	65	23	3	0	0	0	0	0	0	0	0	0	0	0	178	33.3	38.9	30 to 40	74.7	*	*	0	175	3
9/10/2024	19:00	16	2	3	22	60	61	12	1	0	0	0	0	0	0	0	0	0	0	0	177	31.7	37.5	29 to 39	73.4	*	*	0	175	2
9/10/2024	20:00	7	2	2	7	34	40	14	2	2	0	0	0	0	0	0	0	0	0	0	110	33.7	39.5	30 to 40	70.9	*	*	0	107	3
9/10/2024	21:00	1	0	1	4	32	21	5	0	0	0	0	0	0	0	0	0	0	0	0	64	33.9	38	30 to 40	87.5	*	*	0	63	1
9/10/2024	22:00	6	0	1	9	17	17	5	1	0	0	0	0	0	0	0	0	0	0	0	56	31.3	37	26 to 36	69.6	*	*	0	55	1
9/10/2024	23:00	0	2	1	3	8	7	2	0	0	0	0	0	0	0	0	0	0	0	0	23	32.3	36.7	27 to 37	78.3	*	*	0	23	0
24 Hr Summary		73	15	23	110	355	387	122	11	2	0	0	0	0	0	0	0	0	0	1098	32.7	39	29 to 39	71.2	*	*	2	1074	22	

Incoming Histogram

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/11/2024	00:00	1	1	1	6	4	1	2	0	0	0	0	0	0	0	0	0	0	0	16	28.6	35	23 to 33	68.8	*	*	0	16	0
9/11/2024	01:00	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	30.3	36	21 to 31	66.7	*	*	0	3	0
9/11/2024	02:00	1	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	5	26.8	31	21 to 31	60.0	*	*	0	5	0
9/11/2024	03:00	1	0	1	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	5	28	34	30 to 40	60.0	*	*	0	5	0
9/11/2024	04:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	34	34	24 to 34	100.0	*	*	0	1	0
9/11/2024	05:00	1	0	0	4	4	1	2	0	0	0	0	0	0	0	0	0	0	0	12	30.4	35	25 to 35	75.0	*	*	0	10	2
9/11/2024	06:00	0	0	1	1	3	2	2	0	0	0	0	0	0	0	0	0	0	0	9	33.6	40	30 to 40	66.7	*	*	0	9	0
9/11/2024	07:00	1	0	0	2	11	11	0	0	0	0	0	0	0	0	0	0	0	0	25	33.2	37	29 to 39	92.0	*	*	0	25	0
9/11/2024	08:00	6	3	13	31	40	43	15	2	0	0	0	0	0	0	0	0	0	0	153	31.7	38.1	28 to 38	61.4	*	*	0	150	3
9/11/2024	09:00	2	1	5	8	19	28	6	2	1	0	0	0	0	0	0	0	0	0	72	33.3	37.9	28 to 38	69.4	*	*	1	66	5
9/11/2024	10:00	2	0	2	11	31	26	11	1	0	0	0	0	0	0	0	0	0	0	84	33.7	38.8	30 to 40	77.4	*	*	0	80	4
9/11/2024	11:00	5	1	3	7	20	19	15	2	0	0	0	0	0	0	0	0	0	0	72	33	40.5	30 to 40	61.1	*	*	0	69	3
9/11/2024	12:00	0	0	0	4	27	24	10	4	0	0	0	0	0	0	0	0	0	0	69	35.9	40.5	30 to 40	78.3	*	*	0	60	9
9/11/2024	13:00	3	0	4	16	33	39	17	1	0	0	0	0	0	0	0	0	0	0	113	33.8	39.1	29 to 39	71.7	*	*	1	105	7
9/11/2024	14:00	6	3	5	10	30	57	22	3	0	0	0	0	0	0	0	0	0	0	136	34.1	40.1	31 to 41	70.6	*	*	0	126	10
9/11/2024	15:00	8	1	4	21	55	60	28	3	0	0	0	0	0	0	0	0	0	0	180	33.6	39.5	31 to 41	70.0	*	*	0	176	4
9/11/2024	16:00	7	4	2	9	40	67	23	1	0	0	0	0	0	0	0	0	0	0	153	34	39.1	30 to 40	74.5	*	*	0	152	1
9/11/2024	17:00	6	2	2	9	45	77	44	10	0	0	0	0	0	0	0	0	0	0	195	35.9	41.3	31 to 41	74.4	*	*	0	193	2
9/11/2024	18:00	7	0	4	16	52	100	47	12	0	0	0	0	0	0	0	0	0	0	238	35.7	41	33 to 43	71.4	*	*	0	233	5
9/11/2024	19:00	8	2	6	18	61	77	38	4	0	0	0	0	0	0	0	0	0	0	214	34.2	39.7	30 to 40	71.0	*	*	1	211	2
9/11/2024	20:00	2	1	2	4	25	52	22	4	0	0	0	0	0	0	0	0	0	0	112	36	40.5	31 to 41	77.7	*	*	0	108	4
9/11/2024	21:00	4	1	3	10	31	33	15	1	0	0	0	0	0	0	0	0	0	0	98	33.5	39.2	30 to 40	71.4	*	*	0	97	1
9/11/2024	22:00	0	3	3	3	14	13	5	2	0	0	0	0	0	0	0	0	0	0	43	33.1	40	29 to 39	65.1	*	*	0	42	1
9/11/2024	23:00	1	0	2	3	8	9	1	0	0	0	0	0	0	0	0	0	0	0	24	31.9	37	28 to 38	79.2	*	*	0	24	0
24 Hr Summary		72	23	64	194	559	741	326	52	1	0	0	0	0	0	0	0	0	0	2032	34.1	40	31 to 41	69.2	*	*	3	1966	63

Incoming Histogram

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/12/2024	00:00	0	0	0	1	6	5	1	0	0	0	0	0	0	0	0	0	0	0	13	34.6	37	28 to 38	92.3	*	*	0	13	0
9/12/2024	01:00	1	0	0	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	6	28.8	34	25 to 35	83.3	*	*	0	5	1
9/12/2024	02:00	0	0	2	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	5	27.8	32	22 to 32	80.0	*	*	0	5	0
9/12/2024	03:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	34	34	24 to 34	100.0	*	*	0	1	0
9/12/2024	04:00	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	18.5	26	1 to 11	50.0	*	*	0	2	0
9/12/2024	05:00	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3	34.7	38	28 to 38	100.0	*	*	0	2	1
9/12/2024	06:00	1	0	0	3	4	5	1	0	0	0	0	0	0	0	0	0	0	0	14	32.6	38	28 to 38	78.6	*	*	0	12	2
9/12/2024	07:00	1	0	0	2	7	10	1	2	0	0	0	0	0	0	0	0	0	0	23	34.1	39	29 to 39	73.9	*	*	0	21	2
9/12/2024	08:00	4	3	3	27	55	55	14	0	0	0	0	0	0	0	0	0	0	0	161	32.7	37.7	29 to 39	74.5	*	*	0	157	4
9/12/2024	09:00	2	4	6	8	32	30	13	1	0	0	0	0	0	0	0	0	0	0	96	32.8	39	31 to 41	70.8	*	*	2	83	11
9/12/2024	10:00	2	2	2	8	25	26	6	2	0	0	0	0	0	0	0	0	0	0	73	33.2	38	28 to 38	74.0	*	*	0	70	3
9/12/2024	11:00	1	2	2	14	27	22	11	0	0	0	0	0	0	0	0	0	0	0	79	32.9	38.5	27 to 37	73.4	*	*	0	74	5
9/12/2024	12:00	3	1	3	14	31	27	8	1	1	0	0	0	0	0	0	0	0	0	89	32.7	38.2	27 to 37	73.0	*	*	2	82	5
9/12/2024	13:00	6	4	5	18	28	35	9	0	0	0	0	0	0	0	0	0	0	0	105	31	37.6	29 to 39	63.8	*	*	0	100	5
9/12/2024	14:00	1	2	4	23	44	28	14	0	0	0	0	0	0	0	0	0	0	0	116	32.7	38.5	27 to 37	69.8	*	*	0	113	3
9/12/2024	15:00	2	2	6	20	52	63	18	3	0	0	0	0	0	0	0	0	0	0	166	33.6	38.5	30 to 40	73.5	*	*	0	162	4
9/12/2024	16:00	10	3	7	18	63	53	26	2	0	0	0	0	0	0	0	0	0	0	182	32.7	39.1	31 to 41	70.3	*	*	0	177	5
9/12/2024	17:00	8	0	4	22	60	90	24	3	0	0	0	0	0	0	0	0	0	0	211	33.9	38.6	30 to 40	76.3	*	*	0	206	5
9/12/2024	18:00	8	3	7	17	75	73	14	2	0	0	0	0	0	0	0	0	0	0	199	32.7	37.5	29 to 39	76.4	*	*	0	197	2
9/12/2024	19:00	3	2	4	18	55	74	18	3	0	0	0	0	0	0	0	0	0	0	177	34.2	38.5	29 to 39	76.8	*	*	0	175	2
9/12/2024	20:00	5	2	2	10	31	51	9	2	0	0	0	0	0	0	0	0	0	0	112	33.2	38	30 to 40	77.7	*	*	0	110	2
9/12/2024	21:00	3	1	0	14	29	24	4	1	0	0	0	0	0	0	0	0	0	0	76	32.1	37	28 to 38	75.0	*	*	0	74	2
9/12/2024	22:00	2	0	6	9	25	10	3	0	0	0	0	0	0	0	0	0	0	0	55	30.9	34.8	25 to 35	72.7	*	*	0	55	0
9/12/2024	23:00	3	0	0	6	10	8	0	0	0	0	0	0	0	0	0	0	0	0	27	30.1	36	27 to 37	77.8	*	*	0	27	0
24 Hr Summary		67	31	63	254	666	693	194	22	1	0	0	0	0	0	0	0	0	0	1991	32.9	39	29 to 39	72.1	*	*	4	1923	64

Incoming Histogram

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/13/2024	00:00	3	0	1	5	5	2	2	0	0	0	0	0	0	0	0	0	0	0	18	28.3	37	23 to 33	55.6	*	*	0	17	1	
9/13/2024	01:00	0	0	0	1	2	2	3	0	0	0	0	0	0	0	0	0	0	0	8	36.4	42	33 to 43	75.0	*	*	0	7	1	
9/13/2024	02:00	1	0	0	1	3	2	1	0	0	0	0	0	0	0	0	0	0	0	8	31.1	37	27 to 37	75.0	*	*	0	8	0	
9/13/2024	03:00	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	29.7	34	24 to 34	100.0	*	*	0	3	0	
9/13/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/13/2024	05:00	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3	31.7	33	23 to 33	100.0	*	*	0	3	0	
9/13/2024	06:00	1	0	1	3	3	3	0	1	0	0	0	0	0	0	0	0	0	0	12	31.3	38	28 to 38	66.7	*	*	0	10	2	
9/13/2024	07:00	1	4	1	5	8	7	5	1	0	0	0	0	0	0	0	0	0	0	32	31.7	40	28 to 38	56.3	*	*	0	31	1	
9/13/2024	08:00	0	1	2	17	48	55	19	5	0	0	0	0	0	0	0	0	0	0	147	34.8	39.3	29 to 39	74.1	*	*	0	144	3	
9/13/2024	09:00	1	1	2	8	21	25	16	3	0	0	0	0	0	0	0	0	0	0	77	35.2	41	32 to 42	67.5	*	*	1	73	3	
9/13/2024	10:00	3	1	2	6	16	26	20	3	0	0	0	0	0	0	0	0	0	0	77	35.2	40.4	31 to 41	71.4	*	*	0	72	5	
9/13/2024	11:00	5	0	7	13	27	40	14	5	0	0	0	0	0	0	0	0	0	0	111	33.5	39.4	31 to 41	66.7	*	*	0	104	7	
9/13/2024	12:00	6	1	0	4	22	49	15	4	0	0	0	0	0	0	0	0	0	0	101	34.8	40	31 to 41	75.2	*	*	1	95	5	
9/13/2024	13:00	7	2	2	7	36	58	34	3	0	0	0	0	0	0	0	0	0	0	149	34.9	40.7	31 to 41	72.5	*	*	0	147	2	
9/13/2024	14:00	5	3	5	5	37	88	37	10	2	0	0	0	0	0	0	0	0	0	192	35.8	40.9	31 to 41	72.4	*	*	0	185	7	
9/13/2024	15:00	7	1	3	6	53	75	41	6	0	0	0	0	0	0	0	0	0	0	192	35.5	40.6	31 to 41	76.6	*	*	0	190	2	
9/13/2024	16:00	5	3	4	15	50	78	30	8	0	0	0	0	0	0	0	0	0	0	193	34.8	39.9	31 to 41	73.6	*	*	0	186	7	
9/13/2024	17:00	9	2	1	19	51	82	53	6	1	0	0	0	0	0	0	0	0	0	224	35.3	40.7	32 to 42	72.3	*	*	0	221	3	
9/13/2024	18:00	10	4	2	18	59	101	55	7	0	0	0	0	0	0	0	0	0	0	256	35	40.1	31 to 41	72.7	*	*	1	250	5	
9/13/2024	19:00	8	1	6	16	41	56	39	8	1	0	0	0	0	0	0	0	0	0	176	34.6	40.7	31 to 41	68.2	*	*	0	172	4	
9/13/2024	20:00	4	1	0	7	28	45	18	4	0	0	0	0	0	0	0	0	0	0	107	35.1	40.8	30 to 40	70.1	*	*	1	105	1	
9/13/2024	21:00	4	1	0	11	22	50	18	4	0	0	0	0	0	0	0	0	0	0	110	34.9	39.6	31 to 41	76.4	*	*	0	109	1	
9/13/2024	22:00	2	1	3	4	19	21	7	3	0	0	0	0	0	0	0	0	0	0	60	33.7	39.5	28 to 38	71.7	*	*	0	60	0	
9/13/2024	23:00	1	2	0	5	9	10	3	1	0	0	0	0	0	0	0	0	0	0	31	32.2	38	29 to 39	67.7	*	*	0	31	0	
24 Hr Summary		83	29	42	177	565	875	430	82	4	0	0	0	0	0	0	0	0	0	2287	34.8	41	31 to 41	71.0	*	*	4	2223	60	

Incoming Histogram
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/14/2024	00:00	1	0	1	1	4	8	4	0	0	0	0	0	0	0	0	0	0	0	19	34.4	40	33 to 43	84.2	*	*	0	18	1
9/14/2024	01:00	0	0	0	0	3	9	1	1	0	0	0	0	0	0	0	0	0	0	14	36.6	39	30 to 40	92.9	*	*	0	14	0
9/14/2024	02:00	0	1	2	1	5	3	0	0	0	0	0	0	0	0	0	0	0	0	12	30.4	35	27 to 37	66.7	*	*	0	12	0
9/14/2024	03:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	33.5	34	24 to 34	100.0	*	*	0	2	0
9/14/2024	04:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	30	30	20 to 30	100.0	*	*	0	1	0
9/14/2024	05:00	2	1	0	1	4	3	1	0	0	0	0	0	0	0	0	0	0	0	12	28.8	35	27 to 37	66.7	*	*	0	12	0
9/14/2024	06:00	1	0	0	1	2	3	2	0	0	0	0	0	0	0	0	0	0	0	9	32.1	40	30 to 40	66.7	*	*	0	9	0
9/14/2024	07:00	1	1	0	0	8	6	4	2	0	0	0	0	0	0	0	0	0	0	22	34.5	42	30 to 40	72.7	*	*	0	19	3
9/14/2024	08:00	1	0	0	2	13	17	7	2	1	0	0	0	0	0	0	0	0	0	43	36.3	42.3	31 to 41	76.7	*	*	0	42	1
9/14/2024	09:00	5	0	0	7	8	24	24	4	0	0	0	0	0	0	0	0	0	0	72	35.2	41	35 to 45	69.4	*	*	1	68	3
9/14/2024	10:00	5	0	1	6	21	32	17	6	1	0	0	0	0	0	0	0	0	0	89	35.3	41	33 to 43	68.5	*	*	1	86	2
9/14/2024	11:00	2	2	2	8	30	30	6	2	0	0	0	0	0	0	0	0	0	0	82	33.6	38	31 to 41	79.3	*	*	0	77	5
9/14/2024	12:00	2	1	2	10	28	25	16	1	0	0	0	0	0	0	0	0	0	0	85	33.9	39.7	28 to 38	70.6	*	*	2	79	4
9/14/2024	13:00	2	1	1	12	39	43	16	5	0	0	0	0	0	0	0	0	0	0	119	34.7	39.8	30 to 40	72.3	*	*	0	115	4
9/14/2024	14:00	4	3	7	14	38	44	10	0	0	0	0	0	0	0	0	0	0	0	120	32.3	37.4	30 to 40	74.2	*	*	1	112	7
9/14/2024	15:00	7	4	4	16	40	46	14	1	0	0	0	0	0	0	0	0	0	0	132	32.4	38.4	29 to 39	67.4	*	*	0	128	4
9/14/2024	16:00	9	3	5	18	48	47	11	1	0	0	0	0	0	0	0	0	0	0	142	31.8	38.1	29 to 39	70.4	*	*	0	138	4
9/14/2024	17:00	3	1	7	12	33	50	9	0	0	0	0	0	0	0	0	0	0	0	115	32.9	37.7	29 to 39	75.7	*	*	0	114	1
9/14/2024	18:00	7	3	1	11	30	36	15	3	0	0	0	0	0	0	0	0	0	0	106	33.1	39.5	31 to 41	67.9	*	*	0	105	1
9/14/2024	19:00	3	0	2	8	31	29	14	1	0	0	0	0	0	0	0	0	0	0	88	34	39.5	30 to 40	72.7	*	*	0	87	1
9/14/2024	20:00	0	1	1	6	33	30	7	2	0	0	0	0	0	0	0	0	0	0	80	34.3	38.2	30 to 40	83.8	*	*	0	80	0
9/14/2024	21:00	6	1	6	10	22	25	5	1	0	0	0	0	0	0	0	0	0	0	76	31.3	38	31 to 41	67.1	*	*	0	76	0
9/14/2024	22:00	3	1	7	9	12	14	3	0	0	0	0	0	0	0	0	0	0	0	49	30.1	38	26 to 36	55.1	*	*	0	49	0
9/14/2024	23:00	1	0	2	6	12	7	2	1	0	0	0	0	0	0	0	0	0	0	31	31.8	37	25 to 35	71.0	*	*	0	31	0
24 Hr Summary		65	24	51	159	467	531	188	33	2	0	0	0	0	0	0	0	0	0	1520	33.3	39	30 to 40	69.7	*	*	5	1474	41

Incoming Histogram

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/15/2024	00:00	2	0	2	6	10	1	0	0	0	0	0	0	0	0	0	0	0	0	0	21	27.9	32.7	24 to 34	81.0	*	*	0	21	0
9/15/2024	01:00	0	0	0	0	5	2	0	1	0	0	0	0	0	0	0	0	0	0	0	8	34.9	36	26 to 36	87.5	*	*	0	8	0
9/15/2024	02:00	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	29.8	29	19 to 29	75.0	*	*	0	4	0
9/15/2024	03:00	0	0	0	1	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	3	35	47	21 to 31	66.7	*	*	0	2	1
9/15/2024	04:00	1	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	26.8	32	23 to 33	80.0	*	*	0	5	0
9/15/2024	05:00	1	0	0	0	3	2	1	0	0	0	0	0	0	0	0	0	0	0	0	7	30.7	36	26 to 36	71.4	*	*	0	6	1
9/15/2024	06:00	0	0	0	1	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	4	34.3	34	24 to 34	75.0	*	*	0	4	0
9/15/2024	07:00	0	0	1	1	7	2	0	0	0	0	0	0	0	0	0	0	0	0	0	11	31.8	34	24 to 34	81.8	*	*	0	11	0
9/15/2024	08:00	0	1	0	4	2	12	0	2	0	0	0	0	0	0	0	0	0	0	0	21	34.5	38.5	29 to 39	71.4	*	*	0	20	1
9/15/2024	09:00	2	0	0	7	21	22	4	1	0	0	0	0	0	0	0	0	0	0	0	57	33.4	38.2	29 to 39	77.2	*	*	1	54	2
9/15/2024	10:00	5	0	2	2	34	40	17	1	0	0	0	0	0	0	0	0	0	0	0	101	34.2	39.5	30 to 40	79.2	*	*	1	100	0
9/15/2024	11:00	4	1	3	8	27	36	22	1	0	0	0	0	0	0	0	0	0	0	0	102	34.2	39.7	31 to 41	74.5	*	*	0	101	1
9/15/2024	12:00	4	5	0	8	42	40	23	8	0	0	0	0	0	0	0	0	0	0	0	130	34.6	40.9	31 to 41	67.7	*	*	1	127	2
9/15/2024	13:00	3	1	1	7	32	48	22	1	1	0	0	0	0	0	0	0	0	0	0	116	35.3	39.8	32 to 42	79.3	*	*	0	115	1
9/15/2024	14:00	0	2	0	6	33	49	25	4	0	0	0	0	0	0	0	0	0	0	0	119	36.2	41.2	30 to 40	73.1	*	*	0	116	3
9/15/2024	15:00	5	0	1	8	24	42	34	3	0	0	0	0	0	0	0	0	0	0	0	117	35.6	40.9	33 to 43	75.2	*	*	1	115	1
9/15/2024	16:00	6	3	2	4	33	52	20	4	0	0	0	0	0	0	0	0	0	0	0	124	34.6	39.6	30 to 40	75.8	*	*	0	123	1
9/15/2024	17:00	4	1	1	3	21	66	26	8	0	0	0	0	0	0	0	0	0	0	0	130	36.2	41.4	32 to 42	78.5	*	*	0	127	3
9/15/2024	18:00	2	0	0	3	20	42	27	9	1	0	0	0	0	0	0	0	0	0	0	104	37.2	42.8	34 to 44	71.2	*	*	0	103	1
9/15/2024	19:00	1	2	1	3	14	41	26	6	1	0	0	0	0	0	0	0	0	0	0	95	37.1	41.6	32 to 42	76.8	*	*	0	95	0
9/15/2024	20:00	5	0	1	4	25	22	12	1	1	0	0	0	0	0	0	0	0	0	0	71	33.8	39.6	30 to 40	73.2	*	*	2	68	1
9/15/2024	21:00	5	2	2	5	22	26	14	1	0	0	0	0	0	0	0	0	0	0	0	77	33.5	39.8	32 to 42	70.1	*	*	0	77	0
9/15/2024	22:00	1	0	1	3	7	12	9	1	0	0	0	0	0	0	0	0	0	0	0	34	35.7	42	33 to 43	73.5	*	*	0	34	0
9/15/2024	23:00	1	0	1	4	3	7	3	0	0	0	0	0	0	0	0	0	0	0	0	19	32.2	39	27 to 37	57.9	*	*	0	19	0
24 Hr Summary		52	18	19	92	391	565	286	53	4	0	0	0	0	0	0	0	0	0	0	1480	35	41	31 to 41	71.9	*	*	6	1455	19

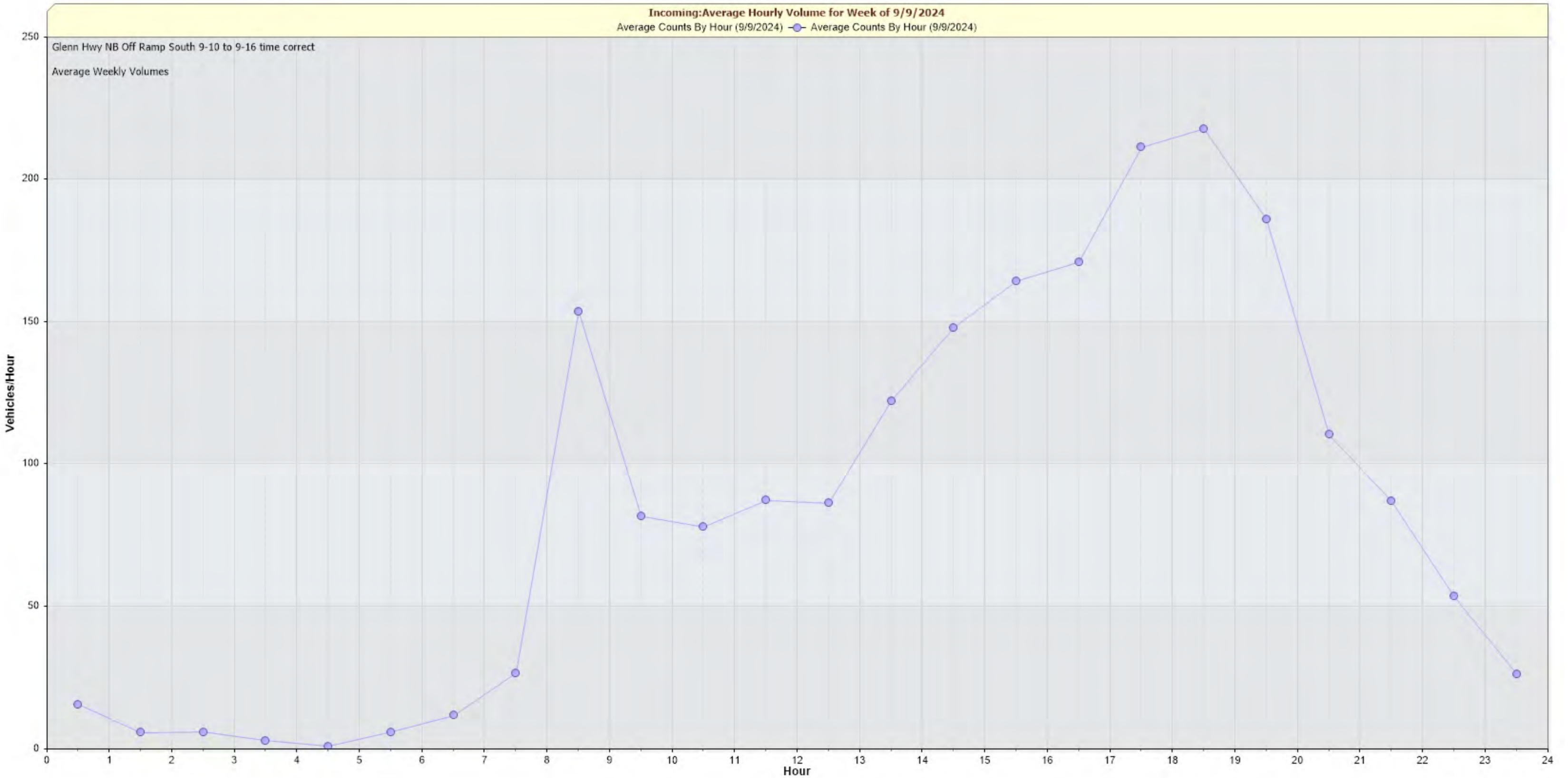
Incoming Histogram
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	0	0	0	1	2	9	2	0	0	0	0	0	0	0	0	0	0	0	14	36	39	31 to 41	92.9	*	*	0	14	0	
9/16/2024	01:00	0	1	1	0	1	5	3	0	0	0	0	0	0	0	0	0	0	0	11	33.9	39.5	30 to 40	72.7	*	*	0	11	0	
9/16/2024	02:00	0	0	0	1	2	1	1	0	0	0	0	0	0	0	0	0	0	0	5	34.4	36	26 to 36	80.0	*	*	0	4	1	
9/16/2024	03:00	0	3	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	7	27.7	37	28 to 38	57.1	*	*	0	7	0	
9/16/2024	04:00	0	0	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	4	33.3	40	16 to 26	50.0	*	*	0	3	1	
9/16/2024	05:00	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4	38.3	39.5	30 to 40	100.0	*	*	0	4	0	
9/16/2024	06:00	2	0	0	2	2	2	0	1	0	0	0	0	0	0	0	0	0	0	9	28.7	39	25 to 35	55.6	*	*	0	9	0	
9/16/2024	07:00	1	0	0	2	10	8	5	0	0	0	0	0	0	0	0	0	0	0	26	34.7	39.5	31 to 41	88.5	*	*	0	25	1	
9/16/2024	08:00	0	0	0	3	1	4	4	0	1	0	0	0	0	0	0	0	0	0	13	37	41	32 to 42	69.2	*	*	0	13	0	
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/16/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		3	4	2	10	19	34	19	1	1	0	0	0	0	0	0	0	0	0	93	34.1	40	31 to 41	73.1	*	*	0	90	3	

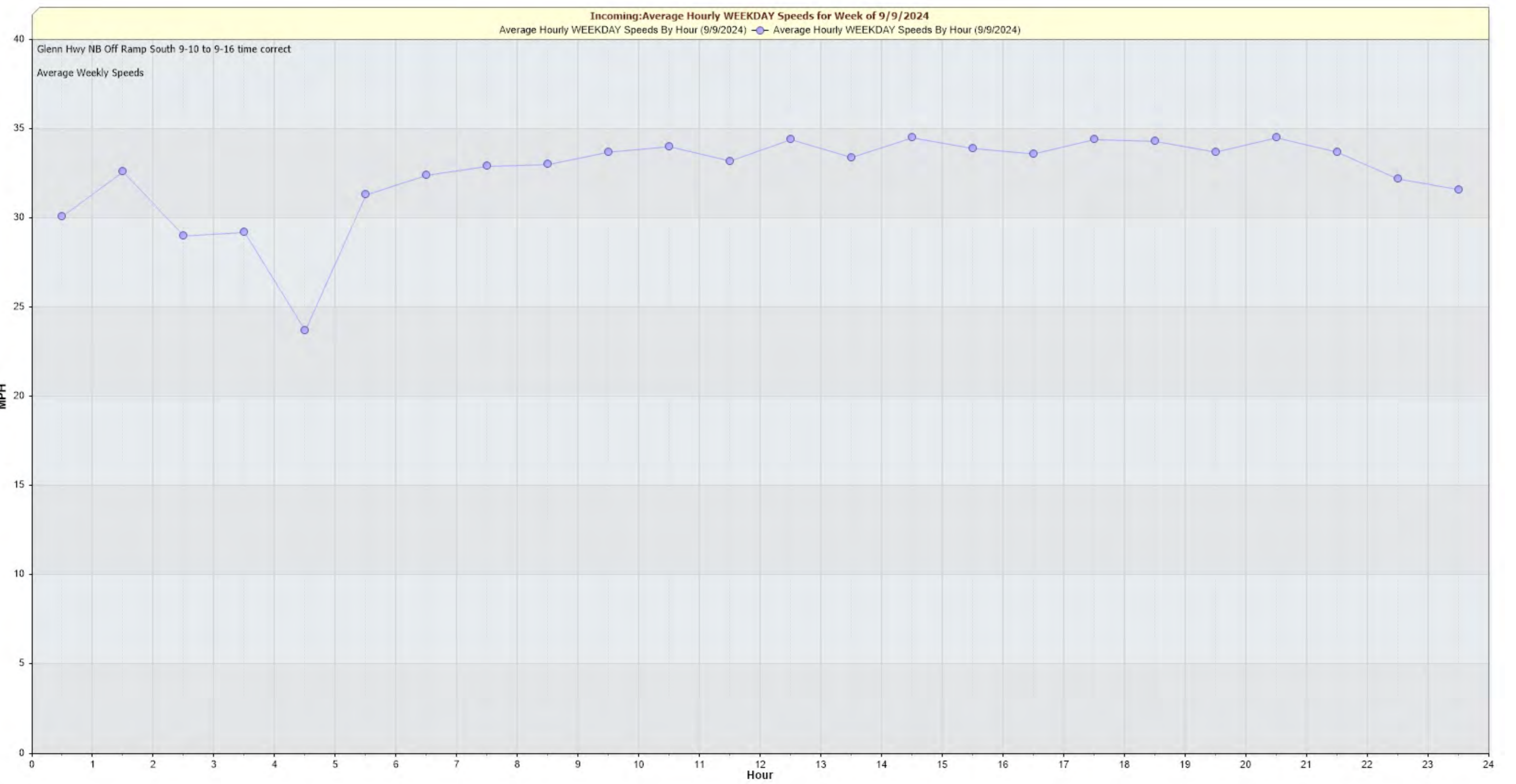
Incoming: Average Hourly Volume for Week of 9/9/2024
Average Counts By Hour (9/9/2024) — Average Counts By Hour (9/9/2024)

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct
Average Weekly Volumes



Incoming: Average Hourly WEEKDAY Speeds for Week of 9/9/2024
Average Hourly WEEKDAY Speeds By Hour (9/9/2024) — Average Hourly WEEKDAY Speeds By Hour (9/9/2024)

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct
Average Weekly Speeds



Incoming:Daily Volume for Week of 9/9/2024

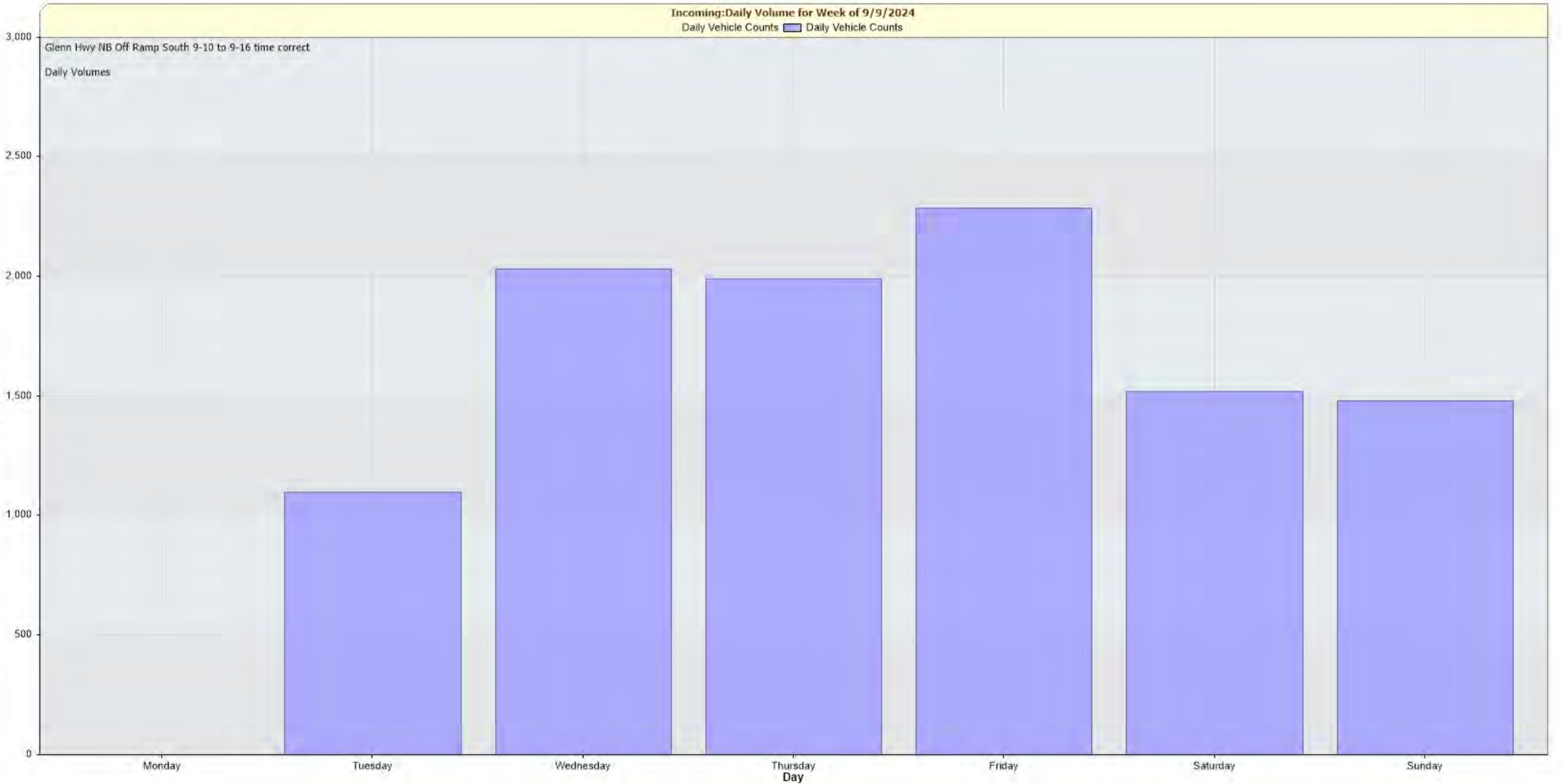
Daily Vehicle Counts



Daily Vehicle Counts

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

Daily Volumes



For Project: Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:52:29 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/10/2024 2:00:00 PM through 9/15/2024 2:59:59 PM

85th Percentile Speed 40 MPH

85th Percentile Vehicles 5006

Max Speed 64 MPH on 9/13/2024 10:45:18 PM

Total Vehicles 5890

AADT: 1168

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1141	981
AM Peak 7:00 AM	110	76
PM Peak 2:00 PM	117	105

Speed

Speed Limit: 45

85th Percentile Speed: 40

50th Percentile Speed: 34

10 MPH Pace Interval: 31.0 MPH to 41.0 MPH

Average Speed: 32.94

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	N/A	8	15	20	26	15	12
% over limit	N/A	1.1	1.1	1.7	1.9	1.6	2.9
Avg Speeder	N/A	47.8	47.6	47.5	48.2	48.3	46.8
Avg Speed	N/A	32.4	32.9	33.0	32.5	33.4	34.4

Class Counts

	Number	%
VEH_SM	2	0
VEH_MED	5511	93.6
VEH_LG	377	6.4
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Incoming Summary
Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/10/2024 3:00:00 PM	32.0	105	123	40	0.0	0.0%	26.5
9/10/2024 4:00:00 PM	38.0	94	110	45	0.0	0.0%	31.4
9/10/2024 5:00:00 PM	40.0	116	136	50	49.3	2.2%	33.4
9/10/2024 6:00:00 PM	41.0	96	113	47	46.5	1.8%	34.8
9/10/2024 7:00:00 PM	40.0	64	75	44	0.0	0.0%	33.5
9/10/2024 8:00:00 PM	42.0	59	69	49	47.5	2.9%	35.2
9/10/2024 9:00:00 PM	41.0	39	46	46	46.0	2.2%	35.5
9/10/2024 10:00:00 PM	37.0	23	27	40	0.0	0.0%	32.3
9/10/2024 11:00:00 PM	42.0	6	7	45	0.0	0.0%	37.7
9/11/2024 12:00:00 AM	41.0	3	4	43	0.0	0.0%	35.0
9/11/2024 1:00:00 AM	31.0	4	5	42	0.0	0.0%	28.6
9/11/2024 2:00:00 AM	32.0	4	5	45	0.0	0.0%	27.2
9/11/2024 3:00:00 AM	43.0	3	3	43	0.0	0.0%	39.3
9/11/2024 4:00:00 AM	37.0	1	1	37	0.0	0.0%	37.0
9/11/2024 5:00:00 AM	35.0	4	5	38	0.0	0.0%	29.6
9/11/2024 6:00:00 AM	37.0	9	11	43	0.0	0.0%	33.6
9/11/2024 7:00:00 AM	39.0	36	42	45	0.0	0.0%	36.3
9/11/2024 8:00:00 AM	38.0	103	121	45	0.0	0.0%	30.7
9/11/2024 9:00:00 AM	40.0	70	82	45	0.0	0.0%	32.5
9/11/2024 10:00:00 AM	40.0	58	68	47	47.0	1.5%	33.8
9/11/2024 11:00:00 AM	41.0	59	69	48	47.5	2.9%	34.3
9/11/2024 12:00:00 PM	40.0	60	71	46	46.0	1.4%	34.7
9/11/2024 1:00:00 PM	39.0	68	80	53	49.5	2.5%	32.1
9/11/2024 2:00:00 PM	41.0	74	87	47	46.5	2.3%	32.7
9/11/2024 3:00:00 PM	35.0	109	128	44	0.0	0.0%	28.2
9/11/2024 4:00:00 PM	38.0	80	94	44	0.0	0.0%	32.1
9/11/2024 5:00:00 PM	40.0	111	131	50	49.0	1.5%	33.9
9/11/2024 6:00:00 PM	41.0	85	100	48	47.3	3.0%	34.8
9/11/2024 7:00:00 PM	42.0	55	65	44	0.0	0.0%	34.4
9/11/2024 8:00:00 PM	40.0	45	53	47	47.0	1.9%	34.3
9/11/2024 9:00:00 PM	39.0	43	51	43	0.0	0.0%	34.4
9/11/2024 10:00:00 PM	38.0	15	18	43	0.0	0.0%	34.1
9/11/2024 11:00:00 PM	40.0	8	10	42	0.0	0.0%	36.4
9/12/2024 12:00:00 AM	35.0	5	6	47	47.0	16.7%	35.3
9/12/2024 1:00:00 AM	34.0	3	3	34	0.0	0.0%	27.0
9/12/2024 2:00:00 AM	46.0	3	3	46	46.0	33.3%	32.0
9/12/2024 3:00:00 AM	39.0	1	1	39	0.0	0.0%	39.0
9/12/2024 4:00:00 AM	15.0	1	1	15	0.0	0.0%	15.0
9/12/2024 5:00:00 AM	42.0	5	6	44	0.0	0.0%	36.5
9/12/2024 6:00:00 AM	39.0	8	9	44	0.0	0.0%	32.7
9/12/2024 7:00:00 AM	42.0	44	52	48	47.5	3.8%	35.8
9/12/2024 8:00:00 AM	36.0	93	109	49	49.0	0.9%	29.3
9/12/2024 9:00:00 AM	38.0	71	84	45	0.0	0.0%	32.0
9/12/2024 10:00:00 AM	41.0	53	62	51	48.5	3.2%	34.5
9/12/2024 11:00:00 AM	40.0	61	72	50	48.0	2.8%	33.4
9/12/2024 12:00:00 PM	39.0	65	77	45	0.0	0.0%	32.8
9/12/2024 1:00:00 PM	41.0	64	75	45	0.0	0.0%	33.3

Incoming Summary
Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

9/12/2024 2:00:00 PM	40.0	65	77	48	47.0	2.6%	32.8
9/12/2024 3:00:00 PM	37.0	77	91	46	46.0	1.1%	28.6
9/12/2024 4:00:00 PM	40.0	62	73	47	47.0	1.4%	33.1
9/12/2024 5:00:00 PM	42.0	73	86	48	46.8	5.8%	34.5
9/12/2024 6:00:00 PM	39.0	82	96	45	0.0	0.0%	33.4
9/12/2024 7:00:00 PM	42.0	60	70	50	48.0	2.9%	35.5
9/12/2024 8:00:00 PM	41.0	37	43	45	0.0	0.0%	35.9
9/12/2024 9:00:00 PM	40.0	27	32	49	49.0	3.1%	36.6
9/12/2024 10:00:00 PM	41.0	14	17	44	0.0	0.0%	35.1
9/12/2024 11:00:00 PM	39.0	7	8	41	0.0	0.0%	33.5
9/13/2024 12:00:00 AM	35.0	2	2	35	0.0	0.0%	33.0
9/13/2024 1:00:00 AM	31.0	2	2	31	0.0	0.0%	25.5
9/13/2024 2:00:00 AM	38.0	1	1	38	0.0	0.0%	38.0
9/13/2024 3:00:00 AM	24.0	2	2	24	0.0	0.0%	20.0
9/13/2024 4:00:00 AM	39.0	2	2	39	0.0	0.0%	37.5
9/13/2024 5:00:00 AM	39.0	4	5	41	0.0	0.0%	33.4
9/13/2024 6:00:00 AM	39.0	10	12	41	0.0	0.0%	30.9
9/13/2024 7:00:00 AM	41.0	43	51	47	47.0	2.0%	36.0
9/13/2024 8:00:00 AM	37.0	87	102	43	0.0	0.0%	29.4
9/13/2024 9:00:00 AM	37.0	74	87	48	48.0	1.1%	31.2
9/13/2024 10:00:00 AM	39.0	69	81	47	46.5	2.5%	31.7
9/13/2024 11:00:00 AM	38.0	46	54	50	48.3	5.6%	32.6
9/13/2024 12:00:00 PM	40.0	74	87	46	46.0	1.1%	32.3
9/13/2024 1:00:00 PM	39.0	82	96	47	47.0	1.0%	31.7
9/13/2024 2:00:00 PM	40.0	103	121	47	47.0	2.5%	32.5
9/13/2024 3:00:00 PM	33.0	108	127	44	0.0	0.0%	27.1
9/13/2024 4:00:00 PM	40.0	111	131	49	47.7	2.3%	32.6
9/13/2024 5:00:00 PM	41.0	81	95	49	47.0	3.2%	34.8
9/13/2024 6:00:00 PM	42.0	91	107	52	49.3	2.8%	34.2
9/13/2024 7:00:00 PM	42.0	70	82	47	47.0	1.2%	36.0
9/13/2024 8:00:00 PM	41.0	54	63	49	49.0	1.6%	33.9
9/13/2024 9:00:00 PM	40.0	39	46	44	0.0	0.0%	35.2
9/13/2024 10:00:00 PM	40.0	22	26	45	0.0	0.0%	35.1
9/13/2024 11:00:00 PM	43.0	10	12	64	55.5	16.7%	38.7
9/14/2024 12:00:00 AM	43.0	6	7	46	46.0	14.3%	39.4
9/14/2024 1:00:00 AM	32.0	2	2	32	0.0	0.0%	24.0
9/14/2024 2:00:00 AM	48.0	3	3	48	48.0	33.3%	39.0
9/14/2024 3:00:00 AM	**No Data**						
9/14/2024 4:00:00 AM	43.0	2	2	43	0.0	0.0%	37.5
9/14/2024 5:00:00 AM	40.0	9	11	45	0.0	0.0%	34.9
9/14/2024 6:00:00 AM	39.0	8	10	45	0.0	0.0%	30.1
9/14/2024 7:00:00 AM	40.0	14	16	45	0.0	0.0%	33.3
9/14/2024 8:00:00 AM	40.0	24	28	45	0.0	0.0%	34.1
9/14/2024 9:00:00 AM	42.0	31	37	49	48.5	5.4%	33.7
9/14/2024 10:00:00 AM	41.0	56	66	48	48.0	3.0%	34.7
9/14/2024 11:00:00 AM	39.0	71	84	46	46.0	1.2%	32.0
9/14/2024 12:00:00 PM	40.0	71	83	50	50.0	1.2%	32.4
9/14/2024 1:00:00 PM	40.0	82	96	47	47.0	1.0%	33.1

Incoming Summary
 Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

9/14/2024 2:00:00 PM	39.0	70	82	51	51.0	1.2%	32.1
9/14/2024 3:00:00 PM	41.0	57	67	52	52.0	1.5%	34.4
9/14/2024 4:00:00 PM	42.0	42	49	46	46.0	4.1%	34.1
9/14/2024 5:00:00 PM	41.0	69	81	48	48.0	1.2%	33.9
9/14/2024 6:00:00 PM	39.0	63	74	43	0.0	0.0%	32.3
9/14/2024 7:00:00 PM	40.0	30	35	50	50.0	2.9%	34.7
9/14/2024 8:00:00 PM	40.0	27	32	45	0.0	0.0%	35.3
9/14/2024 9:00:00 PM	39.0	27	32	45	0.0	0.0%	32.6
9/14/2024 10:00:00 PM	41.0	8	9	47	47.0	11.1%	38.9
9/14/2024 11:00:00 PM	41.0	8	10	44	0.0	0.0%	34.1
9/15/2024 12:00:00 AM	37.0	8	9	38	0.0	0.0%	32.7
9/15/2024 1:00:00 AM	36.0	5	6	38	0.0	0.0%	33.2
9/15/2024 2:00:00 AM	30.0	2	2	30	0.0	0.0%	27.5
9/15/2024 3:00:00 AM	33.0	1	1	33	0.0	0.0%	33.0
9/15/2024 4:00:00 AM	39.0	2	2	39	0.0	0.0%	34.5
9/15/2024 5:00:00 AM	31.0	2	2	31	0.0	0.0%	27.0
9/15/2024 6:00:00 AM	46.0	4	5	47	46.5	40.0%	37.2
9/15/2024 7:00:00 AM	34.0	4	5	38	0.0	0.0%	29.0
9/15/2024 8:00:00 AM	41.0	11	13	43	0.0	0.0%	36.2
9/15/2024 9:00:00 AM	39.0	25	29	46	46.0	3.4%	34.8
9/15/2024 10:00:00 AM	41.0	35	41	46	46.0	2.4%	35.9
9/15/2024 11:00:00 AM	39.0	37	43	44	0.0	0.0%	32.8
9/15/2024 12:00:00 PM	40.0	52	61	46	46.0	3.3%	34.6
9/15/2024 1:00:00 PM	40.0	71	84	47	46.5	2.4%	34.9
9/15/2024 2:00:00 PM	40.0	73	86	49	48.0	3.5%	34.3
9/15/2024 3:00:00 PM	42.0	24	28	47	47.0	3.6%	34.2

Incoming Summary

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/11/2024 12:00:00 AM	40.0	604	710	50	47.8	1.1%	32.4
9/12/2024 12:00:00 AM	40.0	1110	1306	53	47.6	1.1%	32.9
9/13/2024 12:00:00 AM	40.0	977	1149	51	47.5	1.7%	33.0
9/14/2024 12:00:00 AM	40.0	1189	1399	64	48.2	1.9%	32.5
9/15/2024 12:00:00 AM	40.0	780	918	52	48.3	1.6%	33.4
9/15/2024 2:59:59 PM	40.0	347	408	49	46.8	2.9%	34.4

Incoming Weekly Counts

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	5	3	2	2	6	3.33	4	32
1 - 2	*	*	5	3	1	3	2	3	2.5	38.67
2 - 3	*	*	3	1	2	0	1	2	0.5	35.33
3 - 4	*	*	1	1	2	2	2	1.33	2	30.33
4 - 5	*	*	5	6	5	11	2	5.33	6.5	38.67
5 - 6	*	*	11	9	12	10	5	10.67	7.5	38.17
6 - 7	*	*	42	52	51	16	5	48.33	10.5	40.23
7 - 8	*	*	121	109	102	28	13	110.67	20.5	36.37
8 - 9	*	*	82	84	87	37	29	84.33	33	37.9
9 - 10	*	*	68	62	81	66	41	70.33	53.5	39.53
10 - 11	*	*	69	72	54	84	43	65	63.5	39.47
11 - 12	*	*	71	77	87	83	61	78.33	72	39.57
12 - 13	*	*	80	75	96	96	84	83.67	90	39.37
13 - 14	*	*	87	77	121	82	86	95	84	39.8
14 - 15	*	123	128	91	127	67	28	117.25	47.5	34.02
15 - 16	*	110	94	73	131	49	*	102	49	38.45
16 - 17	*	136	131	86	95	81	*	112	81	40.2
17 - 18	*	113	100	96	107	74	*	104	74	40.23
18 - 19	*	75	65	70	82	35	*	73	35	40.85
19 - 20	*	69	53	43	63	32	*	57	32	40.72
20 - 21	*	46	51	32	46	32	*	43.75	32	39.67
21 - 22	*	27	18	17	26	9	*	22	9	38.8
22 - 23	*	7	10	8	12	10	*	9.25	10	41
23 - 24	*	4	6	2	7	9	*	4.75	9	38.5
Totals	0	710	1306	1149	1399	918	408			
% of Total	0%	12.05%	22.17%	19.51%	23.75%	15.59%	6.93%			

Incoming Monthly Counts

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	5	3	2	2	6	3.33	4	32
1 - 2	*	*	5	3	1	3	2	3	2.5	38.67
2 - 3	*	*	3	1	2	0	1	2	0.5	35.33
3 - 4	*	*	1	1	2	2	2	1.33	2	30.33
4 - 5	*	*	5	6	5	11	2	5.33	6.5	38.67
5 - 6	*	*	11	9	12	10	5	10.67	7.5	38.17
6 - 7	*	*	42	52	51	16	5	48.33	10.5	40.23
7 - 8	*	*	121	109	102	28	13	110.67	20.5	36.37
8 - 9	*	*	82	84	87	37	29	84.33	33	37.9
9 - 10	*	*	68	62	81	66	41	70.33	53.5	39.53
10 - 11	*	*	69	72	54	84	43	65	63.5	39.47
11 - 12	*	*	71	77	87	83	61	78.33	72	39.57
12 - 13	*	*	80	75	96	96	84	83.67	90	39.37
13 - 14	*	*	87	77	121	82	86	95	84	39.8
14 - 15	*	123	128	91	127	67	28	117.25	47.5	34.02
15 - 16	*	110	94	73	131	49	*	102	49	38.45
16 - 17	*	136	131	86	95	81	*	112	81	40.2
17 - 18	*	113	100	96	107	74	*	104	74	40.23
18 - 19	*	75	65	70	82	35	*	73	35	40.85
19 - 20	*	69	53	43	63	32	*	57	32	40.72
20 - 21	*	46	51	32	46	32	*	43.75	32	39.67
21 - 22	*	27	18	17	26	9	*	22	9	38.8
22 - 23	*	7	10	8	12	10	*	9.25	10	41
23 - 24	*	4	6	2	7	9	*	4.75	9	38.5
Totals	0	710	1306	1149	1399	918	408			
% of Total	0%	12.05%	22.17%	19.51%	23.75%	15.59%	6.93%			

Incoming Weekly Speeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	28.6	27	25.5	24	33.17	27.5	30.88	32
1 - 2	*	*	27.2	32	38	39	27.5	30	34.4	38.67
2 - 3	*	*	39.33	39	20	0	33	32.83	33	35.33
3 - 4	*	*	37	15	37.5	37.5	34.5	31.75	36	30.33
4 - 5	*	*	29.6	36.5	33.4	34.91	27	33.38	33.69	38.67
5 - 6	*	*	33.64	32.67	30.92	30.1	37.2	32.34	32.47	38.17
6 - 7	*	*	36.31	35.75	36.04	33.31	29	36.01	32.29	40.23
7 - 8	*	*	30.71	29.28	29.4	34.07	36.15	29.84	34.73	36.37
8 - 9	*	*	32.51	31.96	31.23	33.73	34.79	31.89	34.2	37.9
9 - 10	*	*	33.78	34.45	31.7	34.74	35.9	33.18	35.19	39.53
10 - 11	*	*	34.26	33.42	32.63	32.01	32.81	33.5	32.28	39.47
11 - 12	*	*	34.73	32.77	32.34	32.36	34.59	33.2	33.31	39.57
12 - 13	*	*	32.14	33.35	31.69	33.1	34.9	32.33	33.94	39.37
13 - 14	*	*	32.71	32.77	32.51	32.15	34.26	32.64	33.23	39.8
14 - 15	*	26.51	28.22	28.62	27.13	34.42	34.18	27.55	34.35	34.02
15 - 16	*	31.42	32.1	33.05	32.58	34.12	*	32.24	34.12	38.45
16 - 17	*	33.35	33.9	34.47	34.84	33.93	*	34.04	33.93	40.2
17 - 18	*	34.78	34.83	33.43	34.19	32.26	*	34.33	32.26	40.23
18 - 19	*	33.55	34.42	35.53	35.96	34.71	*	34.89	34.71	40.85
19 - 20	*	35.2	34.3	35.91	33.92	35.25	*	34.77	35.25	40.72
20 - 21	*	35.5	34.43	36.59	35.22	32.59	*	35.31	32.59	39.67
21 - 22	*	32.3	34.06	35.12	35.12	38.89	*	34.03	38.89	38.8
22 - 23	*	37.71	36.4	33.5	38.67	34.1	*	36.76	34.1	41
23 - 24	*	35	35.33	33	39.43	32.67	*	36.53	32.67	38.5
Totals	0	32.4	32.9	33	32.5	33.4	34.4			
% of Total	0%	16.31%	16.57%	16.62%	16.36%	16.82%	17.32%			

Incoming Monthly Speeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	28.6	27	25.5	24	33.17	27.5	30.88	32
1 - 2	*	*	27.2	32	38	39	27.5	30	34.4	38.67
2 - 3	*	*	39.33	39	20	0	33	32.83	33	35.33
3 - 4	*	*	37	15	37.5	37.5	34.5	31.75	36	30.33
4 - 5	*	*	29.6	36.5	33.4	34.91	27	33.38	33.69	38.67
5 - 6	*	*	33.64	32.67	30.92	30.1	37.2	32.34	32.47	38.17
6 - 7	*	*	36.31	35.75	36.04	33.31	29	36.01	32.29	40.23
7 - 8	*	*	30.71	29.28	29.4	34.07	36.15	29.84	34.73	36.37
8 - 9	*	*	32.51	31.96	31.23	33.73	34.79	31.89	34.2	37.9
9 - 10	*	*	33.78	34.45	31.7	34.74	35.9	33.18	35.19	39.53
10 - 11	*	*	34.26	33.42	32.63	32.01	32.81	33.5	32.28	39.47
11 - 12	*	*	34.73	32.77	32.34	32.36	34.59	33.2	33.31	39.57
12 - 13	*	*	32.14	33.35	31.69	33.1	34.9	32.33	33.94	39.37
13 - 14	*	*	32.71	32.77	32.51	32.15	34.26	32.64	33.23	39.8
14 - 15	*	26.51	28.22	28.62	27.13	34.42	34.18	27.55	34.35	34.02
15 - 16	*	31.42	32.1	33.05	32.58	34.12	*	32.24	34.12	38.45
16 - 17	*	33.35	33.9	34.47	34.84	33.93	*	34.04	33.93	40.2
17 - 18	*	34.78	34.83	33.43	34.19	32.26	*	34.33	32.26	40.23
18 - 19	*	33.55	34.42	35.53	35.96	34.71	*	34.89	34.71	40.85
19 - 20	*	35.2	34.3	35.91	33.92	35.25	*	34.77	35.25	40.72
20 - 21	*	35.5	34.43	36.59	35.22	32.59	*	35.31	32.59	39.67
21 - 22	*	32.3	34.06	35.12	35.12	38.89	*	34.03	38.89	38.8
22 - 23	*	37.71	36.4	33.5	38.67	34.1	*	36.76	34.1	41
23 - 24	*	35	35.33	33	39.43	32.67	*	36.53	32.67	38.5
Totals	0	32.4	32.9	33	32.5	33.4	34.4			
% of Total	0%	16.31%	16.57%	16.62%	16.36%	16.82%	17.32%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	31	34	31	32	36	32	34	32
1 - 2	*	*	32	46	38	48	30	38.67	39	38.67
2 - 3	*	*	43	39	24	0	33	35.33	33	35.33
3 - 4	*	*	37	15	39	43	39	30.33	41	30.33
4 - 5	*	*	35	42	39	40	31	38.67	35.5	38.67
5 - 6	*	*	37	39	38.5	39	46	38.17	42.5	38.17
6 - 7	*	*	39	41.2	40.5	40	34	40.23	37	40.23
7 - 8	*	*	37.7	35.2	36.2	40	41	36.37	40.5	36.37
8 - 9	*	*	39.3	37.4	37	41.7	38.7	37.9	40.2	37.9
9 - 10	*	*	39.5	40.6	38.5	41	40.5	39.53	40.75	39.53
10 - 11	*	*	40.5	39.9	38	39	39	39.47	39	39.47
11 - 12	*	*	40	38.7	40	39.8	39.9	39.57	39.85	39.57
12 - 13	*	*	38.5	40.4	39.2	40	39.5	39.37	39.75	39.37
13 - 14	*	*	40.4	39.5	39.5	39	39.8	39.8	39.4	39.8
14 - 15	*	31.6	35	36.5	33	40.7	42	34.02	41.35	34.02
15 - 16	*	37.7	37.5	39.5	39.1	41.3	*	38.45	41.3	38.45
16 - 17	*	39.3	39.4	42	40.1	40.2	*	40.2	40.2	40.2
17 - 18	*	40.1	40.7	38.4	41.7	38.3	*	40.23	38.3	40.23
18 - 19	*	39.2	41.5	41.2	41.5	40	*	40.85	40	40.85
19 - 20	*	41.2	40	40.7	41	39.7	*	40.72	39.7	40.72
20 - 21	*	40.8	38.4	39.5	40	39	*	39.67	39	39.67
21 - 22	*	36.5	38	40.7	40	41	*	38.8	41	38.8
22 - 23	*	42	40	39	43	41	*	41	41	41
23 - 24	*	41	35	35	43	37	*	38.5	37	38.5
Totals	0	389.4	915.4	920.4	920.8	920.7	569.4			
% of Total	0%	8.4%	19.75%	19.85%	19.86%	19.86%	12.28%			

Incoming Monthly EightyFifthSpeeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	Sep 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	*	*	31	34	31	32	36	32	34	32
1 - 2	*	*	32	46	38	48	30	38.67	39	38.67
2 - 3	*	*	43	39	24	0	33	35.33	33	35.33
3 - 4	*	*	37	15	39	43	39	30.33	41	30.33
4 - 5	*	*	35	42	39	40	31	38.67	35.5	38.67
5 - 6	*	*	37	39	38.5	39	46	38.17	42.5	38.17
6 - 7	*	*	39	41.2	40.5	40	34	40.23	37	40.23
7 - 8	*	*	37.7	35.2	36.2	40	41	36.37	40.5	36.37
8 - 9	*	*	39.3	37.4	37	41.7	38.7	37.9	40.2	37.9
9 - 10	*	*	39.5	40.6	38.5	41	40.5	39.53	40.75	39.53
10 - 11	*	*	40.5	39.9	38	39	39	39.47	39	39.47
11 - 12	*	*	40	38.7	40	39.8	39.9	39.57	39.85	39.57
12 - 13	*	*	38.5	40.4	39.2	40	39.5	39.37	39.75	39.37
13 - 14	*	*	40.4	39.5	39.5	39	39.8	39.8	39.4	39.8
14 - 15	*	31.6	35	36.5	33	40.7	42	34.02	41.35	34.02
15 - 16	*	37.7	37.5	39.5	39.1	41.3	*	38.45	41.3	38.45
16 - 17	*	39.3	39.4	42	40.1	40.2	*	40.2	40.2	40.2
17 - 18	*	40.1	40.7	38.4	41.7	38.3	*	40.23	38.3	40.23
18 - 19	*	39.2	41.5	41.2	41.5	40	*	40.85	40	40.85
19 - 20	*	41.2	40	40.7	41	39.7	*	40.72	39.7	40.72
20 - 21	*	40.8	38.4	39.5	40	39	*	39.67	39	39.67
21 - 22	*	36.5	38	40.7	40	41	*	38.8	41	38.8
22 - 23	*	42	40	39	43	41	*	41	41	41
23 - 24	*	41	35	35	43	37	*	38.5	37	38.5

Summary of Violators

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM
time correct

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	18	29.0	0	0.0
01:00:00	14	31.6	2	47.0
02:00:00	7	32.9	0	0.0
03:00:00	8	33.9	0	0.0
04:00:00	29	33.5	0	0.0
05:00:00	47	32.4	2	46.5
06:00:00	166	35.5	3	47.3
07:00:00	373	30.4	1	49.0
08:00:00	319	32.4	4	47.8
09:00:00	318	33.9	8	47.4
10:00:00	322	33.0	8	47.8
11:00:00	379	33.2	5	46.8
12:00:00	431	33.0	6	47.7
13:00:00	453	32.9	11	47.5
14:00:00	564	28.7	3	48.3
15:00:00	457	32.4	6	47.0
16:00:00	529	34.0	14	47.8
17:00:00	490	34.0	8	47.9
18:00:00	327	34.9	4	48.3
19:00:00	260	34.8	4	47.8
20:00:00	207	34.9	2	47.5
21:00:00	97	34.5	1	47.0
22:00:00	47	36.2	2	55.5
23:00:00	28	35.3	2	46.5

Incoming Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	14:00	1	4	44	43	22	7	2	0	0	0	0	0	0	0	0	0	0	0	123	26.5	31.6	20 to 30	74.0	*	*	2	112	9	
9/10/2024	15:00	0	6	17	16	26	35	9	1	0	0	0	0	0	0	0	0	0	0	110	31.4	37.7	28 to 38	58.2	*	*	0	99	11	
9/10/2024	16:00	0	4	19	12	28	50	18	3	2	0	0	0	0	0	0	0	0	0	136	33.4	39.3	31 to 41	68.4	3	2.2	0	119	17	
9/10/2024	17:00	0	3	8	9	27	38	26	2	0	0	0	0	0	0	0	0	0	0	113	34.8	40.1	31 to 41	69.9	2	1.8	0	103	10	
9/10/2024	18:00	1	3	8	5	18	28	12	0	0	0	0	0	0	0	0	0	0	0	75	33.5	39.2	30 to 40	68.0	*	*	0	62	13	
9/10/2024	19:00	0	2	5	4	16	26	13	3	0	0	0	0	0	0	0	0	0	0	69	35.2	41.2	32 to 42	66.7	2	2.9	0	59	10	
9/10/2024	20:00	0	2	2	2	8	16	15	1	0	0	0	0	0	0	0	0	0	0	46	35.5	40.8	32 to 42	76.1	1	2.2	0	37	9	
9/10/2024	21:00	0	0	4	2	9	10	2	0	0	0	0	0	0	0	0	0	0	0	27	32.3	36.5	30 to 40	77.8	*	*	0	26	1	
9/10/2024	22:00	0	0	0	0	1	4	1	1	0	0	0	0	0	0	0	0	0	0	7	37.7	42	35 to 45	85.7	*	*	0	7	0	
9/10/2024	23:00	0	1	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	4	35	41	33 to 43	75.0	*	*	0	4	0	
24 Hr Summary		2	25	107	93	155	215	100	11	2	0	0	0	0	0	0	0	0	0	710	32.4	40	31 to 41	58.6	8	1.1	2	628	80	

Incoming Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/11/2024	00:00	0	1	1	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	5	28.6	31	21 to 31	60.0	*	*	0	5	0
9/11/2024	01:00	1	1	0	1	1	0	0	1	0	0	0	0	0	0	0	0	0	0	5	27.2	32	8 to 18	40.0	*	*	0	5	0
9/11/2024	02:00	0	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3	39.3	43	33 to 43	100.0	*	*	0	3	0
9/11/2024	03:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	37	37	27 to 37	100.0	*	*	0	1	0
9/11/2024	04:00	0	1	1	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	5	29.6	35	28 to 38	60.0	*	*	0	4	1
9/11/2024	05:00	0	0	2	0	3	5	1	0	0	0	0	0	0	0	0	0	0	0	11	33.6	37	28 to 38	72.7	*	*	0	10	1
9/11/2024	06:00	0	0	0	1	10	25	5	1	0	0	0	0	0	0	0	0	0	0	42	36.3	39	30 to 40	90.5	*	*	0	41	1
9/11/2024	07:00	1	1	25	26	30	25	12	1	0	0	0	0	0	0	0	0	0	0	121	30.7	37.7	22 to 32	57.9	*	*	0	116	5
9/11/2024	08:00	0	1	15	11	19	23	10	3	0	0	0	0	0	0	0	0	0	0	82	32.5	39.3	29 to 39	54.9	*	*	0	78	4
9/11/2024	09:00	1	3	8	3	15	25	12	1	0	0	0	0	0	0	0	0	0	0	68	33.8	39.5	32 to 42	72.1	1	1.5	0	59	9
9/11/2024	10:00	0	2	11	2	13	28	11	2	0	0	0	0	0	0	0	0	0	0	69	34.3	40.5	33 to 43	68.1	2	2.9	0	60	9
9/11/2024	11:00	0	2	5	4	20	26	13	1	0	0	0	0	0	0	0	0	0	0	71	34.7	40	32 to 42	74.6	1	1.4	0	66	5
9/11/2024	12:00	0	3	12	12	16	27	8	1	1	0	0	0	0	0	0	0	0	0	80	32.1	38.5	29 to 39	57.5	2	2.5	0	76	4
9/11/2024	13:00	0	3	20	5	18	17	20	4	0	0	0	0	0	0	0	0	0	0	87	32.7	40.4	31 to 41	51.7	2	2.3	0	83	4
9/11/2024	14:00	1	9	28	36	33	16	5	0	0	0	0	0	0	0	0	0	0	0	128	28.2	35	22 to 32	65.6	*	*	0	122	6
9/11/2024	15:00	1	1	18	9	22	34	9	0	0	0	0	0	0	0	0	0	0	0	94	32.1	37.5	30 to 40	63.8	*	*	0	92	2
9/11/2024	16:00	0	0	17	8	35	48	20	2	1	0	0	0	0	0	0	0	0	0	131	33.9	39.4	30 to 40	68.7	2	1.5	0	122	9
9/11/2024	17:00	1	2	11	4	18	39	21	4	0	0	0	0	0	0	0	0	0	0	100	34.8	40.7	30 to 40	65.0	3	3.0	0	91	9
9/11/2024	18:00	0	4	6	5	11	18	21	0	0	0	0	0	0	0	0	0	0	0	65	34.4	41.5	34 to 44	66.2	*	*	0	60	5
9/11/2024	19:00	0	2	4	4	14	17	11	1	0	0	0	0	0	0	0	0	0	0	53	34.3	40	30 to 40	66.0	1	1.9	0	52	1
9/11/2024	20:00	0	1	3	4	11	28	4	0	0	0	0	0	0	0	0	0	0	0	51	34.4	38.4	31 to 41	82.4	*	*	0	49	2
9/11/2024	21:00	0	0	2	1	5	7	3	0	0	0	0	0	0	0	0	0	0	0	18	34.1	38	31 to 41	77.8	*	*	0	18	0
9/11/2024	22:00	0	0	1	0	2	4	3	0	0	0	0	0	0	0	0	0	0	0	10	36.4	40	32 to 42	90.0	*	*	0	10	0
9/11/2024	23:00	0	0	0	0	4	1	0	1	0	0	0	0	0	0	0	0	0	0	6	35.3	35	25 to 35	83.3	1	16.7	0	6	0
24 Hr Summary		6	37	190	136	303	417	192	23	2	0	0	0	0	0	0	0	0	0	1306	32.9	40	31 to 41	60.4	15	1.1	0	1229	77

Incoming Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/12/2024	00:00	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	27	34	24 to 34	66.7	*	*	0	3	0
9/12/2024	01:00	0	1	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	3	32	46	6 to 16	33.3	1	33.3	0	3	0
9/12/2024	02:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	39	39	29 to 39	100.0	*	*	0	1	0
9/12/2024	03:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	15	15	5 to 15	100.0	*	*	0	1	0
9/12/2024	04:00	0	0	1	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	6	36.5	42	34 to 44	83.3	*	*	0	4	2
9/12/2024	05:00	0	0	3	0	0	5	1	0	0	0	0	0	0	0	0	0	0	0	9	32.7	39	34 to 44	66.7	*	*	0	7	2
9/12/2024	06:00	0	1	3	2	8	25	11	2	0	0	0	0	0	0	0	0	0	0	52	35.8	41.2	33 to 43	80.8	2	3.8	0	51	1
9/12/2024	07:00	0	2	23	33	31	12	7	1	0	0	0	0	0	0	0	0	0	0	109	29.3	35.2	22 to 32	64.2	1	0.9	0	106	3
9/12/2024	08:00	0	3	10	12	22	34	2	1	0	0	0	0	0	0	0	0	0	0	84	32	37.4	29 to 39	69.0	*	*	0	78	6
9/12/2024	09:00	0	2	5	7	13	17	16	1	1	0	0	0	0	0	0	0	0	0	62	34.5	40.6	31 to 41	64.5	2	3.2	0	57	5
9/12/2024	10:00	0	2	11	4	19	19	15	1	1	0	0	0	0	0	0	0	0	0	72	33.4	39.9	32 to 42	68.1	2	2.8	0	62	10
9/12/2024	11:00	0	2	11	7	24	23	6	4	0	0	0	0	0	0	0	0	0	0	77	32.8	38.7	31 to 41	63.6	*	*	0	69	8
9/12/2024	12:00	0	1	12	11	8	26	15	2	0	0	0	0	0	0	0	0	0	0	75	33.3	40.4	32 to 42	58.7	*	*	0	69	6
9/12/2024	13:00	0	4	15	5	12	27	11	3	0	0	0	0	0	0	0	0	0	0	77	32.8	39.5	32 to 42	57.1	2	2.6	0	69	8
9/12/2024	14:00	1	6	23	25	15	13	7	1	0	0	0	0	0	0	0	0	0	0	91	28.6	36.5	21 to 31	54.9	1	1.1	0	88	3
9/12/2024	15:00	0	3	10	8	13	27	10	2	0	0	0	0	0	0	0	0	0	0	73	33.1	39.5	32 to 42	60.3	1	1.4	0	64	9
9/12/2024	16:00	1	2	9	10	14	30	14	6	0	0	0	0	0	0	0	0	0	0	86	34.5	42	31 to 41	55.8	5	5.8	0	81	5
9/12/2024	17:00	0	0	13	12	21	39	10	1	0	0	0	0	0	0	0	0	0	0	96	33.4	38.4	29 to 39	68.8	*	*	0	88	8
9/12/2024	18:00	0	2	4	3	16	27	16	1	1	0	0	0	0	0	0	0	0	0	70	35.5	41.2	32 to 42	71.4	2	2.9	0	65	5
9/12/2024	19:00	0	0	2	3	10	15	12	1	0	0	0	0	0	0	0	0	0	0	43	35.9	40.7	33 to 43	74.4	*	*	0	40	3
9/12/2024	20:00	0	1	0	1	9	14	6	1	0	0	0	0	0	0	0	0	0	0	32	36.6	39.5	31 to 41	90.6	1	3.1	0	28	4
9/12/2024	21:00	0	1	1	1	2	7	5	0	0	0	0	0	0	0	0	0	0	0	17	35.1	40.7	34 to 44	76.5	*	*	0	15	2
9/12/2024	22:00	0	0	1	0	4	2	1	0	0	0	0	0	0	0	0	0	0	0	8	33.5	39	29 to 39	75.0	*	*	0	8	0
9/12/2024	23:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	33	35	25 to 35	100.0	*	*	0	2	0
24 Hr Summary		2	35	157	145	244	367	167	29	3	0	0	0	0	0	0	0	0	0	1149	33	40	31 to 41	58.3	20	1.7	0	1059	90

Incoming Histogram
 Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/13/2024	00:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25.5	31	10 to 20	50.0	*	*	0	2	0
9/13/2024	01:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	38	38	28 to 38	100.0	*	*	0	1	0
9/13/2024	02:00	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	20	24	14 to 24	100.0	*	*	0	1	1
9/13/2024	03:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	37.5	39	29 to 39	100.0	*	*	0	2	0
9/13/2024	04:00	0	0	1	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	5	33.4	39	31 to 41	80.0	*	*	0	5	0
9/13/2024	05:00	0	2	1	1	3	4	1	0	0	0	0	0	0	0	0	0	0	0	12	30.9	38.5	29 to 39	58.3	*	*	0	10	2
9/13/2024	06:00	0	0	2	2	15	18	12	2	0	0	0	0	0	0	0	0	0	0	51	36	40.5	30 to 40	74.5	1	2.0	0	51	0
9/13/2024	07:00	0	3	16	41	23	10	9	0	0	0	0	0	0	0	0	0	0	0	102	29.4	36.2	23 to 33	67.6	*	*	0	95	7
9/13/2024	08:00	0	4	12	19	20	24	6	2	0	0	0	0	0	0	0	0	0	0	87	31.2	37	27 to 37	60.9	1	1.1	0	85	2
9/13/2024	09:00	0	3	16	8	20	24	8	2	0	0	0	0	0	0	0	0	0	0	81	31.7	38.5	31 to 41	60.5	2	2.5	0	78	3
9/13/2024	10:00	0	2	11	2	12	21	3	2	1	0	0	0	0	0	0	0	0	0	54	32.6	38	29 to 39	61.1	3	5.6	0	49	5
9/13/2024	11:00	1	6	11	12	11	28	15	3	0	0	0	0	0	0	0	0	0	0	87	32.3	40	32 to 42	54.0	1	1.1	0	84	3
9/13/2024	12:00	0	1	26	9	20	25	13	2	0	0	0	0	0	0	0	0	0	0	96	31.7	39.2	31 to 41	55.2	1	1.0	0	92	4
9/13/2024	13:00	1	8	17	11	25	39	16	4	0	0	0	0	0	0	0	0	0	0	121	32.5	39.5	32 to 42	59.5	3	2.5	0	114	7
9/13/2024	14:00	0	5	42	43	23	8	6	0	0	0	0	0	0	0	0	0	0	0	127	27.1	33	21 to 31	72.4	*	*	0	125	2
9/13/2024	15:00	1	9	15	14	23	48	18	3	0	0	0	0	0	0	0	0	0	0	131	32.6	39.1	31 to 41	64.9	3	2.3	0	122	9
9/13/2024	16:00	0	1	8	10	18	39	15	4	0	0	0	0	0	0	0	0	0	0	95	34.8	40.1	32 to 42	73.7	3	3.2	0	90	5
9/13/2024	17:00	0	3	15	7	21	35	22	3	1	0	0	0	0	0	0	0	0	0	107	34.2	41.7	31 to 41	57.9	3	2.8	0	105	2
9/13/2024	18:00	0	1	7	5	6	38	24	1	0	0	0	0	0	0	0	0	0	0	82	36	41.5	34 to 44	78.0	1	1.2	0	79	3
9/13/2024	19:00	0	5	8	3	10	18	18	1	0	0	0	0	0	0	0	0	0	0	63	33.9	41	33 to 43	60.3	1	1.6	0	58	5
9/13/2024	20:00	0	1	2	1	14	20	8	0	0	0	0	0	0	0	0	0	0	0	46	35.2	40	31 to 41	78.3	*	*	0	43	3
9/13/2024	21:00	0	1	1	2	5	11	5	1	0	0	0	0	0	0	0	0	0	0	26	35.1	40	33 to 43	73.1	*	*	0	26	0
9/13/2024	22:00	0	0	1	0	3	4	2	1	0	0	1	0	0	0	0	0	0	0	12	38.7	43	29 to 39	58.3	2	16.7	0	11	1
9/13/2024	23:00	0	0	0	1	1	0	4	1	0	0	0	0	0	0	0	0	0	0	7	39.4	43	33 to 43	71.4	1	14.3	0	7	0
24 Hr Summary		3	56	214	191	275	419	206	32	2	0	1	0	0	0	0	0	0	0	1399	32.5	40	31 to 41	55.5	26	1.9	0	1335	64

Incoming Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/14/2024	00:00	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24	32	6 to 16	50.0	*	*	0	1	1
9/14/2024	01:00	0	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	3	39	48	28 to 38	66.7	1	33.3	0	3	0
9/14/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/14/2024	03:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	37.5	43	22 to 32	50.0	*	*	0	2	0
9/14/2024	04:00	0	0	2	0	3	3	2	1	0	0	0	0	0	0	0	0	0	0	11	34.9	40	30 to 40	63.6	*	*	0	10	1
9/14/2024	05:00	0	2	3	0	1	2	1	1	0	0	0	0	0	0	0	0	0	0	10	30.1	39	14 to 24	50.0	*	*	0	9	1
9/14/2024	06:00	0	0	4	1	1	7	2	1	0	0	0	0	0	0	0	0	0	0	16	33.3	40	35 to 45	62.5	*	*	0	13	3
9/14/2024	07:00	0	0	5	1	5	10	6	1	0	0	0	0	0	0	0	0	0	0	28	34.1	40	31 to 41	67.9	*	*	0	23	5
9/14/2024	08:00	0	3	4	5	4	12	5	4	0	0	0	0	0	0	0	0	0	0	37	33.7	41.7	32 to 42	54.1	2	5.4	0	34	3
9/14/2024	09:00	0	3	5	6	10	28	9	5	0	0	0	0	0	0	0	0	0	0	66	34.7	41	31 to 41	62.1	2	3.0	0	65	1
9/14/2024	10:00	0	3	17	9	15	27	10	3	0	0	0	0	0	0	0	0	0	0	84	32	39	31 to 41	53.6	1	1.2	0	78	6
9/14/2024	11:00	0	8	11	8	15	24	16	0	1	0	0	0	0	0	0	0	0	0	83	32.4	39.8	32 to 42	59.0	1	1.2	0	79	4
9/14/2024	12:00	0	1	18	11	16	31	16	3	0	0	0	0	0	0	0	0	0	0	96	33.1	40	32 to 42	58.3	1	1.0	0	90	6
9/14/2024	13:00	0	3	17	9	13	28	11	0	1	0	0	0	0	0	0	0	0	0	82	32.1	39	31 to 41	54.9	1	1.2	0	78	4
9/14/2024	14:00	0	4	5	8	10	22	15	2	1	0	0	0	0	0	0	0	0	0	67	34.4	40.7	31 to 41	62.7	1	1.5	0	60	7
9/14/2024	15:00	2	1	4	3	11	16	9	3	0	0	0	0	0	0	0	0	0	0	49	34.1	41.3	32 to 42	65.3	2	4.1	0	49	0
9/14/2024	16:00	1	5	8	3	17	31	12	4	0	0	0	0	0	0	0	0	0	0	81	33.9	40.2	31 to 41	66.7	1	1.2	0	75	6
9/14/2024	17:00	1	1	12	10	14	29	7	0	0	0	0	0	0	0	0	0	0	0	74	32.3	38.3	31 to 41	63.5	*	*	0	72	2
9/14/2024	18:00	0	2	2	2	8	13	7	0	1	0	0	0	0	0	0	0	0	0	35	34.7	40	31 to 41	74.3	1	2.9	0	35	0
9/14/2024	19:00	0	2	2	1	4	16	6	1	0	0	0	0	0	0	0	0	0	0	32	35.3	39.7	33 to 43	81.3	*	*	0	30	2
9/14/2024	20:00	0	3	1	6	8	9	4	1	0	0	0	0	0	0	0	0	0	0	32	32.6	39	28 to 38	62.5	*	*	0	32	0
9/14/2024	21:00	0	0	0	0	2	2	4	1	0	0	0	0	0	0	0	0	0	0	9	38.9	41	31 to 41	88.9	1	11.1	0	9	0
9/14/2024	22:00	0	0	1	1	3	2	3	0	0	0	0	0	0	0	0	0	0	0	10	34.1	41	26 to 36	60.0	*	*	0	10	0
9/14/2024	23:00	0	0	1	1	3	4	0	0	0	0	0	0	0	0	0	0	0	0	9	32.7	37	28 to 38	77.8	*	*	0	9	0
24 Hr Summary		4	42	122	85	166	317	146	32	4	0	0	0	0	0	0	0	0	0	918	33.4	40	31 to 41	60.6	15	1.6	0	866	52

Incoming Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

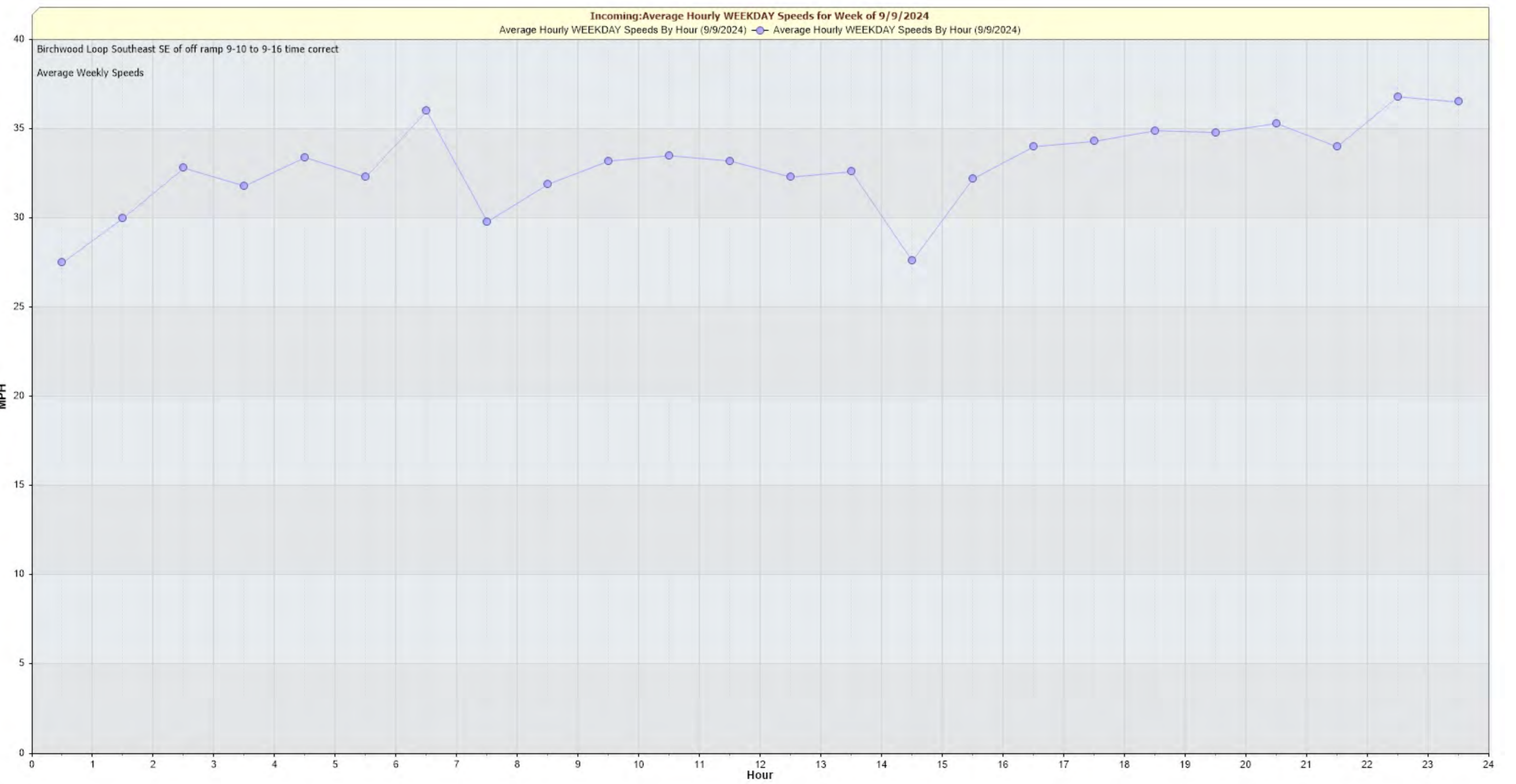
Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/15/2024	00:00	0	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	0	6	33.2	36	26 to 36	83.3	*	*	0	6	0	
9/15/2024	01:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27.5	30	20 to 30	100.0	*	*	0	2	0	
9/15/2024	02:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	33	33	23 to 33	100.0	*	*	0	1	0	
9/15/2024	03:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34.5	39	29 to 39	100.0	*	*	0	2	0	
9/15/2024	04:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27	31	21 to 31	100.0	*	*	0	2	0	
9/15/2024	05:00	0	0	0	2	0	1	0	2	0	0	0	0	0	0	0	0	0	0	5	37.2	46	25 to 35	60.0	2	40.0	0	5	0	
9/15/2024	06:00	0	1	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	5	29	34	28 to 38	60.0	*	*	0	5	0	
9/15/2024	07:00	0	0	0	1	3	5	4	0	0	0	0	0	0	0	0	0	0	0	13	36.2	41	31 to 41	76.9	*	*	0	13	0	
9/15/2024	08:00	0	0	1	1	11	13	2	1	0	0	0	0	0	0	0	0	0	0	29	34.8	38.7	29 to 39	82.8	1	3.4	0	28	1	
9/15/2024	09:00	0	1	2	2	7	18	10	1	0	0	0	0	0	0	0	0	0	0	41	35.9	40.5	32 to 42	82.9	1	2.4	0	40	1	
9/15/2024	10:00	0	0	5	8	11	13	6	0	0	0	0	0	0	0	0	0	0	0	43	32.8	39	30 to 40	60.5	*	*	0	41	2	
9/15/2024	11:00	0	1	3	9	13	20	13	2	0	0	0	0	0	0	0	0	0	0	61	34.6	39.9	32 to 42	68.9	2	3.3	0	60	1	
9/15/2024	12:00	0	1	9	4	17	35	16	2	0	0	0	0	0	0	0	0	0	0	84	34.9	39.5	30 to 40	73.8	2	2.4	0	83	1	
9/15/2024	13:00	0	4	7	8	16	30	17	4	0	0	0	0	0	0	0	0	0	0	86	34.3	39.8	31 to 41	66.3	3	3.5	0	80	6	
9/15/2024	14:00	0	1	3	3	6	8	5	2	0	0	0	0	0	0	0	0	0	0	28	34.2	42	32 to 42	60.7	1	3.6	0	26	2	
9/15/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	9	32	40	92	148	73	14	0	0	0	0	0	0	0	0	0	0	408	34.4	40	31 to 41	67.9	12	2.9	0	394	14	

Incoming:Average Hourly Volume for Week of 9/9/2024
Average Counts By Hour (9/9/2024) — Average Counts By Hour (9/9/2024)



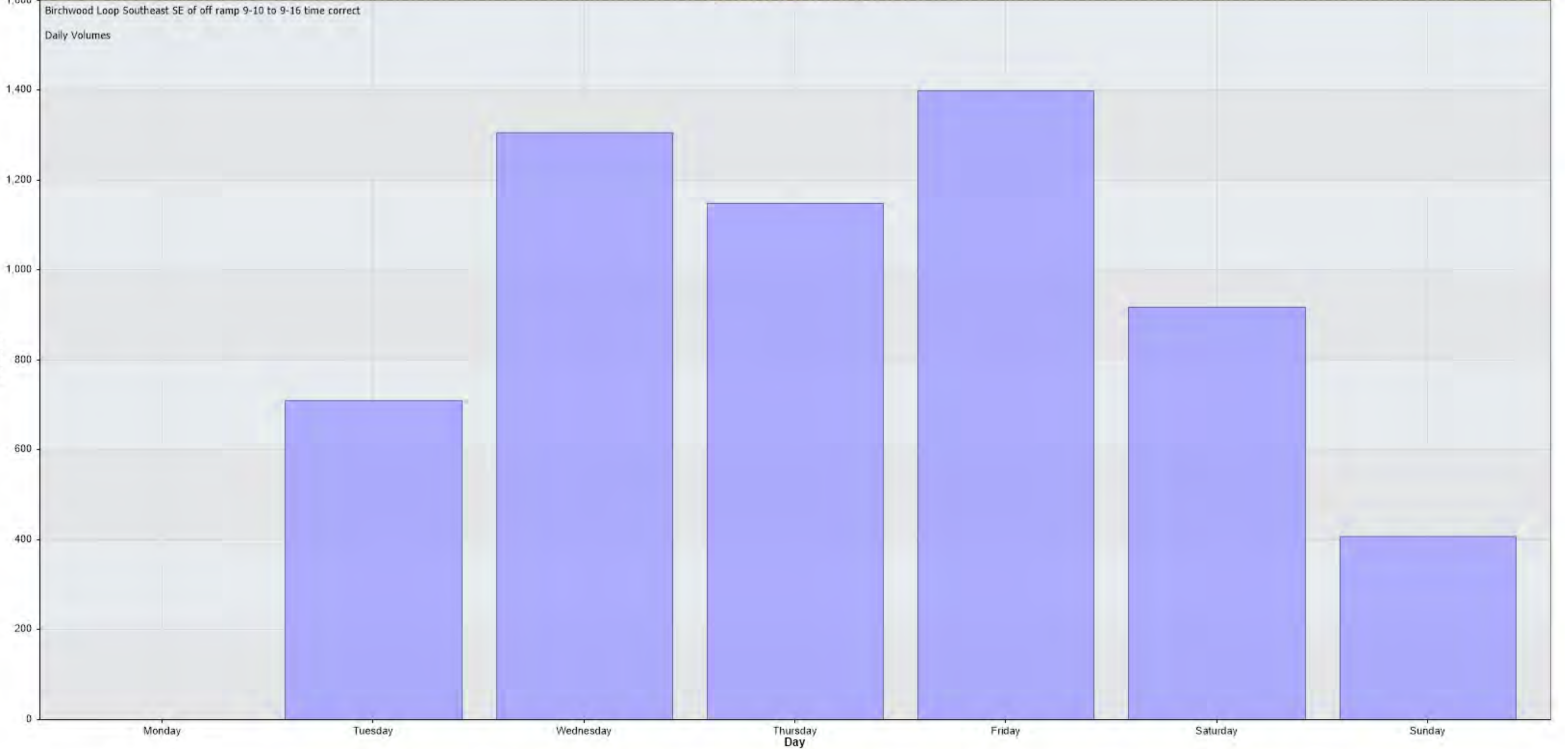
Incoming: Average Hourly WEEKDAY Speeds for Week of 9/9/2024
Average Hourly WEEKDAY Speeds By Hour (9/9/2024) — Average Hourly WEEKDAY Speeds By Hour (9/9/2024)

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct
Average Weekly Speeds



Incoming:Daily Volume for Week of 9/9/2024

Daily Vehicle Counts



For Project: Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:52:30 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/10/2024 2:00:00 PM through 9/15/2024 2:59:59 PM

85th Percentile Speed 39 MPH

85th Percentile Vehicles 8129

Max Speed 70 MPH on 9/13/2024 1:58:22 AM

Total Vehicles 9563

AADT: 1896

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1857	1593
AM Peak 7:00 AM	161	107
PM Peak 5:00 PM	196	177

Speed

Speed Limit: 45

85th Percentile Speed: 39

50th Percentile Speed: 31

10 MPH Pace Interval: 23.0 MPH to 33.0 MPH

Average Speed: 30.5

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	N/A	16	34	30	32	28	18
% over limit	N/A	1.4	1.6	1.4	1.5	1.8	2.9
Avg Speeder	N/A	48.1	47.2	47.3	48.5	49.0	47.4
Avg Speed	N/A	30.7	30.4	29.8	30.0	31.1	33.0

Class Counts

	Number	%
VEH_SM	63	0.7
VEH_MED	9046	94.6
VEH_LG	454	4.7
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Outgoing Summary
Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/10/2024 3:00:00 PM	32.0	93	109	41	0.0	0.0%	25.3
9/10/2024 4:00:00 PM	38.0	138	162	51	51.0	0.6%	30.7
9/10/2024 5:00:00 PM	38.0	172	202	47	46.5	1.0%	30.2
9/10/2024 6:00:00 PM	39.0	156	183	49	47.7	1.6%	31.2
9/10/2024 7:00:00 PM	41.0	125	147	46	46.0	2.0%	32.7
9/10/2024 8:00:00 PM	40.0	112	132	55	51.7	2.3%	32.3
9/10/2024 9:00:00 PM	41.0	77	91	49	47.3	4.4%	33.6
9/10/2024 10:00:00 PM	35.0	47	55	42	0.0	0.0%	29.5
9/10/2024 11:00:00 PM	37.0	13	15	40	0.0	0.0%	26.2
9/11/2024 12:00:00 AM	36.0	8	10	44	0.0	0.0%	25.5
9/11/2024 1:00:00 AM	33.0	3	3	33	0.0	0.0%	27.3
9/11/2024 2:00:00 AM	21.0	3	4	27	0.0	0.0%	20.0
9/11/2024 3:00:00 AM	32.0	4	5	33	0.0	0.0%	29.4
9/11/2024 4:00:00 AM	28.0	3	4	36	0.0	0.0%	27.3
9/11/2024 5:00:00 AM	33.0	9	11	36	0.0	0.0%	26.3
9/11/2024 6:00:00 AM	43.0	14	16	46	46.0	6.3%	34.1
9/11/2024 7:00:00 AM	42.0	53	62	48	46.4	8.1%	35.1
9/11/2024 8:00:00 AM	34.0	137	161	51	48.5	1.2%	26.4
9/11/2024 9:00:00 AM	39.0	86	101	46	46.0	2.0%	30.7
9/11/2024 10:00:00 AM	38.0	68	80	44	0.0	0.0%	30.5
9/11/2024 11:00:00 AM	40.0	84	99	46	46.0	2.0%	32.5
9/11/2024 12:00:00 PM	38.0	89	105	43	0.0	0.0%	32.0
9/11/2024 1:00:00 PM	40.0	94	110	48	47.5	1.8%	29.8
9/11/2024 2:00:00 PM	39.0	114	134	46	46.0	0.7%	29.7
9/11/2024 3:00:00 PM	34.0	140	165	49	49.0	0.6%	26.6
9/11/2024 4:00:00 PM	38.0	161	189	47	46.5	1.1%	30.1
9/11/2024 5:00:00 PM	39.0	141	166	52	48.3	1.8%	30.1
9/11/2024 6:00:00 PM	40.0	177	208	50	47.4	2.4%	31.5
9/11/2024 7:00:00 PM	40.0	176	207	46	46.0	1.0%	31.7
9/11/2024 8:00:00 PM	40.0	81	95	51	48.5	2.1%	32.0
9/11/2024 9:00:00 PM	40.0	79	93	46	46.0	1.1%	31.6
9/11/2024 10:00:00 PM	40.0	35	41	48	47.5	4.9%	31.5
9/11/2024 11:00:00 PM	37.0	15	18	52	52.0	5.6%	31.1
9/12/2024 12:00:00 AM	31.0	6	7	35	0.0	0.0%	25.1
9/12/2024 1:00:00 AM	32.0	3	4	47	47.0	25.0%	35.3
9/12/2024 2:00:00 AM	33.0	2	2	33	0.0	0.0%	26.0
9/12/2024 3:00:00 AM	40.0	3	3	40	0.0	0.0%	26.7
9/12/2024 4:00:00 AM	38.0	2	2	38	0.0	0.0%	28.0
9/12/2024 5:00:00 AM	31.0	6	7	31	0.0	0.0%	25.0
9/12/2024 6:00:00 AM	40.0	15	18	44	0.0	0.0%	30.1
9/12/2024 7:00:00 AM	42.0	49	58	45	0.0	0.0%	34.6
9/12/2024 8:00:00 AM	34.0	144	170	47	46.5	1.2%	26.5
9/12/2024 9:00:00 AM	39.0	78	92	47	47.0	1.1%	30.2
9/12/2024 10:00:00 AM	37.0	89	105	45	0.0	0.0%	30.1
9/12/2024 11:00:00 AM	39.0	84	99	45	0.0	0.0%	30.8
9/12/2024 12:00:00 PM	40.0	115	135	46	46.0	1.5%	31.7
9/12/2024 1:00:00 PM	38.0	102	120	49	46.8	3.3%	29.8

Outgoing Summary
 Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

9/12/2024 2:00:00 PM	38.0	109	128	49	49.0	0.8%	29.2
9/12/2024 3:00:00 PM	33.0	156	183	50	49.0	1.1%	26.3
9/12/2024 4:00:00 PM	38.0	154	181	48	46.7	1.7%	28.8
9/12/2024 5:00:00 PM	38.0	176	207	46	46.0	0.5%	29.1
9/12/2024 6:00:00 PM	39.0	171	201	50	46.8	3.0%	30.4
9/12/2024 7:00:00 PM	41.0	137	161	47	46.5	1.2%	32.3
9/12/2024 8:00:00 PM	40.0	68	80	48	48.0	1.3%	33.1
9/12/2024 9:00:00 PM	38.0	61	72	49	48.0	4.2%	31.7
9/12/2024 10:00:00 PM	39.0	54	63	51	51.0	3.2%	31.2
9/12/2024 11:00:00 PM	36.0	15	18	45	0.0	0.0%	29.1
9/13/2024 12:00:00 AM	38.0	14	17	44	0.0	0.0%	30.2
9/13/2024 1:00:00 AM	30.0	8	9	34	0.0	0.0%	25.3
9/13/2024 2:00:00 AM	38.0	11	13	70	70.0	7.7%	33.5
9/13/2024 3:00:00 AM	26.0	3	3	26	0.0	0.0%	18.0
9/13/2024 4:00:00 AM	26.0	2	2	26	0.0	0.0%	21.5
9/13/2024 5:00:00 AM	22.0	5	6	36	0.0	0.0%	21.5
9/13/2024 6:00:00 AM	39.0	6	7	40	0.0	0.0%	29.0
9/13/2024 7:00:00 AM	41.0	49	58	47	47.0	1.7%	33.4
9/13/2024 8:00:00 AM	34.0	130	153	48	48.0	0.7%	26.6
9/13/2024 9:00:00 AM	39.0	71	83	48	47.0	2.4%	30.8
9/13/2024 10:00:00 AM	38.0	74	87	51	49.5	2.3%	30.8
9/13/2024 11:00:00 AM	40.0	80	94	49	49.0	1.1%	30.6
9/13/2024 12:00:00 PM	38.0	91	107	49	49.0	0.9%	29.5
9/13/2024 1:00:00 PM	40.0	128	150	47	46.7	2.0%	31.1
9/13/2024 2:00:00 PM	38.0	146	172	50	47.7	1.7%	29.9
9/13/2024 3:00:00 PM	34.0	148	174	45	0.0	0.0%	25.9
9/13/2024 4:00:00 PM	38.0	137	161	60	50.3	2.5%	30.1
9/13/2024 5:00:00 PM	39.0	169	199	48	47.0	1.0%	30.1
9/13/2024 6:00:00 PM	40.0	165	194	46	46.0	0.5%	30.7
9/13/2024 7:00:00 PM	41.0	128	151	49	47.0	2.0%	32.4
9/13/2024 8:00:00 PM	39.0	82	96	46	46.0	1.0%	31.2
9/13/2024 9:00:00 PM	39.0	75	88	47	46.7	3.4%	31.6
9/13/2024 10:00:00 PM	39.0	46	54	50	48.5	3.7%	33.1
9/13/2024 11:00:00 PM	37.0	31	36	44	0.0	0.0%	29.8
9/14/2024 12:00:00 AM	43.0	14	16	48	48.0	6.3%	30.8
9/14/2024 1:00:00 AM	35.0	12	14	40	0.0	0.0%	26.8
9/14/2024 2:00:00 AM	40.0	8	10	56	56.0	10.0%	30.5
9/14/2024 3:00:00 AM	49.0	1	1	49	49.0	100.0%	49.0
9/14/2024 4:00:00 AM	45.0	3	3	45	0.0	0.0%	29.7
9/14/2024 5:00:00 AM	31.0	8	10	41	0.0	0.0%	25.5
9/14/2024 6:00:00 AM	22.0	5	6	35	0.0	0.0%	22.7
9/14/2024 7:00:00 AM	39.0	19	22	42	0.0	0.0%	30.0
9/14/2024 8:00:00 AM	38.0	37	44	45	0.0	0.0%	29.3
9/14/2024 9:00:00 AM	40.0	50	59	50	48.3	5.1%	32.5
9/14/2024 10:00:00 AM	38.0	77	91	45	0.0	0.0%	31.1
9/14/2024 11:00:00 AM	41.0	80	94	44	0.0	0.0%	30.8
9/14/2024 12:00:00 PM	40.0	90	106	49	49.0	0.9%	30.9
9/14/2024 1:00:00 PM	38.0	131	154	47	47.0	0.6%	30.1

Outgoing Summary
 Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

9/14/2024 2:00:00 PM	38.0	110	129	46	46.0	0.8%	30.8
9/14/2024 3:00:00 PM	39.0	116	136	55	49.3	2.2%	30.9
9/14/2024 4:00:00 PM	41.0	105	124	52	47.5	3.2%	31.9
9/14/2024 5:00:00 PM	38.0	94	110	48	47.7	2.7%	31.9
9/14/2024 6:00:00 PM	41.0	88	103	55	48.6	4.9%	32.3
9/14/2024 7:00:00 PM	40.0	82	96	45	0.0	0.0%	32.1
9/14/2024 8:00:00 PM	39.0	59	69	67	57.0	2.9%	32.0
9/14/2024 9:00:00 PM	40.0	50	59	47	47.0	1.7%	31.9
9/14/2024 10:00:00 PM	37.0	35	41	48	48.0	2.4%	29.5
9/14/2024 11:00:00 PM	39.0	22	26	45	0.0	0.0%	29.9
9/15/2024 12:00:00 AM	39.0	12	14	48	48.0	7.1%	32.4
9/15/2024 1:00:00 AM	36.0	9	11	41	0.0	0.0%	29.7
9/15/2024 2:00:00 AM	38.0	2	2	38	0.0	0.0%	35.0
9/15/2024 3:00:00 AM	35.0	3	3	35	0.0	0.0%	32.7
9/15/2024 4:00:00 AM	31.0	2	2	31	0.0	0.0%	27.5
9/15/2024 5:00:00 AM	35.0	3	3	35	0.0	0.0%	34.0
9/15/2024 6:00:00 AM	38.0	6	7	45	0.0	0.0%	32.9
9/15/2024 7:00:00 AM	37.0	7	8	40	0.0	0.0%	28.0
9/15/2024 8:00:00 AM	47.0	8	10	50	48.3	30.0%	38.1
9/15/2024 9:00:00 AM	43.0	26	30	44	0.0	0.0%	34.0
9/15/2024 10:00:00 AM	43.0	50	59	49	47.4	8.5%	34.9
9/15/2024 11:00:00 AM	42.0	72	85	49	47.0	3.5%	32.5
9/15/2024 12:00:00 PM	42.0	68	80	50	48.3	3.8%	32.3
9/15/2024 1:00:00 PM	40.0	128	150	47	47.0	0.7%	33.6
9/15/2024 2:00:00 PM	40.0	113	133	47	46.3	2.3%	32.4
9/15/2024 3:00:00 PM	38.0	26	30	43	0.0	0.0%	33.2

Outgoing Summary

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/11/2024 12:00:00 AM	39.0	940	1106	55	48.1	1.4%	30.7
9/12/2024 12:00:00 AM	39.0	1771	2084	52	47.2	1.6%	30.4
9/13/2024 12:00:00 AM	39.0	1807	2126	51	47.3	1.4%	29.8
9/14/2024 12:00:00 AM	39.0	1796	2113	70	48.5	1.5%	30.0
9/15/2024 12:00:00 AM	39.0	1293	1521	67	49.0	1.8%	31.1
9/15/2024 2:59:59 PM	41.0	521	613	50	47.4	2.9%	33.0

Outgoing Weekly Counts

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	3	4	9	14	11	5.33	12.5	31.67
1 - 2	*	*	4	2	13	10	2	6.33	6	30.67
2 - 3	*	*	5	3	3	1	3	3.67	2	32.67
3 - 4	*	*	4	2	2	3	2	2.67	2.5	30.67
4 - 5	*	*	11	7	6	10	3	8	6.5	28.4
5 - 6	*	*	16	18	7	6	7	13.67	6.5	40.5
6 - 7	*	*	62	58	58	22	8	59.33	15	41.33
7 - 8	*	*	161	170	153	44	10	161.33	27	33.73
8 - 9	*	*	101	92	83	59	30	92	44.5	38.83
9 - 10	*	*	80	105	87	91	59	90.67	75	37.07
10 - 11	*	*	99	99	94	94	85	97.33	89.5	39.4
11 - 12	*	*	105	135	107	106	80	115.67	93	38.17
12 - 13	*	*	110	120	150	154	150	126.67	152	38.93
13 - 14	*	*	134	128	172	129	133	144.67	131	37.57
14 - 15	*	109	165	183	174	136	30	157.75	83	32.8
15 - 16	*	162	189	181	161	124	*	173.25	124	37.55
16 - 17	*	202	166	207	199	110	*	193.5	110	37.93
17 - 18	*	183	208	201	194	103	*	196.5	103	39.22
18 - 19	*	147	207	161	151	96	*	166.5	96	40.33
19 - 20	*	132	95	80	96	69	*	100.75	69	39.58
20 - 21	*	91	93	72	88	59	*	86	59	39.12
21 - 22	*	55	41	63	54	41	*	53.25	41	38.05
22 - 23	*	15	18	18	36	26	*	21.75	26	36.62
23 - 24	*	10	7	17	16	14	*	12.5	14	36.88
Totals	0	1106	2084	2126	2113	1521	613			
% of Total	0%	11.57%	21.79%	22.23%	22.1%	15.91%	6.41%			

Outgoing Monthly Counts

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	3	4	9	14	11	5.33	12.5	31.67
1 - 2	*	*	4	2	13	10	2	6.33	6	30.67
2 - 3	*	*	5	3	3	1	3	3.67	2	32.67
3 - 4	*	*	4	2	2	3	2	2.67	2.5	30.67
4 - 5	*	*	11	7	6	10	3	8	6.5	28.4
5 - 6	*	*	16	18	7	6	7	13.67	6.5	40.5
6 - 7	*	*	62	58	58	22	8	59.33	15	41.33
7 - 8	*	*	161	170	153	44	10	161.33	27	33.73
8 - 9	*	*	101	92	83	59	30	92	44.5	38.83
9 - 10	*	*	80	105	87	91	59	90.67	75	37.07
10 - 11	*	*	99	99	94	94	85	97.33	89.5	39.4
11 - 12	*	*	105	135	107	106	80	115.67	93	38.17
12 - 13	*	*	110	120	150	154	150	126.67	152	38.93
13 - 14	*	*	134	128	172	129	133	144.67	131	37.57
14 - 15	*	109	165	183	174	136	30	157.75	83	32.8
15 - 16	*	162	189	181	161	124	*	173.25	124	37.55
16 - 17	*	202	166	207	199	110	*	193.5	110	37.93
17 - 18	*	183	208	201	194	103	*	196.5	103	39.22
18 - 19	*	147	207	161	151	96	*	166.5	96	40.33
19 - 20	*	132	95	80	96	69	*	100.75	69	39.58
20 - 21	*	91	93	72	88	59	*	86	59	39.12
21 - 22	*	55	41	63	54	41	*	53.25	41	38.05
22 - 23	*	15	18	18	36	26	*	21.75	26	36.62
23 - 24	*	10	7	17	16	14	*	12.5	14	36.88
Totals	0	1106	2084	2126	2113	1521	613			
% of Total	0%	11.57%	21.79%	22.23%	22.1%	15.91%	6.41%			

Outgoing Weekly Speeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	27.33	35.25	25.33	26.79	29.73	28.19	28.08	31.67
1 - 2	*	*	20	26	33.54	30.5	35	29.89	31.25	30.67
2 - 3	*	*	29.4	26.67	18	49	32.67	25.55	36.75	32.67
3 - 4	*	*	27.25	28	21.5	29.67	27.5	26	28.8	30.67
4 - 5	*	*	26.27	25	21.5	25.5	34	24.71	27.46	28.4
5 - 6	*	*	34.12	30.06	29	22.67	32.86	31.46	28.15	40.5
6 - 7	*	*	35.13	34.59	33.38	29.95	28	34.38	29.43	41.33
7 - 8	*	*	26.42	26.51	26.58	29.34	38.1	26.5	30.96	33.73
8 - 9	*	*	30.72	30.24	30.81	32.53	34	30.59	33.02	38.83
9 - 10	*	*	30.54	30.13	30.79	31.14	34.93	30.46	32.63	37.07
10 - 11	*	*	32.49	30.84	30.56	30.8	32.53	31.31	31.62	39.4
11 - 12	*	*	32.03	31.65	29.54	30.94	32.33	31.12	31.54	38.17
12 - 13	*	*	29.76	29.77	31.12	30.08	33.56	30.3	31.8	38.93
13 - 14	*	*	29.73	29.22	29.92	30.84	32.37	29.65	31.62	37.57
14 - 15	*	25.36	26.63	26.33	25.89	30.9	33.2	26.12	31.32	32.8
15 - 16	*	30.75	30.05	28.82	30.14	31.93	*	29.91	31.93	37.55
16 - 17	*	30.18	30.08	29.05	30.07	31.91	*	29.83	31.91	37.93
17 - 18	*	31.17	31.53	30.43	30.68	32.29	*	30.96	32.29	39.22
18 - 19	*	32.69	31.71	32.32	32.38	32.11	*	32.23	32.11	40.33
19 - 20	*	32.29	31.99	33.12	31.21	32.04	*	32.13	32.04	39.58
20 - 21	*	33.57	31.63	31.68	31.6	31.9	*	32.15	31.9	39.12
21 - 22	*	29.49	31.49	31.16	33.06	29.51	*	31.27	29.51	38.05
22 - 23	*	26.2	31.06	29.11	29.83	29.92	*	29.31	29.92	36.62
23 - 24	*	25.5	25.14	30.18	30.81	32.36	*	28.74	32.36	36.88
Totals	0	30.7	30.4	29.8	30.1	31.1	33			
% of Total	0%	16.59%	16.42%	16.1%	16.26%	16.8%	17.83%			

Outgoing Monthly Speeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	27.33	35.25	25.33	26.79	29.73	28.19	28.08	31.67
1 - 2	*	*	20	26	33.54	30.5	35	29.89	31.25	30.67
2 - 3	*	*	29.4	26.67	18	49	32.67	25.55	36.75	32.67
3 - 4	*	*	27.25	28	21.5	29.67	27.5	26	28.8	30.67
4 - 5	*	*	26.27	25	21.5	25.5	34	24.71	27.46	28.4
5 - 6	*	*	34.12	30.06	29	22.67	32.86	31.46	28.15	40.5
6 - 7	*	*	35.13	34.59	33.38	29.95	28	34.38	29.43	41.33
7 - 8	*	*	26.42	26.51	26.58	29.34	38.1	26.5	30.96	33.73
8 - 9	*	*	30.72	30.24	30.81	32.53	34	30.59	33.02	38.83
9 - 10	*	*	30.54	30.13	30.79	31.14	34.93	30.46	32.63	37.07
10 - 11	*	*	32.49	30.84	30.56	30.8	32.53	31.31	31.62	39.4
11 - 12	*	*	32.03	31.65	29.54	30.94	32.33	31.12	31.54	38.17
12 - 13	*	*	29.76	29.77	31.12	30.08	33.56	30.3	31.8	38.93
13 - 14	*	*	29.73	29.22	29.92	30.84	32.37	29.65	31.62	37.57
14 - 15	*	25.36	26.63	26.33	25.89	30.9	33.2	26.12	31.32	32.8
15 - 16	*	30.75	30.05	28.82	30.14	31.93	*	29.91	31.93	37.55
16 - 17	*	30.18	30.08	29.05	30.07	31.91	*	29.83	31.91	37.93
17 - 18	*	31.17	31.53	30.43	30.68	32.29	*	30.96	32.29	39.22
18 - 19	*	32.69	31.71	32.32	32.38	32.11	*	32.23	32.11	40.33
19 - 20	*	32.29	31.99	33.12	31.21	32.04	*	32.13	32.04	39.58
20 - 21	*	33.57	31.63	31.68	31.6	31.9	*	32.15	31.9	39.12
21 - 22	*	29.49	31.49	31.16	33.06	29.51	*	31.27	29.51	38.05
22 - 23	*	26.2	31.06	29.11	29.83	29.92	*	29.31	29.92	36.62
23 - 24	*	25.5	25.14	30.18	30.81	32.36	*	28.74	32.36	36.88
Totals	0	30.7	30.4	29.8	30.1	31.1	33			
% of Total	0%	16.59%	16.42%	16.1%	16.26%	16.8%	17.83%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	33	32	30	35	36	31.67	35.5	31.67
1 - 2	*	*	21	33	38	40	38	30.67	39	30.67
2 - 3	*	*	32	40	26	49	35	32.67	42	32.67
3 - 4	*	*	28	38	26	45	31	30.67	38	30.67
4 - 5	*	*	32.7	30.5	22	31	35	28.4	33	28.4
5 - 6	*	*	42.5	40	39	22	38	40.5	30	40.5
6 - 7	*	*	42	41.2	40.8	38.5	37	41.33	37.75	41.33
7 - 8	*	*	34	33.4	33.8	37.5	47	33.73	42.25	33.73
8 - 9	*	*	38.8	39	38.7	39.3	42.5	38.83	40.9	38.83
9 - 10	*	*	37.2	36.3	37.7	38	42.7	37.07	40.35	37.07
10 - 11	*	*	40	38.2	40	40.6	41.2	39.4	40.9	39.4
11 - 12	*	*	37.7	39.4	37.4	39.2	41.5	38.17	40.35	38.17
12 - 13	*	*	39.4	37.7	39.7	37.8	39.4	38.93	38.6	38.93
13 - 14	*	*	38.2	37.4	37.1	38	39.6	37.57	38.8	37.57
14 - 15	*	31.7	33.3	32.6	33.6	38.3	37.7	32.8	38	32.8
15 - 16	*	37.9	37.7	37.4	37.2	41	*	37.55	41	37.55
16 - 17	*	37.1	38.2	37.7	38.7	38	*	37.93	38	37.93
17 - 18	*	38.7	39.3	39	39.9	40.3	*	39.22	40.3	39.22
18 - 19	*	40.7	39.5	40.6	40.5	40	*	40.33	40	40.33
19 - 20	*	40	39.3	40	39	38.7	*	39.58	38.7	39.58
20 - 21	*	40.8	39.7	37.8	38.2	40	*	39.12	40	39.12
21 - 22	*	35	40	38.4	38.8	37	*	38.05	37	38.05
22 - 23	*	36.5	37	36	37	39	*	36.62	39	36.62
23 - 24	*	36	31	38	42.5	39	*	36.88	39	36.88
Totals	0	374.4	871.5	893.6	871.6	922.2	581.6			
% of Total	0%	8.29%	19.3%	19.79%	19.3%	20.43%	12.88%			

Outgoing Monthly EightyFifthSpeeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	33	32	30	35	36	31.67	35.5	31.67
1 - 2	*	*	21	33	38	40	38	30.67	39	30.67
2 - 3	*	*	32	40	26	49	35	32.67	42	32.67
3 - 4	*	*	28	38	26	45	31	30.67	38	30.67
4 - 5	*	*	32.7	30.5	22	31	35	28.4	33	28.4
5 - 6	*	*	42.5	40	39	22	38	40.5	30	40.5
6 - 7	*	*	42	41.2	40.8	38.5	37	41.33	37.75	41.33
7 - 8	*	*	34	33.4	33.8	37.5	47	33.73	42.25	33.73
8 - 9	*	*	38.8	39	38.7	39.3	42.5	38.83	40.9	38.83
9 - 10	*	*	37.2	36.3	37.7	38	42.7	37.07	40.35	37.07
10 - 11	*	*	40	38.2	40	40.6	41.2	39.4	40.9	39.4
11 - 12	*	*	37.7	39.4	37.4	39.2	41.5	38.17	40.35	38.17
12 - 13	*	*	39.4	37.7	39.7	37.8	39.4	38.93	38.6	38.93
13 - 14	*	*	38.2	37.4	37.1	38	39.6	37.57	38.8	37.57
14 - 15	*	31.7	33.3	32.6	33.6	38.3	37.7	32.8	38	32.8
15 - 16	*	37.9	37.7	37.4	37.2	41	*	37.55	41	37.55
16 - 17	*	37.1	38.2	37.7	38.7	38	*	37.93	38	37.93
17 - 18	*	38.7	39.3	39	39.9	40.3	*	39.22	40.3	39.22
18 - 19	*	40.7	39.5	40.6	40.5	40	*	40.33	40	40.33
19 - 20	*	40	39.3	40	39	38.7	*	39.58	38.7	39.58
20 - 21	*	40.8	39.7	37.8	38.2	40	*	39.12	40	39.12
21 - 22	*	35	40	38.4	38.8	37	*	38.05	37	38.05
22 - 23	*	36.5	37	36	37	39	*	36.62	39	36.62
23 - 24	*	36	31	38	42.5	39	*	36.88	39	36.88

Summary of Violators

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM
time correct

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	41	28.1	1	47.0
01:00:00	31	30.4	2	63.0
02:00:00	15	28.5	1	49.0
03:00:00	13	27.1	0	0.0
04:00:00	37	25.7	0	0.0
05:00:00	54	30.7	1	46.0
06:00:00	208	33.7	6	46.5
07:00:00	538	27.0	8	47.9
08:00:00	365	31.2	8	47.3
09:00:00	422	31.2	7	48.0
10:00:00	471	31.4	6	47.0
11:00:00	533	31.3	7	47.9
12:00:00	684	31.0	11	46.9
13:00:00	696	30.4	9	47.0
14:00:00	797	27.2	5	49.4
15:00:00	817	30.2	14	48.2
16:00:00	884	30.1	11	47.4
17:00:00	889	31.1	20	47.5
18:00:00	762	32.2	10	46.4
19:00:00	472	32.1	9	51.1
20:00:00	403	32.1	12	47.2
21:00:00	254	31.0	7	48.9
22:00:00	113	29.5	1	52.0
23:00:00	64	29.5	2	48.0

Outgoing Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	14:00	1	13	46	21	23	4	1	0	0	0	0	0	0	0	0	0	0	0	109	25.4	31.7	19 to 29	67.0	*	*	0	101	8	
9/10/2024	15:00	0	10	31	31	30	46	13	0	1	0	0	0	0	0	0	0	0	0	162	30.7	37.9	29 to 39	53.1	1	0.6	5	142	15	
9/10/2024	16:00	0	10	41	48	42	42	17	2	0	0	0	0	0	0	0	0	0	0	202	30.2	37.1	23 to 33	51.5	2	1.0	1	186	15	
9/10/2024	17:00	0	6	38	31	46	37	20	5	0	0	0	0	0	0	0	0	0	0	183	31.2	38.7	23 to 33	49.7	3	1.6	1	166	16	
9/10/2024	18:00	0	4	23	27	24	37	23	9	0	0	0	0	0	0	0	0	0	0	147	32.7	40.7	31 to 41	47.6	3	2.0	2	134	11	
9/10/2024	19:00	0	5	21	19	36	26	22	1	1	1	0	0	0	0	0	0	0	0	132	32.3	40	31 to 41	51.5	3	2.3	2	119	11	
9/10/2024	20:00	0	2	6	18	20	23	16	6	0	0	0	0	0	0	0	0	0	0	91	33.6	40.8	31 to 41	53.8	4	4.4	1	78	12	
9/10/2024	21:00	0	2	12	13	16	10	2	0	0	0	0	0	0	0	0	0	0	0	55	29.5	35	23 to 33	65.5	*	*	0	53	2	
9/10/2024	22:00	0	3	6	2	1	2	1	0	0	0	0	0	0	0	0	0	0	0	15	26.2	36.5	16 to 26	66.7	*	*	0	15	0	
9/10/2024	23:00	1	2	4	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	10	25.5	36	14 to 24	70.0	*	*	0	10	0	
24 Hr Summary		2	57	228	210	238	229	116	23	2	1	0	0	0	0	0	0	0	0	1106	30.7	39	23 to 33	46.7	16	1.4	12	1004	90	

Outgoing Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/11/2024	00:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	27.3	33	18 to 28	66.7	*	*	0	3	0
9/11/2024	01:00	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	20	21	11 to 21	75.0	*	*	0	2	2
9/11/2024	02:00	0	0	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	5	29.4	32	23 to 33	100.0	*	*	0	5	0
9/11/2024	03:00	0	0	2	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	4	27.3	28	18 to 28	75.0	*	*	0	4	0
9/11/2024	04:00	1	2	2	1	4	1	0	0	0	0	0	0	0	0	0	0	0	0	11	26.3	32.7	23 to 33	54.5	*	*	0	10	1
9/11/2024	05:00	0	1	2	1	4	4	3	1	0	0	0	0	0	0	0	0	0	0	16	34.1	42.5	33 to 43	68.8	1	6.3	0	16	0
9/11/2024	06:00	0	4	7	2	8	24	10	7	0	0	0	0	0	0	0	0	0	0	62	35.1	42	32 to 42	62.9	5	8.1	1	55	6
9/11/2024	07:00	0	25	51	36	25	17	4	2	1	0	0	0	0	0	0	0	0	0	161	26.4	34	18 to 28	60.9	2	1.2	0	158	3
9/11/2024	08:00	0	11	17	16	20	23	11	3	0	0	0	0	0	0	0	0	0	0	101	30.7	38.8	28 to 38	46.5	2	2.0	0	94	7
9/11/2024	09:00	1	3	14	13	22	20	7	0	0	0	0	0	0	0	0	0	0	0	80	30.5	37.2	28 to 38	60.0	*	*	0	73	7
9/11/2024	10:00	0	5	15	10	24	27	14	4	0	0	0	0	0	0	0	0	0	0	99	32.5	40	31 to 41	57.6	2	2.0	2	89	8
9/11/2024	11:00	0	2	14	20	26	34	9	0	0	0	0	0	0	0	0	0	0	0	105	32	37.7	30 to 40	60.0	*	*	0	96	9
9/11/2024	12:00	0	13	23	18	22	16	16	2	0	0	0	0	0	0	0	0	0	0	110	29.8	39.4	23 to 33	41.8	2	1.8	1	103	6
9/11/2024	13:00	0	11	26	33	27	20	15	2	0	0	0	0	0	0	0	0	0	0	134	29.7	38.2	19 to 29	47.8	1	0.7	0	129	5
9/11/2024	14:00	0	21	42	54	25	19	3	1	0	0	0	0	0	0	0	0	0	0	165	26.6	33.3	19 to 29	61.8	1	0.6	0	162	3
9/11/2024	15:00	0	21	29	44	35	37	20	3	0	0	0	0	0	0	0	0	0	0	189	30.1	37.7	24 to 34	47.6	2	1.1	1	177	11
9/11/2024	16:00	1	12	38	29	33	31	16	5	1	0	0	0	0	0	0	0	0	0	166	30.1	38.2	19 to 29	45.8	3	1.8	0	160	6
9/11/2024	17:00	0	11	31	44	40	48	29	4	1	0	0	0	0	0	0	0	0	0	208	31.5	39.3	28 to 38	48.1	5	2.4	0	202	6
9/11/2024	18:00	0	8	34	44	34	51	28	8	0	0	0	0	0	0	0	0	0	0	207	31.7	39.5	31 to 41	47.8	2	1.0	2	196	9
9/11/2024	19:00	0	5	9	22	23	21	12	2	1	0	0	0	0	0	0	0	0	0	95	32	39.3	24 to 34	52.6	2	2.1	0	93	2
9/11/2024	20:00	0	2	21	16	14	24	12	4	0	0	0	0	0	0	0	0	0	0	93	31.6	39.7	31 to 41	49.5	1	1.1	1	92	0
9/11/2024	21:00	0	1	11	5	8	8	4	4	0	0	0	0	0	0	0	0	0	0	41	31.5	40	20 to 30	43.9	2	4.9	0	40	1
9/11/2024	22:00	0	1	4	4	2	4	2	0	1	0	0	0	0	0	0	0	0	0	18	31.1	37	19 to 29	50.0	1	5.6	0	18	0
9/11/2024	23:00	0	1	4	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	7	25.1	31	14 to 24	71.4	*	*	0	7	0
24 Hr Summary		4	161	398	417	401	431	215	52	5	0	0	0	0	0	0	0	0	0	2084	30.4	39	27 to 37	45.0	34	1.6	8	1984	92

Outgoing Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/12/2024	00:00	0	0	0	0	3	0	0	1	0	0	0	0	0	0	0	0	0	0	0	4	35.3	32	22 to 32	75.0	1	25.0	0	4	0
9/12/2024	01:00	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	26	33	9 to 19	50.0	*	*	1	1	0
9/12/2024	02:00	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	26.7	40	10 to 20	66.7	*	*	0	3	0
9/12/2024	03:00	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28	38	8 to 18	50.0	*	*	0	2	0
9/12/2024	04:00	0	1	2	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	25	30.5	21 to 31	85.7	*	*	0	7	0
9/12/2024	05:00	0	5	1	1	4	3	4	0	0	0	0	0	0	0	0	0	0	0	0	18	30.1	40	31 to 41	55.6	*	*	0	16	2
9/12/2024	06:00	0	2	6	5	9	22	13	1	0	0	0	0	0	0	0	0	0	0	0	58	34.6	41.2	34 to 44	69.0	*	*	0	54	4
9/12/2024	07:00	0	11	72	44	20	12	8	3	0	0	0	0	0	0	0	0	0	0	0	170	26.5	33.4	20 to 30	70.6	2	1.2	2	162	6
9/12/2024	08:00	2	5	17	16	22	16	13	1	0	0	0	0	0	0	0	0	0	0	0	92	30.2	39	25 to 35	48.9	1	1.1	2	85	5
9/12/2024	09:00	0	7	16	19	34	22	6	1	0	0	0	0	0	0	0	0	0	0	0	105	30.1	36.3	28 to 38	57.1	*	*	2	97	6
9/12/2024	10:00	0	4	21	15	27	21	9	2	0	0	0	0	0	0	0	0	0	0	0	99	30.8	38.2	25 to 35	50.5	*	*	0	90	9
9/12/2024	11:00	0	5	29	23	19	37	19	3	0	0	0	0	0	0	0	0	0	0	0	135	31.7	39.4	29 to 39	46.7	2	1.5	2	122	11
9/12/2024	12:00	0	11	30	21	20	25	8	5	0	0	0	0	0	0	0	0	0	0	0	120	29.8	37.7	19 to 29	48.3	4	3.3	0	113	7
9/12/2024	13:00	0	13	29	26	22	27	8	3	0	0	0	0	0	0	0	0	0	0	0	128	29.2	37.4	19 to 29	46.9	1	0.8	1	120	7
9/12/2024	14:00	1	18	63	56	24	15	5	0	1	0	0	0	0	0	0	0	0	0	0	183	26.3	32.6	19 to 29	71.0	1	0.5	0	172	11
9/12/2024	15:00	0	14	44	47	30	28	15	3	0	0	0	0	0	0	0	0	0	0	0	181	28.8	37.4	20 to 30	53.0	3	1.7	1	169	11
9/12/2024	16:00	0	13	54	47	44	29	19	1	0	0	0	0	0	0	0	0	0	0	0	207	29.1	37.7	18 to 28	51.7	1	0.5	3	194	10
9/12/2024	17:00	1	9	41	52	29	39	23	6	1	0	0	0	0	0	0	0	0	0	0	201	30.4	39	22 to 32	53.2	6	3.0	0	193	8
9/12/2024	18:00	0	10	19	35	23	38	32	4	0	0	0	0	0	0	0	0	0	0	0	161	32.3	40.6	34 to 44	46.6	2	1.2	0	154	7
9/12/2024	19:00	0	3	10	13	11	26	15	2	0	0	0	0	0	0	0	0	0	0	0	80	33.1	40	32 to 42	57.5	1	1.3	0	76	4
9/12/2024	20:00	2	4	7	13	19	18	4	5	0	0	0	0	0	0	0	0	0	0	0	72	31.7	37.8	27 to 37	54.2	3	4.2	0	67	5
9/12/2024	21:00	0	2	12	13	14	16	4	0	2	0	0	0	0	0	0	0	0	0	0	63	31.2	38.4	22 to 32	55.6	2	3.2	1	60	2
9/12/2024	22:00	0	1	5	5	2	4	0	1	0	0	0	0	0	0	0	0	0	0	0	18	29.1	36	23 to 33	66.7	*	*	0	18	0
9/12/2024	23:00	0	1	4	4	3	3	2	0	0	0	0	0	0	0	0	0	0	0	0	17	30.2	38	18 to 28	52.9	*	*	0	17	0
24 Hr Summary		6	141	484	457	382	402	208	42	4	0	0	0	0	0	0	0	0	0	0	2126	29.8	39	22 to 32	47.2	30	1.4	15	1996	115

Outgoing Histogram
 Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/13/2024	00:00	0	0	4	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	9	25.3	30	20 to 30	88.9	*	*	0	8	1
9/13/2024	01:00	0	0	1	4	5	1	1	0	0	0	0	0	1	0	0	0	0	0	13	33.5	38	23 to 33	76.9	1	7.7	0	13	0
9/13/2024	02:00	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	18	26	5 to 15	66.7	*	*	0	2	1
9/13/2024	03:00	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	21.5	26	16 to 26	100.0	*	*	0	2	0
9/13/2024	04:00	0	3	2	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	6	21.5	22	12 to 22	83.3	*	*	0	5	1
9/13/2024	05:00	0	1	2	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	7	29	39	11 to 21	42.9	*	*	0	6	1
9/13/2024	06:00	0	2	9	5	13	14	13	2	0	0	0	0	0	0	0	0	0	0	58	33.4	40.8	31 to 41	55.2	1	1.7	0	53	5
9/13/2024	07:00	0	15	58	38	20	14	6	2	0	0	0	0	0	0	0	0	0	0	153	26.6	33.8	19 to 29	67.3	1	0.7	2	147	4
9/13/2024	08:00	1	5	14	17	17	19	8	2	0	0	0	0	0	0	0	0	0	0	83	30.8	38.7	24 to 34	47.0	2	2.4	0	80	3
9/13/2024	09:00	0	6	19	11	17	24	8	1	1	0	0	0	0	0	0	0	0	0	87	30.8	37.7	28 to 38	49.4	2	2.3	0	84	3
9/13/2024	10:00	1	9	16	17	13	20	17	1	0	0	0	0	0	0	0	0	0	0	94	30.6	40	32 to 42	44.7	1	1.1	0	90	4
9/13/2024	11:00	0	7	26	22	24	18	8	2	0	0	0	0	0	0	0	0	0	0	107	29.5	37.4	21 to 31	49.5	1	0.9	2	98	7
9/13/2024	12:00	0	3	34	36	23	25	24	5	0	0	0	0	0	0	0	0	0	0	150	31.1	39.7	21 to 31	52.0	3	2.0	1	144	5
9/13/2024	13:00	0	12	40	36	28	42	10	3	1	0	0	0	0	0	0	0	0	0	172	29.9	37.1	22 to 32	48.8	3	1.7	0	163	9
9/13/2024	14:00	1	25	60	45	20	19	3	1	0	0	0	0	0	0	0	0	0	0	174	25.9	33.6	18 to 28	67.2	*	*	3	166	5
9/13/2024	15:00	0	12	28	36	33	36	12	3	0	0	1	0	0	0	0	0	0	0	161	30.1	37.2	25 to 35	50.9	4	2.5	1	153	7
9/13/2024	16:00	0	12	47	42	38	31	24	5	0	0	0	0	0	0	0	0	0	0	199	30.1	38.7	22 to 32	50.3	2	1.0	2	190	7
9/13/2024	17:00	0	9	45	49	22	30	34	5	0	0	0	0	0	0	0	0	0	0	194	30.7	39.9	19 to 29	51.0	1	0.5	2	187	5
9/13/2024	18:00	0	5	24	29	30	32	27	4	0	0	0	0	0	0	0	0	0	0	151	32.4	40.5	32 to 42	49.7	3	2.0	1	143	7
9/13/2024	19:00	1	10	12	16	15	28	11	3	0	0	0	0	0	0	0	0	0	0	96	31.2	39	31 to 41	53.1	1	1.0	1	90	5
9/13/2024	20:00	0	2	13	21	18	24	7	3	0	0	0	0	0	0	0	0	0	0	88	31.6	38.2	26 to 36	55.7	3	3.4	2	84	2
9/13/2024	21:00	0	0	8	8	12	19	4	2	1	0	0	0	0	0	0	0	0	0	54	33.1	38.8	29 to 39	59.3	2	3.7	1	52	1
9/13/2024	22:00	0	3	7	8	8	6	4	0	0	0	0	0	0	0	0	0	0	0	36	29.8	37	23 to 33	55.6	*	*	1	33	2
9/13/2024	23:00	0	2	4	3	1	1	4	1	0	0	0	0	0	0	0	0	0	0	16	30.8	42.5	19 to 29	56.3	1	6.3	0	15	1
24 Hr Summary		5	145	473	449	359	406	226	45	3	0	1	0	1	0	0	0	0	0	2113	30	39	21 to 31	46.6	32	1.5	19	2008	86

Outgoing Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/14/2024	00:00	0	2	5	2	2	2	1	0	0	0	0	0	0	0	0	0	0	0	14	26.8	35	16 to 26	57.1	*	*	0	13	1
9/14/2024	01:00	1	2	1	0	2	1	2	0	0	1	0	0	0	0	0	0	0	0	10	30.5	40	13 to 23	40.0	1	10.0	0	8	2
9/14/2024	02:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	49	49	39 to 49	100.0	1	100.0	0	1	0
9/14/2024	03:00	0	1	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	3	29.7	45	15 to 25	66.7	*	*	0	3	0
9/14/2024	04:00	0	2	4	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	10	25.5	31	16 to 26	70.0	*	*	0	9	1
9/14/2024	05:00	0	1	4	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	6	22.7	22	12 to 22	83.3	*	*	0	5	1
9/14/2024	06:00	0	2	5	3	4	6	2	0	0	0	0	0	0	0	0	0	0	0	22	30	38.5	30 to 40	50.0	*	*	1	18	3
9/14/2024	07:00	0	4	9	13	6	6	5	1	0	0	0	0	0	0	0	0	0	0	44	29.3	37.5	21 to 31	59.1	*	*	1	41	2
9/14/2024	08:00	0	2	8	12	9	18	7	2	1	0	0	0	0	0	0	0	0	0	59	32.5	39.3	30 to 40	50.8	3	5.1	0	56	3
9/14/2024	09:00	1	3	17	16	15	29	8	2	0	0	0	0	0	0	0	0	0	0	91	31.1	38	28 to 38	51.6	*	*	0	90	1
9/14/2024	10:00	0	3	23	21	14	14	19	0	0	0	0	0	0	0	0	0	0	0	94	30.8	40.6	19 to 29	47.9	*	*	1	88	5
9/14/2024	11:00	0	5	23	17	26	18	13	4	0	0	0	0	0	0	0	0	0	0	106	30.9	39.2	23 to 33	48.1	1	0.9	0	104	2
9/14/2024	12:00	0	8	28	41	32	32	11	2	0	0	0	0	0	0	0	0	0	0	154	30.1	37.8	24 to 34	50.6	1	0.6	0	146	8
9/14/2024	13:00	0	6	28	22	22	40	10	1	0	0	0	0	0	0	0	0	0	0	129	30.8	38	29 to 39	50.4	1	0.8	0	123	6
9/14/2024	14:00	1	4	20	36	36	21	12	5	0	1	0	0	0	0	0	0	0	0	136	30.9	38.3	24 to 34	56.6	3	2.2	0	131	5
9/14/2024	15:00	0	6	21	26	14	31	21	4	1	0	0	0	0	0	0	0	0	0	124	31.9	41	33 to 43	45.2	4	3.2	0	118	6
9/14/2024	16:00	0	6	14	14	37	25	11	3	0	0	0	0	0	0	0	0	0	0	110	31.9	38	28 to 38	63.6	3	2.7	2	104	4
9/14/2024	17:00	0	4	22	12	18	26	15	5	0	1	0	0	0	0	0	0	0	0	103	32.3	40.3	32 to 42	54.4	5	4.9	0	99	4
9/14/2024	18:00	0	2	20	12	21	17	23	1	0	0	0	0	0	0	0	0	0	0	96	32.1	40	31 to 41	56.3	*	*	1	92	3
9/14/2024	19:00	0	4	13	8	14	22	6	1	0	0	0	1	0	0	0	0	0	0	69	32	38.7	31 to 41	55.1	2	2.9	0	68	1
9/14/2024	20:00	0	1	10	12	15	8	10	3	0	0	0	0	0	0	0	0	0	0	59	31.9	40	27 to 37	54.2	1	1.7	0	59	0
9/14/2024	21:00	0	3	9	10	6	11	1	1	0	0	0	0	0	0	0	0	0	0	41	29.5	37	18 to 28	51.2	1	2.4	0	41	0
9/14/2024	22:00	0	1	6	7	4	4	3	1	0	0	0	0	0	0	0	0	0	0	26	29.9	39	22 to 32	57.7	*	*	0	25	1
9/14/2024	23:00	0	1	1	5	1	4	1	1	0	0	0	0	0	0	0	0	0	0	14	32.4	39	27 to 37	57.1	1	7.1	0	14	0
24 Hr Summary		3	73	291	291	299	337	182	39	2	3	0	1	0	0	0	0	0	0	1521	31.1	39	28 to 38	45.6	28	1.8	6	1456	59

Outgoing Histogram

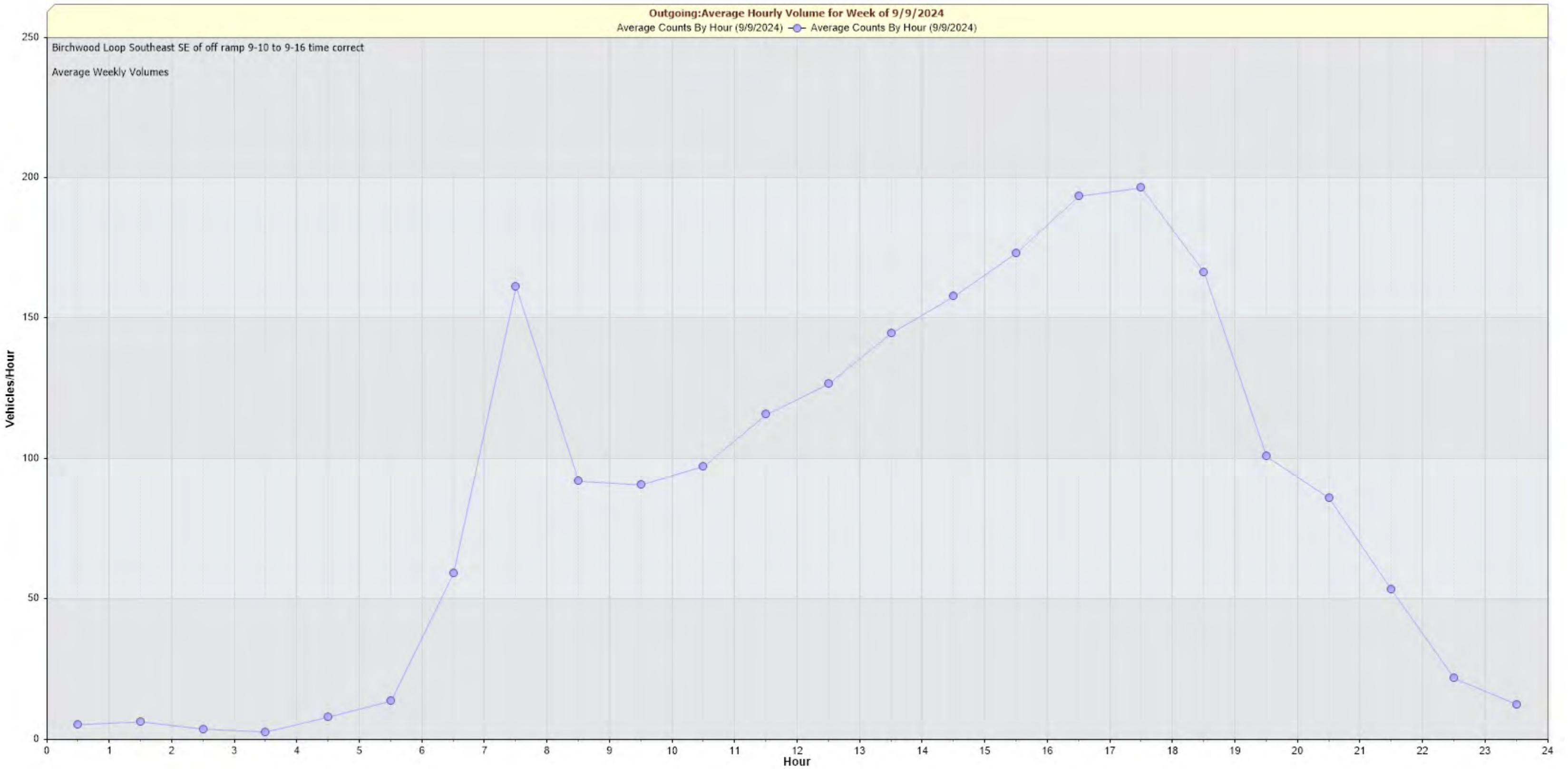
Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG		
9/15/2024	00:00	0	1	2	2	3	2	1	0	0	0	0	0	0	0	0	0	0	0	11	29.7	36	23 to 33	54.5	*	*	0	10	1		
9/15/2024	01:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	35	38	28 to 38	100.0	*	*	0	2	0		
9/15/2024	02:00	0	0	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	0	3	32.7	35	25 to 35	100.0	*	*	0	2	1		
9/15/2024	03:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27.5	31	21 to 31	100.0	*	*	0	2	0		
9/15/2024	04:00	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	3	34	35	25 to 35	100.0	*	*	0	3	0		
9/15/2024	05:00	0	0	1	1	2	2	0	1	0	0	0	0	0	0	0	0	0	0	7	32.9	38	25 to 35	57.1	*	*	0	7	0		
9/15/2024	06:00	0	1	3	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	8	28	37	16 to 26	62.5	*	*	0	8	0		
9/15/2024	07:00	0	0	0	1	3	3	0	2	1	0	0	0	0	0	0	0	0	0	10	38.1	47	26 to 36	60.0	3	30.0	0	10	0		
9/15/2024	08:00	0	0	6	2	8	3	11	0	0	0	0	0	0	0	0	0	0	0	30	34	42.5	30 to 40	53.3	*	*	0	30	0		
9/15/2024	09:00	0	0	8	10	8	16	12	5	0	0	0	0	0	0	0	0	0	0	59	34.9	42.7	34 to 44	50.8	5	8.5	0	59	0		
9/15/2024	10:00	0	5	9	14	22	18	13	4	0	0	0	0	0	0	0	0	0	0	85	32.5	41.2	30 to 40	50.6	3	3.5	0	84	1		
9/15/2024	11:00	0	5	16	12	10	16	14	6	1	0	0	0	0	0	0	0	0	0	80	32.3	41.5	32 to 42	43.8	3	3.8	0	77	3		
9/15/2024	12:00	0	1	18	22	32	51	24	2	0	0	0	0	0	0	0	0	0	0	150	33.6	39.4	31 to 41	66.7	1	0.7	1	147	2		
9/15/2024	13:00	0	2	16	34	21	34	22	4	0	0	0	0	0	0	0	0	0	0	133	32.4	39.6	30 to 40	48.9	3	2.3	2	130	1		
9/15/2024	14:00	0	0	3	6	5	14	2	0	0	0	0	0	0	0	0	0	0	0	30	33.2	37.7	28 to 38	70.0	*	*	0	27	3		
9/15/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	15	83	106	119	164	100	24	2	0	0	0	0	0	0	0	0	0	613	33	41	31 to 41	52.0	18	2.9	3	598	12		

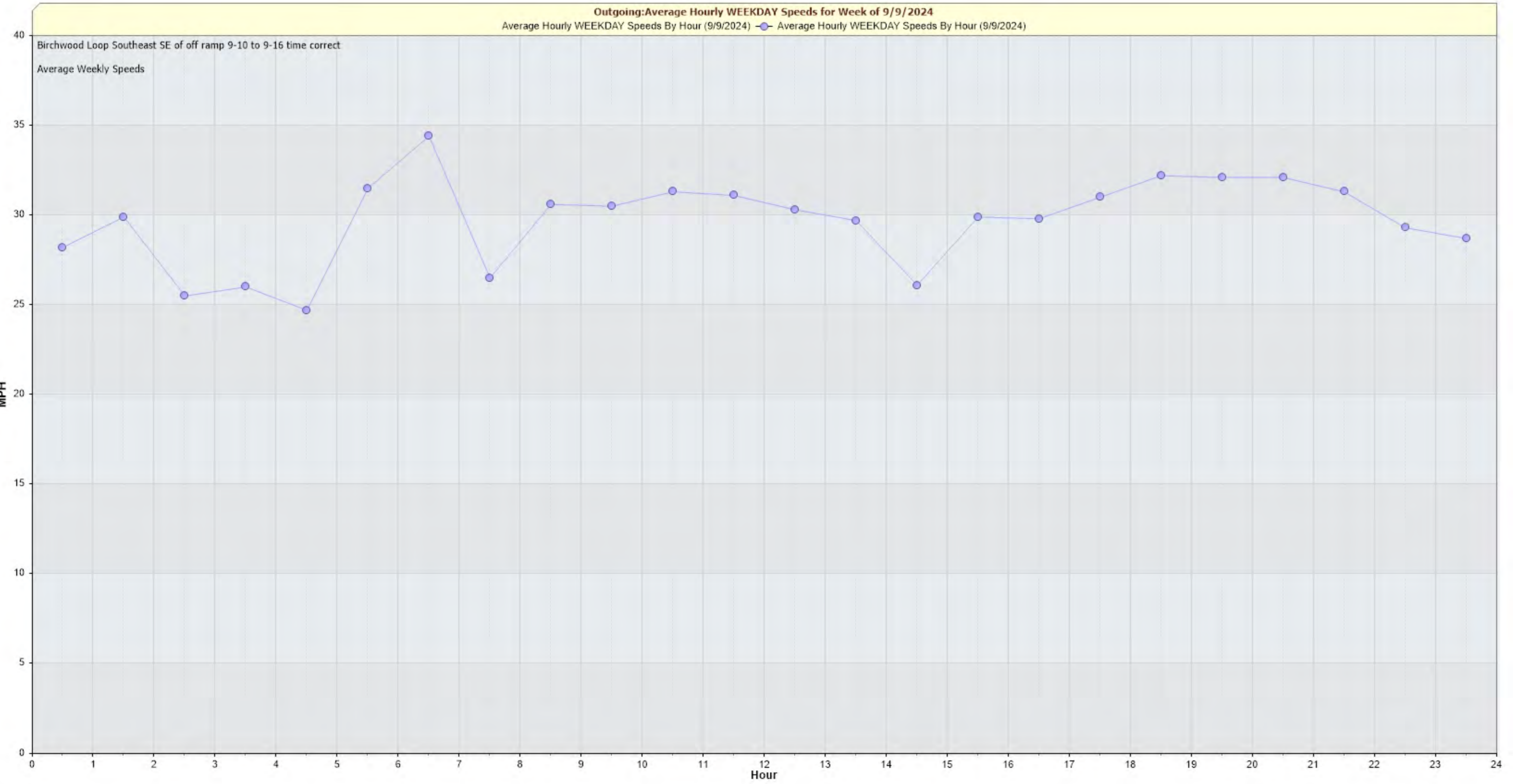
Outgoing:Average Hourly Volume for Week of 9/9/2024
Average Counts By Hour (9/9/2024) ● Average Counts By Hour (9/9/2024)

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/9/2024
Average Hourly WEEKDAY Speeds By Hour (9/9/2024) — Average Hourly WEEKDAY Speeds By Hour (9/9/2024)

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct
Average Weekly Speeds

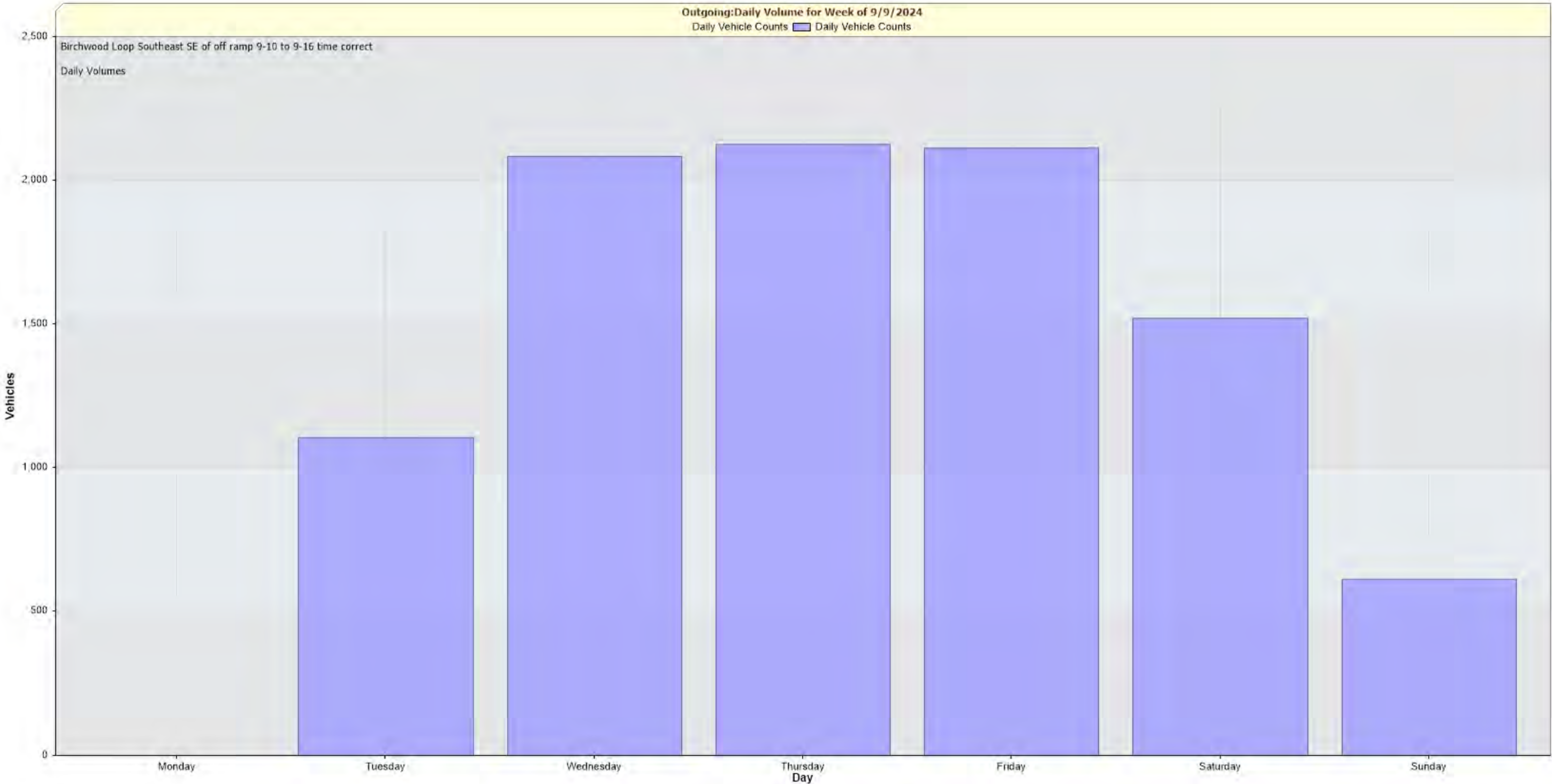


Outgoing: Daily Volume for Week of 9/9/2024

Daily Vehicle Counts

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

Daily Volumes



For Project: Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:54:21 PM

Speed Intervals: 1 MPH

Time Intervals: Instant

Traffic Report From: 9/10/2024 2:00:00 PM through 9/16/2024 7:59:59 AM

85th Percentile Speed: 48 MPH

85th Percentile Vehicles: 7031

Max Speed: 76 MPH on 9/14/2024 11:53:10 AM

Total Vehicles: 8272

AADT: 1438

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1181	1181
AM Peak 11:00 AM	82	83
PM Peak 5:00 PM	173	146

Speed

Speed Limit: 65

85th Percentile Speed: 48

50th Percentile Speed: 43

10 MPH Pace Interval: 38.0 MPH to 48.0 MPH

Average Speed: 43.11

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	1	1	0	1	2	2	2
% over limit	1.8	0.1	0.0	0.1	0.1	0.2	0.2
Avg Speeder	70.0	66.0	0.0	66.0	70.5	72.0	73.5
Avg Speed	42.6	43.3	43.4	42.9	43.1	42.6	43.5

Class Counts

	Number	%
VEH_SM	23	0.3
VEH_MED	7800	94.3
VEH_LG	449	5.4
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/10/2024 3:00:00 PM	47.0	133	157	55	0.0	0.0%	41.7
9/10/2024 4:00:00 PM	47.0	148	174	63	0.0	0.0%	42.2
9/10/2024 5:00:00 PM	49.0	158	186	59	0.0	0.0%	44.1
9/10/2024 6:00:00 PM	51.0	149	175	61	0.0	0.0%	45.0
9/10/2024 7:00:00 PM	49.0	98	115	66	66.0	0.9%	44.1
9/10/2024 8:00:00 PM	49.0	71	84	54	0.0	0.0%	43.8
9/10/2024 9:00:00 PM	47.0	55	65	64	0.0	0.0%	42.8
9/10/2024 10:00:00 PM	45.0	22	26	52	0.0	0.0%	42.4
9/10/2024 11:00:00 PM	48.0	11	13	52	0.0	0.0%	42.4
9/11/2024 12:00:00 AM	36.0	4	5	36	0.0	0.0%	34.0
9/11/2024 1:00:00 AM	44.0	3	3	44	0.0	0.0%	42.0
9/11/2024 2:00:00 AM	48.0	4	5	48	0.0	0.0%	43.2
9/11/2024 3:00:00 AM	47.0	3	3	47	0.0	0.0%	42.3
9/11/2024 4:00:00 AM	**No Data**						
9/11/2024 5:00:00 AM	43.0	2	2	43	0.0	0.0%	39.0
9/11/2024 6:00:00 AM	43.0	5	6	53	0.0	0.0%	42.3
9/11/2024 7:00:00 AM	48.0	19	22	51	0.0	0.0%	44.7
9/11/2024 8:00:00 AM	47.0	73	86	55	0.0	0.0%	42.3
9/11/2024 9:00:00 AM	47.0	78	92	58	0.0	0.0%	42.3
9/11/2024 10:00:00 AM	48.0	71	83	55	0.0	0.0%	42.0
9/11/2024 11:00:00 AM	48.0	56	66	55	0.0	0.0%	43.9
9/11/2024 12:00:00 PM	49.0	68	80	55	0.0	0.0%	43.4
9/11/2024 1:00:00 PM	49.0	80	94	57	0.0	0.0%	43.4
9/11/2024 2:00:00 PM	47.0	76	89	53	0.0	0.0%	42.8
9/11/2024 3:00:00 PM	47.0	132	155	59	0.0	0.0%	42.6
9/11/2024 4:00:00 PM	48.0	122	144	64	0.0	0.0%	43.7
9/11/2024 5:00:00 PM	49.0	139	164	57	0.0	0.0%	44.6
9/11/2024 6:00:00 PM	48.0	139	163	59	0.0	0.0%	43.9
9/11/2024 7:00:00 PM	50.0	106	125	59	0.0	0.0%	44.7
9/11/2024 8:00:00 PM	48.0	71	83	59	0.0	0.0%	43.6
9/11/2024 9:00:00 PM	48.0	73	86	57	0.0	0.0%	42.8
9/11/2024 10:00:00 PM	49.0	26	30	61	0.0	0.0%	42.1
9/11/2024 11:00:00 PM	45.0	9	11	49	0.0	0.0%	41.5
9/12/2024 12:00:00 AM	48.0	8	10	55	0.0	0.0%	42.7
9/12/2024 1:00:00 AM	45.0	6	7	51	0.0	0.0%	39.4
9/12/2024 2:00:00 AM	43.0	3	3	43	0.0	0.0%	37.7
9/12/2024 3:00:00 AM	40.0	2	2	40	0.0	0.0%	38.0
9/12/2024 4:00:00 AM	39.0	3	4	44	0.0	0.0%	38.5
9/12/2024 5:00:00 AM	45.0	3	3	45	0.0	0.0%	42.0
9/12/2024 6:00:00 AM	52.0	8	9	53	0.0	0.0%	39.0
9/12/2024 7:00:00 AM	49.0	19	22	54	0.0	0.0%	43.6
9/12/2024 8:00:00 AM	45.0	76	90	56	0.0	0.0%	41.2
9/12/2024 9:00:00 AM	47.0	70	82	59	0.0	0.0%	42.2
9/12/2024 10:00:00 AM	47.0	55	65	57	0.0	0.0%	42.8
9/12/2024 11:00:00 AM	47.0	59	69	54	0.0	0.0%	41.3
9/12/2024 12:00:00 PM	46.0	73	86	51	0.0	0.0%	42.3
9/12/2024 1:00:00 PM	48.0	81	95	59	0.0	0.0%	43.1

Outgoing Summary
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

9/12/2024 2:00:00 PM	48.0	89	105	60	0.0	0.0%	43.1
9/12/2024 3:00:00 PM	48.0	122	143	58	0.0	0.0%	42.5
9/12/2024 4:00:00 PM	48.0	128	151	58	0.0	0.0%	43.0
9/12/2024 5:00:00 PM	50.0	126	148	66	66.0	0.7%	44.7
9/12/2024 6:00:00 PM	48.0	138	162	57	0.0	0.0%	44.0
9/12/2024 7:00:00 PM	49.0	114	134	64	0.0	0.0%	43.7
9/12/2024 8:00:00 PM	48.0	70	82	57	0.0	0.0%	43.5
9/12/2024 9:00:00 PM	46.0	49	58	56	0.0	0.0%	42.0
9/12/2024 10:00:00 PM	46.0	22	26	56	0.0	0.0%	41.8
9/12/2024 11:00:00 PM	46.0	10	12	53	0.0	0.0%	41.4
9/13/2024 12:00:00 AM	37.0	3	4	57	0.0	0.0%	39.5
9/13/2024 1:00:00 AM	46.0	4	5	54	0.0	0.0%	45.0
9/13/2024 2:00:00 AM	44.0	2	2	44	0.0	0.0%	38.0
9/13/2024 3:00:00 AM	44.0	3	3	44	0.0	0.0%	39.7
9/13/2024 4:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
9/13/2024 5:00:00 AM	50.0	5	6	54	0.0	0.0%	42.3
9/13/2024 6:00:00 AM	40.0	5	6	45	0.0	0.0%	39.2
9/13/2024 7:00:00 AM	49.0	21	25	57	0.0	0.0%	45.0
9/13/2024 8:00:00 AM	46.0	80	94	57	0.0	0.0%	41.8
9/13/2024 9:00:00 AM	47.0	52	61	54	0.0	0.0%	42.6
9/13/2024 10:00:00 AM	48.0	68	80	53	0.0	0.0%	41.4
9/13/2024 11:00:00 AM	48.0	55	65	52	0.0	0.0%	42.1
9/13/2024 12:00:00 PM	47.0	68	80	53	0.0	0.0%	42.7
9/13/2024 1:00:00 PM	48.0	80	94	56	0.0	0.0%	43.5
9/13/2024 2:00:00 PM	47.0	105	123	63	0.0	0.0%	43.3
9/13/2024 3:00:00 PM	48.0	139	164	55	0.0	0.0%	43.4
9/13/2024 4:00:00 PM	49.0	116	136	73	73.0	0.7%	43.6
9/13/2024 5:00:00 PM	48.0	133	156	64	0.0	0.0%	43.5
9/13/2024 6:00:00 PM	48.0	163	192	68	68.0	0.5%	43.8
9/13/2024 7:00:00 PM	48.0	121	142	58	0.0	0.0%	44.2
9/13/2024 8:00:00 PM	50.0	90	106	61	0.0	0.0%	44.2
9/13/2024 9:00:00 PM	49.0	75	88	54	0.0	0.0%	42.4
9/13/2024 10:00:00 PM	46.0	28	33	53	0.0	0.0%	40.2
9/13/2024 11:00:00 PM	49.0	13	15	57	0.0	0.0%	41.8
9/14/2024 12:00:00 AM	44.0	8	10	50	0.0	0.0%	38.9
9/14/2024 1:00:00 AM	43.0	3	4	47	0.0	0.0%	41.0
9/14/2024 2:00:00 AM	45.0	9	11	58	0.0	0.0%	41.8
9/14/2024 3:00:00 AM	68.0	3	3	68	68.0	33.3%	49.3
9/14/2024 4:00:00 AM	53.0	2	2	53	0.0	0.0%	46.5
9/14/2024 5:00:00 AM	48.0	7	8	51	0.0	0.0%	42.9
9/14/2024 6:00:00 AM	41.0	4	5	42	0.0	0.0%	37.8
9/14/2024 7:00:00 AM	43.0	11	13	50	0.0	0.0%	40.9
9/14/2024 8:00:00 AM	47.0	18	21	50	0.0	0.0%	41.9
9/14/2024 9:00:00 AM	47.0	36	42	62	0.0	0.0%	41.3
9/14/2024 10:00:00 AM	45.0	55	65	52	0.0	0.0%	41.3
9/14/2024 11:00:00 AM	48.0	99	117	62	0.0	0.0%	42.6
9/14/2024 12:00:00 PM	47.0	77	91	76	76.0	1.1%	42.7
9/14/2024 1:00:00 PM	47.0	84	99	55	0.0	0.0%	41.8

Outgoing Summary
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

9/14/2024 2:00:00 PM	48.0	88	103	58	0.0	0.0%	42.8
9/14/2024 3:00:00 PM	48.0	86	101	55	0.0	0.0%	43.4
9/14/2024 4:00:00 PM	47.0	84	99	58	0.0	0.0%	42.0
9/14/2024 5:00:00 PM	48.0	83	98	56	0.0	0.0%	42.8
9/14/2024 6:00:00 PM	48.0	87	102	56	0.0	0.0%	43.3
9/14/2024 7:00:00 PM	49.0	67	79	64	0.0	0.0%	44.5
9/14/2024 8:00:00 PM	47.0	52	61	57	0.0	0.0%	42.7
9/14/2024 9:00:00 PM	46.0	48	56	58	0.0	0.0%	41.5
9/14/2024 10:00:00 PM	46.0	29	34	56	0.0	0.0%	41.7
9/14/2024 11:00:00 PM	51.0	11	13	58	0.0	0.0%	45.8
9/15/2024 12:00:00 AM	47.0	8	10	50	0.0	0.0%	41.5
9/15/2024 1:00:00 AM	61.0	3	4	63	0.0	0.0%	48.8
9/15/2024 2:00:00 AM	45.0	3	3	45	0.0	0.0%	39.7
9/15/2024 3:00:00 AM	49.0	3	4	50	0.0	0.0%	46.5
9/15/2024 4:00:00 AM	45.0	1	1	45	0.0	0.0%	45.0
9/15/2024 5:00:00 AM	44.0	3	4	46	0.0	0.0%	44.5
9/15/2024 6:00:00 AM	42.0	1	1	42	0.0	0.0%	42.0
9/15/2024 7:00:00 AM	49.0	5	6	49	0.0	0.0%	42.7
9/15/2024 8:00:00 AM	47.0	13	15	54	0.0	0.0%	42.5
9/15/2024 9:00:00 AM	45.0	19	22	54	0.0	0.0%	41.2
9/15/2024 10:00:00 AM	46.0	33	39	53	0.0	0.0%	43.0
9/15/2024 11:00:00 AM	46.0	67	79	62	0.0	0.0%	42.3
9/15/2024 12:00:00 PM	47.0	67	79	54	0.0	0.0%	41.6
9/15/2024 1:00:00 PM	47.0	111	131	57	0.0	0.0%	43.1
9/15/2024 2:00:00 PM	48.0	90	106	56	0.0	0.0%	44.2
9/15/2024 3:00:00 PM	48.0	87	102	57	0.0	0.0%	43.6
9/15/2024 4:00:00 PM	50.0	69	81	71	71.0	1.2%	44.1
9/15/2024 5:00:00 PM	48.0	82	96	61	0.0	0.0%	43.5
9/15/2024 6:00:00 PM	49.0	72	85	60	0.0	0.0%	43.9
9/15/2024 7:00:00 PM	49.0	68	80	62	0.0	0.0%	44.6
9/15/2024 8:00:00 PM	49.0	63	74	76	76.0	1.4%	44.7
9/15/2024 9:00:00 PM	47.0	63	74	61	0.0	0.0%	43.4
9/15/2024 10:00:00 PM	49.0	15	18	59	0.0	0.0%	44.5
9/15/2024 11:00:00 PM	51.0	14	16	54	0.0	0.0%	42.9
9/16/2024 12:00:00 AM	51.0	7	8	56	0.0	0.0%	41.4
9/16/2024 1:00:00 AM	42.0	3	4	42	0.0	0.0%	38.8
9/16/2024 2:00:00 AM	48.0	3	3	48	0.0	0.0%	38.3
9/16/2024 3:00:00 AM	64.0	3	4	70	70.0	25.0%	53.3
9/16/2024 4:00:00 AM	**No Data**						
9/16/2024 5:00:00 AM	45.0	4	5	48	0.0	0.0%	41.6
9/16/2024 6:00:00 AM	45.0	4	5	49	0.0	0.0%	40.4
9/16/2024 7:00:00 AM	47.0	21	25	52	0.0	0.0%	43.4
9/16/2024 8:00:00 AM	44.0	8	10	48	0.0	0.0%	40.6

Outgoing Summary
Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/11/2024 12:00:00 AM	49.0	850	1000	66	66.0	0.1%	43.3
9/12/2024 12:00:00 AM	48.0	1362	1602	64	0.0	0.0%	43.4
9/13/2024 12:00:00 AM	48.0	1328	1562	66	66.0	0.1%	42.9
9/14/2024 12:00:00 AM	48.0	1434	1687	73	70.5	0.1%	43.1
9/15/2024 12:00:00 AM	48.0	1051	1237	76	72.0	0.2%	42.6
9/16/2024 12:00:00 AM	48.0	959	1128	76	73.5	0.2%	43.5
9/16/2024 7:59:59 AM	48.0	48	56	70	70.0	1.8%	42.6

Outgoing Weekly Counts

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	3	7	5	4	4	5	4	45
1 - 2	*	*	5	3	2	11	3	3.33	7	44.83
2 - 3	*	*	3	2	3	3	4	2.67	3.5	43.67
3 - 4	*	*	0	4	1	2	1	1.67	1.5	37
4 - 5	*	*	2	3	6	8	4	3.67	6	46
5 - 6	*	*	6	9	6	5	1	7	3	45
6 - 7	*	*	22	22	25	13	6	23	9.5	48.5
7 - 8	*	*	86	90	94	21	15	90	18	45.6
8 - 9	*	*	92	82	61	42	22	78.33	32	46.67
9 - 10	*	*	83	65	80	65	39	76	52	47.23
10 - 11	*	*	66	69	65	117	79	66.67	98	47.4
11 - 12	*	*	80	86	80	91	79	82	85	47.13
12 - 13	*	*	94	95	94	99	131	94.33	115	47.63
13 - 14	*	*	89	105	123	103	106	105.67	104.5	47.27
14 - 15	*	157	155	143	164	101	102	154.75	101.5	47.1
15 - 16	*	174	144	151	136	99	81	151.25	90	47.7
16 - 17	*	186	164	148	156	98	96	163.5	97	48.6
17 - 18	*	175	163	162	192	102	85	173	93.5	48.5
18 - 19	*	115	125	134	142	79	80	129	79.5	48.42
19 - 20	*	84	83	82	106	61	74	88.75	67.5	48.35
20 - 21	*	65	86	58	88	56	74	74.25	65	46.98
21 - 22	*	26	30	26	33	34	18	28.75	26	46.38
22 - 23	*	13	11	12	15	13	16	12.75	14.5	46.92
23 - 24	*	5	10	4	10	10	8	7.25	9	41.12
Totals	0	1000	1602	1562	1687	1237	1128			
% of Total	0%	12.17%	19.5%	19.01%	20.53%	15.06%	13.73%			

Outgoing Weekly Counts

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	4	*	*	*	*	*	*	4	0	41.5
1 - 2	3	*	*	*	*	*	*	3	0	48
2 - 3	4	*	*	*	*	*	*	4	0	64
3 - 4	0	*	*	*	*	*	*	0	0	0
4 - 5	5	*	*	*	*	*	*	5	0	45
5 - 6	5	*	*	*	*	*	*	5	0	45
6 - 7	25	*	*	*	*	*	*	25	0	46.5
7 - 8	10	*	*	*	*	*	*	10	0	44
8 - 9	*	*	*	*	*	*	*	0	0	0
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	*	*	*	0	0	0
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	56	0	0	0	0	0	0			
% of Total	100%	0%	0%	0%	0%	0%	0%			

Outgoing Monthly Counts

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	4	*	3	7	5	4	4	4.75	4	44.12
1 - 2	3	*	5	3	2	11	3	3.25	7	45.62
2 - 3	4	*	3	2	3	3	4	3	3.5	48.75
3 - 4	0	*	0	4	1	2	1	1.25	1.5	37
4 - 5	5	*	2	3	6	8	4	4	6	45.75
5 - 6	5	*	6	9	6	5	1	6.5	3	45
6 - 7	25	*	22	22	25	13	6	23.5	9.5	48
7 - 8	10	*	86	90	94	21	15	70	18	45.2
8 - 9	*	*	92	82	61	42	22	78.33	32	46.67
9 - 10	*	*	83	65	80	65	39	76	52	47.23
10 - 11	*	*	66	69	65	117	79	66.67	98	47.4
11 - 12	*	*	80	86	80	91	79	82	85	47.13
12 - 13	*	*	94	95	94	99	131	94.33	115	47.63
13 - 14	*	*	89	105	123	103	106	105.67	104.5	47.27
14 - 15	*	157	155	143	164	101	102	154.75	101.5	47.1
15 - 16	*	174	144	151	136	99	81	151.25	90	47.7
16 - 17	*	186	164	148	156	98	96	163.5	97	48.6
17 - 18	*	175	163	162	192	102	85	173	93.5	48.5
18 - 19	*	115	125	134	142	79	80	129	79.5	48.42
19 - 20	*	84	83	82	106	61	74	88.75	67.5	48.35
20 - 21	*	65	86	58	88	56	74	74.25	65	46.98
21 - 22	*	26	30	26	33	34	18	28.75	26	46.38
22 - 23	*	13	11	12	15	13	16	12.75	14.5	46.92
23 - 24	*	5	10	4	10	10	8	7.25	9	41.12
Totals	56	1000	1602	1562	1687	1237	1128			
% of Total	0.68%	12.09%	19.37%	18.88%	20.39%	14.95%	13.64%			

Outgoing Weekly Speeds

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	42	39.43	45	41	48.75	41.8	44.88	45
1 - 2	*	*	43.2	37.67	38	41.82	39.67	40.5	41.36	44.83
2 - 3	*	*	42.33	38	39.67	49.33	46.5	40.25	47.71	43.67
3 - 4	*	*	0	38.5	35	46.5	45	37.8	46	37
4 - 5	*	*	39	42	42.33	42.88	44.5	41.64	43.42	46
5 - 6	*	*	42.33	39	39.17	37.8	42	40	38.5	45
6 - 7	*	*	44.68	43.59	45.04	40.92	42.67	44.46	41.47	48.5
7 - 8	*	*	42.27	41.16	41.81	41.86	42.53	41.74	42.14	45.6
8 - 9	*	*	42.28	42.17	42.59	41.31	41.23	42.32	41.28	46.67
9 - 10	*	*	41.98	42.77	41.44	41.28	42.97	42.01	41.91	47.23
10 - 11	*	*	43.86	41.33	42.06	42.65	42.33	42.4	42.52	47.4
11 - 12	*	*	43.42	42.27	42.69	42.75	41.57	42.78	42.2	47.13
12 - 13	*	*	43.43	43.06	43.54	41.79	43.14	43.34	42.56	47.63
13 - 14	*	*	42.8	43.12	43.28	42.83	44.15	43.09	43.5	47.27
14 - 15	*	41.71	42.64	42.53	43.38	43.39	43.63	42.58	43.51	47.1
15 - 16	*	42.23	43.69	42.95	43.65	42.03	44.07	43.08	42.95	47.7
16 - 17	*	44.11	44.62	44.7	43.53	42.83	43.51	44.23	43.16	48.6
17 - 18	*	44.97	43.92	43.95	43.75	43.28	43.91	44.14	43.57	48.5
18 - 19	*	44.08	44.69	43.69	44.15	44.54	44.64	44.15	44.59	48.42
19 - 20	*	43.75	43.6	43.48	44.18	42.67	44.73	43.78	43.8	48.35
20 - 21	*	42.8	42.81	42	42.42	41.52	43.36	42.54	42.57	46.98
21 - 22	*	42.42	42.13	41.81	40.21	41.68	44.5	41.57	42.65	46.38
22 - 23	*	42.38	41.55	41.42	41.8	45.85	42.88	41.8	44.21	46.92
23 - 24	*	34	42.7	39.5	38.9	41.5	41.38	39.45	41.44	41.12
Totals	0	43.3	43.4	42.9	43.1	42.6	43.5			
% of Total	0%	16.73%	16.77%	16.58%	16.65%	16.46%	16.81%			

Outgoing Weekly Speeds

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	38.75	*	*	*	*	*	*	38.75	*	41.5
1 - 2	38.33	*	*	*	*	*	*	38.33	*	48
2 - 3	53.25	*	*	*	*	*	*	53.25	*	64
3 - 4	0	*	*	*	*	*	*	*	*	0
4 - 5	41.6	*	*	*	*	*	*	41.6	*	45
5 - 6	40.4	*	*	*	*	*	*	40.4	*	45
6 - 7	43.44	*	*	*	*	*	*	43.44	*	46.5
7 - 8	40.6	*	*	*	*	*	*	40.6	*	44
8 - 9	*	*	*	*	*	*	*	*	*	0
9 - 10	*	*	*	*	*	*	*	*	*	0
10 - 11	*	*	*	*	*	*	*	*	*	0
11 - 12	*	*	*	*	*	*	*	*	*	0
12 - 13	*	*	*	*	*	*	*	*	*	0
13 - 14	*	*	*	*	*	*	*	*	*	0
14 - 15	*	*	*	*	*	*	*	*	*	0
15 - 16	*	*	*	*	*	*	*	*	*	0
16 - 17	*	*	*	*	*	*	*	*	*	0
17 - 18	*	*	*	*	*	*	*	*	*	0
18 - 19	*	*	*	*	*	*	*	*	*	0
19 - 20	*	*	*	*	*	*	*	*	*	0
20 - 21	*	*	*	*	*	*	*	*	*	0
21 - 22	*	*	*	*	*	*	*	*	*	0
22 - 23	*	*	*	*	*	*	*	*	*	0
23 - 24	*	*	*	*	*	*	*	*	*	0
Totals	42.6	0	0	0	0	0	0			
% of Total	100%	0%	0%	0%	0%	0%	0%			

Outgoing Monthly Speeds

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	38.75	*	42	39.43	45	41	48.75	41.16	44.88	44.12
1 - 2	38.33	*	43.2	37.67	38	41.82	39.67	40	41.36	45.62
2 - 3	53.25	*	42.33	38	39.67	49.33	46.5	44.58	47.71	48.75
3 - 4	0	*	0	38.5	35	46.5	45	37.8	46	37
4 - 5	41.6	*	39	42	42.33	42.88	44.5	41.62	43.42	45.75
5 - 6	40.4	*	42.33	39	39.17	37.8	42	40.08	38.5	45
6 - 7	43.44	*	44.68	43.59	45.04	40.92	42.67	44.19	41.47	48
7 - 8	40.6	*	42.27	41.16	41.81	41.86	42.53	41.7	42.14	45.2
8 - 9	*	*	42.28	42.17	42.59	41.31	41.23	42.32	41.28	46.67
9 - 10	*	*	41.98	42.77	41.44	41.28	42.97	42.01	41.91	47.23
10 - 11	*	*	43.86	41.33	42.06	42.65	42.33	42.4	42.52	47.4
11 - 12	*	*	43.42	42.27	42.69	42.75	41.57	42.78	42.2	47.13
12 - 13	*	*	43.43	43.06	43.54	41.79	43.14	43.34	42.56	47.63
13 - 14	*	*	42.8	43.12	43.28	42.83	44.15	43.09	43.5	47.27
14 - 15	*	41.71	42.64	42.53	43.38	43.39	43.63	42.58	43.51	47.1
15 - 16	*	42.23	43.69	42.95	43.65	42.03	44.07	43.08	42.95	47.7
16 - 17	*	44.11	44.62	44.7	43.53	42.83	43.51	44.23	43.16	48.6
17 - 18	*	44.97	43.92	43.95	43.75	43.28	43.91	44.14	43.57	48.5
18 - 19	*	44.08	44.69	43.69	44.15	44.54	44.64	44.15	44.59	48.42
19 - 20	*	43.75	43.6	43.48	44.18	42.67	44.73	43.78	43.8	48.35
20 - 21	*	42.8	42.81	42	42.42	41.52	43.36	42.54	42.57	46.98
21 - 22	*	42.42	42.13	41.81	40.21	41.68	44.5	41.57	42.65	46.38
22 - 23	*	42.38	41.55	41.42	41.8	45.85	42.88	41.8	44.21	46.92
23 - 24	*	34	42.7	39.5	38.9	41.5	41.38	39.45	41.44	41.12
Totals	42.6	43.3	43.4	42.9	43.1	42.6	43.5			
% of Total	14.13%	14.37%	14.4%	14.23%	14.3%	14.13%	14.43%			

Outgoing Weekly EightyFifthSpeeds
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	44	45	46	43	61	45	52	45
1 - 2	*	*	47.5	43	44	45	45	44.83	45	44.83
2 - 3	*	*	47	40	44	68	49	43.67	58.5	43.67
3 - 4	*	*	0	39	35	53	45	37	49	37
4 - 5	*	*	43	45	50	48	44	46	46	46
5 - 6	*	*	43	52	40	41	42	45	41.5	45
6 - 7	*	*	48	48.5	49	43	48.5	48.5	45.75	48.5
7 - 8	*	*	46.5	44.9	45.4	46.5	47	45.6	46.75	45.6
8 - 9	*	*	46.2	47	46.8	47	45	46.67	46	46.67
9 - 10	*	*	47.5	46.5	47.7	44.4	46	47.23	45.2	47.23
10 - 11	*	*	48	46.2	48	47.2	46	47.4	46.6	47.4
11 - 12	*	*	48.8	46	46.6	47	46.3	47.13	46.65	47.13
12 - 13	*	*	48.3	47.1	47.5	46.2	46.7	47.63	46.45	47.63
13 - 14	*	*	46.8	48	47	47.3	47.3	47.27	47.3	47.27
14 - 15	*	46.5	46.7	47.6	47.6	48	48	47.1	48	47.1
15 - 16	*	46.6	47.5	47.7	49	46.3	49.5	47.7	47.9	47.7
16 - 17	*	48.7	48.7	49.8	47.2	47.5	47.3	48.6	47.4	48.6
17 - 18	*	50.5	48	47.6	47.9	47.3	48.7	48.5	48	48.5
18 - 19	*	48.4	49.1	48.3	47.9	48.8	48.8	48.42	48.8	48.42
19 - 20	*	48.5	48	47.6	49.3	46.5	49	48.35	47.75	48.35
20 - 21	*	46.3	47.7	45.8	48.1	46	47	46.98	46.5	46.98
21 - 22	*	45	49	45.5	46	45.7	49	46.38	47.35	46.38
22 - 23	*	48	45	45.7	49	51	51	46.92	51	46.92
23 - 24	*	35.5	48	37	44	46.5	51	41.12	48.75	41.12
Totals	0	464	1082.3	1100.8	1113	1140.2	1148.1			
% of Total	0%	7.67%	17.89%	18.2%	18.4%	18.85%	18.98%			

Outgoing Weekly EightyFifthSpeeds
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	41.5	*	*	*	*	*	*	41.5	0	41.5
1 - 2	48	*	*	*	*	*	*	48	0	48
2 - 3	64	*	*	*	*	*	*	64	0	64
3 - 4	0	*	*	*	*	*	*	0	0	0
4 - 5	45	*	*	*	*	*	*	45	0	45
5 - 6	45	*	*	*	*	*	*	45	0	45
6 - 7	46.5	*	*	*	*	*	*	46.5	0	46.5
7 - 8	44	*	*	*	*	*	*	44	0	44
8 - 9	*	*	*	*	*	*	*	0	0	0
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	*	*	*	0	0	0
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	334	0	0	0	0	0	0			
% of Total	100%	0%	0%	0%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	41.5	*	44	45	46	43	61	44.12	52	44.12
1 - 2	48	*	47.5	43	44	45	45	45.62	45	45.62
2 - 3	64	*	47	40	44	68	49	48.75	58.5	48.75
3 - 4	0	*	0	39	35	53	45	37	49	37
4 - 5	45	*	43	45	50	48	44	45.75	46	45.75
5 - 6	45	*	43	52	40	41	42	45	41.5	45
6 - 7	46.5	*	48	48.5	49	43	48.5	48	45.75	48
7 - 8	44	*	46.5	44.9	45.4	46.5	47	45.2	46.75	45.2
8 - 9	*	*	46.2	47	46.8	47	45	46.67	46	46.67
9 - 10	*	*	47.5	46.5	47.7	44.4	46	47.23	45.2	47.23
10 - 11	*	*	48	46.2	48	47.2	46	47.4	46.6	47.4
11 - 12	*	*	48.8	46	46.6	47	46.3	47.13	46.65	47.13
12 - 13	*	*	48.3	47.1	47.5	46.2	46.7	47.63	46.45	47.63
13 - 14	*	*	46.8	48	47	47.3	47.3	47.27	47.3	47.27
14 - 15	*	46.5	46.7	47.6	47.6	48	48	47.1	48	47.1
15 - 16	*	46.6	47.5	47.7	49	46.3	49.5	47.7	47.9	47.7
16 - 17	*	48.7	48.7	49.8	47.2	47.5	47.3	48.6	47.4	48.6
17 - 18	*	50.5	48	47.6	47.9	47.3	48.7	48.5	48	48.5
18 - 19	*	48.4	49.1	48.3	47.9	48.8	48.8	48.42	48.8	48.42
19 - 20	*	48.5	48	47.6	49.3	46.5	49	48.35	47.75	48.35
20 - 21	*	46.3	47.7	45.8	48.1	46	47	46.98	46.5	46.98
21 - 22	*	45	49	45.5	46	45.7	49	46.38	47.35	46.38
22 - 23	*	48	45	45.7	49	51	51	46.92	51	46.92
23 - 24	*	35.5	48	37	44	46.5	51	41.12	48.75	41.12

Summary of Violators

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	27	42.3	0	0.0
01:00:00	27	40.7	0	0.0
02:00:00	19	45.7	2	69.0
03:00:00	8	40.9	0	0.0
04:00:00	28	42.4	0	0.0
05:00:00	32	39.8	0	0.0
06:00:00	113	43.7	0	0.0
07:00:00	316	41.7	0	0.0
08:00:00	299	42.1	0	0.0
09:00:00	332	42.0	0	0.0
10:00:00	396	42.5	0	0.0
11:00:00	416	42.5	1	76.0
12:00:00	513	43.0	0	0.0
13:00:00	526	43.3	0	0.0
14:00:00	822	42.8	0	0.0
15:00:00	785	43.0	2	72.0
16:00:00	848	44.0	1	66.0
17:00:00	879	44.0	1	68.0
18:00:00	675	44.3	1	66.0
19:00:00	490	43.8	1	76.0
20:00:00	427	42.5	0	0.0
21:00:00	167	41.9	0	0.0
22:00:00	80	42.7	0	0.0
23:00:00	47	40.2	0	0.0

Outgoing Histogram
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	14:00	0	0	1	0	13	35	67	29	11	1	0	0	0	0	0	0	0	0	157	41.7	46.5	36 to 46	73.2	*	*	0	146	11	
9/10/2024	15:00	0	0	1	0	6	38	83	37	7	1	1	0	0	0	0	0	0	0	174	42.2	46.6	38 to 48	77.6	*	*	0	159	15	
9/10/2024	16:00	0	0	0	1	3	28	69	58	22	5	0	0	0	0	0	0	0	0	186	44.1	48.7	38 to 48	78.0	*	*	0	175	11	
9/10/2024	17:00	0	0	0	0	4	24	60	54	19	13	1	0	0	0	0	0	0	0	175	45	50.5	38 to 48	69.7	*	*	0	167	8	
9/10/2024	18:00	0	0	0	0	2	18	44	38	10	1	1	1	0	0	0	0	0	0	115	44.1	48.4	39 to 49	74.8	1	0.9	0	110	5	
9/10/2024	19:00	0	0	0	0	0	16	34	24	10	0	0	0	0	0	0	0	0	0	84	43.8	48.5	39 to 49	77.4	*	*	0	81	3	
9/10/2024	20:00	0	0	0	0	0	13	32	15	3	0	2	0	0	0	0	0	0	0	65	42.8	46.3	38 to 48	81.5	*	*	0	62	3	
9/10/2024	21:00	0	0	0	0	2	3	15	3	3	0	0	0	0	0	0	0	0	0	26	42.4	45	35 to 45	76.9	*	*	0	25	1	
9/10/2024	22:00	0	0	0	1	0	4	2	4	2	0	0	0	0	0	0	0	0	0	13	42.4	48	38 to 48	69.2	*	*	0	12	1	
9/10/2024	23:00	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	5	34	35.5	26 to 36	100.0	*	*	0	5	0	
24 Hr Summary		0	0	2	2	33	181	406	262	87	21	5	1	0	0	0	0	0	0	1000	43.3	49	38 to 48	73.1	1	0.1	0	942	58	

Outgoing Histogram

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/11/2024	00:00	0	0	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	3	42	44	34 to 44	100.0	*	*	0	3	0	
9/11/2024	01:00	0	0	0	0	0	1	2	2	0	0	0	0	0	0	0	0	0	0	5	43.2	47.5	38 to 48	80.0	*	*	1	3	1	
9/11/2024	02:00	0	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	3	42.3	47	37 to 47	100.0	*	*	0	3	0	
9/11/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/11/2024	04:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	39	43	33 to 43	100.0	*	*	0	1	1	
9/11/2024	05:00	0	0	0	0	0	1	4	0	1	0	0	0	0	0	0	0	0	0	6	42.3	43	33 to 43	83.3	*	*	0	6	0	
9/11/2024	06:00	0	0	0	0	0	1	11	7	3	0	0	0	0	0	0	0	0	0	22	44.7	48	41 to 51	90.9	*	*	0	22	0	
9/11/2024	07:00	0	0	0	1	9	15	34	18	8	1	0	0	0	0	0	0	0	0	86	42.3	46.5	37 to 47	72.1	*	*	0	78	8	
9/11/2024	08:00	0	0	1	0	3	21	43	18	5	1	0	0	0	0	0	0	0	0	92	42.3	46.2	37 to 47	78.3	*	*	0	82	10	
9/11/2024	09:00	0	0	0	0	4	24	32	14	8	1	0	0	0	0	0	0	0	0	83	42	47.5	35 to 45	73.5	*	*	0	76	7	
9/11/2024	10:00	0	0	0	0	2	9	27	18	8	2	0	0	0	0	0	0	0	0	66	43.9	48	38 to 48	75.8	*	*	0	63	3	
9/11/2024	11:00	0	0	0	1	2	16	27	23	10	1	0	0	0	0	0	0	0	0	80	43.4	48.8	37 to 47	73.8	*	*	0	74	6	
9/11/2024	12:00	0	0	0	1	5	17	29	30	9	3	0	0	0	0	0	0	0	0	94	43.4	48.3	38 to 48	73.4	*	*	0	92	2	
9/11/2024	13:00	0	0	0	0	6	14	36	24	9	0	0	0	0	0	0	0	0	0	89	42.8	46.8	37 to 47	76.4	*	*	2	81	6	
9/11/2024	14:00	0	0	0	1	5	36	62	36	11	4	0	0	0	0	0	0	0	0	155	42.6	46.7	36 to 46	75.5	*	*	0	143	12	
9/11/2024	15:00	0	0	0	1	4	20	57	45	12	4	1	0	0	0	0	0	0	0	144	43.7	47.5	37 to 47	74.3	*	*	1	138	5	
9/11/2024	16:00	0	0	0	0	1	16	72	52	20	3	0	0	0	0	0	0	0	0	164	44.6	48.7	41 to 51	79.3	*	*	0	159	5	
9/11/2024	17:00	0	0	0	0	2	24	63	58	13	3	0	0	0	0	0	0	0	0	163	43.9	48	38 to 48	80.4	*	*	0	158	5	
9/11/2024	18:00	0	0	0	1	1	13	51	39	16	4	0	0	0	0	0	0	0	0	125	44.7	49.1	40 to 50	77.6	*	*	0	123	2	
9/11/2024	19:00	0	0	0	0	3	10	40	21	7	2	0	0	0	0	0	0	0	0	83	43.6	48	39 to 49	79.5	*	*	1	77	5	
9/11/2024	20:00	0	0	0	0	4	18	34	21	8	1	0	0	0	0	0	0	0	0	86	42.8	47.7	38 to 48	74.4	*	*	0	82	4	
9/11/2024	21:00	0	0	1	0	2	9	7	7	3	0	1	0	0	0	0	0	0	0	30	42.1	49	36 to 46	63.3	*	*	0	27	3	
9/11/2024	22:00	0	0	0	0	2	1	5	3	0	0	0	0	0	0	0	0	0	0	11	41.5	45	36 to 46	72.7	*	*	0	11	0	
9/11/2024	23:00	0	0	1	0	0	2	2	3	1	1	0	0	0	0	0	0	0	0	10	42.7	48	38 to 48	60.0	*	*	0	9	1	
24 Hr Summary		0	0	3	6	55	270	643	440	152	31	2	0	0	0	0	0	0	0	1602	43.4	48	38 to 48	74.6	*	*	5	1511	86	

Outgoing Histogram
Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/12/2024	00:00	0	0	0	0	2	3	0	1	1	0	0	0	0	0	0	0	0	0	7	39.4	45	29 to 39	71.4	*	*	0	6	1
9/12/2024	01:00	0	0	0	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	3	37.7	43	30 to 40	66.7	*	*	0	2	1
9/12/2024	02:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	38	40	30 to 40	100.0	*	*	0	2	0
9/12/2024	03:00	0	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	4	38.5	39	34 to 44	100.0	*	*	0	4	0
9/12/2024	04:00	0	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	3	42	45	35 to 45	100.0	*	*	0	3	0
9/12/2024	05:00	0	0	0	1	2	3	0	1	2	0	0	0	0	0	0	0	0	0	9	39	52	27 to 37	66.7	*	*	1	6	2
9/12/2024	06:00	0	0	0	0	1	2	12	5	2	0	0	0	0	0	0	0	0	0	22	43.6	48.5	36 to 46	77.3	*	*	0	20	2
9/12/2024	07:00	0	0	0	1	9	24	34	16	3	3	0	0	0	0	0	0	0	0	90	41.2	44.9	35 to 45	74.4	*	*	1	79	10
9/12/2024	08:00	0	0	0	0	4	20	36	17	3	2	0	0	0	0	0	0	0	0	82	42.2	47	38 to 48	78.0	*	*	0	74	8
9/12/2024	09:00	0	0	0	0	4	11	30	14	5	1	0	0	0	0	0	0	0	0	65	42.8	46.5	37 to 47	76.9	*	*	0	55	10
9/12/2024	10:00	0	0	0	1	6	16	27	16	3	0	0	0	0	0	0	0	0	0	69	41.3	46.2	35 to 45	69.6	*	*	0	64	5
9/12/2024	11:00	0	0	0	0	2	15	44	22	3	0	0	0	0	0	0	0	0	0	86	42.3	46	38 to 48	83.7	*	*	1	79	6
9/12/2024	12:00	0	0	0	0	6	12	39	33	2	3	0	0	0	0	0	0	0	0	95	43.1	47.1	38 to 48	81.1	*	*	0	91	4
9/12/2024	13:00	0	0	0	0	2	19	54	20	6	3	1	0	0	0	0	0	0	0	105	43.1	48	36 to 46	78.1	*	*	1	97	7
9/12/2024	14:00	0	0	0	0	5	31	59	34	13	1	0	0	0	0	0	0	0	0	143	42.5	47.6	37 to 47	74.1	*	*	1	132	10
9/12/2024	15:00	0	0	0	3	6	22	67	34	17	2	0	0	0	0	0	0	0	0	151	43	47.7	40 to 50	74.8	*	*	1	142	8
9/12/2024	16:00	0	0	0	0	2	20	59	39	23	1	3	1	0	0	0	0	0	0	148	44.7	49.8	39 to 49	73.0	1	0.7	0	143	5
9/12/2024	17:00	0	0	0	0	0	27	66	51	13	5	0	0	0	0	0	0	0	0	162	44	47.6	38 to 48	79.6	*	*	0	160	2
9/12/2024	18:00	0	0	0	3	1	20	53	41	15	0	1	0	0	0	0	0	0	0	134	43.7	48.3	38 to 48	74.6	*	*	1	127	6
9/12/2024	19:00	0	0	0	0	1	16	34	25	4	2	0	0	0	0	0	0	0	0	82	43.5	47.6	38 to 48	81.7	*	*	0	80	2
9/12/2024	20:00	0	0	0	0	2	19	17	16	3	1	0	0	0	0	0	0	0	0	58	42	45.8	36 to 46	81.0	*	*	0	54	4
9/12/2024	21:00	0	0	0	0	2	7	9	6	1	1	0	0	0	0	0	0	0	0	26	41.8	45.5	37 to 47	80.8	*	*	0	26	0
9/12/2024	22:00	0	0	0	1	0	3	4	3	1	0	0	0	0	0	0	0	0	0	12	41.4	45.7	36 to 46	75.0	*	*	0	11	1
9/12/2024	23:00	0	0	0	0	2	1	0	0	0	1	0	0	0	0	0	0	0	0	4	39.5	37	27 to 37	75.0	*	*	0	3	1
24 Hr Summary		0	0	0	10	61	295	649	395	120	26	5	1	0	0	0	0	0	0	1562	42.9	48	38 to 48	74.8	1	0.1	7	1460	95

Outgoing Histogram
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/13/2024	00:00	0	0	0	0	0	1	1	2	1	0	0	0	0	0	0	0	0	0	5	45	46	36 to 46	80.0	*	*	0	5	0
9/13/2024	01:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	38	44	22 to 32	50.0	*	*	0	2	0
9/13/2024	02:00	0	0	0	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	3	39.7	44	34 to 44	66.7	*	*	0	2	1
9/13/2024	03:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	0	1
9/13/2024	04:00	0	0	0	0	0	4	0	0	2	0	0	0	0	0	0	0	0	0	6	42.3	50	29 to 39	66.7	*	*	0	6	0
9/13/2024	05:00	0	0	0	0	0	4	1	1	0	0	0	0	0	0	0	0	0	0	6	39.2	40	35 to 45	100.0	*	*	0	4	2
9/13/2024	06:00	0	0	0	0	1	4	4	12	2	2	0	0	0	0	0	0	0	0	25	45	49	39 to 49	68.0	*	*	0	24	1
9/13/2024	07:00	0	0	0	0	6	22	41	21	3	1	0	0	0	0	0	0	0	0	94	41.8	45.4	36 to 46	84.0	*	*	0	84	10
9/13/2024	08:00	0	0	0	1	4	7	28	16	5	0	0	0	0	0	0	0	0	0	61	42.6	46.8	40 to 50	75.4	*	*	0	60	1
9/13/2024	09:00	0	0	1	1	5	21	34	10	8	0	0	0	0	0	0	0	0	0	80	41.4	47.7	34 to 44	71.3	*	*	0	69	11
9/13/2024	10:00	0	0	0	0	3	17	25	16	4	0	0	0	0	0	0	0	0	0	65	42.1	48	36 to 46	73.8	*	*	1	60	4
9/13/2024	11:00	0	0	0	0	6	16	24	30	4	0	0	0	0	0	0	0	0	0	80	42.7	46.6	37 to 47	73.8	*	*	0	75	5
9/13/2024	12:00	0	0	0	0	3	14	40	30	5	2	0	0	0	0	0	0	0	0	94	43.5	47.5	38 to 48	81.9	*	*	1	89	4
9/13/2024	13:00	0	0	0	0	2	23	56	29	9	3	1	0	0	0	0	0	0	0	123	43.3	47	37 to 47	79.7	*	*	0	118	5
9/13/2024	14:00	0	0	0	0	5	23	78	41	15	2	0	0	0	0	0	0	0	0	164	43.4	47.6	38 to 48	78.7	*	*	1	155	8
9/13/2024	15:00	0	0	0	3	4	22	52	35	12	5	2	0	1	0	0	0	0	0	136	43.6	49	38 to 48	69.9	1	0.7	0	125	11
9/13/2024	16:00	0	0	0	0	2	22	72	46	11	2	1	0	0	0	0	0	0	0	156	43.5	47.2	37 to 47	80.8	*	*	0	152	4
9/13/2024	17:00	0	0	0	1	6	29	65	73	14	2	1	1	0	0	0	0	0	0	192	43.8	47.9	38 to 48	75.5	1	0.5	0	183	9
9/13/2024	18:00	0	0	0	0	0	16	63	47	14	2	0	0	0	0	0	0	0	0	142	44.2	47.9	39 to 49	82.4	*	*	0	137	5
9/13/2024	19:00	0	0	0	0	2	19	35	32	16	1	1	0	0	0	0	0	0	0	106	44.2	49.3	38 to 48	69.8	*	*	0	102	4
9/13/2024	20:00	0	0	0	0	4	24	30	23	7	0	0	0	0	0	0	0	0	0	88	42.4	48.1	36 to 46	72.7	*	*	0	84	4
9/13/2024	21:00	0	0	0	0	3	16	7	3	4	0	0	0	0	0	0	0	0	0	33	40.2	46	32 to 42	75.8	*	*	0	31	2
9/13/2024	22:00	0	0	0	0	2	4	4	3	1	1	0	0	0	0	0	0	0	0	15	41.8	49	36 to 46	66.7	*	*	0	15	0
9/13/2024	23:00	0	0	0	0	4	1	3	1	1	0	0	0	0	0	0	0	0	0	10	38.9	44	25 to 35	50.0	*	*	0	7	3
24 Hr Summary		0	0	1	6	64	310	666	471	138	23	6	1	1	0	0	0	0	0	1687	43.1	48	38 to 48	73.3	2	0.1	3	1589	95

Outgoing Histogram

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/14/2024	00:00	0	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	4	41	43	37 to 47	75.0	*	*	0	4	0
9/14/2024	01:00	0	0	0	0	0	6	1	2	1	1	0	0	0	0	0	0	0	0	11	41.8	45	35 to 45	81.8	*	*	0	10	1
9/14/2024	02:00	0	0	0	0	0	1	1	0	0	0	0	1	0	0	0	0	0	0	3	49.3	68	32 to 42	66.7	1	33.3	0	3	0
9/14/2024	03:00	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	46.5	53	30 to 40	50.0	*	*	0	2	0
9/14/2024	04:00	0	0	0	0	0	1	5	1	1	0	0	0	0	0	0	0	0	0	8	42.9	48	33 to 43	75.0	*	*	0	7	1
9/14/2024	05:00	0	0	0	0	1	2	2	0	0	0	0	0	0	0	0	0	0	0	5	37.8	41	31 to 41	80.0	*	*	0	3	2
9/14/2024	06:00	0	0	0	0	1	4	6	1	1	0	0	0	0	0	0	0	0	0	13	40.9	43	33 to 43	84.6	*	*	0	12	1
9/14/2024	07:00	0	0	0	0	1	7	6	6	1	0	0	0	0	0	0	0	0	0	21	41.9	46.5	37 to 47	76.2	*	*	0	20	1
9/14/2024	08:00	0	0	0	1	6	10	12	11	1	0	1	0	0	0	0	0	0	0	42	41.3	47	36 to 46	64.3	*	*	1	37	4
9/14/2024	09:00	0	0	1	0	4	13	35	9	3	0	0	0	0	0	0	0	0	0	65	41.3	44.4	35 to 45	81.5	*	*	1	59	5
9/14/2024	10:00	0	0	0	0	5	25	48	31	6	1	1	0	0	0	0	0	0	0	117	42.6	47.2	39 to 49	74.4	*	*	0	109	8
9/14/2024	11:00	0	0	0	2	2	21	36	24	5	0	0	0	0	1	0	0	0	0	91	42.7	47	37 to 47	73.6	1	1.1	0	88	3
9/14/2024	12:00	0	0	0	0	7	21	44	22	4	1	0	0	0	0	0	0	0	0	99	41.8	46.2	37 to 47	75.8	*	*	0	93	6
9/14/2024	13:00	0	0	1	1	2	20	44	28	4	3	0	0	0	0	0	0	0	0	103	42.8	47.3	38 to 48	76.7	*	*	0	94	9
9/14/2024	14:00	0	0	0	1	1	14	45	29	10	1	0	0	0	0	0	0	0	0	101	43.4	48	39 to 49	77.2	*	*	0	92	9
9/14/2024	15:00	0	0	0	3	5	21	38	26	4	2	0	0	0	0	0	0	0	0	99	42	46.3	37 to 47	78.8	*	*	0	96	3
9/14/2024	16:00	0	0	0	0	4	18	43	20	11	2	0	0	0	0	0	0	0	0	98	42.8	47.5	37 to 47	75.5	*	*	0	94	4
9/14/2024	17:00	0	0	0	0	1	20	42	27	8	4	0	0	0	0	0	0	0	0	102	43.3	47.3	37 to 47	80.4	*	*	1	93	8
9/14/2024	18:00	0	0	0	0	1	13	29	25	7	3	1	0	0	0	0	0	0	0	79	44.5	48.8	39 to 49	74.7	*	*	0	77	2
9/14/2024	19:00	0	0	0	2	1	11	27	14	5	1	0	0	0	0	0	0	0	0	61	42.7	46.5	37 to 47	80.3	*	*	1	56	4
9/14/2024	20:00	0	0	0	0	3	18	21	12	1	1	0	0	0	0	0	0	0	0	56	41.5	46	35 to 45	76.8	*	*	0	55	1
9/14/2024	21:00	0	0	0	0	4	7	13	7	2	1	0	0	0	0	0	0	0	0	34	41.7	45.7	36 to 46	76.5	*	*	0	33	1
9/14/2024	22:00	0	0	0	0	0	1	5	3	3	1	0	0	0	0	0	0	0	0	13	45.8	51	37 to 47	69.2	*	*	0	13	0
9/14/2024	23:00	0	0	0	0	1	2	4	2	1	0	0	0	0	0	0	0	0	0	10	41.5	46.5	37 to 47	70.0	*	*	0	10	0
24 Hr Summary		0	0	2	10	51	256	510	301	80	22	3	1	0	1	0	0	0	0	1237	42.6	48	37 to 47	74.0	2	0.2	4	1160	73

Outgoing Histogram

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/15/2024	00:00	0	0	0	0	1	1	0	0	0	0	2	0	0	0	0	0	0	0	0	4	48.8	61	28 to 38	50.0	*	*	0	3	1
9/15/2024	01:00	0	0	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	3	39.7	45	32 to 42	66.7	*	*	0	3	0
9/15/2024	02:00	0	0	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	4	46.5	49	40 to 50	75.0	*	*	0	4	0
9/15/2024	03:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	45	45	35 to 45	100.0	*	*	0	1	0
9/15/2024	04:00	0	0	0	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4	44.5	44	36 to 46	100.0	*	*	0	3	1
9/15/2024	05:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	42	42	32 to 42	100.0	*	*	0	1	0
9/15/2024	06:00	0	0	0	0	1	0	3	2	0	0	0	0	0	0	0	0	0	0	0	6	42.7	48.5	39 to 49	83.3	*	*	0	6	0
9/15/2024	07:00	0	0	0	0	2	3	5	3	2	0	0	0	0	0	0	0	0	0	0	15	42.5	47	37 to 47	66.7	*	*	0	14	1
9/15/2024	08:00	0	0	0	0	0	7	11	2	2	0	0	0	0	0	0	0	0	0	0	22	41.2	45	35 to 45	86.4	*	*	0	22	0
9/15/2024	09:00	0	0	0	0	1	5	18	13	2	0	0	0	0	0	0	0	0	0	0	39	43	46	36 to 46	82.1	*	*	0	37	2
9/15/2024	10:00	0	0	0	0	4	14	38	19	2	1	1	0	0	0	0	0	0	0	0	79	42.3	46	36 to 46	79.7	*	*	0	78	1
9/15/2024	11:00	0	1	1	0	3	19	32	18	5	0	0	0	0	0	0	0	0	0	0	79	41.6	46.3	37 to 47	74.7	*	*	0	76	3
9/15/2024	12:00	0	0	0	0	2	20	67	34	7	1	0	0	0	0	0	0	0	0	0	131	43.1	46.7	38 to 48	81.7	*	*	0	129	2
9/15/2024	13:00	0	0	0	0	2	12	42	38	8	4	0	0	0	0	0	0	0	0	0	106	44.2	47.3	38 to 48	82.1	*	*	0	99	7
9/15/2024	14:00	0	0	0	0	6	12	40	31	11	2	0	0	0	0	0	0	0	0	0	102	43.6	48	38 to 48	73.5	*	*	1	97	4
9/15/2024	15:00	0	0	0	1	2	13	33	19	9	3	0	0	1	0	0	0	0	0	0	81	44.1	49.5	38 to 48	72.8	1	1.2	0	80	1
9/15/2024	16:00	0	0	0	1	4	13	37	32	7	1	1	0	0	0	0	0	0	0	0	96	43.5	47.3	38 to 48	78.1	*	*	1	93	2
9/15/2024	17:00	0	0	0	1	1	15	30	27	8	2	1	0	0	0	0	0	0	0	0	85	43.9	48.7	39 to 49	70.6	*	*	0	81	4
9/15/2024	18:00	0	0	0	0	1	9	31	28	8	2	1	0	0	0	0	0	0	0	0	80	44.6	48.8	39 to 49	78.8	*	*	0	79	1
9/15/2024	19:00	0	0	0	0	1	6	32	24	8	1	1	0	0	1	0	0	0	0	0	74	44.7	49	39 to 49	78.4	1	1.4	1	73	0
9/15/2024	20:00	0	0	0	0	1	15	29	21	5	2	1	0	0	0	0	0	0	0	0	74	43.4	47	37 to 47	81.1	*	*	0	74	0
9/15/2024	21:00	0	0	0	0	0	3	7	5	1	2	0	0	0	0	0	0	0	0	0	18	44.5	49	37 to 47	77.8	*	*	0	17	1
9/15/2024	22:00	0	0	0	0	1	2	7	3	3	0	0	0	0	0	0	0	0	0	0	16	42.9	51	35 to 45	75.0	*	*	0	14	2
9/15/2024	23:00	0	0	0	1	2	0	2	1	1	1	0	0	0	0	0	0	0	0	0	8	41.4	51	24 to 34	37.5	*	*	0	8	0
24 Hr Summary		0	1	1	4	36	170	469	325	90	22	8	0	1	1	0	0	0	0	0	1128	43.5	48	38 to 48	75.4	2	0.2	3	1092	33

Outgoing Histogram
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

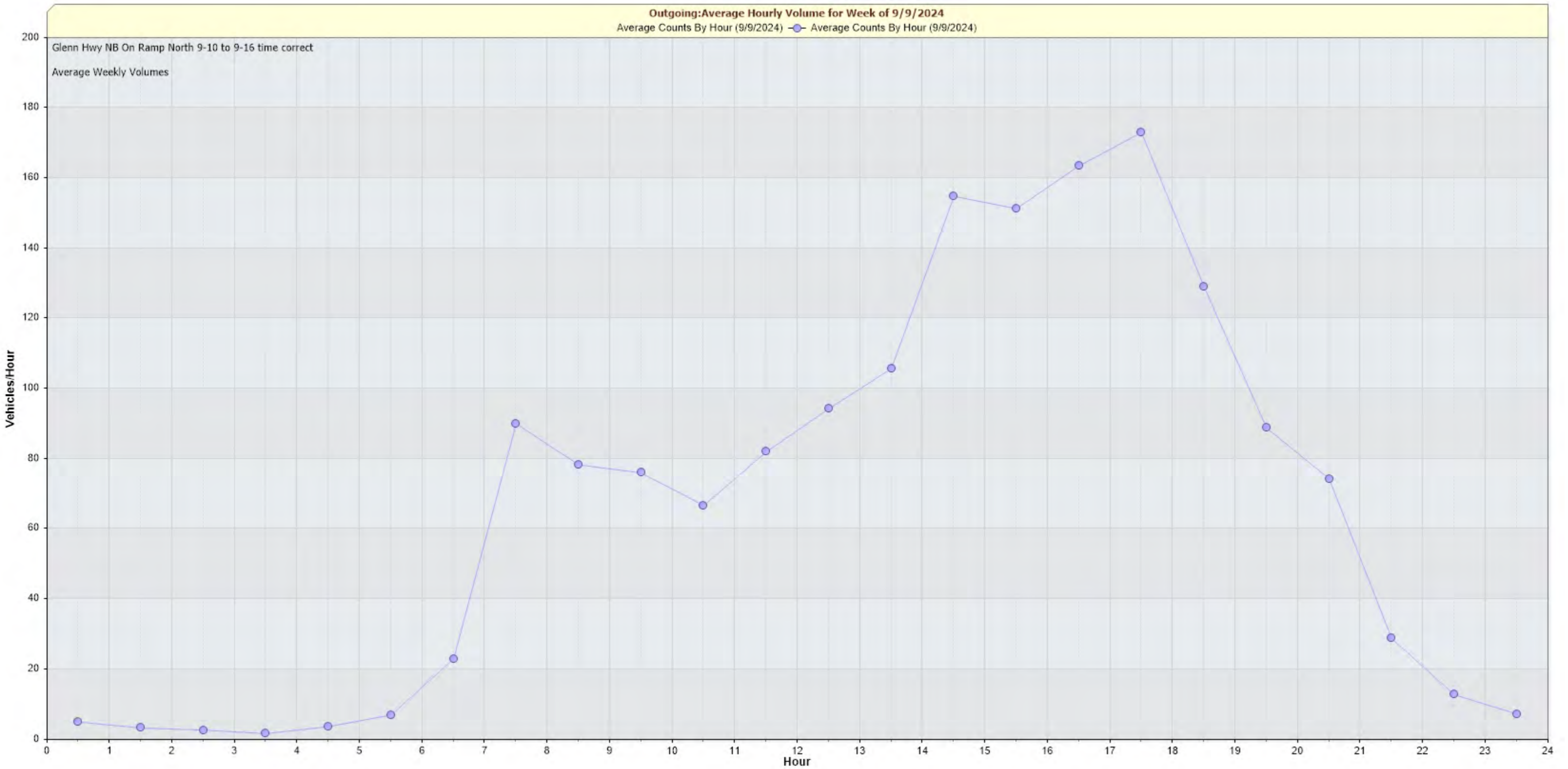
Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	0	0	0	0	1	0	3	0	0	0	0	0	0	0	0	0	0	0	4	38.8	41.5	32 to 42	75.0	*	*	1	2	1	
9/16/2024	01:00	0	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	3	38.3	48	25 to 35	66.7	*	*	0	2	1	
9/16/2024	02:00	0	0	0	0	0	1	1	0	0	0	1	0	1	0	0	0	0	0	4	53.3	64	30 to 40	50.0	1	25.0	0	4	0	
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	0	0	0	0	0	2	1	2	0	0	0	0	0	0	0	0	0	0	5	41.6	45	35 to 45	80.0	*	*	0	4	1	
9/16/2024	05:00	0	0	0	0	1	2	0	2	0	0	0	0	0	0	0	0	0	0	5	40.4	45	28 to 38	60.0	*	*	0	4	1	
9/16/2024	06:00	0	0	0	0	1	4	7	11	2	0	0	0	0	0	0	0	0	0	25	43.4	46.5	37 to 47	84.0	*	*	0	22	3	
9/16/2024	07:00	0	0	0	0	1	3	4	2	0	0	0	0	0	0	0	0	0	0	10	40.6	44	34 to 44	70.0	*	*	0	8	2	
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	0	0	5	13	16	18	2	0	1	0	1	0	0	0	0	0	56	42.6	48	38 to 48	67.9	1	1.8	1	46	9	

Outgoing: Average Hourly Volume for Week of 9/9/2024

Average Counts By Hour (9/9/2024) — Average Counts By Hour (9/9/2024)

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/9/2024
Average Hourly WEEKDAY Speeds By Hour (9/9/2024) — Average Hourly WEEKDAY Speeds By Hour (9/9/2024)

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct
Average Weekly Speeds

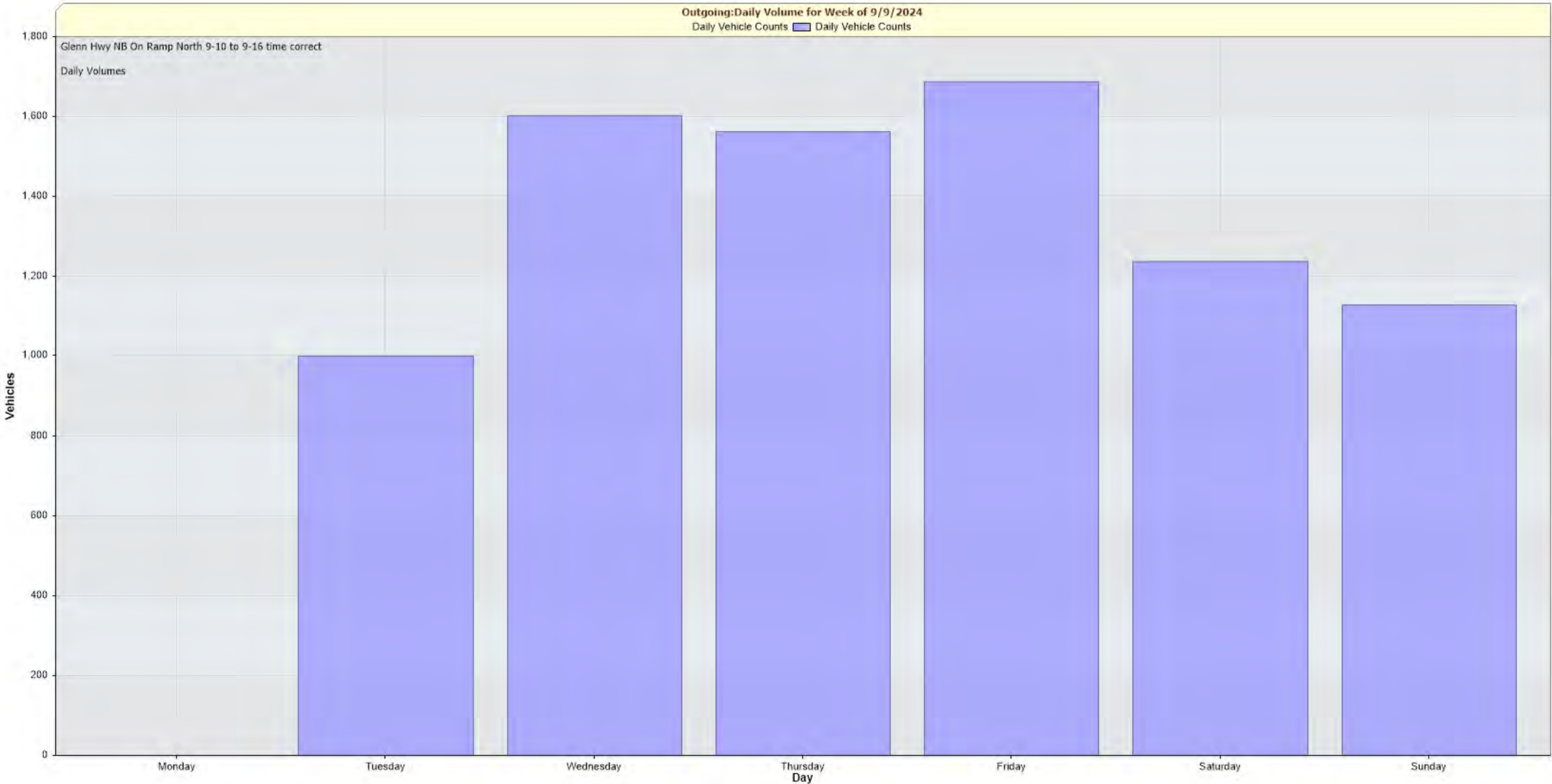


Outgoing: Daily Volume for Week of 9/9/2024

Daily Vehicle Counts

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

Daily Volumes



For Project: Old Glenn Hwy South South 9-16 to-9-20 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:56:18 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/16/2024 1:00:00 PM through 9/20/2024 10:59:59 AM

85th Percentile Speed 45 MPH

85th Percentile Vehicles 10649

Max Speed 71 MPH on 9/17/2024 6:00:00 PM

Total Vehicles 12528

AADT: 3198

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	2505	2505
AM Peak 8:00 AM	212	212
PM Peak 6:00 PM	314	314

Speed

Speed Limit: 45

85th Percentile Speed: 45

50th Percentile Speed: 37

10 MPH Pace Interval: 37.0 MPH to 47.0 MPH

Average Speed: 36.36

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	258	452	508	538	62	N/A	N/A
% over limit	13.5	13.9	15.6	15.5	9.5	N/A	N/A
Avg Speeder	47.9	48.2	48.1	48.3	48.9	N/A	N/A
Avg Speed	36.2	36.3	36.5	36.7	34.0	N/A	N/A

Class Counts

	Number	%
VEH_SM	54	0.4
VEH_MED	11770	93.9
VEH_LG	704	5.6
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/16/2024 2:00:00 PM	45.0	98	115	52	47.6	12.2%	35.6
9/16/2024 3:00:00 PM	43.0	198	233	53	47.9	6.0%	33.3
9/16/2024 4:00:00 PM	43.0	222	261	53	48.5	7.7%	32.2
9/16/2024 5:00:00 PM	45.0	231	272	52	47.9	14.0%	37.3
9/16/2024 6:00:00 PM	46.0	233	274	57	48.6	15.3%	37.4
9/16/2024 7:00:00 PM	46.0	252	296	54	47.4	19.6%	38.3
9/16/2024 8:00:00 PM	47.0	167	196	54	47.9	19.4%	38.4
9/16/2024 9:00:00 PM	45.0	99	116	51	47.9	14.7%	37.7
9/16/2024 10:00:00 PM	44.0	70	82	50	46.9	8.5%	35.5
9/16/2024 11:00:00 PM	46.0	41	48	48	46.9	16.7%	38.0
9/17/2024 12:00:00 AM	42.0	21	25	52	49.0	8.0%	33.9
9/17/2024 1:00:00 AM	42.0	14	17	47	46.5	11.8%	36.5
9/17/2024 2:00:00 AM	47.0	8	9	52	49.5	22.2%	39.3
9/17/2024 3:00:00 AM	46.0	6	7	53	49.5	28.6%	38.4
9/17/2024 4:00:00 AM	48.0	7	8	58	53.0	25.0%	41.9
9/17/2024 5:00:00 AM	32.0	2	2	32	0.0	0.0%	30.0
9/17/2024 6:00:00 AM	46.0	14	16	49	46.8	31.3%	41.6
9/17/2024 7:00:00 AM	46.0	62	73	54	49.2	17.8%	38.9
9/17/2024 8:00:00 AM	43.0	104	122	52	48.1	9.0%	31.4
9/17/2024 9:00:00 AM	41.0	184	216	53	48.3	6.0%	29.8
9/17/2024 10:00:00 AM	44.0	156	183	52	47.3	8.7%	34.4
9/17/2024 11:00:00 AM	45.0	163	192	56	48.9	12.5%	36.3
9/17/2024 12:00:00 PM	45.0	160	188	63	50.2	12.2%	36.9
9/17/2024 1:00:00 PM	46.0	161	189	55	48.1	15.9%	37.0
9/17/2024 2:00:00 PM	46.0	201	236	53	48.0	16.1%	37.8
9/17/2024 3:00:00 PM	44.0	159	187	51	47.3	10.7%	33.4
9/17/2024 4:00:00 PM	43.0	204	240	55	48.2	6.7%	32.6
9/17/2024 5:00:00 PM	45.0	230	271	57	48.1	14.4%	37.8
9/17/2024 6:00:00 PM	46.0	258	303	71	48.0	18.4%	38.2
9/17/2024 7:00:00 PM	46.0	249	293	71	48.5	18.8%	38.8
9/17/2024 8:00:00 PM	46.0	180	212	51	47.6	18.9%	39.1
9/17/2024 9:00:00 PM	46.0	110	130	52	47.8	16.2%	38.6
9/17/2024 10:00:00 PM	45.0	61	72	52	48.3	15.3%	38.0
9/17/2024 11:00:00 PM	45.0	48	56	53	49.4	12.5%	38.3
9/18/2024 12:00:00 AM	48.0	21	25	50	48.1	28.0%	38.0
9/18/2024 1:00:00 AM	47.0	6	7	50	47.7	42.9%	41.0
9/18/2024 2:00:00 AM	37.0	6	7	51	51.0	14.3%	33.3
9/18/2024 3:00:00 AM	45.0	6	7	46	46.0	14.3%	40.7
9/18/2024 4:00:00 AM	60.0	1	1	60	60.0	100.0%	60.0
9/18/2024 5:00:00 AM	40.0	7	8	43	0.0	0.0%	34.8
9/18/2024 6:00:00 AM	46.0	20	23	53	48.5	17.4%	37.9
9/18/2024 7:00:00 AM	46.0	63	74	52	48.2	18.9%	38.1
9/18/2024 8:00:00 AM	43.0	99	117	51	47.3	6.0%	33.1
9/18/2024 9:00:00 AM	41.0	176	207	52	47.2	6.3%	30.4
9/18/2024 10:00:00 AM	46.0	146	172	59	48.3	16.3%	35.5
9/18/2024 11:00:00 AM	46.0	135	159	56	48.4	17.0%	37.1
9/18/2024 12:00:00 PM	46.0	125	147	55	48.4	19.0%	38.2

Incoming Summary
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

9/18/2024 1:00:00 PM	46.0	173	204	53	47.9	17.6%	37.8
9/18/2024 2:00:00 PM	46.0	173	203	51	47.6	18.2%	38.3
9/18/2024 3:00:00 PM	43.0	186	219	51	48.3	6.4%	32.7
9/18/2024 4:00:00 PM	44.0	205	241	55	48.9	9.5%	32.7
9/18/2024 5:00:00 PM	46.0	238	280	59	48.4	19.6%	38.8
9/18/2024 6:00:00 PM	46.0	250	294	62	48.0	18.0%	38.9
9/18/2024 7:00:00 PM	46.0	264	310	54	47.9	20.6%	38.4
9/18/2024 8:00:00 PM	47.0	172	202	65	48.5	19.3%	38.9
9/18/2024 9:00:00 PM	45.0	105	124	53	48.0	15.3%	36.7
9/18/2024 10:00:00 PM	46.0	112	132	55	47.9	15.9%	36.4
9/18/2024 11:00:00 PM	45.0	56	66	51	47.2	15.2%	36.6
9/19/2024 12:00:00 AM	47.0	37	43	57	48.8	23.3%	38.7
9/19/2024 1:00:00 AM	48.0	19	22	49	47.9	31.8%	37.5
9/19/2024 2:00:00 AM	46.0	8	9	48	46.7	33.3%	35.1
9/19/2024 3:00:00 AM	49.0	7	8	55	52.0	25.0%	37.0
9/19/2024 4:00:00 AM	41.0	11	13	50	50.0	7.7%	33.5
9/19/2024 5:00:00 AM	45.0	7	8	45	0.0	0.0%	37.8
9/19/2024 6:00:00 AM	45.0	18	21	57	50.0	14.3%	39.9
9/19/2024 7:00:00 AM	46.0	57	67	54	48.3	22.1%	39.1
9/19/2024 8:00:00 AM	42.0	92	108	52	48.4	4.6%	31.6
9/19/2024 9:00:00 AM	43.0	184	217	50	47.2	9.2%	30.4
9/19/2024 10:00:00 AM	44.0	162	191	53	47.9	13.1%	35.4
9/19/2024 11:00:00 AM	46.0	140	165	56	48.6	15.8%	36.3
9/19/2024 12:00:00 PM	46.0	162	190	54	47.9	15.3%	37.9
9/19/2024 1:00:00 PM	47.0	201	237	56	48.2	24.1%	38.9
9/19/2024 2:00:00 PM	46.0	185	218	55	47.9	15.6%	37.8
9/19/2024 3:00:00 PM	44.0	180	212	56	48.5	9.9%	33.6
9/19/2024 4:00:00 PM	43.0	239	281	53	48.1	6.0%	32.7
9/19/2024 5:00:00 PM	45.0	228	268	54	48.5	13.4%	38.4
9/19/2024 6:00:00 PM	46.0	271	319	56	47.9	20.7%	38.2
9/19/2024 7:00:00 PM	46.0	305	359	55	47.9	20.1%	39.3
9/19/2024 8:00:00 PM	47.0	182	214	61	49.0	20.6%	38.6
9/19/2024 9:00:00 PM	46.0	119	140	63	49.1	16.4%	39.2
9/19/2024 10:00:00 PM	48.0	86	101	60	49.7	18.8%	39.7
9/19/2024 11:00:00 PM	45.0	55	65	63	49.4	13.8%	36.3
9/20/2024 12:00:00 AM	46.0	25	29	52	48.8	17.2%	38.4
9/20/2024 1:00:00 AM	47.0	11	13	48	47.3	23.1%	35.9
9/20/2024 2:00:00 AM	41.0	4	5	44	0.0	0.0%	39.4
9/20/2024 3:00:00 AM	41.0	2	2	41	0.0	0.0%	37.5
9/20/2024 4:00:00 AM	31.0	5	6	53	53.0	16.7%	31.5
9/20/2024 5:00:00 AM	47.0	5	6	47	47.0	33.3%	42.5
9/20/2024 6:00:00 AM	44.0	14	17	47	47.0	5.9%	38.6
9/20/2024 7:00:00 AM	45.0	56	66	57	49.6	15.2%	38.4
9/20/2024 8:00:00 AM	44.0	82	96	53	49.3	12.5%	32.7
9/20/2024 9:00:00 AM	42.0	178	210	52	48.5	6.2%	31.0
9/20/2024 10:00:00 AM	44.0	127	149	50	48.1	7.4%	35.1
9/20/2024 11:00:00 AM	45.0	71	84	58	50.0	10.7%	35.4

Incoming Summary
Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/17/2024 12:00:00 AM	45.0	1630	1918	57	47.9	13.5%	36.2
9/18/2024 12:00:00 AM	45.0	2760	3247	71	48.2	13.9%	36.3
9/19/2024 12:00:00 AM	46.0	2760	3247	65	48.1	15.6%	36.5
9/20/2024 12:00:00 AM	46.0	2943	3462	63	48.3	15.5%	36.7
9/20/2024 10:59:59 AM	44.0	556	654	58	48.9	9.5%	34.0

Incoming Weekly Counts

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	17	7	22	13	*	*	14.75	0	45.75
1 - 2	*	9	7	9	5	*	*	7.5	0	42.75
2 - 3	*	7	7	8	2	*	*	6	0	45.25
3 - 4	*	8	1	13	6	*	*	7	0	45
4 - 5	*	2	8	8	6	*	*	6	0	40.75
5 - 6	*	16	23	21	17	*	*	19.25	0	45.12
6 - 7	*	73	74	67	66	*	*	70	0	45.62
7 - 8	*	122	117	108	96	*	*	110.75	0	42.8
8 - 9	*	216	207	217	210	*	*	212.5	0	41.47
9 - 10	*	183	172	191	149	*	*	173.75	0	44.18
10 - 11	*	192	159	165	84	*	*	150	0	44.82
11 - 12	*	188	147	190	*	*	*	175	0	45.27
12 - 13	*	189	204	237	*	*	*	210	0	45.7
13 - 14	115	236	203	218	*	*	*	193	0	45.1
14 - 15	233	187	219	212	*	*	*	212.75	0	43.1
15 - 16	261	240	241	281	*	*	*	255.75	0	42.9
16 - 17	272	271	280	268	*	*	*	272.75	0	45.08
17 - 18	274	303	294	319	*	*	*	297.5	0	45.45
18 - 19	296	293	310	359	*	*	*	314.5	0	45.85
19 - 20	196	212	202	214	*	*	*	206	0	45.97
20 - 21	116	130	124	140	*	*	*	127.5	0	45.1
21 - 22	82	72	132	101	*	*	*	96.75	0	45.25
22 - 23	48	56	66	65	*	*	*	58.75	0	44.88
23 - 24	25	25	43	29	*	*	*	30.5	0	45.62
Totals	1918	3247	3247	3462	654	0	0			
% of Total	15.31%	25.92%	25.92%	27.63%	5.22%	0%	0%			

Incoming Monthly Counts

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	17	7	22	13	*	*	14.75	0	45.75
1 - 2	*	9	7	9	5	*	*	7.5	0	42.75
2 - 3	*	7	7	8	2	*	*	6	0	45.25
3 - 4	*	8	1	13	6	*	*	7	0	45
4 - 5	*	2	8	8	6	*	*	6	0	40.75
5 - 6	*	16	23	21	17	*	*	19.25	0	45.12
6 - 7	*	73	74	67	66	*	*	70	0	45.62
7 - 8	*	122	117	108	96	*	*	110.75	0	42.8
8 - 9	*	216	207	217	210	*	*	212.5	0	41.47
9 - 10	*	183	172	191	149	*	*	173.75	0	44.18
10 - 11	*	192	159	165	84	*	*	150	0	44.82
11 - 12	*	188	147	190	*	*	*	175	0	45.27
12 - 13	*	189	204	237	*	*	*	210	0	45.7
13 - 14	115	236	203	218	*	*	*	193	0	45.1
14 - 15	233	187	219	212	*	*	*	212.75	0	43.1
15 - 16	261	240	241	281	*	*	*	255.75	0	42.9
16 - 17	272	271	280	268	*	*	*	272.75	0	45.08
17 - 18	274	303	294	319	*	*	*	297.5	0	45.45
18 - 19	296	293	310	359	*	*	*	314.5	0	45.85
19 - 20	196	212	202	214	*	*	*	206	0	45.97
20 - 21	116	130	124	140	*	*	*	127.5	0	45.1
21 - 22	82	72	132	101	*	*	*	96.75	0	45.25
22 - 23	48	56	66	65	*	*	*	58.75	0	44.88
23 - 24	25	25	43	29	*	*	*	30.5	0	45.62
Totals	1918	3247	3247	3462	654	0	0			
% of Total	15.31%	25.92%	25.92%	27.63%	5.22%	0%	0%			

Incoming Weekly Speeds

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	36.53	41	37.5	35.92	*	*	37.29	*	45.75
1 - 2	*	39.33	33.29	35.11	39.4	*	*	36.67	*	42.75
2 - 3	*	38.43	40.71	37	37.5	*	*	38.54	*	45.25
3 - 4	*	41.88	60	33.54	31.5	*	*	36.43	*	45
4 - 5	*	30	34.75	37.75	42.5	*	*	37.29	*	40.75
5 - 6	*	41.56	37.87	39.9	38.65	*	*	39.36	*	45.12
6 - 7	*	38.95	38.14	39.1	38.39	*	*	38.64	*	45.62
7 - 8	*	31.44	33.06	31.59	32.73	*	*	32.19	*	42.8
8 - 9	*	29.8	30.43	30.38	31	*	*	30.4	*	41.47
9 - 10	*	34.4	35.53	35.41	35.13	*	*	35.11	*	44.18
10 - 11	*	36.26	37.11	36.31	35.39	*	*	36.38	*	44.82
11 - 12	*	36.93	38.16	37.89	*	*	*	37.62	*	45.27
12 - 13	*	36.96	37.75	38.93	*	*	*	37.96	*	45.7
13 - 14	35.89	37.8	38.26	37.77	*	*	*	37.63	*	45.1
14 - 15	33.26	33.45	32.69	33.62	*	*	*	33.25	*	43.1
15 - 16	32.25	32.64	32.72	32.71	*	*	*	32.58	*	42.9
16 - 17	37.29	37.78	38.84	38.45	*	*	*	38.09	*	45.08
17 - 18	37.41	38.23	38.94	38.18	*	*	*	38.2	*	45.45
18 - 19	38.26	38.77	38.35	39.29	*	*	*	38.7	*	45.85
19 - 20	38.38	39.12	38.86	38.56	*	*	*	38.74	*	45.97
20 - 21	37.71	38.58	36.67	39.16	*	*	*	38.08	*	45.1
21 - 22	35.49	38.03	36.4	39.72	*	*	*	37.38	*	45.25
22 - 23	38.02	38.27	36.58	36.34	*	*	*	37.21	*	44.88
23 - 24	33.92	38	38.74	38.41	*	*	*	37.52	*	45.62
Totals	36.2	36.3	36.5	36.7	34	0	0			
% of Total	20.14%	20.2%	20.31%	20.42%	18.92%	0%	0%			

Incoming Monthly Speeds
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	36.53	41	37.5	35.92	*	*	37.29	*	45.75
1 - 2	*	39.33	33.29	35.11	39.4	*	*	36.67	*	42.75
2 - 3	*	38.43	40.71	37	37.5	*	*	38.54	*	45.25
3 - 4	*	41.88	60	33.54	31.5	*	*	36.43	*	45
4 - 5	*	30	34.75	37.75	42.5	*	*	37.29	*	40.75
5 - 6	*	41.56	37.87	39.9	38.65	*	*	39.36	*	45.12
6 - 7	*	38.95	38.14	39.1	38.39	*	*	38.64	*	45.62
7 - 8	*	31.44	33.06	31.59	32.73	*	*	32.19	*	42.8
8 - 9	*	29.8	30.43	30.38	31	*	*	30.4	*	41.47
9 - 10	*	34.4	35.53	35.41	35.13	*	*	35.11	*	44.18
10 - 11	*	36.26	37.11	36.31	35.39	*	*	36.38	*	44.82
11 - 12	*	36.93	38.16	37.89	*	*	*	37.62	*	45.27
12 - 13	*	36.96	37.75	38.93	*	*	*	37.96	*	45.7
13 - 14	35.89	37.8	38.26	37.77	*	*	*	37.63	*	45.1
14 - 15	33.26	33.45	32.69	33.62	*	*	*	33.25	*	43.1
15 - 16	32.25	32.64	32.72	32.71	*	*	*	32.58	*	42.9
16 - 17	37.29	37.78	38.84	38.45	*	*	*	38.09	*	45.08
17 - 18	37.41	38.23	38.94	38.18	*	*	*	38.2	*	45.45
18 - 19	38.26	38.77	38.35	39.29	*	*	*	38.7	*	45.85
19 - 20	38.38	39.12	38.86	38.56	*	*	*	38.74	*	45.97
20 - 21	37.71	38.58	36.67	39.16	*	*	*	38.08	*	45.1
21 - 22	35.49	38.03	36.4	39.72	*	*	*	37.38	*	45.25
22 - 23	38.02	38.27	36.58	36.34	*	*	*	37.21	*	44.88
23 - 24	33.92	38	38.74	38.41	*	*	*	37.52	*	45.62
Totals	36.2	36.3	36.5	36.7	34	0	0			
% of Total	20.14%	20.2%	20.31%	20.42%	18.92%	0%	0%			

Incoming Weekly EightyFifthSpeeds
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	42	47	47.5	46.5	*	*	45.75	0	45.75
1 - 2	*	47	37	46	41	*	*	42.75	0	42.75
2 - 3	*	46	45	49	41	*	*	45.25	0	45.25
3 - 4	*	48	60	41	31	*	*	45	0	45
4 - 5	*	32	40	44.5	46.5	*	*	40.75	0	40.75
5 - 6	*	46	46	45	43.5	*	*	45.12	0	45.12
6 - 7	*	45.5	46	46	45	*	*	45.62	0	45.62
7 - 8	*	43	43	41.2	44	*	*	42.8	0	42.8
8 - 9	*	40.8	40.8	42.8	41.5	*	*	41.47	0	41.47
9 - 10	*	43.7	45.2	44	43.8	*	*	44.18	0	44.18
10 - 11	*	44.4	45.4	45.2	44.3	*	*	44.82	0	44.82
11 - 12	*	44.7	46	45.1	*	*	*	45.27	0	45.27
12 - 13	*	45.2	45.5	46.4	*	*	*	45.7	0	45.7
13 - 14	44.4	45.3	45.6	45.1	*	*	*	45.1	0	45.1
14 - 15	43	43.4	42.5	43.5	*	*	*	43.1	0	43.1
15 - 16	42.7	42.6	43.8	42.5	*	*	*	42.9	0	42.9
16 - 17	44.8	44.9	45.8	44.8	*	*	*	45.08	0	45.08
17 - 18	45.1	45.4	45.5	45.8	*	*	*	45.45	0	45.45
18 - 19	45.6	45.9	45.9	46	*	*	*	45.85	0	45.85
19 - 20	46.1	45.5	46.1	46.2	*	*	*	45.97	0	45.97
20 - 21	45	45.2	45	45.2	*	*	*	45.1	0	45.1
21 - 22	43.6	45	45.2	47.2	*	*	*	45.25	0	45.25
22 - 23	45.3	44.7	45	44.5	*	*	*	44.88	0	44.88
23 - 24	42	48	46.5	46	*	*	*	45.62	0	45.62
Totals	487.6	1064.2	1083.8	1080.5	468.1	0	0			
% of Total	11.65%	25.43%	25.9%	25.82%	11.19%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	42	47	47.5	46.5	*	*	45.75	0	45.75
1 - 2	*	47	37	46	41	*	*	42.75	0	42.75
2 - 3	*	46	45	49	41	*	*	45.25	0	45.25
3 - 4	*	48	60	41	31	*	*	45	0	45
4 - 5	*	32	40	44.5	46.5	*	*	40.75	0	40.75
5 - 6	*	46	46	45	43.5	*	*	45.12	0	45.12
6 - 7	*	45.5	46	46	45	*	*	45.62	0	45.62
7 - 8	*	43	43	41.2	44	*	*	42.8	0	42.8
8 - 9	*	40.8	40.8	42.8	41.5	*	*	41.47	0	41.47
9 - 10	*	43.7	45.2	44	43.8	*	*	44.18	0	44.18
10 - 11	*	44.4	45.4	45.2	44.3	*	*	44.82	0	44.82
11 - 12	*	44.7	46	45.1	*	*	*	45.27	0	45.27
12 - 13	*	45.2	45.5	46.4	*	*	*	45.7	0	45.7
13 - 14	44.4	45.3	45.6	45.1	*	*	*	45.1	0	45.1
14 - 15	43	43.4	42.5	43.5	*	*	*	43.1	0	43.1
15 - 16	42.7	42.6	43.8	42.5	*	*	*	42.9	0	42.9
16 - 17	44.8	44.9	45.8	44.8	*	*	*	45.08	0	45.08
17 - 18	45.1	45.4	45.5	45.8	*	*	*	45.45	0	45.45
18 - 19	45.6	45.9	45.9	46	*	*	*	45.85	0	45.85
19 - 20	46.1	45.5	46.1	46.2	*	*	*	45.97	0	45.97
20 - 21	45	45.2	45	45.2	*	*	*	45.1	0	45.1
21 - 22	43.6	45	45.2	47.2	*	*	*	45.25	0	45.25
22 - 23	45.3	44.7	45	44.5	*	*	*	44.88	0	44.88
23 - 24	42	48	46.5	46	*	*	*	45.62	0	45.62

Summary of Violators

Old Glenn Hwy South South 9-16 to-9-20 time correct from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	59	37.3	15	47.5
01:00:00	30	36.7	6	48.3
02:00:00	24	38.5	5	49.8
03:00:00	28	36.4	5	53.8
04:00:00	24	37.3	2	47.0
05:00:00	77	39.4	13	48.1
06:00:00	280	38.6	51	48.8
07:00:00	443	32.2	35	48.4
08:00:00	850	30.4	59	47.7
09:00:00	695	35.1	80	48.0
10:00:00	600	36.4	86	48.8
11:00:00	525	37.6	80	48.8
12:00:00	630	38.0	123	48.1
13:00:00	772	37.6	123	47.8
14:00:00	851	33.2	69	48.0
15:00:00	1023	32.6	76	48.5
16:00:00	1091	38.1	168	48.2
17:00:00	1190	38.2	216	48.0
18:00:00	1258	38.7	249	47.9
19:00:00	824	38.7	161	48.3
20:00:00	510	38.1	80	48.2
21:00:00	387	37.4	58	48.4
22:00:00	235	37.2	34	48.2
23:00:00	122	37.5	24	48.6

Incoming Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	13:00	0	0	3	20	33	21	19	16	3	0	0	0	0	0	0	0	0	0	115	35.9	44.4	26 to 36	54.8	14	12.2	1	107	7	
9/16/2024	14:00	2	4	29	43	67	22	43	22	1	0	0	0	0	0	0	0	0	0	233	33.3	43	24 to 34	49.8	14	6.0	3	207	23	
9/16/2024	15:00	2	10	65	38	39	27	55	20	5	0	0	0	0	0	0	0	0	0	261	32.2	42.7	20 to 30	42.9	20	7.7	2	239	20	
9/16/2024	16:00	0	0	10	42	49	46	74	44	7	0	0	0	0	0	0	0	0	0	272	37.3	44.8	35 to 45	48.9	38	14.0	2	253	17	
9/16/2024	17:00	0	0	7	41	59	45	71	39	11	1	0	0	0	0	0	0	0	0	274	37.4	45.1	36 to 46	46.7	42	15.3	1	257	16	
9/16/2024	18:00	1	2	11	28	55	48	79	66	6	0	0	0	0	0	0	0	0	0	296	38.3	45.6	37 to 47	55.1	58	19.6	1	283	12	
9/16/2024	19:00	1	0	4	25	34	30	54	43	5	0	0	0	0	0	0	0	0	0	196	38.4	46.1	39 to 49	52.6	38	19.4	0	186	10	
9/16/2024	20:00	0	1	1	22	13	22	34	19	4	0	0	0	0	0	0	0	0	0	116	37.7	45	37 to 47	57.8	17	14.7	0	111	5	
9/16/2024	21:00	2	3	3	12	13	13	26	9	1	0	0	0	0	0	0	0	0	0	82	35.5	43.6	36 to 46	54.9	7	8.5	0	81	1	
9/16/2024	22:00	0	1	4	4	1	17	11	10	0	0	0	0	0	0	0	0	0	0	48	38	45.3	36 to 46	66.7	8	16.7	0	48	0	
9/16/2024	23:00	0	0	3	6	5	4	4	2	1	0	0	0	0	0	0	0	0	0	25	33.9	42	21 to 31	48.0	2	8.0	0	24	1	
24 Hr Summary		8	21	140	281	368	295	470	290	44	1	0	0	0	0	0	0	0	0	1918	36.2	45	37 to 47	45.4	258	13.5	10	1796	112	

Incoming Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/17/2024	00:00	0	0	1	3	2	4	4	3	0	0	0	0	0	0	0	0	0	0	17	36.5	42	37 to 47	58.8	2	11.8	0	17	0
9/17/2024	01:00	0	0	0	2	1	0	4	1	1	0	0	0	0	0	0	0	0	0	9	39.3	47	33 to 43	55.6	2	22.2	0	9	0
9/17/2024	02:00	0	0	0	1	1	2	1	1	1	0	0	0	0	0	0	0	0	0	7	38.4	46	27 to 37	57.1	2	28.6	0	7	0
9/17/2024	03:00	0	0	0	1	0	1	4	1	0	1	0	0	0	0	0	0	0	0	8	41.9	48	34 to 44	62.5	2	25.0	0	8	0
9/17/2024	04:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30	32	22 to 32	100.0	*	*	0	2	0
9/17/2024	05:00	0	0	0	1	2	1	7	5	0	0	0	0	0	0	0	0	0	0	16	41.6	46	39 to 49	81.3	5	31.3	0	15	1
9/17/2024	06:00	1	1	2	5	9	16	20	13	6	0	0	0	0	0	0	0	0	0	73	38.9	45.5	36 to 46	58.9	13	17.8	0	71	2
9/17/2024	07:00	2	8	25	23	20	12	19	11	2	0	0	0	0	0	0	0	0	0	122	31.4	43	21 to 31	44.3	11	9.0	0	113	9
9/17/2024	08:00	3	15	54	50	30	21	26	15	2	0	0	0	0	0	0	0	0	0	216	29.8	40.8	20 to 30	52.8	13	6.0	0	197	19
9/17/2024	09:00	3	6	10	40	35	26	39	22	2	0	0	0	0	0	0	0	0	0	183	34.4	43.7	25 to 35	44.3	16	8.7	1	160	22
9/17/2024	10:00	2	3	11	28	34	38	43	25	6	2	0	0	0	0	0	0	0	0	192	36.3	44.4	35 to 45	46.9	24	12.5	1	166	25
9/17/2024	11:00	1	6	4	25	38	36	40	28	7	2	1	0	0	0	0	0	0	0	188	36.9	44.7	35 to 45	48.4	23	12.2	2	176	10
9/17/2024	12:00	3	2	6	28	38	28	45	31	7	1	0	0	0	0	0	0	0	0	189	37	45.2	37 to 47	48.7	30	15.9	1	179	9
9/17/2024	13:00	2	2	5	19	57	43	59	40	9	0	0	0	0	0	0	0	0	0	236	37.8	45.3	37 to 47	49.6	38	16.1	1	219	16
9/17/2024	14:00	1	5	25	43	34	20	36	21	2	0	0	0	0	0	0	0	0	0	187	33.4	43.4	23 to 33	46.5	20	10.7	2	170	15
9/17/2024	15:00	3	10	45	38	40	34	47	19	3	1	0	0	0	0	0	0	0	0	240	32.6	42.6	21 to 31	42.1	16	6.7	3	220	17
9/17/2024	16:00	2	3	5	28	56	46	76	47	7	1	0	0	0	0	0	0	0	0	271	37.8	44.9	37 to 47	51.7	39	14.4	0	250	21
9/17/2024	17:00	2	1	5	36	52	47	95	57	7	1	0	0	0	0	0	0	0	0	303	38.2	45.4	36 to 46	56.1	55	18.2	2	288	13
9/17/2024	18:00	1	1	4	38	47	45	82	62	12	0	0	0	1	0	0	0	0	0	293	38.8	45.9	38 to 48	52.6	55	18.8	0	285	8
9/17/2024	19:00	1	0	1	28	27	30	71	49	5	0	0	0	0	0	0	0	0	0	212	39.1	45.5	38 to 48	61.8	40	18.9	1	207	4
9/17/2024	20:00	0	0	0	21	18	20	45	23	3	0	0	0	0	0	0	0	0	0	130	38.6	45.2	38 to 48	62.3	21	16.2	0	126	4
9/17/2024	21:00	0	0	3	8	10	15	22	11	3	0	0	0	0	0	0	0	0	0	72	38	45	35 to 45	55.6	11	15.3	0	69	3
9/17/2024	22:00	0	0	2	6	10	8	20	6	4	0	0	0	0	0	0	0	0	0	56	38.3	44.7	37 to 47	58.9	7	12.5	0	53	3
9/17/2024	23:00	0	0	3	1	5	4	4	7	1	0	0	0	0	0	0	0	0	0	25	38	48	39 to 49	48.0	7	28.0	0	25	0
24 Hr Summary		27	63	211	474	567	497	809	498	90	9	1	0	1	0	0	0	0	0	3247	36.3	45	37 to 47	46.2	452	13.9	14	3032	201

Incoming Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/18/2024	00:00	0	0	0	0	2	1	1	2	1	0	0	0	0	0	0	0	0	0	7	41	47	40 to 50	57.1	3	42.9	0	7	0
9/18/2024	01:00	0	0	1	2	2	1	0	0	1	0	0	0	0	0	0	0	0	0	7	33.3	37	24 to 34	71.4	1	14.3	0	7	0
9/18/2024	02:00	0	0	0	0	1	1	3	2	0	0	0	0	0	0	0	0	0	0	7	40.7	45	36 to 46	85.7	1	14.3	0	7	0
9/18/2024	03:00	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	60	60	50 to 60	100.0	1	100.0	0	1	0
9/18/2024	04:00	0	0	0	2	1	3	2	0	0	0	0	0	0	0	0	0	0	0	8	34.8	40	27 to 37	75.0	*	*	0	7	1
9/18/2024	05:00	0	0	1	4	2	5	7	3	1	0	0	0	0	0	0	0	0	0	23	37.9	46	34 to 44	52.2	4	17.4	0	23	0
9/18/2024	06:00	1	2	2	6	11	14	23	12	3	0	0	0	0	0	0	0	0	0	74	38.1	46	34 to 44	52.7	14	18.9	0	70	4
9/18/2024	07:00	1	3	13	26	27	14	21	11	1	0	0	0	0	0	0	0	0	0	117	33.1	43	25 to 35	49.6	7	6.0	0	109	8
9/18/2024	08:00	1	13	58	36	24	35	24	15	1	0	0	0	0	0	0	0	0	0	207	30.4	40.8	19 to 29	49.3	13	6.3	0	196	11
9/18/2024	09:00	2	7	10	25	33	29	35	26	2	3	0	0	0	0	0	0	0	0	172	35.5	45.2	30 to 40	43.6	28	16.3	0	161	11
9/18/2024	10:00	0	0	10	18	35	31	31	28	4	2	0	0	0	0	0	0	0	0	159	37.1	45.4	30 to 40	47.2	27	17.0	0	143	16
9/18/2024	11:00	1	0	3	17	31	27	29	31	7	1	0	0	0	0	0	0	0	0	147	38.2	46	38 to 48	46.9	28	19.0	1	130	16
9/18/2024	12:00	1	1	7	24	40	39	47	39	6	0	0	0	0	0	0	0	0	0	204	37.8	45.5	36 to 46	47.5	36	17.6	0	192	12
9/18/2024	13:00	1	2	7	22	31	34	60	39	7	0	0	0	0	0	0	0	0	0	203	38.3	45.6	37 to 47	54.7	37	18.2	1	183	19
9/18/2024	14:00	3	9	41	23	48	41	32	18	4	0	0	0	0	0	0	0	0	0	219	32.7	42.5	28 to 38	42.5	14	6.4	3	200	16
9/18/2024	15:00	6	13	47	32	35	34	40	25	8	1	0	0	0	0	0	0	0	0	241	32.7	43.8	19 to 29	35.3	23	9.5	2	227	12
9/18/2024	16:00	1	2	7	33	36	52	74	62	9	4	0	0	0	0	0	0	0	0	280	38.8	45.8	37 to 47	53.6	55	19.6	1	259	20
9/18/2024	17:00	1	2	2	27	52	50	91	60	6	2	1	0	0	0	0	0	0	0	294	38.9	45.5	38 to 48	56.5	53	18.0	1	284	9
9/18/2024	18:00	0	1	2	41	62	62	63	68	11	0	0	0	0	0	0	0	0	0	310	38.4	45.9	38 to 48	47.7	64	20.6	0	296	14
9/18/2024	19:00	1	0	3	17	40	39	53	38	10	0	0	1	0	0	0	0	0	0	202	38.9	46.1	37 to 47	53.0	39	19.3	0	198	4
9/18/2024	20:00	1	0	3	20	29	28	21	18	4	0	0	0	0	0	0	0	0	0	124	36.7	45	29 to 39	51.6	19	15.3	0	121	3
9/18/2024	21:00	1	7	4	16	26	20	35	20	2	1	0	0	0	0	0	0	0	0	132	36.4	45.2	38 to 48	47.0	21	15.9	0	130	2
9/18/2024	22:00	1	2	5	8	9	8	18	14	1	0	0	0	0	0	0	0	0	0	66	36.6	45	36 to 46	54.5	10	15.2	0	62	4
9/18/2024	23:00	0	1	0	5	10	5	8	11	2	1	0	0	0	0	0	0	0	0	43	38.7	46.5	37 to 47	46.5	10	23.3	0	42	1
24 Hr Summary		23	65	226	404	587	573	718	542	91	15	2	1	0	0	0	0	0	0	3247	36.5	46	37 to 47	44.6	508	15.6	9	3055	183

Incoming Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/19/2024	00:00	0	0	1	4	5	2	3	7	0	0	0	0	0	0	0	0	0	0	22	37.5	47.5	39 to 49	50.0	7	31.8	0	22	0
9/19/2024	01:00	1	0	2	0	1	0	1	4	0	0	0	0	0	0	0	0	0	0	9	35.1	46	38 to 48	55.6	3	33.3	0	9	0
9/19/2024	02:00	0	0	1	1	2	1	0	2	0	1	0	0	0	0	0	0	0	0	8	37	49	21 to 31	50.0	2	25.0	0	7	1
9/19/2024	03:00	0	0	2	2	3	3	2	0	1	0	0	0	0	0	0	0	0	0	13	33.5	41	21 to 31	53.8	1	7.7	0	12	1
9/19/2024	04:00	0	0	0	1	0	4	1	2	0	0	0	0	0	0	0	0	0	0	8	37.8	44.5	35 to 45	87.5	*	*	0	8	0
9/19/2024	05:00	0	0	1	1	3	3	8	4	0	1	0	0	0	0	0	0	0	0	21	39.9	45	36 to 46	66.7	3	14.3	0	21	0
9/19/2024	06:00	0	0	1	4	14	16	15	13	4	0	0	0	0	0	0	0	0	0	67	39.1	46	32 to 42	55.2	14	20.9	1	63	3
9/19/2024	07:00	1	2	22	23	23	17	14	4	2	0	0	0	0	0	0	0	0	0	108	31.6	41.2	21 to 31	52.8	5	4.6	1	100	7
9/19/2024	08:00	5	10	67	35	30	15	27	26	2	0	0	0	0	0	0	0	0	0	217	30.4	42.8	20 to 30	48.8	20	9.2	1	199	17
9/19/2024	09:00	1	4	13	29	43	29	43	25	4	0	0	0	0	0	0	0	0	0	191	35.4	44	27 to 37	42.9	25	13.1	1	181	9
9/19/2024	10:00	5	2	6	23	36	23	37	26	6	1	0	0	0	0	0	0	0	0	165	36.3	45.2	37 to 47	44.2	26	15.8	4	147	14
9/19/2024	11:00	0	2	4	18	38	41	53	27	7	0	0	0	0	0	0	0	0	0	190	37.9	45.1	36 to 46	54.7	29	15.3	1	180	9
9/19/2024	12:00	1	2	7	22	39	40	56	57	12	1	0	0	0	0	0	0	0	0	237	38.9	46.4	37 to 47	52.3	57	24.1	1	217	19
9/19/2024	13:00	1	1	3	36	38	28	67	37	6	1	0	0	0	0	0	0	0	0	218	37.8	45.1	37 to 47	52.3	34	15.6	0	206	12
9/19/2024	14:00	1	5	34	39	40	32	35	20	3	3	0	0	0	0	0	0	0	0	212	33.6	43.5	22 to 32	44.8	21	9.9	3	195	14
9/19/2024	15:00	5	21	45	36	37	56	58	20	3	0	0	0	0	0	0	0	0	0	281	32.7	42.5	33 to 43	42.7	17	6.0	2	264	15
9/19/2024	16:00	1	1	5	25	44	63	73	45	11	0	0	0	0	0	0	0	0	0	268	38.4	44.8	35 to 45	58.2	36	13.4	0	256	12
9/19/2024	17:00	4	2	5	46	53	47	78	70	13	1	0	0	0	0	0	0	0	0	319	38.2	45.8	38 to 48	50.5	66	20.7	1	306	12
9/19/2024	18:00	2	2	2	26	62	69	103	80	12	1	0	0	0	0	0	0	0	0	359	39.3	46	37 to 47	55.4	72	20.1	1	351	7
9/19/2024	19:00	2	1	5	30	32	30	57	44	10	2	1	0	0	0	0	0	0	0	214	38.6	46.2	39 to 49	49.1	44	20.6	0	213	1
9/19/2024	20:00	1	1	1	10	28	24	41	26	7	0	1	0	0	0	0	0	0	0	140	39.2	45.2	36 to 46	57.9	23	16.4	0	138	2
9/19/2024	21:00	1	0	0	11	14	12	34	22	5	1	1	0	0	0	0	0	0	0	101	39.7	47.2	39 to 49	59.4	19	18.8	0	98	3
9/19/2024	22:00	0	1	3	16	9	10	15	8	2	0	1	0	0	0	0	0	0	0	65	36.3	44.5	37 to 47	47.7	9	13.8	0	64	1
9/19/2024	23:00	0	0	1	4	6	2	9	5	2	0	0	0	0	0	0	0	0	0	29	38.4	46	39 to 49	51.7	5	17.2	0	29	0
24 Hr Summary		32	57	231	442	600	567	830	574	112	13	4	0	0	0	0	0	0	0	3462	36.7	46	37 to 47	46.4	538	15.5	17	3286	159

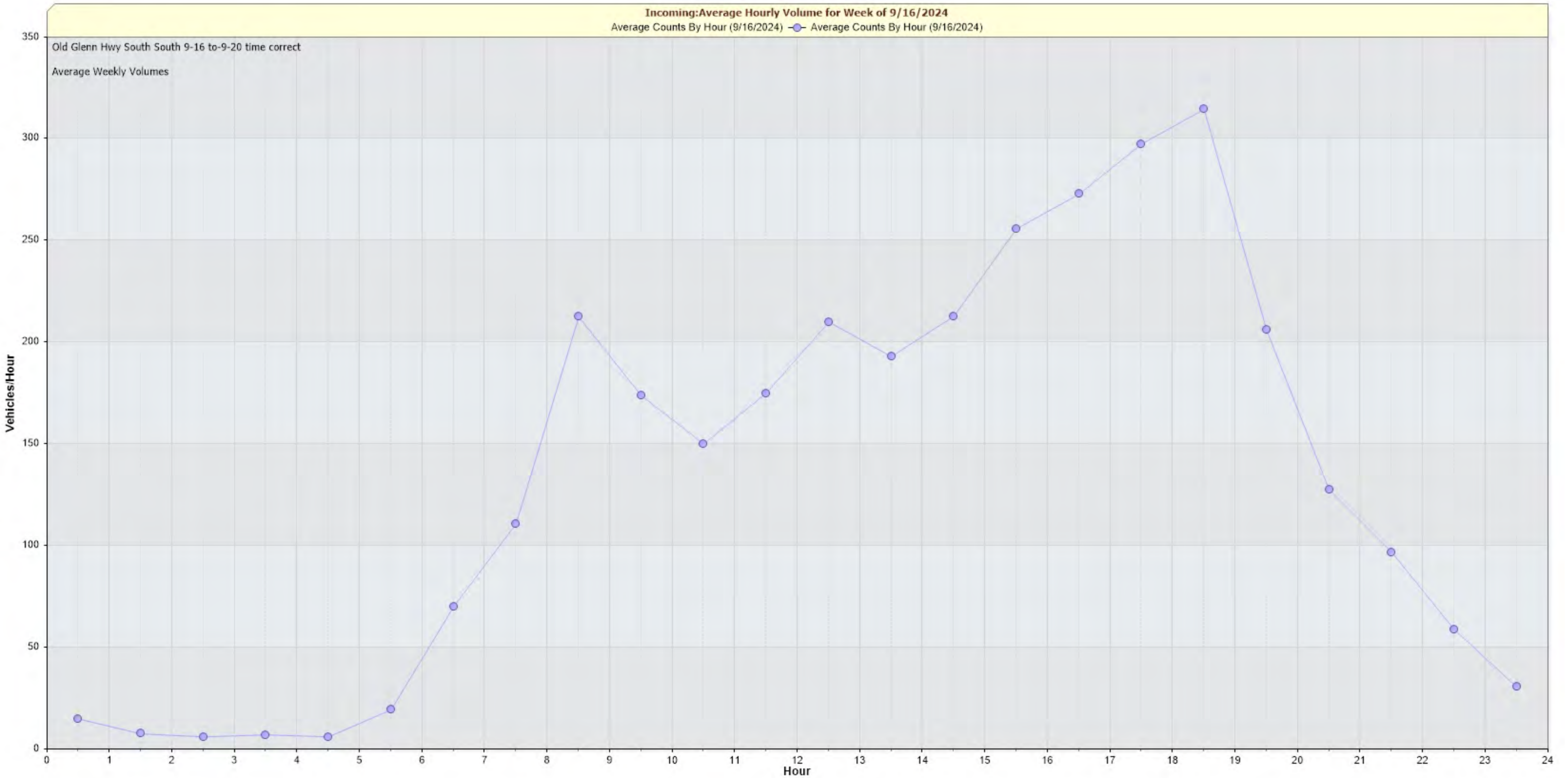
Incoming Histogram
Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/20/2024	00:00	0	0	1	1	5	2	1	3	0	0	0	0	0	0	0	0	0	0	13	35.9	46.5	25 to 35	61.5	3	23.1	0	13	0	
9/20/2024	01:00	0	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5	39.4	41	34 to 44	100.0	*	*	0	5	0	
9/20/2024	02:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	37.5	41	31 to 41	100.0	*	*	0	2	0	
9/20/2024	03:00	0	0	1	3	1	0	0	0	1	0	0	0	0	0	0	0	0	0	6	31.5	31	21 to 31	83.3	1	16.7	0	5	1	
9/20/2024	04:00	0	0	0	0	1	1	2	2	0	0	0	0	0	0	0	0	0	0	6	42.5	46.5	37 to 47	83.3	2	33.3	0	6	0	
9/20/2024	05:00	0	0	0	3	1	3	9	1	0	0	0	0	0	0	0	0	0	0	17	38.6	43.5	34 to 44	76.5	1	5.9	0	15	2	
9/20/2024	06:00	1	1	0	6	10	18	17	9	3	1	0	0	0	0	0	0	0	0	66	38.4	45	35 to 45	57.6	10	15.2	1	62	3	
9/20/2024	07:00	1	2	17	23	16	10	13	9	5	0	0	0	0	0	0	0	0	0	96	32.7	44	22 to 32	47.9	12	12.5	1	90	5	
9/20/2024	08:00	3	10	50	40	33	25	32	12	5	0	0	0	0	0	0	0	0	0	210	31	41.5	19 to 29	45.7	13	6.2	2	190	18	
9/20/2024	09:00	1	4	10	21	38	19	35	18	3	0	0	0	0	0	0	0	0	0	149	35.1	43.8	26 to 36	43.6	11	7.4	0	137	12	
9/20/2024	10:00	0	0	3	14	28	12	12	12	1	2	0	0	0	0	0	0	0	0	84	35.4	44.3	26 to 36	57.1	9	10.7	0	76	8	
9/20/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		6	17	82	111	134	93	124	66	18	3	0	0	0	0	0	0	0	0	654	34	44	26 to 36	41.3	62	9.5	4	601	49	

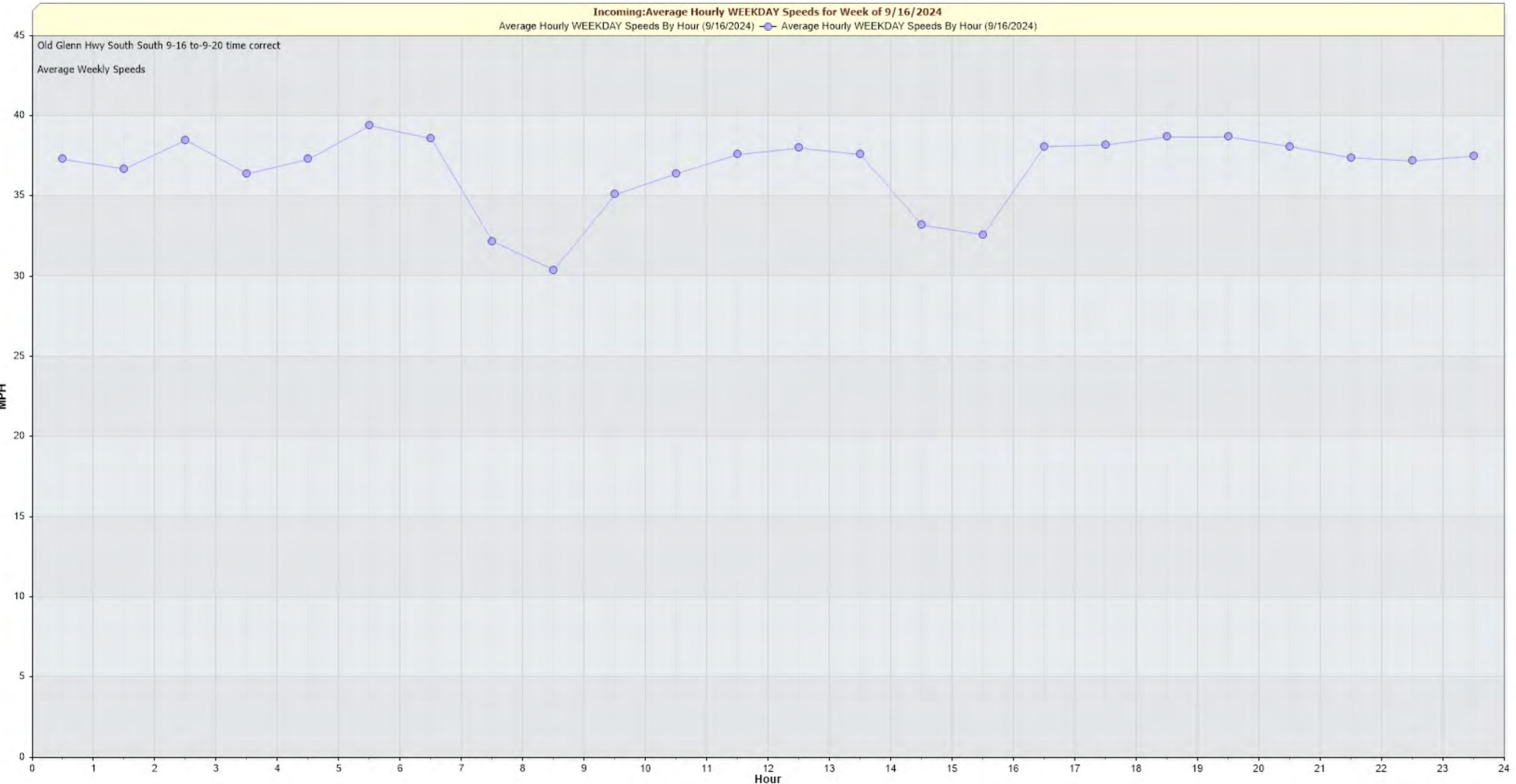
Incoming: Average Hourly Volume for Week of 9/16/2024
Average Counts By Hour (9/16/2024) ● Average Counts By Hour (9/16/2024)

Old Glenn Hwy South South 9-16 to-9-20 time correct
Average Weekly Volumes



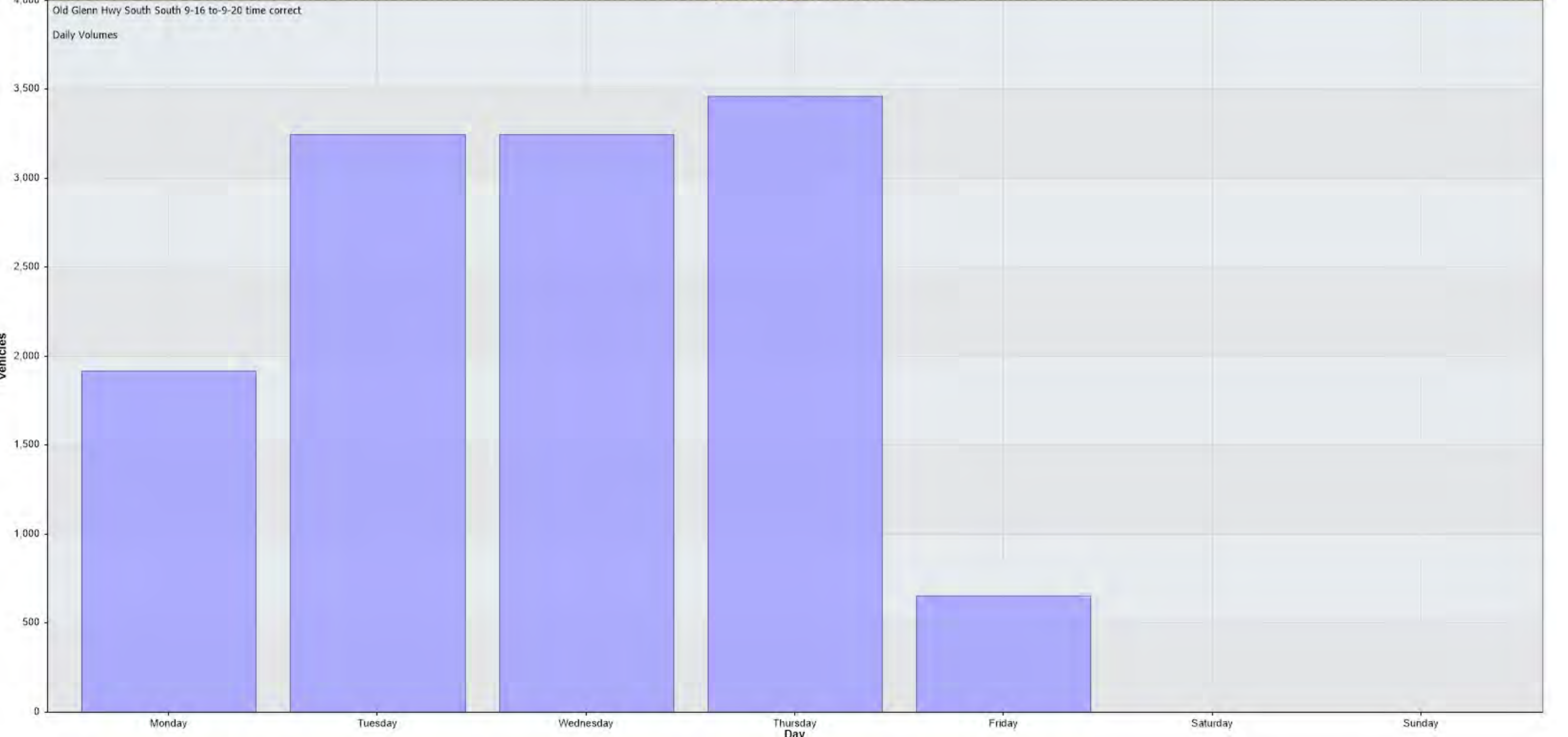
Incoming:Average Hourly WEEKDAY Speeds for Week of 9/16/2024
Average Hourly WEEKDAY Speeds By Hour (9/16/2024) — Average Hourly WEEKDAY Speeds By Hour (9/16/2024)

Old Glenn Hwy South South 9-16 to-9-20 time correct
Average Weekly Speeds



Incoming:Daily Volume for Week of 9/16/2024

Daily Vehicle Counts



For Project: Old Glenn Hwy South South 9-16 to-9-20 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:56:18 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/16/2024 1:00:00 PM through 9/20/2024 10:59:59 AM

85th Percentile Speed 43 MPH

85th Percentile Vehicles 9308

Max Speed 58 MPH on 9/18/2024 8:15:43 AM

Total Vehicles 10951

AADT: 2796

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	2190	2190
AM Peak	7:00 AM 172	172
PM Peak	5:00 PM 294	294

Speed

Speed Limit: 45

85th Percentile Speed: 43

50th Percentile Speed: 33

10 MPH Pace Interval: 36.0 MPH to 46.0 MPH

Average Speed: 33.27

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	90	178	183	181	16	N/A	N/A
% over limit	5.2	6.2	6.5	6.0	3.0	N/A	N/A
Avg Speeder	47.4	47.6	47.7	47.7	48.0	N/A	N/A
Avg Speed	33.1	33.3	33.5	33.6	30.8	N/A	N/A

Class Counts

	Number	%
VEH_SM	6	0.1
VEH_MED	10145	92.6
VEH_LG	800	7.3
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/16/2024 2:00:00 PM	44.0	99	116	49	47.4	6.9%	34.9
9/16/2024 3:00:00 PM	39.0	210	247	51	47.5	2.4%	29.0
9/16/2024 4:00:00 PM	42.0	221	260	49	46.7	4.6%	31.9
9/16/2024 5:00:00 PM	43.0	236	278	56	47.6	6.1%	33.9
9/16/2024 6:00:00 PM	44.0	226	266	52	47.6	6.0%	36.0
9/16/2024 7:00:00 PM	43.0	175	206	53	47.3	7.3%	34.1
9/16/2024 8:00:00 PM	45.0	97	114	50	47.5	9.6%	34.6
9/16/2024 9:00:00 PM	42.0	75	88	45	0.0	0.0%	33.0
9/16/2024 10:00:00 PM	43.0	60	70	47	46.7	4.3%	32.6
9/16/2024 11:00:00 PM	40.0	42	50	48	48.0	4.0%	32.1
9/17/2024 12:00:00 AM	37.0	21	25	45	0.0	0.0%	31.1
9/17/2024 1:00:00 AM	45.0	8	9	54	54.0	11.1%	32.4
9/17/2024 2:00:00 AM	41.0	10	12	49	49.0	8.3%	36.3
9/17/2024 3:00:00 AM	44.0	7	8	48	48.0	12.5%	35.6
9/17/2024 4:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
9/17/2024 5:00:00 AM	44.0	1	1	44	0.0	0.0%	44.0
9/17/2024 6:00:00 AM	45.0	3	4	48	48.0	25.0%	40.3
9/17/2024 7:00:00 AM	43.0	21	25	47	46.5	7.7%	34.1
9/17/2024 8:00:00 AM	41.0	138	162	52	49.7	1.9%	29.0
9/17/2024 9:00:00 AM	42.0	104	122	51	47.8	4.9%	30.1
9/17/2024 10:00:00 AM	42.0	131	154	52	48.4	5.2%	32.2
9/17/2024 11:00:00 AM	43.0	131	154	53	47.8	5.2%	32.6
9/17/2024 12:00:00 PM	44.0	133	156	55	48.2	5.8%	33.7
9/17/2024 1:00:00 PM	45.0	148	174	51	47.5	10.3%	35.3
9/17/2024 2:00:00 PM	43.0	158	186	51	47.5	7.0%	33.8
9/17/2024 3:00:00 PM	39.0	199	234	48	46.7	2.6%	28.9
9/17/2024 4:00:00 PM	42.0	230	270	56	48.2	4.8%	31.4
9/17/2024 5:00:00 PM	42.0	235	277	50	47.6	4.3%	34.4
9/17/2024 6:00:00 PM	44.0	275	323	54	47.4	8.4%	35.8
9/17/2024 7:00:00 PM	45.0	212	250	53	46.7	12.4%	36.9
9/17/2024 8:00:00 PM	43.0	120	141	52	48.8	2.8%	34.5
9/17/2024 9:00:00 PM	41.0	78	92	49	48.3	4.3%	33.8
9/17/2024 10:00:00 PM	43.0	50	59	50	49.0	6.8%	34.9
9/17/2024 11:00:00 PM	46.0	26	31	47	46.2	19.4%	33.6
9/18/2024 12:00:00 AM	45.0	8	10	45	0.0	0.0%	35.2
9/18/2024 1:00:00 AM	36.0	12	14	46	46.0	7.1%	30.0
9/18/2024 2:00:00 AM	44.0	4	5	47	47.0	20.0%	36.8
9/18/2024 3:00:00 AM	**No Data**						
9/18/2024 4:00:00 AM	40.0	2	2	40	0.0	0.0%	39.0
9/18/2024 5:00:00 AM	26.0	2	2	26	0.0	0.0%	23.0
9/18/2024 6:00:00 AM	43.0	4	5	44	0.0	0.0%	35.0
9/18/2024 7:00:00 AM	44.0	20	23	50	48.5	8.7%	35.1
9/18/2024 8:00:00 AM	39.0	139	163	53	48.6	3.0%	28.5
9/18/2024 9:00:00 AM	43.0	103	121	58	50.0	6.6%	31.2
9/18/2024 10:00:00 AM	42.0	116	137	49	47.3	2.9%	32.5
9/18/2024 11:00:00 AM	43.0	106	125	54	48.2	7.2%	34.1
9/18/2024 12:00:00 PM	44.0	144	170	51	48.3	8.8%	35.1

Outgoing Summary
Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

9/18/2024 1:00:00 PM	44.0	125	147	48	46.6	8.2%	35.4
9/18/2024 2:00:00 PM	42.0	133	157	54	47.5	8.9%	33.5
9/18/2024 3:00:00 PM	40.0	206	242	51	48.0	2.5%	29.4
9/18/2024 4:00:00 PM	44.0	215	253	51	47.5	8.3%	33.6
9/18/2024 5:00:00 PM	44.0	224	264	58	47.9	6.1%	35.4
9/18/2024 6:00:00 PM	44.0	244	287	51	47.1	9.8%	35.7
9/18/2024 7:00:00 PM	44.0	195	229	52	47.6	8.3%	34.6
9/18/2024 8:00:00 PM	42.0	139	163	53	48.0	5.5%	33.0
9/18/2024 9:00:00 PM	44.0	112	132	53	47.9	6.8%	35.3
9/18/2024 10:00:00 PM	42.0	82	97	48	47.0	2.1%	32.3
9/18/2024 11:00:00 PM	44.0	35	41	49	48.0	4.9%	35.6
9/19/2024 12:00:00 AM	44.0	23	27	45	0.0	0.0%	34.0
9/19/2024 1:00:00 AM	43.0	8	9	43	0.0	0.0%	31.1
9/19/2024 2:00:00 AM	44.0	8	10	46	46.0	10.0%	31.3
9/19/2024 3:00:00 AM	39.0	5	6	41	0.0	0.0%	28.2
9/19/2024 4:00:00 AM	43.0	4	5	49	49.0	20.0%	35.4
9/19/2024 5:00:00 AM	35.0	5	6	51	51.0	16.7%	34.3
9/19/2024 6:00:00 AM	43.0	5	6	50	50.0	16.7%	38.0
9/19/2024 7:00:00 AM	42.0	19	22	46	46.0	4.5%	35.4
9/19/2024 8:00:00 AM	39.0	156	184	52	48.0	3.3%	28.4
9/19/2024 9:00:00 AM	40.0	96	113	49	47.7	2.7%	29.7
9/19/2024 10:00:00 AM	44.0	123	145	49	47.7	4.8%	32.9
9/19/2024 11:00:00 AM	42.0	141	166	50	48.0	4.8%	33.3
9/19/2024 12:00:00 PM	44.0	147	173	50	47.6	8.7%	35.2
9/19/2024 1:00:00 PM	43.0	142	167	48	46.9	4.8%	34.4
9/19/2024 2:00:00 PM	44.0	167	196	53	47.8	9.7%	34.8
9/19/2024 3:00:00 PM	41.0	221	260	50	47.5	3.1%	30.6
9/19/2024 4:00:00 PM	42.0	230	271	52	47.5	5.5%	33.0
9/19/2024 5:00:00 PM	43.0	240	282	50	47.3	5.7%	34.5
9/19/2024 6:00:00 PM	44.0	257	302	50	47.1	8.3%	35.7
9/19/2024 7:00:00 PM	44.0	214	252	58	47.2	7.9%	35.0
9/19/2024 8:00:00 PM	43.0	157	185	58	49.5	5.9%	35.3
9/19/2024 9:00:00 PM	44.0	101	119	55	49.2	10.1%	36.7
9/19/2024 10:00:00 PM	42.0	66	78	50	48.5	2.6%	32.6
9/19/2024 11:00:00 PM	41.0	42	50	49	49.0	2.0%	30.7
9/20/2024 12:00:00 AM	44.0	19	22	45	0.0	0.0%	33.2
9/20/2024 1:00:00 AM	43.0	3	3	43	0.0	0.0%	33.0
9/20/2024 2:00:00 AM	28.0	4	5	29	0.0	0.0%	25.2
9/20/2024 3:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
9/20/2024 4:00:00 AM	38.0	4	5	44	0.0	0.0%	29.8
9/20/2024 5:00:00 AM	25.0	2	2	25	0.0	0.0%	23.5
9/20/2024 6:00:00 AM	43.0	5	6	45	0.0	0.0%	38.2
9/20/2024 7:00:00 AM	43.0	16	19	49	47.5	10.5%	34.2
9/20/2024 8:00:00 AM	38.0	154	181	49	47.3	2.2%	28.5
9/20/2024 9:00:00 AM	42.0	100	118	50	47.3	3.4%	30.7
9/20/2024 10:00:00 AM	42.0	116	136	54	49.0	3.7%	33.0
9/20/2024 11:00:00 AM	40.0	55	65	50	50.0	1.5%	31.2

Outgoing Summary
Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/17/2024 12:00:00 AM	43.0	1462	1720	56	47.4	5.2%	33.1
9/18/2024 12:00:00 AM	43.0	2427	2855	56	47.6	6.2%	33.3
9/19/2024 12:00:00 AM	43.0	2385	2806	58	47.7	6.5%	33.5
9/20/2024 12:00:00 AM	43.0	2575	3029	58	47.7	6.0%	33.6
9/20/2024 10:59:59 AM	41.0	460	541	54	48.0	3.0%	30.8

Outgoing Weekly Counts
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	9	14	9	3	*	*	8.75	0	41.68
1 - 2	*	12	5	10	5	*	*	8	0	39.25
2 - 3	*	8	0	6	1	*	*	3.75	0	39.33
3 - 4	*	1	2	5	5	*	*	3.25	0	37.25
4 - 5	*	1	2	6	2	*	*	2.75	0	32.5
5 - 6	*	4	5	6	6	*	*	5.25	0	43.5
6 - 7	*	25	23	22	19	*	*	22.25	0	42.62
7 - 8	*	162	163	184	181	*	*	172.5	0	38.88
8 - 9	*	122	121	113	118	*	*	118.5	0	41.47
9 - 10	*	154	137	145	136	*	*	143	0	42.08
10 - 11	*	154	125	166	65	*	*	127.5	0	41.7
11 - 12	*	156	170	173	*	*	*	166.33	0	43.4
12 - 13	*	174	147	167	*	*	*	162.67	0	43.53
13 - 14	116	186	157	196	*	*	*	163.75	0	42.85
14 - 15	247	234	242	260	*	*	*	245.75	0	39.48
15 - 16	260	270	253	271	*	*	*	263.5	0	42.15
16 - 17	278	277	264	282	*	*	*	275.25	0	42.5
17 - 18	266	323	287	302	*	*	*	294.5	0	43.48
18 - 19	206	250	229	252	*	*	*	234.25	0	43.4
19 - 20	114	141	163	185	*	*	*	150.75	0	42.75
20 - 21	88	92	132	119	*	*	*	107.75	0	42.3
21 - 22	70	59	97	78	*	*	*	76	0	41.92
22 - 23	50	31	41	50	*	*	*	43	0	42.38
23 - 24	25	10	27	22	*	*	*	21	0	42.32
Totals	1720	2855	2806	3029	541	0	0			
% of Total	15.71%	26.07%	25.62%	27.66%	4.94%	0%	0%			

Outgoing Monthly Counts
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	9	14	9	3	*	*	8.75	0	41.68
1 - 2	*	12	5	10	5	*	*	8	0	39.25
2 - 3	*	8	0	6	1	*	*	3.75	0	39.33
3 - 4	*	1	2	5	5	*	*	3.25	0	37.25
4 - 5	*	1	2	6	2	*	*	2.75	0	32.5
5 - 6	*	4	5	6	6	*	*	5.25	0	43.5
6 - 7	*	25	23	22	19	*	*	22.25	0	42.62
7 - 8	*	162	163	184	181	*	*	172.5	0	38.88
8 - 9	*	122	121	113	118	*	*	118.5	0	41.47
9 - 10	*	154	137	145	136	*	*	143	0	42.08
10 - 11	*	154	125	166	65	*	*	127.5	0	41.7
11 - 12	*	156	170	173	*	*	*	166.33	0	43.4
12 - 13	*	174	147	167	*	*	*	162.67	0	43.53
13 - 14	116	186	157	196	*	*	*	163.75	0	42.85
14 - 15	247	234	242	260	*	*	*	245.75	0	39.48
15 - 16	260	270	253	271	*	*	*	263.5	0	42.15
16 - 17	278	277	264	282	*	*	*	275.25	0	42.5
17 - 18	266	323	287	302	*	*	*	294.5	0	43.48
18 - 19	206	250	229	252	*	*	*	234.25	0	43.4
19 - 20	114	141	163	185	*	*	*	150.75	0	42.75
20 - 21	88	92	132	119	*	*	*	107.75	0	42.3
21 - 22	70	59	97	78	*	*	*	76	0	41.92
22 - 23	50	31	41	50	*	*	*	43	0	42.38
23 - 24	25	10	27	22	*	*	*	21	0	42.32
Totals	1720	2855	2806	3029	541	0	0			
% of Total	15.71%	26.07%	25.62%	27.66%	4.94%	0%	0%			

Outgoing Weekly Speeds
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	32.44	30	31.11	33	*	*	31.17	*	41.68
1 - 2	*	36.25	36.8	31.3	25.2	*	*	33.06	*	39.25
2 - 3	*	35.62	0	28.17	35	*	*	32.6	*	39.33
3 - 4	*	28	39	35.4	29.8	*	*	33.23	*	37.25
4 - 5	*	44	23	34.33	23.5	*	*	31.18	*	32.5
5 - 6	*	40.25	35	38	38.17	*	*	37.76	*	43.5
6 - 7	*	34.08	35.13	35.36	34.21	*	*	34.7	*	42.62
7 - 8	*	28.96	28.5	28.42	28.5	*	*	28.59	*	38.88
8 - 9	*	30.12	31.24	29.73	30.73	*	*	30.46	*	41.47
9 - 10	*	32.21	32.5	32.9	33.02	*	*	32.65	*	42.08
10 - 11	*	32.62	34.13	33.32	31.17	*	*	33.03	*	41.7
11 - 12	*	33.72	35.11	35.21	*	*	*	34.71	*	43.4
12 - 13	*	35.31	35.38	34.41	*	*	*	35.02	*	43.53
13 - 14	34.69	33.85	33.55	34.83	*	*	*	34.22	*	42.85
14 - 15	28.96	28.89	29.37	30.6	*	*	*	29.48	*	39.48
15 - 16	31.9	31.42	33.63	32.96	*	*	*	32.46	*	42.15
16 - 17	33.93	34.45	35.36	34.49	*	*	*	34.55	*	42.5
17 - 18	35.98	35.82	35.7	35.74	*	*	*	35.81	*	43.48
18 - 19	34.07	36.88	34.56	34.99	*	*	*	35.19	*	43.4
19 - 20	34.55	34.5	32.98	35.26	*	*	*	34.33	*	42.75
20 - 21	33.05	33.84	35.33	36.71	*	*	*	34.93	*	42.3
21 - 22	32.63	34.9	32.34	32.62	*	*	*	32.97	*	41.92
22 - 23	32.12	33.61	35.63	30.72	*	*	*	32.82	*	42.38
23 - 24	31.12	35.2	33.96	33.23	*	*	*	33.07	*	42.32
Totals	33.2	33.3	33.5	33.5	30.7	0	0			
% of Total	20.22%	20.28%	20.4%	20.4%	18.7%	0%	0%			

Outgoing Monthly Speeds

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	32.44	30	31.11	33	*	*	31.17	*	41.68
1 - 2	*	36.25	36.8	31.3	25.2	*	*	33.06	*	39.25
2 - 3	*	35.62	0	28.17	35	*	*	32.6	*	39.33
3 - 4	*	28	39	35.4	29.8	*	*	33.23	*	37.25
4 - 5	*	44	23	34.33	23.5	*	*	31.18	*	32.5
5 - 6	*	40.25	35	38	38.17	*	*	37.76	*	43.5
6 - 7	*	34.08	35.13	35.36	34.21	*	*	34.7	*	42.62
7 - 8	*	28.96	28.5	28.42	28.5	*	*	28.59	*	38.88
8 - 9	*	30.12	31.24	29.73	30.73	*	*	30.46	*	41.47
9 - 10	*	32.21	32.5	32.9	33.02	*	*	32.65	*	42.08
10 - 11	*	32.62	34.13	33.32	31.17	*	*	33.03	*	41.7
11 - 12	*	33.72	35.11	35.21	*	*	*	34.71	*	43.4
12 - 13	*	35.31	35.38	34.41	*	*	*	35.02	*	43.53
13 - 14	34.69	33.85	33.55	34.83	*	*	*	34.22	*	42.85
14 - 15	28.96	28.89	29.37	30.6	*	*	*	29.48	*	39.48
15 - 16	31.9	31.42	33.63	32.96	*	*	*	32.46	*	42.15
16 - 17	33.93	34.45	35.36	34.49	*	*	*	34.55	*	42.5
17 - 18	35.98	35.82	35.7	35.74	*	*	*	35.81	*	43.48
18 - 19	34.07	36.88	34.56	34.99	*	*	*	35.19	*	43.4
19 - 20	34.55	34.5	32.98	35.26	*	*	*	34.33	*	42.75
20 - 21	33.05	33.84	35.33	36.71	*	*	*	34.93	*	42.3
21 - 22	32.63	34.9	32.34	32.62	*	*	*	32.97	*	41.92
22 - 23	32.12	33.61	35.63	30.72	*	*	*	32.82	*	42.38
23 - 24	31.12	35.2	33.96	33.23	*	*	*	33.07	*	42.32
Totals	33.2	33.3	33.5	33.5	30.7	0	0			
% of Total	20.22%	20.28%	20.4%	20.4%	18.7%	0%	0%			

Outgoing Weekly EightyFifthSpeeds
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	45	36	42.7	43	*	*	41.68	0	41.68
1 - 2	*	41	44	44	28	*	*	39.25	0	39.25
2 - 3	*	44	0	39	35	*	*	39.33	0	39.33
3 - 4	*	28	40	43	38	*	*	37.25	0	37.25
4 - 5	*	44	26	35	25	*	*	32.5	0	32.5
5 - 6	*	45	43	43	43	*	*	43.5	0	43.5
6 - 7	*	42.3	43.7	42	42.5	*	*	42.62	0	42.62
7 - 8	*	40.6	38.5	39	37.4	*	*	38.88	0	38.88
8 - 9	*	41.9	42.2	39.8	42	*	*	41.47	0	41.47
9 - 10	*	41.7	41.2	43.4	42	*	*	42.08	0	42.08
10 - 11	*	42.2	42.9	41.7	40	*	*	41.7	0	41.7
11 - 12	*	43.1	43.2	43.9	*	*	*	43.4	0	43.4
12 - 13	*	44.1	43.5	43	*	*	*	43.53	0	43.53
13 - 14	43.3	42.5	42	43.6	*	*	*	42.85	0	42.85
14 - 15	38.6	38.7	40	40.6	*	*	*	39.48	0	39.48
15 - 16	41.8	41.8	43.1	41.9	*	*	*	42.15	0	42.15
16 - 17	42.4	42	43.1	42.5	*	*	*	42.5	0	42.5
17 - 18	43.5	43.3	43.7	43.4	*	*	*	43.48	0	43.48
18 - 19	42.9	44.3	43.2	43.2	*	*	*	43.4	0	43.4
19 - 20	44.1	42.6	41.7	42.6	*	*	*	42.75	0	42.75
20 - 21	41.8	40.5	43.3	43.6	*	*	*	42.3	0	42.3
21 - 22	42.2	42.5	41.4	41.6	*	*	*	41.92	0	41.92
22 - 23	40	45.2	44	40.3	*	*	*	42.38	0	42.38
23 - 24	37	44.3	44	44	*	*	*	42.32	0	42.32
Totals	457.6	1010.6	953.7	1006.8	415.9	0	0			
% of Total	11.9%	26.29%	24.81%	26.19%	10.82%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	45	36	42.7	43	*	*	41.68	0	41.68
1 - 2	*	41	44	44	28	*	*	39.25	0	39.25
2 - 3	*	44	0	39	35	*	*	39.33	0	39.33
3 - 4	*	28	40	43	38	*	*	37.25	0	37.25
4 - 5	*	44	26	35	25	*	*	32.5	0	32.5
5 - 6	*	45	43	43	43	*	*	43.5	0	43.5
6 - 7	*	42.3	43.7	42	42.5	*	*	42.62	0	42.62
7 - 8	*	40.6	38.5	39	37.4	*	*	38.88	0	38.88
8 - 9	*	41.9	42.2	39.8	42	*	*	41.47	0	41.47
9 - 10	*	41.7	41.2	43.4	42	*	*	42.08	0	42.08
10 - 11	*	42.2	42.9	41.7	40	*	*	41.7	0	41.7
11 - 12	*	43.1	43.2	43.9	*	*	*	43.4	0	43.4
12 - 13	*	44.1	43.5	43	*	*	*	43.53	0	43.53
13 - 14	43.3	42.5	42	43.6	*	*	*	42.85	0	42.85
14 - 15	38.6	38.7	40	40.6	*	*	*	39.48	0	39.48
15 - 16	41.8	41.8	43.1	41.9	*	*	*	42.15	0	42.15
16 - 17	42.4	42	43.1	42.5	*	*	*	42.5	0	42.5
17 - 18	43.5	43.3	43.7	43.4	*	*	*	43.48	0	43.48
18 - 19	42.9	44.3	43.2	43.2	*	*	*	43.4	0	43.4
19 - 20	44.1	42.6	41.7	42.6	*	*	*	42.75	0	42.75
20 - 21	41.8	40.5	43.3	43.6	*	*	*	42.3	0	42.3
21 - 22	42.2	42.5	41.4	41.6	*	*	*	41.92	0	41.92
22 - 23	40	45.2	44	40.3	*	*	*	42.38	0	42.38
23 - 24	37	44.3	44	44	*	*	*	42.32	0	42.32

Summary of Violators

Old Glenn Hwy South South 9-16 to-9-20 time correct from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	35	31.2	2	50.0
01:00:00	32	33.1	3	47.3
02:00:00	15	32.6	1	48.0
03:00:00	13	33.2	1	49.0
04:00:00	11	31.2	1	51.0
05:00:00	21	37.8	2	49.0
06:00:00	89	34.7	7	47.3
07:00:00	690	28.6	18	48.3
08:00:00	474	30.5	21	48.5
09:00:00	572	32.6	24	48.1
10:00:00	510	33.0	26	48.1
11:00:00	499	34.7	39	48.0
12:00:00	488	35.0	38	47.1
13:00:00	655	34.2	54	47.6
14:00:00	983	29.5	26	47.4
15:00:00	1054	32.5	61	47.5
16:00:00	1101	34.5	61	47.6
17:00:00	1178	35.8	96	47.3
18:00:00	937	35.2	85	47.1
19:00:00	603	34.3	35	48.4
20:00:00	431	34.9	25	48.6
21:00:00	304	33.0	11	47.9
22:00:00	172	32.8	11	47.1
23:00:00	84	33.1	0	0.0

Outgoing Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	13:00	0	2	15	20	20	15	32	12	0	0	0	0	0	0	0	0	0	0	116	34.7	43.3	36 to 46	45.7	8	6.9	6	105	5	
9/16/2024	14:00	0	23	74	52	27	38	23	9	1	0	0	0	0	0	0	0	0	0	247	29	38.6	19 to 29	55.5	6	2.4	0	227	20	
9/16/2024	15:00	2	16	38	57	49	32	49	17	0	0	0	0	0	0	0	0	0	0	260	31.9	41.8	22 to 32	43.5	12	4.6	0	242	18	
9/16/2024	16:00	1	10	34	46	52	34	76	23	1	1	0	0	0	0	0	0	0	0	278	33.9	42.4	36 to 46	43.5	17	6.1	0	249	29	
9/16/2024	17:00	0	2	31	26	45	52	78	31	1	0	0	0	0	0	0	0	0	0	266	36	43.5	35 to 45	54.9	16	6.0	0	252	14	
9/16/2024	18:00	1	2	21	49	36	29	49	18	1	0	0	0	0	0	0	0	0	0	206	34.1	42.9	23 to 33	43.2	15	7.3	0	195	11	
9/16/2024	19:00	1	2	14	20	15	21	23	16	2	0	0	0	0	0	0	0	0	0	114	34.6	44.1	38 to 48	47.4	11	9.6	0	108	6	
9/16/2024	20:00	0	2	13	22	12	11	25	3	0	0	0	0	0	0	0	0	0	0	88	33	41.8	22 to 32	47.7	*	*	0	86	2	
9/16/2024	21:00	2	6	8	7	14	15	14	4	0	0	0	0	0	0	0	0	0	0	70	32.6	42.2	34 to 44	48.6	3	4.3	0	65	5	
9/16/2024	22:00	0	0	8	15	4	12	9	2	0	0	0	0	0	0	0	0	0	0	50	32.1	40	21 to 31	48.0	2	4.0	0	50	0	
9/16/2024	23:00	0	1	4	6	5	6	2	1	0	0	0	0	0	0	0	0	0	0	25	31.1	37	26 to 36	60.0	*	*	0	24	1	
24 Hr Summary		7	66	260	320	279	265	380	136	6	1	0	0	0	0	0	0	0	0	1720	33.1	43	35 to 45	40.6	90	5.2	6	1603	111	

Outgoing Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/17/2024	00:00	0	0	3	3	0	0	1	1	1	0	0	0	0	0	0	0	0	0	9	32.4	45	19 to 29	66.7	1	11.1	0	7	2
9/17/2024	01:00	0	0	1	2	1	3	4	1	0	0	0	0	0	0	0	0	0	0	12	36.3	41	31 to 41	58.3	1	8.3	0	12	0
9/17/2024	02:00	0	0	0	3	0	1	3	1	0	0	0	0	0	0	0	0	0	0	8	35.6	44	34 to 44	50.0	1	12.5	0	8	0
9/17/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0
9/17/2024	04:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	44	44	34 to 44	100.0	*	*	0	1	0
9/17/2024	05:00	0	0	0	1	0	0	1	2	0	0	0	0	0	0	0	0	0	0	4	40.3	45	38 to 48	75.0	1	25.0	0	4	0
9/17/2024	06:00	0	0	3	6	4	4	6	2	0	0	0	0	0	0	0	0	0	0	25	34.1	42.3	21 to 31	52.0	2	8.0	0	23	2
9/17/2024	07:00	0	17	49	34	15	17	22	6	2	0	0	0	0	0	0	0	0	0	162	29	40.6	18 to 28	55.6	3	1.9	0	151	11
9/17/2024	08:00	0	20	28	22	7	13	21	10	1	0	0	0	0	0	0	0	0	0	122	30.1	41.9	17 to 27	49.2	6	4.9	0	108	14
9/17/2024	09:00	0	14	24	28	21	23	33	9	2	0	0	0	0	0	0	0	0	0	154	32.2	41.7	22 to 32	39.0	8	5.2	0	131	23
9/17/2024	10:00	0	9	31	22	23	25	31	12	1	0	0	0	0	0	0	0	0	0	154	32.6	42.2	37 to 47	40.3	8	5.2	0	137	17
9/17/2024	11:00	1	6	22	31	16	31	34	14	0	1	0	0	0	0	0	0	0	0	156	33.7	43.1	35 to 45	45.5	9	5.8	0	141	15
9/17/2024	12:00	1	9	16	21	30	23	47	24	3	0	0	0	0	0	0	0	0	0	174	35.3	44.1	36 to 46	48.3	18	10.3	0	155	19
9/17/2024	13:00	0	10	17	34	38	23	48	13	3	0	0	0	0	0	0	0	0	0	186	33.8	42.5	23 to 33	41.9	13	7.0	0	172	14
9/17/2024	14:00	1	19	64	61	26	30	23	10	0	0	0	0	0	0	0	0	0	0	234	28.9	38.7	18 to 28	57.7	6	2.6	0	215	19
9/17/2024	15:00	3	26	55	35	40	34	60	15	1	1	0	0	0	0	0	0	0	0	270	31.4	41.8	19 to 29	37.0	13	4.8	0	250	20
9/17/2024	16:00	1	5	27	55	38	64	63	22	2	0	0	0	0	0	0	0	0	0	277	34.4	42	35 to 45	50.2	12	4.3	0	253	24
9/17/2024	17:00	2	5	24	49	53	50	100	37	3	0	0	0	0	0	0	0	0	0	323	35.8	43.3	36 to 46	50.8	27	8.4	0	308	15
9/17/2024	18:00	0	4	11	35	37	48	74	40	1	0	0	0	0	0	0	0	0	0	250	36.9	44.3	37 to 47	58.4	31	12.4	0	237	13
9/17/2024	19:00	0	5	13	30	12	32	38	9	2	0	0	0	0	0	0	0	0	0	141	34.5	42.6	35 to 45	54.6	4	2.8	0	133	8
9/17/2024	20:00	0	1	10	17	21	16	21	6	0	0	0	0	0	0	0	0	0	0	92	33.8	40.5	31 to 41	52.2	4	4.3	0	89	3
9/17/2024	21:00	0	0	8	10	8	16	10	5	2	0	0	0	0	0	0	0	0	0	59	34.9	42.5	32 to 42	49.2	4	6.8	0	59	0
9/17/2024	22:00	0	0	8	5	3	3	5	7	0	0	0	0	0	0	0	0	0	0	31	33.6	45.2	21 to 31	45.2	6	19.4	0	30	1
9/17/2024	23:00	0	0	2	1	2	1	1	3	0	0	0	0	0	0	0	0	0	0	10	35.2	44.3	35 to 45	50.0	*	*	0	9	1
24 Hr Summary		9	150	416	506	395	457	647	249	24	2	0	0	0	0	0	0	0	0	2855	33.3	43	36 to 46	42.3	178	6.2	0	2634	221

Outgoing Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/18/2024	00:00	0	0	4	4	1	4	0	1	0	0	0	0	0	0	0	0	0	0	14	30	36	20 to 30	64.3	1	7.1	0	14	0	
9/18/2024	01:00	0	0	0	1	1	1	1	1	0	0	0	0	0	0	0	0	0	0	5	36.8	44	26 to 36	60.0	1	20.0	0	5	0	
9/18/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/18/2024	03:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	39	40	30 to 40	100.0	*	*	0	2	0	
9/18/2024	04:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	23	26	16 to 26	100.0	*	*	0	2	0	
9/18/2024	05:00	0	0	1	1	0	1	2	0	0	0	0	0	0	0	0	0	0	0	5	35	43	34 to 44	60.0	*	*	0	5	0	
9/18/2024	06:00	0	0	3	5	3	2	8	1	1	0	0	0	0	0	0	0	0	0	23	35.1	43.7	34 to 44	43.5	2	8.7	0	20	3	
9/18/2024	07:00	0	15	55	30	24	18	15	5	1	0	0	0	0	0	0	0	0	0	163	28.5	38.5	18 to 28	56.4	5	3.1	0	150	13	
9/18/2024	08:00	0	12	27	22	14	15	22	6	2	1	0	0	0	0	0	0	0	0	121	31.2	42.2	19 to 29	44.6	8	6.6	0	115	6	
9/18/2024	09:00	2	7	19	25	20	29	26	9	0	0	0	0	0	0	0	0	0	0	137	32.5	41.2	33 to 43	45.3	4	2.9	0	120	17	
9/18/2024	10:00	1	5	18	17	20	18	33	11	2	0	0	0	0	0	0	0	0	0	125	34.1	42.9	37 to 47	44.8	9	7.2	0	115	10	
9/18/2024	11:00	0	3	18	40	16	23	48	18	4	0	0	0	0	0	0	0	0	0	170	35.1	43.2	36 to 46	47.1	15	8.8	0	152	18	
9/18/2024	12:00	0	5	12	25	20	25	42	18	0	0	0	0	0	0	0	0	0	0	147	35.4	43.5	36 to 46	53.1	12	8.2	0	126	21	
9/18/2024	13:00	0	4	14	40	29	31	23	14	2	0	0	0	0	0	0	0	0	0	157	33.5	42	24 to 34	46.5	14	8.9	0	141	16	
9/18/2024	14:00	1	22	66	50	36	28	31	6	2	0	0	0	0	0	0	0	0	0	242	29.4	40	19 to 29	51.7	6	2.5	0	223	19	
9/18/2024	15:00	1	15	34	42	40	33	59	25	4	0	0	0	0	0	0	0	0	0	253	33.6	43.1	36 to 46	43.9	21	8.3	0	235	18	
9/18/2024	16:00	1	10	20	42	37	46	82	23	2	1	0	0	0	0	0	0	0	0	264	35.4	43.1	35 to 45	52.3	16	6.1	0	244	20	
9/18/2024	17:00	1	6	17	55	41	52	77	36	2	0	0	0	0	0	0	0	0	0	287	35.7	43.7	37 to 47	49.5	28	9.8	0	273	14	
9/18/2024	18:00	0	4	23	48	42	26	58	25	3	0	0	0	0	0	0	0	0	0	229	34.6	43.2	24 to 34	43.7	19	8.3	0	213	16	
9/18/2024	19:00	0	6	19	37	31	27	33	7	3	0	0	0	0	0	0	0	0	0	163	33	41.7	23 to 33	46.0	9	5.5	0	152	11	
9/18/2024	20:00	0	4	11	19	23	19	42	13	1	0	0	0	0	0	0	0	0	0	132	35.3	43.3	35 to 45	50.0	9	6.8	0	128	4	
9/18/2024	21:00	0	0	21	23	10	18	23	2	0	0	0	0	0	0	0	0	0	0	97	32.3	41.4	20 to 30	47.4	2	2.1	0	93	4	
9/18/2024	22:00	0	0	6	6	4	8	11	6	0	0	0	0	0	0	0	0	0	0	41	35.6	44	35 to 45	56.1	2	4.9	0	39	2	
9/18/2024	23:00	0	1	5	6	1	2	8	4	0	0	0	0	0	0	0	0	0	0	27	34	44	35 to 45	51.9	*	*	0	27	0	
24 Hr Summary		7	119	394	539	413	427	645	231	29	2	0	0	0	0	0	0	0	0	2806	33.5	43	36 to 46	41.2	183	6.5	0	2594	212	

Outgoing Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/19/2024	00:00	0	0	2	4	0	0	3	0	0	0	0	0	0	0	0	0	0	0	9	31.1	42.7	19 to 29	66.7	*	*	0	8	1
9/19/2024	01:00	0	0	3	3	1	0	1	2	0	0	0	0	0	0	0	0	0	0	10	31.3	44	17 to 27	60.0	1	10.0	0	9	1
9/19/2024	02:00	0	1	2	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	6	28.2	39	17 to 27	66.7	*	*	0	4	2
9/19/2024	03:00	0	0	2	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	5	35.4	43	39 to 49	60.0	1	20.0	0	5	0
9/19/2024	04:00	0	0	0	3	1	1	0	0	1	0	0	0	0	0	0	0	0	0	6	34.3	35	25 to 35	83.3	1	16.7	0	6	0
9/19/2024	05:00	0	0	1	0	1	1	2	0	1	0	0	0	0	0	0	0	0	0	6	38	43	33 to 43	66.7	1	16.7	0	6	0
9/19/2024	06:00	0	0	2	3	4	5	7	1	0	0	0	0	0	0	0	0	0	0	22	35.4	42	34 to 44	59.1	1	4.5	0	19	3
9/19/2024	07:00	0	12	66	44	25	9	17	10	1	0	0	0	0	0	0	0	0	0	184	28.4	39	19 to 29	64.1	6	3.3	0	175	9
9/19/2024	08:00	1	12	23	25	17	14	14	7	0	0	0	0	0	0	0	0	0	0	113	29.7	39.8	20 to 30	48.7	3	2.7	0	104	9
9/19/2024	09:00	2	8	27	26	12	23	28	19	0	0	0	0	0	0	0	0	0	0	145	32.9	43.4	35 to 45	43.4	7	4.8	0	129	16
9/19/2024	10:00	1	6	24	30	23	32	41	8	1	0	0	0	0	0	0	0	0	0	166	33.3	41.7	33 to 43	46.4	8	4.8	0	144	22
9/19/2024	11:00	2	9	11	29	22	27	48	23	2	0	0	0	0	0	0	0	0	0	173	35.2	43.9	35 to 45	49.1	15	8.7	0	155	18
9/19/2024	12:00	2	8	19	28	13	31	52	14	0	0	0	0	0	0	0	0	0	0	167	34.4	43	35 to 45	53.3	8	4.8	0	156	11
9/19/2024	13:00	0	4	16	46	31	26	48	20	5	0	0	0	0	0	0	0	0	0	196	34.8	43.6	23 to 33	42.9	19	9.7	0	175	21
9/19/2024	14:00	1	14	58	68	38	27	39	14	1	0	0	0	0	0	0	0	0	0	260	30.6	40.6	20 to 30	51.9	8	3.1	0	241	19
9/19/2024	15:00	2	19	30	47	47	50	58	15	3	0	0	0	0	0	0	0	0	0	271	33	41.9	34 to 44	43.9	15	5.5	0	253	18
9/19/2024	16:00	1	7	29	43	62	38	77	24	1	0	0	0	0	0	0	0	0	0	282	34.5	42.5	33 to 43	45.7	16	5.7	0	262	20
9/19/2024	17:00	1	6	21	42	58	52	84	37	1	0	0	0	0	0	0	0	0	0	302	35.7	43.4	36 to 46	50.7	25	8.3	0	291	11
9/19/2024	18:00	0	7	20	49	38	48	57	31	1	1	0	0	0	0	0	0	0	0	252	35	43.2	36 to 46	50.4	20	7.9	0	243	9
9/19/2024	19:00	0	3	16	33	27	37	52	13	2	2	0	0	0	0	0	0	0	0	185	35.3	42.6	35 to 45	51.4	11	5.9	0	180	5
9/19/2024	20:00	0	2	6	18	19	16	43	12	2	1	0	0	0	0	0	0	0	0	119	36.7	43.6	36 to 46	52.9	12	10.1	0	114	5
9/19/2024	21:00	0	3	15	13	11	14	19	2	1	0	0	0	0	0	0	0	0	0	78	32.6	41.6	33 to 43	46.2	2	2.6	0	77	1
9/19/2024	22:00	0	2	16	7	6	6	12	1	0	0	0	0	0	0	0	0	0	0	50	30.7	40.3	21 to 31	54.0	1	2.0	0	49	1
9/19/2024	23:00	0	0	3	6	4	3	3	3	0	0	0	0	0	0	0	0	0	0	22	33.2	44	24 to 34	50.0	*	*	0	21	1
24 Hr Summary		13	123	412	568	460	461	708	257	23	4	0	0	0	0	0	0	0	0	3029	33.6	43	36 to 46	42.2	181	6.0	0	2826	203

Outgoing Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG		
9/20/2024	00:00	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	3	33	43	20 to 30	66.7	*	*	0	3	0		
9/20/2024	01:00	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	25.2	28	19 to 29	100.0	*	*	0	5	0		
9/20/2024	02:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	1	0		
9/20/2024	03:00	0	1	1	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	5	29.8	38	15 to 25	60.0	*	*	0	4	1		
9/20/2024	04:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	23.5	25	15 to 25	100.0	*	*	0	1	1		
9/20/2024	05:00	0	0	1	0	0	1	3	1	0	0	0	0	0	0	0	0	0	0	6	38.2	43	35 to 45	83.3	*	*	0	6	0		
9/20/2024	06:00	0	0	3	3	4	2	5	2	0	0	0	0	0	0	0	0	0	0	19	34.2	42.5	30 to 40	47.4	2	10.5	0	17	2		
9/20/2024	07:00	0	12	64	38	24	21	17	5	0	0	0	0	0	0	0	0	0	0	181	28.5	37.4	19 to 29	59.7	4	2.2	0	170	11		
9/20/2024	08:00	3	15	22	17	13	19	21	7	1	0	0	0	0	0	0	0	0	0	118	30.7	42	19 to 29	39.0	4	3.4	0	107	11		
9/20/2024	09:00	0	5	21	26	22	27	26	8	1	0	0	0	0	0	0	0	0	0	136	33	42	33 to 43	43.4	5	3.7	0	115	21		
9/20/2024	10:00	0	4	13	15	6	14	10	2	1	0	0	0	0	0	0	0	0	0	65	31.2	40	19 to 29	49.2	1	1.5	0	59	6		
9/20/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		3	37	129	104	70	86	84	25	3	0	0	0	0	0	0	0	0	0	541	30.8	41	19 to 29	46.6	16	3.0	0	488	53		

Outgoing: Average Hourly Volume for Week of 9/16/2024

Average Counts By Hour (9/16/2024) ● Average Counts By Hour (9/16/2024)



Old Glenn Hwy South South 9-16 to-9-20 time correct
Average Weekly Volumes

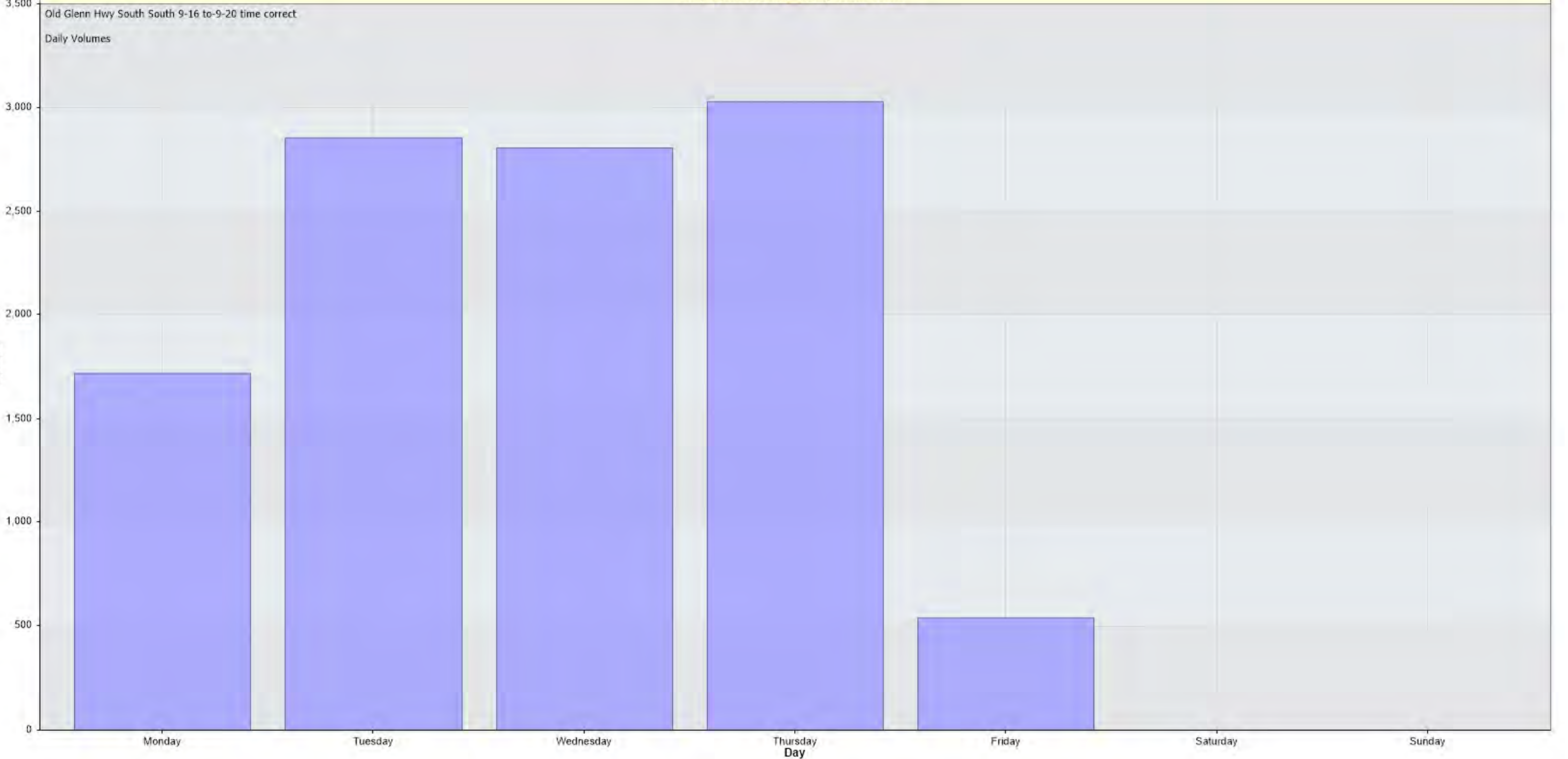
Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/16/2024
Average Hourly WEEKDAY Speeds By Hour (9/16/2024) — Average Hourly WEEKDAY Speeds By Hour (9/16/2024)

Old Glenn Hwy South South 9-16 to 9-20 time correct
Average Weekly Speeds



Outgoing:Daily Volume for Week of 9/16/2024

Daily Vehicle Counts



For Project: Old Glenn Hwy North North 9-16 to 9-20 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:57:57 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/16/2024 12:00:00 PM through 9/20/2024 9:59:59 AM

85th Percentile Speed 46 MPH

85th Percentile Vehicles 3832

Max Speed 65 MPH on 9/18/2024 7:23:38 PM

Total Vehicles 4508

AADT: 1150

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	901	901
AM Peak 11:00 AM	79	79
PM Peak 5:00 PM	93	93

Speed

Speed Limit: 45

85th Percentile Speed: 46

50th Percentile Speed: 40

10 MPH Pace Interval: 37.0 MPH to 47.0 MPH

Average Speed: 38.49

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	117	203	236	256	39	N/A	N/A
% over limit	17.7	17.7	20.2	20.5	13.6	N/A	N/A
Avg Speeder	48.1	48.2	48.2	48.4	49.2	N/A	N/A
Avg Speed	38.6	38.3	38.5	39.0	37.0	N/A	N/A

Class Counts

	Number	%
VEH_SM	56	1.2
VEH_MED	4203	93.2
VEH_LG	249	5.5
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/16/2024 1:00:00 PM	45.0	38	45	50	48.4	15.6%	39.5
9/16/2024 2:00:00 PM	47.0	70	82	52	47.8	23.2%	40.2
9/16/2024 3:00:00 PM	39.0	58	68	46	46.0	1.5%	28.7
9/16/2024 4:00:00 PM	47.0	76	90	52	48.1	18.9%	39.8
9/16/2024 5:00:00 PM	45.0	80	94	52	47.9	12.8%	39.4
9/16/2024 6:00:00 PM	48.0	75	88	57	49.0	23.9%	40.0
9/16/2024 7:00:00 PM	46.0	58	68	52	47.9	20.6%	39.4
9/16/2024 8:00:00 PM	48.0	46	54	54	48.1	31.5%	41.7
9/16/2024 9:00:00 PM	45.0	28	33	50	48.0	12.1%	38.9
9/16/2024 10:00:00 PM	42.0	24	28	50	47.7	10.7%	37.0
9/16/2024 11:00:00 PM	44.0	8	10	47	47.0	20.0%	41.0
9/17/2024 12:00:00 AM	33.0	1	1	33	0.0	0.0%	33.0
9/17/2024 1:00:00 AM	41.0	4	5	42	0.0	0.0%	36.8
9/17/2024 2:00:00 AM	42.0	3	4	43	0.0	0.0%	39.5
9/17/2024 3:00:00 AM	**No Data**						
9/17/2024 4:00:00 AM	58.0	3	3	58	58.0	33.3%	47.0
9/17/2024 5:00:00 AM	32.0	1	1	32	0.0	0.0%	32.0
9/17/2024 6:00:00 AM	46.0	11	13	49	46.8	30.8%	43.0
9/17/2024 7:00:00 AM	46.0	52	61	54	49.1	19.7%	39.9
9/17/2024 8:00:00 AM	44.0	68	80	52	48.0	12.5%	31.8
9/17/2024 9:00:00 AM	45.0	60	70	53	48.1	12.9%	35.9
9/17/2024 10:00:00 AM	46.0	64	75	50	47.3	16.0%	38.8
9/17/2024 11:00:00 AM	46.0	67	79	53	48.3	16.5%	40.0
9/17/2024 12:00:00 PM	45.0	71	84	63	50.7	15.5%	39.7
9/17/2024 1:00:00 PM	46.0	60	70	55	47.9	22.9%	40.1
9/17/2024 2:00:00 PM	46.0	81	95	52	47.9	20.0%	39.0
9/17/2024 3:00:00 PM	42.0	59	69	49	47.0	8.7%	29.2
9/17/2024 4:00:00 PM	46.0	77	91	55	48.3	16.5%	38.5
9/17/2024 5:00:00 PM	47.0	78	92	57	49.0	17.4%	39.5
9/17/2024 6:00:00 PM	46.0	82	97	55	47.6	28.9%	41.3
9/17/2024 7:00:00 PM	47.0	41	48	52	48.4	18.8%	39.9
9/17/2024 8:00:00 PM	45.0	42	50	50	47.5	16.0%	39.2
9/17/2024 9:00:00 PM	46.0	24	28	52	47.5	21.4%	39.9
9/17/2024 10:00:00 PM	45.0	16	19	48	46.7	15.8%	40.1
9/17/2024 11:00:00 PM	44.0	7	8	52	52.0	12.5%	41.9
9/18/2024 12:00:00 AM	46.0	5	6	49	47.5	33.3%	37.8
9/18/2024 1:00:00 AM	50.0	2	2	50	50.0	50.0%	40.0
9/18/2024 2:00:00 AM	**No Data**						
9/18/2024 3:00:00 AM	42.0	3	4	45	0.0	0.0%	42.5
9/18/2024 4:00:00 AM	60.0	1	1	60	60.0	100.0%	60.0
9/18/2024 5:00:00 AM	40.0	4	5	43	0.0	0.0%	36.4
9/18/2024 6:00:00 AM	47.0	12	14	53	48.5	28.6%	41.9
9/18/2024 7:00:00 AM	47.0	53	62	52	48.6	19.4%	39.1
9/18/2024 8:00:00 AM	44.0	60	70	51	47.4	7.1%	33.1
9/18/2024 9:00:00 AM	45.0	68	80	52	47.2	15.0%	36.2
9/18/2024 10:00:00 AM	46.0	64	75	55	47.2	25.3%	38.8
9/18/2024 11:00:00 AM	48.0	54	63	56	48.8	30.2%	41.5

Incoming Summary
Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

9/18/2024 12:00:00 PM	46.0	61	72	51	47.9	18.1%	38.6
9/18/2024 1:00:00 PM	46.0	63	74	50	47.8	17.6%	39.4
9/18/2024 2:00:00 PM	46.0	78	92	51	47.7	19.6%	39.2
9/18/2024 3:00:00 PM	40.0	71	84	47	47.0	3.6%	28.0
9/18/2024 4:00:00 PM	47.0	71	83	55	49.1	20.5%	39.4
9/18/2024 5:00:00 PM	48.0	78	92	55	48.7	26.1%	41.0
9/18/2024 6:00:00 PM	47.0	76	90	55	48.3	25.6%	40.4
9/18/2024 7:00:00 PM	48.0	56	66	53	48.4	33.3%	42.4
9/18/2024 8:00:00 PM	47.0	50	59	65	49.1	28.8%	41.3
9/18/2024 9:00:00 PM	46.0	31	36	49	47.0	16.7%	38.0
9/18/2024 10:00:00 PM	43.0	17	20	50	48.0	10.0%	41.3
9/18/2024 11:00:00 PM	46.0	12	14	46	46.0	21.4%	38.1
9/19/2024 12:00:00 AM	46.0	8	9	46	46.0	22.2%	37.6
9/19/2024 1:00:00 AM	47.0	5	6	49	48.0	33.3%	39.0
9/19/2024 2:00:00 AM	46.0	3	3	46	46.0	33.3%	27.3
9/19/2024 3:00:00 AM	55.0	2	2	55	55.0	50.0%	45.5
9/19/2024 4:00:00 AM	39.0	3	4	50	50.0	25.0%	38.8
9/19/2024 5:00:00 AM	36.0	3	4	45	0.0	0.0%	35.8
9/19/2024 6:00:00 AM	46.0	13	15	57	50.0	20.0%	43.2
9/19/2024 7:00:00 AM	47.0	44	52	52	47.9	26.4%	40.9
9/19/2024 8:00:00 AM	43.0	53	62	52	48.5	6.5%	32.2
9/19/2024 9:00:00 AM	46.0	66	78	50	47.2	20.5%	36.9
9/19/2024 10:00:00 AM	46.0	71	84	53	48.1	19.0%	39.8
9/19/2024 11:00:00 AM	46.0	56	66	56	49.4	21.2%	40.3
9/19/2024 12:00:00 PM	46.0	69	81	54	48.0	19.8%	40.4
9/19/2024 1:00:00 PM	47.0	84	99	56	48.6	32.3%	41.7
9/19/2024 2:00:00 PM	47.0	72	85	55	48.3	24.7%	41.1
9/19/2024 3:00:00 PM	40.0	67	79	56	48.7	7.6%	29.6
9/19/2024 4:00:00 PM	44.0	86	101	53	48.3	8.8%	37.6
9/19/2024 5:00:00 PM	48.0	73	86	54	48.7	24.4%	41.2
9/19/2024 6:00:00 PM	48.0	85	100	56	48.1	37.0%	42.1
9/19/2024 7:00:00 PM	47.0	85	100	55	48.3	22.0%	40.5
9/19/2024 8:00:00 PM	46.0	50	59	58	49.6	16.9%	38.8
9/19/2024 9:00:00 PM	46.0	33	39	52	47.9	17.9%	39.7
9/19/2024 10:00:00 PM	43.0	21	25	50	50.0	4.0%	36.8
9/19/2024 11:00:00 PM	43.0	10	12	48	48.0	8.3%	36.5
9/20/2024 12:00:00 AM	47.0	3	4	49	48.0	50.0%	42.0
9/20/2024 1:00:00 AM	35.0	4	5	35	0.0	0.0%	33.6
9/20/2024 2:00:00 AM	37.0	2	2	37	0.0	0.0%	37.0
9/20/2024 3:00:00 AM	41.0	1	1	41	0.0	0.0%	41.0
9/20/2024 4:00:00 AM	53.0	1	1	53	53.0	100.0%	53.0
9/20/2024 5:00:00 AM	44.0	3	4	47	47.0	25.0%	42.3
9/20/2024 6:00:00 AM	44.0	12	14	47	47.0	7.1%	41.1
9/20/2024 7:00:00 AM	46.0	48	56	57	49.8	16.1%	39.1
9/20/2024 8:00:00 AM	45.0	61	72	53	49.7	13.9%	33.0
9/20/2024 9:00:00 AM	45.0	68	80	52	48.9	13.8%	37.2
9/20/2024 10:00:00 AM	45.0	43	51	50	48.2	11.8%	38.6

Incoming Summary

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/17/2024 12:00:00 AM	46.0	562	661	57	48.1	17.7%	38.6
9/18/2024 12:00:00 AM	46.0	976	1148	63	48.2	17.7%	38.3
9/19/2024 12:00:00 AM	46.0	992	1167	65	48.2	20.2%	38.5
9/20/2024 12:00:00 AM	46.0	1059	1246	58	48.4	20.5%	39.0
9/20/2024 9:59:59 AM	45.0	243	286	57	49.2	13.6%	37.0

Incoming Weekly Counts

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	5	2	6	5	*	*	4.5	0	43.12
1 - 2	*	4	0	3	2	*	*	2.25	0	41.67
2 - 3	*	0	4	2	1	*	*	1.75	0	46
3 - 4	*	3	1	4	1	*	*	2.25	0	52.5
4 - 5	*	1	5	4	4	*	*	3.5	0	38
5 - 6	*	13	14	15	14	*	*	14	0	45.62
6 - 7	*	61	62	52	56	*	*	57.75	0	46.18
7 - 8	*	80	70	62	72	*	*	71	0	43.58
8 - 9	*	70	80	78	80	*	*	77	0	44.97
9 - 10	*	75	75	84	51	*	*	71.25	0	45.35
10 - 11	*	79	63	66	*	*	*	69.33	0	46.27
11 - 12	*	84	72	81	*	*	*	79	0	45.47
12 - 13	45	70	74	99	*	*	*	72	0	45.9
13 - 14	82	95	92	85	*	*	*	88.5	0	46.18
14 - 15	68	69	84	79	*	*	*	75	0	40.25
15 - 16	90	91	83	101	*	*	*	91.25	0	45.42
16 - 17	94	92	92	86	*	*	*	91	0	46.4
17 - 18	88	97	90	100	*	*	*	93.75	0	46.7
18 - 19	68	48	66	100	*	*	*	70.5	0	46.65
19 - 20	54	50	59	59	*	*	*	55.5	0	46.08
20 - 21	33	28	36	39	*	*	*	34	0	45.25
21 - 22	28	19	20	25	*	*	*	23	0	43.18
22 - 23	10	8	14	12	*	*	*	11	0	44.08
23 - 24	1	6	9	4	*	*	*	5	0	42.88
Totals	661	1148	1167	1246	286	0	0			
% of Total	12.33%	21.42%	21.78%	23.25%	5.34%	0%	0%			

Incoming Monthly Counts
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	5	2	6	5	*	*	4.5	0	43.12
1 - 2	*	4	0	3	2	*	*	2.25	0	41.67
2 - 3	*	0	4	2	1	*	*	1.75	0	46
3 - 4	*	3	1	4	1	*	*	2.25	0	52.5
4 - 5	*	1	5	4	4	*	*	3.5	0	38
5 - 6	*	13	14	15	14	*	*	14	0	45.62
6 - 7	*	61	62	52	56	*	*	57.75	0	46.18
7 - 8	*	80	70	62	72	*	*	71	0	43.58
8 - 9	*	70	80	78	80	*	*	77	0	44.97
9 - 10	*	75	75	84	51	*	*	71.25	0	45.35
10 - 11	*	79	63	66	*	*	*	69.33	0	46.27
11 - 12	*	84	72	81	*	*	*	79	0	45.47
12 - 13	45	70	74	99	*	*	*	72	0	45.9
13 - 14	82	95	92	85	*	*	*	88.5	0	46.18
14 - 15	68	69	84	79	*	*	*	75	0	40.25
15 - 16	90	91	83	101	*	*	*	91.25	0	45.42
16 - 17	94	92	92	86	*	*	*	91	0	46.4
17 - 18	88	97	90	100	*	*	*	93.75	0	46.7
18 - 19	68	48	66	100	*	*	*	70.5	0	46.65
19 - 20	54	50	59	59	*	*	*	55.5	0	46.08
20 - 21	33	28	36	39	*	*	*	34	0	45.25
21 - 22	28	19	20	25	*	*	*	23	0	43.18
22 - 23	10	8	14	12	*	*	*	11	0	44.08
23 - 24	1	6	9	4	*	*	*	5	0	42.88
Totals	661	1148	1167	1246	286	0	0			
% of Total	14.66%	25.47%	25.89%	27.64%	6.34%	0%	0%			

Incoming Weekly Speeds

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	36.8	40	39	33.6	*	*	37	*	43.12
1 - 2	*	39.5	0	27.33	37	*	*	34.89	*	41.67
2 - 3	*	0	42.5	45.5	41	*	*	43.14	*	46
3 - 4	*	47	60	38.75	53	*	*	45.44	*	52.5
4 - 5	*	32	36.4	35.75	42.25	*	*	37.57	*	38
5 - 6	*	43	41.86	43.2	41.07	*	*	42.29	*	45.62
6 - 7	*	39.87	39.08	40.88	39.09	*	*	39.7	*	46.18
7 - 8	*	31.79	33.09	32.23	32.97	*	*	32.5	*	43.58
8 - 9	*	35.93	36.16	36.91	37.19	*	*	36.56	*	44.97
9 - 10	*	38.81	38.83	39.82	38.57	*	*	39.07	*	45.35
10 - 11	*	39.96	41.49	40.35	*	*	*	40.55	*	46.27
11 - 12	*	39.69	38.57	40.36	*	*	*	39.58	*	45.47
12 - 13	39.67	40.13	39.42	41.66	*	*	*	40.4	*	45.9
13 - 14	40.17	39.01	39.22	41.09	*	*	*	39.83	*	46.18
14 - 15	28.69	29.19	28.02	29.59	*	*	*	28.86	*	40.25
15 - 16	39.83	38.45	39.4	37.58	*	*	*	38.77	*	45.42
16 - 17	39.45	39.5	41	41.19	*	*	*	40.26	*	46.4
17 - 18	40.02	41.28	40.41	42.1	*	*	*	40.99	*	46.7
18 - 19	39.41	39.88	42.44	40.48	*	*	*	40.58	*	46.65
19 - 20	41.69	39.24	41.29	38.85	*	*	*	40.27	*	46.08
20 - 21	38.94	39.93	37.97	39.74	*	*	*	39.12	*	45.25
21 - 22	36.96	40.05	41.25	36.76	*	*	*	38.48	*	43.18
22 - 23	41	41.88	38.14	36.5	*	*	*	39.02	*	44.08
23 - 24	33	37.83	37.56	42	*	*	*	38.3	*	42.88
Totals	38.6	38.3	38.4	39	37	0	0			
% of Total	20.18%	20.02%	20.07%	20.39%	19.34%	0%	0%			

Incoming Monthly Speeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	36.8	40	39	33.6	*	*	37	*	43.12
1 - 2	*	39.5	0	27.33	37	*	*	34.89	*	41.67
2 - 3	*	0	42.5	45.5	41	*	*	43.14	*	46
3 - 4	*	47	60	38.75	53	*	*	45.44	*	52.5
4 - 5	*	32	36.4	35.75	42.25	*	*	37.57	*	38
5 - 6	*	43	41.86	43.2	41.07	*	*	42.29	*	45.62
6 - 7	*	39.87	39.08	40.88	39.09	*	*	39.7	*	46.18
7 - 8	*	31.79	33.09	32.23	32.97	*	*	32.5	*	43.58
8 - 9	*	35.93	36.16	36.91	37.19	*	*	36.56	*	44.97
9 - 10	*	38.81	38.83	39.82	38.57	*	*	39.07	*	45.35
10 - 11	*	39.96	41.49	40.35	*	*	*	40.55	*	46.27
11 - 12	*	39.69	38.57	40.36	*	*	*	39.58	*	45.47
12 - 13	39.67	40.13	39.42	41.66	*	*	*	40.4	*	45.9
13 - 14	40.17	39.01	39.22	41.09	*	*	*	39.83	*	46.18
14 - 15	28.69	29.19	28.02	29.59	*	*	*	28.86	*	40.25
15 - 16	39.83	38.45	39.4	37.58	*	*	*	38.77	*	45.42
16 - 17	39.45	39.5	41	41.19	*	*	*	40.26	*	46.4
17 - 18	40.02	41.28	40.41	42.1	*	*	*	40.99	*	46.7
18 - 19	39.41	39.88	42.44	40.48	*	*	*	40.58	*	46.65
19 - 20	41.69	39.24	41.29	38.85	*	*	*	40.27	*	46.08
20 - 21	38.94	39.93	37.97	39.74	*	*	*	39.12	*	45.25
21 - 22	36.96	40.05	41.25	36.76	*	*	*	38.48	*	43.18
22 - 23	41	41.88	38.14	36.5	*	*	*	39.02	*	44.08
23 - 24	33	37.83	37.56	42	*	*	*	38.3	*	42.88
Totals	38.6	38.3	38.4	39	37	0	0			
% of Total	20.18%	20.02%	20.07%	20.39%	19.34%	0%	0%			

Incoming Weekly EightyFifthSpeeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	41	50	47	34.5	*	*	43.12	0	43.12
1 - 2	*	42	0	46	37	*	*	41.67	0	41.67
2 - 3	*	0	42	55	41	*	*	46	0	46
3 - 4	*	58	60	39	53	*	*	52.5	0	52.5
4 - 5	*	32	40	36	44	*	*	38	0	38
5 - 6	*	45.7	47	46	43.8	*	*	45.62	0	45.62
6 - 7	*	45.8	46.7	46.2	46	*	*	46.18	0	46.18
7 - 8	*	44	43.4	42.4	44.5	*	*	43.58	0	43.58
8 - 9	*	44.5	45	45.7	44.7	*	*	44.97	0	44.97
9 - 10	*	45.2	45.8	45.8	44.6	*	*	45.35	0	45.35
10 - 11	*	45.3	47.5	46	*	*	*	46.27	0	46.27
11 - 12	*	45	45.7	45.7	*	*	*	45.47	0	45.47
12 - 13	45	45.9	45.7	47	*	*	*	45.9	0	45.9
13 - 14	46.4	46	45.8	46.5	*	*	*	46.18	0	46.18
14 - 15	39	42	40	40	*	*	*	40.25	0	40.25
15 - 16	46.2	45.2	47	43.3	*	*	*	45.42	0	45.42
16 - 17	44.6	46.2	47.5	47.3	*	*	*	46.4	0	46.4
17 - 18	47.2	45.9	46.4	47.3	*	*	*	46.7	0	46.7
18 - 19	46	46.5	47.7	46.4	*	*	*	46.65	0	46.65
19 - 20	47.2	45	46.8	45.3	*	*	*	46.08	0	46.08
20 - 21	44.5	45.7	45.5	45.3	*	*	*	45.25	0	45.25
21 - 22	42	45	43	42.7	*	*	*	43.18	0	43.18
22 - 23	44	44	45.3	43	*	*	*	44.08	0	44.08
23 - 24	33	46	45.5	47	*	*	*	42.88	0	42.88
Totals	525.1	1031.9	1059.3	1081.9	433.1	0	0			
% of Total	12.71%	24.98%	25.64%	26.19%	10.48%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	41	50	47	34.5	*	*	43.12	0	43.12
1 - 2	*	42	0	46	37	*	*	41.67	0	41.67
2 - 3	*	0	42	55	41	*	*	46	0	46
3 - 4	*	58	60	39	53	*	*	52.5	0	52.5
4 - 5	*	32	40	36	44	*	*	38	0	38
5 - 6	*	45.7	47	46	43.8	*	*	45.62	0	45.62
6 - 7	*	45.8	46.7	46.2	46	*	*	46.18	0	46.18
7 - 8	*	44	43.4	42.4	44.5	*	*	43.58	0	43.58
8 - 9	*	44.5	45	45.7	44.7	*	*	44.97	0	44.97
9 - 10	*	45.2	45.8	45.8	44.6	*	*	45.35	0	45.35
10 - 11	*	45.3	47.5	46	*	*	*	46.27	0	46.27
11 - 12	*	45	45.7	45.7	*	*	*	45.47	0	45.47
12 - 13	45	45.9	45.7	47	*	*	*	45.9	0	45.9
13 - 14	46.4	46	45.8	46.5	*	*	*	46.18	0	46.18
14 - 15	39	42	40	40	*	*	*	40.25	0	40.25
15 - 16	46.2	45.2	47	43.3	*	*	*	45.42	0	45.42
16 - 17	44.6	46.2	47.5	47.3	*	*	*	46.4	0	46.4
17 - 18	47.2	45.9	46.4	47.3	*	*	*	46.7	0	46.7
18 - 19	46	46.5	47.7	46.4	*	*	*	46.65	0	46.65
19 - 20	47.2	45	46.8	45.3	*	*	*	46.08	0	46.08
20 - 21	44.5	45.7	45.5	45.3	*	*	*	45.25	0	45.25
21 - 22	42	45	43	42.7	*	*	*	43.18	0	43.18
22 - 23	44	44	45.3	43	*	*	*	44.08	0	44.08
23 - 24	33	46	45.5	47	*	*	*	42.88	0	42.88

Summary of Violators

Old Glenn Hwy North North 9-16 to 9-20 time correct from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	18	37.0	3	48.7
01:00:00	9	34.9	1	46.0
02:00:00	7	43.1	1	55.0
03:00:00	9	45.4	4	55.3
04:00:00	14	37.6	1	47.0
05:00:00	56	42.3	12	48.2
06:00:00	231	39.7	46	48.8
07:00:00	284	32.5	29	48.6
08:00:00	308	36.6	48	47.8
09:00:00	285	39.1	53	47.6
10:00:00	208	40.5	46	48.8
11:00:00	237	39.6	42	48.8
12:00:00	288	40.4	68	48.3
13:00:00	354	39.8	77	47.9
14:00:00	300	28.9	16	47.6
15:00:00	365	38.8	58	48.5
16:00:00	364	40.3	73	48.6
17:00:00	375	41.0	109	48.2
18:00:00	282	40.6	67	48.3
19:00:00	222	40.3	52	48.6
20:00:00	136	39.1	23	47.6
21:00:00	92	38.5	9	47.7
22:00:00	44	39.0	7	47.4
23:00:00	20	38.3	6	47.2

Incoming Histogram

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	2	0	0	1	2	16	12	10	2	0	0	0	0	0	0	0	0	0	45	39.7	45	35 to 45	73.3	7	15.6	0	45	0	
9/16/2024	13:00	0	1	0	2	13	21	18	23	4	0	0	0	0	0	0	0	0	0	82	40.2	46.4	35 to 45	57.3	19	23.2	2	72	8	
9/16/2024	14:00	1	4	25	11	8	9	9	1	0	0	0	0	0	0	0	0	0	0	68	28.7	39	19 to 29	55.9	1	1.5	3	60	5	
9/16/2024	15:00	1	0	2	3	14	13	35	19	3	0	0	0	0	0	0	0	0	0	90	39.8	46.2	38 to 48	64.4	17	18.9	2	82	6	
9/16/2024	16:00	0	0	1	5	11	26	34	15	2	0	0	0	0	0	0	0	0	0	94	39.4	44.6	34 to 44	70.2	12	12.8	2	89	3	
9/16/2024	17:00	0	0	0	4	15	25	19	17	7	1	0	0	0	0	0	0	0	0	88	40	47.2	36 to 46	56.8	21	23.9	1	84	3	
9/16/2024	18:00	1	2	1	1	8	18	17	16	4	0	0	0	0	0	0	0	0	0	68	39.4	46	37 to 47	64.7	14	20.6	1	65	2	
9/16/2024	19:00	0	0	0	2	4	11	19	15	3	0	0	0	0	0	0	0	0	0	54	41.7	47.2	38 to 48	64.8	17	31.5	0	52	2	
9/16/2024	20:00	0	1	0	4	1	8	13	5	1	0	0	0	0	0	0	0	0	0	33	38.9	44.5	36 to 46	72.7	4	12.1	0	32	1	
9/16/2024	21:00	1	1	0	1	4	9	9	2	1	0	0	0	0	0	0	0	0	0	28	37	42	32 to 42	71.4	3	10.7	0	27	1	
9/16/2024	22:00	0	0	0	0	0	5	3	2	0	0	0	0	0	0	0	0	0	0	10	41	44	37 to 47	90.0	2	20.0	0	10	0	
9/16/2024	23:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	33	33	23 to 33	100.0	*	*	0	1	0	
24 Hr Summary		6	9	29	34	81	161	188	125	27	1	0	0	0	0	0	0	0	0	661	38.6	46	35 to 45	58.2	117	17.7	11	619	31	

Incoming Histogram
Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/17/2024	00:00	0	0	1	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	5	36.8	41	32 to 42	80.0	*	*	0	5	0	
9/17/2024	01:00	0	0	0	0	1	0	3	0	0	0	0	0	0	0	0	0	0	0	4	39.5	42	33 to 43	100.0	*	*	0	4	0	
9/17/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/17/2024	03:00	0	0	0	0	0	0	2	0	0	1	0	0	0	0	0	0	0	0	3	47	58	33 to 43	66.7	1	33.3	0	3	0	
9/17/2024	04:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	32	32	22 to 32	100.0	*	*	0	1	0	
9/17/2024	05:00	0	0	0	0	1	1	7	4	0	0	0	0	0	0	0	0	0	0	13	43	45.7	39 to 49	92.3	4	30.8	0	12	1	
9/17/2024	06:00	1	1	1	1	7	15	17	13	5	0	0	0	0	0	0	0	0	0	61	39.9	45.8	36 to 46	63.9	12	19.7	0	60	1	
9/17/2024	07:00	2	6	24	6	7	8	15	10	2	0	0	0	0	0	0	0	0	0	80	31.8	44	17 to 27	42.5	10	12.5	0	74	6	
9/17/2024	08:00	0	4	4	10	9	13	19	10	1	0	0	0	0	0	0	0	0	0	70	35.9	44.5	38 to 48	51.4	9	12.9	0	63	7	
9/17/2024	09:00	1	1	0	5	12	15	26	14	1	0	0	0	0	0	0	0	0	0	75	38.8	45.2	36 to 46	62.7	12	16.0	1	68	6	
9/17/2024	10:00	0	1	2	2	5	23	27	15	4	0	0	0	0	0	0	0	0	0	79	40	45.3	35 to 45	70.9	13	16.5	1	70	8	
9/17/2024	11:00	1	4	0	2	9	18	26	19	2	2	1	0	0	0	0	0	0	0	84	39.7	45	35 to 45	65.5	13	15.5	2	80	2	
9/17/2024	12:00	1	2	0	2	8	12	24	18	2	1	0	0	0	0	0	0	0	0	70	40.1	45.9	37 to 47	72.9	16	22.9	1	67	2	
9/17/2024	13:00	2	2	0	3	15	23	26	20	4	0	0	0	0	0	0	0	0	0	95	39	46	37 to 47	60.0	19	20.0	1	90	4	
9/17/2024	14:00	1	6	21	14	8	5	8	6	0	0	0	0	0	0	0	0	0	0	69	29.2	42	18 to 28	53.6	6	8.7	2	61	6	
9/17/2024	15:00	2	1	3	5	13	18	28	17	3	1	0	0	0	0	0	0	0	0	91	38.5	45.2	37 to 47	62.6	15	16.5	3	79	9	
9/17/2024	16:00	1	1	0	2	18	19	29	16	5	1	0	0	0	0	0	0	0	0	92	39.5	46.2	37 to 47	59.8	16	17.4	0	85	7	
9/17/2024	17:00	2	1	0	0	9	15	36	30	3	1	0	0	0	0	0	0	0	0	97	41.3	45.9	37 to 47	73.2	28	28.9	2	90	5	
9/17/2024	18:00	1	0	0	3	5	10	19	8	2	0	0	0	0	0	0	0	0	0	48	39.9	46.5	38 to 48	64.6	9	18.8	0	46	2	
9/17/2024	19:00	0	0	0	3	6	18	13	9	1	0	0	0	0	0	0	0	0	0	50	39.2	45	36 to 46	72.0	8	16.0	1	48	1	
9/17/2024	20:00	0	0	0	3	2	6	11	5	1	0	0	0	0	0	0	0	0	0	28	39.9	45.7	36 to 46	71.4	6	21.4	0	28	0	
9/17/2024	21:00	0	0	0	1	1	5	8	4	0	0	0	0	0	0	0	0	0	0	19	40.1	45	36 to 46	84.2	3	15.8	0	18	1	
9/17/2024	22:00	0	0	0	1	0	1	5	0	1	0	0	0	0	0	0	0	0	0	8	41.9	44	34 to 44	75.0	1	12.5	0	6	2	
9/17/2024	23:00	0	0	1	0	0	3	0	2	0	0	0	0	0	0	0	0	0	0	6	37.8	46	29 to 39	50.0	2	33.3	0	6	0	
24 Hr Summary		15	30	57	63	137	229	352	220	37	7	1	0	0	0	0	0	0	0	1148	38.3	46	37 to 47	58.8	203	17.7	14	1064	70	

Incoming Histogram
Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/18/2024	00:00	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	2	40	50	20 to 30	50.0	1	50.0	0	2	0	
9/18/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/18/2024	02:00	0	0	0	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	4	42.5	42	35 to 45	100.0	*	*	0	4	0	
9/18/2024	03:00	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	60	60	50 to 60	100.0	1	100.0	0	1	0	
9/18/2024	04:00	0	0	0	1	0	2	2	0	0	0	0	0	0	0	0	0	0	0	5	36.4	40	33 to 43	80.0	*	*	0	4	1	
9/18/2024	05:00	0	0	0	1	0	3	6	3	1	0	0	0	0	0	0	0	0	0	14	41.9	47	38 to 48	71.4	4	28.6	0	14	0	
9/18/2024	06:00	1	2	1	3	7	13	22	10	3	0	0	0	0	0	0	0	0	0	62	39.1	46.7	34 to 44	59.7	12	19.4	0	58	4	
9/18/2024	07:00	1	3	11	15	7	8	18	6	1	0	0	0	0	0	0	0	0	0	70	33.1	43.4	21 to 31	42.9	5	7.1	0	66	4	
9/18/2024	08:00	0	3	11	2	9	24	16	14	1	0	0	0	0	0	0	0	0	0	80	36.2	45	33 to 43	55.0	12	15.0	0	78	2	
9/18/2024	09:00	1	2	1	5	6	18	23	18	0	1	0	0	0	0	0	0	0	0	75	38.8	45.8	37 to 47	64.0	19	25.3	0	70	5	
9/18/2024	10:00	0	0	0	2	8	13	18	17	3	2	0	0	0	0	0	0	0	0	63	41.5	47.5	38 to 48	66.7	19	30.2	0	60	3	
9/18/2024	11:00	1	0	1	4	16	16	16	15	3	0	0	0	0	0	0	0	0	0	72	38.6	45.7	33 to 43	58.3	13	18.1	1	63	8	
9/18/2024	12:00	1	1	0	2	11	19	22	17	1	0	0	0	0	0	0	0	0	0	74	39.4	45.7	35 to 45	62.2	13	17.6	0	69	5	
9/18/2024	13:00	1	2	1	3	12	20	32	18	3	0	0	0	0	0	0	0	0	0	92	39.2	45.8	36 to 46	63.0	18	19.6	1	81	10	
9/18/2024	14:00	3	9	34	7	5	11	11	4	0	0	0	0	0	0	0	0	0	0	84	28	40	16 to 26	56.0	3	3.6	3	74	7	
9/18/2024	15:00	4	1	0	2	11	17	25	16	6	1	0	0	0	0	0	0	0	0	83	39.4	47	35 to 45	57.8	17	20.5	2	76	5	
9/18/2024	16:00	1	1	0	3	7	21	30	22	6	1	0	0	0	0	0	0	0	0	92	41	47.5	36 to 46	65.2	24	26.1	1	84	7	
9/18/2024	17:00	0	2	0	4	9	23	25	22	3	2	0	0	0	0	0	0	0	0	90	40.4	46.4	37 to 47	61.1	23	25.6	1	86	3	
9/18/2024	18:00	0	0	0	4	3	13	19	21	6	0	0	0	0	0	0	0	0	0	66	42.4	47.7	38 to 48	69.7	22	33.3	1	58	7	
9/18/2024	19:00	0	0	1	2	4	15	20	11	5	0	0	1	0	0	0	0	0	0	59	41.3	46.8	37 to 47	67.8	17	28.8	0	58	1	
9/18/2024	20:00	1	0	0	4	5	11	7	8	0	0	0	0	0	0	0	0	0	0	36	38	45.5	35 to 45	55.6	6	16.7	0	36	0	
9/18/2024	21:00	0	0	0	0	1	5	12	1	1	0	0	0	0	0	0	0	0	0	20	41.3	43	34 to 44	90.0	2	10.0	0	19	1	
9/18/2024	22:00	0	0	1	2	2	0	5	4	0	0	0	0	0	0	0	0	0	0	14	38.1	45.3	36 to 46	64.3	3	21.4	0	14	0	
9/18/2024	23:00	0	1	0	0	2	2	0	4	0	0	0	0	0	0	0	0	0	0	9	37.6	45.5	36 to 46	66.7	2	22.2	0	8	1	
24 Hr Summary		15	27	62	66	126	254	332	232	44	7	1	1	0	0	0	0	0	0	1167	38.5	46	37 to 47	56.0	236	20.2	10	1083	74	

Incoming Histogram

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/19/2024	00:00	0	0	0	0	2	2	0	2	0	0	0	0	0	0	0	0	0	0	6	39	47	29 to 39	66.7	2	33.3	0	6	0
9/19/2024	01:00	1	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	3	27.3	46	2 to 12	33.3	1	33.3	0	3	0
9/19/2024	02:00	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2	45.5	55	26 to 36	50.0	1	50.0	0	2	0
9/19/2024	03:00	0	0	0	0	1	2	0	0	1	0	0	0	0	0	0	0	0	0	4	38.8	39	29 to 39	75.0	1	25.0	0	3	1
9/19/2024	04:00	0	0	0	1	0	2	0	1	0	0	0	0	0	0	0	0	0	0	4	35.8	36	26 to 36	75.0	*	*	0	4	0
9/19/2024	05:00	0	0	0	0	1	2	7	4	0	1	0	0	0	0	0	0	0	0	15	43.2	46	37 to 47	86.7	3	20.0	0	15	0
9/19/2024	06:00	0	0	0	0	7	16	13	13	3	0	0	0	0	0	0	0	0	0	52	40.9	46.2	37 to 47	67.3	13	25.0	1	49	2
9/19/2024	07:00	1	2	16	8	7	10	13	3	2	0	0	0	0	0	0	0	0	0	62	32.2	42.4	21 to 31	45.2	4	6.5	1	59	2
9/19/2024	08:00	2	2	11	5	7	6	22	21	2	0	0	0	0	0	0	0	0	0	78	36.9	45.7	37 to 47	59.0	16	20.5	1	72	5
9/19/2024	09:00	1	2	0	4	4	20	35	15	3	0	0	0	0	0	0	0	0	0	84	39.8	45.8	37 to 47	70.2	16	19.0	1	79	4
9/19/2024	10:00	1	1	0	1	10	14	23	10	5	1	0	0	0	0	0	0	0	0	66	40.3	46	34 to 44	65.2	14	21.2	4	57	5
9/19/2024	11:00	0	2	0	2	7	22	29	15	4	0	0	0	0	0	0	0	0	0	81	40.4	45.7	36 to 46	72.8	16	19.8	1	76	4
9/19/2024	12:00	0	1	1	3	8	21	27	27	10	1	0	0	0	0	0	0	0	0	99	41.7	47	37 to 47	64.6	32	32.3	1	88	10
9/19/2024	13:00	1	0	0	4	6	17	32	19	5	1	0	0	0	0	0	0	0	0	85	41.1	46.5	37 to 47	64.7	21	24.7	0	80	5
9/19/2024	14:00	1	5	28	11	11	9	7	5	1	1	0	0	0	0	0	0	0	0	79	29.6	40	21 to 31	54.4	6	7.6	3	71	5
9/19/2024	15:00	1	2	0	7	16	36	28	9	2	0	0	0	0	0	0	0	0	0	101	37.6	43.3	33 to 43	67.3	9	8.9	2	93	6
9/19/2024	16:00	1	0	0	0	10	25	22	22	6	0	0	0	0	0	0	0	0	0	86	41.2	47.3	37 to 47	64.0	21	24.4	0	85	1
9/19/2024	17:00	2	0	1	3	4	18	29	33	9	1	0	0	0	0	0	0	0	0	100	42.1	47.3	37 to 47	67.0	37	37.0	1	95	4
9/19/2024	18:00	2	1	0	2	8	25	35	22	4	1	0	0	0	0	0	0	0	0	100	40.5	46.4	37 to 47	67.0	22	22.0	1	98	1
9/19/2024	19:00	2	0	1	2	11	9	22	9	1	2	0	0	0	0	0	0	0	0	59	38.8	45.3	34 to 44	55.9	10	16.9	0	59	0
9/19/2024	20:00	1	1	0	0	6	8	9	13	1	0	0	0	0	0	0	0	0	0	39	39.7	45.3	36 to 46	66.7	7	17.9	0	38	1
9/19/2024	21:00	1	0	0	2	6	4	9	2	1	0	0	0	0	0	0	0	0	0	25	36.8	42.7	33 to 43	60.0	1	4.0	0	23	2
9/19/2024	22:00	0	0	1	2	1	2	5	1	0	0	0	0	0	0	0	0	0	0	12	36.5	43	34 to 44	58.3	1	8.3	0	12	0
9/19/2024	23:00	0	0	0	0	1	1	0	2	0	0	0	0	0	0	0	0	0	0	4	42	47	39 to 49	75.0	2	50.0	0	4	0
24 Hr Summary		18	19	60	57	134	272	367	249	60	10	0	0	0	0	0	0	0	0	1246	39	46	37 to 47	59.2	256	20.5	17	1171	58

Incoming Histogram

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG			
9/20/2024	00:00	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	5	33.6	34.5	25 to 35	100.0	*	*	0	5	0		
9/20/2024	01:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	37	37	27 to 37	100.0	*	*	0	2	0		
9/20/2024	02:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	41	41	31 to 41	100.0	*	*	0	1	0		
9/20/2024	03:00	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	53	53	43 to 53	100.0	1	100.0	0	1	0		
9/20/2024	04:00	0	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	4	42.3	44	34 to 44	75.0	1	25.0	0	4	0		
9/20/2024	05:00	0	0	0	0	1	3	9	1	0	0	0	0	0	0	0	0	0	0	0	14	41.1	43.8	34 to 44	92.9	1	7.1	0	12	2		
9/20/2024	06:00	1	1	0	2	9	16	16	7	3	1	0	0	0	0	0	0	0	0	0	56	39.1	46	35 to 45	60.7	9	16.1	1	52	3		
9/20/2024	07:00	1	2	16	14	8	7	12	7	5	0	0	0	0	0	0	0	0	0	0	72	33	44.5	20 to 30	43.1	10	13.9	1	69	2		
9/20/2024	08:00	2	1	4	8	8	18	25	9	5	0	0	0	0	0	0	0	0	0	0	80	37.2	44.7	34 to 44	57.5	11	13.8	2	73	5		
9/20/2024	09:00	1	2	0	1	9	10	17	9	2	0	0	0	0	0	0	0	0	0	0	51	38.6	44.6	35 to 45	62.7	6	11.8	0	47	4		
9/20/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		5	6	20	25	39	58	82	34	16	1	0	0	0	0	0	0	0	0	0	286	37	45	35 to 45	53.1	39	13.6	4	266	16		

Incoming: Average Hourly Volume for Week of 9/16/2024

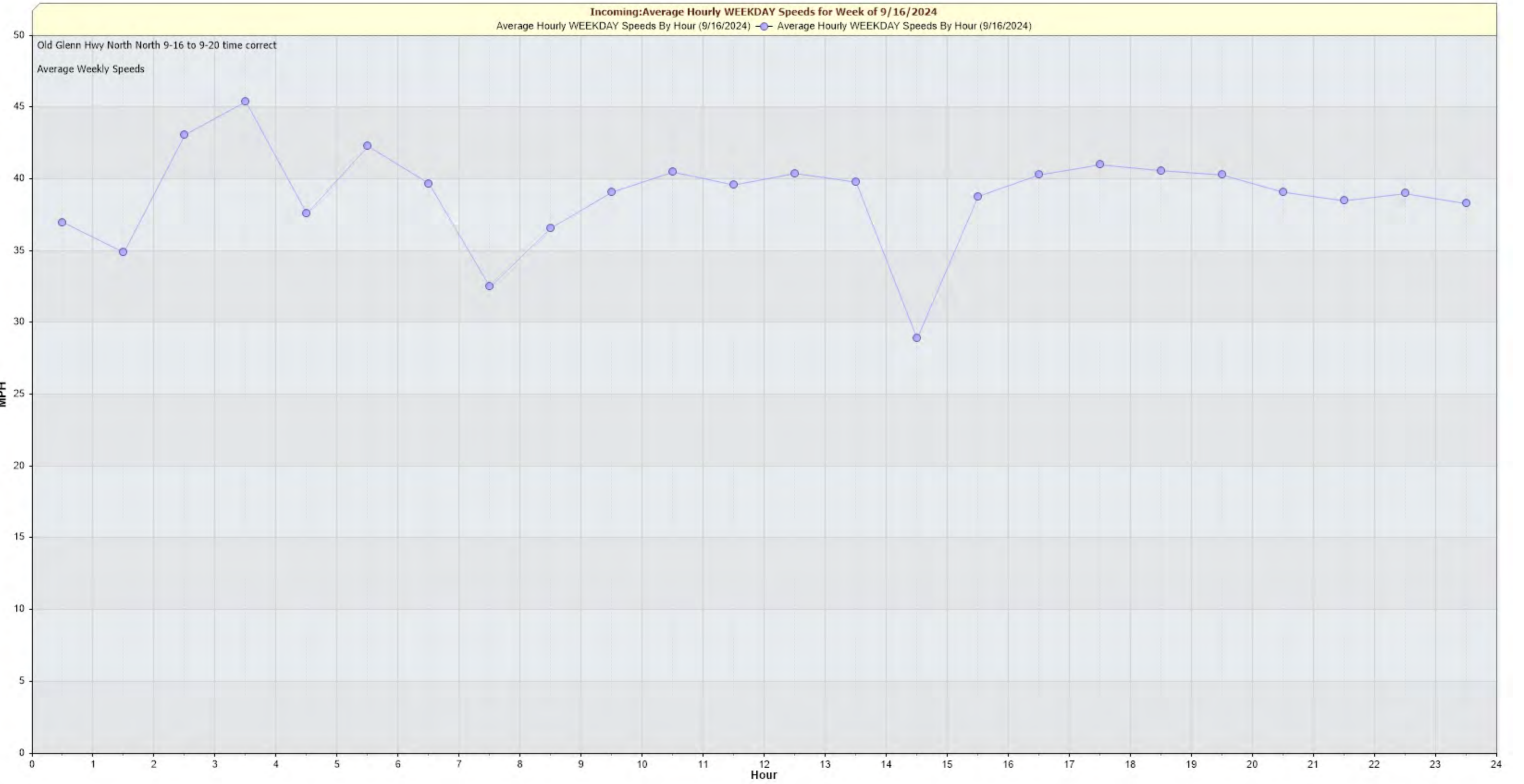
Average Counts By Hour (9/16/2024) —●— Average Counts By Hour (9/16/2024)

Old Glenn Hwy North North 9-16 to 9-20 time correct
Average Weekly Volumes



Incoming: Average Hourly WEEKDAY Speeds for Week of 9/16/2024
Average Hourly WEEKDAY Speeds By Hour (9/16/2024) — Average Hourly WEEKDAY Speeds By Hour (9/16/2024)

Old Glenn Hwy North North 9-16 to 9-20 time correct
Average Weekly Speeds

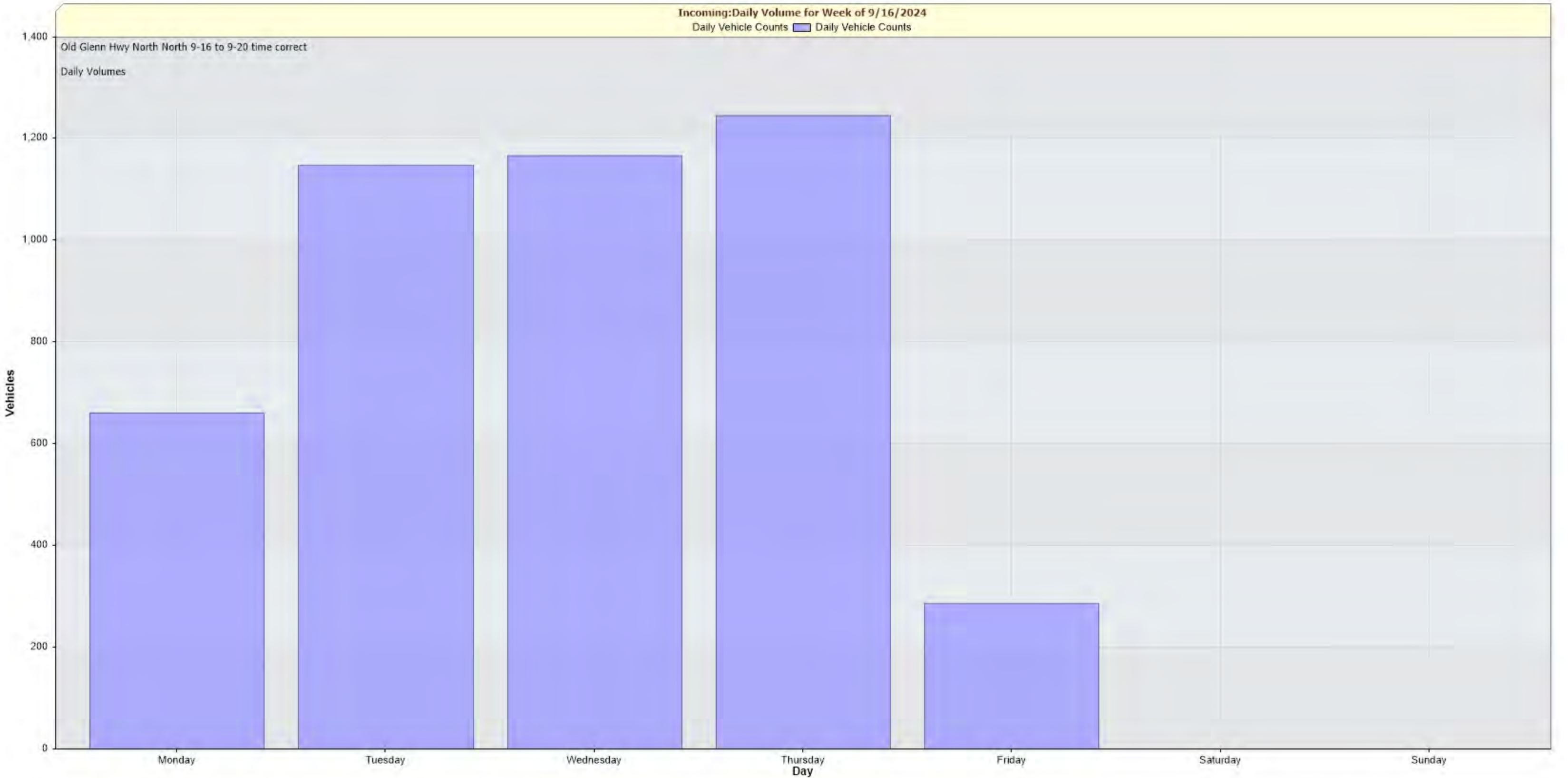


Incoming:Daily Volume for Week of 9/16/2024

Daily Vehicle Counts

Old Glenn Hwy North North 9-16 to 9-20 time correct

Daily Volumes



For Project: Old Glenn Hwy North North 9-16 to 9-20 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:57:57 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/16/2024 12:00:00 PM through 9/20/2024 9:59:59 AM

85th Percentile Speed 44 MPH

85th Percentile Vehicles 5601

Max Speed 58 MPH on 9/18/2024 8:15:43 AM

Total Vehicles 6589

AADT: 1682

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1317	1317
AM Peak	7:00 AM 125	125
PM Peak	5:00 PM 205	205

Speed

Speed Limit: 45

85th Percentile Speed: 44

50th Percentile Speed: 37

10 MPH Pace Interval: 36.0 MPH to 46.0 MPH

Average Speed: 35.35

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	72	139	133	130	8	N/A	N/A
% over limit	6.3	8.2	8.0	7.1	3.0	N/A	N/A
Avg Speeder	47.2	47.6	47.7	47.7	48.0	N/A	N/A
Avg Speed	35.4	35.7	35.6	35.4	31.0	N/A	N/A

Class Counts

	Number	%
VEH_SM	1	0
VEH_MED	6246	94.8
VEH_LG	342	5.2
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/16/2024 1:00:00 PM	41.0	48	57	44	0.0	0.0%	33.5
9/16/2024 2:00:00 PM	43.0	98	115	49	47.3	6.1%	36.1
9/16/2024 3:00:00 PM	37.0	132	155	47	46.5	1.3%	27.9
9/16/2024 4:00:00 PM	44.0	144	170	49	46.7	7.1%	35.6
9/16/2024 5:00:00 PM	44.0	144	169	56	47.5	8.9%	37.0
9/16/2024 6:00:00 PM	45.0	155	182	52	47.6	8.2%	38.5
9/16/2024 7:00:00 PM	44.0	99	117	49	46.7	8.5%	37.0
9/16/2024 8:00:00 PM	45.0	54	63	50	47.4	14.3%	38.3
9/16/2024 9:00:00 PM	42.0	38	45	45	0.0	0.0%	34.7
9/16/2024 10:00:00 PM	43.0	37	43	47	46.5	4.7%	35.0
9/16/2024 11:00:00 PM	42.0	14	17	43	0.0	0.0%	34.9
9/17/2024 12:00:00 AM	42.0	11	13	45	0.0	0.0%	32.9
9/17/2024 1:00:00 AM	45.0	6	7	54	54.0	14.3%	34.4
9/17/2024 2:00:00 AM	44.0	5	6	49	49.0	16.7%	39.7
9/17/2024 3:00:00 AM	48.0	2	2	48	48.0	50.0%	42.0
9/17/2024 4:00:00 AM	**No Data**						
9/17/2024 5:00:00 AM	44.0	1	1	44	0.0	0.0%	44.0
9/17/2024 6:00:00 AM	48.0	3	3	48	48.0	33.3%	45.0
9/17/2024 7:00:00 AM	43.0	12	14	46	46.0	6.7%	34.9
9/17/2024 8:00:00 AM	38.0	102	120	50	50.0	0.8%	27.7
9/17/2024 9:00:00 AM	43.0	48	56	51	49.0	3.6%	33.7
9/17/2024 10:00:00 AM	44.0	57	67	52	48.7	10.4%	37.1
9/17/2024 11:00:00 AM	44.0	54	64	53	47.8	9.4%	36.9
9/17/2024 12:00:00 PM	44.0	76	90	55	48.6	7.8%	35.4
9/17/2024 1:00:00 PM	45.0	90	106	50	47.5	14.2%	37.9
9/17/2024 2:00:00 PM	43.0	88	104	50	47.3	8.7%	36.1
9/17/2024 3:00:00 PM	37.0	127	149	48	47.0	1.3%	28.0
9/17/2024 4:00:00 PM	43.0	141	166	56	48.2	7.8%	36.5
9/17/2024 5:00:00 PM	43.0	157	185	50	47.5	5.4%	36.7
9/17/2024 6:00:00 PM	45.0	192	226	54	47.4	11.1%	38.3
9/17/2024 7:00:00 PM	45.0	138	162	53	46.7	14.8%	38.6
9/17/2024 8:00:00 PM	43.0	67	79	50	47.7	3.8%	38.0
9/17/2024 9:00:00 PM	43.0	38	45	49	48.7	6.7%	37.4
9/17/2024 10:00:00 PM	43.0	29	34	50	48.7	8.8%	37.6
9/17/2024 11:00:00 PM	46.0	8	10	47	46.3	40.0%	42.7
9/18/2024 12:00:00 AM	45.0	4	5	45	0.0	0.0%	38.8
9/18/2024 1:00:00 AM	35.0	5	6	36	0.0	0.0%	28.7
9/18/2024 2:00:00 AM	44.0	3	4	47	47.0	25.0%	39.3
9/18/2024 3:00:00 AM	**No Data**						
9/18/2024 4:00:00 AM	**No Data**						
						
9/18/2024 6:00:00 AM	44.0	3	3	44	0.0	0.0%	41.0
9/18/2024 7:00:00 AM	44.0	11	13	44	0.0	0.0%	37.9
9/18/2024 8:00:00 AM	34.0	100	118	48	48.0	0.8%	26.7
9/18/2024 9:00:00 AM	44.0	48	57	58	50.0	10.5%	35.0
9/18/2024 10:00:00 AM	43.0	48	57	45	0.0	0.0%	36.0
9/18/2024 11:00:00 AM	45.0	48	57	50	47.4	12.3%	38.0

Outgoing Summary
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

9/18/2024 12:00:00 PM	44.0	77	91	51	49.0	8.8%	38.0
9/18/2024 1:00:00 PM	45.0	72	85	48	46.6	10.6%	38.2
9/18/2024 2:00:00 PM	43.0	78	92	54	47.8	13.0%	36.4
9/18/2024 3:00:00 PM	39.0	139	163	51	51.0	0.6%	28.2
9/18/2024 4:00:00 PM	45.0	146	172	51	47.6	11.6%	37.8
9/18/2024 5:00:00 PM	44.0	158	186	58	47.6	7.0%	37.5
9/18/2024 6:00:00 PM	45.0	161	189	51	47.0	11.1%	37.7
9/18/2024 7:00:00 PM	45.0	104	122	50	47.3	12.3%	37.6
9/18/2024 8:00:00 PM	43.0	69	81	53	48.0	11.1%	35.9
9/18/2024 9:00:00 PM	44.0	67	79	53	48.1	8.9%	37.3
9/18/2024 10:00:00 PM	42.0	40	47	48	48.0	2.1%	35.0
9/18/2024 11:00:00 PM	45.0	20	23	49	48.0	8.7%	39.9
9/19/2024 12:00:00 AM	45.0	7	8	45	0.0	0.0%	38.0
9/19/2024 1:00:00 AM	43.0	3	3	43	0.0	0.0%	31.0
9/19/2024 2:00:00 AM	44.0	3	4	46	46.0	25.0%	34.8
9/19/2024 3:00:00 AM	39.0	2	2	39	0.0	0.0%	33.0
9/19/2024 4:00:00 AM	43.0	2	2	43	0.0	0.0%	41.5
9/19/2024 5:00:00 AM	33.0	1	1	33	0.0	0.0%	33.0
9/19/2024 6:00:00 AM	50.0	3	3	50	50.0	33.3%	43.0
9/19/2024 7:00:00 AM	41.0	6	7	46	46.0	14.3%	37.1
9/19/2024 8:00:00 AM	32.0	112	132	48	47.5	1.5%	26.5
9/19/2024 9:00:00 AM	43.0	47	55	48	47.0	3.6%	33.1
9/19/2024 10:00:00 AM	45.0	54	63	49	47.8	9.5%	36.7
9/19/2024 11:00:00 AM	42.0	58	68	50	48.7	4.4%	37.0
9/19/2024 12:00:00 PM	45.0	80	94	50	47.6	9.6%	38.7
9/19/2024 1:00:00 PM	44.0	82	97	47	46.8	4.1%	36.2
9/19/2024 2:00:00 PM	43.0	96	113	53	48.1	8.0%	36.0
9/19/2024 3:00:00 PM	40.0	151	178	49	47.0	1.7%	29.8
9/19/2024 4:00:00 PM	43.0	173	204	52	47.4	6.9%	35.6
9/19/2024 5:00:00 PM	44.0	156	183	50	47.2	7.7%	37.4
9/19/2024 6:00:00 PM	45.0	190	224	50	47.0	9.8%	37.5
9/19/2024 7:00:00 PM	45.0	128	150	58	47.4	11.3%	37.0
9/19/2024 8:00:00 PM	44.0	83	98	58	49.7	9.2%	37.3
9/19/2024 9:00:00 PM	45.0	65	76	54	48.7	13.2%	38.3
9/19/2024 10:00:00 PM	43.0	34	40	50	48.5	5.0%	35.8
9/19/2024 11:00:00 PM	41.0	19	22	49	49.0	4.5%	35.8
9/20/2024 12:00:00 AM	45.0	6	7	45	0.0	0.0%	36.9
9/20/2024 1:00:00 AM	43.0	1	1	43	0.0	0.0%	43.0
9/20/2024 2:00:00 AM	28.0	2	2	28	0.0	0.0%	25.0
9/20/2024 3:00:00 AM	**No Data**						
9/20/2024 4:00:00 AM	44.0	1	1	44	0.0	0.0%	44.0
9/20/2024 5:00:00 AM	**No Data**						
9/20/2024 6:00:00 AM	43.0	3	4	45	0.0	0.0%	41.0
9/20/2024 7:00:00 AM	40.0	8	10	49	47.5	20.0%	35.3
9/20/2024 8:00:00 AM	34.0	112	132	44	0.0	0.0%	26.9
9/20/2024 9:00:00 AM	43.0	54	64	47	46.5	3.1%	33.8
9/20/2024 10:00:00 AM	44.0	42	49	54	49.0	8.2%	36.4

Outgoing Summary
Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/17/2024 12:00:00 AM	43.0	974	1146	56	47.2	6.3%	35.4
9/18/2024 12:00:00 AM	44.0	1446	1701	56	47.6	8.2%	35.7
9/19/2024 12:00:00 AM	44.0	1405	1653	58	47.7	8.0%	35.6
9/20/2024 12:00:00 AM	44.0	1552	1826	58	47.7	7.1%	35.4
9/20/2024 9:59:59 AM	41.0	224	263	54	48.0	3.0%	31.0

Outgoing Weekly Counts

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	7	6	3	1	*	*	4.25	0	41.5
1 - 2	*	6	4	4	2	*	*	4	0	40
2 - 3	*	2	0	2	0	*	*	1	0	43.5
3 - 4	*	0	0	2	1	*	*	0.75	0	43.5
4 - 5	*	1	0	1	0	*	*	0.5	0	38.5
5 - 6	*	3	3	3	4	*	*	3.25	0	46.25
6 - 7	*	14	13	7	10	*	*	11	0	41.58
7 - 8	*	120	118	132	132	*	*	125.5	0	34.38
8 - 9	*	56	57	55	64	*	*	58	0	42.65
9 - 10	*	67	57	63	49	*	*	59	0	43.6
10 - 11	*	64	57	68	*	*	*	63	0	43.33
11 - 12	*	90	91	94	*	*	*	91.67	0	43.87
12 - 13	57	106	85	97	*	*	*	86.25	0	43.3
13 - 14	115	104	92	113	*	*	*	106	0	42.9
14 - 15	155	149	163	178	*	*	*	161.25	0	37.88
15 - 16	170	166	172	204	*	*	*	178	0	43.18
16 - 17	169	185	186	183	*	*	*	180.75	0	43.3
17 - 18	182	226	189	224	*	*	*	205.25	0	44.18
18 - 19	117	162	122	150	*	*	*	137.75	0	44.35
19 - 20	63	79	81	98	*	*	*	80.25	0	43.58
20 - 21	45	45	79	76	*	*	*	61.25	0	43.2
21 - 22	43	34	47	40	*	*	*	41	0	42.62
22 - 23	17	10	23	22	*	*	*	18	0	43.25
23 - 24	13	5	8	7	*	*	*	8.25	0	43.88
Totals	1146	1701	1653	1826	263	0	0			
% of Total	17.39%	25.82%	25.09%	27.71%	3.99%	0%	0%			

Outgoing Monthly Counts
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	7	6	3	1	*	*	4.25	0	41.5
1 - 2	*	6	4	4	2	*	*	4	0	40
2 - 3	*	2	0	2	0	*	*	1	0	43.5
3 - 4	*	0	0	2	1	*	*	0.75	0	43.5
4 - 5	*	1	0	1	0	*	*	0.5	0	38.5
5 - 6	*	3	3	3	4	*	*	3.25	0	46.25
6 - 7	*	14	13	7	10	*	*	11	0	41.58
7 - 8	*	120	118	132	132	*	*	125.5	0	34.38
8 - 9	*	56	57	55	64	*	*	58	0	42.65
9 - 10	*	67	57	63	49	*	*	59	0	43.6
10 - 11	*	64	57	68	*	*	*	63	0	43.33
11 - 12	*	90	91	94	*	*	*	91.67	0	43.87
12 - 13	57	106	85	97	*	*	*	86.25	0	43.3
13 - 14	115	104	92	113	*	*	*	106	0	42.9
14 - 15	155	149	163	178	*	*	*	161.25	0	37.88
15 - 16	170	166	172	204	*	*	*	178	0	43.18
16 - 17	169	185	186	183	*	*	*	180.75	0	43.3
17 - 18	182	226	189	224	*	*	*	205.25	0	44.18
18 - 19	117	162	122	150	*	*	*	137.75	0	44.35
19 - 20	63	79	81	98	*	*	*	80.25	0	43.58
20 - 21	45	45	79	76	*	*	*	61.25	0	43.2
21 - 22	43	34	47	40	*	*	*	41	0	42.62
22 - 23	17	10	23	22	*	*	*	18	0	43.25
23 - 24	13	5	8	7	*	*	*	8.25	0	43.88
Totals	1146	1701	1653	1826	263	0	0			
% of Total	17.39%	25.82%	25.09%	27.71%	3.99%	0%	0%			

Outgoing Weekly Speeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	34.43	28.67	31	43	*	*	32.29	*	41.5
1 - 2	*	39.67	39.25	34.75	25	*	*	36.5	*	40
2 - 3	*	42	0	33	0	*	*	37.5	*	43.5
3 - 4	*	0	0	41.5	44	*	*	42.33	*	43.5
4 - 5	*	44	0	33	0	*	*	38.5	*	38.5
5 - 6	*	45	41	43	41	*	*	42.38	*	46.25
6 - 7	*	34.86	37.92	37.14	35.3	*	*	36.23	*	41.58
7 - 8	*	27.67	26.74	26.46	26.86	*	*	26.92	*	34.38
8 - 9	*	33.66	34.96	33.11	33.84	*	*	33.9	*	42.65
9 - 10	*	37.13	36	36.67	36.37	*	*	36.58	*	43.6
10 - 11	*	36.92	38	36.99	*	*	*	37.27	*	43.33
11 - 12	*	35.43	38.03	38.69	*	*	*	37.41	*	43.87
12 - 13	33.51	37.87	38.16	36.23	*	*	*	36.76	*	43.3
13 - 14	36.12	36.08	36.36	35.96	*	*	*	36.12	*	42.9
14 - 15	27.86	28.04	28.25	29.81	*	*	*	28.54	*	37.88
15 - 16	35.58	36.52	37.81	35.64	*	*	*	36.36	*	43.18
16 - 17	37.05	36.67	37.54	37.44	*	*	*	37.18	*	43.3
17 - 18	38.55	38.28	37.7	37.51	*	*	*	38	*	44.18
18 - 19	37.02	38.58	37.61	36.97	*	*	*	37.6	*	44.35
19 - 20	38.32	38.03	35.93	37.31	*	*	*	37.33	*	43.58
20 - 21	34.69	37.38	37.27	38.32	*	*	*	37.14	*	43.2
21 - 22	35	37.56	34.98	35.75	*	*	*	35.71	*	42.62
22 - 23	34.94	42.7	39.91	35.77	*	*	*	37.86	*	43.25
23 - 24	32.92	38.8	38	36.86	*	*	*	35.88	*	43.88
Totals	35.4	35.7	35.6	35.4	31	0	0			
% of Total	20.45%	20.62%	20.57%	20.45%	17.91%	0%	0%			

Outgoing Monthly Speeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	34.43	28.67	31	43	*	*	32.29	*	41.5
1 - 2	*	39.67	39.25	34.75	25	*	*	36.5	*	40
2 - 3	*	42	0	33	0	*	*	37.5	*	43.5
3 - 4	*	0	0	41.5	44	*	*	42.33	*	43.5
4 - 5	*	44	0	33	0	*	*	38.5	*	38.5
5 - 6	*	45	41	43	41	*	*	42.38	*	46.25
6 - 7	*	34.86	37.92	37.14	35.3	*	*	36.23	*	41.58
7 - 8	*	27.67	26.74	26.46	26.86	*	*	26.92	*	34.38
8 - 9	*	33.66	34.96	33.11	33.84	*	*	33.9	*	42.65
9 - 10	*	37.13	36	36.67	36.37	*	*	36.58	*	43.6
10 - 11	*	36.92	38	36.99	*	*	*	37.27	*	43.33
11 - 12	*	35.43	38.03	38.69	*	*	*	37.41	*	43.87
12 - 13	33.51	37.87	38.16	36.23	*	*	*	36.76	*	43.3
13 - 14	36.12	36.08	36.36	35.96	*	*	*	36.12	*	42.9
14 - 15	27.86	28.04	28.25	29.81	*	*	*	28.54	*	37.88
15 - 16	35.58	36.52	37.81	35.64	*	*	*	36.36	*	43.18
16 - 17	37.05	36.67	37.54	37.44	*	*	*	37.18	*	43.3
17 - 18	38.55	38.28	37.7	37.51	*	*	*	38	*	44.18
18 - 19	37.02	38.58	37.61	36.97	*	*	*	37.6	*	44.35
19 - 20	38.32	38.03	35.93	37.31	*	*	*	37.33	*	43.58
20 - 21	34.69	37.38	37.27	38.32	*	*	*	37.14	*	43.2
21 - 22	35	37.56	34.98	35.75	*	*	*	35.71	*	42.62
22 - 23	34.94	42.7	39.91	35.77	*	*	*	37.86	*	43.25
23 - 24	32.92	38.8	38	36.86	*	*	*	35.88	*	43.88
Totals	35.4	35.7	35.6	35.4	31	0	0			
% of Total	20.45%	20.62%	20.57%	20.45%	17.91%	0%	0%			

Outgoing Weekly EightyFifthSpeeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	45	35	43	43	*	*	41.5	0	41.5
1 - 2	*	44	44	44	28	*	*	40	0	40
2 - 3	*	48	0	39	0	*	*	43.5	0	43.5
3 - 4	*	0	0	43	44	*	*	43.5	0	43.5
4 - 5	*	44	0	33	0	*	*	38.5	0	38.5
5 - 6	*	48	44	50	43	*	*	46.25	0	46.25
6 - 7	*	42	43.3	41	40	*	*	41.58	0	41.58
7 - 8	*	37.5	34	32	34	*	*	34.38	0	34.38
8 - 9	*	42.7	43.3	42.5	42.1	*	*	42.65	0	42.65
9 - 10	*	44	42.3	44.6	43.5	*	*	43.6	0	43.6
10 - 11	*	43.8	44.3	41.9	*	*	*	43.33	0	43.33
11 - 12	*	43.7	43.5	44.4	*	*	*	43.87	0	43.87
12 - 13	41	44.9	44.2	43.1	*	*	*	43.3	0	43.3
13 - 14	43	42.8	42.8	43	*	*	*	42.9	0	42.9
14 - 15	36.8	36.8	38.2	39.7	*	*	*	37.88	0	37.88
15 - 16	43.1	43	44.2	42.4	*	*	*	43.18	0	43.18
16 - 17	43.5	43	43.5	43.2	*	*	*	43.3	0	43.3
17 - 18	44.2	44.2	44.2	44.1	*	*	*	44.18	0	44.18
18 - 19	43.4	45	44.6	44.4	*	*	*	44.35	0	44.35
19 - 20	45	43	43	43.3	*	*	*	43.58	0	43.58
20 - 21	42	42.3	44	44.5	*	*	*	43.2	0	43.2
21 - 22	43	43	42	42.5	*	*	*	42.62	0	42.62
22 - 23	41.5	45.7	44.8	41	*	*	*	43.25	0	43.25
23 - 24	42	44.5	44.5	44.5	*	*	*	43.88	0	43.88
Totals	508.5	1000.9	893.7	1014.1	317.6	0	0			
% of Total	13.62%	26.8%	23.93%	27.15%	8.5%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	45	35	43	43	*	*	41.5	0	41.5
1 - 2	*	44	44	44	28	*	*	40	0	40
2 - 3	*	48	0	39	0	*	*	43.5	0	43.5
3 - 4	*	0	0	43	44	*	*	43.5	0	43.5
4 - 5	*	44	0	33	0	*	*	38.5	0	38.5
5 - 6	*	48	44	50	43	*	*	46.25	0	46.25
6 - 7	*	42	43.3	41	40	*	*	41.58	0	41.58
7 - 8	*	37.5	34	32	34	*	*	34.38	0	34.38
8 - 9	*	42.7	43.3	42.5	42.1	*	*	42.65	0	42.65
9 - 10	*	44	42.3	44.6	43.5	*	*	43.6	0	43.6
10 - 11	*	43.8	44.3	41.9	*	*	*	43.33	0	43.33
11 - 12	*	43.7	43.5	44.4	*	*	*	43.87	0	43.87
12 - 13	41	44.9	44.2	43.1	*	*	*	43.3	0	43.3
13 - 14	43	42.8	42.8	43	*	*	*	42.9	0	42.9
14 - 15	36.8	36.8	38.2	39.7	*	*	*	37.88	0	37.88
15 - 16	43.1	43	44.2	42.4	*	*	*	43.18	0	43.18
16 - 17	43.5	43	43.5	43.2	*	*	*	43.3	0	43.3
17 - 18	44.2	44.2	44.2	44.1	*	*	*	44.18	0	44.18
18 - 19	43.4	45	44.6	44.4	*	*	*	44.35	0	44.35
19 - 20	45	43	43	43.3	*	*	*	43.58	0	43.58
20 - 21	42	42.3	44	44.5	*	*	*	43.2	0	43.2
21 - 22	43	43	42	42.5	*	*	*	42.62	0	42.62
22 - 23	41.5	45.7	44.8	41	*	*	*	43.25	0	43.25
23 - 24	42	44.5	44.5	44.5	*	*	*	43.88	0	43.88

Summary of Violators

Old Glenn Hwy North North 9-16 to 9-20 time correct from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	17	32.3	1	54.0
01:00:00	16	36.5	3	47.3
02:00:00	4	37.5	1	48.0
03:00:00	3	42.3	0	0.0
04:00:00	2	38.5	0	0.0
05:00:00	13	42.4	2	49.0
06:00:00	44	36.2	4	46.8
07:00:00	502	26.9	4	48.3
08:00:00	232	33.9	12	48.8
09:00:00	236	36.6	17	48.5
10:00:00	189	37.3	16	47.8
11:00:00	275	37.4	24	48.3
12:00:00	345	36.8	28	47.1
13:00:00	424	36.1	37	47.7
14:00:00	645	28.5	8	47.4
15:00:00	712	36.4	59	47.5
16:00:00	723	37.2	52	47.4
17:00:00	821	38.0	83	47.2
18:00:00	551	37.6	66	47.0
19:00:00	321	37.3	30	48.3
20:00:00	245	37.1	20	48.5
21:00:00	164	35.7	8	48.0
22:00:00	72	37.9	7	47.1
23:00:00	33	35.9	0	0.0

Outgoing Histogram
Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/16/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	0	3	6	10	8	16	14	0	0	0	0	0	0	0	0	0	0	0	57	33.5	41	33 to 43	61.4	*	*	1	54	2
9/16/2024	13:00	0	0	8	16	28	14	40	9	0	0	0	0	0	0	0	0	0	0	115	36.1	43	32 to 42	54.8	7	6.1	0	108	7
9/16/2024	14:00	0	17	49	30	22	24	11	2	0	0	0	0	0	0	0	0	0	0	155	27.9	36.8	17 to 27	56.1	2	1.3	0	149	6
9/16/2024	15:00	0	1	4	36	43	26	43	17	0	0	0	0	0	0	0	0	0	0	170	35.6	43.1	27 to 37	52.9	12	7.1	0	166	4
9/16/2024	16:00	1	1	0	20	44	25	56	21	0	1	0	0	0	0	0	0	0	0	169	37	43.5	33 to 43	55.0	15	8.9	0	159	10
9/16/2024	17:00	0	0	3	14	33	36	66	29	1	0	0	0	0	0	0	0	0	0	182	38.5	44.2	35 to 45	64.3	15	8.2	0	175	7
9/16/2024	18:00	1	1	2	14	26	19	42	12	0	0	0	0	0	0	0	0	0	0	117	37	43.4	34 to 44	57.3	10	8.5	0	113	4
9/16/2024	19:00	0	1	2	3	8	19	19	10	1	0	0	0	0	0	0	0	0	0	63	38.3	45	38 to 48	68.3	9	14.3	0	62	1
9/16/2024	20:00	0	1	0	13	8	8	12	3	0	0	0	0	0	0	0	0	0	0	45	34.7	42	35 to 45	51.1	*	*	0	45	0
9/16/2024	21:00	2	1	1	3	11	12	10	3	0	0	0	0	0	0	0	0	0	0	43	35	43	34 to 44	60.5	2	4.7	0	42	1
9/16/2024	22:00	0	0	2	3	0	5	7	0	0	0	0	0	0	0	0	0	0	0	17	34.9	41.5	33 to 43	70.6	*	*	0	17	0
9/16/2024	23:00	0	1	0	3	3	3	2	1	0	0	0	0	0	0	0	0	0	0	13	32.9	42	25 to 35	61.5	*	*	0	13	0
24 Hr Summary		4	27	77	165	234	207	322	107	2	1	0	0	0	0	0	0	0	0	1146	35.4	43	34 to 44	50.1	72	6.3	1	1103	42

Outgoing Histogram

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/17/2024	00:00	0	0	2	2	0	0	1	1	1	0	0	0	0	0	0	0	0	0	7	34.4	45	19 to 29	57.1	1	14.3	0	5	2	
9/17/2024	01:00	0	0	0	0	1	2	2	1	0	0	0	0	0	0	0	0	0	0	6	39.7	44	31 to 41	66.7	1	16.7	0	6	0	
9/17/2024	02:00	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	42	48	26 to 36	50.0	1	50.0	0	2	0	
9/17/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/17/2024	04:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	44	44	34 to 44	100.0	*	*	0	1	0	
9/17/2024	05:00	0	0	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	3	45	48	38 to 48	100.0	1	33.3	0	3	0	
9/17/2024	06:00	0	0	1	3	2	4	3	1	0	0	0	0	0	0	0	0	0	0	14	34.9	42	27 to 37	57.1	1	7.1	0	13	1	
9/17/2024	07:00	0	15	41	24	12	14	9	4	1	0	0	0	0	0	0	0	0	0	120	27.7	37.5	18 to 28	59.2	1	0.8	0	114	6	
9/17/2024	08:00	0	5	5	12	6	7	17	3	1	0	0	0	0	0	0	0	0	0	56	33.7	42.7	37 to 47	48.2	2	3.6	0	51	5	
9/17/2024	09:00	0	3	1	6	12	15	20	8	2	0	0	0	0	0	0	0	0	0	67	37.1	44	33 to 43	58.2	7	10.4	0	56	11	
9/17/2024	10:00	0	0	2	9	14	13	17	8	1	0	0	0	0	0	0	0	0	0	64	36.9	43.8	37 to 47	53.1	6	9.4	0	60	4	
9/17/2024	11:00	0	3	8	16	10	21	20	11	0	1	0	0	0	0	0	0	0	0	90	35.4	43.7	35 to 45	51.1	7	7.8	0	85	5	
9/17/2024	12:00	1	1	4	9	21	13	35	20	2	0	0	0	0	0	0	0	0	0	106	37.9	44.9	36 to 46	56.6	15	14.2	0	99	7	
9/17/2024	13:00	0	1	6	14	22	18	32	9	2	0	0	0	0	0	0	0	0	0	104	36.1	42.8	31 to 41	51.9	9	8.7	0	98	6	
9/17/2024	14:00	1	14	43	34	21	22	12	2	0	0	0	0	0	0	0	0	0	0	149	28	36.8	18 to 28	57.0	2	1.3	0	141	8	
9/17/2024	15:00	0	1	7	20	36	31	54	15	1	1	0	0	0	0	0	0	0	0	166	36.5	43	34 to 44	54.8	13	7.8	0	157	9	
9/17/2024	16:00	1	1	4	26	32	49	52	19	1	0	0	0	0	0	0	0	0	0	185	36.7	43	35 to 45	60.0	10	5.4	0	171	14	
9/17/2024	17:00	1	1	3	17	45	37	85	34	3	0	0	0	0	0	0	0	0	0	226	38.3	44.2	36 to 46	60.2	25	11.1	0	219	7	
9/17/2024	18:00	0	2	0	13	27	32	56	31	1	0	0	0	0	0	0	0	0	0	162	38.6	45	36 to 46	65.4	24	14.8	0	157	5	
9/17/2024	19:00	0	1	0	8	9	23	31	6	1	0	0	0	0	0	0	0	0	0	79	38	43	35 to 45	73.4	3	3.8	0	76	3	
9/17/2024	20:00	0	0	1	3	10	10	16	5	0	0	0	0	0	0	0	0	0	0	45	37.4	42.3	31 to 41	68.9	3	6.7	0	44	1	
9/17/2024	21:00	0	0	2	3	4	12	9	3	1	0	0	0	0	0	0	0	0	0	34	37.6	43	35 to 45	64.7	3	8.8	0	34	0	
9/17/2024	22:00	0	0	0	0	0	2	4	4	0	0	0	0	0	0	0	0	0	0	10	42.7	45.7	37 to 47	90.0	4	40.0	0	10	0	
9/17/2024	23:00	0	0	0	0	2	1	0	2	0	0	0	0	0	0	0	0	0	0	5	38.8	44.5	26 to 36	60.0	*	*	0	5	0	
24 Hr Summary		4	48	130	219	286	327	477	190	18	2	0	0	0	0	0	0	0	0	1701	35.7	44	36 to 46	51.6	139	8.2	0	1607	94	

Outgoing Histogram

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/18/2024	00:00	0	0	2	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	6	28.7	35	20 to 30	66.7	*	*	0	6	0	
9/18/2024	01:00	0	0	0	0	1	1	1	1	0	0	0	0	0	0	0	0	0	0	4	39.3	44	26 to 36	50.0	1	25.0	0	4	0	
9/18/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/18/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/18/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/18/2024	05:00	0	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3	41	44	34 to 44	100.0	*	*	0	3	0	
9/18/2024	06:00	0	0	0	2	2	2	7	0	0	0	0	0	0	0	0	0	0	0	13	37.9	43.3	34 to 44	69.2	*	*	0	11	2	
9/18/2024	07:00	0	15	44	18	23	12	5	1	0	0	0	0	0	0	0	0	0	0	118	26.7	34	18 to 28	60.2	1	0.8	0	112	6	
9/18/2024	08:00	0	1	8	9	9	8	15	5	1	1	0	0	0	0	0	0	0	0	57	35	43.3	30 to 40	42.1	6	10.5	0	54	3	
9/18/2024	09:00	2	1	0	5	11	17	19	2	0	0	0	0	0	0	0	0	0	0	57	36	42.3	34 to 44	68.4	*	*	0	47	10	
9/18/2024	10:00	1	0	2	1	13	10	20	9	1	0	0	0	0	0	0	0	0	0	57	38	44.3	37 to 47	61.4	7	12.3	0	54	3	
9/18/2024	11:00	0	0	1	16	11	14	38	8	3	0	0	0	0	0	0	0	0	0	91	38	43.5	35 to 45	60.4	8	8.8	0	83	8	
9/18/2024	12:00	0	1	2	8	14	15	31	14	0	0	0	0	0	0	0	0	0	0	85	38.2	44.2	36 to 46	65.9	9	10.6	0	74	11	
9/18/2024	13:00	0	3	2	13	19	22	21	10	2	0	0	0	0	0	0	0	0	0	92	36.4	42.8	33 to 43	55.4	12	13.0	0	84	8	
9/18/2024	14:00	1	18	52	27	26	19	17	2	1	0	0	0	0	0	0	0	0	0	163	28.2	38.2	19 to 29	53.4	1	0.6	0	156	7	
9/18/2024	15:00	0	1	1	23	34	29	56	24	4	0	0	0	0	0	0	0	0	0	172	37.8	44.2	36 to 46	59.9	20	11.6	0	162	10	
9/18/2024	16:00	1	4	4	18	29	36	72	20	1	1	0	0	0	0	0	0	0	0	186	37.5	43.5	36 to 46	63.4	13	7.0	0	179	7	
9/18/2024	17:00	1	3	3	20	30	41	61	29	1	0	0	0	0	0	0	0	0	0	189	37.7	44.2	37 to 47	59.8	21	11.1	0	182	7	
9/18/2024	18:00	0	2	2	12	26	20	38	21	1	0	0	0	0	0	0	0	0	0	122	37.6	44.6	36 to 46	54.1	15	12.3	0	117	5	
9/18/2024	19:00	0	2	2	10	20	20	18	6	3	0	0	0	0	0	0	0	0	0	81	35.9	43	30 to 40	56.8	9	11.1	0	80	1	
9/18/2024	20:00	0	2	3	5	17	12	28	11	1	0	0	0	0	0	0	0	0	0	79	37.3	44	35 to 45	57.0	7	8.9	0	79	0	
9/18/2024	21:00	0	0	6	6	6	13	15	1	0	0	0	0	0	0	0	0	0	0	47	35	42	33 to 43	61.7	1	2.1	0	45	2	
9/18/2024	22:00	0	0	1	1	3	2	10	6	0	0	0	0	0	0	0	0	0	0	23	39.9	44.8	35 to 45	69.6	2	8.7	0	23	0	
9/18/2024	23:00	0	1	0	1	0	0	4	2	0	0	0	0	0	0	0	0	0	0	8	38	44.5	35 to 45	75.0	*	*	0	8	0	
24 Hr Summary		6	54	135	196	295	296	478	172	19	2	0	0	0	0	0	0	0	0	1653	35.6	44	36 to 46	50.7	133	8.0	0	1563	90	

Outgoing Histogram
Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/19/2024	00:00	0	0	1	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	3	31	43	19 to 29	66.7	*	*	0	3	0
9/19/2024	01:00	0	0	1	1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	4	34.8	44	15 to 25	50.0	1	25.0	0	4	0
9/19/2024	02:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	33	39	17 to 27	50.0	*	*	0	2	0
9/19/2024	03:00	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	41.5	43	33 to 43	100.0	*	*	0	2	0
9/19/2024	04:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	33	33	23 to 33	100.0	*	*	0	1	0
9/19/2024	05:00	0	0	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	3	43	50	31 to 41	66.7	1	33.3	0	3	0
9/19/2024	06:00	0	0	0	1	1	3	1	1	0	0	0	0	0	0	0	0	0	0	7	37.1	41	28 to 38	71.4	1	14.3	0	4	3
9/19/2024	07:00	0	12	55	28	24	3	6	4	0	0	0	0	0	0	0	0	0	0	132	26.5	32	19 to 29	68.9	2	1.5	0	126	6
9/19/2024	08:00	0	0	9	11	12	8	10	5	0	0	0	0	0	0	0	0	0	0	55	33.1	42.5	23 to 33	49.1	2	3.6	0	49	6
9/19/2024	09:00	1	1	3	10	8	14	12	14	0	0	0	0	0	0	0	0	0	0	63	36.7	44.6	35 to 45	54.0	6	9.5	0	57	6
9/19/2024	10:00	1	1	2	4	13	18	25	3	1	0	0	0	0	0	0	0	0	0	68	37	41.9	33 to 43	67.6	3	4.4	0	61	7
9/19/2024	11:00	0	2	1	9	12	13	40	16	1	0	0	0	0	0	0	0	0	0	94	38.7	44.4	38 to 48	66.0	9	9.6	0	85	9
9/19/2024	12:00	1	6	3	12	9	21	37	8	0	0	0	0	0	0	0	0	0	0	97	36.2	43.1	35 to 45	63.9	4	4.1	0	91	6
9/19/2024	13:00	0	2	3	17	28	22	29	9	3	0	0	0	0	0	0	0	0	0	113	36	43	30 to 40	49.6	9	8.0	0	102	11
9/19/2024	14:00	1	13	48	40	21	21	28	6	0	0	0	0	0	0	0	0	0	0	178	29.8	39.7	19 to 29	52.8	3	1.7	0	167	11
9/19/2024	15:00	0	3	9	34	42	46	53	15	2	0	0	0	0	0	0	0	0	0	204	35.6	42.4	34 to 44	53.9	14	6.9	0	196	8
9/19/2024	16:00	0	1	1	16	52	27	65	20	1	0	0	0	0	0	0	0	0	0	183	37.4	43.2	32 to 42	57.9	14	7.7	0	175	8
9/19/2024	17:00	1	4	2	20	49	43	70	34	1	0	0	0	0	0	0	0	0	0	224	37.5	44.1	36 to 46	58.9	22	9.8	0	217	7
9/19/2024	18:00	0	5	1	17	29	37	35	24	1	1	0	0	0	0	0	0	0	0	150	37	44.4	36 to 46	59.3	17	11.3	0	145	5
9/19/2024	19:00	0	1	2	13	18	18	33	10	1	2	0	0	0	0	0	0	0	0	98	37.3	43.3	35 to 45	56.1	9	9.2	0	98	0
9/19/2024	20:00	0	2	1	6	14	11	30	10	2	0	0	0	0	0	0	0	0	0	76	38.3	44.5	36 to 46	60.5	10	13.2	0	74	2
9/19/2024	21:00	0	3	1	3	7	11	12	2	1	0	0	0	0	0	0	0	0	0	40	35.8	42.5	33 to 43	60.0	2	5.0	0	40	0
9/19/2024	22:00	0	1	3	0	3	6	8	1	0	0	0	0	0	0	0	0	0	0	22	35.8	41	31 to 41	68.2	1	4.5	0	22	0
9/19/2024	23:00	0	0	0	2	1	1	1	2	0	0	0	0	0	0	0	0	0	0	7	36.9	44.5	35 to 45	57.1	*	*	0	7	0
24 Hr Summary		5	57	146	246	344	325	500	185	15	3	0	0	0	0	0	0	0	0	1826	35.4	44	36 to 46	49.9	130	7.1	0	1731	95

Outgoing Histogram
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/20/2024	00:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	43	43	33 to 43	100.0	*	*	0	1	0	
9/20/2024	01:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25	28	18 to 28	100.0	*	*	0	2	0	
9/20/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	03:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	44	44	34 to 44	100.0	*	*	0	1	0	
9/20/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	05:00	0	0	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	4	41	43	35 to 45	100.0	*	*	0	4	0	
9/20/2024	06:00	0	0	0	2	4	1	1	2	0	0	0	0	0	0	0	0	0	0	10	35.3	40	25 to 35	70.0	2	20.0	0	9	1	
9/20/2024	07:00	0	11	56	24	21	13	7	0	0	0	0	0	0	0	0	0	0	0	132	26.9	34	18 to 28	65.2	*	*	0	125	7	
9/20/2024	08:00	1	3	6	10	10	13	17	4	0	0	0	0	0	0	0	0	0	0	64	33.8	42.1	33 to 43	53.1	2	3.1	0	57	7	
9/20/2024	09:00	0	2	2	7	6	14	13	4	1	0	0	0	0	0	0	0	0	0	49	36.4	43.5	34 to 44	57.1	4	8.2	0	43	6	
9/20/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		1	16	65	44	41	42	42	11	1	0	0	0	0	0	0	0	0	0	263	31	41	21 to 31	44.1	8	3.0	0	242	21	

Outgoing: Average Hourly Volume for Week of 9/16/2024

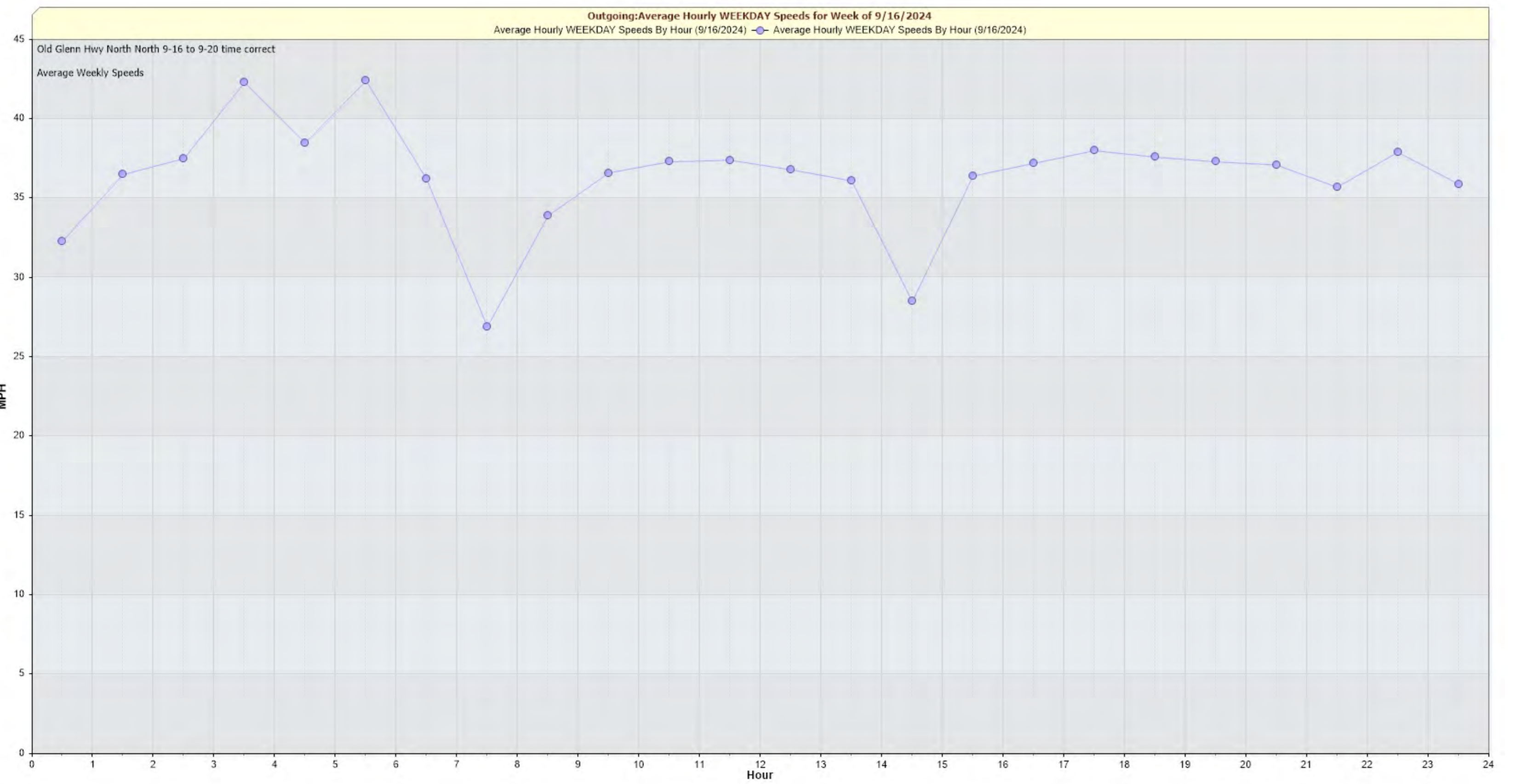
Average Counts By Hour (9/16/2024) ● Average Counts By Hour (9/16/2024)



Old Glenn Hwy North North 9-16 to 9-20 time correct
Average Weekly Volumes

Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/16/2024
Average Hourly WEEKDAY Speeds By Hour (9/16/2024) — Average Hourly WEEKDAY Speeds By Hour (9/16/2024)

Old Glenn Hwy North North 9-16 to 9-20 time correct
Average Weekly Speeds

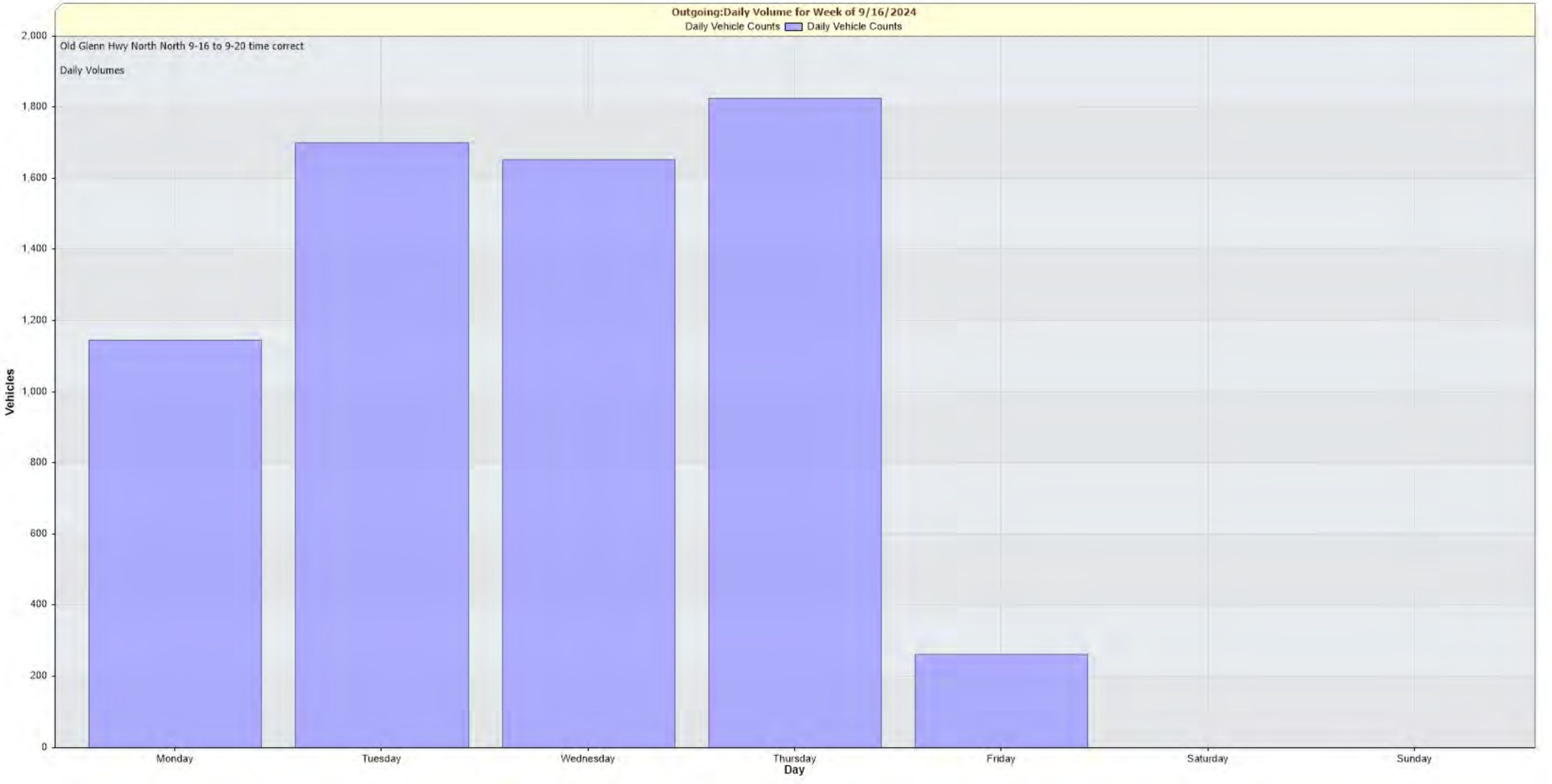


Outgoing:Daily Volume for Week of 9/16/2024

Daily Vehicle Counts

Old Glenn Hwy North North 9-16 to 9-20 time correct

Daily Volumes



For Project: Birchwood Loop northwest northwest 9-16 to 9-20 time correct
 Project Notes:
 Location/Name: Incoming
 Report Generated: 10/3/2024 3:59:16 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 9/16/2024 12:00:00 PM through 9/20/2024 9:59:59 AM
 85th Percentile Speed: 35 MPH
 85th Percentile Vehicles: 6138
 Max Speed: 55 MPH on 9/19/2024 2:00:12 PM
 Total Vehicles: 7221
 AADT: 1843

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1444	1444
AM Peak	7:00 AM 120	120
PM Peak	5:00 PM 189	189

Speed

Speed Limit: 45
 85th Percentile Speed: 35
 50th Percentile Speed: 30
 10 MPH Pace Interval: 25.0 MPH to 35.0 MPH
 Average Speed: 29.04

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	3	1	3	1	0	N/A	N/A
% over limit	0.2	0.1	0.2	0.1	0.0	N/A	N/A
Avg Speeder	47.7	46.0	47.3	55.0	0.0	N/A	N/A
Avg Speed	28.8	29.0	29.3	29.3	27.2	N/A	N/A

Class Counts

	Number	%
VEH_SM	38	0.5
VEH_MED	6906	95.6
VEH_LG	277	3.8
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/16/2024 1:00:00 PM	35.0	63	74	39	0.0	0.0%	29.2
9/16/2024 2:00:00 PM	35.0	125	147	47	47.0	0.7%	29.6
9/16/2024 3:00:00 PM	31.0	147	173	39	0.0	0.0%	24.9
9/16/2024 4:00:00 PM	35.0	162	190	40	0.0	0.0%	28.8
9/16/2024 5:00:00 PM	34.0	148	174	42	0.0	0.0%	29.9
9/16/2024 6:00:00 PM	35.0	148	174	44	0.0	0.0%	29.7
9/16/2024 7:00:00 PM	35.0	96	113	50	50.0	0.9%	30.1
9/16/2024 8:00:00 PM	34.0	63	74	46	46.0	1.4%	29.4
9/16/2024 9:00:00 PM	34.0	59	69	40	0.0	0.0%	28.5
9/16/2024 10:00:00 PM	33.0	41	48	40	0.0	0.0%	29.1
9/16/2024 11:00:00 PM	33.0	16	19	36	0.0	0.0%	29.1
9/17/2024 12:00:00 AM	31.0	7	8	34	0.0	0.0%	29.0
9/17/2024 1:00:00 AM	38.0	5	6	41	0.0	0.0%	31.2
9/17/2024 2:00:00 AM	30.0	4	5	32	0.0	0.0%	29.8
9/17/2024 3:00:00 AM	30.0	2	2	30	0.0	0.0%	29.0
9/17/2024 4:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
9/17/2024 5:00:00 AM	38.0	1	1	38	0.0	0.0%	38.0
9/17/2024 6:00:00 AM	34.0	7	8	39	0.0	0.0%	29.4
9/17/2024 7:00:00 AM	36.0	32	38	40	0.0	0.0%	30.3
9/17/2024 8:00:00 AM	30.0	88	104	43	0.0	0.0%	24.0
9/17/2024 9:00:00 AM	35.0	88	104	43	0.0	0.0%	29.3
9/17/2024 10:00:00 AM	36.0	74	87	43	0.0	0.0%	32.2
9/17/2024 11:00:00 AM	35.0	67	79	43	0.0	0.0%	30.4
9/17/2024 12:00:00 PM	34.0	80	94	39	0.0	0.0%	29.6
9/17/2024 1:00:00 PM	35.0	103	121	40	0.0	0.0%	29.4
9/17/2024 2:00:00 PM	35.0	112	132	46	46.0	0.8%	29.9
9/17/2024 3:00:00 PM	31.0	166	195	37	0.0	0.0%	23.9
9/17/2024 4:00:00 PM	35.0	134	158	41	0.0	0.0%	29.6
9/17/2024 5:00:00 PM	35.0	152	179	43	0.0	0.0%	30.5
9/17/2024 6:00:00 PM	35.0	157	185	44	0.0	0.0%	30.0
9/17/2024 7:00:00 PM	34.0	126	148	40	0.0	0.0%	29.7
9/17/2024 8:00:00 PM	33.0	54	64	40	0.0	0.0%	30.3
9/17/2024 9:00:00 PM	34.0	48	56	37	0.0	0.0%	29.3
9/17/2024 10:00:00 PM	32.0	30	35	37	0.0	0.0%	29.0
9/17/2024 11:00:00 PM	32.0	12	14	37	0.0	0.0%	29.4
9/18/2024 12:00:00 AM	34.0	7	8	37	0.0	0.0%	29.8
9/18/2024 1:00:00 AM	34.0	7	8	39	0.0	0.0%	28.3
9/18/2024 2:00:00 AM	37.0	3	3	37	0.0	0.0%	33.3
9/18/2024 3:00:00 AM	30.0	1	1	30	0.0	0.0%	30.0
9/18/2024 4:00:00 AM	32.0	3	4	33	0.0	0.0%	25.3
9/18/2024 5:00:00 AM	34.0	3	4	35	0.0	0.0%	33.5
9/18/2024 6:00:00 AM	31.0	9	11	37	0.0	0.0%	30.6
9/18/2024 7:00:00 AM	34.0	37	43	40	0.0	0.0%	30.2
9/18/2024 8:00:00 AM	31.0	103	121	40	0.0	0.0%	24.6
9/18/2024 9:00:00 AM	35.0	76	89	42	0.0	0.0%	28.8
9/18/2024 10:00:00 AM	36.0	63	74	41	0.0	0.0%	29.6
9/18/2024 11:00:00 AM	34.0	76	89	41	0.0	0.0%	29.6

Incoming Summary
Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

9/18/2024 12:00:00 PM	34.0	75	88	49	48.0	2.3%	29.4
9/18/2024 1:00:00 PM	35.0	90	106	42	0.0	0.0%	29.5
9/18/2024 2:00:00 PM	36.0	97	114	46	46.0	0.9%	30.7
9/18/2024 3:00:00 PM	29.0	151	178	40	0.0	0.0%	24.1
9/18/2024 4:00:00 PM	35.0	133	156	40	0.0	0.0%	29.8
9/18/2024 5:00:00 PM	35.0	148	174	42	0.0	0.0%	31.1
9/18/2024 6:00:00 PM	35.0	157	185	41	0.0	0.0%	30.6
9/18/2024 7:00:00 PM	36.0	136	160	45	0.0	0.0%	32.1
9/18/2024 8:00:00 PM	36.0	78	92	45	0.0	0.0%	30.6
9/18/2024 9:00:00 PM	35.0	74	87	45	0.0	0.0%	31.0
9/18/2024 10:00:00 PM	33.0	26	30	36	0.0	0.0%	29.4
9/18/2024 11:00:00 PM	31.0	16	19	38	0.0	0.0%	28.5
9/19/2024 12:00:00 AM	31.0	8	9	36	0.0	0.0%	26.7
9/19/2024 1:00:00 AM	31.0	8	10	35	0.0	0.0%	29.1
9/19/2024 2:00:00 AM	36.0	5	6	40	0.0	0.0%	28.8
9/19/2024 3:00:00 AM	28.0	4	5	30	0.0	0.0%	27.6
9/19/2024 4:00:00 AM	34.0	2	2	34	0.0	0.0%	34.0
9/19/2024 5:00:00 AM	35.0	2	2	35	0.0	0.0%	34.0
9/19/2024 6:00:00 AM	37.0	8	10	38	0.0	0.0%	33.1
9/19/2024 7:00:00 AM	35.0	33	39	43	0.0	0.0%	30.3
9/19/2024 8:00:00 AM	31.0	118	139	40	0.0	0.0%	24.6
9/19/2024 9:00:00 AM	34.0	84	99	41	0.0	0.0%	28.5
9/19/2024 10:00:00 AM	34.0	88	104	43	0.0	0.0%	30.3
9/19/2024 11:00:00 AM	36.0	70	82	42	0.0	0.0%	31.6
9/19/2024 12:00:00 PM	36.0	82	97	43	0.0	0.0%	30.6
9/19/2024 1:00:00 PM	35.0	89	105	42	0.0	0.0%	29.5
9/19/2024 2:00:00 PM	36.0	115	135	41	0.0	0.0%	31.0
9/19/2024 3:00:00 PM	32.0	139	164	55	55.0	0.6%	25.1
9/19/2024 4:00:00 PM	34.0	190	223	40	0.0	0.0%	29.1
9/19/2024 5:00:00 PM	35.0	140	165	45	0.0	0.0%	30.7
9/19/2024 6:00:00 PM	36.0	180	212	41	0.0	0.0%	30.7
9/19/2024 7:00:00 PM	35.0	114	134	43	0.0	0.0%	30.8
9/19/2024 8:00:00 PM	34.0	60	71	42	0.0	0.0%	30.6
9/19/2024 9:00:00 PM	35.0	57	67	38	0.0	0.0%	28.5
9/19/2024 10:00:00 PM	34.0	43	51	35	0.0	0.0%	28.3
9/19/2024 11:00:00 PM	35.0	19	22	39	0.0	0.0%	29.9
9/20/2024 12:00:00 AM	34.0	8	9	37	0.0	0.0%	30.2
9/20/2024 1:00:00 AM	34.0	3	4	37	0.0	0.0%	32.8
9/20/2024 2:00:00 AM	28.0	2	2	28	0.0	0.0%	28.0
9/20/2024 3:00:00 AM	31.0	4	5	35	0.0	0.0%	25.6
9/20/2024 4:00:00 AM	25.0	3	4	36	0.0	0.0%	26.3
9/20/2024 5:00:00 AM	34.0	2	2	34	0.0	0.0%	32.0
9/20/2024 6:00:00 AM	32.0	7	8	35	0.0	0.0%	27.4
9/20/2024 7:00:00 AM	35.0	36	42	37	0.0	0.0%	29.3
9/20/2024 8:00:00 AM	29.0	99	117	39	0.0	0.0%	23.5
9/20/2024 9:00:00 AM	34.0	74	87	39	0.0	0.0%	28.8
9/20/2024 10:00:00 AM	35.0	55	65	43	0.0	0.0%	30.0

Incoming Summary

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/17/2024 12:00:00 AM	34.0	1074	1263	50	47.7	0.2%	28.8
9/18/2024 12:00:00 AM	34.0	1550	1824	46	46.0	0.1%	29.0
9/19/2024 12:00:00 AM	35.0	1568	1845	49	47.3	0.2%	29.3
9/20/2024 12:00:00 AM	35.0	1660	1953	55	55.0	0.1%	29.3
9/20/2024 9:59:59 AM	34.0	286	336	43	0.0	0.0%	27.2

Incoming Weekly Counts

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	6	8	10	4	*	*	7	0	34.25
1 - 2	*	5	3	6	2	*	*	4	0	32.75
2 - 3	*	2	1	5	5	*	*	3.25	0	29.75
3 - 4	*	1	4	2	4	*	*	2.75	0	29
4 - 5	*	1	4	2	2	*	*	2.25	0	35.25
5 - 6	*	8	11	10	8	*	*	9.25	0	33.38
6 - 7	*	38	43	39	42	*	*	40.5	0	34.6
7 - 8	*	104	121	139	117	*	*	120.25	0	30.2
8 - 9	*	104	89	99	87	*	*	94.75	0	34.15
9 - 10	*	87	74	104	65	*	*	82.5	0	34.92
10 - 11	*	79	89	82	*	*	*	83.33	0	34.67
11 - 12	*	94	88	97	*	*	*	93	0	34.57
12 - 13	74	121	106	105	*	*	*	101.5	0	34.3
13 - 14	147	132	114	135	*	*	*	132	0	35.08
14 - 15	173	195	178	164	*	*	*	177.5	0	30.18
15 - 16	190	158	156	223	*	*	*	181.75	0	34.28
16 - 17	174	179	174	165	*	*	*	173	0	34.52
17 - 18	174	185	185	212	*	*	*	189	0	34.68
18 - 19	113	148	160	134	*	*	*	138.75	0	34.52
19 - 20	74	64	92	71	*	*	*	75.25	0	33.92
20 - 21	69	56	87	67	*	*	*	69.75	0	33.9
21 - 22	48	35	30	51	*	*	*	41	0	32.62
22 - 23	19	14	19	22	*	*	*	18.5	0	32.75
23 - 24	8	8	9	9	*	*	*	8.5	0	32.5
Totals	1263	1824	1845	1953	336	0	0			
% of Total	17.49%	25.26%	25.55%	27.05%	4.65%	0%	0%			

Incoming Monthly Counts

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	6	8	10	4	*	*	7	0	34.25
1 - 2	*	5	3	6	2	*	*	4	0	32.75
2 - 3	*	2	1	5	5	*	*	3.25	0	29.75
3 - 4	*	1	4	2	4	*	*	2.75	0	29
4 - 5	*	1	4	2	2	*	*	2.25	0	35.25
5 - 6	*	8	11	10	8	*	*	9.25	0	33.38
6 - 7	*	38	43	39	42	*	*	40.5	0	34.6
7 - 8	*	104	121	139	117	*	*	120.25	0	30.2
8 - 9	*	104	89	99	87	*	*	94.75	0	34.15
9 - 10	*	87	74	104	65	*	*	82.5	0	34.92
10 - 11	*	79	89	82	*	*	*	83.33	0	34.67
11 - 12	*	94	88	97	*	*	*	93	0	34.57
12 - 13	74	121	106	105	*	*	*	101.5	0	34.3
13 - 14	147	132	114	135	*	*	*	132	0	35.08
14 - 15	173	195	178	164	*	*	*	177.5	0	30.18
15 - 16	190	158	156	223	*	*	*	181.75	0	34.28
16 - 17	174	179	174	165	*	*	*	173	0	34.52
17 - 18	174	185	185	212	*	*	*	189	0	34.68
18 - 19	113	148	160	134	*	*	*	138.75	0	34.52
19 - 20	74	64	92	71	*	*	*	75.25	0	33.92
20 - 21	69	56	87	67	*	*	*	69.75	0	33.9
21 - 22	48	35	30	51	*	*	*	41	0	32.62
22 - 23	19	14	19	22	*	*	*	18.5	0	32.75
23 - 24	8	8	9	9	*	*	*	8.5	0	32.5
Totals	1263	1824	1845	1953	336	0	0			
% of Total	17.49%	25.26%	25.55%	27.05%	4.65%	0%	0%			

Incoming Weekly Speeds

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	31.17	28.25	29.1	32.75	*	*	29.82	*	34.25
1 - 2	*	29.8	33.33	28.83	28	*	*	29.87	*	32.75
2 - 3	*	29	30	27.6	25.6	*	*	27.23	*	29.75
3 - 4	*	25	25.25	34	26.25	*	*	27.18	*	29
4 - 5	*	38	33.5	34	32	*	*	33.78	*	35.25
5 - 6	*	29.38	30.64	33.1	27.38	*	*	30.32	*	33.38
6 - 7	*	30.29	30.21	30.28	29.29	*	*	30.01	*	34.6
7 - 8	*	23.96	24.55	24.57	23.49	*	*	24.17	*	30.2
8 - 9	*	29.28	28.8	28.47	28.82	*	*	28.85	*	34.15
9 - 10	*	32.22	29.59	30.29	29.97	*	*	30.58	*	34.92
10 - 11	*	30.42	29.58	31.59	*	*	*	30.5	*	34.67
11 - 12	*	29.57	29.36	30.61	*	*	*	29.87	*	34.57
12 - 13	29.34	29.37	29.49	29.52	*	*	*	29.44	*	34.3
13 - 14	29.61	29.89	30.7	31.02	*	*	*	30.28	*	35.08
14 - 15	24.88	23.93	24.05	25.09	*	*	*	24.46	*	30.18
15 - 16	28.82	29.63	29.81	29.07	*	*	*	29.28	*	34.28
16 - 17	29.85	30.5	31.08	30.73	*	*	*	30.54	*	34.52
17 - 18	29.72	30.03	30.56	30.73	*	*	*	30.28	*	34.68
18 - 19	30.06	29.66	32.09	30.84	*	*	*	30.72	*	34.52
19 - 20	29.38	30.31	30.6	30.61	*	*	*	30.24	*	33.92
20 - 21	28.48	29.32	31.03	28.48	*	*	*	29.44	*	33.9
21 - 22	29.12	29	29.4	28.29	*	*	*	28.89	*	32.62
22 - 23	29.05	29.36	28.47	29.91	*	*	*	29.22	*	32.75
23 - 24	29	29.75	26.67	30.22	*	*	*	28.88	*	32.5
Totals	28.8	29	29.3	29.3	27.2	0	0			
% of Total	20.06%	20.19%	20.4%	20.4%	18.94%	0%	0%			

Incoming Monthly Speeds

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	31.17	28.25	29.1	32.75	*	*	29.82	*	34.25
1 - 2	*	29.8	33.33	28.83	28	*	*	29.87	*	32.75
2 - 3	*	29	30	27.6	25.6	*	*	27.23	*	29.75
3 - 4	*	25	25.25	34	26.25	*	*	27.18	*	29
4 - 5	*	38	33.5	34	32	*	*	33.78	*	35.25
5 - 6	*	29.38	30.64	33.1	27.38	*	*	30.32	*	33.38
6 - 7	*	30.29	30.21	30.28	29.29	*	*	30.01	*	34.6
7 - 8	*	23.96	24.55	24.57	23.49	*	*	24.17	*	30.2
8 - 9	*	29.28	28.8	28.47	28.82	*	*	28.85	*	34.15
9 - 10	*	32.22	29.59	30.29	29.97	*	*	30.58	*	34.92
10 - 11	*	30.42	29.58	31.59	*	*	*	30.5	*	34.67
11 - 12	*	29.57	29.36	30.61	*	*	*	29.87	*	34.57
12 - 13	29.34	29.37	29.49	29.52	*	*	*	29.44	*	34.3
13 - 14	29.61	29.89	30.7	31.02	*	*	*	30.28	*	35.08
14 - 15	24.88	23.93	24.05	25.09	*	*	*	24.46	*	30.18
15 - 16	28.82	29.63	29.81	29.07	*	*	*	29.28	*	34.28
16 - 17	29.85	30.5	31.08	30.73	*	*	*	30.54	*	34.52
17 - 18	29.72	30.03	30.56	30.73	*	*	*	30.28	*	34.68
18 - 19	30.06	29.66	32.09	30.84	*	*	*	30.72	*	34.52
19 - 20	29.38	30.31	30.6	30.61	*	*	*	30.24	*	33.92
20 - 21	28.48	29.32	31.03	28.48	*	*	*	29.44	*	33.9
21 - 22	29.12	29	29.4	28.29	*	*	*	28.89	*	32.62
22 - 23	29.05	29.36	28.47	29.91	*	*	*	29.22	*	32.75
23 - 24	29	29.75	26.67	30.22	*	*	*	28.88	*	32.5
Totals	28.8	29	29.3	29.3	27.2	0	0			
% of Total	20.06%	20.19%	20.4%	20.4%	18.94%	0%	0%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	38	34	31	34	*	*	34.25	0	34.25
1 - 2	*	30	37	36	28	*	*	32.75	0	32.75
2 - 3	*	30	30	28	31	*	*	29.75	0	29.75
3 - 4	*	25	32	34	25	*	*	29	0	29
4 - 5	*	38	34	35	34	*	*	35.25	0	35.25
5 - 6	*	34	31	36.5	32	*	*	33.38	0	33.38
6 - 7	*	35.5	34	34.7	34.2	*	*	34.6	0	34.6
7 - 8	*	30	31	31	28.8	*	*	30.2	0	30.2
8 - 9	*	35	34.3	33.6	33.7	*	*	34.15	0	34.15
9 - 10	*	35.8	35.2	34	34.7	*	*	34.92	0	34.92
10 - 11	*	34.8	33.7	35.5	*	*	*	34.67	0	34.67
11 - 12	*	34	34	35.7	*	*	*	34.57	0	34.57
12 - 13	34.4	34.1	34.2	34.5	*	*	*	34.3	0	34.3
13 - 14	34.1	34.7	35.5	36	*	*	*	35.08	0	35.08
14 - 15	30.6	30.2	28.7	31.2	*	*	*	30.18	0	30.18
15 - 16	34.1	34.6	34.6	33.8	*	*	*	34.28	0	34.28
16 - 17	34	34.7	34.8	34.6	*	*	*	34.52	0	34.52
17 - 18	34.6	34.2	34.8	35.1	*	*	*	34.68	0	34.68
18 - 19	34.2	33.8	35.7	34.4	*	*	*	34.52	0	34.52
19 - 20	33.6	33	35.4	33.7	*	*	*	33.92	0	33.92
20 - 21	33.2	33.4	34.8	34.2	*	*	*	33.9	0	33.9
21 - 22	32.5	31.7	33	33.3	*	*	*	32.62	0	32.62
22 - 23	33	32	31	35	*	*	*	32.75	0	32.75
23 - 24	31	34	31	34	*	*	*	32.5	0	32.5
Totals	399.3	800.5	803.7	814.8	315.4	0	0			
% of Total	12.74%	25.54%	25.65%	26%	10.06%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	38	34	31	34	*	*	34.25	0	34.25
1 - 2	*	30	37	36	28	*	*	32.75	0	32.75
2 - 3	*	30	30	28	31	*	*	29.75	0	29.75
3 - 4	*	25	32	34	25	*	*	29	0	29
4 - 5	*	38	34	35	34	*	*	35.25	0	35.25
5 - 6	*	34	31	36.5	32	*	*	33.38	0	33.38
6 - 7	*	35.5	34	34.7	34.2	*	*	34.6	0	34.6
7 - 8	*	30	31	31	28.8	*	*	30.2	0	30.2
8 - 9	*	35	34.3	33.6	33.7	*	*	34.15	0	34.15
9 - 10	*	35.8	35.2	34	34.7	*	*	34.92	0	34.92
10 - 11	*	34.8	33.7	35.5	*	*	*	34.67	0	34.67
11 - 12	*	34	34	35.7	*	*	*	34.57	0	34.57
12 - 13	34.4	34.1	34.2	34.5	*	*	*	34.3	0	34.3
13 - 14	34.1	34.7	35.5	36	*	*	*	35.08	0	35.08
14 - 15	30.6	30.2	28.7	31.2	*	*	*	30.18	0	30.18
15 - 16	34.1	34.6	34.6	33.8	*	*	*	34.28	0	34.28
16 - 17	34	34.7	34.8	34.6	*	*	*	34.52	0	34.52
17 - 18	34.6	34.2	34.8	35.1	*	*	*	34.68	0	34.68
18 - 19	34.2	33.8	35.7	34.4	*	*	*	34.52	0	34.52
19 - 20	33.6	33	35.4	33.7	*	*	*	33.92	0	33.92
20 - 21	33.2	33.4	34.8	34.2	*	*	*	33.9	0	33.9
21 - 22	32.5	31.7	33	33.3	*	*	*	32.62	0	32.62
22 - 23	33	32	31	35	*	*	*	32.75	0	32.75
23 - 24	31	34	31	34	*	*	*	32.5	0	32.5

Summary of Violators

Birchwood Loop northwest northwest 9-16 to 9-20 time from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM
correct

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	28	29.8	0	0.0
01:00:00	16	29.9	0	0.0
02:00:00	13	27.2	0	0.0
03:00:00	11	27.2	0	0.0
04:00:00	9	33.8	0	0.0
05:00:00	37	30.3	0	0.0
06:00:00	162	30.0	0	0.0
07:00:00	481	24.2	0	0.0
08:00:00	379	28.8	0	0.0
09:00:00	330	30.6	0	0.0
10:00:00	250	30.5	0	0.0
11:00:00	279	29.9	2	48.0
12:00:00	406	29.4	0	0.0
13:00:00	528	30.3	3	46.3
14:00:00	710	24.5	1	55.0
15:00:00	727	29.3	0	0.0
16:00:00	692	30.5	0	0.0
17:00:00	756	30.3	0	0.0
18:00:00	555	30.7	1	50.0
19:00:00	301	30.2	1	46.0
20:00:00	279	29.4	0	0.0
21:00:00	164	28.9	0	0.0
22:00:00	74	29.2	0	0.0
23:00:00	34	28.9	0	0.0

Incoming Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	2	1	6	28	24	13	0	0	0	0	0	0	0	0	0	0	0	0	74	29.3	34.4	26 to 36	78.4	*	*	3	61	10	
9/16/2024	13:00	2	12	8	39	63	18	3	2	0	0	0	0	0	0	0	0	0	0	147	29.6	34.1	25 to 35	75.5	1	0.7	1	131	15	
9/16/2024	14:00	4	31	54	38	37	9	0	0	0	0	0	0	0	0	0	0	0	0	173	24.9	30.6	21 to 31	61.8	*	*	1	163	9	
9/16/2024	15:00	3	10	27	56	65	27	2	0	0	0	0	0	0	0	0	0	0	0	190	28.8	34.1	23 to 33	69.5	*	*	0	176	14	
9/16/2024	16:00	2	8	15	44	79	23	3	0	0	0	0	0	0	0	0	0	0	0	174	29.9	34	26 to 36	77.0	*	*	0	166	8	
9/16/2024	17:00	4	5	13	52	66	31	3	0	0	0	0	0	0	0	0	0	0	0	174	29.7	34.6	26 to 36	77.0	*	*	0	170	4	
9/16/2024	18:00	1	6	8	30	49	16	1	1	1	0	0	0	0	0	0	0	0	0	113	30.1	34.2	26 to 36	77.9	1	0.9	0	108	5	
9/16/2024	19:00	0	3	12	20	31	6	1	1	0	0	0	0	0	0	0	0	0	0	74	29.4	33.6	26 to 36	74.3	1	1.4	0	74	0	
9/16/2024	20:00	1	3	11	24	23	6	1	0	0	0	0	0	0	0	0	0	0	0	69	28.5	33.2	24 to 34	75.4	*	*	1	68	0	
9/16/2024	21:00	0	0	4	21	19	3	1	0	0	0	0	0	0	0	0	0	0	0	48	29.1	32.5	24 to 34	87.5	*	*	0	48	0	
9/16/2024	22:00	0	0	3	7	6	3	0	0	0	0	0	0	0	0	0	0	0	0	19	29.1	33	26 to 36	84.2	*	*	0	19	0	
9/16/2024	23:00	0	0	1	3	4	0	0	0	0	0	0	0	0	0	0	0	0	0	8	29	31	24 to 34	100.0	*	*	0	7	1	
24 Hr Summary		19	79	162	362	466	155	15	4	1	0	0	0	0	0	0	0	0	0	1263	28.8	34	25 to 35	70.5	3	0.2	6	1191	66	

Incoming Histogram
 Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/17/2024	00:00	0	0	2	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	6	31.2	38	20 to 30	50.0	*	*	0	5	1
9/17/2024	01:00	0	0	0	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	5	29.8	30	22 to 32	100.0	*	*	0	5	0
9/17/2024	02:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	29	30	20 to 30	100.0	*	*	0	2	0
9/17/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0
9/17/2024	04:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	38	38	28 to 38	100.0	*	*	0	1	0
9/17/2024	05:00	0	0	1	3	3	1	0	0	0	0	0	0	0	0	0	0	0	0	8	29.4	34	20 to 30	75.0	*	*	0	8	0
9/17/2024	06:00	1	1	2	11	15	7	1	0	0	0	0	0	0	0	0	0	0	0	38	30.3	35.5	24 to 34	73.7	*	*	0	36	2
9/17/2024	07:00	3	20	45	16	13	5	2	0	0	0	0	0	0	0	0	0	0	0	104	24	30	17 to 27	68.3	*	*	0	100	4
9/17/2024	08:00	2	6	13	24	40	14	5	0	0	0	0	0	0	0	0	0	0	0	104	29.3	35	26 to 36	67.3	*	*	4	88	12
9/17/2024	09:00	0	1	4	16	40	22	4	0	0	0	0	0	0	0	0	0	0	0	87	32.2	35.8	27 to 37	85.1	*	*	6	69	12
9/17/2024	10:00	1	5	3	19	36	11	4	0	0	0	0	0	0	0	0	0	0	0	79	30.4	34.8	26 to 36	75.9	*	*	4	74	1
9/17/2024	11:00	0	6	5	27	42	14	0	0	0	0	0	0	0	0	0	0	0	0	94	29.6	34	25 to 35	79.8	*	*	1	87	6
9/17/2024	12:00	5	7	9	27	54	17	2	0	0	0	0	0	0	0	0	0	0	0	121	29.4	34.1	27 to 37	76.9	*	*	0	114	7
9/17/2024	13:00	2	11	8	29	60	18	3	1	0	0	0	0	0	0	0	0	0	0	132	29.9	34.7	27 to 37	75.0	1	0.8	3	117	12
9/17/2024	14:00	7	42	58	51	30	7	0	0	0	0	0	0	0	0	0	0	0	0	195	23.9	30.2	17 to 27	64.1	*	*	2	186	7
9/17/2024	15:00	1	10	16	42	60	23	6	0	0	0	0	0	0	0	0	0	0	0	158	29.6	34.6	25 to 35	70.3	*	*	0	153	5
9/17/2024	16:00	0	2	17	57	67	29	7	0	0	0	0	0	0	0	0	0	0	0	179	30.5	34.7	25 to 35	76.5	*	*	0	174	5
9/17/2024	17:00	2	7	8	58	79	28	3	0	0	0	0	0	0	0	0	0	0	0	185	30	34.2	25 to 35	80.5	*	*	0	181	4
9/17/2024	18:00	2	8	11	39	69	18	1	0	0	0	0	0	0	0	0	0	0	0	148	29.7	33.8	26 to 36	78.4	*	*	0	143	5
9/17/2024	19:00	0	2	3	18	31	9	1	0	0	0	0	0	0	0	0	0	0	0	64	30.3	33	25 to 35	82.8	*	*	0	62	2
9/17/2024	20:00	1	0	4	21	26	4	0	0	0	0	0	0	0	0	0	0	0	0	56	29.3	33.4	25 to 35	87.5	*	*	0	56	0
9/17/2024	21:00	0	0	5	13	16	1	0	0	0	0	0	0	0	0	0	0	0	0	35	29	31.7	23 to 33	94.3	*	*	0	35	0
9/17/2024	22:00	0	0	2	5	5	2	0	0	0	0	0	0	0	0	0	0	0	0	14	29.4	32	22 to 32	85.7	*	*	0	14	0
9/17/2024	23:00	0	0	2	2	3	1	0	0	0	0	0	0	0	0	0	0	0	0	8	29.8	34	24 to 34	75.0	*	*	0	8	0
24 Hr Summary		27	128	218	481	696	233	40	1	0	0	0	0	0	0	0	0	0	0	1824	29	34	25 to 35	69.2	1	0.1	20	1719	85

Incoming Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/18/2024	00:00	0	0	3	1	3	1	0	0	0	0	0	0	0	0	0	0	0	0	8	28.3	34	21 to 31	75.0	*	*	0	8	0
9/18/2024	01:00	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	3	33.3	37	27 to 37	100.0	*	*	0	3	0
9/18/2024	02:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	30	30	20 to 30	100.0	*	*	0	1	0
9/18/2024	03:00	0	1	1	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	25.3	32	11 to 21	50.0	*	*	0	4	0
9/18/2024	04:00	0	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	0	0	4	33.5	34	25 to 35	100.0	*	*	0	4	0
9/18/2024	05:00	0	0	0	3	6	2	0	0	0	0	0	0	0	0	0	0	0	0	11	30.6	31	25 to 35	90.9	*	*	0	10	1
9/18/2024	06:00	1	2	2	6	26	5	1	0	0	0	0	0	0	0	0	0	0	0	43	30.2	34	25 to 35	83.7	*	*	0	41	2
9/18/2024	07:00	5	17	45	23	22	8	1	0	0	0	0	0	0	0	0	0	0	0	121	24.6	31	20 to 30	61.2	*	*	0	121	0
9/18/2024	08:00	0	4	20	22	28	13	2	0	0	0	0	0	0	0	0	0	0	0	89	28.8	34.3	27 to 37	65.2	*	*	0	86	3
9/18/2024	09:00	1	5	6	21	27	11	3	0	0	0	0	0	0	0	0	0	0	0	74	29.6	35.2	26 to 36	71.6	*	*	0	69	5
9/18/2024	10:00	1	4	9	26	38	10	1	0	0	0	0	0	0	0	0	0	0	0	89	29.6	33.7	26 to 36	78.7	*	*	2	82	5
9/18/2024	11:00	2	4	6	28	35	11	0	2	0	0	0	0	0	0	0	0	0	0	88	29.4	34	25 to 35	75.0	2	2.3	0	86	2
9/18/2024	12:00	2	5	8	32	42	15	2	0	0	0	0	0	0	0	0	0	0	0	106	29.5	34.2	25 to 35	75.5	*	*	0	102	4
9/18/2024	13:00	0	5	10	27	47	20	4	1	0	0	0	0	0	0	0	0	0	0	114	30.7	35.5	26 to 36	71.9	1	0.9	0	107	7
9/18/2024	14:00	2	29	72	50	20	4	1	0	0	0	0	0	0	0	0	0	0	0	178	24.1	28.7	18 to 28	73.0	*	*	0	174	4
9/18/2024	15:00	2	7	10	44	67	25	1	0	0	0	0	0	0	0	0	0	0	0	156	29.8	34.6	26 to 36	77.6	*	*	0	147	9
9/18/2024	16:00	1	4	5	45	84	28	7	0	0	0	0	0	0	0	0	0	0	0	174	31.1	34.8	26 to 36	81.6	*	*	0	170	4
9/18/2024	17:00	1	2	23	39	79	38	3	0	0	0	0	0	0	0	0	0	0	0	185	30.6	34.8	26 to 36	74.6	*	*	0	180	5
9/18/2024	18:00	1	0	10	29	72	40	7	1	0	0	0	0	0	0	0	0	0	0	160	32.1	35.7	26 to 36	78.1	*	*	0	155	5
9/18/2024	19:00	1	2	5	26	38	18	1	1	0	0	0	0	0	0	0	0	0	0	92	30.6	35.4	26 to 36	75.0	*	*	0	90	2
9/18/2024	20:00	1	0	3	28	37	15	2	1	0	0	0	0	0	0	0	0	0	0	87	31	34.8	26 to 36	83.9	*	*	0	86	1
9/18/2024	21:00	0	0	4	11	12	3	0	0	0	0	0	0	0	0	0	0	0	0	30	29.4	33	26 to 36	86.7	*	*	0	30	0
9/18/2024	22:00	0	0	3	9	6	1	0	0	0	0	0	0	0	0	0	0	0	0	19	28.5	31	23 to 33	89.5	*	*	0	19	0
9/18/2024	23:00	1	1	0	4	2	1	0	0	0	0	0	0	0	0	0	0	0	0	9	26.7	31	26 to 36	77.8	*	*	0	9	0
24 Hr Summary		22	92	245	474	699	271	36	6	0	0	0	0	0	0	0	0	0	0	1845	29.3	35	26 to 36	69.5	3	0.2	2	1784	59

Incoming Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/19/2024	00:00	0	0	0	7	2	1	0	0	0	0	0	0	0	0	0	0	0	0	10	29.1	31	25 to 35	100.0	*	*	0	10	0
9/19/2024	01:00	0	1	1	2	0	1	1	0	0	0	0	0	0	0	0	0	0	0	6	28.8	36	18 to 28	66.7	*	*	0	5	1
9/19/2024	02:00	0	0	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	5	27.6	28	20 to 30	100.0	*	*	0	5	0
9/19/2024	03:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	34	34	24 to 34	100.0	*	*	0	2	0
9/19/2024	04:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	35	25 to 35	100.0	*	*	0	2	0
9/19/2024	05:00	0	0	0	1	5	4	0	0	0	0	0	0	0	0	0	0	0	0	10	33.1	36.5	27 to 37	90.0	*	*	0	9	1
9/19/2024	06:00	1	2	2	8	18	7	1	0	0	0	0	0	0	0	0	0	0	0	39	30.3	34.7	27 to 37	82.1	*	*	0	38	1
9/19/2024	07:00	2	27	49	29	21	10	1	0	0	0	0	0	0	0	0	0	0	0	139	24.6	31	16 to 26	63.3	*	*	0	135	4
9/19/2024	08:00	0	8	16	29	34	10	2	0	0	0	0	0	0	0	0	0	0	0	99	28.5	33.6	25 to 35	68.7	*	*	0	97	2
9/19/2024	09:00	2	1	5	30	50	15	1	0	0	0	0	0	0	0	0	0	0	0	104	30.3	34	25 to 35	79.8	*	*	1	97	6
9/19/2024	10:00	1	4	4	12	34	23	4	0	0	0	0	0	0	0	0	0	0	0	82	31.6	35.5	27 to 37	78.0	*	*	1	78	3
9/19/2024	11:00	0	6	7	22	39	22	1	0	0	0	0	0	0	0	0	0	0	0	97	30.6	35.7	26 to 36	71.1	*	*	1	89	7
9/19/2024	12:00	0	6	11	30	39	16	3	0	0	0	0	0	0	0	0	0	0	0	105	29.5	34.5	25 to 35	71.4	*	*	0	104	1
9/19/2024	13:00	0	5	8	33	58	28	3	0	0	0	0	0	0	0	0	0	0	0	135	31	36	27 to 37	74.8	*	*	0	132	3
9/19/2024	14:00	4	30	51	35	35	6	2	0	0	1	0	0	0	0	0	0	0	0	164	25.1	31.2	18 to 28	60.4	1	0.6	3	155	6
9/19/2024	15:00	0	9	25	84	74	28	3	0	0	0	0	0	0	0	0	0	0	0	223	29.1	33.8	25 to 35	76.7	*	*	1	215	7
9/19/2024	16:00	1	5	7	46	74	27	4	1	0	0	0	0	0	0	0	0	0	0	165	30.7	34.6	26 to 36	80.6	*	*	1	159	5
9/19/2024	17:00	3	5	12	52	96	40	4	0	0	0	0	0	0	0	0	0	0	0	212	30.7	35.1	26 to 36	79.7	*	*	0	210	2
9/19/2024	18:00	0	4	7	30	69	23	1	0	0	0	0	0	0	0	0	0	0	0	134	30.8	34.4	25 to 35	80.6	*	*	0	130	4
9/19/2024	19:00	1	2	3	15	41	6	3	0	0	0	0	0	0	0	0	0	0	0	71	30.6	33.7	26 to 36	84.5	*	*	0	71	0
9/19/2024	20:00	3	3	8	22	20	11	0	0	0	0	0	0	0	0	0	0	0	0	67	28.5	34.2	26 to 36	76.1	*	*	0	67	0
9/19/2024	21:00	0	2	8	19	18	4	0	0	0	0	0	0	0	0	0	0	0	0	51	28.3	33.3	25 to 35	80.4	*	*	0	50	1
9/19/2024	22:00	0	1	2	5	10	4	0	0	0	0	0	0	0	0	0	0	0	0	22	29.9	35	24 to 34	72.7	*	*	0	22	0
9/19/2024	23:00	0	0	0	4	4	1	0	0	0	0	0	0	0	0	0	0	0	0	9	30.2	34	24 to 34	88.9	*	*	0	9	0
24 Hr Summary		18	121	226	519	745	288	34	1	0	1	0	0	0	0	0	0	0	0	1953	29.3	35	25 to 35	69.9	1	0.1	8	1891	54

Incoming Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG			
9/20/2024	00:00	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	32.8	34	27 to 37	100.0	*	*	0	4	0		
9/20/2024	01:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28	28	18 to 28	100.0	*	*	0	2	0		
9/20/2024	02:00	0	1	2	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	5	25.6	31	13 to 23	60.0	*	*	0	5	0		
9/20/2024	03:00	0	0	2	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26.3	25	15 to 25	75.0	*	*	0	4	0		
9/20/2024	04:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	32	34	24 to 34	100.0	*	*	0	2	0		
9/20/2024	05:00	0	0	2	3	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	8	27.4	32	22 to 32	75.0	*	*	0	8	0		
9/20/2024	06:00	2	2	5	9	17	7	0	0	0	0	0	0	0	0	0	0	0	0	0	42	29.3	34.2	27 to 37	76.2	*	*	0	40	2		
9/20/2024	07:00	3	20	52	25	11	6	0	0	0	0	0	0	0	0	0	0	0	0	0	117	23.5	28.8	18 to 28	73.5	*	*	0	116	1		
9/20/2024	08:00	0	5	15	27	29	11	0	0	0	0	0	0	0	0	0	0	0	0	0	87	28.8	33.7	23 to 33	71.3	*	*	0	80	7		
9/20/2024	09:00	0	1	8	19	25	10	2	0	0	0	0	0	0	0	0	0	0	0	0	65	30	34.7	25 to 35	72.3	*	*	2	60	3		
9/20/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		5	29	86	87	89	38	2	0	0	0	0	0	0	0	0	0	0	0	0	336	27.2	34	20 to 30	58.6	*	*	2	321	13		

Incoming: Average Hourly Volume for Week of 9/16/2024

Average Counts By Hour (9/16/2024) — Average Counts By Hour (9/16/2024)

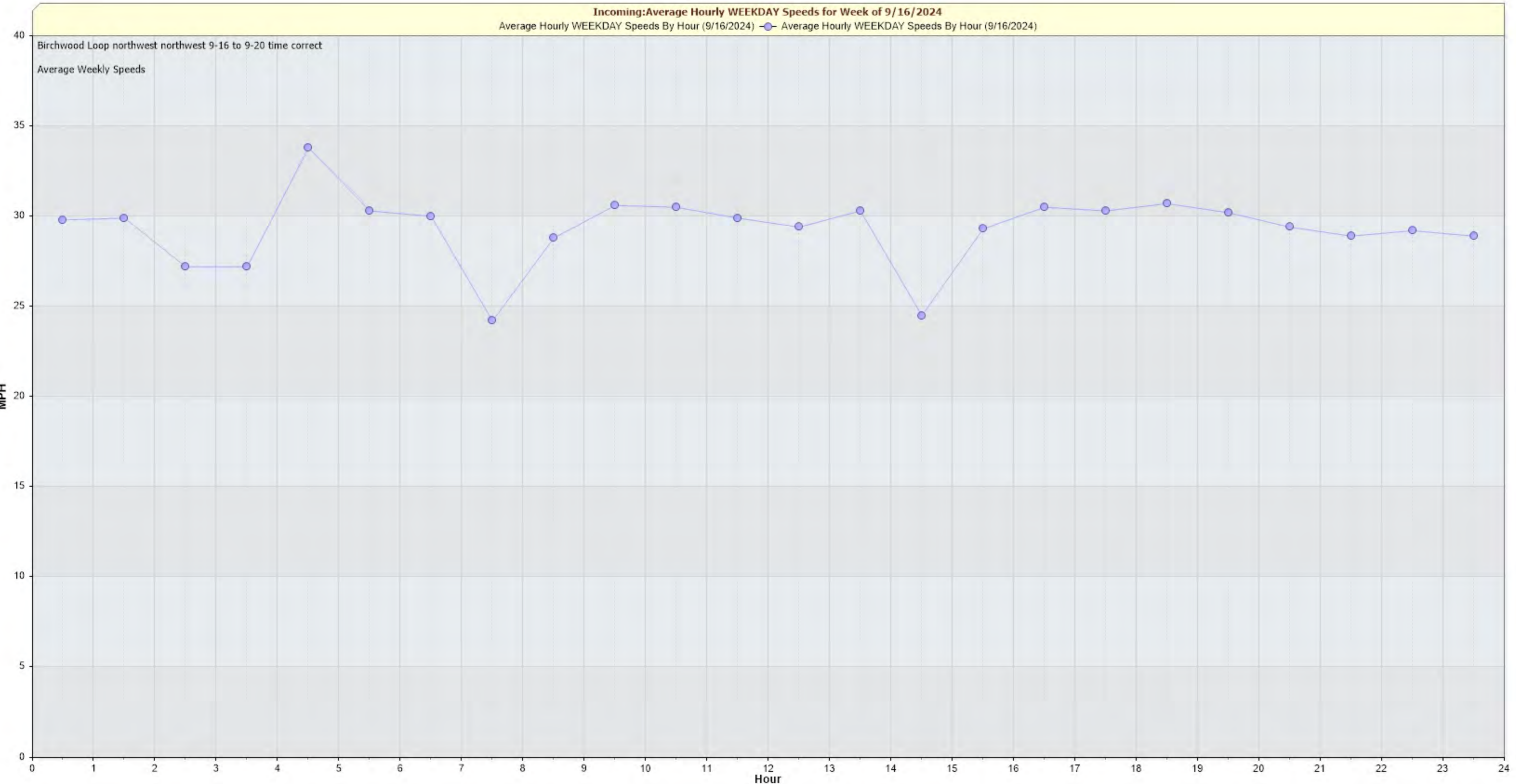
Birchwood Loop northwest northwest 9-16 to 9-20 time correct

Average Weekly Volumes



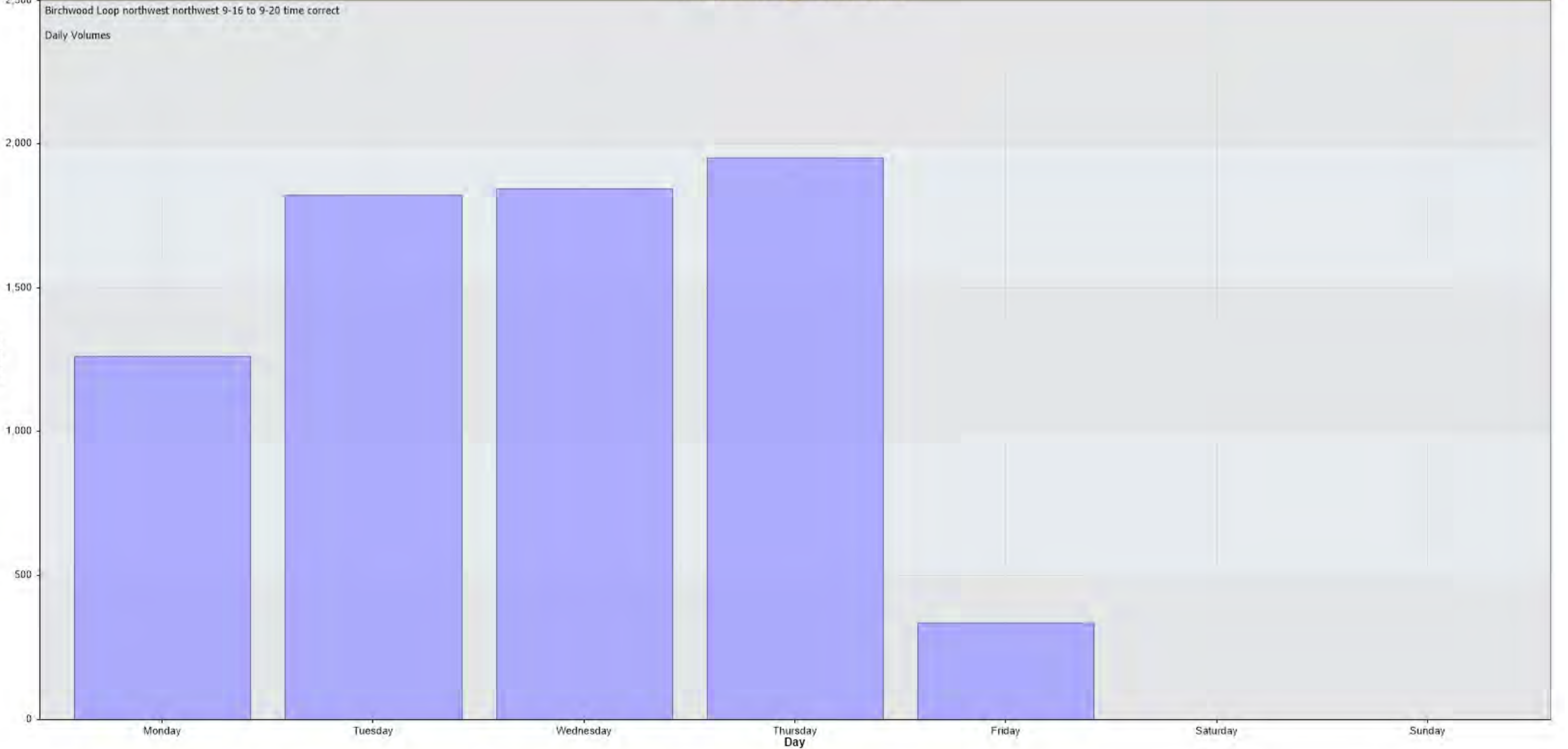
Incoming: Average Hourly WEEKDAY Speeds for Week of 9/16/2024
Average Hourly WEEKDAY Speeds By Hour (9/16/2024) — Average Hourly WEEKDAY Speeds By Hour (9/16/2024)

Birchwood Loop northwest northwest 9-16 to 9-20 time correct
Average Weekly Speeds



Incoming:Daily Volume for Week of 9/16/2024

Daily Vehicle Counts



Birchwood Loop northwest northwest 9-16 to 9-20 time correct

Daily Volumes

Vehicles

Day

For Project: Birchwood Loop northwest northwest 9-16 to 9-20 time correct
 Project Notes:
 Location/Name: Outgoing
 Report Generated: 10/3/2024 3:59:16 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 9/16/2024 12:00:00 PM through 9/20/2024 9:59:59 AM
 85th Percentile Speed: 31 MPH
 85th Percentile Vehicles: 5092
 Max Speed: 49 MPH on 9/16/2024 3:32:07 PM
 Total Vehicles: 5990
 AADT: 1529

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1198	1198
AM Peak	7:00 AM 105	105
PM Peak	5:00 PM 145	145

Speed

Speed Limit: 45
 85th Percentile Speed: 31
 50th Percentile Speed: 27
 10 MPH Pace Interval: 22.0 MPH to 32.0 MPH
 Average Speed: 27.01

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	1	0	0	1	0	N/A	N/A
% over limit	0.1	0.0	0.0	0.1	0.0	N/A	N/A
Avg Speeder	49.0	0.0	0.0	46.0	0.0	N/A	N/A
Avg Speed	26.9	27.0	27.1	27.1	26.3	N/A	N/A

Class Counts

	Number	%
VEH_SM	5	0.1
VEH_MED	5576	93.1
VEH_LG	409	6.8
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Outgoing Summary
Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/16/2024 1:00:00 PM	31.0	67	79	34	0.0	0.0%	26.7
9/16/2024 2:00:00 PM	31.0	115	135	37	0.0	0.0%	27.2
9/16/2024 3:00:00 PM	27.0	94	111	32	0.0	0.0%	23.3
9/16/2024 4:00:00 PM	32.0	98	115	49	49.0	0.9%	27.8
9/16/2024 5:00:00 PM	31.0	106	125	42	0.0	0.0%	27.5
9/16/2024 6:00:00 PM	32.0	119	140	38	0.0	0.0%	28.1
9/16/2024 7:00:00 PM	32.0	80	94	37	0.0	0.0%	27.9
9/16/2024 8:00:00 PM	31.0	47	55	35	0.0	0.0%	26.5
9/16/2024 9:00:00 PM	30.0	32	38	34	0.0	0.0%	26.3
9/16/2024 10:00:00 PM	31.0	20	23	39	0.0	0.0%	26.8
9/16/2024 11:00:00 PM	29.0	14	17	33	0.0	0.0%	26.0
9/17/2024 12:00:00 AM	27.0	5	6	28	0.0	0.0%	24.5
9/17/2024 1:00:00 AM	24.0	3	4	28	0.0	0.0%	23.8
9/17/2024 2:00:00 AM	29.0	3	3	29	0.0	0.0%	25.3
9/17/2024 3:00:00 AM	28.0	3	3	28	0.0	0.0%	26.7
9/17/2024 4:00:00 AM	34.0	3	3	34	0.0	0.0%	29.0
9/17/2024 5:00:00 AM	26.0	3	4	37	0.0	0.0%	28.3
9/17/2024 6:00:00 AM	28.0	8	10	31	0.0	0.0%	26.1
9/17/2024 7:00:00 AM	29.0	60	71	35	0.0	0.0%	26.5
9/17/2024 8:00:00 AM	29.0	94	111	34	0.0	0.0%	25.0
9/17/2024 9:00:00 AM	30.0	87	102	38	0.0	0.0%	25.6
9/17/2024 10:00:00 AM	31.0	89	105	37	0.0	0.0%	26.3
9/17/2024 11:00:00 AM	30.0	84	99	37	0.0	0.0%	26.8
9/17/2024 12:00:00 PM	30.0	86	101	38	0.0	0.0%	26.2
9/17/2024 1:00:00 PM	32.0	80	94	40	0.0	0.0%	27.3
9/17/2024 2:00:00 PM	31.0	101	119	41	0.0	0.0%	26.7
9/17/2024 3:00:00 PM	30.0	76	90	39	0.0	0.0%	24.8
9/17/2024 4:00:00 PM	32.0	98	115	39	0.0	0.0%	27.4
9/17/2024 5:00:00 PM	32.0	129	152	37	0.0	0.0%	28.2
9/17/2024 6:00:00 PM	33.0	123	145	41	0.0	0.0%	29.0
9/17/2024 7:00:00 PM	32.0	63	74	38	0.0	0.0%	28.6
9/17/2024 8:00:00 PM	33.0	59	69	41	0.0	0.0%	28.6
9/17/2024 9:00:00 PM	29.0	27	32	36	0.0	0.0%	26.5
9/17/2024 10:00:00 PM	30.0	23	27	33	0.0	0.0%	26.9
9/17/2024 11:00:00 PM	30.0	11	13	37	0.0	0.0%	27.4
9/18/2024 12:00:00 AM	32.0	3	3	32	0.0	0.0%	28.7
9/18/2024 1:00:00 AM	27.0	4	5	29	0.0	0.0%	26.2
9/18/2024 2:00:00 AM	29.0	3	4	35	0.0	0.0%	30.0
9/18/2024 3:00:00 AM	32.0	3	3	32	0.0	0.0%	27.7
9/18/2024 4:00:00 AM	31.0	3	3	31	0.0	0.0%	26.7
9/18/2024 5:00:00 AM	30.0	9	11	32	0.0	0.0%	27.4
9/18/2024 6:00:00 AM	30.0	14	16	33	0.0	0.0%	27.0
9/18/2024 7:00:00 AM	31.0	64	75	43	0.0	0.0%	27.5
9/18/2024 8:00:00 AM	28.0	81	95	33	0.0	0.0%	25.2
9/18/2024 9:00:00 AM	31.0	84	99	38	0.0	0.0%	26.6
9/18/2024 10:00:00 AM	33.0	79	93	39	0.0	0.0%	27.1
9/18/2024 11:00:00 AM	30.0	60	71	34	0.0	0.0%	26.4

Outgoing Summary
Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

9/18/2024 12:00:00 PM	30.0	77	91	38	0.0	0.0%	26.5
9/18/2024 1:00:00 PM	31.0	71	84	36	0.0	0.0%	27.8
9/18/2024 2:00:00 PM	31.0	93	109	41	0.0	0.0%	27.4
9/18/2024 3:00:00 PM	29.0	88	103	38	0.0	0.0%	24.5
9/18/2024 4:00:00 PM	31.0	101	119	44	0.0	0.0%	27.3
9/18/2024 5:00:00 PM	31.0	100	118	39	0.0	0.0%	27.7
9/18/2024 6:00:00 PM	33.0	136	160	41	0.0	0.0%	28.5
9/18/2024 7:00:00 PM	33.0	81	95	41	0.0	0.0%	28.5
9/18/2024 8:00:00 PM	31.0	67	79	36	0.0	0.0%	27.9
9/18/2024 9:00:00 PM	32.0	67	79	36	0.0	0.0%	27.6
9/18/2024 10:00:00 PM	29.0	15	18	32	0.0	0.0%	25.7
9/18/2024 11:00:00 PM	32.0	14	16	36	0.0	0.0%	28.8
9/19/2024 12:00:00 AM	30.0	13	15	31	0.0	0.0%	26.1
9/19/2024 1:00:00 AM	33.0	4	5	46	46.0	20.0%	32.6
9/19/2024 2:00:00 AM	29.0	3	4	30	0.0	0.0%	25.3
9/19/2024 3:00:00 AM	28.0	8	9	29	0.0	0.0%	24.2
9/19/2024 4:00:00 AM	40.0	3	3	40	0.0	0.0%	34.0
9/19/2024 5:00:00 AM	31.0	7	8	32	0.0	0.0%	28.0
9/19/2024 6:00:00 AM	28.0	13	15	32	0.0	0.0%	26.9
9/19/2024 7:00:00 AM	30.0	62	73	36	0.0	0.0%	27.2
9/19/2024 8:00:00 AM	30.0	94	110	37	0.0	0.0%	25.7
9/19/2024 9:00:00 AM	32.0	97	114	40	0.0	0.0%	27.7
9/19/2024 10:00:00 AM	30.0	72	85	35	0.0	0.0%	26.8
9/19/2024 11:00:00 AM	31.0	71	84	34	0.0	0.0%	27.2
9/19/2024 12:00:00 PM	31.0	96	113	38	0.0	0.0%	27.3
9/19/2024 1:00:00 PM	30.0	91	107	37	0.0	0.0%	26.6
9/19/2024 2:00:00 PM	31.0	82	96	36	0.0	0.0%	26.5
9/19/2024 3:00:00 PM	29.0	94	110	39	0.0	0.0%	24.1
9/19/2024 4:00:00 PM	32.0	103	121	41	0.0	0.0%	27.4
9/19/2024 5:00:00 PM	32.0	138	162	41	0.0	0.0%	28.3
9/19/2024 6:00:00 PM	33.0	115	135	39	0.0	0.0%	29.0
9/19/2024 7:00:00 PM	33.0	79	93	41	0.0	0.0%	28.2
9/19/2024 8:00:00 PM	33.0	58	68	37	0.0	0.0%	27.8
9/19/2024 9:00:00 PM	31.0	29	34	34	0.0	0.0%	26.7
9/19/2024 10:00:00 PM	31.0	35	41	35	0.0	0.0%	26.5
9/19/2024 11:00:00 PM	28.0	10	12	31	0.0	0.0%	25.9
9/20/2024 12:00:00 AM	29.0	8	10	29	0.0	0.0%	25.2
9/20/2024 1:00:00 AM	29.0	5	6	40	0.0	0.0%	27.7
9/20/2024 2:00:00 AM	26.0	2	2	26	0.0	0.0%	25.0
9/20/2024 3:00:00 AM	23.0	3	4	24	0.0	0.0%	20.5
9/20/2024 4:00:00 AM	33.0	1	1	33	0.0	0.0%	33.0
9/20/2024 5:00:00 AM	31.0	4	5	34	0.0	0.0%	28.0
9/20/2024 6:00:00 AM	28.0	8	10	29	0.0	0.0%	26.4
9/20/2024 7:00:00 AM	30.0	42	50	34	0.0	0.0%	26.8
9/20/2024 8:00:00 AM	30.0	88	104	37	0.0	0.0%	26.2
9/20/2024 9:00:00 AM	31.0	69	81	33	0.0	0.0%	26.8
9/20/2024 10:00:00 AM	29.0	57	67	37	0.0	0.0%	25.7

Outgoing Summary

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/17/2024 12:00:00 AM	31.0	797	938	49	49.0	0.1%	26.9
9/18/2024 12:00:00 AM	31.0	1317	1549	41	0.0	0.0%	27.0
9/19/2024 12:00:00 AM	31.0	1327	1561	44	0.0	0.0%	27.1
9/20/2024 12:00:00 AM	32.0	1370	1612	46	46.0	0.1%	27.1
9/20/2024 9:59:59 AM	30.0	280	330	40	0.0	0.0%	26.3

Outgoing Weekly Counts

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	4	5	5	6	*	*	5	0	28.25
1 - 2	*	3	4	4	2	*	*	3.25	0	28.25
2 - 3	*	3	3	9	4	*	*	4.75	0	27.75
3 - 4	*	3	3	3	1	*	*	2.5	0	34.5
4 - 5	*	4	11	8	5	*	*	7	0	29.5
5 - 6	*	10	16	15	10	*	*	12.75	0	28.5
6 - 7	*	71	75	73	50	*	*	67.25	0	29.88
7 - 8	*	111	95	110	104	*	*	105	0	28.98
8 - 9	*	102	99	114	81	*	*	99	0	30.48
9 - 10	*	105	93	85	67	*	*	87.5	0	30.32
10 - 11	*	99	71	84	*	*	*	84.67	0	29.93
11 - 12	*	101	91	113	*	*	*	101.67	0	29.87
12 - 13	79	94	84	107	*	*	*	91	0	30.7
13 - 14	135	119	109	96	*	*	*	114.75	0	30.45
14 - 15	111	90	103	110	*	*	*	103.5	0	28.52
15 - 16	115	115	119	121	*	*	*	117.5	0	31.5
16 - 17	125	152	118	162	*	*	*	139.25	0	31.17
17 - 18	140	145	160	135	*	*	*	145	0	32.35
18 - 19	94	74	95	93	*	*	*	89	0	32.2
19 - 20	55	69	79	68	*	*	*	67.75	0	31.68
20 - 21	38	32	79	34	*	*	*	45.75	0	30.12
21 - 22	23	27	18	41	*	*	*	27.25	0	30.08
22 - 23	17	13	16	12	*	*	*	14.5	0	29.75
23 - 24	6	3	15	10	*	*	*	8.5	0	29.25
Totals	938	1549	1561	1612	330	0	0			
% of Total	15.66%	25.86%	26.06%	26.91%	5.51%	0%	0%			

Outgoing Monthly Counts

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	4	5	5	6	*	*	5	0	28.25
1 - 2	*	3	4	4	2	*	*	3.25	0	28.25
2 - 3	*	3	3	9	4	*	*	4.75	0	27.75
3 - 4	*	3	3	3	1	*	*	2.5	0	34.5
4 - 5	*	4	11	8	5	*	*	7	0	29.5
5 - 6	*	10	16	15	10	*	*	12.75	0	28.5
6 - 7	*	71	75	73	50	*	*	67.25	0	29.88
7 - 8	*	111	95	110	104	*	*	105	0	28.98
8 - 9	*	102	99	114	81	*	*	99	0	30.48
9 - 10	*	105	93	85	67	*	*	87.5	0	30.32
10 - 11	*	99	71	84	*	*	*	84.67	0	29.93
11 - 12	*	101	91	113	*	*	*	101.67	0	29.87
12 - 13	79	94	84	107	*	*	*	91	0	30.7
13 - 14	135	119	109	96	*	*	*	114.75	0	30.45
14 - 15	111	90	103	110	*	*	*	103.5	0	28.52
15 - 16	115	115	119	121	*	*	*	117.5	0	31.5
16 - 17	125	152	118	162	*	*	*	139.25	0	31.17
17 - 18	140	145	160	135	*	*	*	145	0	32.35
18 - 19	94	74	95	93	*	*	*	89	0	32.2
19 - 20	55	69	79	68	*	*	*	67.75	0	31.68
20 - 21	38	32	79	34	*	*	*	45.75	0	30.12
21 - 22	23	27	18	41	*	*	*	27.25	0	30.08
22 - 23	17	13	16	12	*	*	*	14.5	0	29.75
23 - 24	6	3	15	10	*	*	*	8.5	0	29.25
Totals	938	1549	1561	1612	330	0	0			
% of Total	15.66%	25.86%	26.06%	26.91%	5.51%	0%	0%			

Outgoing Weekly Speeds

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	23.75	26.2	32.6	27.67	*	*	27.75	*	28.25
1 - 2	*	25.33	30	25.25	25	*	*	26.69	*	28.25
2 - 3	*	26.67	27.67	24.22	20.5	*	*	24.37	*	27.75
3 - 4	*	29	26.67	34	33	*	*	30.2	*	34.5
4 - 5	*	28.25	27.36	28	28	*	*	27.79	*	29.5
5 - 6	*	26.1	27	26.93	26.4	*	*	26.69	*	28.5
6 - 7	*	26.51	27.45	27.15	26.82	*	*	27	*	29.88
7 - 8	*	25.01	25.19	25.72	26.17	*	*	25.52	*	28.98
8 - 9	*	25.63	26.59	27.7	26.81	*	*	26.71	*	30.48
9 - 10	*	26.27	27.13	26.78	25.69	*	*	26.51	*	30.32
10 - 11	*	26.8	26.37	27.23	*	*	*	26.82	*	29.93
11 - 12	*	26.16	26.45	27.33	*	*	*	26.68	*	29.87
12 - 13	26.72	27.3	27.82	26.59	*	*	*	27.09	*	30.7
13 - 14	27.18	26.66	27.45	26.48	*	*	*	26.96	*	30.45
14 - 15	23.34	24.8	24.47	24.05	*	*	*	24.13	*	28.52
15 - 16	27.77	27.43	27.28	27.37	*	*	*	27.46	*	31.5
16 - 17	27.52	28.19	27.66	28.28	*	*	*	27.95	*	31.17
17 - 18	28.15	28.99	28.49	28.99	*	*	*	28.65	*	32.35
18 - 19	27.87	28.58	28.53	28.16	*	*	*	28.27	*	32.2
19 - 20	26.55	28.61	27.86	27.82	*	*	*	27.77	*	31.68
20 - 21	26.29	26.53	27.61	26.68	*	*	*	26.97	*	30.12
21 - 22	26.83	26.93	25.67	26.46	*	*	*	26.52	*	30.08
22 - 23	26	27.38	28.81	25.92	*	*	*	27.07	*	29.75
23 - 24	24.5	28.67	26.07	25.2	*	*	*	25.76	*	29.25
Totals	26.9	27	27.1	27.1	26.3	0	0			
% of Total	20.01%	20.09%	20.16%	20.16%	19.57%	0%	0%			

Outgoing Monthly Speeds

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	23.75	26.2	32.6	27.67	*	*	27.75	*	28.25
1 - 2	*	25.33	30	25.25	25	*	*	26.69	*	28.25
2 - 3	*	26.67	27.67	24.22	20.5	*	*	24.37	*	27.75
3 - 4	*	29	26.67	34	33	*	*	30.2	*	34.5
4 - 5	*	28.25	27.36	28	28	*	*	27.79	*	29.5
5 - 6	*	26.1	27	26.93	26.4	*	*	26.69	*	28.5
6 - 7	*	26.51	27.45	27.15	26.82	*	*	27	*	29.88
7 - 8	*	25.01	25.19	25.72	26.17	*	*	25.52	*	28.98
8 - 9	*	25.63	26.59	27.7	26.81	*	*	26.71	*	30.48
9 - 10	*	26.27	27.13	26.78	25.69	*	*	26.51	*	30.32
10 - 11	*	26.8	26.37	27.23	*	*	*	26.82	*	29.93
11 - 12	*	26.16	26.45	27.33	*	*	*	26.68	*	29.87
12 - 13	26.72	27.3	27.82	26.59	*	*	*	27.09	*	30.7
13 - 14	27.18	26.66	27.45	26.48	*	*	*	26.96	*	30.45
14 - 15	23.34	24.8	24.47	24.05	*	*	*	24.13	*	28.52
15 - 16	27.77	27.43	27.28	27.37	*	*	*	27.46	*	31.5
16 - 17	27.52	28.19	27.66	28.28	*	*	*	27.95	*	31.17
17 - 18	28.15	28.99	28.49	28.99	*	*	*	28.65	*	32.35
18 - 19	27.87	28.58	28.53	28.16	*	*	*	28.27	*	32.2
19 - 20	26.55	28.61	27.86	27.82	*	*	*	27.77	*	31.68
20 - 21	26.29	26.53	27.61	26.68	*	*	*	26.97	*	30.12
21 - 22	26.83	26.93	25.67	26.46	*	*	*	26.52	*	30.08
22 - 23	26	27.38	28.81	25.92	*	*	*	27.07	*	29.75
23 - 24	24.5	28.67	26.07	25.2	*	*	*	25.76	*	29.25
Totals	26.9	27	27.1	27.1	26.3	0	0			
% of Total	20.01%	20.09%	20.16%	20.16%	19.57%	0%	0%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	24	27	33	29	*	*	28.25	0	28.25
1 - 2	*	29	29	29	26	*	*	28.25	0	28.25
2 - 3	*	28	32	28	23	*	*	27.75	0	27.75
3 - 4	*	34	31	40	33	*	*	34.5	0	34.5
4 - 5	*	26	30	31	31	*	*	29.5	0	29.5
5 - 6	*	28	30	28	28	*	*	28.5	0	28.5
6 - 7	*	29	30.7	30	29.8	*	*	29.88	0	29.88
7 - 8	*	28.6	27.8	29.7	29.8	*	*	28.98	0	28.98
8 - 9	*	29.4	30.7	31.2	30.6	*	*	30.48	0	30.48
9 - 10	*	30.4	32.1	30	28.8	*	*	30.32	0	30.32
10 - 11	*	29.4	29.6	30.8	*	*	*	29.93	0	29.93
11 - 12	*	29.5	29.7	30.4	*	*	*	29.87	0	29.87
12 - 13	30.8	31.3	31	29.7	*	*	*	30.7	0	30.7
13 - 14	30.4	30.2	31	30.2	*	*	*	30.45	0	30.45
14 - 15	27	29.6	28.8	28.7	*	*	*	28.52	0	28.52
15 - 16	31.8	31.8	31	31.4	*	*	*	31.5	0	31.5
16 - 17	30.7	31.9	30.2	31.9	*	*	*	31.17	0	31.17
17 - 18	31.7	32.7	32.3	32.7	*	*	*	32.35	0	32.35
18 - 19	31.9	32	32.4	32.5	*	*	*	32.2	0	32.2
19 - 20	30.6	33	30.8	32.3	*	*	*	31.68	0	31.68
20 - 21	29.5	28.8	31.2	31	*	*	*	30.12	0	30.12
21 - 22	31	30	29	30.3	*	*	*	30.08	0	30.08
22 - 23	29	30	32	28	*	*	*	29.75	0	29.75
23 - 24	27	32	29.7	28.3	*	*	*	29.25	0	29.25
Totals	361.4	718.6	729	738.1	289	0	0			
% of Total	12.74%	25.34%	25.7%	26.03%	10.19%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	24	27	33	29	*	*	28.25	0	28.25
1 - 2	*	29	29	29	26	*	*	28.25	0	28.25
2 - 3	*	28	32	28	23	*	*	27.75	0	27.75
3 - 4	*	34	31	40	33	*	*	34.5	0	34.5
4 - 5	*	26	30	31	31	*	*	29.5	0	29.5
5 - 6	*	28	30	28	28	*	*	28.5	0	28.5
6 - 7	*	29	30.7	30	29.8	*	*	29.88	0	29.88
7 - 8	*	28.6	27.8	29.7	29.8	*	*	28.98	0	28.98
8 - 9	*	29.4	30.7	31.2	30.6	*	*	30.48	0	30.48
9 - 10	*	30.4	32.1	30	28.8	*	*	30.32	0	30.32
10 - 11	*	29.4	29.6	30.8	*	*	*	29.93	0	29.93
11 - 12	*	29.5	29.7	30.4	*	*	*	29.87	0	29.87
12 - 13	30.8	31.3	31	29.7	*	*	*	30.7	0	30.7
13 - 14	30.4	30.2	31	30.2	*	*	*	30.45	0	30.45
14 - 15	27	29.6	28.8	28.7	*	*	*	28.52	0	28.52
15 - 16	31.8	31.8	31	31.4	*	*	*	31.5	0	31.5
16 - 17	30.7	31.9	30.2	31.9	*	*	*	31.17	0	31.17
17 - 18	31.7	32.7	32.3	32.7	*	*	*	32.35	0	32.35
18 - 19	31.9	32	32.4	32.5	*	*	*	32.2	0	32.2
19 - 20	30.6	33	30.8	32.3	*	*	*	31.68	0	31.68
20 - 21	29.5	28.8	31.2	31	*	*	*	30.12	0	30.12
21 - 22	31	30	29	30.3	*	*	*	30.08	0	30.08
22 - 23	29	30	32	28	*	*	*	29.75	0	29.75
23 - 24	27	32	29.7	28.3	*	*	*	29.25	0	29.25

Summary of Violators

Birchwood Loop northwest northwest 9-16 to 9-20 time from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM
correct

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	20	27.8	1	46.0
01:00:00	13	26.7	0	0.0
02:00:00	19	24.4	0	0.0
03:00:00	10	30.2	0	0.0
04:00:00	28	27.8	0	0.0
05:00:00	51	26.7	0	0.0
06:00:00	269	27.0	0	0.0
07:00:00	420	25.5	0	0.0
08:00:00	396	26.7	0	0.0
09:00:00	350	26.5	0	0.0
10:00:00	254	26.8	0	0.0
11:00:00	305	26.7	0	0.0
12:00:00	364	27.1	0	0.0
13:00:00	459	27.0	0	0.0
14:00:00	414	24.1	0	0.0
15:00:00	470	27.5	1	49.0
16:00:00	557	28.0	0	0.0
17:00:00	580	28.7	0	0.0
18:00:00	356	28.3	0	0.0
19:00:00	271	27.8	0	0.0
20:00:00	183	27.0	0	0.0
21:00:00	109	26.5	0	0.0
22:00:00	58	27.1	0	0.0
23:00:00	34	25.8	0	0.0

Outgoing Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	1	3	16	38	21	0	0	0	0	0	0	0	0	0	0	0	0	0	79	26.7	30.8	22 to 32	86.1	*	*	1	74	4	
9/16/2024	13:00	0	2	26	70	35	2	0	0	0	0	0	0	0	0	0	0	0	0	135	27.2	30.4	21 to 31	88.9	*	*	0	121	14	
9/16/2024	14:00	0	18	56	28	9	0	0	0	0	0	0	0	0	0	0	0	0	0	111	23.3	27	18 to 28	85.6	*	*	1	98	12	
9/16/2024	15:00	0	1	24	55	28	5	1	1	0	0	0	0	0	0	0	0	0	0	115	27.8	31.8	23 to 33	80.9	1	0.9	0	108	7	
9/16/2024	16:00	0	2	28	56	34	4	1	0	0	0	0	0	0	0	0	0	0	0	125	27.5	30.7	21 to 31	82.4	*	*	0	111	14	
9/16/2024	17:00	0	2	15	77	36	10	0	0	0	0	0	0	0	0	0	0	0	0	140	28.1	31.7	23 to 33	86.4	*	*	0	136	4	
9/16/2024	18:00	0	2	12	50	26	4	0	0	0	0	0	0	0	0	0	0	0	0	94	27.9	31.9	24 to 34	83.0	*	*	1	84	9	
9/16/2024	19:00	0	3	16	23	12	1	0	0	0	0	0	0	0	0	0	0	0	0	55	26.5	30.6	22 to 32	85.5	*	*	0	53	2	
9/16/2024	20:00	0	4	9	18	7	0	0	0	0	0	0	0	0	0	0	0	0	0	38	26.3	29.5	22 to 32	86.8	*	*	0	37	1	
9/16/2024	21:00	0	1	5	13	2	2	0	0	0	0	0	0	0	0	0	0	0	0	23	26.8	31	22 to 32	87.0	*	*	0	22	1	
9/16/2024	22:00	0	1	5	8	3	0	0	0	0	0	0	0	0	0	0	0	0	0	17	26	29	20 to 30	88.2	*	*	0	16	1	
9/16/2024	23:00	0	1	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	24.5	27	18 to 28	100.0	*	*	0	6	0	
24 Hr Summary		1	40	213	440	213	28	2	1	0	0	0	0	0	0	0	0	0	0	938	26.9	31	22 to 32	81.8	1	0.1	3	866	69	

Outgoing Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/17/2024	00:00	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	23.8	24	18 to 28	100.0	*	*	0	4	0
9/17/2024	01:00	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	25.3	29	19 to 29	100.0	*	*	0	3	0
9/17/2024	02:00	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26.7	28	18 to 28	100.0	*	*	0	3	0
9/17/2024	03:00	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	29	34	24 to 34	100.0	*	*	0	3	0
9/17/2024	04:00	0	0	1	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	4	28.3	26	16 to 26	75.0	*	*	0	3	1
9/17/2024	05:00	0	0	2	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	10	26.1	28	21 to 31	100.0	*	*	0	9	1
9/17/2024	06:00	0	2	14	44	10	1	0	0	0	0	0	0	0	0	0	0	0	0	71	26.5	29	22 to 32	90.1	*	*	0	68	3
9/17/2024	07:00	0	9	43	45	14	0	0	0	0	0	0	0	0	0	0	0	0	0	111	25	28.6	19 to 29	83.8	*	*	0	103	8
9/17/2024	08:00	1	9	32	42	13	5	0	0	0	0	0	0	0	0	0	0	0	0	102	25.6	29.4	20 to 30	79.4	*	*	0	84	18
9/17/2024	09:00	0	5	29	46	23	2	0	0	0	0	0	0	0	0	0	0	0	0	105	26.3	30.4	22 to 32	82.9	*	*	1	88	16
9/17/2024	10:00	0	0	23	58	16	2	0	0	0	0	0	0	0	0	0	0	0	0	99	26.8	29.4	21 to 31	92.9	*	*	0	91	8
9/17/2024	11:00	0	8	25	52	13	3	0	0	0	0	0	0	0	0	0	0	0	0	101	26.2	29.5	22 to 32	85.1	*	*	0	92	9
9/17/2024	12:00	0	1	27	40	21	4	1	0	0	0	0	0	0	0	0	0	0	0	94	27.3	31.3	23 to 33	80.9	*	*	0	88	6
9/17/2024	13:00	0	6	32	54	24	2	1	0	0	0	0	0	0	0	0	0	0	0	119	26.7	30.2	21 to 31	84.0	*	*	0	110	9
9/17/2024	14:00	1	11	34	27	14	3	0	0	0	0	0	0	0	0	0	0	0	0	90	24.8	29.6	19 to 29	73.3	*	*	0	83	7
9/17/2024	15:00	0	0	34	48	28	5	0	0	0	0	0	0	0	0	0	0	0	0	115	27.4	31.8	21 to 31	79.1	*	*	1	103	11
9/17/2024	16:00	0	3	22	76	41	10	0	0	0	0	0	0	0	0	0	0	0	0	152	28.2	31.9	23 to 33	84.2	*	*	0	148	4
9/17/2024	17:00	0	1	19	63	49	11	2	0	0	0	0	0	0	0	0	0	0	0	145	29	32.7	24 to 34	81.4	*	*	0	139	6
9/17/2024	18:00	1	1	6	36	26	4	0	0	0	0	0	0	0	0	0	0	0	0	74	28.6	32	23 to 33	85.1	*	*	0	72	2
9/17/2024	19:00	0	1	7	37	17	6	1	0	0	0	0	0	0	0	0	0	0	0	69	28.6	33	24 to 34	84.1	*	*	0	64	5
9/17/2024	20:00	0	1	6	21	3	1	0	0	0	0	0	0	0	0	0	0	0	0	32	26.5	28.8	23 to 33	90.6	*	*	0	31	1
9/17/2024	21:00	0	0	6	14	7	0	0	0	0	0	0	0	0	0	0	0	0	0	27	26.9	30	21 to 31	88.9	*	*	0	26	1
9/17/2024	22:00	0	0	3	7	2	1	0	0	0	0	0	0	0	0	0	0	0	0	13	27.4	30	20 to 30	84.6	*	*	0	12	1
9/17/2024	23:00	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	28.7	32	22 to 32	100.0	*	*	0	3	0
24 Hr Summary		3	59	369	728	324	61	5	0	0	0	0	0	0	0	0	0	0	0	1549	27	31	22 to 32	80.1	*	*	2	1430	117

Outgoing Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/18/2024	00:00	0	0	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	26.2	27	19 to 29	100.0	*	*	0	5	0
9/18/2024	01:00	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	0	0	0	4	30	29	25 to 35	100.0	*	*	0	4	0
9/18/2024	02:00	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	27.7	32	22 to 32	100.0	*	*	0	3	0
9/18/2024	03:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26.7	31	21 to 31	100.0	*	*	0	3	0
9/18/2024	04:00	0	0	3	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	11	27.4	30	22 to 32	100.0	*	*	0	10	1
9/18/2024	05:00	0	0	3	10	3	0	0	0	0	0	0	0	0	0	0	0	0	0	16	27	30	22 to 32	93.8	*	*	0	16	0
9/18/2024	06:00	0	1	14	41	16	1	2	0	0	0	0	0	0	0	0	0	0	0	75	27.5	30.7	22 to 32	92.0	*	*	0	72	3
9/18/2024	07:00	0	6	31	52	6	0	0	0	0	0	0	0	0	0	0	0	0	0	95	25.2	27.8	20 to 30	92.6	*	*	0	90	5
9/18/2024	08:00	0	2	32	38	23	4	0	0	0	0	0	0	0	0	0	0	0	0	99	26.6	30.7	21 to 31	78.8	*	*	0	92	7
9/18/2024	09:00	0	2	26	39	19	7	0	0	0	0	0	0	0	0	0	0	0	0	93	27.1	32.1	20 to 30	76.3	*	*	0	85	8
9/18/2024	10:00	0	2	15	40	14	0	0	0	0	0	0	0	0	0	0	0	0	0	71	26.4	29.6	21 to 31	87.3	*	*	0	63	8
9/18/2024	11:00	0	4	25	46	13	3	0	0	0	0	0	0	0	0	0	0	0	0	91	26.5	29.7	22 to 32	83.5	*	*	0	84	7
9/18/2024	12:00	0	1	14	42	24	3	0	0	0	0	0	0	0	0	0	0	0	0	84	27.8	31	24 to 34	88.1	*	*	0	79	5
9/18/2024	13:00	0	0	25	54	26	3	1	0	0	0	0	0	0	0	0	0	0	0	109	27.4	31	23 to 33	89.9	*	*	0	104	5
9/18/2024	14:00	0	14	41	35	9	4	0	0	0	0	0	0	0	0	0	0	0	0	103	24.5	28.8	19 to 29	80.6	*	*	0	93	10
9/18/2024	15:00	0	7	28	49	27	7	1	0	0	0	0	0	0	0	0	0	0	0	119	27.3	31	23 to 33	81.5	*	*	0	110	9
9/18/2024	16:00	0	1	19	67	27	4	0	0	0	0	0	0	0	0	0	0	0	0	118	27.7	30.2	23 to 33	89.0	*	*	0	110	8
9/18/2024	17:00	0	3	21	78	45	10	3	0	0	0	0	0	0	0	0	0	0	0	160	28.5	32.3	24 to 34	83.1	*	*	0	149	11
9/18/2024	18:00	0	1	12	49	26	6	1	0	0	0	0	0	0	0	0	0	0	0	95	28.5	32.4	23 to 33	83.2	*	*	0	89	6
9/18/2024	19:00	0	2	8	44	22	3	0	0	0	0	0	0	0	0	0	0	0	0	79	27.9	30.8	24 to 34	86.1	*	*	0	73	6
9/18/2024	20:00	0	0	18	36	23	2	0	0	0	0	0	0	0	0	0	0	0	0	79	27.6	31.2	23 to 33	86.1	*	*	0	77	2
9/18/2024	21:00	0	2	4	9	3	0	0	0	0	0	0	0	0	0	0	0	0	0	18	25.7	29	20 to 30	83.3	*	*	0	16	2
9/18/2024	22:00	0	0	2	8	4	2	0	0	0	0	0	0	0	0	0	0	0	0	16	28.8	32	22 to 32	87.5	*	*	0	16	0
9/18/2024	23:00	1	0	2	8	4	0	0	0	0	0	0	0	0	0	0	0	0	0	15	26.1	29.7	21 to 31	93.3	*	*	0	14	1
24 Hr Summary		1	48	345	760	339	60	8	0	0	0	0	0	0	0	0	0	0	0	1561	27.1	31	22 to 32	81.7	*	*	0	1457	104

Outgoing Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/19/2024	00:00	0	0	0	2	2	0	0	1	0	0	0	0	0	0	0	0	0	0	5	32.6	33	23 to 33	80.0	1	20.0	0	5	0
9/19/2024	01:00	0	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	25.3	29	20 to 30	100.0	*	*	0	3	1
9/19/2024	02:00	0	1	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	24.2	28	18 to 28	88.9	*	*	0	9	0
9/19/2024	03:00	0	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	3	34	40	26 to 36	66.7	*	*	0	2	1
9/19/2024	04:00	0	0	1	4	3	0	0	0	0	0	0	0	0	0	0	0	0	0	8	28	31	22 to 32	100.0	*	*	0	8	0
9/19/2024	05:00	0	0	3	10	2	0	0	0	0	0	0	0	0	0	0	0	0	0	15	26.9	28	22 to 32	100.0	*	*	0	14	1
9/19/2024	06:00	0	1	18	34	18	2	0	0	0	0	0	0	0	0	0	0	0	0	73	27.2	30	23 to 33	87.7	*	*	0	68	5
9/19/2024	07:00	0	7	36	49	15	3	0	0	0	0	0	0	0	0	0	0	0	0	110	25.7	29.7	19 to 29	81.8	*	*	0	104	6
9/19/2024	08:00	0	6	13	60	29	5	1	0	0	0	0	0	0	0	0	0	0	0	114	27.7	31.2	23 to 33	84.2	*	*	0	105	9
9/19/2024	09:00	0	0	24	44	14	3	0	0	0	0	0	0	0	0	0	0	0	0	85	26.8	30	23 to 33	89.4	*	*	0	77	8
9/19/2024	10:00	0	3	18	35	28	0	0	0	0	0	0	0	0	0	0	0	0	0	84	27.2	30.8	22 to 32	84.5	*	*	0	77	7
9/19/2024	11:00	0	3	20	63	22	5	0	0	0	0	0	0	0	0	0	0	0	0	113	27.3	30.4	21 to 31	83.2	*	*	0	105	8
9/19/2024	12:00	1	3	22	60	19	2	0	0	0	0	0	0	0	0	0	0	0	0	107	26.6	29.7	23 to 33	86.9	*	*	0	100	7
9/19/2024	13:00	0	3	28	43	20	2	0	0	0	0	0	0	0	0	0	0	0	0	96	26.5	30.2	22 to 32	87.5	*	*	0	90	6
9/19/2024	14:00	0	19	47	30	12	2	0	0	0	0	0	0	0	0	0	0	0	0	110	24.1	28.7	19 to 29	77.3	*	*	0	104	6
9/19/2024	15:00	0	3	31	49	32	4	2	0	0	0	0	0	0	0	0	0	0	0	121	27.4	31.4	23 to 33	86.0	*	*	0	116	5
9/19/2024	16:00	0	2	24	76	48	9	3	0	0	0	0	0	0	0	0	0	0	0	162	28.3	31.9	23 to 33	83.3	*	*	0	153	9
9/19/2024	17:00	0	1	14	63	45	12	0	0	0	0	0	0	0	0	0	0	0	0	135	29	32.7	23 to 33	83.7	*	*	0	129	6
9/19/2024	18:00	0	4	15	40	28	5	1	0	0	0	0	0	0	0	0	0	0	0	93	28.2	32.5	23 to 33	78.5	*	*	0	90	3
9/19/2024	19:00	0	0	18	29	16	5	0	0	0	0	0	0	0	0	0	0	0	0	68	27.8	32.3	22 to 32	82.4	*	*	0	66	2
9/19/2024	20:00	0	3	6	15	10	0	0	0	0	0	0	0	0	0	0	0	0	0	34	26.7	31	22 to 32	82.4	*	*	0	34	0
9/19/2024	21:00	0	0	10	23	7	1	0	0	0	0	0	0	0	0	0	0	0	0	41	26.5	30.3	21 to 31	90.2	*	*	0	38	3
9/19/2024	22:00	0	0	5	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	12	25.9	28	21 to 31	100.0	*	*	0	12	0
9/19/2024	23:00	0	1	2	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	25.2	28.3	19 to 29	90.0	*	*	0	9	1
24 Hr Summary		1	60	362	746	373	61	8	1	0	0	0	0	0	0	0	0	0	0	1612	27.1	32	23 to 33	81.2	1	0.1	0	1518	94

Outgoing Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/20/2024	00:00	0	1	1	3	0	0	1	0	0	0	0	0	0	0	0	0	0	0	6	27.7	29	19 to 29	66.7	*	*	0	6	0	
9/20/2024	01:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25	26	16 to 26	100.0	*	*	0	2	0	
9/20/2024	02:00	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	20.5	23	14 to 24	100.0	*	*	0	3	1	
9/20/2024	03:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	33	33	23 to 33	100.0	*	*	0	1	0	
9/20/2024	04:00	0	0	2	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	5	28	31	21 to 31	80.0	*	*	0	5	0	
9/20/2024	05:00	0	0	0	10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	26.4	28	19 to 29	100.0	*	*	0	9	1	
9/20/2024	06:00	0	2	10	26	12	0	0	0	0	0	0	0	0	0	0	0	0	0	50	26.8	29.8	22 to 32	92.0	*	*	0	49	1	
9/20/2024	07:00	0	5	32	47	16	4	0	0	0	0	0	0	0	0	0	0	0	0	104	26.2	29.8	21 to 31	82.7	*	*	0	100	4	
9/20/2024	08:00	0	2	18	42	19	0	0	0	0	0	0	0	0	0	0	0	0	0	81	26.8	30.6	23 to 33	90.1	*	*	0	71	10	
9/20/2024	09:00	0	5	19	34	7	2	0	0	0	0	0	0	0	0	0	0	0	0	67	25.7	28.8	22 to 32	85.1	*	*	0	59	8	
9/20/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	17	85	164	57	6	1	0	0	0	0	0	0	0	0	0	0	0	330	26.3	30	22 to 32	85.2	*	*	0	305	25	

Outgoing: Average Hourly Volume for Week of 9/16/2024

Average Counts By Hour (9/16/2024) — Average Counts By Hour (9/16/2024)

Birchwood Loop northwest northwest 9-16 to 9-20 time correct
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/16/2024
Average Hourly WEEKDAY Speeds By Hour (9/16/2024) — Average Hourly WEEKDAY Speeds By Hour (9/16/2024)

Birchwood Loop northwest northwest 9-16 to 9-20 time correct
Average Weekly Speeds

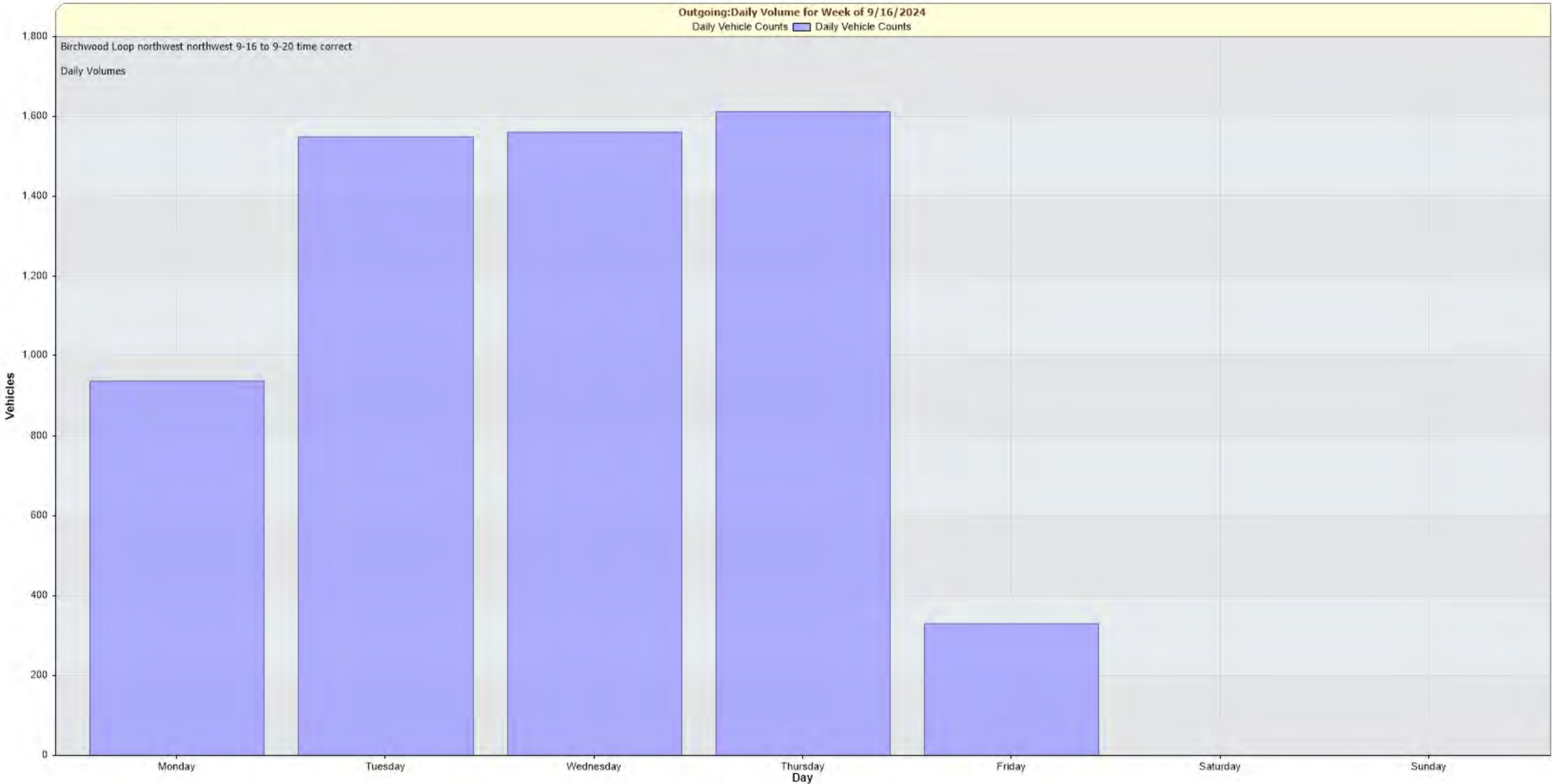


Outgoing:Daily Volume for Week of 9/16/2024

Daily Vehicle Counts

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

Daily Volumes



Appendix B

Grading and Drainage Study

EKLUTNA NATIVE VILLAGE GAMING FACILITY PROJECT

CHUGIAK, ALASKA

Grading and Drainage Study

Final Submission

August 30, 2024



Prepared By:

EE ENTERPRISE
ENGINEERING, INC.

3601 C St., Suite 650, Anchorage, Alaska 99503

Kyle Markel, PE – Civil Engineer
Carl Bassler, PE – Principal, Civil Engineer

EI Project No. 10336
Acorn Project No. 2415

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APPENDICES

Concept Site Plan and Existing Site Topography.....	Appendix A
Concept Grading and Drainage Sketch	Appendix B
Pre-Development Runoff Calculation	Appendix C
Stormwater Infiltration Calculation and Basins Exhibit.....	Appendix D
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1.0 PROJECT INTRODUCTION

1.1 Project description

The proposed project development includes a new 58,000 SF gaming facility building, with supporting utilities, paved parking, driveways, and landscaping. The purpose of this Grading and Drainage Study is to analyze the existing topography and drainage patterns, develop a conceptual grading and drainage design, investigate potential stormwater management strategies, and evaluate potential drainage impacts for the proposed site development.



Figure 1: Concept Site Plan

2.0 EXISTING SITE CONDITIONS

2.1 Existing Conditions

The project site is located about 25 miles north of Anchorage, off Birchwood Spur Road in Chugiak, Alaska. The 6.4-acre property includes three lots which are heavily vegetated with mature deciduous and coniferous trees and thick underbrush, and formerly one small (roughly 1,000 SF) single story house, which has been demolished. The topography mostly slopes gradually down to the north. The highest portion of the site is the southeast corner at elevation 88, and the lowest part of the site is in the northeast corner at elevation 73. There is a bench cutting across the northeast corner of the property that drops about 8 feet and then flattens out toward the creek bank. Aside from the steep slope along the bench, typical slopes across the site are generally around 1% to 3%. Peters Creek runs along the east side of the site and is ultimately where the stormwater, that does not infiltrate, discharges. A

topographic survey was not performed for this study, however LiDAR contours from the Municipality of Anchorage (MOA) Geographic Data Information Center provide sufficient information to understand the general drainage patterns. An exhibit of the existing site topography can be found in Appendix A.

The United States Geological Survey (USGS) published a generalized geologic map of the Eagle River-Birchwood Area in 1971 which shows the project site to be in a geological area classified as “F” defined as “Large alluvial-fan deposits. Gravel and sand; cobbles and small boulders common; small amounts of silt. Good water-bearing unit; may yield large quantities of water to wells.” Though this information comes from a generalization of large areas, it is a good indication that gravel and sand can be anticipated beneath the top overburden layer.

A preliminary geotechnical investigation was performed in July of 2024 which included visual observation of four test pits and three percolation tests. The following conditions were noted:

A surface layer of vegetation and other organics about 6 inches thick was observed in all the test pits. Beneath the organics is a Sandy Silt layer, extending to a depth of 1 to 3.5 feet below ground surface (bgs). A Sandy Gravel material underlies these surface layers in all pits, and extends to the bottom of the test pits, 6 to 12.5 feet bgs. This granular soil is moist, and medium dense to dense. The maximum particle size ranges from 2 inches to large cobbles and boulders over 1.5 feet in diameter. Groundwater was encountered at 9 to 12.5 ft below ground surface (bgs) in three of the four pits. The test pit in the northeast corner of the site encountered groundwater at 6 ft bgs which was not surprising considering the much lower elevation and close proximity to Peters Creek. Because of the high water table encountered in the northeast test pit, a percolation test was not performed.

3.0 GRADING AND EARTHWORK

3.1 Conceptual Grading

A concept grading plan was developed and can be found in Appendix B. The developer’s desire to minimize the length and height of retaining walls was factored into the grading design, however the site plan is laid out such that the pavement limits are within a couple feet of the property line across much of the site to maximize parking spaces. General grading parameters for the site include the following:

- Positive slope away from the building.
- Matching existing grades at both driveway locations.
- Asphalt Parking Lot Slopes: 1.5% - 4.5%
- Unpaved slopes not to exceed 3H:1V.

The current site layout and concept grading will necessitate retaining walls on the north and east side of the property. The northwest, southwest, and southeast corners of the site will closely match existing grades, however the northeast corner of the site will need to be raised by 3 to 7 feet above existing grades as the northeast portion of the site has a steep drop off of about 7 to 10 feet towards the east and Peters Creek. Retaining walls will likely not be needed along the west and south property lines as the proposed grades are very close to existing grades.

The finished grade along the north and east sides of the building was dropped about 4 feet below the building finished floor. This lowered area is intended to facilitate the truck loading bay area, reduce the amount of fill required for the northeast corner of the site, and minimize the retaining wall heights. The disadvantage of the lowered grade is that staircases (about 6 steps) will be required to access the

doorways on those sides of the building. It is understood that these doorways do not serve as main entrances and would be used much less frequently.

Grading for the west half of the proposed site will closely follow the slope of the existing site towards a low point near the northwest corner. The east half of the site will slope towards the northeast corner of the site. At this conceptual level of site grading, the finished floor elevation for the building is set at elevation 84. This will allow for positive drainage away from the building while still minimizing the fill quantities as much as possible. The southeast corner of the site will be the highest elevation (matching existing grades) and will be above the finished floor of the building. Drainage will sheet flow toward the building but will be intercepted by a grade break or curb and gutter along the south side of the building to direct the drainage around the building to the north. Storm drain catch basins will be located at low points across the site with storm drain pipes connecting them and directing runoff to the buried infiltration beds. The concept grading and drainage sketch can be found in Appendix B.

3.2 Earthwork Quantities

It is assumed that the top 1 foot of soil will need to be stripped across the entire site to remove tree roots, organics, and topsoil/peat. Stripping 1 foot over the entire property (6.42 acres) equates to 10,350 cubic yards of unusable excavation that will need to be hauled off-site.

The assumed pavement section for this site is 2.5 ft thick to include pavement and classified fill. The silty layer of soil encountered onsite in the top couple feet does not meet the requirements for classified fill and must be removed where it is within the new pavement section. This results in about 15,800 CY of poor silty material to be excavated. Of that, 4,200 CY can be reused across the site to fill the lower areas. This results in a net volume of 11,600 CY of unusable excavation. 20,500 CY of classified material will be imported to the site for the pavement section. It is possible that a 'mining' approach could be used to reduce the quantity of imported classified fill, where classified fill is obtained from on-site excavations, and replaced with excess silty material.

It is assumed the building section will include classified material to 3.5 feet below finished floor as necessary to remove the shallow silty layer. This will result in an additional 5,400 CY of excavation under building after site stripping, which is all unusable excavation. 6,500 CY of classified material will then be imported to the site for the building section.

During design, the cut/fill calculations and sections would be more accurately defined, and grading could be adjusted, if possible, to minimize this additional fill that would be needed. Depending on the results of the future geotechnical investigation, the amount of usable excavation could change. Additionally, where the subgrade below the pavement section is a silty material, a geotextile is recommended to prevent silt from migrating up into the gravel fill.

A breakdown of estimated earthwork and pavement quantities is as follows:

- Unclassified Fill – 4,200 cubic yards (reused material from site excavations)
- Classified Fill (aggregate base course, structural fill, etc.) – 27,000 cubic yards
- Concrete (sidewalks, curbs, building floor slab) – 1,150 cubic yards
- Asphalt (2" thick) – 1,100 cubic yards
- Topsoil – 500 cubic yards

3.3 Flooding

With Peters Creek running along the east side of the property, flooding potential must be considered for this site. The Federal Emergency Management Agency (FEMA) administers the National Flood Insurance Program, which includes mapping floodplains and delineating boundaries of major rivers for different design storm events. FEMA publishes Flood Insurance Rate Maps (FIRM) which identify flood hazard areas including the floodway, and base flood (100-year) and 500-year flood limits. The FIRM for the project site is included in Appendix E. Figure 2 below shows an approximate overlay of the proposed building footprint and property lines on the FIRM.



Figure 2: Proposed Site in Relation to FEMA Floodplains

The critical items to note for this project are that the building is set at elevation 84, and all other proposed site development is outside of the 100-year floodplain. Also, the current site design avoids the placement of any fill or retaining walls within the 100-year floodplain.

FEMA typically reviews developments that include placement of fill within the Special Flood Hazard Area (SFHA), which is identified by the blue and blue/red striped areas in Figure 2. More precise mapping with the FIRM would be beneficial in design but it is expected that the project would only place fill and retaining walls within the brown zone and not in the SFHA. The brown zone (Zone X) represents a 0.2% annual chance (500-year) of flood hazard. Refer to the FIRM in Appendix E for more detail.

4.0 DRAINAGE (HYDROLOGY AND HYDRAULICS)

4.1 Methodology

In the Municipality of Anchorage (MOA), site peak stormwater runoff from the post-development condition is typically designed to be less than or equal to 1.05 times the pre-development peak flow. For this project, the amount of impervious surface area proposed for the site is substantially more than the existing pre-development condition, which will result in additional stormwater runoff. Onsite subsurface infiltration will be the main method of reducing stormwater runoff from the site. Infiltration can be accomplished in various ways, including vegetated swales, dry wells, infiltration basins, perforated storm drain pipes, or a combination thereof. Stormwater detention was also considered as an option for this site, however it would likely be less cost effective than infiltration.

For the purposes of this study, we have calculated a total quantity of stormwater to be infiltrated and used an infiltration rate of 1 minute per inch based on initial percolation tests. The type and configuration of infiltration structures and methods will be dependent on the final site layout and must be ultimately determined in the design phase. Further hydraulic analysis of the site and stormwater system would be performed during design and could likely incorporate detention methods with infiltration methods and potentially decrease the footprint of the stormwater infrastructure.

4.2 Results of Analysis

The Rational Method was used to analyze the stormwater runoff for the pre-development condition and a 24-hour stormwater modeling calculation was used for the post-development condition. To be conservative, the 100-year 24-hour storm event was used as the design storm. The Anchorage Stormwater Manual (ASM) provides design guidance for stormwater runoff within the MOA, including design storm depths with location adjustment factors. The total rainfall depth for the 100-year, 24-hour design storm event is 3.59 inches.

Pre-Development Condition:

The Rational Method for peak stormwater runoff states that the flowrate is equal to the runoff coefficient multiplied by the peak rainfall intensity, multiplied by the contributing area. The pre-developed site was analyzed as a single 6.42-acre drainage basin of forest with minimal slopes which results in a runoff coefficient of 0.20. The peak rainfall intensity factor, adjusted for the site location, is 0.75 inches per hour. The result for the pre-development peak stormwater runoff for the 100-year storm event is 0.96 cfs. The full pre-development runoff calculation can be found in Appendix C.

Post-Development Condition:

The site post-development condition was analyzed for the amount of stormwater runoff produced in the 100-year storm event. Because the existing site is so heavily forested, the pre-development runoff is very low compared to the post-development condition. It is probable that some stormwater could run off the site, likely up to 5% more than the amount calculated in the pre-development condition. However, considering the small amount of pre-development runoff and for the intent of being conservative at this stage of the project, stormwater infiltration systems were sized to infiltrate the entire 100-year design storm event.

The site was split into two different basins, Basin A (west side) and Basin B (east side) and analyzed for the 100-year storm event. Runoff coefficients for the different land cover surfaces were incorporated per the proposed concept site plan. An infiltration rate of 60 inches per hour was used for the

infiltration system founded in the in-situ soils, correlating with the percolation tests performed in July of 2024. The calculation resulted in an infiltration field or “trench” size of 2,400 sf by 3 ft deep for Basin A, and 1,600 sf by 3 ft deep for Basin B. The full infiltration calculation can be found in Appendix D.

The proposed stormwater infiltration system would include rows of buried half round pipe or chambers that would distribute the stormwater over a bed of well-draining rock material. The system would be designed to withstand vehicle traffic loading and be installed beneath the paved parking lot, however the first chamber is typically designed to be accessible for periodically removing sediment. The system would be designed so that an overflow point would be near the low end of the site. If the infiltration bed backed up, drainage would overflow offsite at an elevation lower than the building finished floor.

As an alternative to an infiltration system, a stormwater detention system could be provided, designed to limit the peak offsite flowrate to no greater than the predevelopment peak. The detention storage would be provided by buried pipes or chambers, rather than a pond, given the site limitations. Preliminary calculations were performed for a stormwater detention system. In general, the detention system would require 4,000 – 5,000 linear feet of 24-inch diameter pipe to store the site stormwater for the 10-year storm event and allow controlled discharge similar to the peak pre-development rate. Larger diameter pipe would decrease the total length of pipe required, however there is a limit on how deep the pipe can be buried as it has to gravity drain to a discharge point near the low end of the site. Based on preliminary calculations, a stormwater detention system for this site would be more costly than an infiltration system.

If additional stormwater treatment is desired, sediment traps and grit chambers can be provided upstream of infiltration or detention systems, to remove detrimental levels of sediment and other debris. These measures can increase the level of treatment the system provides and facilitate maintenance. An operations and maintenance plan should also be considered for the facility to include bi-annual sweeping of the paved surfaces, especially in the springtime, to remove sand/gravel spread for traction throughout the winter months. Storm drain inlet structures should also be routinely cleaned out to further reduce the amount of sediment and debris migrating to the infiltration or detention system. Pretreatment and regular maintenance is recommended as it will extend the life of the stormwater system.

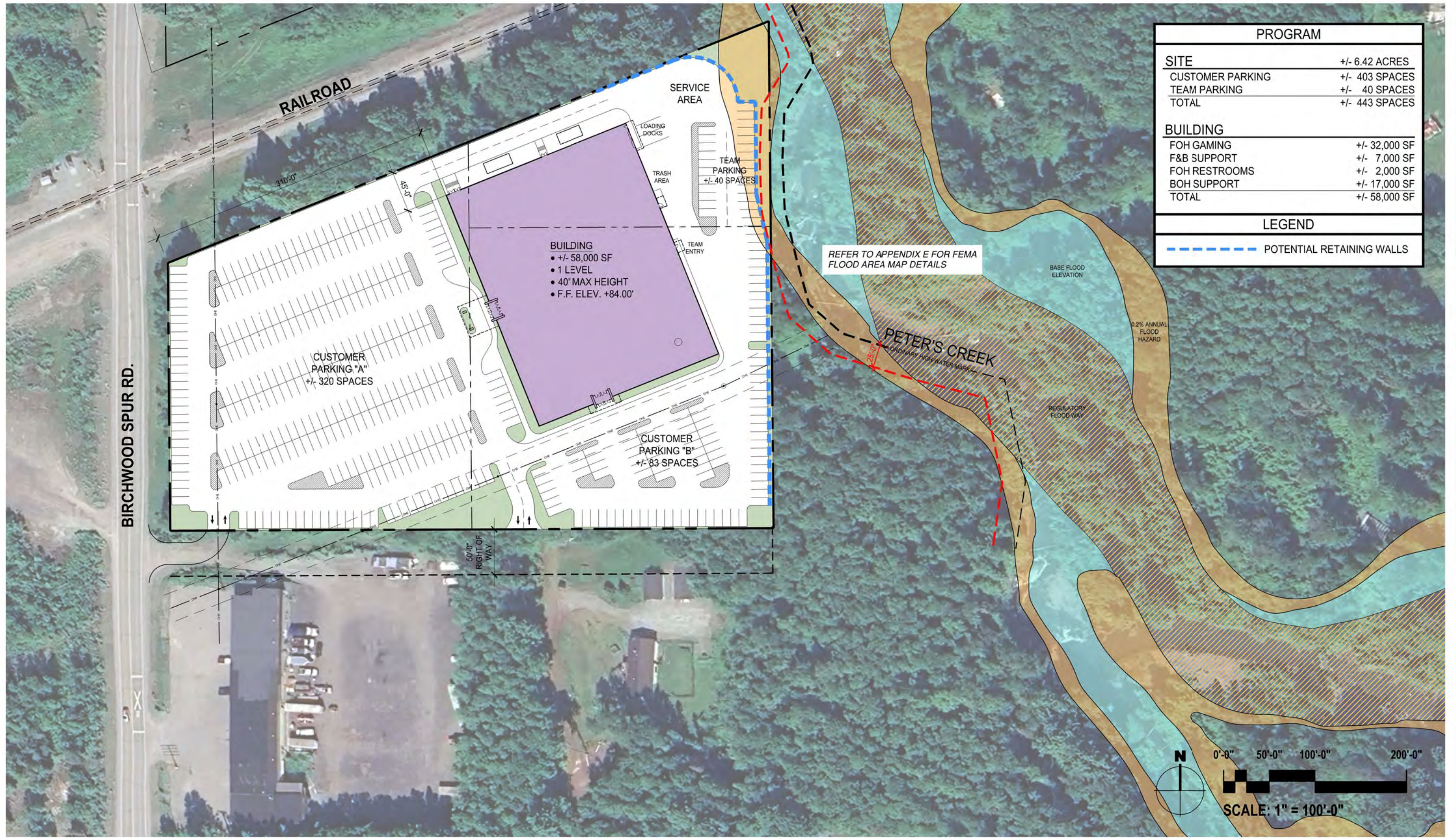
5.0 REFERENCES

Anchorage Stormwater Manual (ASM), Volume 1 Management and Design Criteria, Version 1.0 December 2017, Municipality of Anchorage (MOA).

Federal Emergency Management Agency (FEMA), July 8, 2020

APPENDIX A

1. Concept Site Plan
2. Existing Site Topography



PROGRAM	
SITE	+/- 6.42 ACRES
CUSTOMER PARKING	+/- 403 SPACES
TEAM PARKING	+/- 40 SPACES
TOTAL	+/- 443 SPACES
BUILDING	
FOH GAMING	+/- 32,000 SF
F&B SUPPORT	+/- 7,000 SF
FOH RESTROOMS	+/- 2,000 SF
BOH SUPPORT	+/- 17,000 SF
TOTAL	+/- 58,000 SF
LEGEND	
	POTENTIAL RETAINING WALLS

EKLUTNA GAMING HALL - SITE PLAN STUDY
 CONCEPT SITE PLAN



A1

EKLUTNA GAMING HALL - EXISTING LIDAR CONTOURS

SCALE: 1" = 40'

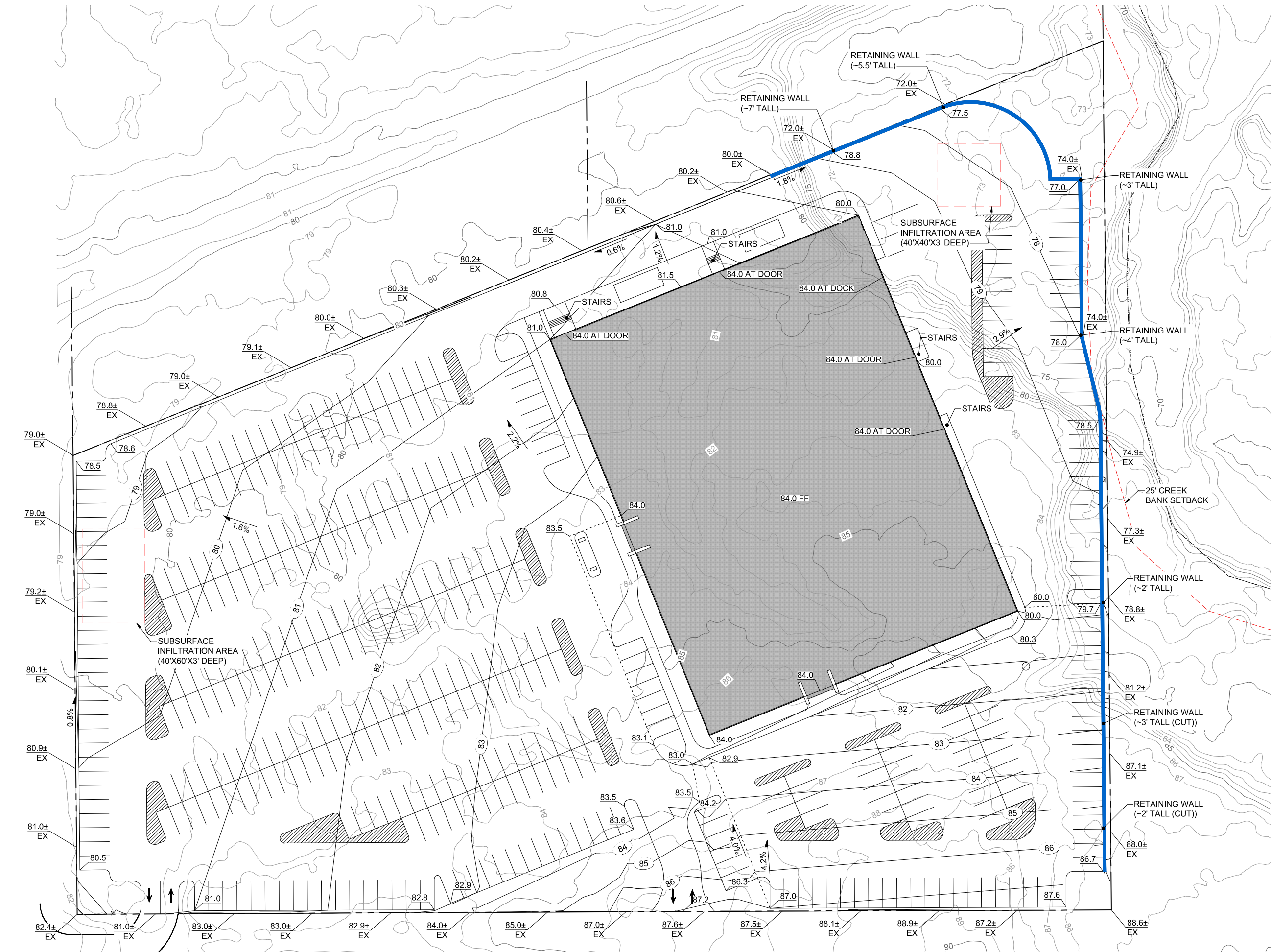


APPENDIX B

Concept Grading and Drainage Sketch

NOTES

- 1. STORM DRAIN SYSTEM NOT SHOWN.



APPENDIX C

Pre-Development Runoff Calculation

DRAINAGE CALCULATION WORKSHEET
Pre-development, 100 yr

Date: 7-Jul-24

By: RPP/KIM

Project Name: Eklutna Native Village Gaming Hall - Grading and Drainage Study EEI Project No.: 10336

DRAINAGE BASIN

On-Site Drainage Basin		
Subbasin I.D.	Area, sf	Area, ac
Basin	279,696	6.42
Totals	279,696	6.42

Calculated weighted C value per MOA ASM Vol. 1 Table 4.4-2

Basin	Characterization	Area, acres	% Area	Avg HSG	Avg Slope	C (Factor)	C (Weighted)
Forest, Brush		279,696	6.42	100.0%	D	2-6%	0.20
Totals		279,696	6.42	100%			0.200

TIME OF CONCENTRATION

Basin
 Total Length (ft) 800

Overland Flow: $T_c = 0.225(L^{0.42})(S^{-0.19})(C^{-1.0})$

Length, L (ft) 150
 Average Slope, S (ft/ft) 0.012
 Runoff Coefficient, C 0.160
 Flow Time, T_c (min) 26.83

Shallow Concentrated Flow: $T = L/33kS^{0.5}$

Length, L (ft) 650
 Average Slope, S (ft/ft) 0.012
 Surface type forest
 k value 0.076
 Flow Time, T (min) 39.82

Time of Concentration, D (min): 66.65

RAINFALL INTENSITY

The peak rainfalls are interpolated from Fig. 4.2-1, MOA ASM Vol. 1, IDF Values. This storm is adjusted by the Orographic Factor, adjusting specific site to Anchorage data.

Peak rainfall intensity (Figure 4.2-1) **100 -YEAR** 0.75
Basin
 Orographic Factor (Fig 4.2-3 MOA ASM Vol.1) 1.000

Adjusted Peak Rainfall (I_{pa}=I_p x PI): 0.75

PEAK STORM RUNOFF

Peak runoff is calculated using the Rational Method formula, $Q=C \times I \times A$

Where Q=peak runoff (c.f.s.)

C=Runoff Coefficient

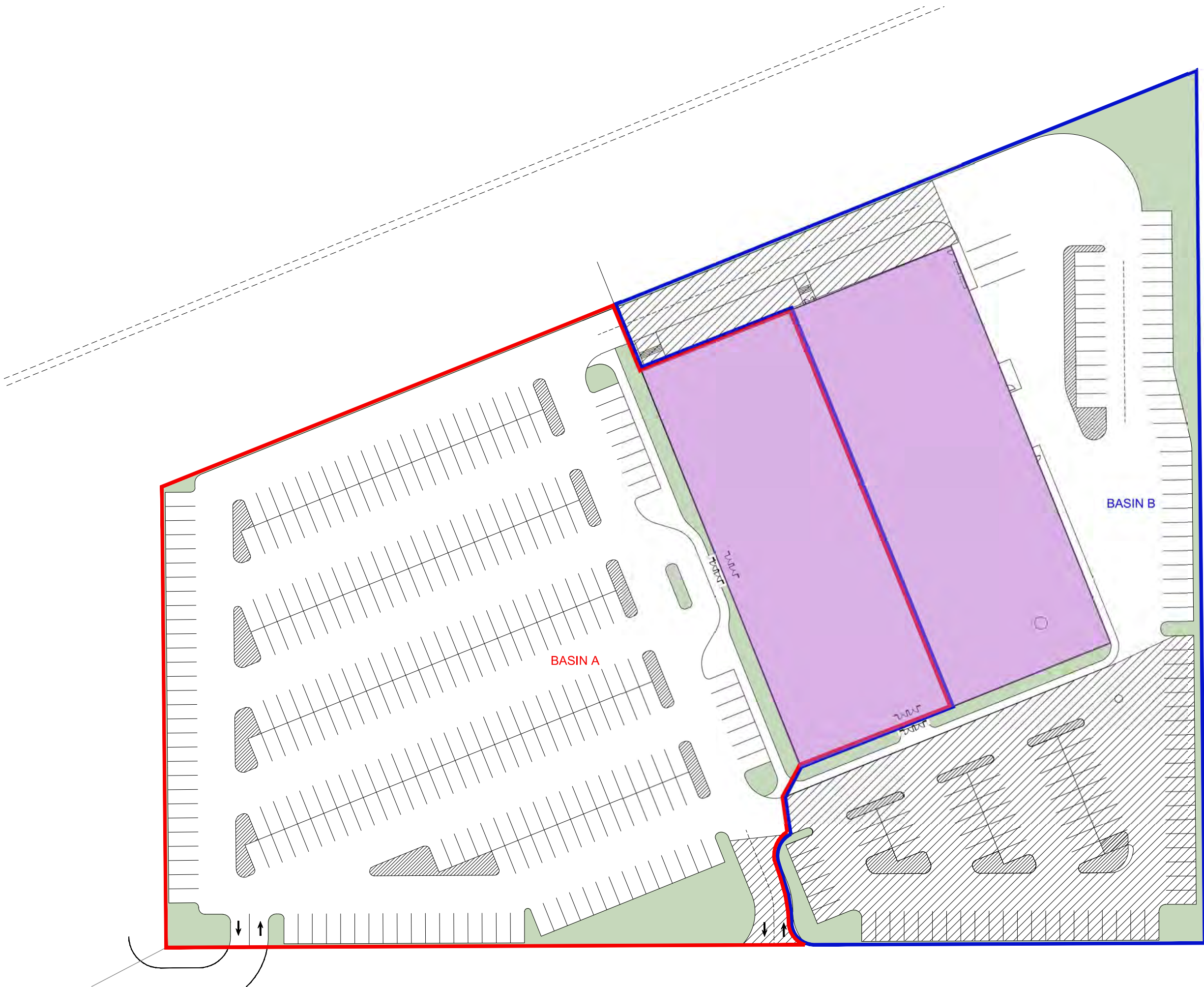
I=Peak rainfall intensity (I_{pa}, in/hr)

A=Area of contributing basin (acres)







	Total	Basin
		0.200
		0.75
		6.42
Q (cfs)	0.96	0.96

APPENDIX D

Stormwater Infiltration Calculation and Basins Exhibit



LEGEND

-  LANDSCAPE (0-2%)
-  PAVEMENT (0-2%)
-  PAVEMENT (2-6%)
-  ROOF
-  BASIN A
-  BASIN B

Project: Eklutna Native Village Gaming Hall - Grading and Drainage Study
 EEI Project No: 10336
 Calculated By: Ryan Parpan Date: 8/13/2024
 Checked By: Carl Bassler Date: 8/14/2024

INFILTRATION CALCULATION - BASIN A

Determine Size of Infiltration Basin. Runoff based on rational method coefficients. 100-year, 24-hour storm used due to being only means of drainage (i.e. no storm drain system backup). Storm data from Anchorage Design Criteria Manual Chapter 2 Drainage. Calculation uses time-step intervals.

Basin A Landcover

Landcover	Area (SF)		Runoff Coefficient	
	A	C	C*A	
Landscape (0-2%)	17,228	0.17	2,929	
Pavement (0-2%)	113,392	0.95	107,722	
Pavement (2-6%)	1,870	0.96	1,795	
Roof	29,000	1.00	29,000	
			0	
Total	161,490		141,446	
Composite Runoff Coefficient		0.876		

Basin A Parameters

Infiltration Pipe and Trench				Storm Data		
Infil Rate	K	60	IN/HR	Select storm	100	YR, 24-HOUR
Perf Pipe Diam	D _p	0	FT	Total Rainfall	3.59	IN (MOA DCM CH2 V1, Table 4.2-1)
Trench Width	W _r	40	FT	Area factor	1.00	I _p (Anchorage DCM CH 2 V1, Figure 4.2-4)
Rock Height	H _r	3	FT	Time interval	6.00	MIN
Trench Length	L _t	60	FT			
Pipe Length	L _p	0	FT			
Porosity	n	0.35	ratio			
Additional Storage Volume	V _a	0	FT ³			

Calculated System Data

Pipe Volume	0	V_p	FT^3	$L_p * \pi * (D_p/2)^2$
Rock Void Volume	2,520	V_r	FT^3	$(L_r * W_r * H_r - V_p) * n$
Total Store Volume	2,520	V_t	FT^3	$V_p + V_r + V_a$
Bottom Area	2,400	A_b	FT^3	$L * W$
Base Infiltration Rate	0.001389	I_b	FT/S	$k/(12*3600)$
Base Flow Rate	3.33	Q_b	FT^3/S	$I_b * A_b$

Basin Performance Results

On-site Peak Flow	8.86	FT^3/S	Q_r, Max
Off-site Peak Flow	0.00	FT^3/S	Q_o, Max
Off-site Volume	0	FT^3	V_o, Max
Trench Inf Volume	42,424	FT^3	Sum Infiltration Rate * Time Interval

Off-site volume is 0. Entire 100-year, 24-hour storm is infiltrated.

Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	Volume Totals, Accumulated		
									On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q_r	Q_{inf}	Q_e	V_e	V_s	Q_o	V_{inf}	V_o	V_r
HR		IN/HR	FT^3/S	FT^3/S	FT^3/S	FT^3	FT^3	FT^3/S	FT^3	FT^3	FT^3
t = timeprev + timeint		I(t)	$Q(t) = C * I(t) * A$	$Q_{inf} = I_b * A_b * (head\ increase)$	$Q_e = Q_r - Q_{inf}$	$V_e = Q_e * timeint$	$V_s = \Sigma V_e (if < V_t)$	$Q_o = Q_e\ or\ 0$	$V_{inf} = V_{inf}prev + Q_{inf} * timeint$	$V_o = Q_o * timeint + V_o,prev - \Delta V_s$	$V_r = V_r,prev + Q_r * timeint$
0	0.000	0.000	0.000	0.000	0.000	0.00	0.00	0.00	0.0	0.0	0.0
0.1	0.002	0.072	0.235	0.235	0.000	0.00	0.00	0.00	84.6	0.0	84.6
0.2	0.004	0.068	0.223	0.223	0.000	0.00	0.00	0.00	165.0	0.0	165.0
0.3	0.006	0.072	0.235	0.235	0.000	0.00	0.00	0.00	249.7	0.0	249.7
0.4	0.008	0.072	0.235	0.235	0.000	0.00	0.00	0.00	334.3	0.0	334.3
0.5	0.010	0.072	0.235	0.235	0.000	0.00	0.00	0.00	418.9	0.0	418.9
0.6	0.012	0.072	0.235	0.235	0.000	0.00	0.00	0.00	503.6	0.0	503.6
0.7	0.014	0.075	0.247	0.247	0.000	0.00	0.00	0.00	592.4	0.0	592.4
0.8	0.016	0.072	0.235	0.235	0.000	0.00	0.00	0.00	677.1	0.0	677.1
0.9	0.018	0.072	0.235	0.235	0.000	0.00	0.00	0.00	761.7	0.0	761.7
1	0.020	0.075	0.247	0.247	0.000	0.00	0.00	0.00	850.6	0.0	850.6
1.1	0.022	0.072	0.235	0.235	0.000	0.00	0.00	0.00	935.2	0.0	935.2
1.2	0.024	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,024.0	0.0	1,024.0
1.3	0.026	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,112.9	0.0	1,112.9
1.4	0.028	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,201.8	0.0	1,201.8
1.5	0.031	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,290.6	0.0	1,290.6
1.6	0.033	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,379.5	0.0	1,379.5

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
1.7	0.035	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,468.4	0.0	1,468.4
1.8	0.037	0.079	0.259	0.259	0.000	0.00	0.00	0.00	1,561.5	0.0	1,561.5
1.9	0.039	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,650.3	0.0	1,650.3
2	0.041	0.079	0.259	0.259	0.000	0.00	0.00	0.00	1,743.4	0.0	1,743.4
2.1	0.043	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,832.3	0.0	1,832.3
2.2	0.046	0.079	0.259	0.259	0.000	0.00	0.00	0.00	1,925.4	0.0	1,925.4
2.3	0.048	0.079	0.259	0.259	0.000	0.00	0.00	0.00	2,018.5	0.0	2,018.5
2.4	0.050	0.079	0.259	0.259	0.000	0.00	0.00	0.00	2,111.6	0.0	2,111.6
2.5	0.052	0.079	0.259	0.259	0.000	0.00	0.00	0.00	2,204.7	0.0	2,204.7
2.6	0.054	0.079	0.259	0.259	0.000	0.00	0.00	0.00	2,297.8	0.0	2,297.8
2.7	0.057	0.083	0.270	0.270	0.000	0.00	0.00	0.00	2,395.1	0.0	2,395.1
2.8	0.059	0.079	0.259	0.259	0.000	0.00	0.00	0.00	2,488.2	0.0	2,488.2
2.9	0.061	0.083	0.270	0.270	0.000	0.00	0.00	0.00	2,585.5	0.0	2,585.5
3	0.063	0.083	0.270	0.270	0.000	0.00	0.00	0.00	2,682.8	0.0	2,682.8
3.1	0.066	0.083	0.270	0.270	0.000	0.00	0.00	0.00	2,780.2	0.0	2,780.2
3.2	0.068	0.083	0.270	0.270	0.000	0.00	0.00	0.00	2,877.5	0.0	2,877.5
3.3	0.070	0.083	0.270	0.270	0.000	0.00	0.00	0.00	2,974.8	0.0	2,974.8
3.4	0.073	0.083	0.270	0.270	0.000	0.00	0.00	0.00	3,072.1	0.0	3,072.1
3.5	0.075	0.086	0.282	0.282	0.000	0.00	0.00	0.00	3,173.7	0.0	3,173.7
3.6	0.077	0.083	0.270	0.270	0.000	0.00	0.00	0.00	3,271.0	0.0	3,271.0
3.7	0.080	0.086	0.282	0.282	0.000	0.00	0.00	0.00	3,372.6	0.0	3,372.6
3.8	0.082	0.086	0.282	0.282	0.000	0.00	0.00	0.00	3,474.1	0.0	3,474.1
3.9	0.085	0.086	0.282	0.282	0.000	0.00	0.00	0.00	3,575.7	0.0	3,575.7
4	0.087	0.086	0.282	0.282	0.000	0.00	0.00	0.00	3,677.3	0.0	3,677.3
4.1	0.089	0.090	0.294	0.294	0.000	0.00	0.00	0.00	3,783.1	0.0	3,783.1
4.2	0.092	0.086	0.282	0.282	0.000	0.00	0.00	0.00	3,884.6	0.0	3,884.6
4.3	0.094	0.090	0.294	0.294	0.000	0.00	0.00	0.00	3,990.4	0.0	3,990.4
4.4	0.097	0.090	0.294	0.294	0.000	0.00	0.00	0.00	4,096.2	0.0	4,096.2
4.5	0.099	0.090	0.294	0.294	0.000	0.00	0.00	0.00	4,202.0	0.0	4,202.0
4.6	0.102	0.090	0.294	0.294	0.000	0.00	0.00	0.00	4,307.8	0.0	4,307.8
4.7	0.104	0.090	0.294	0.294	0.000	0.00	0.00	0.00	4,413.6	0.0	4,413.6
4.8	0.107	0.093	0.306	0.306	0.000	0.00	0.00	0.00	4,523.6	0.0	4,523.6
4.9	0.110	0.093	0.306	0.306	0.000	0.00	0.00	0.00	4,633.6	0.0	4,633.6
5	0.112	0.093	0.306	0.306	0.000	0.00	0.00	0.00	4,743.6	0.0	4,743.6
5.1	0.115	0.093	0.306	0.306	0.000	0.00	0.00	0.00	4,853.6	0.0	4,853.6
5.2	0.117	0.093	0.306	0.306	0.000	0.00	0.00	0.00	4,963.7	0.0	4,963.7
5.3	0.120	0.097	0.317	0.317	0.000	0.00	0.00	0.00	5,077.9	0.0	5,077.9
5.4	0.123	0.097	0.317	0.317	0.000	0.00	0.00	0.00	5,192.2	0.0	5,192.2
5.5	0.125	0.097	0.317	0.317	0.000	0.00	0.00	0.00	5,306.4	0.0	5,306.4
5.6	0.128	0.097	0.317	0.317	0.000	0.00	0.00	0.00	5,420.7	0.0	5,420.7

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
5.7	0.131	0.097	0.317	0.317	0.000	0.00	0.00	0.00	5,534.9	0.0	5,534.9
5.8	0.134	0.101	0.329	0.329	0.000	0.00	0.00	0.00	5,653.4	0.0	5,653.4
5.9	0.136	0.101	0.329	0.329	0.000	0.00	0.00	0.00	5,771.9	0.0	5,771.9
6	0.139	0.101	0.329	0.329	0.000	0.00	0.00	0.00	5,890.4	0.0	5,890.4
6.1	0.142	0.104	0.341	0.341	0.000	0.00	0.00	0.00	6,013.1	0.0	6,013.1
6.2	0.145	0.101	0.329	0.329	0.000	0.00	0.00	0.00	6,131.6	0.0	6,131.6
6.3	0.148	0.104	0.341	0.341	0.000	0.00	0.00	0.00	6,254.3	0.0	6,254.3
6.4	0.151	0.104	0.341	0.341	0.000	0.00	0.00	0.00	6,377.0	0.0	6,377.0
6.5	0.154	0.108	0.353	0.353	0.000	0.00	0.00	0.00	6,504.0	0.0	6,504.0
6.6	0.157	0.108	0.353	0.353	0.000	0.00	0.00	0.00	6,630.9	0.0	6,630.9
6.7	0.160	0.108	0.353	0.353	0.000	0.00	0.00	0.00	6,757.9	0.0	6,757.9
6.8	0.163	0.108	0.353	0.353	0.000	0.00	0.00	0.00	6,884.8	0.0	6,884.8
6.9	0.166	0.111	0.364	0.364	0.000	0.00	0.00	0.00	7,016.0	0.0	7,016.0
7	0.169	0.111	0.364	0.364	0.000	0.00	0.00	0.00	7,147.2	0.0	7,147.2
7.1	0.172	0.111	0.364	0.364	0.000	0.00	0.00	0.00	7,278.4	0.0	7,278.4
7.2	0.175	0.115	0.376	0.376	0.000	0.00	0.00	0.00	7,413.8	0.0	7,413.8
7.3	0.178	0.115	0.376	0.376	0.000	0.00	0.00	0.00	7,549.2	0.0	7,549.2
7.4	0.182	0.115	0.376	0.376	0.000	0.00	0.00	0.00	7,684.6	0.0	7,684.6
7.5	0.185	0.118	0.388	0.388	0.000	0.00	0.00	0.00	7,824.2	0.0	7,824.2
7.6	0.188	0.118	0.388	0.388	0.000	0.00	0.00	0.00	7,963.9	0.0	7,963.9
7.7	0.192	0.118	0.388	0.388	0.000	0.00	0.00	0.00	8,103.5	0.0	8,103.5
7.8	0.195	0.122	0.400	0.400	0.000	0.00	0.00	0.00	8,247.4	0.0	8,247.4
7.9	0.198	0.122	0.400	0.400	0.000	0.00	0.00	0.00	8,391.3	0.0	8,391.3
8	0.202	0.126	0.411	0.411	0.000	0.00	0.00	0.00	8,539.4	0.0	8,539.4
8.1	0.205	0.126	0.411	0.411	0.000	0.00	0.00	0.00	8,687.5	0.0	8,687.5
8.2	0.209	0.129	0.423	0.423	0.000	0.00	0.00	0.00	8,839.8	0.0	8,839.8
8.3	0.213	0.129	0.423	0.423	0.000	0.00	0.00	0.00	8,992.2	0.0	8,992.2
8.4	0.216	0.133	0.435	0.435	0.000	0.00	0.00	0.00	9,148.7	0.0	9,148.7
8.5	0.220	0.133	0.435	0.435	0.000	0.00	0.00	0.00	9,305.3	0.0	9,305.3
8.6	0.224	0.136	0.447	0.447	0.000	0.00	0.00	0.00	9,466.1	0.0	9,466.1
8.7	0.228	0.140	0.458	0.458	0.000	0.00	0.00	0.00	9,631.1	0.0	9,631.1
8.8	0.232	0.140	0.458	0.458	0.000	0.00	0.00	0.00	9,796.2	0.0	9,796.2
8.9	0.235	0.140	0.458	0.458	0.000	0.00	0.00	0.00	9,961.2	0.0	9,961.2
9	0.240	0.147	0.482	0.482	0.000	0.00	0.00	0.00	10,134.7	0.0	10,134.7
9.1	0.244	0.147	0.482	0.482	0.000	0.00	0.00	0.00	10,308.2	0.0	10,308.2
9.2	0.248	0.151	0.494	0.494	0.000	0.00	0.00	0.00	10,485.9	0.0	10,485.9
9.3	0.252	0.151	0.494	0.494	0.000	0.00	0.00	0.00	10,663.6	0.0	10,663.6
9.4	0.256	0.158	0.517	0.517	0.000	0.00	0.00	0.00	10,849.8	0.0	10,849.8
9.5	0.261	0.158	0.517	0.517	0.000	0.00	0.00	0.00	11,036.0	0.0	11,036.0
9.6	0.265	0.162	0.529	0.529	0.000	0.00	0.00	0.00	11,226.4	0.0	11,226.4

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
9.7	0.270	0.165	0.541	0.541	0.000	0.00	0.00	0.00	11,421.1	0.0	11,421.1
9.8	0.275	0.172	0.564	0.564	0.000	0.00	0.00	0.00	11,624.2	0.0	11,624.2
9.9	0.280	0.172	0.564	0.564	0.000	0.00	0.00	0.00	11,827.3	0.0	11,827.3
10	0.285	0.179	0.588	0.588	0.000	0.00	0.00	0.00	12,038.9	0.0	12,038.9
10.1	0.290	0.183	0.599	0.599	0.000	0.00	0.00	0.00	12,254.7	0.0	12,254.7
10.2	0.295	0.187	0.611	0.611	0.000	0.00	0.00	0.00	12,474.8	0.0	12,474.8
10.3	0.300	0.194	0.635	0.635	0.000	0.00	0.00	0.00	12,703.3	0.0	12,703.3
10.4	0.306	0.201	0.658	0.658	0.000	0.00	0.00	0.00	12,940.2	0.0	12,940.2
10.5	0.312	0.205	0.670	0.670	0.000	0.00	0.00	0.00	13,181.4	0.0	13,181.4
10.6	0.318	0.215	0.705	0.705	0.000	0.00	0.00	0.00	13,435.3	0.0	13,435.3
10.7	0.324	0.219	0.717	0.717	0.000	0.00	0.00	0.00	13,693.5	0.0	13,693.5
10.8	0.330	0.233	0.764	0.764	0.000	0.00	0.00	0.00	13,968.5	0.0	13,968.5
10.9	0.337	0.241	0.788	0.788	0.000	0.00	0.00	0.00	14,252.0	0.0	14,252.0
11	0.344	0.251	0.823	0.823	0.000	0.00	0.00	0.00	14,548.3	0.0	14,548.3
11.1	0.351	0.266	0.870	0.870	0.000	0.00	0.00	0.00	14,861.4	0.0	14,861.4
11.2	0.359	0.284	0.929	0.929	0.000	0.00	0.00	0.00	15,195.7	0.0	15,195.7
11.3	0.367	0.298	0.976	0.976	0.000	0.00	0.00	0.00	15,546.9	0.0	15,546.9
11.4	0.376	0.323	1.058	1.058	0.000	0.00	0.00	0.00	15,927.8	0.0	15,927.8
11.5	0.386	0.352	1.152	1.152	0.000	0.00	0.00	0.00	16,342.5	0.0	16,342.5
11.6	0.397	0.388	1.269	1.269	0.000	0.00	0.00	0.00	16,799.5	0.0	16,799.5
11.7	0.409	0.442	1.446	1.446	0.000	0.00	0.00	0.00	17,320.0	0.0	17,320.0
11.8	0.424	0.521	1.704	1.704	0.000	0.00	0.00	0.00	17,933.5	0.0	17,933.5
11.9	0.442	0.657	2.151	2.151	0.000	0.00	0.00	0.00	18,707.9	0.0	18,707.9
12	0.471	1.048	3.432	3.333	0.099	35.63	35.63	0.00	19,907.9	0.0	19,943.5
12.1	0.547	2.707	8.863	3.380	5.482	1973.66	2,009.29	0.00	21,124.9	0.0	23,134.2
12.2	0.569	0.790	2.586	5.991	-3.405	-1225.85	783.44	0.00	23,281.7	0.0	24,065.1
12.3	0.585	0.578	1.892	4.370	-2.477	-891.78	0.00	0.00	24,854.8	0.0	24,746.4
12.4	0.598	0.474	1.552	1.552	0.000	0.00	0.00	0.00	25,413.3	0.0	25,305.0
12.5	0.610	0.413	1.352	1.352	0.000	0.00	0.00	0.00	25,900.0	0.0	25,791.6
12.6	0.620	0.370	1.211	1.211	0.000	0.00	0.00	0.00	26,335.8	0.0	26,227.5
12.7	0.629	0.337	1.105	1.105	0.000	0.00	0.00	0.00	26,733.6	0.0	26,625.2
12.8	0.638	0.309	1.011	1.011	0.000	0.00	0.00	0.00	27,097.5	0.0	26,989.2
12.9	0.646	0.291	0.952	0.952	0.000	0.00	0.00	0.00	27,440.3	0.0	27,331.9
13	0.654	0.273	0.893	0.893	0.000	0.00	0.00	0.00	27,761.9	0.0	27,653.5
13.1	0.661	0.258	0.846	0.846	0.000	0.00	0.00	0.00	28,066.5	0.0	27,958.2
13.2	0.668	0.248	0.811	0.811	0.000	0.00	0.00	0.00	28,358.5	0.0	28,250.2
13.3	0.674	0.237	0.776	0.776	0.000	0.00	0.00	0.00	28,637.8	0.0	28,529.5
13.4	0.681	0.226	0.741	0.741	0.000	0.00	0.00	0.00	28,904.4	0.0	28,796.1
13.5	0.687	0.215	0.705	0.705	0.000	0.00	0.00	0.00	29,158.3	0.0	29,050.0
13.6	0.692	0.212	0.694	0.694	0.000	0.00	0.00	0.00	29,408.0	0.0	29,299.6

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
13.7	0.698	0.201	0.658	0.658	0.000	0.00	0.00	0.00	29,644.9	0.0	29,536.6
13.8	0.704	0.197	0.646	0.646	0.000	0.00	0.00	0.00	29,877.7	0.0	29,769.3
13.9	0.709	0.190	0.623	0.623	0.000	0.00	0.00	0.00	30,101.9	0.0	29,993.6
14	0.714	0.187	0.611	0.611	0.000	0.00	0.00	0.00	30,322.0	0.0	30,213.6
14.1	0.719	0.180	0.588	0.588	0.000	0.00	0.00	0.00	30,533.6	0.0	30,425.2
14.2	0.724	0.176	0.576	0.576	0.000	0.00	0.00	0.00	30,740.9	0.0	30,632.6
14.3	0.729	0.172	0.564	0.564	0.000	0.00	0.00	0.00	30,944.0	0.0	30,835.7
14.4	0.733	0.169	0.552	0.552	0.000	0.00	0.00	0.00	31,142.9	0.0	31,034.6
14.5	0.738	0.162	0.529	0.529	0.000	0.00	0.00	0.00	31,333.3	0.0	31,225.0
14.6	0.742	0.162	0.529	0.529	0.000	0.00	0.00	0.00	31,523.8	0.0	31,415.4
14.7	0.747	0.158	0.517	0.517	0.000	0.00	0.00	0.00	31,710.0	0.0	31,601.6
14.8	0.751	0.154	0.505	0.505	0.000	0.00	0.00	0.00	31,891.9	0.0	31,783.6
14.9	0.755	0.151	0.494	0.494	0.000	0.00	0.00	0.00	32,069.6	0.0	31,961.3
15	0.760	0.151	0.494	0.494	0.000	0.00	0.00	0.00	32,247.4	0.0	32,139.0
15.1	0.764	0.144	0.470	0.470	0.000	0.00	0.00	0.00	32,416.6	0.0	32,308.3
15.2	0.768	0.144	0.470	0.470	0.000	0.00	0.00	0.00	32,585.9	0.0	32,477.6
15.3	0.772	0.144	0.470	0.470	0.000	0.00	0.00	0.00	32,755.2	0.0	32,646.8
15.4	0.775	0.140	0.458	0.458	0.000	0.00	0.00	0.00	32,920.2	0.0	32,811.9
15.5	0.779	0.136	0.447	0.447	0.000	0.00	0.00	0.00	33,081.0	0.0	32,972.7
15.6	0.783	0.133	0.435	0.435	0.000	0.00	0.00	0.00	33,237.6	0.0	33,129.2
15.7	0.787	0.133	0.435	0.435	0.000	0.00	0.00	0.00	33,394.1	0.0	33,285.8
15.8	0.790	0.133	0.435	0.435	0.000	0.00	0.00	0.00	33,550.7	0.0	33,442.4
15.9	0.794	0.129	0.423	0.423	0.000	0.00	0.00	0.00	33,703.0	0.0	33,594.7
16	0.797	0.126	0.411	0.411	0.000	0.00	0.00	0.00	33,851.1	0.0	33,742.8
16.1	0.801	0.126	0.411	0.411	0.000	0.00	0.00	0.00	33,999.3	0.0	33,890.9
16.2	0.804	0.126	0.411	0.411	0.000	0.00	0.00	0.00	34,147.4	0.0	34,039.0
16.3	0.808	0.122	0.400	0.400	0.000	0.00	0.00	0.00	34,291.2	0.0	34,182.9
16.4	0.811	0.118	0.388	0.388	0.000	0.00	0.00	0.00	34,430.9	0.0	34,322.5
16.5	0.815	0.122	0.400	0.400	0.000	0.00	0.00	0.00	34,574.8	0.0	34,466.4
16.6	0.818	0.118	0.388	0.388	0.000	0.00	0.00	0.00	34,714.4	0.0	34,606.1
16.7	0.821	0.115	0.376	0.376	0.000	0.00	0.00	0.00	34,849.8	0.0	34,741.5
16.8	0.824	0.115	0.376	0.376	0.000	0.00	0.00	0.00	34,985.2	0.0	34,876.9
16.9	0.827	0.115	0.376	0.376	0.000	0.00	0.00	0.00	35,120.6	0.0	35,012.3
17	0.831	0.115	0.376	0.376	0.000	0.00	0.00	0.00	35,256.0	0.0	35,147.7
17.1	0.834	0.111	0.364	0.364	0.000	0.00	0.00	0.00	35,387.2	0.0	35,278.9
17.2	0.837	0.108	0.353	0.353	0.000	0.00	0.00	0.00	35,514.2	0.0	35,405.8
17.3	0.840	0.111	0.364	0.364	0.000	0.00	0.00	0.00	35,645.3	0.0	35,537.0
17.4	0.843	0.108	0.353	0.353	0.000	0.00	0.00	0.00	35,772.3	0.0	35,664.0
17.5	0.846	0.108	0.353	0.353	0.000	0.00	0.00	0.00	35,899.2	0.0	35,790.9
17.6	0.849	0.108	0.353	0.353	0.000	0.00	0.00	0.00	36,026.2	0.0	35,917.9

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
17.7	0.852	0.104	0.341	0.341	0.000	0.00	0.00	0.00	36,148.9	0.0	36,040.6
17.8	0.855	0.104	0.341	0.341	0.000	0.00	0.00	0.00	36,271.6	0.0	36,163.3
17.9	0.858	0.104	0.341	0.341	0.000	0.00	0.00	0.00	36,394.3	0.0	36,286.0
18	0.860	0.101	0.329	0.329	0.000	0.00	0.00	0.00	36,512.8	0.0	36,404.5
18.1	0.863	0.104	0.341	0.341	0.000	0.00	0.00	0.00	36,635.5	0.0	36,527.2
18.2	0.866	0.101	0.329	0.329	0.000	0.00	0.00	0.00	36,754.0	0.0	36,645.7
18.3	0.869	0.101	0.329	0.329	0.000	0.00	0.00	0.00	36,872.5	0.0	36,764.2
18.4	0.872	0.097	0.317	0.317	0.000	0.00	0.00	0.00	36,986.8	0.0	36,878.4
18.5	0.874	0.097	0.317	0.317	0.000	0.00	0.00	0.00	37,101.0	0.0	36,992.7
18.6	0.877	0.101	0.329	0.329	0.000	0.00	0.00	0.00	37,219.5	0.0	37,111.2
18.7	0.880	0.093	0.306	0.306	0.000	0.00	0.00	0.00	37,329.5	0.0	37,221.2
18.8	0.882	0.097	0.317	0.317	0.000	0.00	0.00	0.00	37,443.8	0.0	37,335.4
18.9	0.885	0.097	0.317	0.317	0.000	0.00	0.00	0.00	37,558.0	0.0	37,449.7
19	0.888	0.093	0.306	0.306	0.000	0.00	0.00	0.00	37,668.1	0.0	37,559.7
19.1	0.890	0.093	0.306	0.306	0.000	0.00	0.00	0.00	37,778.1	0.0	37,669.7
19.2	0.893	0.093	0.306	0.306	0.000	0.00	0.00	0.00	37,888.1	0.0	37,779.8
19.3	0.895	0.093	0.306	0.306	0.000	0.00	0.00	0.00	37,998.1	0.0	37,889.8
19.4	0.898	0.090	0.294	0.294	0.000	0.00	0.00	0.00	38,103.9	0.0	37,995.6
19.5	0.900	0.090	0.294	0.294	0.000	0.00	0.00	0.00	38,209.7	0.0	38,101.4
19.6	0.903	0.093	0.306	0.306	0.000	0.00	0.00	0.00	38,319.7	0.0	38,211.4
19.7	0.906	0.090	0.294	0.294	0.000	0.00	0.00	0.00	38,425.5	0.0	38,317.2
19.8	0.908	0.086	0.282	0.282	0.000	0.00	0.00	0.00	38,527.1	0.0	38,418.7
19.9	0.910	0.090	0.294	0.294	0.000	0.00	0.00	0.00	38,632.9	0.0	38,524.5
20	0.913	0.086	0.282	0.282	0.000	0.00	0.00	0.00	38,734.4	0.0	38,626.1
20.1	0.915	0.090	0.294	0.294	0.000	0.00	0.00	0.00	38,840.2	0.0	38,731.9
20.2	0.918	0.086	0.282	0.282	0.000	0.00	0.00	0.00	38,941.8	0.0	38,833.4
20.3	0.920	0.086	0.282	0.282	0.000	0.00	0.00	0.00	39,043.3	0.0	38,935.0
20.4	0.923	0.086	0.282	0.282	0.000	0.00	0.00	0.00	39,144.9	0.0	39,036.5
20.5	0.925	0.083	0.270	0.270	0.000	0.00	0.00	0.00	39,242.2	0.0	39,133.9
20.6	0.927	0.086	0.282	0.282	0.000	0.00	0.00	0.00	39,343.8	0.0	39,235.4
20.7	0.930	0.083	0.270	0.270	0.000	0.00	0.00	0.00	39,441.1	0.0	39,332.8
20.8	0.932	0.086	0.282	0.282	0.000	0.00	0.00	0.00	39,542.7	0.0	39,434.3
20.9	0.934	0.083	0.270	0.270	0.000	0.00	0.00	0.00	39,640.0	0.0	39,531.6
21	0.937	0.083	0.270	0.270	0.000	0.00	0.00	0.00	39,737.3	0.0	39,629.0
21.1	0.939	0.083	0.270	0.270	0.000	0.00	0.00	0.00	39,834.6	0.0	39,726.3
21.2	0.941	0.079	0.259	0.259	0.000	0.00	0.00	0.00	39,927.7	0.0	39,819.4
21.3	0.943	0.083	0.270	0.270	0.000	0.00	0.00	0.00	40,025.1	0.0	39,916.7
21.4	0.946	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,118.2	0.0	40,009.8
21.5	0.948	0.083	0.270	0.270	0.000	0.00	0.00	0.00	40,215.5	0.0	40,107.1
21.6	0.950	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,308.6	0.0	40,200.2

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
21.7	0.952	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,401.7	0.0	40,293.3
21.8	0.954	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,494.8	0.0	40,386.4
21.9	0.957	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,587.9	0.0	40,479.5
22	0.959	0.075	0.247	0.247	0.000	0.00	0.00	0.00	40,676.7	0.0	40,568.4
22.1	0.961	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,769.8	0.0	40,661.5
22.2	0.963	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,862.9	0.0	40,754.6
22.3	0.965	0.075	0.247	0.247	0.000	0.00	0.00	0.00	40,951.8	0.0	40,843.4
22.4	0.967	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,040.6	0.0	40,932.3
22.5	0.969	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,129.5	0.0	41,021.2
22.6	0.972	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,218.4	0.0	41,110.0
22.7	0.974	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,307.2	0.0	41,198.9
22.8	0.976	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,396.1	0.0	41,287.8
22.9	0.978	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,485.0	0.0	41,376.6
23	0.980	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,573.8	0.0	41,465.5
23.1	0.982	0.072	0.235	0.235	0.000	0.00	0.00	0.00	41,658.5	0.0	41,550.1
23.2	0.984	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,747.3	0.0	41,639.0
23.3	0.986	0.072	0.235	0.235	0.000	0.00	0.00	0.00	41,832.0	0.0	41,723.6
23.4	0.988	0.072	0.235	0.235	0.000	0.00	0.00	0.00	41,916.6	0.0	41,808.2
23.5	0.990	0.075	0.247	0.247	0.000	0.00	0.00	0.00	42,005.4	0.0	41,897.1
23.6	0.992	0.072	0.235	0.235	0.000	0.00	0.00	0.00	42,090.1	0.0	41,981.7
23.7	0.994	0.072	0.235	0.235	0.000	0.00	0.00	0.00	42,174.7	0.0	42,066.4
23.8	0.996	0.068	0.223	0.223	0.000	0.00	0.00	0.00	42,255.1	0.0	42,146.8
23.9	0.998	0.072	0.235	0.235	0.000	0.00	0.00	0.00	42,339.7	0.0	42,231.4
24	1.000	0.072	0.235	0.235	0.000	0.00	0.00	0.00	42,424.4	0.0	42,316.0

Project: Eklutna Native Village Gaming Hall - Grading and Drainage Study
 EEI Project No: 10336
 Calculated By: Ryan Parpan Date: 8/13/2024
 Checked By: Carl Bassler Date: 8/14/2024

INFILTRATION CALCULATION - BASIN B

Determine Size of Infiltration Basin. Runoff based on rational method coefficients. 100-year, 24-hour storm used due to being only means of drainage (i.e. no storm drain system backup). During design it may be possible to utilize detention or a combination detention/infiltration system which has an overflow to Peter's Creek. Storm data from Anchorage Design Criteria Manual Chapter 2 Drainage. Calculation uses time-step intervals.

Basin B Landcover

Landcover	Area (SF)		Runoff Coefficient	
	A	C	C*A	
Landscape (0-2%)	8,907	0.17	1,514	
Pavement (0-2%)	36,264	0.95	34,451	
Pavement (2-6%)	42,790	0.96	41,078	
Roof	29,000	1.00	29,000	
			0	
Total	116,960		106,043	
Composite Runoff Coefficient		0.907		

Basin B Parameters

Infiltration Pipe and Trench				Storm Data		
Infil Rate	K	60	IN/HR	Select storm	100	YR, 24-HOUR
Perf Pipe Diam	D _p	0	FT	Total Rainfall	3.59	IN (MOA DCM CH2 V1, Table 4.2-1) I _p (Anchorage DCM CH 2 V1, Figure 4.2-3)
Trench Width	W _r	40	FT	Area factor	1.00	
Rock Height	H _r	3	FT	Time interval	6.00	MIN
Trench Length	L _t	40	FT			
Pipe Length	L _p	0	FT			
Porosity	n	0.35	ratio			
Additional Storage Volume	V _a	0	FT ³			

Calculated System Data

Pipe Volume	0	V_p	FT^3	$L_p * \pi * (D_p/2)^2$
Rock Void Volume	1,680	V_r	FT^3	$(L_r * W_r * H_r - V_p) * n$
Total Store Volume	1,680	V_t	FT^3	$V_p + V_r + V_a$
Bottom Area	1,600	A_b	FT^2	$L * W$
Base Infiltration Rate	0.001389	I_b	FT/S	$k/(12*3600)$
Base Flow Rate	2.22	Q_b	FT^3/S	$I_b * A_b$

Basin Performance Results

On-site Peak Flow	6.64	FT^3/S	Q_r, Max
Off-site Peak Flow	0.00	FT^3/S	Q_o, Max
Off-site Volume	0	FT^3	V_o, Max
Trench Inf Volume	32,047	FT^3	Sum Infiltration Rate * Time Interval

Off-site volume is 0. Entire 100-year, 24-hour storm is infiltrated.

Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	Volume Totals, Accumulated		
									On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q_r	Q_{inf}	Q_e	V_e	V_s	Q_o	V_{inf}	V_o	V_r
HR		IN/HR	FT^3/S	FT^3/S	FT^3/S	FT^3	FT^3	FT^3/S	FT^3	FT^3	FT^3
t = timeprev + timeint		I(t)	$Q(t) = C * I(t) * A$	$Q_{inf} = I_b * A_b * (head\ increase)$	$Q_e = Q_r - Q_{inf}$	$V_e = Q_e * timeint$	$V_s = \Sigma V_e (if < V_t)$	$Q_o = Q_e\ or\ 0$	$V_{inf} = V_{inf}prev + Q_{inf} * timeint$	$V_o = Q_o * timeint + V_o,prev - \Delta V_s$	$V_r = V_r,prev + Q_r * timeint$
0	0.000	0.000	0.000	0.000	0.000	0.00	0.00	0.00	0.0	0.0	0.0
0.1	0.002	0.072	0.176	0.176	0.000	0.00	0.00	0.00	63.4	0.0	63.4
0.2	0.004	0.068	0.167	0.167	0.000	0.00	0.00	0.00	123.7	0.0	123.7
0.3	0.006	0.072	0.176	0.176	0.000	0.00	0.00	0.00	187.2	0.0	187.2
0.4	0.008	0.072	0.176	0.176	0.000	0.00	0.00	0.00	250.6	0.0	250.6
0.5	0.010	0.072	0.176	0.176	0.000	0.00	0.00	0.00	314.1	0.0	314.1
0.6	0.012	0.072	0.176	0.176	0.000	0.00	0.00	0.00	377.5	0.0	377.5
0.7	0.014	0.075	0.185	0.185	0.000	0.00	0.00	0.00	444.1	0.0	444.1
0.8	0.016	0.072	0.176	0.176	0.000	0.00	0.00	0.00	507.6	0.0	507.6
0.9	0.018	0.072	0.176	0.176	0.000	0.00	0.00	0.00	571.0	0.0	571.0
1	0.020	0.075	0.185	0.185	0.000	0.00	0.00	0.00	637.7	0.0	637.7
1.1	0.022	0.072	0.176	0.176	0.000	0.00	0.00	0.00	701.1	0.0	701.1
1.2	0.024	0.075	0.185	0.185	0.000	0.00	0.00	0.00	767.7	0.0	767.7
1.3	0.026	0.075	0.185	0.185	0.000	0.00	0.00	0.00	834.4	0.0	834.4
1.4	0.028	0.075	0.185	0.185	0.000	0.00	0.00	0.00	901.0	0.0	901.0
1.5	0.031	0.075	0.185	0.185	0.000	0.00	0.00	0.00	967.6	0.0	967.6
1.6	0.033	0.075	0.185	0.185	0.000	0.00	0.00	0.00	1,034.2	0.0	1,034.2

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
1.7	0.035	0.075	0.185	0.185	0.000	0.00	0.00	0.00	1,100.8	0.0	1,100.8
1.8	0.037	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,170.6	0.0	1,170.6
1.9	0.039	0.075	0.185	0.185	0.000	0.00	0.00	0.00	1,237.3	0.0	1,237.3
2	0.041	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,307.1	0.0	1,307.1
2.1	0.043	0.075	0.185	0.185	0.000	0.00	0.00	0.00	1,373.7	0.0	1,373.7
2.2	0.046	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,443.5	0.0	1,443.5
2.3	0.048	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,513.3	0.0	1,513.3
2.4	0.050	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,583.1	0.0	1,583.1
2.5	0.052	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,652.9	0.0	1,652.9
2.6	0.054	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,722.6	0.0	1,722.6
2.7	0.057	0.083	0.203	0.203	0.000	0.00	0.00	0.00	1,795.6	0.0	1,795.6
2.8	0.059	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,865.4	0.0	1,865.4
2.9	0.061	0.083	0.203	0.203	0.000	0.00	0.00	0.00	1,938.4	0.0	1,938.4
3	0.063	0.083	0.203	0.203	0.000	0.00	0.00	0.00	2,011.3	0.0	2,011.3
3.1	0.066	0.083	0.203	0.203	0.000	0.00	0.00	0.00	2,084.3	0.0	2,084.3
3.2	0.068	0.083	0.203	0.203	0.000	0.00	0.00	0.00	2,157.3	0.0	2,157.3
3.3	0.070	0.083	0.203	0.203	0.000	0.00	0.00	0.00	2,230.2	0.0	2,230.2
3.4	0.073	0.083	0.203	0.203	0.000	0.00	0.00	0.00	2,303.2	0.0	2,303.2
3.5	0.075	0.086	0.211	0.211	0.000	0.00	0.00	0.00	2,379.3	0.0	2,379.3
3.6	0.077	0.083	0.203	0.203	0.000	0.00	0.00	0.00	2,452.3	0.0	2,452.3
3.7	0.080	0.086	0.211	0.211	0.000	0.00	0.00	0.00	2,528.4	0.0	2,528.4
3.8	0.082	0.086	0.211	0.211	0.000	0.00	0.00	0.00	2,604.6	0.0	2,604.6
3.9	0.085	0.086	0.211	0.211	0.000	0.00	0.00	0.00	2,680.7	0.0	2,680.7
4	0.087	0.086	0.211	0.211	0.000	0.00	0.00	0.00	2,756.9	0.0	2,756.9
4.1	0.089	0.090	0.220	0.220	0.000	0.00	0.00	0.00	2,836.2	0.0	2,836.2
4.2	0.092	0.086	0.211	0.211	0.000	0.00	0.00	0.00	2,912.3	0.0	2,912.3
4.3	0.094	0.090	0.220	0.220	0.000	0.00	0.00	0.00	2,991.6	0.0	2,991.6
4.4	0.097	0.090	0.220	0.220	0.000	0.00	0.00	0.00	3,070.9	0.0	3,070.9
4.5	0.099	0.090	0.220	0.220	0.000	0.00	0.00	0.00	3,150.2	0.0	3,150.2
4.6	0.102	0.090	0.220	0.220	0.000	0.00	0.00	0.00	3,229.6	0.0	3,229.6
4.7	0.104	0.090	0.220	0.220	0.000	0.00	0.00	0.00	3,308.9	0.0	3,308.9
4.8	0.107	0.093	0.229	0.229	0.000	0.00	0.00	0.00	3,391.4	0.0	3,391.4
4.9	0.110	0.093	0.229	0.229	0.000	0.00	0.00	0.00	3,473.8	0.0	3,473.8
5	0.112	0.093	0.229	0.229	0.000	0.00	0.00	0.00	3,556.3	0.0	3,556.3
5.1	0.115	0.093	0.229	0.229	0.000	0.00	0.00	0.00	3,638.8	0.0	3,638.8
5.2	0.117	0.093	0.229	0.229	0.000	0.00	0.00	0.00	3,721.3	0.0	3,721.3
5.3	0.120	0.097	0.238	0.238	0.000	0.00	0.00	0.00	3,806.9	0.0	3,806.9
5.4	0.123	0.097	0.238	0.238	0.000	0.00	0.00	0.00	3,892.6	0.0	3,892.6
5.5	0.125	0.097	0.238	0.238	0.000	0.00	0.00	0.00	3,978.3	0.0	3,978.3
5.6	0.128	0.097	0.238	0.238	0.000	0.00	0.00	0.00	4,063.9	0.0	4,063.9

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
5.7	0.131	0.097	0.238	0.238	0.000	0.00	0.00	0.00	4,149.6	0.0	4,149.6
5.8	0.134	0.101	0.247	0.247	0.000	0.00	0.00	0.00	4,238.4	0.0	4,238.4
5.9	0.136	0.101	0.247	0.247	0.000	0.00	0.00	0.00	4,327.2	0.0	4,327.2
6	0.139	0.101	0.247	0.247	0.000	0.00	0.00	0.00	4,416.1	0.0	4,416.1
6.1	0.142	0.104	0.256	0.256	0.000	0.00	0.00	0.00	4,508.1	0.0	4,508.1
6.2	0.145	0.101	0.247	0.247	0.000	0.00	0.00	0.00	4,596.9	0.0	4,596.9
6.3	0.148	0.104	0.256	0.256	0.000	0.00	0.00	0.00	4,688.9	0.0	4,688.9
6.4	0.151	0.104	0.256	0.256	0.000	0.00	0.00	0.00	4,780.9	0.0	4,780.9
6.5	0.154	0.108	0.264	0.264	0.000	0.00	0.00	0.00	4,876.1	0.0	4,876.1
6.6	0.157	0.108	0.264	0.264	0.000	0.00	0.00	0.00	4,971.2	0.0	4,971.2
6.7	0.160	0.108	0.264	0.264	0.000	0.00	0.00	0.00	5,066.4	0.0	5,066.4
6.8	0.163	0.108	0.264	0.264	0.000	0.00	0.00	0.00	5,161.6	0.0	5,161.6
6.9	0.166	0.111	0.273	0.273	0.000	0.00	0.00	0.00	5,259.9	0.0	5,259.9
7	0.169	0.111	0.273	0.273	0.000	0.00	0.00	0.00	5,358.3	0.0	5,358.3
7.1	0.172	0.111	0.273	0.273	0.000	0.00	0.00	0.00	5,456.6	0.0	5,456.6
7.2	0.175	0.115	0.282	0.282	0.000	0.00	0.00	0.00	5,558.1	0.0	5,558.1
7.3	0.178	0.115	0.282	0.282	0.000	0.00	0.00	0.00	5,659.7	0.0	5,659.7
7.4	0.182	0.115	0.282	0.282	0.000	0.00	0.00	0.00	5,761.2	0.0	5,761.2
7.5	0.185	0.118	0.291	0.291	0.000	0.00	0.00	0.00	5,865.9	0.0	5,865.9
7.6	0.188	0.118	0.291	0.291	0.000	0.00	0.00	0.00	5,970.6	0.0	5,970.6
7.7	0.192	0.118	0.291	0.291	0.000	0.00	0.00	0.00	6,075.3	0.0	6,075.3
7.8	0.195	0.122	0.300	0.300	0.000	0.00	0.00	0.00	6,183.1	0.0	6,183.1
7.9	0.198	0.122	0.300	0.300	0.000	0.00	0.00	0.00	6,291.0	0.0	6,291.0
8	0.202	0.126	0.308	0.308	0.000	0.00	0.00	0.00	6,402.0	0.0	6,402.0
8.1	0.205	0.126	0.308	0.308	0.000	0.00	0.00	0.00	6,513.1	0.0	6,513.1
8.2	0.209	0.129	0.317	0.317	0.000	0.00	0.00	0.00	6,627.3	0.0	6,627.3
8.3	0.213	0.129	0.317	0.317	0.000	0.00	0.00	0.00	6,741.5	0.0	6,741.5
8.4	0.216	0.133	0.326	0.326	0.000	0.00	0.00	0.00	6,858.9	0.0	6,858.9
8.5	0.220	0.133	0.326	0.326	0.000	0.00	0.00	0.00	6,976.2	0.0	6,976.2
8.6	0.224	0.136	0.335	0.335	0.000	0.00	0.00	0.00	7,096.8	0.0	7,096.8
8.7	0.228	0.140	0.344	0.344	0.000	0.00	0.00	0.00	7,220.5	0.0	7,220.5
8.8	0.232	0.140	0.344	0.344	0.000	0.00	0.00	0.00	7,344.2	0.0	7,344.2
8.9	0.235	0.140	0.344	0.344	0.000	0.00	0.00	0.00	7,468.0	0.0	7,468.0
9	0.240	0.147	0.361	0.361	0.000	0.00	0.00	0.00	7,598.0	0.0	7,598.0
9.1	0.244	0.147	0.361	0.361	0.000	0.00	0.00	0.00	7,728.1	0.0	7,728.1
9.2	0.248	0.151	0.370	0.370	0.000	0.00	0.00	0.00	7,861.3	0.0	7,861.3
9.3	0.252	0.151	0.370	0.370	0.000	0.00	0.00	0.00	7,994.6	0.0	7,994.6
9.4	0.256	0.158	0.388	0.388	0.000	0.00	0.00	0.00	8,134.2	0.0	8,134.2
9.5	0.261	0.158	0.388	0.388	0.000	0.00	0.00	0.00	8,273.8	0.0	8,273.8
9.6	0.265	0.162	0.397	0.397	0.000	0.00	0.00	0.00	8,416.5	0.0	8,416.5

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
9.7	0.270	0.165	0.405	0.405	0.000	0.00	0.00	0.00	8,562.5	0.0	8,562.5
9.8	0.275	0.172	0.423	0.423	0.000	0.00	0.00	0.00	8,714.7	0.0	8,714.7
9.9	0.280	0.172	0.423	0.423	0.000	0.00	0.00	0.00	8,867.0	0.0	8,867.0
10	0.285	0.179	0.441	0.441	0.000	0.00	0.00	0.00	9,025.6	0.0	9,025.6
10.1	0.290	0.183	0.449	0.449	0.000	0.00	0.00	0.00	9,187.4	0.0	9,187.4
10.2	0.295	0.187	0.458	0.458	0.000	0.00	0.00	0.00	9,352.4	0.0	9,352.4
10.3	0.300	0.194	0.476	0.476	0.000	0.00	0.00	0.00	9,523.7	0.0	9,523.7
10.4	0.306	0.201	0.493	0.493	0.000	0.00	0.00	0.00	9,701.4	0.0	9,701.4
10.5	0.312	0.205	0.502	0.502	0.000	0.00	0.00	0.00	9,882.2	0.0	9,882.2
10.6	0.318	0.215	0.529	0.529	0.000	0.00	0.00	0.00	10,072.6	0.0	10,072.6
10.7	0.324	0.219	0.538	0.538	0.000	0.00	0.00	0.00	10,266.1	0.0	10,266.1
10.8	0.330	0.233	0.573	0.573	0.000	0.00	0.00	0.00	10,472.3	0.0	10,472.3
10.9	0.337	0.241	0.590	0.590	0.000	0.00	0.00	0.00	10,684.8	0.0	10,684.8
11	0.344	0.251	0.617	0.617	0.000	0.00	0.00	0.00	10,906.9	0.0	10,906.9
11.1	0.351	0.266	0.652	0.652	0.000	0.00	0.00	0.00	11,141.7	0.0	11,141.7
11.2	0.359	0.284	0.696	0.696	0.000	0.00	0.00	0.00	11,392.3	0.0	11,392.3
11.3	0.367	0.298	0.731	0.731	0.000	0.00	0.00	0.00	11,655.6	0.0	11,655.6
11.4	0.376	0.323	0.793	0.793	0.000	0.00	0.00	0.00	11,941.1	0.0	11,941.1
11.5	0.386	0.352	0.864	0.864	0.000	0.00	0.00	0.00	12,252.0	0.0	12,252.0
11.6	0.397	0.388	0.952	0.952	0.000	0.00	0.00	0.00	12,594.7	0.0	12,594.7
11.7	0.409	0.442	1.084	1.084	0.000	0.00	0.00	0.00	12,984.9	0.0	12,984.9
11.8	0.424	0.521	1.278	1.278	0.000	0.00	0.00	0.00	13,444.9	0.0	13,444.9
11.9	0.442	0.657	1.613	1.613	0.000	0.00	0.00	0.00	14,025.4	0.0	14,025.4
12	0.471	1.048	2.573	2.222	0.351	126.36	126.36	0.00	14,825.4	0.0	14,951.8
12.1	0.547	2.707	6.645	2.389	4.255	1531.86	1,658.22	0.00	15,685.6	0.0	17,343.8
12.2	0.569	0.790	1.939	4.416	-2.477	-891.69	766.53	0.00	17,275.2	0.0	18,041.8
12.3	0.585	0.578	1.419	3.236	-1.817	-654.25	112.28	0.00	18,440.2	0.0	18,552.5
12.4	0.598	0.474	1.163	2.371	-1.208	-434.70	0.00	0.00	19,293.7	0.0	18,971.3
12.5	0.610	0.413	1.013	1.013	0.000	0.00	0.00	0.00	19,658.5	0.0	19,336.1
12.6	0.620	0.370	0.908	0.908	0.000	0.00	0.00	0.00	19,985.3	0.0	19,662.9
12.7	0.629	0.337	0.828	0.828	0.000	0.00	0.00	0.00	20,283.5	0.0	19,961.1
12.8	0.638	0.309	0.758	0.758	0.000	0.00	0.00	0.00	20,556.4	0.0	20,233.9
12.9	0.646	0.291	0.714	0.714	0.000	0.00	0.00	0.00	20,813.3	0.0	20,490.9
13	0.654	0.273	0.670	0.670	0.000	0.00	0.00	0.00	21,054.4	0.0	20,732.0
13.1	0.661	0.258	0.634	0.634	0.000	0.00	0.00	0.00	21,282.8	0.0	20,960.4
13.2	0.668	0.248	0.608	0.608	0.000	0.00	0.00	0.00	21,501.7	0.0	21,179.3
13.3	0.674	0.237	0.582	0.582	0.000	0.00	0.00	0.00	21,711.1	0.0	21,388.7
13.4	0.681	0.226	0.555	0.555	0.000	0.00	0.00	0.00	21,911.0	0.0	21,588.6
13.5	0.687	0.215	0.529	0.529	0.000	0.00	0.00	0.00	22,101.3	0.0	21,778.9
13.6	0.692	0.212	0.520	0.520	0.000	0.00	0.00	0.00	22,288.5	0.0	21,966.1

Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	Volume Totals, Accumulated		
									On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
13.7	0.698	0.201	0.493	0.493	0.000	0.00	0.00	0.00	22,466.2	0.0	22,143.7
13.8	0.704	0.197	0.485	0.485	0.000	0.00	0.00	0.00	22,640.7	0.0	22,318.2
13.9	0.709	0.190	0.467	0.467	0.000	0.00	0.00	0.00	22,808.8	0.0	22,486.4
14	0.714	0.187	0.458	0.458	0.000	0.00	0.00	0.00	22,973.8	0.0	22,651.3
14.1	0.719	0.180	0.441	0.441	0.000	0.00	0.00	0.00	23,132.4	0.0	22,810.0
14.2	0.724	0.176	0.432	0.432	0.000	0.00	0.00	0.00	23,287.8	0.0	22,965.4
14.3	0.729	0.172	0.423	0.423	0.000	0.00	0.00	0.00	23,440.1	0.0	23,117.7
14.4	0.733	0.169	0.414	0.414	0.000	0.00	0.00	0.00	23,589.2	0.0	23,266.8
14.5	0.738	0.162	0.397	0.397	0.000	0.00	0.00	0.00	23,732.0	0.0	23,409.6
14.6	0.742	0.162	0.397	0.397	0.000	0.00	0.00	0.00	23,874.7	0.0	23,552.3
14.7	0.747	0.158	0.388	0.388	0.000	0.00	0.00	0.00	24,014.3	0.0	23,691.9
14.8	0.751	0.154	0.379	0.379	0.000	0.00	0.00	0.00	24,150.7	0.0	23,828.3
14.9	0.755	0.151	0.370	0.370	0.000	0.00	0.00	0.00	24,284.0	0.0	23,961.6
15	0.760	0.151	0.370	0.370	0.000	0.00	0.00	0.00	24,417.2	0.0	24,094.8
15.1	0.764	0.144	0.352	0.352	0.000	0.00	0.00	0.00	24,544.1	0.0	24,221.7
15.2	0.768	0.144	0.352	0.352	0.000	0.00	0.00	0.00	24,671.0	0.0	24,348.6
15.3	0.772	0.144	0.352	0.352	0.000	0.00	0.00	0.00	24,797.9	0.0	24,475.5
15.4	0.775	0.140	0.344	0.344	0.000	0.00	0.00	0.00	24,921.7	0.0	24,599.2
15.5	0.779	0.136	0.335	0.335	0.000	0.00	0.00	0.00	25,042.2	0.0	24,719.8
15.6	0.783	0.133	0.326	0.326	0.000	0.00	0.00	0.00	25,159.6	0.0	24,837.2
15.7	0.787	0.133	0.326	0.326	0.000	0.00	0.00	0.00	25,277.0	0.0	24,954.5
15.8	0.790	0.133	0.326	0.326	0.000	0.00	0.00	0.00	25,394.3	0.0	25,071.9
15.9	0.794	0.129	0.317	0.317	0.000	0.00	0.00	0.00	25,508.6	0.0	25,186.1
16	0.797	0.126	0.308	0.308	0.000	0.00	0.00	0.00	25,619.6	0.0	25,297.2
16.1	0.801	0.126	0.308	0.308	0.000	0.00	0.00	0.00	25,730.6	0.0	25,408.2
16.2	0.804	0.126	0.308	0.308	0.000	0.00	0.00	0.00	25,841.7	0.0	25,519.2
16.3	0.808	0.122	0.300	0.300	0.000	0.00	0.00	0.00	25,949.5	0.0	25,627.1
16.4	0.811	0.118	0.291	0.291	0.000	0.00	0.00	0.00	26,054.2	0.0	25,731.8
16.5	0.815	0.122	0.300	0.300	0.000	0.00	0.00	0.00	26,162.1	0.0	25,839.7
16.6	0.818	0.118	0.291	0.291	0.000	0.00	0.00	0.00	26,266.8	0.0	25,944.4
16.7	0.821	0.115	0.282	0.282	0.000	0.00	0.00	0.00	26,368.3	0.0	26,045.9
16.8	0.824	0.115	0.282	0.282	0.000	0.00	0.00	0.00	26,469.8	0.0	26,147.4
16.9	0.827	0.115	0.282	0.282	0.000	0.00	0.00	0.00	26,571.3	0.0	26,248.9
17	0.831	0.115	0.282	0.282	0.000	0.00	0.00	0.00	26,672.8	0.0	26,350.4
17.1	0.834	0.111	0.273	0.273	0.000	0.00	0.00	0.00	26,771.2	0.0	26,448.8
17.2	0.837	0.108	0.264	0.264	0.000	0.00	0.00	0.00	26,866.4	0.0	26,543.9
17.3	0.840	0.111	0.273	0.273	0.000	0.00	0.00	0.00	26,964.7	0.0	26,642.3
17.4	0.843	0.108	0.264	0.264	0.000	0.00	0.00	0.00	27,059.9	0.0	26,737.5
17.5	0.846	0.108	0.264	0.264	0.000	0.00	0.00	0.00	27,155.1	0.0	26,832.6
17.6	0.849	0.108	0.264	0.264	0.000	0.00	0.00	0.00	27,250.2	0.0	26,927.8

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
17.7	0.852	0.104	0.256	0.256	0.000	0.00	0.00	0.00	27,342.2	0.0	27,019.8
17.8	0.855	0.104	0.256	0.256	0.000	0.00	0.00	0.00	27,434.2	0.0	27,111.8
17.9	0.858	0.104	0.256	0.256	0.000	0.00	0.00	0.00	27,526.2	0.0	27,203.8
18	0.860	0.101	0.247	0.247	0.000	0.00	0.00	0.00	27,615.1	0.0	27,292.6
18.1	0.863	0.104	0.256	0.256	0.000	0.00	0.00	0.00	27,707.1	0.0	27,384.6
18.2	0.866	0.101	0.247	0.247	0.000	0.00	0.00	0.00	27,795.9	0.0	27,473.5
18.3	0.869	0.101	0.247	0.247	0.000	0.00	0.00	0.00	27,884.7	0.0	27,562.3
18.4	0.872	0.097	0.238	0.238	0.000	0.00	0.00	0.00	27,970.4	0.0	27,648.0
18.5	0.874	0.097	0.238	0.238	0.000	0.00	0.00	0.00	28,056.0	0.0	27,733.6
18.6	0.877	0.101	0.247	0.247	0.000	0.00	0.00	0.00	28,144.9	0.0	27,822.4
18.7	0.880	0.093	0.229	0.229	0.000	0.00	0.00	0.00	28,227.4	0.0	27,904.9
18.8	0.882	0.097	0.238	0.238	0.000	0.00	0.00	0.00	28,313.0	0.0	27,990.6
18.9	0.885	0.097	0.238	0.238	0.000	0.00	0.00	0.00	28,398.7	0.0	28,076.2
19	0.888	0.093	0.229	0.229	0.000	0.00	0.00	0.00	28,481.1	0.0	28,158.7
19.1	0.890	0.093	0.229	0.229	0.000	0.00	0.00	0.00	28,563.6	0.0	28,241.2
19.2	0.893	0.093	0.229	0.229	0.000	0.00	0.00	0.00	28,646.1	0.0	28,323.7
19.3	0.895	0.093	0.229	0.229	0.000	0.00	0.00	0.00	28,728.6	0.0	28,406.2
19.4	0.898	0.090	0.220	0.220	0.000	0.00	0.00	0.00	28,807.9	0.0	28,485.5
19.5	0.900	0.090	0.220	0.220	0.000	0.00	0.00	0.00	28,887.2	0.0	28,564.8
19.6	0.903	0.093	0.229	0.229	0.000	0.00	0.00	0.00	28,969.7	0.0	28,647.3
19.7	0.906	0.090	0.220	0.220	0.000	0.00	0.00	0.00	29,049.0	0.0	28,726.6
19.8	0.908	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,125.2	0.0	28,802.7
19.9	0.910	0.090	0.220	0.220	0.000	0.00	0.00	0.00	29,204.5	0.0	28,882.0
20	0.913	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,280.6	0.0	28,958.2
20.1	0.915	0.090	0.220	0.220	0.000	0.00	0.00	0.00	29,359.9	0.0	29,037.5
20.2	0.918	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,436.1	0.0	29,113.6
20.3	0.920	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,512.2	0.0	29,189.8
20.4	0.923	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,588.3	0.0	29,265.9
20.5	0.925	0.083	0.203	0.203	0.000	0.00	0.00	0.00	29,661.3	0.0	29,338.9
20.6	0.927	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,737.4	0.0	29,415.0
20.7	0.930	0.083	0.203	0.203	0.000	0.00	0.00	0.00	29,810.4	0.0	29,488.0
20.8	0.932	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,886.5	0.0	29,564.1
20.9	0.934	0.083	0.203	0.203	0.000	0.00	0.00	0.00	29,959.5	0.0	29,637.1
21	0.937	0.083	0.203	0.203	0.000	0.00	0.00	0.00	30,032.5	0.0	29,710.1
21.1	0.939	0.083	0.203	0.203	0.000	0.00	0.00	0.00	30,105.4	0.0	29,783.0
21.2	0.941	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,175.2	0.0	29,852.8
21.3	0.943	0.083	0.203	0.203	0.000	0.00	0.00	0.00	30,248.2	0.0	29,925.8
21.4	0.946	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,318.0	0.0	29,995.6
21.5	0.948	0.083	0.203	0.203	0.000	0.00	0.00	0.00	30,391.0	0.0	30,068.5
21.6	0.950	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,460.8	0.0	30,138.3

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
21.7	0.952	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,530.6	0.0	30,208.1
21.8	0.954	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,600.4	0.0	30,277.9
21.9	0.957	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,670.1	0.0	30,347.7
22	0.959	0.075	0.185	0.185	0.000	0.00	0.00	0.00	30,736.8	0.0	30,414.3
22.1	0.961	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,806.6	0.0	30,484.1
22.2	0.963	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,876.4	0.0	30,553.9
22.3	0.965	0.075	0.185	0.185	0.000	0.00	0.00	0.00	30,943.0	0.0	30,620.6
22.4	0.967	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,009.6	0.0	30,687.2
22.5	0.969	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,076.2	0.0	30,753.8
22.6	0.972	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,142.8	0.0	30,820.4
22.7	0.974	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,209.5	0.0	30,887.0
22.8	0.976	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,276.1	0.0	30,953.7
22.9	0.978	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,342.7	0.0	31,020.3
23	0.980	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,409.3	0.0	31,086.9
23.1	0.982	0.072	0.176	0.176	0.000	0.00	0.00	0.00	31,472.8	0.0	31,150.4
23.2	0.984	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,539.4	0.0	31,217.0
23.3	0.986	0.072	0.176	0.176	0.000	0.00	0.00	0.00	31,602.8	0.0	31,280.4
23.4	0.988	0.072	0.176	0.176	0.000	0.00	0.00	0.00	31,666.3	0.0	31,343.9
23.5	0.990	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,732.9	0.0	31,410.5
23.6	0.992	0.072	0.176	0.176	0.000	0.00	0.00	0.00	31,796.4	0.0	31,473.9
23.7	0.994	0.072	0.176	0.176	0.000	0.00	0.00	0.00	31,859.8	0.0	31,537.4
23.8	0.996	0.068	0.167	0.167	0.000	0.00	0.00	0.00	31,920.1	0.0	31,597.7
23.9	0.998	0.072	0.176	0.176	0.000	0.00	0.00	0.00	31,983.5	0.0	31,661.1
24	1.000	0.072	0.176	0.176	0.000	0.00	0.00	0.00	32,047.0	0.0	31,724.6

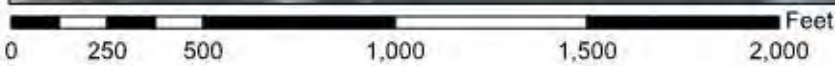
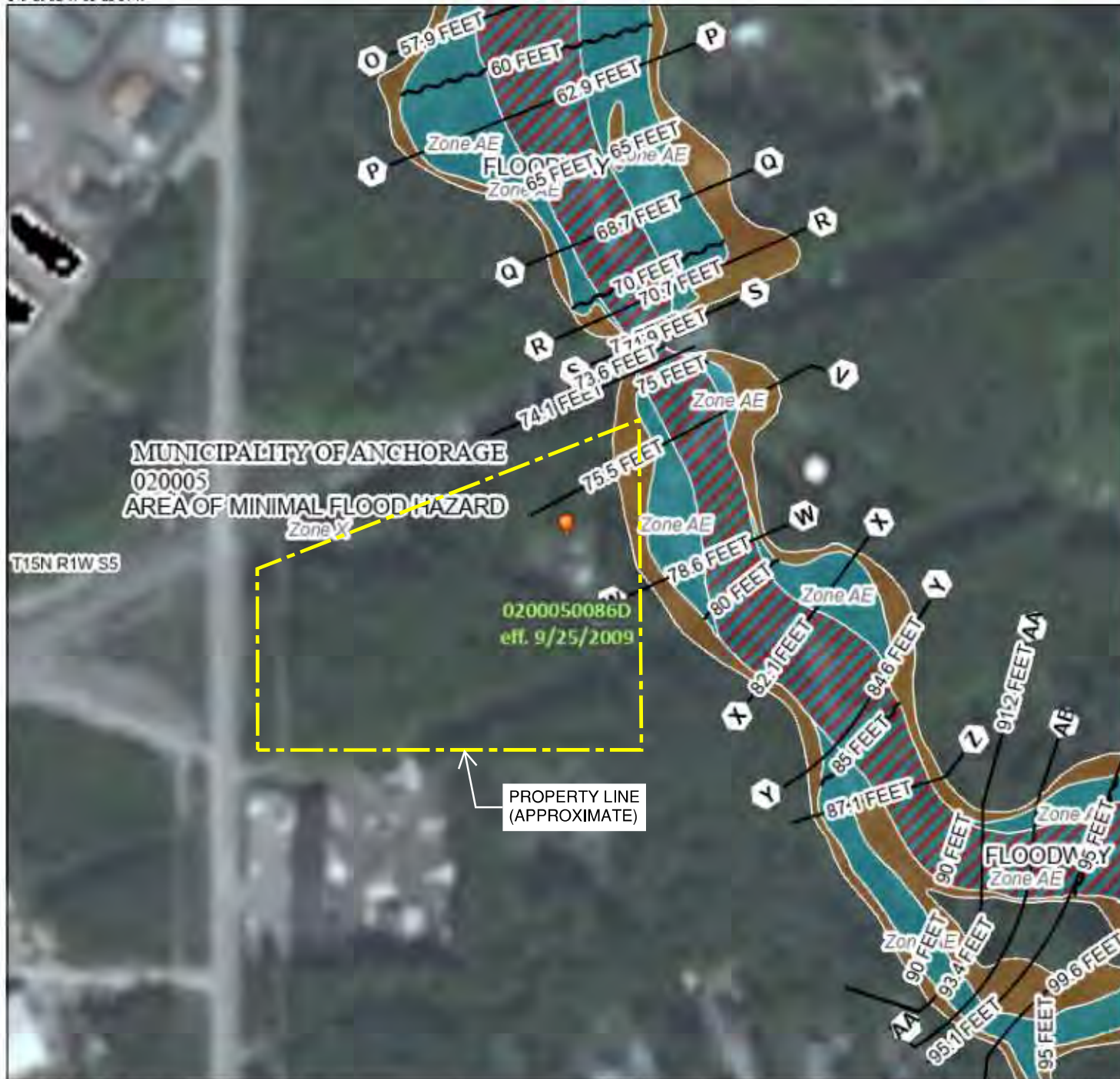
APPENDIX E

FEMA Flood Insurance Rate Map (FIRM)

National Flood Hazard Layer FIRMMette



149°29'52"W 61°25'14"N



149°29'14"W 61°24'56"N
 Basemap Imagery Source: USGS National Map 2023

Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AN
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
		Area of Undetermined Flood Hazard Zone D
GENERAL STRUCTURES		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall
OTHER FEATURES		20.2 Cross Sections with 1% Annual Chance Water Surface Elevation 17.5
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
		Coastal Transect Baseline
		Profile Baseline
	Hydrographic Feature	
MAP PANELS		Digital Data Available
		No Digital Data Available
		Unmapped
		The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards.

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 6/12/2024 at 4:44 PM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

Appendix C

Water and Wastewater Study

EKLUTNA NATIVE VILLAGE GAMING HALL PROJECT

CHUGIAK, ALASKA

Water and Wastewater Study

Final Submission

November 8, 2024



Prepared By:



3601 C St., Suite 650, Anchorage, Alaska 99503

Kyle Markel, PE – Civil Engineer
Carl Bassler, PE – Principal, Civil Engineer

EI Project No. 10336
Acorn Project No. 2415

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1.0 PROJECT INTRODUCTION

1.1 Project Description

The proposed project development includes a new 58,000 SF gaming hall building, with supporting utilities, paved parking, driveways, and landscaping. The purpose of this Water and Wastewater Study is to analyze existing site conditions and facilities relative to water and wastewater, investigate options for new water and wastewater systems to meet the anticipated demands, and evaluate the potential impacts to the proposed site.



Figure 1: Concept Site Plan

2.0 EXISTING SITE CONDITIONS

The proposed facility will be located on Lots 64, 66, and 67, Township 15 North, Range 1 West, Section 5, off Birchwood Spur Road, in Chugiak, Alaska. Lots 64 and 66 are mostly undeveloped and heavily vegetated. Lot 67 included a small (roughly 1,000 SF) single story house, which has recently been demolished, and a water well and septic system. Refer to Appendix A for the Property Boundary and As-Built Survey.

2.1 Existing Water Supply Conditions

The existing building on Lot 67 is served by an on-site water well. A well log for this well has not been obtained at the time of this study, such that specific details are unknown. The local property owner reported that water occasionally flows from a leak in the top of the well. Evidence of the well leak was

present at the time of the site visit in June of 2024, indicating the well was in an ‘artesian’ state. An artesian well is indicative of an aquifer under pressure due to confining impermeable layers. The existing well is not adequate to serve the proposed gaming hall facility. It will be abandoned as part of this project, in accordance with environmental standards.

On-site water wells are the most common water source for this area. The Alaska Department of Natural Resources (DNR) tracks water well locations and basic information. Below is a figure showing the documented wells in proximity to the project site.

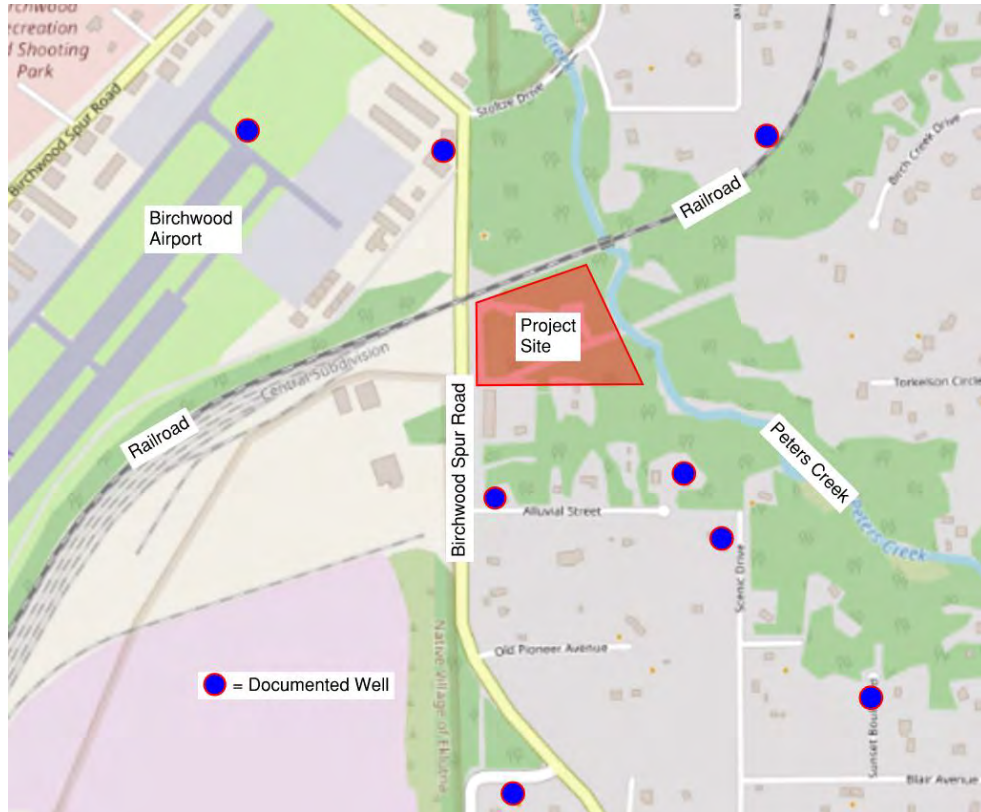


Figure 2: Documented Water Well Locations Map

Table 1 below shows basic data from all well logs documented within about one-half mile of the project site. Well depths range from 38 feet deep to 223 feet deep. Recorded static water levels range from 5 feet to 120 feet below ground surface. These wells are generally 6-inch diameter with an open bottom and no well screens. Reported pumping rates ranged from 4 gpm to 40 gpm. The lower flowrate recordings of 4 and 5 gpm are likely attributed to small residential wells where higher flowrates are not needed, and these rates are not reflective of what the aquifer could produce.

Table 1: Documented Well Log Data

Log Id	Date of Completion	Lot No.	Well Depth	Static Water Depth	MTRS	Pumping Rate
244		L084	157		S015N001W5SW SE NW SE	
3089	06/02/1981	L086	223		S015N001W5	
3846	02/28/1981	L038	98		S015N001W5SW NE SW NE	
3967	09/26/1983	L009	81		S015N001W5	
3979	09/14/1983	L003 B1	80		S015N001W5	
5817	07/08/1984	L075	161		S015N001W5SE	
5883	08/23/1984	L021	55	25	S015N001W5	
10273		L098	188		S015N001W5SW SE	
14425	05/22/1972	L108	105		S015N001W5NE SW SE SE	
14937	09/24/1979	L048	75	68	S015N001W5NW SW SE NE	10 gpm
15644	06/05/1976	L109	102		S015N001W5NW SW SE SE	
22530	12/17/1993	L111	195	120	S015N001W5NW SE SW SE	4 gpm
24843	08/21/1998	L097	205	40	S015N001W5SW SE	4 gpm
25084	08/21/1973	L101	54	24	S015N001W5SW NW SE	5 gpm
25109	06/08/1999	L090	133	5	S015N001W5SE NW NE SE	40 gpm
77942	08/01/1973	L94	150	(artesian)	S015N001W05NE NE SW SE	10 gpm
84535	12/16/2019	LT 1D	38	25	S015N001W05NE SE SE NW	15 gpm

The United States Geological Survey (USGS) published a generalized geologic map of the Eagle River-Birchwood Area in 1971 which shows the project site to be in a geological area classified as “F” defined as “Large alluvial-fan deposits. Gravel and sand; cobbles and small boulders common; small amounts of silt. Good water-bearing unit; may yield large quantities of water to wells.” Though this information comes from a generalization of large areas, it is a good indication that the groundwater will be plentiful.

Four test pits were completed onsite in July of 2024. Groundwater was encountered in all pits at 6 to 12.5 feet below ground surface (bgs), within a permeable gravel layer. This finding confirms that aquifer conditions appear conducive to high-producing water wells at this site, at least for shallow wells.

The Anchorage Water and Wastewater Utility (AWWU) is the water and wastewater utility company, owned and operated by the Municipality of Anchorage (MOA), and is responsible for providing safe and reliable water and wastewater services for the Anchorage area. AWWU does not have any water infrastructure in the immediate area of the project site. AWWU stated that they have no future plans to extend their systems near the site. AWWU also noted that almost all their new infrastructure is installed by developers constructing new subdivisions with a cost-causer/cost-payer relationship where the party creating the cost pays the cost. The closest such water mainline to the site was constructed for a subdivision located approximately 1/2 mile to the northeast, across Peters Creek. There are multiple private properties between the project site and the distribution system, without utility easements, essentially blocking any potential line extensions.

The other public water line nearest the site is AWWU’s Eklutna transmission main, located over a mile away to the southeast, where it crosses Birchwood Loop Road. The line is not permitted to be tapped with service lines. A large vault would be required to connect to the 54” transmission main, and about 7,000 linear feet of water distribution main along Birchwood Loop Road and Birchwood Spur Drive, to the far frontage of the lot. This course of action would require far more effort and costs than an onsite water well and is therefore not the best option for the project.

2.2 Existing Wastewater Disposal Conditions

The former house on Lot 67 was served by an onsite septic system extending to the northwest toward Lot 64. It is a typical gravity septic system including a septic tank with a leach field as is common for the area. The property owner on-site reported that there have not been any known issues with the existing leach field.

There are no known public wastewater systems in the Birchwood area that could be connected to. The nearest public system is located south of the Glenn Highway, roughly 1.5 miles away. AWWU does not have any future plans to extend wastewater infrastructure near the site, similar to the water system as discussed in the previous section. Onsite septic systems are common for the area, and in-situ soils are anticipated to be sufficiently permeable to allow use of on-site infiltration and disposal. The USGS generalized geologic map (mentioned previously) describes the in-situ soils as gravel and sand which work well for infiltration.

The existing septic system is sized for the small house and would not be utilized for the new facility. Part of the site development for the project would include decommissioning the septic system and removing the septic tank.

3.0 WATER SUPPLY ASSESSMENT

3.1 Water Supply Demands

The proposed gaming hall facility will have multiple functions which will require water including a restaurant, bar, restrooms, general maintenance, and for fire suppression systems.

An ADEC document “Best Management Practices for Private Drinking Water Systems” provides a table for typical daily domestic water usage from different sources, which is referenced from the EPA Design Manual. The published facility source types do not include a gaming hall or similar facility. The source types “Bar” or “Assembly Hall” do not appear to accurately represent how the facility will be used. The developer provided water supply information based on similar facilities constructed in other states. In general, the gaming hall facility’s major daily water demands will come from the restrooms, several sinks and a dish washer for the bar and restaurant. The fixtures will generally be low flow units, including waterless urinals, low flow water closets and lavatories, and electronically operated controls, to reduce the water and wastewater demands. The restaurant is planned to be deli style with minimal dishes and other items requiring washing. A letter from the developer which describes the intended fixtures in more detail can be found in Appendix C.

The estimated daily water supply demands are based on the following:

30 Employees x 3 shifts/day x 4.5 gpd	=	405 gpd
3,000 Guests/day x 2.6 gpd	=	7,800 gpd
<u>Additional Water Fixtures</u>	=	<u>476 gpd</u>
Total Water Demand	=	8,681 gpd

The peak demand flowrate was estimated by using the curve from the International Plumbing Code (IPC), Appendix E, for a restaurant facility with approximately 200 fixture units. The anticipated peak demand flowrate was found to be approximately 90 gpm.

Water storage must be provided to be available for use by the building’s fire suppression system, pressurized by a fire pump. The flow and volume of water required for the building sprinkler system is

dependent on multiple factors including the building materials, use of the building, and total square footage. Fire water storage tanks will be sized accordingly and likely installed below ground within the building. The water use for filling of the tanks is effectively a onetime occurrence and is insignificant compared to the domestic water consumption, therefore is not included in the average daily facility water demands. Fire hydrants will likely not be required for this facility as the Alaska Division of Fire and Life Safety consistently waives the requirement where a public water system is not available, and the building is protected with a sprinkler system.

3.2 Water Supply Strategies

Based on the existing site and surrounding site's common use of groundwater wells, and the infeasibility of connecting to a public distribution system, a new onsite water well is recommended to meet the potable water and fire water demands of the facility. Water wells in the surrounding area are typically installed for small residential purposes and do not require high flowrates. To achieve higher flowrates for the proposed gaming hall facility, a well would be designed and constructed to achieve a higher production rate, such as by targeting specific water-bearing formations, installation of appropriate well screens, and likely be 8" diameter or larger, to enhance inflow and to accommodate a larger pump. Until the well is drilled it is unknown if the well could meet the anticipated peak demands, without the need for a storage tank.

The anticipated peak domestic water flowrate is 90 gpm. As this is greater than the known existing well supply rates, storage tank(s) may be required to supplement the well flowrate during periods of peak demand. The total storage volume is dependent on the actual pumping capacity of the well and durations of peak water demands, therefore the water tank(s) must be sized accordingly in the design phase once pumping tests have been performed. Tanks could consist of a single or multiple pressure tanks, or a gravity tank which would require a separate supply pump. If a large gravity tank is required, it would likely be underground so that it would have minimal impact to building or site use.

ADEC 18 AAC 80 Drinking Water provides guidance for drinking water systems in the state of Alaska. Water wells are classified as private or public water systems. Public water systems are then further defined as community or non-community, and transient or non-transient. The well for this project would be considered a public water system and would likely be classified as a non-transient non-community water system, assuming there is a consistent staff of at least 25 people for the majority of the year.

18 AAC 80.1990 defines public water systems as follows:

Public Water System:

- (A) Means a system for the provision to the public of water for human consumption through pipes or other constructed conveyances, if the system has at least 15 service connections or regularly serves an average of at least 25 individuals daily at least 60 days out of the year;
- (B) is either a community water system or a non-community water system;
- (C) includes
 - a. a collection, treatment, storage, or distribution facility, including a water haul vehicle, under control of the operator of the system and used primarily in connection with the system; and
 - b. a collection or pretreatment storage facility not under control of the system operator that are used primarily in connection with the system;

(D) does not include a private water system;

Community Water System: A public water system that serves at least 15 service connections used by year-round residents or regularly serves at least 25 year-round residents.

Transient non-community water system: A non-community water system that serves at least 25 individuals daily for 60 or more days per year but does not regularly serve a daily average of at least 25 of the same individuals for more than six months per year.

Non-transient non-community water system: A public water system that is not a community water system and that regularly serves at least 25 of the same individuals over six months per year

3.3 Water Well Site Constraints

ADEC provides guidance for locating new wells. The general intent is to reduce the risk of contaminating drinking water sources. Public water wells should be at least 200 feet away (horizontally) from wastewater disposal systems, sewer manholes, sewer lines and cleanouts, and wastewater holding tanks. 18 AAC 80.020 provides a minimum separation distances as shown in Table 2 below.

Table 2: ADEC Minimum Separation Distances for Drinking Water

TABLE A. Minimum Separation Distances ^a Between Drinking Water Sources and Potential Sources of Contamination (Measured horizontally in feet)	
Type of Drinking Water System	
Potential Sources of Contamination	Community Water Systems, Non-transient Non-Community Water Systems, and Transient Non-Community Water Systems
Wastewater treatment works, ^b wastewater disposal system, ^b pit privy, ^b sewer manhole, lift station, sewer line, and sewer line cleanout	200
Community sewer line, sewer main, wastewater holding tank, ^b other potential sources of contamination ^c	200
Private sewer line, sewer service line, drain (buried in the ground), petroleum lines and storage tanks, ^d drinking water treatment waste ^e	100

In addition to wastewater systems, ADEC keeps a record of contaminated sites across the state and their cleanup status. There are no documented contaminated sites within 500 feet of the project site, and therefore contaminated sites should not be a concern for a new well on the site.

A concept site plan showing the proposed general location of the water well can be found in Appendix B.

3.4 Cumulative Impacts

When installing a new water well it is important to be aware of the potential impacts to water wells in the surrounding area which are drawing from the same aquifer. In general, impacts to surrounding wells are not a concern for an area of large lot sizes of 1 acre plus. Because of the larger lot sizes, the existing wells in the area are well spread out, as shown in Figure 2. A high recharge rate is expected for the site due to the close proximity to Peters Creek flowing along the east side of the site, and well-draining sandy gravel soil. The anticipated short recovery time (the time for a well's water level to return to the pre-pumping level) would reduce cumulative impacts to surrounding water wells.

4.0 WASTEWATER TREATMENT AND DISPOSAL ASSESSMENT

4.1 Wastewater Generation

Functions of the gaming hall facility that will generate a majority of the wastewater include a restaurant, bar, and restrooms. Similar to the water demand tables, EPA's Onsite Wastewater Treatment Manual and ADEC's Onsite Wastewater Systems Installation Manual do not include a source that accurately represents the proposed gaming hall facility. For simplicity and the purposes of this study, we have assumed the daily wastewater generation quantity to match the water demand quantity of 8,681 gpd. Refer to section 3.1 Water Supply Demands for more detail of the total water demand.

4.2 Wastewater Treatment and Disposal Strategies

There are various methods for treating and disposing of wastewater from a commercial facility with varying levels of siting constraints, construction costs, maintenance costs, and service life expectancy. In general, wastewater is typically handled in one of three ways; connecting to an existing sanitary sewer system that feeds to a large offsite treatment facility, infiltrated onsite, or stored in holding tanks onsite then transported to a treatment facility.

Similar to the water systems, AWWU manages the wastewater network in the greater Anchorage area. Unfortunately, there is no existing wastewater system near the site which could be connected to. Upon inquiry with AWWU over long term master plans for development in the Birchwood area, AWWU expressed they have no plans to extend wastewater infrastructure near the site. This makes a connection to an existing public wastewater system infeasible for the project.

Onsite infiltration of wastewater via septic tanks and leach fields is the most common method of wastewater treatment and disposal in the Birchwood area. This method utilizes a septic tank to settle out solids before the effluent reaches a drain field comprised of well-draining rock fill. The drain rock functions as a dispersal media to the surrounding in-situ soils which serve as treatment for the effluent. The drain fields are typically designed as a deep trench or as a bed. A deep trench is designed to infiltrate effluent through the sidewalls, whereas a bed system is designed to be shallower and infiltrate through the bottom of the bed. A bed type system typically requires a larger footprint than a single deep trench, but it can be advantageous when a high groundwater table is present. When multiple deep trenches are utilized, they must have a horizontal separation of at least two times the depth of the trench.

A holding tank system for wastewater is simply a large tank that offers no treatment or disposal of wastewater. Wastewater is contained in the tank and must be routinely pumped out by a truck and transported to a treatment facility. Holding tank systems are typically only installed in areas where connection to a public system and on-site disposal are not feasible.

For the proposed site and facility, an onsite septic system with advanced treatment is the recommended method of handling wastewater. Connecting to a public wastewater system is not feasible in this location, and an onsite holding tank system would not be practical for the amount of wastewater anticipated.

4.3 Wastewater System Sizing

ADEC 18 AAC 72 Wastewater Disposal provides guidelines for sizing of the septic tanks and absorption areas based on anticipated wastewater daily flowrates, and the soil characteristics.

Per 18 AAC 72.530(e)(2), The net minimum septic tank size for a commercial facility with daily design flows greater than 1,250 gpd is calculated by the following equation: 1,125 gal + (0.75 x Daily Design Flow). With a daily design flow of 8,681 gpd, a total tankage of 7,636 gal is required. The net tank capacity can be accomplished with a single tank, or if that is impractical, multiple tanks in series or parallel could be used. The current estimated design flow indicates a single 8,000 gal nominal tank will be required. A tank of this size in this region would typically be of coated steel, cylindrical, likely 8-foot diameter.

The minimum absorption area is calculated from wastewater application rates provided in the ADEC Onsite Wastewater Systems Installation Manual for systems with secondary treatment as follows:

Table 3: ADEC Wastewater Application Rates with Secondary Treatment

Wastewater Application Rates for soil absorption systems receiving effluent meeting secondary treatment standards				
Percolation Rate ^a (minutes/inch)	Soil Texture (Unified Soil Classification)	with gravity distribution	with timed dose or pressurized distribution	with timed dose or pressurized distribution and nitrogen removal
Faster than 1	Gravel (GW/GP)	Not Suitable	Not Suitable ^b	Not Suitable ^b
1 – 5	Gravel (GW/GP)	1.8	2.0	3.0
1 – 15	Medium to coarse sand (SW/SP)	1.5	1.8	2.8
6 – 15	Fine sand or loamy sand (SP-SM)	1.2	1.5	2.5
16 – 30	Sandy loam, silty gravel (GM), silty sand (SM)	0.9	1.2	2.0
31 – 60	Loam, silt loam, silt (ML)	0.5	0.8	1.0
61 – 90	Silty clay loam, clay loam ^c	0.3	0.4	0.5
91 – 120 ^d	Any soil texture ^c	Not Suitable ^d	0.15	0.25

a. Percolation tests must be performed in accordance with either a method publicly identified by EPA or the department as acceptable. The application rate must be based on either the percolation test or soil texture/classification, whichever is the most conservative.

b. Soils classified as gravel (GW or GP) for which a percolation test has not been conducted or a percolation test result is faster than one minute per inch may still be used if a shallow trench or bed system is installed with a two-foot sand liner below the distribution media and the application rates listed for SW/SP is used; sand must meet the specifications publicly identified by the department; the department may waive the sand liner requirement in a manner set out in 18 AAC 72.540 or disinfection must be included as part of the treatment prior to discharge to the leach field; for treatment that includes disinfection (ex. UV) the application rates listed for gravel (GW/GP) may be used.

c. Soils with expandable clays or soil types not listed in this table require an engineer design and prior department approval.

d. Soils with percolation rates slower than 90 min/inch require prior department approval; soils with percolation rates slower than 120 minutes per inch are considered impermeable.

Four test pits and three percolation tests were completed onsite in July of 2024. Soils encountered across the four test pits were fairly consistent and percolation rates were observed to be faster than 1 minute/inch. Groundwater was encountered at 9 to 12.5 ft below ground surface (bgs) in three of the four pits. The test pit in the northeast corner of the site encountered groundwater at 6 ft bgs which was not surprising considering the much lower elevation and close proximity to Peters Creek. Because of the high water table encountered in the northeast test pit, a percolation test was not performed. With a relatively high groundwater table and fast percolating soils, advanced secondary treatment will be required for the wastewater on this project. Footnote b., in Table 3 above, allows infiltration in soils that percolate faster than 1 min/inch if disinfection is included in the treatment process.

Secondary treatment, as defined in 18 AAC 72.990(79), generally means effluent that is treated to less than 30 mg/L of Biochemical Oxygen Demand (BOD) and Total Suspended Solids (TSS). The application rates for infiltration sizing are increased when there is secondary treatment. Increasing the application rates results in decreasing the required absorption area. Minimizing the absorption area is critical, as the intent of the proposed site design is to pave most of the property and maximize the amount of parking spaces. There is also an option to discharge effluent to the surface if it is treated to a high enough level.

There are several options for secondary treatment of effluent. The treatment systems are typically packaged from a specialized manufacturer and typically operate by recirculating effluent through a series of filter media, or by a biological reaction process. One example of an advanced treatment system that could meet the secondary treatment requirements is the AdvanTex AX-Max manufactured by Orenco Systems, Inc. It would be installed flush with grade in a non-traffic area. Primary treatment would still be accomplished by a traditional septic tank, then effluent would receive secondary treatment by the AdvanTex AX-Max, then it would drain to the absorption area. Another example is the ExtremeSTP Sewage Treatment Plant Containerized MBR by Lifewater Engineering. This system uses a membrane bioreactor treatment process following primary treatment within solids settling tanks. Tanks could be located below grade, and the equipment that required maintenance access would be located above grade in a container structure.

Product information for both advanced treatment systems can be found in Appendix D.

In addition to the effluent treatment system, a grease interceptor will be provided for the restaurant's kitchen to separate grease from the rest of the wastewater, and thereby reduce the load on the treatment system and the absorption and filtering media. Grease would be removed from the interceptor tank periodically and disposed of offsite.

If secondary treatment is provided for the system with timed dosing or pressurized distribution, nitrogen removal, and disinfection, an application rate of 3.0 gpd/sf could be used. For a bed style leach field, this would require a total bed area of approximately 3,000 sf. A concept site plan showing the proposed general location and arrangement of the wastewater system can be found in Appendix B.

For systems with design flows exceeding 2,500 gpd, nitrate calculations should be prepared to demonstrate that nitrate concentrations in the groundwater aquifer will not exceed five milligrams per liter beyond a distance measured from the edge of the soil absorption field to the nearest downgradient property line, or to a point as necessary to protect public health, public and private water systems, and the environment. If the secondary treatment system selected includes nitrogen removal, the nitrogen level in the disposed wastewater is considerably reduced, such that nitrate contamination will likely not be an issue.

4.4 Cumulative Impacts

Impacts from the proposed septic system to the site and surrounding properties are expected to be extremely minimal. Onsite wastewater systems must be sufficiently separated from water wells, streams, groundwater aquifers, lot lines, and slopes exceeding 25%. Furthermore, advanced secondary treatment of the wastewater, including nitrogen removal and disinfection will likely be provided, which will further mitigate the risk of negative impacts to the environment.

5.0 REFERENCES

Appendix E Sizing of Water Piping Systems, 2018, International Plumbing Code (IPC)

Best Management Practices for Private Drinking Water Systems, Alaska Department of Environmental Conservation (ADEC)

Onsite Wastewater Systems Installation Manual - Technical Guidance and Approved Best Management Practices, April 1, 2024, Alaska Department of Environmental Conservation (ADEC)

Onsite Wastewater Treatment System Manual, February 2002, The U.S. Environmental Protection Agency (USEPA)

18 AAC 72 Wastewater Disposal, As Amended through October 1, 2023, Alaska Department of Environmental Conservation (ADEC)

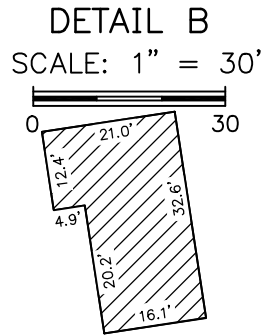
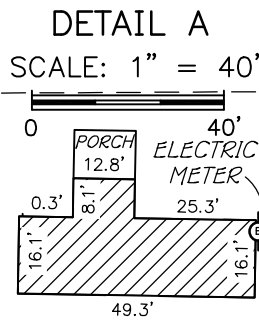
18 AAC 80 Drinking Water, Amended as of October 1, 2023, Alaska Department of Environmental Conservation (ADEC)

APPENDIX A

Property Boundary and As-Built Survey

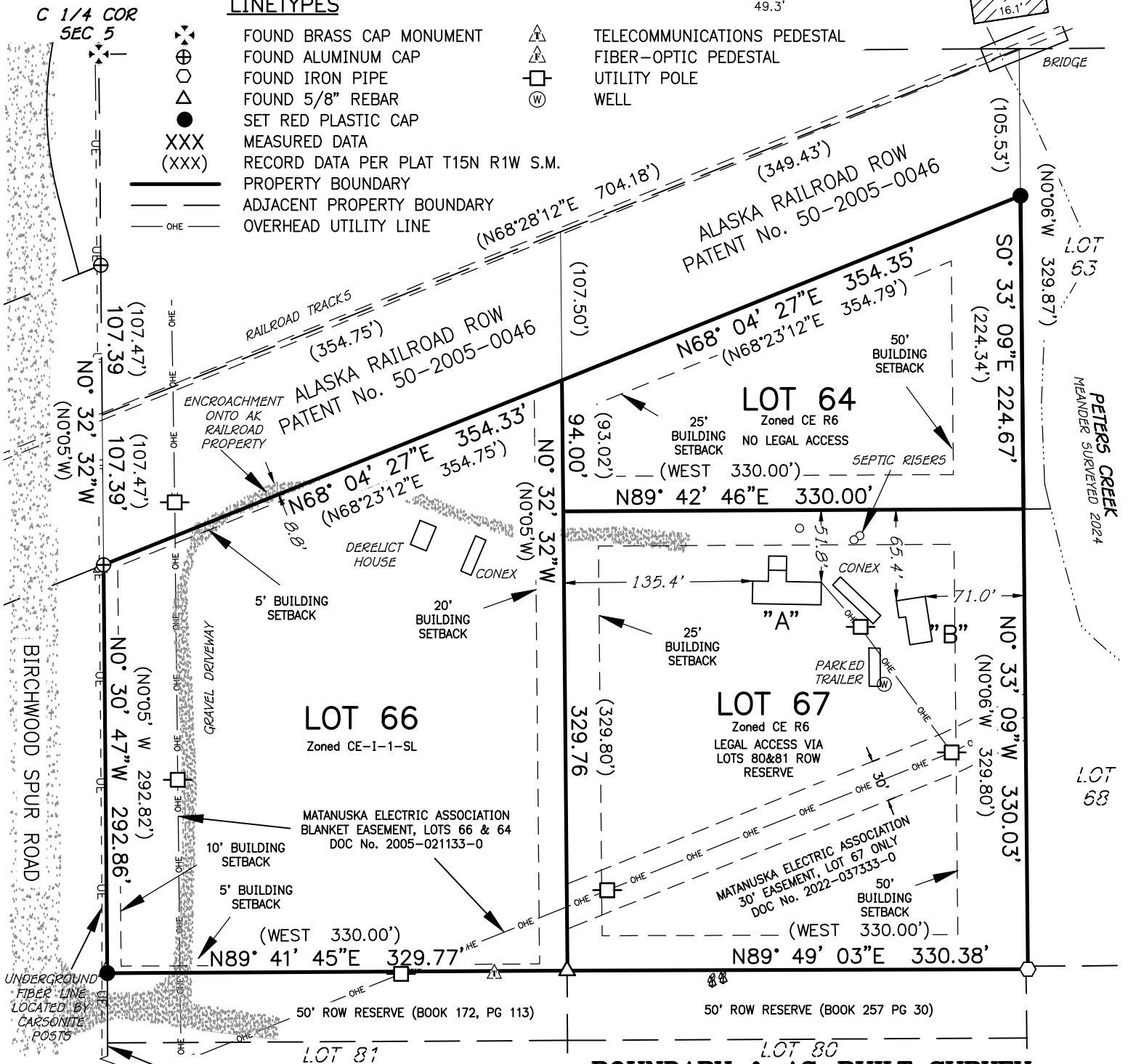
NOTES

1. INFORMATION SHOWN HEREON BASED ON FIELD SURVEY CONDUCTED BY McLINTOCK LAND ASSOCIATES, INC APRIL 2024.
2. THE DISTANCES AND BEARINGS SHOWN ON THIS AS-BUILT SURVEY ARE IN GRID. NAD83(2011) ALASKA STATE PLANE ZONE 4.
3. THIS LOT IS SUBJECT TO ANY EASEMENTS OF RECORD, EVEN IF NOT SHOWN HEREON.



LINETYPES

- | | | | |
|--|------------------------------------|--|-----------------------------|
| | FOUND BRASS CAP MONUMENT | | TELECOMMUNICATIONS PEDESTAL |
| | FOUND ALUMINUM CAP | | FIBER-OPTIC PEDESTAL |
| | FOUND IRON PIPE | | UTILITY POLE |
| | FOUND 5/8" REBAR | | WELL |
| | SET RED PLASTIC CAP | | |
| | MEASURED DATA | | |
| | RECORD DATA PER PLAT T15N R1W S.M. | | |
| | PROPERTY BOUNDARY | | |
| | ADJACENT PROPERTY BOUNDARY | | |
| | OVERHEAD UTILITY LINE | | |



BOUNDARY & AS-BUILT SURVEY

I hereby certify that I have, or someone under my supervision has, inspected the property described as:

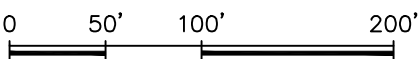
Lots 64 66 & 67
 Township 15 North Range 1 West
 Seward Meridian
 Anchorage Recording District

Monuments have been found and/or set as indicated. The improvements situated thereon are within the property lines and do not encroach or overlap onto adjacent properties, nor do any improvements from adjacent properties encroach or overlap onto this property, except as indicated hereon.

The property dimensions shown in parenthesis are from the record plat and are not necessarily as we may have measured. The improvement locations are approximate and have been determined only to the extent that enables us to determine if there are any encroachments onto or off of the lot. Drafting and reproduction may produce graphic inconsistencies; therefore scaling should not be attempted to determine unshown dimensions. This drawing is not adequate for additional construction or for determining the exact location of property lines. Only easements from the recorded subdivision plat noted above are shown hereon and it is the responsibility of the owner to determine if any other easements, covenants, or restrictions exist.

© McClintock Land Associates, Inc. (MLA) 2024

This document is copyrighted and is authorized to be used for one real property transaction or project only. Any copy is to be considered unauthentic unless it bears an original surveyor's signature (usually in blue ink) or a stamp showing recorder's office data. Copyright restrictions (but not re-use restrictions) are waived if this document has been officially recorded. Lending institutions may also make additional copies for their own records. Liability to MLA is limited to fees received for this project.



SCALE IN FEET



McCLINTOCK LAND ASSOCIATES, INC.
 16942 NORTH EAGLE RIVER LOOP ROAD
 EAGLE RIVER, ALASKA 99577-7801
 (907) 206-5000
 LICENSE No. AECC596

FIELD WORK DATE: APRIL 16&22 2024 PARTY CHIEF: BWS
 DRAWN DATE: APRIL-30-2024 DRAWN BY: BWS

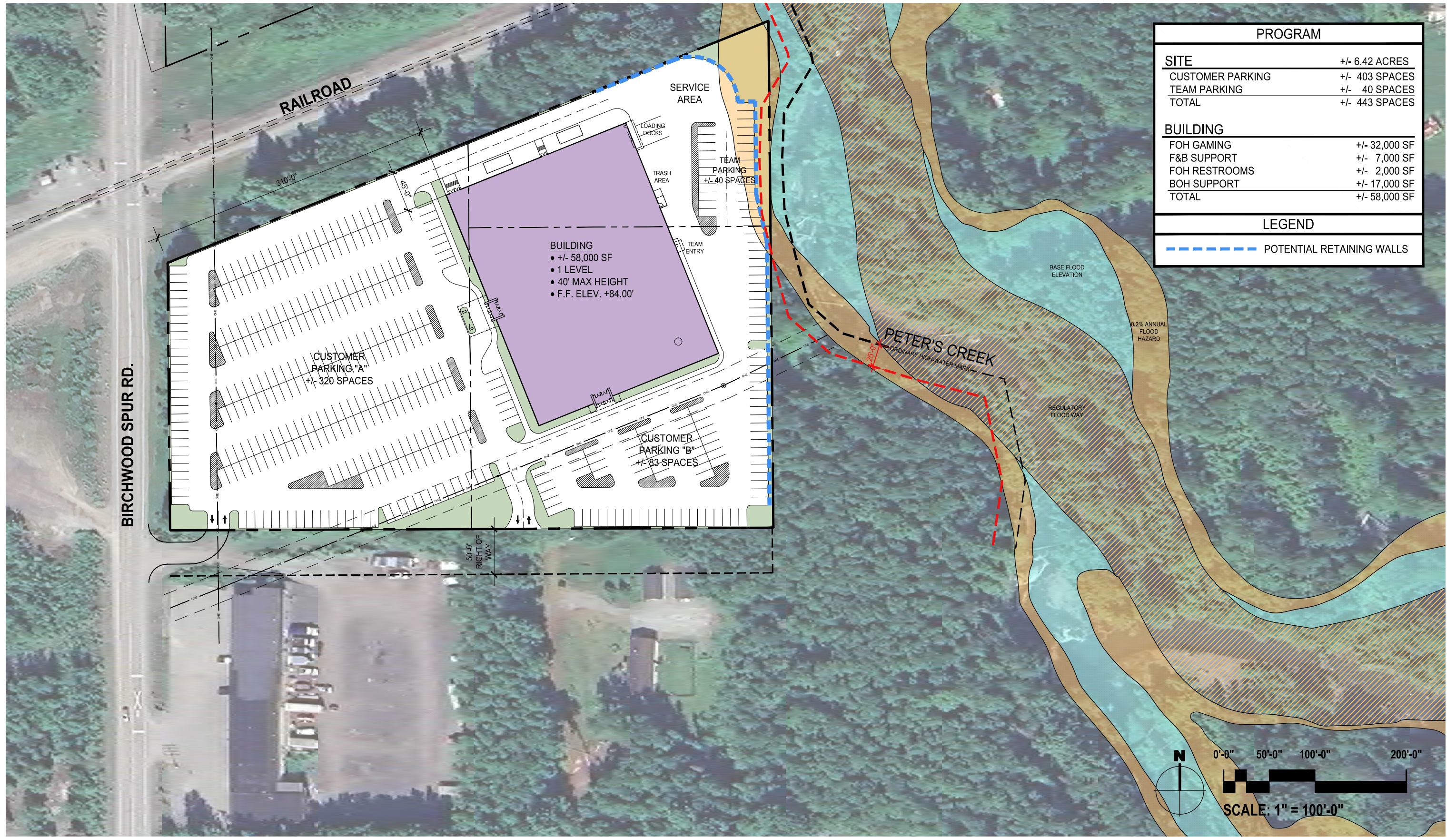
FIELD BOOK: NA
 GRID: NW1457

PLOT SCALE: 1"=100'
 DWG NAME: AB24-117

JOB NO: 24-117

APPENDIX B

1. Concept Site Plan
2. Concept Water and Wastewater Plan



PROGRAM	
SITE	+/- 6.42 ACRES
CUSTOMER PARKING	+/- 403 SPACES
TEAM PARKING	+/- 40 SPACES
TOTAL	+/- 443 SPACES
BUILDING	
FOH GAMING	+/- 32,000 SF
F&B SUPPORT	+/- 7,000 SF
FOH RESTROOMS	+/- 2,000 SF
BOH SUPPORT	+/- 17,000 SF
TOTAL	+/- 58,000 SF
LEGEND	
	POTENTIAL RETAINING WALLS

BUILDING
 • +/- 58,000 SF
 • 1 LEVEL
 • 40' MAX HEIGHT
 • F.F. ELEV. +84.00'

CUSTOMER PARKING "A"
 +/- 320 SPACES

CUSTOMER PARKING "B"
 +/- 83 SPACES

SERVICE AREA

TEAM PARKING
 +/- 40 SPACES

PETER'S CREEK
 ORDINARY HIGH WATER MARK

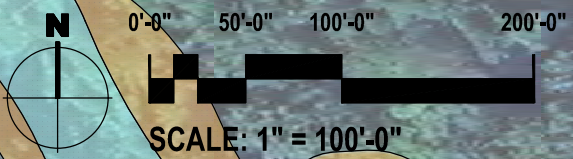
BASE FLOOD ELEVATION

0.2% ANNUAL FLOOD HAZARD

REGULATORY FLOODWAY

BIRCHWOOD SPUR RD.

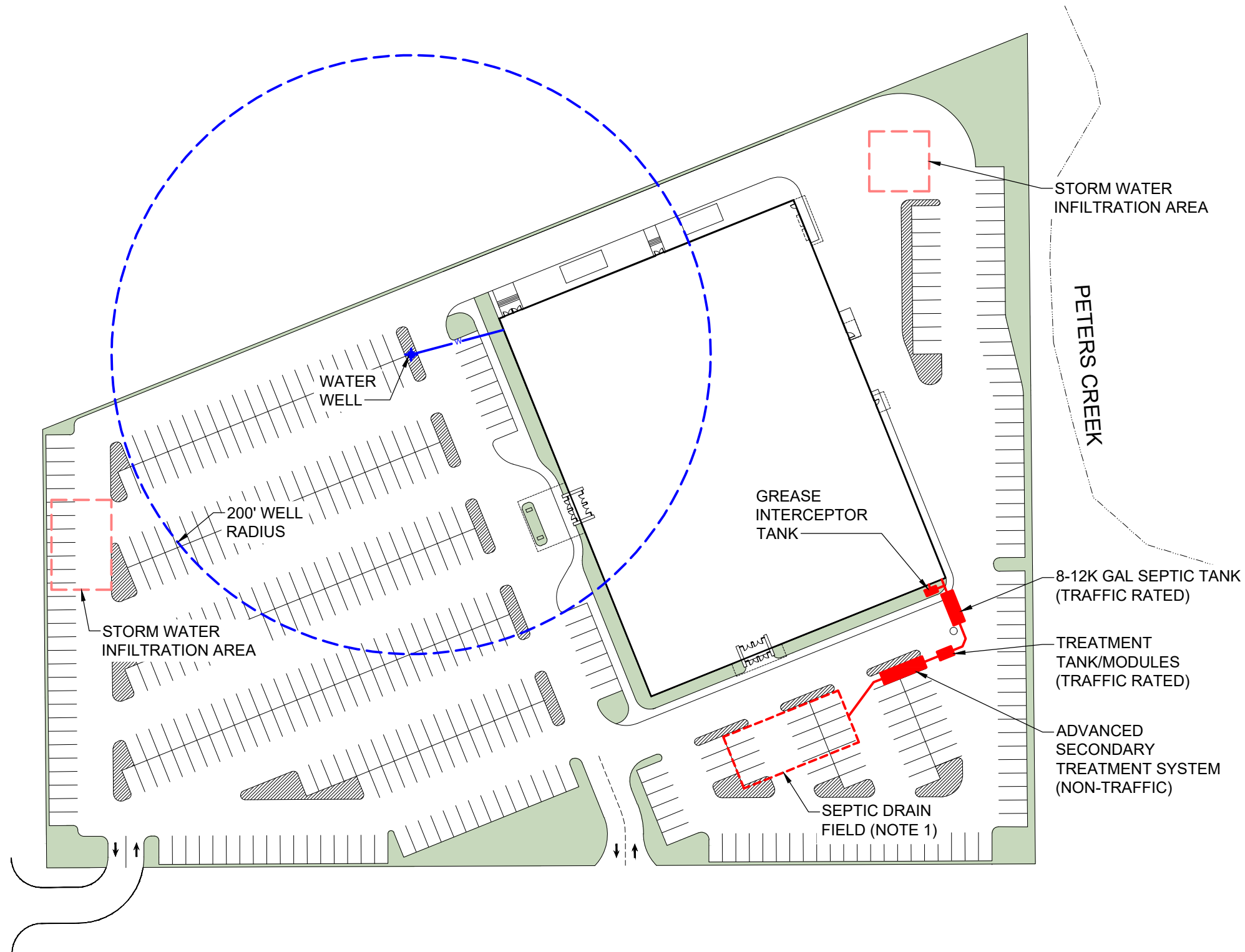
RAILROAD



EKLUTNA GAMING HALL - SITE PLAN STUDY
 CONCEPT SITE PLAN

NOTES

1. WITH SUFFICIENT ADVANCED TREATMENT, DISCHARGE OF EFFLUENT TO SURFACE WATER COULD BE PERMITTED BY ADEC. IF APPROVED, THE DRAIN FIELD WOULD NOT BE REQUIRED.



APPENDIX C

Water Usage Letter



30 October 2024

Marnell Gaming Management
222 Via Marnell Way
Las Vegas, NV 89119

Attention: Anthony A. Marnell III
Reference: Eklutna Permanent Casino
Water Use Capacity
Consultant's Project No.: 242009

Dear Anthony,

Marnell Consulting has reviewed Enterprise Engineering's anticipated daily water demands at Eklutna Casino, submitted on July 18, 2024 that states the following:

- 30 employees x 3 shifts/day x 13 gpd = 1,170 gpd
 - 4,000 guests/day x 3 gpd = 12,000 gpd
- Total Water Demand = 13,170 gpd

Based on the above we have been asked to investigate the recommended Environmental Protection Agency (EPA) guidelines that have been used by Enterprise Engineering and reevaluate to come up with a more realistic number of Gallons-per-Day use (gpd) that are anticipated in Eklutna Casino.

The EPA or Federal Buildings Metering Guidance suggests that the water usage/day (gpd) by an employee would be between 8-13 gpd and guest usage/day would be between 4-8 gpd. Other similar calculations that were used for casino buildings (without hotel rooms) were based on 4.5 gpd/employee and 2.6 gpd/guest, meaning that each occupant/guest would use the restroom 3x during their stay in the casino. This is excessive and not realistic. In addition, we applied 75% diversity to the numbers, which is a more reasonable representation of water demands in a casino such as Eklutna.

Using Marnell Consulting's logic shown above – anticipated daily water demand would look like this:

- 30 employees x 3 shifts/day x 4.5 gpd = 405 gpd
 - 3,000 guests/day x 2.6 gpd = 7,800 gpd
- (Note: based on waterless urinals that would be installed, along with low flow water closets, lavatories and electronically operated controls)*
- Total Water Demand = 8,205 gpd
Total Water Demand w. 75% diversity applied = 6,154 gpd



Other Anticipated Water Use Fixtures:

• 15-seat bar and an 8-seat Bar with 4-sinks	=	25 gpd
• Food court type of a kitchen with 1-hand sink and 3-3-compartment sinks	=	35 gpd
• 1 dish machine x 100 GAL at 3 wash cycles per day	=	300 gpd
• 1 ice machine at 26.5 GAL at 4-harvesting cycles/day	=	106 gpd
• 1 employee dining room sink	=	10 gpd
Total Water Demand	=	476 gpd
Total Water Demand w. 75% diversity applied	=	357 gpd
<hr/>		
Grand Total Water Demand	=	8,681 gpd
Grand Total Water Demand w. 75% diversity applied	=	6,511 gpd

Based on the data that was submitted to us, it appears that we would be fully capable of reducing the Gallons-per-Day (gpd) water demand as projected by Enterprise Engineering, for Eklutna Casino. The data being presented above was based on research of water demands at similar casino settings, around the country.

Please let us know if you require any additional information. Thank you for your time.

Sincerely,

MARNELL CONSULTING



James "Mitch" Milicevic
Director of Systems Integration – MEP

cc: Dave Howryla, Marnell Architecture
Doug Elsarelli, Marnell Architecture

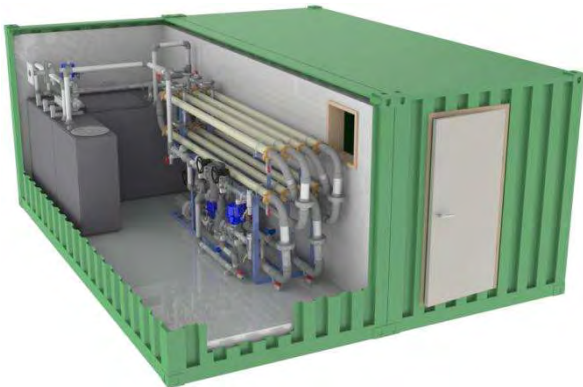
APPENDIX D

Advanced Treatment Systems Brochures



ExtremeSTP™ Sewage Treatment Plant Containerized MBRs (Aboveground, Commercial, Membrane Bioreactors)

Commercial ExtremeSTP™ sewage treatment plants are designed for use anywhere on Earth, including extremely cold climates (-60°F/-51°C). Each system can be customized to meet the client's needs. Our typical membrane bioreactor (MBR) treatment process includes a surge tank, a fine solids removing screen, bioreactor to provide aerobic decomposition, tubular membrane ultrafiltration, UV disinfection, and dosed effluent discharge. However, different processes may be used to better meet the client's needs.



Standard Features:

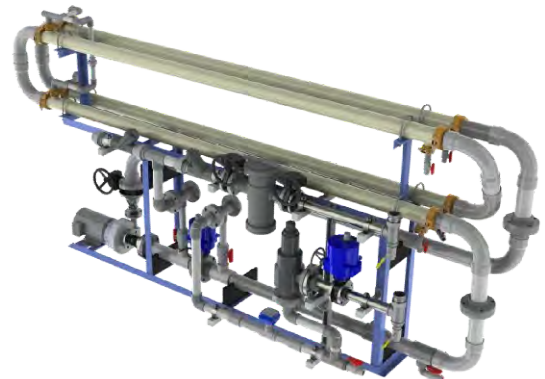
- Top quality ISO shipping containers provides rugged, weatherproof enclosures. Customer selects the paint color
- Insulation is applied to the interior of the container. Accessible areas are finished with easily cleanable fiberglass reinforced plastic
- PLC controlled, remote monitoring with web cam
- Most process tanks are made of extrusion welded, heavy duty, copolymer polypropylene plastic. Some are rotationally molded plastic tanks, and others are made of welded stainless steel. All are corrosion-free, low maintenance tanks that never need painting
- 'Plug-and-play' (or better expressed as 'plug-and-poop') design makes installation simple

Options:

- Air-transportable design for shipment to remote northern communities
- Portable design that allows the system to be partially drained down, transported to another site, and started up again. This design is especially well suited for ice road and exploration camps that must be quickly and easily relocated
- On-board generator for treatment plant operation
- Other types of membranes besides tubular UF membranes

Sizes:

- Any size from 500 gpd to 100,000 gpd and beyond



AdvanTex[®] Treatment Systems
AX-Max[™]
Manufactured by **Orenco Systems**^{®, Inc.}



This full-sized AdvanTex[®] AX-Max[™] wastewater system was installed at a 50-site campground in the LaPine State Park, LaPine, Oregon, to handle design flows of 7,500 gpd (28.4 m³/day).

Decentralized Wastewater Treatment for Commercial Properties and Communities

Applications:

- Municipal systems
- Subdivisions, apartments
- Golf course developments, resorts
- Manufactured home parks
- Parks, RV parks, campgrounds
- Schools, churches, businesses
- Rest areas, truck stops



814 Airway Avenue, Sutherlin, Oregon, USA 97479
Toll-Free: 800-348-9843 • +1-541-459-4449 • www.orenco.com

AdvanTex® AX-Max™ Treatment System

Reliable, Energy-Efficient Wastewater Treatment



The Yakama Nations Housing Authority in Washington state added five AdvanTex® AX-Max units (background) to its ten AdvanTex AX-100 units, increasing the capacity of its wastewater system by 50%. Photo courtesy of Fextex Systems, Inc.

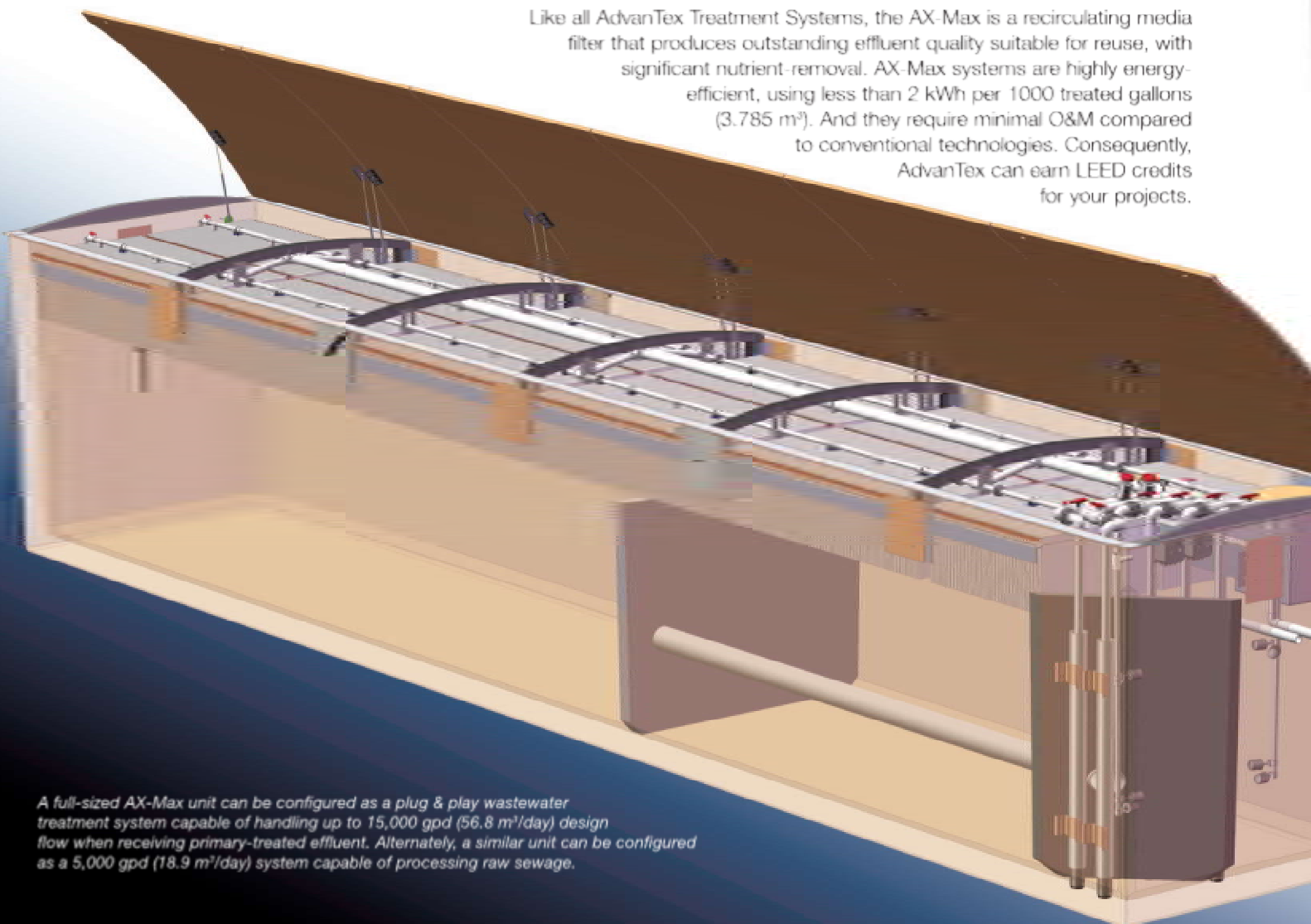
Everywhere!

For more than 15 years, Orenco's AdvanTex® Treatment Systems have been providing reliable, energy-efficient wastewater treatment inside and outside the urban core. AdvanTex textile filter technology has been winning awards and coming out on top in field trials and demo projects, all over the world.

Orenco's newest product in the AdvanTex line is the AX-Max™: a completely-integrated, fully-plumbed, and compact wastewater treatment plant that's ideal for commercial properties and communities. It's also ideal for projects with strict discharge limits, limited budgets, and part-time operators.

A Sustainable Solution for Wastewater Treatment

Like all AdvanTex Treatment Systems, the AX-Max is a recirculating media filter that produces outstanding effluent quality suitable for reuse, with significant nutrient-removal. AX-Max systems are highly energy-efficient, using less than 2 kWh per 1000 treated gallons (3.785 m³). And they require minimal O&M compared to conventional technologies. Consequently, AdvanTex can earn LEED credits for your projects.



A full-sized AX-Max unit can be configured as a plug & play wastewater treatment system capable of handling up to 15,000 gpd (56.8 m³/day) design flow when receiving primary-treated effluent. Alternately, a similar unit can be configured as a 5,000 gpd (18.9 m³/day) system capable of processing raw sewage.

AdvanTex® AX-Max™ Treatment System



Units range from 14'-42' in length. This 21' unit is ideal for lower flows.

Set, Plumb, Wire, and Go

The AX-Max is pre-plumbed and easy to install, so AX-Max projects can meet the tightest deadlines. The entire system — including treatment, recirculation, and discharge — is built inside an insulated fiberglass tank that ranges from 14-42 feet (4.3-12.8 m) in length. AX-Max units can be installed above-ground — for maximum versatility in temporary or variable-flow situations — or in-ground. They can also be installed individually or in multi-tank arrays, treating up to 1 MGD (3,800 m³/day).

For Every Climate and Condition

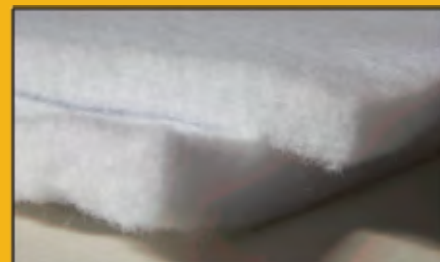
AX-Max systems provide excellent treatment anywhere, and they have been installed all over the world. For example, AX-Max systems have been installed at Malibu's famous beach parks and New Zealand's Glendhu Bay campground. Several more were installed in Soyo, Africa, to serve a new hospital and school. Other AX-Max systems have been installed on top of Alaska's frozen tundra and St. Lucia's volcanic rock. Still more have been installed in mining camps from Alberta to Texas and, in the Midwest, at a U.S. Department of Defense demo site.



Benefits

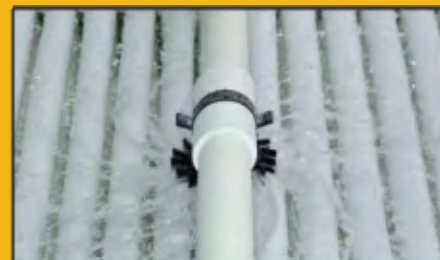
- Containerized, fully-plumbed
- Capable of meeting stringent permit limits
 - Reuse-quality effluent
 - Significant reductions in ammonia, total nitrogen
- Compact and versatile
- Above-ground or in-ground installation
- Easy to set
- Simple to operate
- Low energy usage: <2 kWh per 1000 treated gal. (<2 kWh per 3.785 m³)*

* When treating domestic waste



Textile Treatment Media

The treatment medium is a uniform, engineered textile. AdvanTex textile is easy to clean and allows loading rates as high as 50 gpd/ft² (2000 L/day/m²) with primary-treated influent.



Effluent Distribution

High-quality, low-horsepower pumps micro-dose the treatment media at regular intervals, and proprietary spin nozzles efficiently distribute the effluent, optimizing treatment.



Telemetry Controls

Drenco's telemetry-enabled control panels use a dedicated phone line or ethernet connection, ensuring 24/7 monitoring and real-time remote control.

AdvanTex® AX-Max™ Treatment System

Carefully Engineered by Orenco

Orenco Systems has been researching, designing, manufacturing, and selling leading-edge products for small-scale wastewater treatment systems since 1981. The company has grown to become an industry leader, with about 300 employees and 300 points of distribution in North America, Australasia, Europe, Africa, and Southwest Asia. Our systems have been installed in more than 70 countries around the world.

Orenco maintains an environmental lab and employs dozens of civil, electrical, mechanical, and manufacturing engineers, as well as wastewater treatment system operators. Orenco's technologies are based on sound scientific principles of chemistry, biology, mechanical structure, and hydraulics. As a result, our research appears in numerous publications and our engineers are regularly asked to give workshops and trainings.



814 Airway Avenue
Sutherlin, OR 97479 USA

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T: 541-459-4449
F: 541-459-2884

www.orenco.com

ABR-ATX-MAX-1
Rev. 1.5, © 03/17
Orenco Systems®, Inc.

Project Summary



Installation photos courtesy of BioSolutions, Inc.

Point Dume State Beach and Preserve, Southern California

In spring, 2011, Los Angeles County needed to quickly upgrade restrooms at Malibu's Point Dume State Beach in time for the long — and busy — Memorial Day weekend.

The county's engineer specified three AX-Max units, one for each restroom, and all three were installed in a matter of days. The small footprint of this configuration saved the county valuable space for visitor parking. After disinfection, the treated effluent is dispersed right into the sand. Point Dume is part of a large-scale upgrade of L.A. County beach parks, virtually all of which include AdvanTex Treatment Systems of various sizes and configurations.



Fully Supported by Orenco

AdvanTex Treatment Systems are part of a comprehensive program that includes ...

- Designer, installer, and operator training
- Design assistance, technical specifications, and plan reviews
- Installation and operation manuals
- Lifetime technical support

Distributed by:

Appendix D

Socioeconomic Impact Analysis



**THE
INNOVATION
GROUP**

**Eklutna Native Village Gaming Facility Project
Socio-Economic Impact Analysis:
Chugiak, Alaska**

Prepared for:

Acorn Environmental

July 2024

Prepared by:

The Innovation Group
7852 South Elati St. Suite 103
Littleton, Colorado 80120
www.theinnovationgroup.com

Eklutna Native Village Gaming Facility Project Socio-Economic Impact Analysis

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EXECUTIVE SUMMARY

Acorn Environmental (Acorn or Client) commissioned The Innovation Group for an Economic Impact Analysis and Community Impact Analysis of the proposed Native Village of Eklutna gaming facility in Chugiak, Alaska in support of environmental documentation being prepared in compliance with the National Environmental Policy Act (NEPA). The Project is being pursued by the Native Village of Eklutna (Eklutna) and is expected to open with the first year of operation being 2027. Impacts are expressed in Year Two stabilized operations or 2028. Construction impacts are expressed in one calendar year prior to the opening on the casino, 2026.

The report summarizes the economic and community impacts—including job creation, increases in labor income, and positive revenue impacts for other local and state businesses, among others — that could be expected from the planned development, as well as the projected impacts on other tribal gaming facilities in the area.

The executive summary details the key findings and conclusions from our analysis.

Ongoing Economic Impacts

Combining the local and rest of state impacts, the following table displays the total ongoing economic impacts from the gaming development in its second full year of operations, which is projected to inject total value added of \$67.6 million into the state economy annually.

Impact Type	Employment	Labor Income (MM)	Value Added (MM)	Output (MM)
Direct Effect	228	\$12.2	\$48.1	\$84.8
Indirect Effect	116	\$6.8	\$11.2	\$21.6
Induced Effect	74	\$4.6	\$8.3	\$13.8
Total	419	\$23.5	\$67.6	\$120.2

Source: IMPLAN Group, LLC

Fiscal impacts resulting from IMPLAN¹ include business taxes, payroll taxes, property taxes, and other relevant taxes both locally and statewide. Please note, we have excluded direct sales tax, property taxes, and corporate tax at the local, state, and federal level estimated by IMPLAN under the assumption that the Eklutna is exempt from these taxes. Based on the forecasted operations of the gaming development, IMPLAN estimates that \$457,200 of tax revenue would accrue to local governments, while over \$2.5 million of tax revenue would accrue to the state government, and an additional \$5.4 million would accrue to the federal government. It is important to note that the

¹ The Innovation Group utilized IMPLAN Online software and data in completing the Economic Impact Analysis

fiscal impacts estimated by IMPLAN and illustrated in the tables below exclude any gaming compact contributions generated for Alaska and includes taxes from direct, indirect, and induced effects

Table 2: Local Tax Impact: Eklutna **Casino Ongoing Operations (000's)**

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$0.0	\$0.0	\$0.0	\$0.0
Social Insurance Tax- Employer Contribution	\$0.0	\$0.0	\$0.0	\$0.0
TOPI: Sales Tax	\$0.0	\$38.4	\$44.5	\$82.9
TOPI: Property Tax	\$0.0	\$155.1	\$179.9	\$335.0
TOPI: Motor Vehicle License	\$5.4	\$0.6	\$0.6	\$6.6
TOPI: Severance Tax	\$1.4	\$0.1	\$0.2	\$1.7
TOPI: Other Taxes	\$14.2	\$1.5	\$1.7	\$17.3
TOPI: Special Assessments	\$6.5	\$0.7	\$0.8	\$7.9
OPI: Corporate Profits Tax	\$0.0	\$0.0	\$0.0	\$0.0
Personal Tax: Motor Vehicle License	\$2.2	\$1.2	\$0.8	\$4.2
Personal Tax: Property Taxes	\$0.9	\$0.5	\$0.3	\$1.7
Personal Tax: Other Tax (Fish/Hunt)	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$30.6	\$198.0	\$228.7	\$457.2

Source: IMPLAN Group, LLC

Table 3: State Tax Impact: Eklutna **Casino Ongoing Operations (000's)**

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$1.1	\$0.4	\$0.3	\$1.8
Social Insurance Tax- Employer Contribution	\$1.1	\$0.5	\$0.3	\$1.9
TOPI: Sales Tax	\$0.0	\$26.4	\$30.7	\$57.1
TOPI: Property Tax	\$0.0	\$12.2	\$14.1	\$26.3
TOPI: Motor Vehicle License	\$9.7	\$1.0	\$1.2	\$11.9
TOPI: Severance Tax	\$1,815.4	\$186.8	\$216.6	\$2,218.7
TOPI: Other Taxes	\$60.8	\$6.3	\$7.3	\$74.3
TOPI: Special Assessments	\$0.0	\$0.0	\$0.0	\$0.0
OPI: Corporate Profits Tax	\$0.0	\$66.5	\$54.2	\$120.7
Personal Tax: Motor Vehicle License	\$4.0	\$2.1	\$1.4	\$7.4
Personal Tax: Property Taxes	\$0.1	\$0.0	\$0.0	\$0.1
Personal Tax: Other Tax (Fish/Hunt)	\$45.8	\$25.3	\$16.4	\$87.6
Total	\$1,892.1	\$302.2	\$325.9	\$2,520.2

Source: IMPLAN Group, LLC

Table 4: **Federal Tax Impact: Eklutna Casino Ongoing Operations (000's)**

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$843.0	\$404.0	\$275.0	\$1,522.0
Social Insurance Tax- Employer Contribution	\$752.0	\$306.9	\$217.4	\$1,276.3
TOPI: Excise Tax	\$79.2	\$8.2	\$9.7	\$97.1
TOPI: Custom Duty	\$88.2	\$9.1	\$10.8	\$108.2
OPI: Corporate Profit Tax	\$0.0	\$158.7	\$132.5	\$291.2
Personal Tax: Income Tax	\$1,111.6	\$591.9	\$393.5	\$2,097.0
Personal Tax: Estate and Gift Tax	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$2,874.1	\$1,478.8	\$1,039.0	\$5,391.9

Source: IMPLAN Group, LLC

Construction Economic Impacts

Combining the local and rest of state impacts, the following table displays the total economic impacts from the construction of the development, which is projected to inject total value added of \$45.8 million into the state economy.

Table 5: Eklutna Casino Local Construction Impacts

Impact Type	Employment	Labor Income (MM)	Value Added (MM)	Output (MM)
Direct Effect	336	\$27.6	\$28.4	\$46.7
Indirect Effect	35	\$2.5	\$4.7	\$9.4
Induced Effect	115	\$7.1	\$12.8	\$21.2
Total	487	\$37.2	\$45.8	\$77.3

Source: IMPLAN Group, LLC

Note: Single Year Equivalents

Fiscal impacts resulting from IMPLAN include business taxes (including sales taxes), payroll taxes, property taxes, and other relevant taxes both locally and statewide as well as at the federal level. Based on the construction cost estimates for the Eklutna casino gaming development, the one-time fiscal impacts from construction would contribute \$628,400, \$1.0 million, and \$7.6 million to the local, state, and federal governments, respectively.

Table 6: Local Tax Impact from Construction: Eklutna **Casino (000's)**

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$0.0	\$0.0	\$0.0	\$0.0
Social Insurance Tax- Employer Contribution	\$0.0	\$0.0	\$0.0	\$0.0
TOPI: Sales Tax	\$21.8	\$31.6	\$67.8	\$121.1
TOPI: Property Tax	\$88.0	\$127.6	\$274.0	\$489.5
TOPI: Motor Vehicle License	\$0.3	\$0.5	\$1.0	\$1.8
TOPI: Severance Tax	\$0.1	\$0.1	\$0.2	\$0.4
TOPI: Other Taxes	\$0.8	\$1.2	\$2.6	\$4.6
TOPI: Special Assessments	\$0.4	\$0.5	\$1.2	\$2.1
OPI: Corporate Profits Tax	\$0.0	\$0.0	\$0.0	\$0.0
Personal Tax: Motor Vehicle License	\$4.7	\$0.4	\$1.2	\$6.3
Personal Tax: Property Taxes	\$1.9	\$0.2	\$0.5	\$2.6
Personal Tax: Other Tax (Fish/Hunt)	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$118.0	\$162.1	\$348.4	\$628.4

Source: IMPLAN Group, LLC

Table 7: State Tax Impact from Construction: Eklutna **Casino (000's)**

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$1.6	\$0.2	\$0.5	\$2.2
Social Insurance Tax- Employer Contribution	\$1.6	\$0.2	\$0.5	\$2.3
TOPI: Sales Tax	\$15.0	\$21.8	\$46.7	\$83.5
TOPI: Property Tax	\$6.9	\$10.0	\$21.5	\$38.5
TOPI: Motor Vehicle License	\$0.6	\$0.8	\$1.8	\$3.1
TOPI: Severance Tax	\$105.9	\$153.6	\$329.9	\$589.5
TOPI: Other Taxes	\$3.5	\$5.1	\$11.0	\$19.7
TOPI: Special Assessments	\$0.0	\$0.0	\$0.0	\$0.0
OPI: Corporate Profits Tax	\$9.1	\$29.0	\$82.5	\$120.5
Personal Tax: Motor Vehicle License	\$8.3	\$0.7	\$2.1	\$11.1
Personal Tax: Property Taxes	\$0.2	\$0.0	\$0.0	\$0.2
Personal Tax: Other Tax (Fish/Hunt)	\$106.3	\$8.8	\$25.0	\$140.0
Total	\$259.0	\$230.2	\$521.4	\$1,010.6

Source: IMPLAN Group, LLC

Table 8: Federal Tax Impact from Construction: Eklutna Casino (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$1,660.9	\$148.1	\$423.7	\$2,232.7
Social Insurance Tax- Employer Contribution	\$1,126.9	\$115.4	\$335.1	\$1,577.4
TOPI: Excise Tax	\$4.8	\$7.0	\$15.0	\$26.8
TOPI: Custom Duty	\$5.4	\$7.8	\$16.7	\$29.9
OPI: Corporate Profit Tax	\$22.5	\$71.9	\$204.3	\$298.7
Personal Tax: Income Tax	\$2,582.8	\$213.9	\$606.1	\$3,402.7
Personal Tax: Estate and Gift Tax	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$5,403.3	\$564.1	\$1,600.9	\$7,568.3

Source: IMPLAN Group, LLC

Social and Community Impacts

Vital to having local governments and citizens get on board with a casino opening in their region is an examination of the social and community impacts of the proposed casino development.

Increased local services and costs resulting from casino gaming operations generally fall into three categories: those arising from population and development growth, those arising from the impacts of increased visitation and traffic, and social impacts resulting from problem gambling.

Population and Employment Related Impacts

The Eklutna casino in Chugiak is expected to produce \$83.8 million in annual gross revenue and directly employ 228 people by the development's assumed second full year of operations.

For the purposes of this section, the greater Anchorage area, the host of the Eklutna casino gaming development, as well as the city of Chugiak will be used as the primary study areas of impacts as pertaining to employment and increased population.

Employment Impact

As noted, the proposed Eklutna casino gaming development is projected to require 228 jobs. These jobs are expected to be filled by a combination of local unemployed workers, local out-bound commuters, residents of neighboring cities, and new residents.

Unemployment in area stood at approximately 5,000 workers in 2023. While the 2020 recession adversely impacted unemployment in the area, pre-2020 we observed declines in unemployment levels in the market area. Additionally, 2021 and 2022 data reveal that unemployment continued its downward trajectory following the 2020 recession.

As the following table illustrates, the existing workforce in the area is estimated to fill nearly 88.0% of the new jobs created by a casino development, and the remaining 12% of workers are estimated to represent new residents moving into the area. Based on our quantitative analysis, we believe the subject development will have a neutral-to-positive on employment in the local area. The facility's operations will create jobs for the unemployed and bring more workers to the area

while employing a low percentage (1.1%) of Anchorage’s existing workforce—allowing other local employers to maintain their ability to attract workers.

Table 9: Eklutna Casino Project Source of Workforce

Source of Employment	Proposed Project	Metric Applied	Source
A. Number of New Employees	228	Total Jobs	Marnell Companies
B. Number of Commuters from outside Anchorage	1	0.5% based on Anchorage Workforce Commuting Patterns	US Census
C. Unemployed back to work	39	1.1% of a future estimate of 3,511 Unemployed Workers	US Census
D. Former Commuters Staying within Anchorage	162	3.0% of 5,384 Anchorage workers who currently commute outside the area	US Census
E. Total from Existing Area Workforce	201	B+C+D	
F. Estimated Total New Workers Needed	27	A-E	
% of New Employees	11.7%	F/A	

Household and Population Impact

Assuming approximately 1.1 casino workers per household, the total number of new households to Anchorage is estimated at 24. On average, households in Anchorage comprise 2.57 persons, bringing the total population increase to Anchorage to 63 people, or 0.02% of the projected 2027 Anchorage population

Table 10: Eklutna casino Development Impact on Households and Population

New employees moving to area	27
# of jobs per household	1.1
Number of new Households	24
Anchorage Avg. Household Size	2.57
New Area Population	63
% Increase of 2027 Total Area Pop	0.02%

Source: The Innovation Group

School Impact

Based on the number of new households and Alaska household metrics, the increase to school enrollment is estimated to be approximately 13 children in Anchorage. These represent an increase of 0.03% over projected 2027 enrollment.

Table 11: Eklutna Casino Impact on School Enrollment

	Metric	Anchorage
Number of New Households		24
Number of Households That Will Have Children*	32.1%	8
<i>Number of Children per Household**</i>	2.1	
Total Number of Children		17
Projected School Age Children	80%	13
2027 Projected School Enrollment		48,779
2027 Projected School Enrollment with Eklutna Casino		48,792
<i>% Change</i>		<i>0.03%</i>

Source: ESRI, US Census Bureau ACS

*Anchorage ACS 2018-22 estimated percent of households with presence of children under the age of 18

**Anchorage ACS 2018-22 average number of children per household with children under the age of 18

Housing Impact

The additional households that are projected to relocate in and around the Anchorage area will have a small, but positive impact on the housing market. New households will support the current and future development of housing in the community.

Currently, the number of housing units is nearly 10% greater than the number of households in Anchorage. This denotes an adequate number of units available for residents. Five-year projections estimate that the growth of housing units will outpace growth of families and households in the market. According to ESRI data, the number of housing units is projected to grow by about 240 units by 2027 in Anchorage. This will be sufficient to absorb the households estimated to relocate.

Table 12: Anchorage Population and Housing Trends

	2010 Census	2024 ESRI	2027 Projection
Population	291,826	287,385	284,766
<i>Percent Change</i>		-0.11%	-0.30%
Households	107,332	109,102	109,221
<i>Percent Change</i>		0.12%	0.04%
Families	70,544	68,078	67,811
<i>Percent Change</i>		-0.25%	-0.13%
Housing Units	113,032	119,622	119,861
<i>Percent Change</i>		0.41%	0.07%

Source: ArcGIS/ESRI

Anchorage had a vacancy rate for housing units of nearly 9% in 2024, which translated to roughly 10,500 units. If the same percentage is applied to the number of housing units projected for 2027, then there will be approximately 10,550 available housing units to accommodate the incoming households. Consequently, there is a substantial and sufficient housing supply to accommodate the 24 households estimated to relocate to Anchorage.

Conclusion

Overall, the proposed casino development in Anchorage is projected to increase local population by approximately 0.02 percent and increase school enrollment by approximately 0.03 percent.

Municipal Services Impact

Host communities should expect impacts similar in kind to other commercial development of similar scope and visitor potential. The projected increase in visitor population should be expected to lead to increases in public safety services and judicial system caseload.

Evidence suggests that on-going impacts from gaming developments to local communities are highly manageable and are typically offset by the new local tax dollars generated by the development. Of most direct consequence to casino development are police, fire, and EMS. These will largely fall into the purview of the local police and fire departments serving Anchorage.

Police

The Anchorage Police Department (APD) serves the entire Anchorage Municipality with its patrol area encompassing the core "Anchorage bowl", the Seward Highway corridor from Potter Creek south to McHugh Creek, the Glenn Highway corridor north of the Anchorage bowl to the municipality's border with the Matanuska-Susitna Municipality, and includes the communities of Eagle River, Chugiak, and Eklutna. APD is the largest municipal police force in Alaska serving the state's largest city. The department employs 610 people who hold both sworn and non-sworn positions.

According to the 2024 Approved General Government Operating Budget Report, the approved 2024 budget was increased by about 3.5% to about \$140 million from the revised 2023 budget of about \$135 million. Roughly 78.4% of the approved 2024 budget funds the police department's salaries and benefits. From 2022 to 2023, the department's budget increased by about 6% from roughly \$127 million to about \$135 million. In 2023, the police department's salaries and benefits accounted for 76.4% of the total budget.

In 2023, the Anchorage Municipality saw almost 40,000 trial court case filings with a judicial budget of nearly \$10 million, divided between the Equal Rights Commission, Equity & Justice, and Municipal Attorney. The Municipal Attorney budget was the largest segment at nearly \$8.6 million. Although the Anchorage Correctional Complex is located within the Anchorage Municipality, the Alaska Department of Corrections (DOC) is responsible for managing the state's correctional facilities. The Alaska DOC had a budget of nearly \$423 million for fiscal year 2023, and during that same period there were almost 28,000 bookings.

Fire

The Anchorage Fire Department (AFD) provides fire protection and emergency medical services to Anchorage. The AFD serves numerous areas including the incorporated areas of downtown Anchorage, Bird, Bootleggers Cove, Eagle River, Fairview, Indian, Mountain View, Muldoon, and Spenard, AFD is assisted by two volunteer fire departments operating in the outlying areas of the Anchorage Municipality. According to the 2024 Approved General Government Operating Budget, there are 15 fire stations with 14 fire engines, 13 medic units, 5 fire trucks, and 5 tenders.

The entire fire department employs just over 400 full-time positions. AFD’s approved budget for 2024 is approximately \$115 million, increasing 2.3% from the previous year. Salaries and benefits for 76.4% of the 2024 budget. From 2022 to 2023, the AFD budget decreased by about 2.2% from roughly \$115 million to about \$112 million.

Emergency Medical Services (EMS)

The Anchorage Fire Department also provides EMS to Anchorage with a mutual aid agreement between the Anchorage Fire Department, Ted Stevens International Airport Police and Fire Department, Joint Base Elmendorf-Richardson, Girdwood Fire Department, and Chugiak Volunteer Fire and Rescue Department. In 2023, EMS calls accounted for nearly 71% of the AFD’s total service calls, averaging about 73 EMS calls per day throughout the year.

Comparative Analysis

Research in other jurisdictions show that impacts to local communities are manageable and are typically offset by the new local tax dollars generated by the development. Based on casino evaluations performed by Purdue University and other research institutions on behalf of the Indiana Gaming Commission, statewide average actual costs borne by host communities are approximately 0.3% of gaming revenues. A study of the fiscal impact of Belterra on Switzerland County, Indiana in 2005 concluded, “...the added property, wagering and admissions taxes, and the incentive payments, exceeded the costs imposed by the riverboat. This allowed the county to increase appropriations—to cover added riverboat costs and to provide more public services—while charging Switzerland taxpayers less”.²

The introduction of a casino can lead to an increase in traffic patrol requirements and in the number of calls for police service. Arrests or citations related to increased visitation to the local area will create increased caseloads for the local judiciary. Even calls not resulting in arrest or citation can result in a need for increased police staffing.

A large, well-equipped fire department usually does not have to increase fire personnel in order to respond to incidents at a new casino. However, rural communities which do not have the types of equipment needed to respond to incidents at buildings beyond a certain height (e.g. ladder truck) often invest in new equipment. In general, rural municipalities have more limited service-infrastructure to handle large-scale developments and increased visitation than large cities, where impacts are marginal relative to resources.

² *Five-Year License Renewal: Belterra Resort Indiana, LLC*, performed by Indiana University-Purdue University Indianapolis on behalf of the Indiana Gaming Commission, October 2005, page 36.

Average Incident Rates

Incident rates from the data above were utilized in order to establish an average incident rate that will be applied to the proposed Eklutna development in order to project the number of calls and charges that can be expected with the addition of a casino to the community.

Table 13: Average Criminal Incident Rate

	Estimated Calls	Estimated Arrests
Yakama Nation Legends Casino - Toppenish, WA 2021	0.026%	0.006%
Northern Quest Resort & Casino - Airway Heights, WA 2021	0.025%	0.006%
Western US Tribal Gaming Facility 2019	0.044%	0.010%
Pacific Northwestern US Tribal Gaming Facility 2019	0.008%	0.002%
Average	0.026%	0.006%

In 2023, there were over 353,000 calls for service reported to Anchorage Police, excluding traffic stops. It is estimated that the proposed Eklutna casino will have approximately 199 police calls in 2027. These represent an increase of roughly 0.1% over projected 2027 volumes.

Table 14: Eklutna Casino Police Arrests Estimate

2027 Projected Casino Attendance	766,900
Average arrest rate	0.026%
Projected Service Calls	199
2027 Projected Calls for Service (Excluding Traffic Stops)	387,100
2027 Projected Service Calls with Eklutna Casino	387,299
% Change	0.1%

For additional information on criminal incidents and qualitative data from other jurisdictions, see Appendix B.

Fire Incident Rates

Incident rates from the data above was utilized in order to establish an incident rate that is applicable to the proposed Eklutna facility to project the number of calls the local fire departments can expect.

Table 15: Average Fire Department Call Rate Estimate

	Estimated Fire Incident Rate
Western US Tribal Gaming Facility	0.005%
Pacific Northwestern US Tribal Gaming Facility	0.007%
Average	0.006%

Source: The Innovation Group

As shown in the following table, it is estimated that the proposed casinos will result in an increase of 46 service calls in Anchorage. This is an increase of approximately 0.1% over the projected 2027 call volume.

Table 16: Eklutna Casino Fire/EMS Service Call Estimates

2027 Projected Casino Attendance	766,900
<i>Average service call rate</i>	<i>0.006%</i>
Projected Fire Service/EMS Calls	46
2027 Projected Fire/EMS Service Calls	47,245
2027 Projected Fire/EMS Service Calls with Eklutna Casino	47,291
<i>% Change</i>	<i>0.10%</i>

Conclusion and Implications

Fiscal Impacts and Municipal Services

Impacts arising from population and development growth would be effectively diluted by the size of Anchorage’s existing labor force, housing stock, and school capacity. Impacts arising from increased visitation—such as police, fire and EMS calls—are detailed below.

The gains in tax revenues that will accrue to the local governments as a result of increased economic activity generated by the Eklutna casino development and its employees, as detailed previously, will mitigate increases in municipal services expenses.

Estimated Municipal Expenses

The following table shows the estimated expenses attributable to the subject development from police, courts and public attorneys (both defending and prosecuting), corrections, and fire/EMS services, based on the Comparative Analysis section above and details below, with percent change estimates from that analysis applied to future budget estimates.

Table 17: Anchorage Municipality Police, Courts, and Fire/EMS Expense Increase – 2027

	Annual Budget		
Police	\$154,256,582	<i>Incremental %</i>	0.1%
		<i>Incremental \$</i>	\$79,457
Courts & Public Attorneys	\$11,543,410	<i>Incremental %</i>	0.5%
		<i>Incremental \$</i>	\$46,628
Corrections*	\$56,350,800	<i>Incremental %</i>	0.5%
		<i>Incremental \$</i>	\$284,718
Fire/EMS	\$125,520,339	<i>Incremental %</i>	0.1%
		<i>Incremental \$</i>	\$122,646
Total Increase in Municipal Services			\$533,449

Sources: City of Anchorage, The Innovation Group

*Statewide Alaska Corrections Budget and Incremental cost estimates based on data availability

To estimate court and attorney costs for the Anchorage Municipality, we calculated the maximum costs to the Anchorage judicial system under the highly conservative assumption that all 46 arrests estimated at the proposed Eklutna casino would not have pleaded out. Nearly 10,000 trial superior court cases were filed in the Anchorage Municipality in 2023; 46 cases would represent just 0.5%. The Municipality would likely be able to absorb that de minimis increase without incurring additional costs. However, to err on the side of caution, the attributable cost is estimated to be approximately over \$46,600.

Table 18: Anchorage Municipality Judicial Costs

	2023 Budget
Equal Rights Commission	\$836,568
Equity and Justice	\$437,953
Municipal Attorney	\$8,599,366
Total	\$9,873,887
<i>Annual Trial Superior Court Case Filings in Anchorage 2023</i>	<i>9,741</i>
<i>Annual Cases at Eklutna Casino</i>	<i>46</i>
<i>% at Eklutna Casino</i>	<i>0.5%</i>
<i>Cost</i>	<i>\$46,628</i>

Sources: City of Anchorage, The Innovation Group

While the Anchorage Municipality is home to the Anchorage Correctional Complex, the Alaska Department of Corrections is responsible for managing the state’s correctional facilities. Within the state budget for the fiscal year of 2023, a total of about \$56.4 million was allocated to the two correctional facilities in the Anchorage Municipality: Anchorage Correctional Complex and Hiland Mountain Correctional Center. With a total of approximately 9,000 bookings per year, the Anchorage Municipality spends an average of approximately \$6,000 per booking. The 46 projected bookings from the Eklutna casino only represent a 0.5% increase in bookings, so it is likely that the Alaska Department of Corrections could absorb this impact with little if any additional spending as a significant portion of their budget goes to fixed costs that would not

increase due to additional bookings. However, to be conservative, the attributable cost is estimated to be roughly \$285,000.

Table 19: Alaska Correctional Costs

FY 2023 Alaska Corrections Budget	\$56,350,800
FY2023 Bookings by AK Dept. of Corrections	9,107
Annual Projected Bookings from Eklutna Casino	46
Cost	\$284,718
% Change	0.5%

Source: Alaska Department of Corrections, The Innovation Group

Problem Gambling

Since Alaska does not currently offer any state-funded problem gambling services, we do expect a fiscal impact from the creation of prevention and treatment programs. The size and scope of these services will depend on the prevalence of problem gambling in the state. Since gambling is already in Alaska via Charitable Gaming, it is reasonable to assume a problem gambling population currently exists. We look to other markets and research studies to estimate the current number of problem gamblers in Alaska.

One of the most frequently cited studies on prevalence rates is *Estimating the Prevalence of Disordered Gambling Behavior in the United States and Canada: A Meta-analysis by the Harvard Medical School Division on Addictions*. The Harvard Medical School study analyzed 152 distinct previous prevalence studies and determined that 2.9% of the adult population could be considered problem or pathological gamblers. The *2021 Survey of Problem Gambling Services in the United States*³ conducted by the Association of Problem Gambling Service Administrators and the National Council on Problem Gambling estimated 2.2% of Alaska adults have a gambling problem. Combining these two studies, we estimate there are currently around 14,000 problem gamblers in Alaska, of which about 5,500 reside in Anchorage. Studies on problem gambling comorbidities show high rates of alcohol use disorder among problem gamblers⁴. Given Alaska’s prevalence of binge drinking⁵ is near the upper end of the national distribution, we would forecast the likely population of Alaska problem gamblers, after casinos are introduced, to be higher than the national average. Therefore, when assessing mitigation costs, we analyze a range from 2.2% (current estimates) to 5%, which we believe will be comfortably higher than the actual prevalence of problem gambling.

³ https://naadgs.org/wp-content/uploads/2022/06/NAADGS_2021_Survey_of_Publicly_Funded_Problem_Gambling_Services_in_the_United_States_v2.pdf

⁴ <https://pubmed.ncbi.nlm.nih.gov/15889941/>

⁵ Alaska has the 10th highest binge drinking prevalence in the country according to data obtained from the CDC <https://www.cdc.gov/alcohol/data-stats.htm>

To estimate mitigation costs for Alaska, we return to the *2021 Survey of Problem Gambling Services in the United States*, which includes data on state-funded problem gambling programs. We used three approaches from this report to triangulate the costs of a mitigation program for the state. First, we consider the distribution of per capita problem gambling services spend across the states. Second, we look at the distribution of total problem gambling services spend by state. Third, the study regresses problem gambling services spend by state on estimated number of problem gamblers by state. We use Alaska’s estimated number of problem gamblers to predict spend. These three approaches, described in more detail below, yield a range of about \$1 million to \$1.25 million in problem gambling services spend.

Competitive Effects Analysis

Alaska does not offer any Class III gaming; however, there are two approved Class II Indian Gaming facilities that operate under provisions approved by the National Indian Gaming Commission (NIGC). They are Klawock Bingo in Klawock and M.I.C. Gaming Hall in Metlakatla. They are located in the southeastern most portions of Alaska approximately 19 hours south of Juneau that includes both driving and a ferry ride. In short, these facilities are located multiple days travel away from the proposed Eklutna facility. Based on the analysis completed by The Innovation Group, we do not believe there will be any impact on these two existing facilities.

Charitable gaming, in the forms of pull-tabs, raffle/lottery, and bingo make up nearly 99% of all charitable gaming offered in Alaska. In 2019, the most recent available report not impacted by COVID, the charitable gaming adjusted gross income is estimated at over \$92 million. In 2020, as reported by the Alaska Department of Revenue, charitable gaming revenue decreased to under \$68 million, a decrease of about 30%. However, as with other gaming jurisdictions around the county, we believe that charitable gaming revenue has rebounded and even surpassed that of pre pandemic levels.

The Innovation Group used a gravity model to assist in realizing the impact of the proposed Class II Eklutna casino on the existing pull tab market. Pull tabs are offered at a wide range of locations across the state, including dozens in the greater Anchorage area. As a result, we utilized several of the more prominent locations in the area as a proxy within the model to represent the greater Anchorage pull tab market. We first calibrated the model to map the estimated current Anchorage pull tab market, then added the proposed Eklutna facility to the model to assess the impact it would have on that market.

The following table quantifies the impacts on charitable gaming from the proposed Eklutna casino development.

Table 20: Impact on Charitable Gaming from Casino

	2019	2027	Impact
Adjusted Gross Income (MM)	\$92.5	\$112.7	-\$24.3
Net Proceeds	-	21.8%	-21.6%

Source: AK Dept of Revenue; The Innovation Group

INTRODUCTION

Acorn Environmental (Acorn or Client) commissioned The Innovation Group for an Economic Impact Analysis and Community Impact Analysis of the proposed Native Village of Eklutna casino in Chugiak, Alaska in support of environmental documentation being prepared in compliance with the National Environmental Policy Act (NEPA). The Project is being pursued by the Native Village of Eklutna (Eklutna) and is expected to open with the first year of operation being 2027. Impacts are expressed in Year Two stabilized operations or 2028. Construction impacts are expressed in one calendar year prior to the opening on the casino, 2026.

The report summarizes the economic and community impacts—including job creation, increases in labor income, and positive revenue impacts for other local and state businesses, among others—that could be expected from the planned development, as well as the projected impacts on other tribal gaming facilities in the area.

Within the report we detail the Economic Impact Analysis, summarizing both our methodology and the corresponding results, followed by the Social and Community Impact Analysis and Competitive Effects Analysis.

ECONOMIC IMPACT ANALYSIS

The economic benefits—the revenues, jobs, and earnings—that accrue from the annual operations of an enterprise are termed *ongoing* impacts. The construction phase of a project is considered a *one-time* benefit to an area. This refers to the fact that these dollars will be introduced into the economy only during construction; construction impacts are expressed in single-year equivalence to be consistent in presentation with ongoing annual impacts.

The economic impact of an industry consists of three layers of impacts:

1. Direct effects
2. Indirect effects
3. Induced effects

The **direct effect** is the economic activity that occurs within the industry itself. The direct effect for casino operations represents the expenditures made by the facility in the form of employee compensation and purchases of goods and services (direct expenditures), which ultimately derive from patron spending on the casino floor, and patron spending on non-gaming amenities is an additional direct effect.

Indirect effects are the impact of the direct expenditures on other business sectors: for example, the advertising firm who handles a casino's local media marketing. Indirect effects reflect the economic spin-off that is made possible by the direct purchases of a casino. Firms providing goods and services to a casino have incomes partially attributable to the casino.

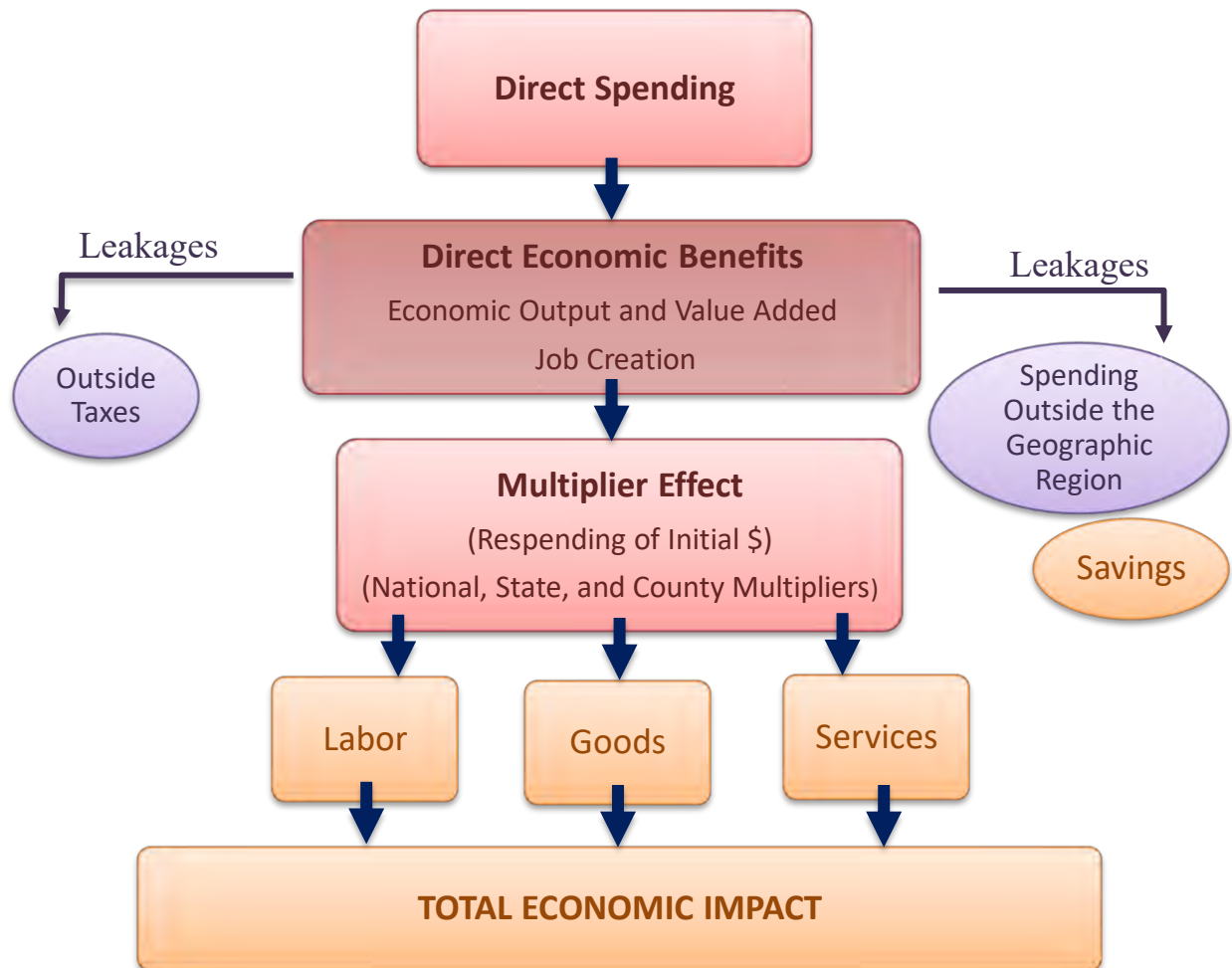
Finally, the **induced effects** result from the spending of labor income: for example, casino employees using their income to purchase consumer goods locally. As household incomes are affected by direct employment and spending, this money is recirculated through the household spending patterns causing further local economic activity.

The **total** economic impact of an industry is the sum of the three components.

Determining the direct economic impact is a critical first step in conducting a valid economic impact analysis. Once the direct expenditures are identified, the indirect and induced effects are calculated using multipliers derived from an input-output model⁶ of the economy. The IMPLAN input-output model identifies the relationships between various industries. The model is then used to estimate the effects of expenditures by one industry on other industries so that the total impact can be determined. Industry multipliers are developed based on U.S. Census data. IMPLAN accounts closely follow the accounting conventions used in the "Input-Output Study of the U.S. Economy" by the Bureau of Economic Analysis.

⁶ IMPLAN Online software and data were utilized for this study.

The following flow-chart shows how the economic impact model operates.



Multipliers

As shown in the following table, the standard data from IMPLAN for Sector 503-Gambling Industries (Except Casino Hotels) at the state level showed Other Property Income (OPI) at approximately 41% of total Output per Worker. Based on our experience analyzing the economic impacts of gaming within states that have existing casino resort operations and our knowledge of casino industry profitability, The Innovation Group believes this is an appropriate OPI to total Output per Worker ratio. We believe the Alaska state data within IMPLAN will provides realistic estimate of casino profitability and the corresponding economic impacts that will flow through the state's economy due to the introduction of gaming.

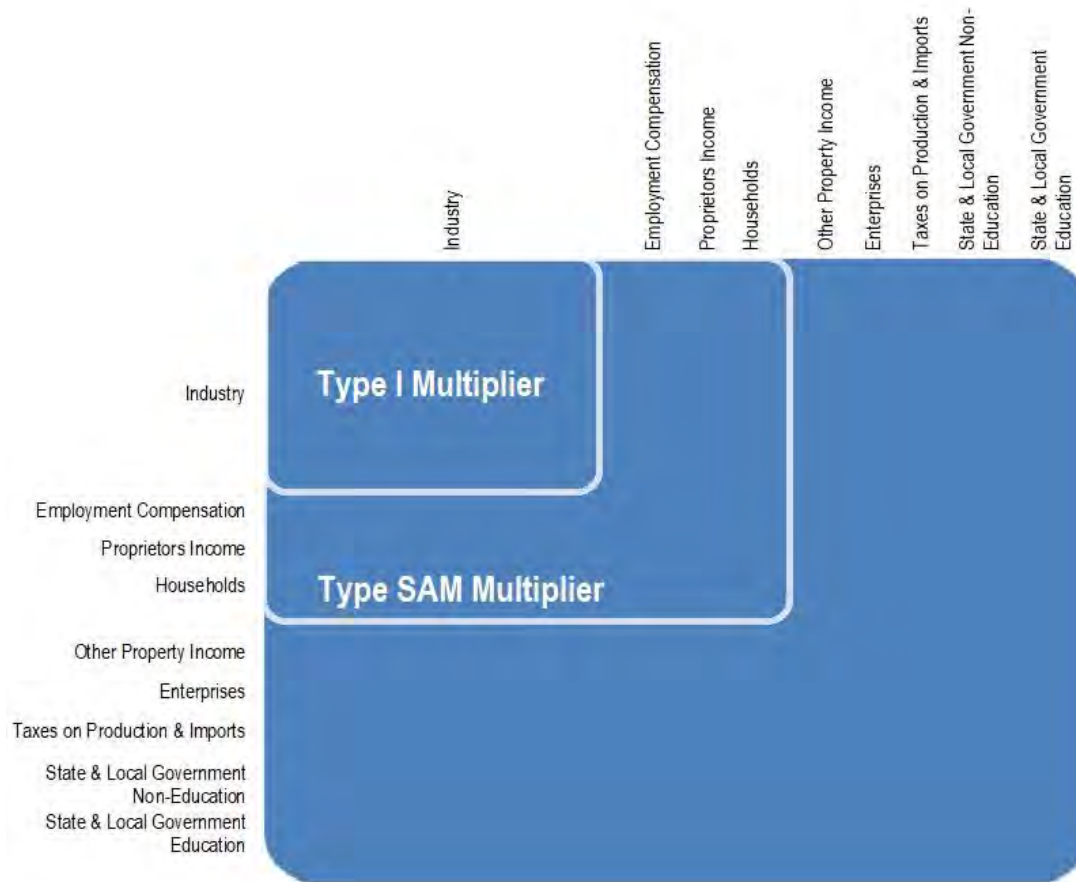
Table 21: IMPLAN Industry Sector 503 – Alaska State Data

Industry Ratio	Value (000's)	%
Employment Compensation (EC)	\$71,395	22%
Proprietor Income (PI)	\$7,280	2%
Other Property Income (OPI)	\$133,174	41%
Tax on Production & Imports (TOPI)	\$15,012	5%
<i>Value Added</i>	<i>\$226,860</i>	<i>69%</i>
Intermediate Expenditures (IE)	\$101,676	31%
Output per worker	\$328,536	

Source: IMPLAN Group, LLC, IMPLAN System (data and software); The Innovation Group

Compared to other industries with lower profitability levels, the gaming industry’s multipliers are lower since more of the output is shifted away from Intermediate Expenditures into Other Property Income (OPI). Multipliers are not applied to OPI in an economic impact analysis since it does not stimulate any additional impacts that can be attributed to the study area. For example, corporate profits from a casino operation may accrue to a company based in another state, effectively a leakage from the model. In other words, by generating higher OPI, more of the Output is effectively leaked out of state, and the multiplier effect is reduced. The table below illustrates.

Figure 1: IMPLAN Modeling Components



The following table shows the output multipliers for the Alaska state model for industry sector 503, Gambling Industries (Except Casino Hotels). To illustrate, an increase in direct effect of \$1,000,000 would produce a total effect of \$1,480,000 in the model.

Table 22: Output Multipliers for IMPLAN Industry Sector 503 – Alaska State

Multiplier	Standard Model
Type I	0.27
Type II	0.22
Total (SAM)	0.48

Source: IMPLAN Group, LLC, IMPLAN System (data and software); The Innovation Group

The Analysis-by-Parts method results in a much more conservative and we believe realistic estimate of the indirect and induced (or multiplier) effects of the operation of the casino component. The inputs into the IMPLAN casino model consist solely of the proforma estimates of employee compensation and purchases by the casino of goods and services. Operating profit and gaming taxes are excluded from the multiplier effect, although they are included in the displays of direct effects.

A Note on Substitution

Casino development frequently elicits concern that a substitution of consumer spending (the substitution effect) will negatively impact local businesses, especially smaller “mom and pop” retail, restaurant, and other entertainment industries. Intuitively it seems to be logical that spending at a casino would be diverted from other consumer activities such as going to a movie or taking a trip to the beach. However, numerous empirical studies have failed to find any conclusive evidence of significant economic substitution after the introduction of new casinos, nor is there any conclusive evidence as to the amount of spending that is substituted or the industry that it would have otherwise been spent in.

It is likely that countervailing positive effects dilute or outweigh any substitution that occurs. First, there is the increased household income in the area from casino employment. Secondly, there is a substantial body of research and case studies demonstrating the positive impacts that casinos have on surrounding local businesses. A review of studies of casino impacts on local business shows that casinos can stimulate local economies, resulting in communitywide growth, including in the local food and beverage business and retail businesses. Casino visitors stop at local retail outlets and restaurants in addition to some overnight casino guests patronizing local non-casino hotels. More information on local business impacts is contained in Appendix C.

It was determined after careful consideration that any substitution effects that may occur in the state as a result of the greater Anchorage area gaming operations would not be modeled in the economic impact analysis.

Economic Impact Modeling

The IMPLAN tools utilized to model direct effects vary according to the type of data collected for each input segment. There are six types of economic activity changes that IMPLAN is designed

to model for: industry, commodity, labor income, household income, industry spending pattern, and institutional (government) spending patterns. The most commonly used activity is an industry change, as the business generating a change in revenue, labor, or employment is often known and attributable to a specific industry sector.

The IMPLAN sectoring scheme is based on the 6-digit North American Industry Classification System (NAICS), developed under the auspices of the Office of Management and Budget (OMB), which classifies business establishments based on the activities they are primarily engaged in or the commodities they create. IMPLAN's current sectoring scheme aggregates the 2017 version of the NAICS classification scheme down to just 546 industry sectors. When an industry and the commodity produced by the industry have the same name, the commodity is considered the primary product of that industry and will share the same sector code. Other commodities produced by that industry are considered secondary products of that industry. Therefore, it is possible for more than one industry to produce a specific good or service.

When using the Industry Change function, the direct effect values are entered into IMPLAN using the appropriate sector and IMPLAN calculates the multiplier effects resulting from that direct spending. A commodity change will distribute the total demand or sales for the good or service as an industry change across all producing industries or institutions, based on their regional market share distribution of that commodity. For construction impacts as well as impacts from architectural and engineering, the Industry Change function was most appropriate for modeling the costs associated with land improvements, building, and design related costs. Costs associated with purchases of Furniture, Fixtures and Equipment (FF&E) and gaming machines were modeled using the Commodity Change function. The Industry Change function was also utilized for casino-related amenity operations including hotel, food and beverage, meetings and entertainment, retail/other, and museum/cultural center revenues.

Analysis-by-Parts for Gaming-Related Operating Impacts

The Analysis-by-Parts (ABP) differs from the traditional Industry Change Activity, as it separates out the multiplier effects into individual impact components, Intermediate Expenditure (indirect impacts from Type I multipliers) and Labor Income (induced impacts from Type II multipliers).⁷ This allows for more flexibility and customization capabilities in the analysis to model actual business operations.

For the Labor Income (LI) component we used a Labor Income Change activity to analyze the impact of the payroll of casino operations necessary to meet the demand or production level. The direct input for Labor Income in the casino analysis consisted of Employee Compensation from our operating pro forma models.

⁷ Economic impact multipliers consist of Type I, which measures only business-to-business purchases (indirect). Type II multipliers in the Bureau of Economic Analysis method measure the effects of local Household spending (induced). SAM (social accounting matrix) multipliers in the IMPLAN systems measure the combined indirect and induced effects.

For Intermediate Expenditures (IE), we import an Industry Spending Pattern to specify the goods and services of industry purchases needed for the sector 503 - Gambling industries (except casino hotels) in order to satisfy projected casino revenues. The purchase of these goods and services from local sources actually represents the first round of indirect purchases by the casino industry. The coefficients listed in an Industry Spending Pattern represent the amount spent on each commodity to produce one dollar of the industry's output, while the sum of all commodity coefficients equals total intermediate expenditures used by that industry sector.

Since the ABP technique shifts the direct inputs to indirect and induced impact results, the direct effects of these components are imputed using proforma operating statements, and in this instance was provided by Marnell Companies. The Analysis-by-Parts method results in a much more conservative and we believe realistic estimate of the indirect and induced (or multiplier) effects of the operation of the casino component. The inputs into the IMPLAN casino model consist solely of the proforma estimates of employee compensation and purchases by the casino of goods and services. Operating profit and gaming taxes are excluded from the multiplier effect, although they are included in the displays of direct effects.

Interpreting Results

The IMPLAN analysis expresses impacts (direct, indirect, and induced) for the following four economic variables:

Employment is measured in IMPLAN and by the U.S. Census as headcount, in other words the number of full and part-time workers supported by an economic activity.

Labor Income (LI) is compensation to all workers both employees and owners in terms of wages and salaries as well as benefits and payroll taxes. Profits from self-employed businesses can also be included in this category as compensation to the owner. These are known as employment compensation (EC) and proprietor income (PI) in IMPLAN. $LI = EC + PI$

Value-Added (VA) measures the industry or event's contribution to Gross Domestic Product (GDP). It consists of labor income (as described above), taxes on production and imports (TOPI), and other property income (OPI, such as corporate profits, rent payments, and royalties). It is the difference between a business or industry's total sales and the cost of all input materials or intermediate expenditures. $VA = LI + TOPI + OPI$

Output is the total value of industry production; it consists of value-added plus intermediate expenditures (IE). Output is frequently the total price paid by consumers for a good or service. $Output = VA + IE$

Value-Added is the most appropriate measure of economic impact because it excludes intermediate inputs, which are the goods and services (including energy, raw materials, semi-finished goods, and services purchased from all sources) used in the production process to produce *other* goods or services rather than for *final* consumption. For example, the paper stock used in a magazine publication is an intermediate input whereas paper stock sold in an office-supply store

is the final product sold to the consumer. The value of producing the magazine’s paper stock is accounted for in measures of GDP within the Paper Manufacturing sector, not in the Publishing sector.

Ongoing Operations

The ongoing operations of the gaming components in Chugiak and the greater Anchorage area will result in ongoing economic benefits that will accrue annually to the state of Alaska. Direct inputs for the casino development were derived from The Innovation Group’s previously completed gaming market assessment and pro forma analysis as provided by Marnell Companies of the casino property.

Proposed development on the project site includes a 58,000 square foot (sf) casino that will hold between 700 and 1,000 Class II gaming devices, dependent of floor layout plan, and 438 paved surface parking spaces. The building would consist of one-story and would include approximately 33,000 square feet of gaming floor and approximately 24,000 square feet of support and circulation space. The casino would be open for up to 24 hours a day, 7 days a week, and would employ approximately 225 staff members (inclusive of food and beverage operations).

Operating Inputs

Direct effect inputs for casino operations account for the workers employed at the facility and the compensation they earn as well as direct spending (less any promotional rewards or benefits received) by the gaming operations. Staffing and employment compensation estimates were based on Marnell Companies (Marnell) operating pro forma model and input into the IMPLAN software. Marnell, as a long stand company with a great reputation in the industry as an architect, developer, interior designer, construction supervisor, and property manager, used the top line revenue inputs provided by The Innovation Group to estimate proper staffing of this size and magnitude. The staffing model has been calibrated to actual operating data from existing casinos and is on a Headcount basis.

The following table shows the total inputs utilized in the IMPLAN modeling for the Eklutna casino gaming development. Please note, relevant values for the assumed second full year of operations were used as input values for the direct effect inputs.

Table 23: Direct Effect Inputs – Ongoing Operations (000’s)

Industry Change	Revenue (MM)	Employment	Employee Compensation (MM)
495 Gambling industries (except casino hotels)	\$77,524	137	\$8,847
501 Full-service restaurants	\$7,272	91	\$3,316
Total	\$84,796	228	\$12,163

Source: IMPLAN Group, LLC, IMPLAN System (data and software); The Innovation Group.

Annual Economic Impacts from Operations

The following section presents the ongoing economic impacts resulting from the Eklutna casino gaming operating activities. These impacts occur annually and can be thought of as long-term benefits both locally and within the state as a whole.

Based on the operating data forecasted by The Innovation Group, the operations of the potential Eklutna gaming development are estimated to directly support 228 local workers annually, with annual labor income equaling \$12.2 million and total added value to the economy of \$48.1 million. These direct impacts drive a further \$19.6 million in added value to the economy and 190 jobs from indirect and induced effects.

In total, the local region is estimated to benefit from annual employment impacts of 419 workers, \$23.5 million in labor income and \$67.6 million in total value added, as shown in the table below.

Table 24: Eklutna Casino Local Ongoing Impacts

Impact Type	Employment	Labor Income (MM)	Value Added (MM)	Output (MM)
Direct Effect	228	\$12.2	\$48.1	\$84.8
Indirect Effect	116	\$6.8	\$11.2	\$21.6
Induced Effect	74	\$4.6	\$8.3	\$13.8
Total	419	\$23.5	\$67.6	\$120.2

Source: IMPLAN Group, LLC

Fiscal Impacts

Fiscal impacts from ongoing operations were primarily estimated utilizing IMPLAN software. Fiscal impacts resulting from IMPLAN include business taxes, payroll taxes, property taxes, and other relevant taxes both locally and statewide. Please note, we have excluded direct sales tax, property taxes, and corporate tax at the local, state, and federal level estimated by IMPLAN under the assumption that the Eklutna is exempt from these taxes. Based on the forecasted operations of the gaming development, IMPLAN estimates that \$446,000 of tax revenue would accrue to local governments, while over \$2.4 million of tax revenue would accrue to the state government, and an additional \$5.4 million would accrue to the federal government. It is important to note that the fiscal impacts estimated by IMPLAN and illustrated in the tables below exclude any gaming compact contributions generated for Alaska and includes taxes from direct, indirect, and induced effects.

Table 25: Local Tax Impact: Eklutna Casino Ongoing Operations (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$0.0	\$0.0	\$0.0	\$0.0
Social Insurance Tax- Employer Contribution	\$0.0	\$0.0	\$0.0	\$0.0
TOPI: Sales Tax	\$0.0	\$37.0	\$43.9	\$80.9
TOPI: Property Tax	\$0.0	\$149.5	\$177.5	\$327.0
TOPI: Motor Vehicle License	\$5.2	\$0.5	\$0.6	\$6.4
TOPI: Severance Tax	\$1.3	\$0.1	\$0.2	\$1.6
TOPI: Other Taxes	\$13.6	\$1.4	\$1.7	\$16.7
TOPI: Special Assessments	\$6.2	\$0.6	\$0.8	\$7.6
OPI: Corporate Profits Tax	\$0.0	\$0.0	\$0.0	\$0.0
Personal Tax: Motor Vehicle License	\$2.2	\$1.1	\$0.8	\$4.1
Personal Tax: Property Taxes	\$0.9	\$0.5	\$0.3	\$1.7
Personal Tax: Other Tax (Fish/Hunt)	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$29.4	\$190.7	\$225.7	\$445.9

Source: IMPLAN Group, LLC

Table 26: State Tax Impact: Eklutna Casino Ongoing Operations (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$1.1	\$0.4	\$0.3	\$1.8
Social Insurance Tax- Employer Contribution	\$1.1	\$0.4	\$0.3	\$1.8
TOPI: Sales Tax	\$0.0	\$25.5	\$30.3	\$55.8
TOPI: Property Tax	\$0.0	\$11.7	\$13.9	\$25.7
TOPI: Motor Vehicle License	\$9.3	\$1.0	\$1.1	\$11.4
TOPI: Severance Tax	\$1,740.9	\$180.0	\$213.8	\$2,134.6
TOPI: Other Taxes	\$58.3	\$6.0	\$7.2	\$71.5
TOPI: Special Assessments	\$0.0	\$0.0	\$0.0	\$0.0
OPI: Corporate Profits Tax	\$0.0	\$64.0	\$53.5	\$117.5
Personal Tax: Motor Vehicle License	\$4.0	\$2.0	\$1.3	\$7.3
Personal Tax: Property Taxes	\$0.1	\$0.0	\$0.0	\$0.1
Personal Tax: Other Tax (Fish/Hunt)	\$45.8	\$24.4	\$16.2	\$86.4
Total	\$1,814.7	\$291.1	\$321.7	\$2,427.5

Source: IMPLAN Group, LLC

Table 27: Federal Tax Impact: Eklutna Casino Ongoing Operations (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$843.0	\$404.0	\$275.0	\$1,522.0
Social Insurance Tax- Employer Contribution	\$752.0	\$306.9	\$217.4	\$1,276.3
TOPI: Excise Tax	\$79.2	\$8.2	\$9.7	\$97.1
TOPI: Custom Duty	\$88.2	\$9.1	\$10.8	\$108.2
OPI: Corporate Profit Tax	\$0.0	\$158.7	\$132.5	\$291.2
Personal Tax: Income Tax	\$1,111.6	\$591.9	\$393.5	\$2,097.0
Personal Tax: Estate and Gift Tax	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$2,874.1	\$1,478.8	\$1,039.0	\$5,391.9

Source: IMPLAN Group, LLC

Construction

Construction of the proposed development would bring one-time (non-recurring) benefits to Alaska. Construction impacts are expressed on a single-year basis. Therefore, the employment figures, for example, represent person-year equivalents; for a construction period of two years, the actual number of workers onsite would be half the person-year equivalent.

The impact of construction only relates to expenditures made directly by the development company to design, build and outfit the physical structure. For construction, architectural, and engineering impacts, the Industry Change function was employed using sectors 55-Construction of New Commercial Structures, Including Farm Structures and 457-Architectural, Engineering, and Related Services. Costs associated with purchases of Furniture, Fixtures and Equipment (FF&E) and machine purchases were modeled using the Commodity Change function sectors 3393-Wholesale Trade Services-Professional and Commercial Equipment and Supplies and 3391-All Other Miscellaneous Manufactured Products.

Construction Inputs

Based on high-level construction capital costs estimated by Marnell Companies, the following table outlines the final inputs used to calculate the economic impact by sector. The cost of gaming machines was separated out from the other FF&E. IMPLAN estimates what percentage of the purchases, including gaming machines, will originate from within the study area based on its Social Accounting Matrix (SAM).

Table 28: Eklutna Casino Estimated Construction Cost Inputs

Component	
<i>Industry Change</i>	
55 Construction of New Commercial Structures	\$42,000,000
457 Architectural, engineering, and related services	\$2,500,000
<i>Commodity Change</i>	
3393 Wholesale trade services	\$9,500,000
3391 All other miscellaneous manufactured products	\$17,500,000
Total Direct	\$71,500,000

Source: IMPLAN Group, LLC; Marnell Companies

Economic Impacts from Construction

Based on the construction capital costs estimated by The Innovation Group, the IMPLAN model estimates that construction of the Eklutna casino development will directly support 336 workers locally, with labor income equaling \$27.6 million and total added value to the economy of \$28.4 million. These direct impacts drive a further \$17.5 million in added value to the economy and 150 jobs from indirect and induced effects.

In total, the local region is estimated to have benefited from a one-time, single-year equivalent employment impact of 487 workers, \$37.2 million in labor income and \$45.8 million in total value added, as shown in the table below.

Table 29: Eklutna Casino Local Construction Impacts

Impact Type	Employment	Labor Income (MM)	Value Added (MM)	Output (MM)
Direct Effect	336	\$27.6	\$28.4	\$46.7
Indirect Effect	35	\$2.5	\$4.7	\$9.4
Induced Effect	115	\$7.1	\$12.8	\$21.2
Total	487	\$37.2	\$45.8	\$77.3

Source: IMPLAN Group, LLC
 Note: Single Year Equivalents

Fiscal Impacts

Fiscal impacts resulting from IMPLAN include business taxes (including sales taxes), payroll taxes, property taxes, and other relevant taxes both locally and statewide as well as at the federal level. Based on the construction cost estimates for the Eklutna casino gaming development, the one-time fiscal impacts from construction would contribute \$628,400, \$1.0 million, and \$7.6 million to the local, state, and federal governments, respectively.

Table 30: Local Tax Impact from Construction: Eklutna Casino (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$0.0	\$0.0	\$0.0	\$0.0
Social Insurance Tax- Employer Contribution	\$0.0	\$0.0	\$0.0	\$0.0
TOPI: Sales Tax	\$21.8	\$31.6	\$67.8	\$121.1
TOPI: Property Tax	\$88.0	\$127.6	\$274.0	\$489.5
TOPI: Motor Vehicle License	\$0.3	\$0.5	\$1.0	\$1.8
TOPI: Severance Tax	\$0.1	\$0.1	\$0.2	\$0.4
TOPI: Other Taxes	\$0.8	\$1.2	\$2.6	\$4.6
TOPI: Special Assessments	\$0.4	\$0.5	\$1.2	\$2.1
OPI: Corporate Profits Tax	\$0.0	\$0.0	\$0.0	\$0.0
Personal Tax: Motor Vehicle License	\$4.7	\$0.4	\$1.2	\$6.3
Personal Tax: Property Taxes	\$1.9	\$0.2	\$0.5	\$2.6
Personal Tax: Other Tax (Fish/Hunt)	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$118.0	\$162.1	\$348.4	\$628.4

Source: IMPLAN Group, LLC

Table 31: State Tax Impact from Construction: Eklutna Casino (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$1.6	\$0.2	\$0.5	\$2.2
Social Insurance Tax- Employer Contribution	\$1.6	\$0.2	\$0.5	\$2.3
TOPI: Sales Tax	\$15.0	\$21.8	\$46.7	\$83.5
TOPI: Property Tax	\$6.9	\$10.0	\$21.5	\$38.5
TOPI: Motor Vehicle License	\$0.6	\$0.8	\$1.8	\$3.1
TOPI: Severance Tax	\$105.9	\$153.6	\$329.9	\$589.5
TOPI: Other Taxes	\$3.5	\$5.1	\$11.0	\$19.7
TOPI: Special Assessments	\$0.0	\$0.0	\$0.0	\$0.0
OPI: Corporate Profits Tax	\$9.1	\$29.0	\$82.5	\$120.5
Personal Tax: Motor Vehicle License	\$8.3	\$0.7	\$2.1	\$11.1
Personal Tax: Property Taxes	\$0.2	\$0.0	\$0.0	\$0.2
Personal Tax: Other Tax (Fish/Hunt)	\$106.3	\$8.8	\$25.0	\$140.0
Total	\$259.0	\$230.2	\$521.4	\$1,010.6

Source: IMPLAN Group, LLC

Table 32: Federal Tax Impact from Construction: Eklutna Casino (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$1,660.9	\$148.1	\$423.7	\$2,232.7
Social Insurance Tax- Employer Contribution	\$1,126.9	\$115.4	\$335.1	\$1,577.4
TOPI: Excise Tax	\$4.8	\$7.0	\$15.0	\$26.8
TOPI: Custom Duty	\$5.4	\$7.8	\$16.7	\$29.9
OPI: Corporate Profit Tax	\$22.5	\$71.9	\$204.3	\$298.7
Personal Tax: Income Tax	\$2,582.8	\$213.9	\$606.1	\$3,402.7
Personal Tax: Estate and Gift Tax	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$5,403.3	\$564.1	\$1,600.9	\$7,568.3

Source: IMPLAN Group, LLC

SOCIAL AND COMMUNITY IMPACT ANALYSIS

This section assesses the social and community impacts of the proposed casino development in the Native Village of Eklutna.

Increased local services and costs due to casino gaming operations generally fall into three categories: those arising from population and development growth, those arising from the impacts of increased visitation and traffic, and social impacts resulting from problem gambling.

The analysis draws upon social science research as well as data analysis conducted by the Innovation Group. Although casinos are perceived to be different in kind from other commercial developments of comparable size and visitor base, inordinate negative impacts from casino development have not materialized, even in small communities with limited infrastructure and resources. In fact, experience over the past two decades has demonstrated that mitigation payments designed in anticipation of drastic impacts have often exceeded the actual need of the communities.

The perception that casinos breed crime is not supported by the evidence. While the *number* of reported crimes can increase, as with any commercial development that attracts visitors, casino gaming has not been shown to lead to an increase in crime *rates*.

Host communities should expect impacts similar in kind to other commercial developments of similar scope and visitor potential. The projected increase in visitor population should be expected to lead to increases in public safety services and judicial system caseload. The one significant difference in kind relates to the association between problem gambling and other social pathologies as discussed in Appendix A.

In summary, evidence suggests that on-going impacts to local communities are highly manageable, typically requiring only a small fraction of gaming revenues to address fully.

Section One: Population and Employment Related Impacts

The proposed Eklutna casino is expected to draw 816,000 annual gaming visits⁸ in 2027 and directly employ 228 people.

For the purposes of this section, the Anchorage Municipality will be used as the primary study area of impacts as it pertains to employment and increased population.

Employment Impact

As noted, the proposed Eklutna development is projected to require 228 jobs⁹ in 2027, the first full year of operations. These jobs are expected to be filled by a combination of local unemployed workers, local out-bound commuters, residents of neighboring cities, and new residents.

Unemployment in Anchorage stands at a little over 5,000 workers in 2023. While the 2020 recession adversely impacted unemployment in the Municipality, unemployment has declined below pre-covid levels as of 2023. Based on the historical growth rate, we have forecasted 3,511 unemployed Anchorage residents in 2027.

Table 33: Anchorage, AK Average Annual Employment Statistics

Year	Civilian labor force	Employment	Unemployment	Unemployment Rate
2010	157,743	146,993	10,750	6.8%
2011	158,382	148,731	9,651	6.1%
2012	157,748	149,177	8,571	5.4%
2013	157,637	149,423	8,214	5.2%
2014	157,650	149,591	8,059	5.1%
2015	156,476	148,856	7,620	4.9%
2016	155,471	147,254	8,217	5.3%
2017	154,046	145,674	8,372	5.4%
2018	151,067	143,566	7,501	5.0%
2019	149,118	142,485	6,633	4.4%
2020	148,392	137,421	10,971	7.4%
2021	149,263	140,525	8,738	5.9%
2022	149,832	144,596	5,236	3.5%
2023	150,823	145,654	5,169	3.4%

Source: Bureau of Labor Statistics, LAUS series, Not-seasonally adjusted; The Innovation Group

According to data from the US Census Bureau, over 96% of the 145,508 workers residing in Anchorage also work there, meaning nearly 4% commute out to other locations or work remotely

⁸ Visits, not individual patrons. Casino customers typically make more than one visit per year.

⁹ Headcount, including full and part-time workers.

for businesses located elsewhere. The percentage of jobs in Anchorage held by residents outside of Anchorage is 0.5%.

Table 34: Anchorage Commuting Patterns

A. Workers residing in Anchorage	145,508
B. Number of Jobs in Anchorage	140,800
C. Live & work in Anchorage	140,124
% who live & work in Anchorage	96.3%
% of A who commute OUT	3.7%
% of B who commute IN	0.5%

Source: US Census, Anchorage Economic Development Corporation (2022)

As the following tables illustrate, the existing workforce in the three locations is estimated to fill about 88% of the new jobs created by a casino development, and the remaining approximately 12% of workers are estimated to represent new residents moving into the area. Much of the analysis is based on comparable study data, such as the estimate 1.1% of future estimated employees going back to work. This estimate was based on observations of actual sources of employees at operating casino—we analyzed the percentage of employees at comparable casinos that came from their respective local unemployed populations and utilized an iterative process to determine a reasonable estimate for the subject casino. The subject casino is estimated to get 39 of its 201 workers (or 19.4% of its total workforce) from the local unemployed population. This 19.4% figure is in-line with The Innovation Group’s observations of actual casinos, with previously unemployed workers accounting for 15.0% to 20.0% of their respective workforces, on average. Additionally, there will be a number of employees that currently live in the Anchorage area that commute to areas confined by Anchorage but would then choose to change employment and not have to commute outside the area.

Table 35: Eklutna Casino Project Source of Workforce

Source of Employment	Proposed Project	Metric Applied	Source
A. Number of New Employees	228	Total Jobs	Marnell Companies
B. Number of Commuters from Outside Anchorage	1	0.5% based on Anchorage Workforce Commuting Patterns	US Census
C. Unemployed Back to Work	39	1.1% of a future estimate of 3,511 Unemployed Workers	US Census
D. Former Commuters Staying within Anchorage	162	3.0% of 5,384 Anchorage workers who currently commute outside the area	US Census
E. Total from Existing Area Workforce	201	B+C+D	
F. Estimated Total New Workers Needed	27	A-E	
% of New Employees	11.7%	F/A	

These estimates were based on previous Innovation Group research and results from an analysis of Plainridge casino in Plainville, Massachusetts. The survey of Plainridge employees demonstrates that casino employment is comprised mainly of workers already residing within commuting distance: a mixture of previously employed local residents looking for a better opportunity or work closer to home, along with previously unemployed local residents. The percentage of workers who moved to take the position with Plainridge was a small percentage of the staff. Furthermore, most casino workers had not had prior casino work experience.

Table 36: Plainridge Casino Source of Workforce

	# of Responses	Percentage
<i>Prior Employment status:</i>		
Unemployed	162	15.5%
Employed Part-time	363	34.7%
<i>Underemployed</i>	189	18.1%
Employed Full-time	522	49.9%
Total	1,047	100.0%
<i>Reason for taking the position</i>		
Job closer to home	305	29.1%
<i>Other results</i>		
No prior casino experience	902	86.2%
Moved to take the position	75	7.2%

New Employee Survey at Plainridge Park Casino: Analysis of First Two Years of Data Collection
 University of Massachusetts Donahue Institute, Economic and Public Policy Research Group, May 10, 2017

Other studies show similar impacts on employment. The Rappaport Institute for Greater Boston and the John F. Kennedy School of Economics at Harvard University (Baxandall and Sacerdote 2005) in a national, county-level study of Native American casinos found a slight decrease in unemployment rates after casinos opened. The analysis included all California casinos in existence in the 1990s. From their total sample of 156 casino counties, the Rappaport study isolated out 57 counties with large casinos and relatively low population and nine counties with both large casinos and large populations to see if there were statistical differences in terms of community impacts. The authors compared the county unemployment rate averaged for the year before and after a casino opens in a county, and then subtracted that number from the average state change in unemployment to isolate the county-specific effect. The following table shows their results:

Table 37: Rappaport Study Employment Results

	All Casino- Counties ¹	Counties with Large- Capacity Casinos ²	Populous Casino Counties ³
Population Growth (%)	+5*	+8.6	+8.1*
Total Employment (%)	+6.7*	+14.9*	+5.7
Unemployment (%)	-0.3	-1.2*	+0.5

*Statistically significant results at 99% confidence interval.

1. Reports how adjusted outcomes in 156 counties that introduced Indian-run casinos during the 1990s differed from the other 2,959 that did not.

2. The effect for 21 counties in the top 10th percentile in terms of number of slot machines (over 1,760).

3. The effect for the 57 casino counties in the top population quartile (over 55,000 residents).

The Rappaport study also highlighted results for three counties in southern California: Riverside, San Bernardino, and San Diego. In all three counties, the unemployment decreased relative to the state average. For example, before casino development, Riverside County had a slightly lower unemployment rate than the state average (by 0.3%). After casino development, the county’s unemployment rate was 1.7% lower than the state average, a relative decrease of 1.4 percentage points. San Bernardino had a relative decrease of 0.5 points and San Diego 0.4.

Table 38: Rappaport Study California County Results for Employment (%)

	Relative Unemployment % (County - State Average) Before	Relative Unemployment % (County - State Average) After	Change in Relative % Unemployment (Before - After)
Riverside, CA	-0.3	-1.7	-1.4
San Bernardino, CA	-2.2	-2.7	-0.5
San Diego, CA	-4.1	-4.5	-0.4

Household and Population Impact

Assuming approximately 1.1 casino workers per household, the total number of new households to Anchorage is estimated at 24. On average, households in Anchorage comprise 2.57 persons, bringing the total population increase to Anchorage to 63 people, or 0.02% of the projected 2027 Anchorage population.

Table 39: Eklutna Casino Impact on Households and Population

New employees moving to area	27
# of jobs per household	1.1
Number of new Households	24
Anchorage Avg. Household Size	2.57
New Area Population	63
% Increase of 2027 Total Area Pop	0.02%

School Impact

Based on the number of new households and Alaska household metrics, the increase to school enrollment is estimated to be approximately 13 children in Anchorage. These represent an increase of 0.03% over projected 2027 enrollment.

Table 40: Eklutna Casino Impact on School Enrollment

	Metric	Anchorage
Number of New Households		24
Number of Households That Will Have Children*	32.1%	8
<i>Number of Children per Household**</i>	<i>2.1</i>	
Total Number of Children		17
Projected School Age Children	80%	13
2027 Projected School Enrollment		48,779
2027 Projected School Enrollment with Eklutna Casino		48,792
<i>% Change</i>		<i>0.03%</i>

Source: ESRI, US Census Bureau ACS

*Anchorage ACS 2018-22 estimated percent of households with presence of children under the age of 18

**Anchorage ACS 2018-22 average number of children per household with children under the age of 18

School enrollment trends for the districts serving Anchorage can be found in the table below.

Table 41: Anchorage Annual School Enrollment

	2015-16	2016-17	2017-18	2018-19	2019-20	2020-21	2021-22	2022-23	CAGR
All Students (Public and AE)	48,827	48,789	48,098	46,689	46,701	41,856	43,054	43,727	-1.6%
Pre-Kindergarten	767	825	878	749	987	696	385	438	-7.7%
Kindergarten	3,929	3,827	3,786	3,641	3,595	2,869	3,364	3,285	-2.5%
Grades 1-8	29,673	29,732	29,415	28,407	28,643	25,457	26,271	26,852	-1.4%
Grades 9-12	14,458	14,405	14,019	13,446	13,476	12,834	13,034	13,152	-1.3%

Source: National Center for Educational Statistics (NCES)

Housing Impact

The additional households that are projected to relocate in and around the Anchorage area will have a small impact on the housing market. New households will support the current and future development of housing in the community.

Currently, the number of housing units is nearly 10% greater than the number of households in Anchorage. This denotes an adequate number of units available for residents. Five-year projections estimate that the growth of housing units will outpace growth of families and households in the market. According to ESRI data, the number of housing units is projected to grow by about 240 units by 2027 in Anchorage. This will be sufficient to absorb the households estimated to relocate.

Table 42: Anchorage Population and Housing Trends

	2010 Census	2024 ESRI	2027 Projection
Population	291,826	287,385	284,766
<i>Percent Change</i>		-0.11%	-0.30%
Households	107,332	109,102	109,221
<i>Percent Change</i>		0.12%	0.04%
Families	70,544	68,078	67,811
<i>Percent Change</i>		-0.25%	-0.13%
Housing Units	113,032	119,622	119,861
<i>Percent Change</i>		0.41%	0.07%

Source: ArcGIS/ESRI

As can be seen in the following table, Anchorage had a vacancy rate for housing units of nearly 9% in 2024, which translated to roughly 10,500 units. If the same percentage is applied to the number of housing units projected for 2027, then there will be approximately 10,550 available housing units to accommodate the incoming households. Consequently, there is a substantial and sufficient housing supply to accommodate the 24 households estimated to relocate to Anchorage.

The Anchorage Municipality has higher than average rates of renter-occupied housing compared to both the Alaska and national averages. The new jobs created at the proposed casino developments will provide stable employment for existing residents that can support homeownership, increasing homeownership figures throughout the local area. The addition of new families has the potential to support current and planned housing development throughout the city without overwhelming existing infrastructure.

Table 43: 2024 Occupied Housing Units by Tenure

	Anchorage	Alaska	United States
Owner-Occupied	55.7%	53.1%	57.9%
Renter-Occupied	35.5%	29.0%	32.1%
Vacant	8.8%	17.9%	10.0%

Source: ArcGIS/ESRI

Conclusion

Overall, the proposed casino development in Anchorage is projected to increase local population by approximately 0.02 percent and increase school enrollment by approximately 0.03 percent.

Section Two: Municipal Services Impact

Host communities should expect impacts similar in kind to other commercial developments of similar scope and visitor potential. The projected increase in visitor population should be expected to lead to increases in public safety services and judicial system caseload.

Evidence suggests that on-going impacts to local communities are highly manageable, typically requiring only a small fraction of gaming revenues to address fully. Of most direct consequence to casino development are police, fire, and EMS. These will largely fall into the purview of the local police and fire department serving Anchorage.

Police

The Anchorage Police Department (APD) serves the entire Anchorage Municipality with its patrol area encompassing the core "Anchorage bowl", the Seward Highway corridor from Potter Creek south to McHugh Creek, the Glenn Highway corridor north of the Anchorage bowl to the municipality's border with the Matanuska-Susitna Municipality, and includes the communities of Eagle River, Chugiak, and Eklutna. APD is the largest municipal police force in Alaska serving the state's largest city. The department employs 610 people who hold both sworn and non-sworn positions.

According to the 2024 Approved General Government Operating Budget Report, the approved 2024 budget was increased by about 3.5% to about \$140 million from the revised 2023 budget of about \$135 million. Roughly 78.4% of the approved 2024 budget funds the police department's salaries and benefits. From 2022 to 2023, the department's budget increased by about 6% from roughly \$127 million to about \$135 million. In 2023, the police department's salaries and benefits accounted for 76.4% of the total budget.

In 2023, the Anchorage Municipality saw almost 40,000 trial court case filings with a judicial budget of nearly \$10 million, divided between the Equal Rights Commission, Equity & Justice, and Municipal Attorney. The Municipal Attorney budget was the largest segment at nearly \$8.6 million. Although the Anchorage Correctional Complex is located within the Anchorage Municipality, the Alaska Department of Corrections (DOC) is responsible for managing the state's correctional facilities. The Alaska DOC had a budget of nearly \$423 million for fiscal year 2023, and during that same period there were almost 28,000 bookings.

Fire

The Anchorage Fire Department (AFD) provides fire protection and emergency medical services to Anchorage. The AFD serves numerous areas including the incorporated areas of downtown Anchorage, Bird, Bootleggers Cove, Eagle River, Fairview, Indian, Mountain View, Muldoon, and Spenard, AFD is assisted by two volunteer fire departments operating in the outlying areas of the Anchorage Municipality, specifically speaking about the Chugiak Volunteer Fire and Rescue Company (CVFRD) and the Girdwood Fire Department. According to the 2024 Approved General Government Operating Budget, there are 15 fire stations with 14 fire engines, 13 medic units, 5 fire trucks, and 5 tenders. The entire fire department employs just over 400 full-time positions. AFD's approved budget for 2024 is approximately \$115 million, increasing 2.3% from the previous year. Salaries and benefits for 76.4% of the 2024 budget. From 2022 to 2023, the AFD

budget decreased by about 2.2% from roughly \$115 million to about \$112 million. The figures are inclusive of the CVFRD budget that consists of over 100 members and responds to more than 1,000 emergency calls per year, of which most are medically related. Of note, Station 34 is the nearest fire station to the Project Site and is located approximately 0.2 miles to the north and is serviced by the CVFRD.

Emergency Medical Services (EMS)

The Anchorage Fire Department also provides EMS to Anchorage with a mutual aid agreement between the Anchorage Fire Department, Ted Stevens International Airport Police and Fire Department, Joint Base Elmendorf-Richardson, Girdwood Fire Department, and Chugiak Volunteer Fire and Rescue Department. In 2023, EMS calls accounted for nearly 71% of the AFD's total service calls, averaging about 73 EMS calls per day throughout the year.

Comparative Analysis

Research in other jurisdictions shows that impacts to local communities are manageable, typically requiring only a small fraction of gaming revenues to address fully. Based on casino evaluations performed by Purdue University and other research institutions on behalf of the Indiana Gaming Commission, statewide average actual costs borne by host communities are approximately 0.3% of gaming revenues. A study of the fiscal impact of Belterra on Switzerland County, Indiana in 2005 concluded, "...the added property, wagering and admissions taxes, and the incentive payments, exceeded the costs imposed by the riverboat. This allowed the county to increase appropriations—to cover added riverboat costs and to provide more public services—while charging Switzerland taxpayers less".¹⁰

The introduction of a casino can lead to an increase in traffic patrol requirements and in the number of calls for police service. Arrests or citations related to increased visitation to the local area will create increased caseloads for the local judiciary. Even calls not resulting in arrest or citation can result in a need for increased police staffing.

A large, well-equipped fire department usually does not have to increase fire personnel in order to respond to incidents at a new casino. However, rural communities, which do not have the types of equipment needed to respond to incidents at buildings beyond a certain height (e.g. ladder truck), often invest in new equipment. In general, rural municipalities have more limited service-infrastructure to handle large-scale developments and increased visitation than large cities, where impacts are marginal relative to resources.

Criminal Incident Rates

The following section provides data for the number of calls police respond to and the numbers of arrests made at a sampling of casinos throughout the western United States. The analysis includes

¹⁰ *Five-Year License Renewal: Belterra Resort Indiana, LLC*, performed by Indiana University-Purdue University Indianapolis on behalf of the Indiana Gaming Commission, October 2005, page 36.

several individual properties. Please note, due to the confidential nature of some of the data included in this section, some of the property names have been given generic labels.

Yakama Nation Legends Casino – Toppenish, Washington

Located in Toppenish, Washington, the Yakama Nation Legends Casino is an approximately 25-minute drive time from Yakima, Washington. The facility is roughly 3 hours northeast of Portland and two and a half hours southeast of Seattle. The following table provides attendance and service call figures utilized to calculate the incident rates for calls made to local police.

Table 44: Toppenish, WA Service Call Rates

Casino Attendance 2021	1,291,575
2021 Casino Service Calls	334
Incident Rate for Service Calls	0.026%

Source: Toppenish Police Department, Washington State Gambling Commission, The Innovation Group

Northern Quest Resort & Casino – Airway Heights, Washington

The Northern Quest Resort and Casino is located just outside of Spokane, Washington—roughly 15 minutes to the west. The following table provides attendance and service call figures utilized to calculate the incident rates for calls made to local police.

Table 45: Airway Heights, WA Service Call Rates

Casino Attendance 2021	1,691,348
2021 Casino Service Calls	422
Incident Rate for Service Calls	0.025%

Source: Airway Heights Police Department, Washington State Gambling Commission, The Innovation Group

Western US Tribal Gaming Facility

This western US tribal gaming facility received over 2 million visits in 2019. The following table provides attendance and service call figures utilized to calculate the incident rates for calls made to local police.

Table 46: Western US Tribal Gaming Facility Service Call Rates

Casino Attendance 2019	2,043,000
Average Casino Service Calls	895
Incident Rate for Service Calls	0.044%

Source: Proprietary and confidential data, The Innovation Group

Pacific Northwestern US Tribal Gaming Facility

This Pacific Northwestern US tribal gaming facility received nearly 2.5 million visits in 2019. The following table provides attendance and average annual service call figures utilized to calculate the incident rates for calls made to local police.

Table 47: Pacific Northwestern Tribal Gaming Facility Service Call Rates

Casino Attendance 2019	2,485,159
Average Casino Service Calls	191
Incident Rate for Service Calls	0.008%

Source: Proprietary and confidential data, The Innovation Group

Average Incident Rates

Incident rates from the data above were utilized in order to establish an average incident rate that will be applied to the proposed Eklutna development in order to project the number of calls and charges that can be expected with the addition of a casino to the community.

Table 48: Average Criminal Incident Rate

	Estimated Calls	Estimated Arrests
Yakama Nation Legends Casino - Toppenish, WA 2021	0.026%	0.006%
Northern Quest Resort & Casino - Airway Heights, WA 2021	0.025%	0.006%
Western US Tribal Gaming Facility 2019	0.044%	0.010%
Pacific Northwestern US Tribal Gaming Facility 2019	0.008%	0.002%
Average	0.026%	0.006%

In 2023, there were over 353,000 calls for service reported to Anchorage Police, excluding traffic stops. It is estimated that the proposed Eklutna casino will have approximately 199 police calls in 2027. These represent an increase of roughly 0.1% over projected 2027 volumes.

Table 49: Eklutna Casino Police Arrests Estimate

2027 Projected Casino Attendance	766,900
Service Call Rate	0.026%
Projected Service Calls	199
2027 Projected Calls for Service (Excluding Traffic Stops)	387,100
2027 Projected Service Calls with Eklutna Casino	387,299
% Change	0.05%

For additional information on criminal incidents and qualitative data from other jurisdictions, see appendix.

Fire Incident Rates

The following section provides data for the number of calls local fire departments respond to for two casinos in the western United States. Most calls made to the area around the casino are calls for EMS service or traffic accidents. There are a very small number of calls that are labeled as fire and usually involve a false alarm.

Western US Tribal Gaming Facility

The following table provides attendance and service call figures utilized to calculate the incident rates for calls made to the local fire department.

Table 50: Western US Tribal Gaming Facility Fire Call Rate

Casino Attendance 2019	2,043,000
Average Casino Service Calls	97
Incident Rate for Service Calls	0.005%

Source: Proprietary and confidential data, The Innovation Group

Pacific Northwestern US Tribal Gaming Facility

This pacific northwestern US tribal gaming facility received nearly 2.5 million visits in 2019. The following table provides attendance and average annual service call figures utilized to calculate the incident rates for calls made to local fire department and EMS services.

Table 51: Pacific Northwestern Tribal Gaming Facility Service Fire Call Rate

Casino Attendance 2019	2,485,159
Average Casino Service Calls	167
Incident Rate for Service Calls	0.007%

Source: Proprietary and confidential data, The Innovation Group

Average Fire Incident Rates

Incident rates from the data above was utilized in order to establish an incident rate that is applicable to the proposed Eklutna facility to project the number of calls the local fire departments can expect.

Table 52: Average Fire Department Call Rate Estimate

	Estimated Fire Incident Rate
Western US Tribal Gaming Facility	0.005%
Pacific Northwestern US Tribal Gaming Facility	0.007%
Average	0.006%

Source: The Innovation Group

As shown in the following table, it is estimated that the proposed casinos will result in an increase of 46 service calls in Anchorage. This is an increase of approximately 0.1% over the projected 2027 call volume.

Table 53: Eklutna Casino Fire/EMS Service Call Estimates

2027 Projected Casino Attendance	766,900
Average service call rate	0.006%
Projected Fire Service/EMS Calls	46
2027 Projected Fire/EMS Service Calls	47,245
2027 Projected Fire/EMS Service Calls with Eklutna Casino	47,291
% Change	0.1%

The following provides a breakdown of calls to the Anchorage Fire Department.

Table 54: Anchorage Fire Department Calls 2021-2023 (Includes EMS)

Type	2021	2022	2023
Total	37,652	41,000	42,504
EMS Incidents by Type	26,557	28,326	29,798
Medical	25,304	26,907	28,271
Vehicle Accidents	1,210	1,331	1,427
Rescue (All Types)	43	88	100
Non-Emergent Incidents by Type	9,889	10,928	11,173
Good Intent	4,867	4,771	5,263
Service Calls	2,374	3,171	3,079
False Alarms	2,648	2,986	2,831
Fire Incidents by Type	1,206	1,746	1,533
Structure Fires	279	383	400
Vehicle Fires	122	196	162
Wildland/Outdoor	98	218	115
Trash/Unauthorized	707	949	856

Total Population	288,330	288,015	287,700
<i>Incidents per 100 residents</i>	<i>13.1</i>	<i>14.2</i>	<i>14.8</i>

Conclusion and Implications

Fiscal Impacts and Municipal Services

Impacts arising from population and development growth would be effectively diluted by the size of Anchorage’s existing labor force, housing stock, and school capacity. Impacts arising from increased visitation—such as police, fire and EMS calls—are less than significant but detailed below. Moreover, the gains in tax revenues that will accrue to the local governments due to increased economic activity generated by the casino and its employees will mitigate increases in municipal services expenses. In sum, the casino will have a significant positive impact on Anchorage’s governmental services.

Estimated Municipal Expenses

The following table shows the estimated expenses attributable to the subject development from police, courts and public attorneys (both defending and prosecuting), corrections, and fire/EMS services, based on the Comparative Analysis section above and details below, with percent change estimates from that analysis applied to future budget estimates.

Table 55: Anchorage Municipality Police, Courts, and Fire/EMS Expense Increase – 2027

	Annual Budget		
Police	\$154,256,582	<i>Incremental %</i>	<i>0.1%</i>
		<i>Incremental \$</i>	<i>\$79,457</i>
Courts & Public Attorneys	\$11,543,410	<i>Incremental %</i>	<i>0.5%</i>
		<i>Incremental \$</i>	<i>\$46,628</i>
Corrections*	\$56,350,800	<i>Incremental %</i>	<i>0.5%</i>
		<i>Incremental \$</i>	<i>\$284,718</i>
Fire/EMS	\$125,520,339	<i>Incremental %</i>	<i>0.1%</i>
		<i>Incremental \$</i>	<i>\$122,646</i>
Total Increase in Municipal Services			\$533,449

Sources: City of Anchorage, The Innovation Group

*Budget for correctional facilities within Anchorage Municipality from the Statewide Alaska Corrections Budget

To estimate court and attorney costs for the Anchorage Municipality, we calculated the maximum costs to the Anchorage judicial system under the highly conservative assumption that all 46 arrests estimated at the proposed Eklutna casino would not have pleaded out. The 46 service calls were calculated by multiplying the total visits to the property (approximately 767,000) by the comparable sets average service call rate (0.006%). Nearly 10,000 trial superior court cases were filed in the Anchorage Municipality in 2023; 46 cases would represent just 0.5%. The Municipality would likely be able to absorb that de minimis increase without incurring additional costs.

However, to err on the side of caution, the attributable cost is estimated to be approximately over \$46,600.

Table 56: Anchorage Municipality Judicial Costs

	2023 Budget
Equal Rights Commission	\$836,568
Equity and Justice	\$437,953
Municipal Attorney	\$8,599,366
Total	\$9,873,887
<i>Annual Trial Superior Court Case Filings in Anchorage 2023</i>	9,741
<i>Annual Cases at Eklutna Casino</i>	46
<i>% at Eklutna Casino</i>	0.5%
<i>Cost</i>	\$46,628

Sources: City of Anchorage, The Innovation Group

While the Anchorage Municipality is home to the Anchorage Correctional Complex, the Alaska Department of Corrections is responsible for managing the state’s correctional facilities. Within the state budget for the fiscal year of 2023, a total of about \$56.4 million was allocated to the two correctional facilities in the Anchorage Municipality: Anchorage Correctional Complex and Hiland Mountain Correctional Center. With a total of approximately 9,000 bookings per year, the Anchorage Municipality spends an average of approximately \$6,000 per booking. The 46 projected bookings from the Eklutna casino only represent a 0.5% increase in bookings, so it is likely that the Alaska Department of Corrections could absorb this impact with little if any additional spending as a significant portion of their budget goes to fixed costs that would not increase due to additional bookings. However, to be conservative, the attributable cost is estimated to be roughly \$285,000.

Table 57: Anchorage Correctional Costs

FY 2023 Anchorage Municipality Corrections Budget	\$56,350,800
FY2023 Anchorage Municipality Bookings	9,107
Annual Projected Bookings from Eklutna Casino	46
Cost	\$284,718
<i>% Change</i>	0.5%

Source: Alaska Department of Corrections, The Innovation Group

Problem Gambling

Since Alaska does not currently offer any state-funded problem gambling services, we do expect a fiscal impact from the creation of prevention and treatment programs. The size and scope of these services will depend on the prevalence of problem gambling in the state. Since gambling is already in Alaska via Charitable Gaming, it is reasonable to assume a problem gambling population

currently exists. We look to other markets and research studies to estimate the current number of problem gamblers in Alaska.

One of the most frequently cited studies on prevalence rates is *Estimating the Prevalence of Disordered Gambling Behavior in the United States and Canada: A Meta-analysis by the Harvard Medical School Division on Addictions*. The Harvard Medical School study analyzed 152 distinct previous prevalence studies and determined that 2.9% of the adult population could be considered problem or pathological gamblers. The *2021 Survey of Problem Gambling Services in the United States*¹¹ conducted by the Association of Problem Gambling Service Administrators and the National Council on Problem Gambling estimated 2.2% of Alaska adults have a gambling problem. Combining these two studies, we estimate there are currently around 14,000 problem gamblers in Alaska, of which about 5,500 reside in Anchorage. Studies on problem gambling comorbidities show high rates of alcohol use disorder among problem gamblers¹². Given Alaska's prevalence of binge drinking¹³ is near the upper end of the national distribution, we would forecast the likely population of Alaska problem gamblers, after casinos are introduced, to be higher than the national average. Therefore, when assessing mitigation costs, we analyze a range from 2.2% (current estimates) to 5%, which we believe will be comfortably higher than the actual prevalence of problem gambling.

To estimate mitigation costs for Alaska, we return to the *2021 Survey of Problem Gambling Services in the United States*, which includes data on state-funded problem gambling programs. We used three approaches from this report to triangulate the costs of a mitigation program for the state. First, we consider the distribution of per capita problem gambling services spend across the states. Second, we look at the distribution of total problem gambling services spend by state. Third, the study regresses problem gambling services spend by state on estimated number of problem gamblers by state. We use Alaska's estimated number of problem gamblers to predict spend. These three approaches, described in more detail below, yield a range of about \$1 million to \$1.25 million in problem gambling services spend.

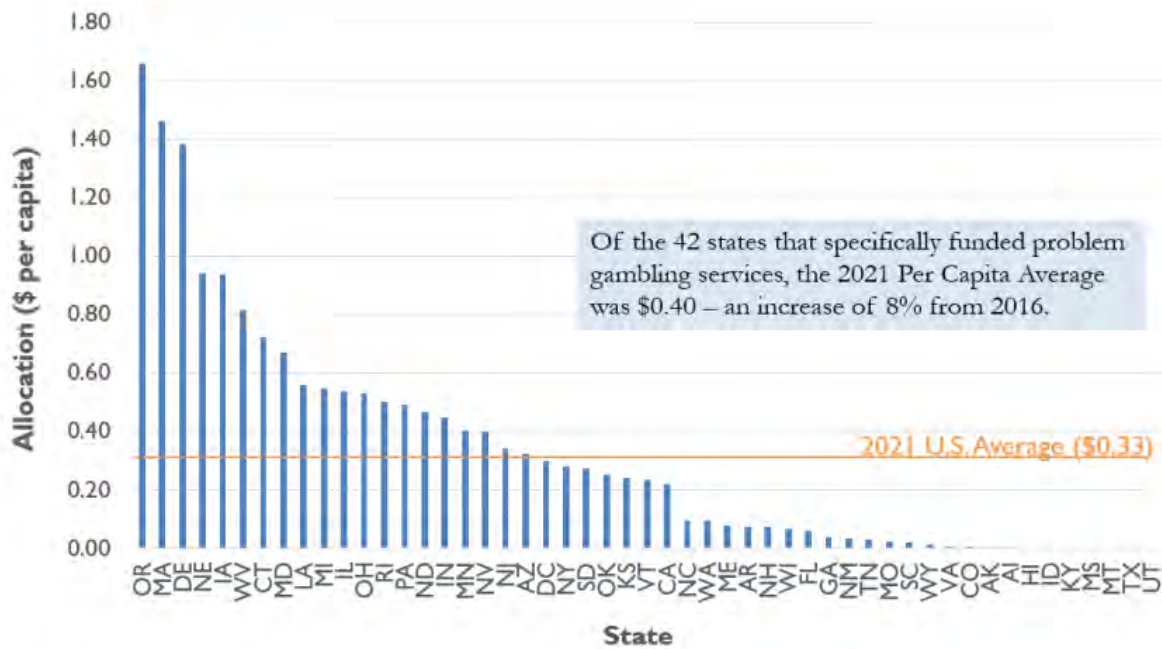
The bar chart below shows per capita funding for problem gambling services. Among the states with state-funded problem gambling programs, average per capita funding is \$0.40, an increase of 8% from 2016. Oregon spends the most per capita at over \$1.60. Assuming Alaska matched Oregon's per capita spend, Alaska would spend approximately \$1.17 million annually on problem gambling prevention and mitigation.

¹¹ https://naadgs.org/wp-content/uploads/2022/06/NAADGS_2021_Survey_of_Publicly_Funded_Problem_Gambling_Services_in_the_United_States_v2.pdf

¹² <https://pubmed.ncbi.nlm.nih.gov/15889941/>

¹³ Alaska has the 10th highest binge drinking prevalence in the country according to data obtained from the CDC <https://www.cdc.gov/alcohol/data-stats.htm>

Figure 2: 2021 Per Capita Allocation for Problem Gambling Services by U.S. State



Note: Includes only funds line itemed for problem gambling services and passing through a state agency, including the District of Columbia. Per Capita Average is based on the 2020 Census state population, including minors. Some states had problem gambling service allocations at the level depicted on this graph, however, had expended fewer than 10% of the funds from that allocation in 2021 (AR, DC, WY) while others expended approximately 70% of the allocation in 2021 (NJ & MI)

Source: 2021 Survey of Problem Gambling Services in the United States

Next, we analyzed total spend on problem gambling services by state. That chart below lists total spend by state (note: nine states do not have any funding for problem gambling services, three more than in 2016). If Alaska matched the median level of funding, it would spend \$1.25 million on problem gambling services.

Table 58: Total Spend on Problem Gambling Services by State (Fiscal Year 2021)

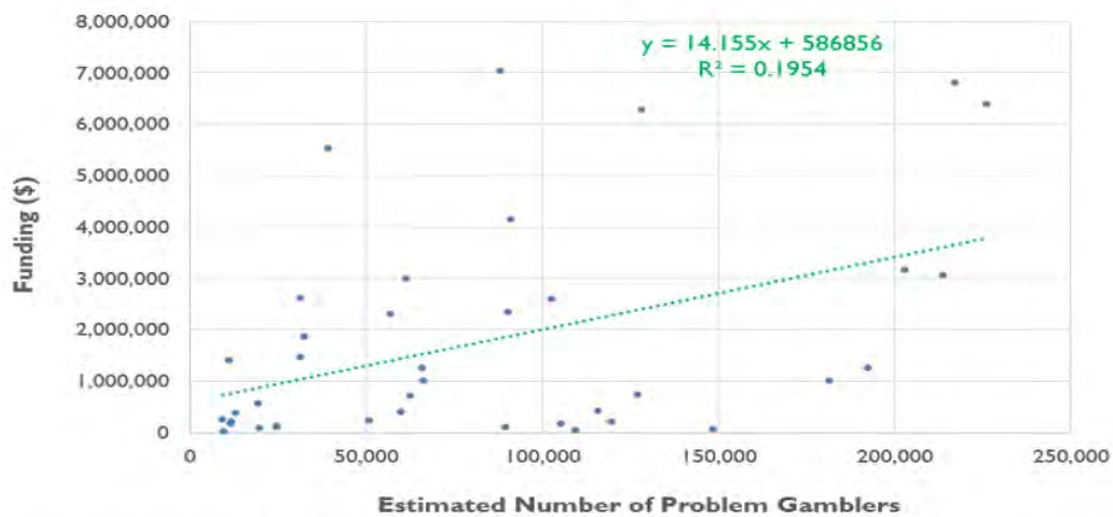
State	Total Spend on Problem Gambling Services
Arizona	\$2,344,300
Arkansas	\$220,000
California	\$8,681,501
Colorado	\$30,000
Connecticut	\$2,609,799
Delaware	\$1,389,842
Florida	\$1,250,000
Georgia	\$400,000
Illinois	\$6,800,000
Indiana	\$3,047,034
Iowa	\$2,992,114
Kansas	\$712,316
Louisiana	\$2,583,873
Maine	\$106,000
Maryland	\$4,132,375
Massachusetts	\$10,200,000
Michigan	\$5,515,300
Minnesota	\$2,301,271
Missouri	\$153,606
Nebraska	\$1,850,000
Nevada	\$1,252,693
New Hampshire	\$100,000
New Jersey	\$3,150,000
New Mexico	\$70,000
New York	\$5,557,398
North Carolina	\$1,000,000
North Dakota	\$360,000
Ohio	\$6,265,840
Oklahoma	\$1,000,000
Oregon	\$7,034,955
Pennsylvania	\$6,369,000
Rhode Island	\$549,617
South Carolina	\$100,000
South Dakota	\$244,000
Tennessee	\$200,000
Vermont	\$149,800
Virginia	\$55,000
Washington	\$730,000
West Virginia	\$1,453,840
Wisconsin	\$396,000
Wyoming	\$7,188
Median	\$1,250,000

Source: 2021 Survey of Problem Gambling Services in the United States

Finally, we use a regression of problem gambling services funding on a state’s estimated number of problem gamblers to predict Alaska’s problem gambling services spend. The best fit line shown

below has an intercept of about 600,000, suggesting baseline spend regardless of problem gamer population size is \$600,000. The slope of the line is approximately 14, indicating that for every one problem gambler, a state’s problem gambling budget increases by \$14 above the baseline \$600,000. At what we believe is a substantial overestimate of 5% of Alaskan adult population being problem gamblers (solely for the purposes of developing a conservative budget estimate), the regression would indicate a state spend of just under \$1 million on problem gambling mitigation.

Figure 3: Relationship between a State’s Estimated Number of Adult Problem Gamblers and Problem Gambling Service Funding



▶ Linear least squares regression of Funding vs Estimated Number of Problem Gamblers, p-value <.01. (3 outliers removed. N=39)

Source: 2021 Survey of Problem Gambling Services in the United States

According to the most recent data available, Alaska does not currently have any state funded problem gambling services. Therefore, we used the three approaches described above to calculate three estimates of the funds needed to support a problem gambling mitigation program in Alaska. This resulted in estimates of approximately \$1.17 million, \$1.25 million, and \$1 million, indicating that the state of Alaska would need to spend between roughly \$1 million and about \$1.25 million on problem gambling services.

More information on problem gambling research is contained in Appendix A.

COMPETITIVE EFFECTS ANALYSIS

Based on a previously conducted analysis completed by The Innovation Group, we identified one possible category of substitution effects would arise from development of the Eklutna project:

1. The casino's impact on charitable gaming in the region.

Impacts on Charitable Gaming

Alaska Charitable Gaming

Currently charitable gaming is the only legal form of gambling in Alaska. Despite several attempts to legalize casinos and lotteries, Alaska's gaming laws remain among the strictest in the country.

Dating back more than 30 years there has been attempts to legalize different forms of gaming and lotteries across Alaska. In the early days, as early as 1990, a referendum was voted down to legalize gaming and establish an Alaska Gambling Board by nearly a 2 to 1 vote. As time progressed into the late 1990's and early 2000's several other pieces of legislature were either passed to limit gaming in the state such as a 1995 ban on cruise ships operating casino gaming in Alaska waters or rejected to stop the introduction of gambling in the state such as the 2004 rejections of a proposal to establish a gaming commission or the 2005 and 2006 rejections of attempts to place an Alaska Video Lottery Law on the ballots.

A more recent examination of the gaming landscape in Alaska includes charitable gaming establishments that offer pull tabs (sing play card with breakopen tabs where a winning "card" displays a certain symbol or number), bingo, and raffles but there has been some recent push to expand gaming within the state but to no avail.

However, in the last couple years there has been measure looking to allow other forms of gambling such as electronic pull tabs and the addition of some type of gambling aboard Alaska Marine Highway system ships. House Bill 200 and Senate Bill 146 from 2024 both address the introduction of electronic pull tabs to the state while House Bill 197 address gambling on the state ferries.

Authorized in "AS 05.15 The Alaska Gaming Reform Act (Act) and 15 AAC 160 Authorized Games of Skill and Chance," charitable gaming gives "permittees" the opportunity to "generate net proceeds for political, educational, civic, public, charitable, patriotic, and religious uses in the state."

Permitted games include: "Bingo, raffles and lotteries, pull-tab games, authorized classics (for example, ice classics, cabbage classics), fish and moose derbies, Calcutta pools, and contests of skill." Pull-tabs account for approximately two-thirds of proceeds to charities, followed by raffle/lottery at 22% and bingo at 10%.

There are four categories of operations:

1. Permittee (Self-directed): A municipality or qualified organization that holds a permit to conduct gaming activities.
2. Multiple-Beneficiary Permittee (MBP): Two to six municipalities or qualified organizations that jointly obtain a permit in order to aggregate prize limits.
3. Vendor: A for-profit business that sells pull-tabs on behalf of a permittee, from a location licensed by the Alcoholic Beverage Control Board.
4. Operator: A for-profit business licensed to conduct gaming activities on behalf of a permittee.

Qualified organizations include “civic or service organizations, religious, charitable, fraternal, veterans, labor, political and education organizations, police and fire departments, associations and derbies, and nonprofit trade associations.”

Adjusted Gross Income (AGI) is the amount retained by the operation after prizes and federal excises taxes are deducted from Gross Sales. Net Proceeds is the amount retained by permittees after expenses and state taxes are deducted from AGI.

Please note The Innovation Group included 2019 as reference data as the most recent available data from the Alaska Department of Revenue – Tax Division and the Annual Charitable Gaming Report is from 2020 and has the corresponding COVID impact. 2019 is a more realistic representation of charitable gaming data under normal market conditions.

Table 59: Alaska Charitable Gaming 2019

	# of Licenses	Gross Sales	Federal Excise Tax	Cost of Prizes	Adjusted Gross Income (AGI)	Total Expenses	Net Proceeds
Permittees	1,101	\$149,711,684	\$165,580	\$109,967,009	\$39,579,096	\$18,893,603	\$20,685,492
MBPs	13	\$77,269,375	\$11,405	\$60,625,212	\$16,632,757	\$11,834,354	\$4,798,404
Vendors	165	\$40,732,412	\$17,115	\$32,439,528	\$8,275,769	\$4,055,567	\$4,220,202
Operators	24	\$117,512,615	\$174,116	\$89,321,211	\$28,017,287	\$21,100,511	\$6,916,776
Total	1,303	\$385,226,086	\$368,216	\$292,352,960	\$92,504,909	\$55,884,035	\$36,620,874

Source: Alaska Department of Revenue

The percentage of AGI allowed for expenses varies by and game type. Expenses may not exceed 70% of pull-tab AGI and 90% of bingo and raffle AGI. The expense ratio results also vary by category of operation, with Operators having the highest expense ratios. All (100%) Net Proceeds must be remitted to a permittee. Vendors only operate pull-tabs.

Table 60: Pull-Tab Proceeds 2019

	Gross Sales	Federal Excise Tax	Cost of Prizes	Adjusted Gross Income (AGI)	Total Expense %	Net Proceeds
MBP	\$52,292,715	\$11,405	\$42,488,298	\$9,793,012	65%	\$3,449,249
Operator	\$92,927,334	\$173,003	\$73,263,557	\$19,490,774	70%	\$5,876,503
Self-directed	\$116,863,920	\$156,084	\$92,523,851	\$24,183,986	56%	\$10,539,978
Vendor	\$40,732,412	\$17,115	\$32,439,528	\$8,275,769	49%	\$4,220,202
Total	\$302,816,381	\$357,607	\$240,715,234	\$61,743,541	61%	\$24,085,932

Source: Alaska Department of Revenue

Bingo accounts for 17% of sales but because of higher operating expenses only 10% of proceeds.

Table 61: Bingo Proceeds 2019

	Gross Sales	Federal Excise Tax	Cost of Prizes	Adjusted Gross Income (AGI)	Total Expense %	Net Proceeds
MBP	\$24,926,512	\$0	\$18,101,054	\$6,825,459	80%	\$1,345,927
Operator	\$22,352,232	\$0	\$14,721,032	\$7,631,200	88%	\$900,586
Self-directed	\$17,148,443	\$2,892	\$12,171,400	\$4,974,151	69%	\$1,527,109
Total	\$64,427,187	\$2,892	\$44,993,486	\$19,430,810	81%	\$3,773,622

Source: Alaska Department of Revenue

Raffles are dominated by Self-directed Permittees and have little overhead, meaning that nearly 85% of AGI is retained as Net Proceeds.

Table 62: Raffle Proceeds 2019

	Gross Sales	Federal Excise Tax	Cost of Prizes	Adjusted Gross Income (AGI)	Total Expense %	Net Proceeds
MBP	\$50,148	\$0	\$35,861	\$14,287	77%	\$3,228
Operator	\$1,050,244	\$1,113	\$539,087	\$510,044	81%	\$97,669
Self-directed	\$13,703,658	\$6,604	\$4,369,745	\$9,327,308	12%	\$8,214,956
Total	\$14,804,050	\$7,717	\$4,944,693	\$9,851,639	16%	\$8,315,853

Source: Alaska Department of Revenue

Annual win (or AGI) per adult is nearly \$170, which is significantly lower than casinos typically penetrate but very high compared to charitable gaming in other states, where other gambling options are more accessible.

Alaska Casino Gaming

Alaska does not offer any Class III gaming; however, there are two approved Class II Indian Gaming facilities that operate under provisions approved by the National Indian Gaming Commission (NIGC). They are Klawock Bingo in Klawock and M.I.C. Gaming Hall in Metlakatla.

They are located in the southeastern most portions of Alaska approximately 19 hours south of Juneau that includes both driving and a ferry ride. In short, these facilities are located multiple days travel away from the proposed Eklutna facility. Based on the analysis completed by The Innovation Group, we do not believe there will be any impact on these two existing facilities.

Charitable Gaming Impact

Charitable gaming, in the forms of pull-tabs, raffle/lottery, and bingo make up nearly 99% of all charitable gaming offered in Alaska. In 2019, the most recent available report not impacted by COVID, the charitable gaming adjusted gross income is estimated at over \$92 million.

In 2020, as reported by the Alaska Department of Revenue, charitable gaming revenue decreased to under \$68 million, a decrease of about 30%. However, as with other gaming jurisdictions around the county, we believe that charitable gaming revenue has rebounded and even surpassed that of pre pandemic levels.

The gravity model was initially calibrated using charitable gaming data from the Department of Revenue. As discussed previously in this section, Adjusted Gross Income (AGI, which is equivalent to gaming revenue in the gaming industry) in 2019 was \$92.5 million, a majority of which we estimate originated from the defined gravity model market area. The Innovation Group then estimated the amount of charitable gaming in each market area adjusted to account for population difference and average annual household income.

Based on Net Proceeds data by physical location (that is, where the spending on charitable gaming occurred as opposed to which Permittee the Net Proceed was distributed), we estimate that 64% of Net Proceeds was derived by spending at facilities in the gravity model area. However, the MBPs and Operators, which predominantly operate in the Anchorage-Wasilla/Mat-Su Valley region and Fairbanks region, have higher expense margins, which means that the AGI generated in the gravity model area is substantially higher than 64%.

As previously noted, The Innovation Group used a gravity model to assist in realizing the impact of the proposed Class II Eklutna casino on the existing pull tab market. Pull tabs are offered at a wide range of locations across the state, including dozens in the greater Anchorage area. As a result, we utilized several of the more prominent locations in the area as a proxy within the model to represent the greater Anchorage pull tab market. We first calibrated the model to map the estimated current Anchorage pull tab market, then added the proposed Eklutna facility to the model to assess the impact it would have on that market.

The locations of each permit as the holder of the permit and location of the pull tabs are somewhat undefined as described by the Alaska Department of Revenue; however, we estimated the greater anchorage revenue in relation to the rest of the state by analyzing population distribution then applying a premium given the tourist aspect of Anchorage relative to other parts of the state. Based on our estimation we believe between 70% and 80% of the pull tab revenue takes place in the greater Anchorage area. With that being said, due to the unclear nature of the exact pull tab locations and an understanding of their exact market dynamics, The Innovation Group assessed the impact on a state level with an understanding that a vast majority of the impact would take place in the greater Anchorage area.

However, we would like to note that the impact on organizations that host charitable gaming on one off or semi-frequent basis, such as fire stations, elderly homes, or fraternal organizations would be minimally impacted but the standalone pull tab facilities would shoulder much of the impact.

The following table quantifies the impacts on charitable gaming from the proposed Eklutna casino development.

Table 63: Impact on Charitable Gaming from Casino

	2019	2027	Impact
Adjusted Gross Income (MM)	\$92.5	\$112.7	-\$24.3
Growth/Impact	-	21.8%	-21.6%

Source: AK Dept of Revenue; The Innovation Group

APPENDIX A: PROBLEM GAMBLING MITIGATION

Definition and Prevalence

A majority of Americans, about 86%, report having gambled at least once in their lifetime¹⁴. Most people gamble for recreational purposes without the behavior becoming a problem. Studies, however, estimate that 0.4%-1.6% of the United States population can be classified as pathological gamblers.^{15,16} Pathological gambling has been commonly associated with relationship problems, employment issues, and significant financial difficulties.

The American Psychiatric Association (2004) defines a pathological gambler as a person who features a continuous loss of control over gambling. Furthermore, this gambler illustrates a progression, in gambling frequency and amounts wagered, in the preoccupation with gambling and in obtaining monies with which to gamble. However, problem gambling is a more loosely defined term and is commonly associated with gaming-related difficulties that are considered less serious than those of a pathological gambler. For the sake of this report, we will utilize the definition by noted researchers Cox, Rosenthal and Volberg which defines problem gambling as a pattern of gambling behavior that compromise, disrupt or damage personal, family or vocational pursuits.¹⁷

The National Research Council¹⁸ utilizes a three-level metric. Level 1 gambling is considered social and or recreational gambling with no appreciable harmful effects. Level 2 gambling is synonymous with problem gambling. Level 3 gambling is synonymous with pathological gambling. Problem gambling is an urge to gamble despite harmful negative consequences or a desire to stop. It is often defined by whether harm is experienced by the gambler or others, such as the gamblers family, significant other, spouse, friends, or coworkers. A problem gambler may or may not be a pathological gambler. Pathological or compulsive gambling is defined as a mental disorder characterized by a continuous or periodic loss of control over gambling, a preoccupation with gambling and with obtaining money with which to gamble, irrational thinking, and a continuation of the behavior despite adverse consequences.

Prevalence rates to determine adult problem gambling rates are measured by administering a survey (often a variation of the South Oaks Gambling Screen or a modified DSM-IV questionnaire) to a statistically valid sample of the adult population of the jurisdiction being

¹⁴ James KC, Bible WA, Dobson JC, Lanni JT, Leone RC, Loescher RW, et al. *National gambling impact study commission final report*. National Gambling Impact Study Commission. 1999.

¹⁵ Shaffer HJ, Hall MN, Vander Bilt J. "Estimating the prevalence of disordered gambling behavior in America and Canada: a research synthesis." *Am J Public Health*. 1999

¹⁶ Petry NM, Stinson FS, Grant BF. "Comorbidity of DSM-IV pathological gambling and other psychiatric disorders: results from the national epidemiologic survey on alcohol and related conditions." *J Clin Psychiatry*. 2005

¹⁷ Cox, S., H. R. Lesieur, R. J. Rosenthal & R. A. Volberg. 1997. *Problem and Pathological Gambling in America: The National Picture*. Columbia, MD: National Council on Problem Gambling.

¹⁸ National Research Council, pp. 20-21.

measured. Adolescent rates are measured in a similar manner. Such a method and analysis of data that accompanies the process is referred to as a general population prevalence study.

Jurisdictions, both domestically and internationally, have conducted studies to estimate the percentage of the population that could be classified as having some level of problem gambling behavior. These studies, commonly referred to as prevalence studies, are designed to reflect the scope and severity of problem gambling behavior.¹⁹

One of the most frequently cited studies on prevalence rates is *Estimating the Prevalence of Disordered Gambling Behavior in the United States and Canada: A Meta-analysis by the Harvard Medical School Division on Addictions*. The meta-analysis method of estimating prevalence rates has been used in related addiction fields of drug prevention and patterns of alcohol use and alcohol treatment. It is considered a more cost-effective method than a national study since it makes use of existing research already conducted in a field.

The Harvard Medical School study, believed to be the first to use meta-analysis measurements for problem gambling prevalence rates, analyzed 152 distinct previous prevalence studies available for review by June 15, 1997. The study determined that 2.0 percent of the adult population could be considered as Level 2 of disordered gambling (often referred to as problem gambling) and 0.9 percent of Level 3 or disordered gambling (also referred to as pathological gambling) during the past year. The vast majority of adults in the general population, then, do not experience gambling-related problems of any clinical significance.

The meta-analysis raw data was given to the Committee on the Social and Economic Impact of Pathological Gambling of the National Research Council (NRC) in its analysis for the National Gaming Impact Study Commission. After an extensive review, the NRC agreed with the above rates of problem gambling and used the numbers in its own analysis of problem gambling in its final report.

The introduction of casino gambling has the potential of negative social impacts. These potential impacts can be controlled and minimized through proper planning, awareness campaigns, and prevention and treatment programs applied in a coordinated manner by all relevant stakeholders. By utilizing some of the many proven prevention and treatment programs, the potential social impact of the advent of gaming can be minimized. Allocating funds to problem gambling services can help mitigate problem gambling and promote responsible gambling.

As an example, by devoting more resources to prevention and treatment, Connecticut was able to cut prevalence rates despite further gaming development. In 1996, Connecticut had only a single clinic, but by the time of an updated study in 2008, the state had 17 clinics.²⁰ Prevalence rates

¹⁹ *Estimating the Prevalence of Disordered Gambling Behavior in the United States and Canada: A Meta-analysis*, Harvard Medical School Division on Addictions, 1997.

²⁰ Spectrum Gaming Group, *Gambling in Connecticut: Analyzing the Economic and Social Impacts*, prepared for the State of Connecticut, Division of Special Revenue, June 2009.

declined substantially during that period, despite the opening of Mohegan Sun late in 1996 and further expansion at Foxwoods, including the opening of Grand Pequot Tower hotel in 1997.

Table 64: Connecticut Prevalence Rates

	2008 Survey	1997 Study
Problem Gamblers	0.90%	2.20%
Probable Pathological Gamblers	0.70%	0.60%
Total Disordered Gamblers	1.60%	2.80%

Source: Spectrum Gaming Group.

Responsible Gaming and Harm Minimization

Responsible gambling/gaming programs take several forms in an effort to combat and prevent gambling-related harms. Instances of problem gambling manifest in two categories of harm: (1) personal harm, including effects on health, well-being, and relationships, and/or (2) economic harm. Research on responsible gaming falls short of the levels of scientific analysis necessary to develop responsible gaming “best practices.” While various publications have attempted to synthesize existing research on common responsible gaming and harm minimization practices, the field of research often lacks peer-reviewed scientific analyses.

In their current form, the most common responsible gaming practices reflected in the field of research are self-exclusion programs, gambling help lines, tracking behavioral characteristics, setting gambling limits, providing responsible gaming-oriented game features, and employee training. Each of these strategies will be discussed below.

As a condition of licensing, commercial casino states may mandate that casinos prepare and submit for approval a wide-ranging plan for addressing responsible gaming issues. Required elements of the plan often include employee training and public awareness efforts along with other policies that various states have addressed specifically through standalone statutes, or regulations, that address only a single subject. The required elements of these plans vary by state.

In Maryland, for example, a responsible gambling program must consist of mechanisms that both mitigate the effects of problem gambling in the State and maximize the access of individuals with a gambling problem to problem gambling resources.²¹

Massachusetts makes the issuance of gaming licenses contingent upon the submission of a plan to “address lottery mitigation, compulsive gambling problems, workforce development and community development [,] and host and surrounding community impact and mitigation issues.”²² The State intends for these requirements to advance its objective of providing a gaming

²¹ Maryland responsible gaming plan statute. COMAR 36.01.03.07(B).

²² Massachusetts responsible gaming statute. M.G.L. Ch. 23K, § 15(6).

environment that is safe and productive for all stakeholders. In furtherance of this objective, Massachusetts prompts gaming licensees to develop plans that train employees to identify patrons exhibiting problems with gambling, and prevention programs for vulnerable populations.²³

Other states, such as Ohio, connect their responsible gaming plans to other mitigation mechanisms, such as voluntary exclusion programs, to better protect vulnerable groups.²⁴ Overall, the development of responsible gaming plans serves to establish concrete frameworks to better promote safe gaming.

Self-Exclusion Programs

Voluntary self-exclusion programs, typically operated by casinos and online gambling sites or gaming regulators, give individuals the ability to exclude themselves from gambling activities. Many states require that patrons have the ability to authorize a casino to refuse their right to gamble and to expel them if they are found gambling or, in some cases, otherwise found on the premises. Program management models vary; in some cases, they are run by the state or a state-appointed group, in others they are managed directly by licensees. State statutes vary in the length of the self-exclusion periods available – typically ranging from a six month ban to lifetime restriction – and in the procedures for reversing self-exclusion. In some states, third parties also have the ability to voluntarily exclude patrons exhibiting problem gambling behavior. Many state laws specify that, in addition to banning play, the casino must also eliminate direct promotional outreach to these individuals as well as exclude them from complimentary offerings (“comps”) or access to credit. Such programs illustrate efforts to mitigate the potential social harms of expanded gaming in a state, including mental health issues, relationship concerns, and financial and work problems resulting from problem gambling.²⁵ As one of the most investigated responsible gaming strategies, self-exclusion programs benefit from a robust body of research conducted around the world.

Generally, the research on the effectiveness of self-exclusion programs concludes that this method is a safe and, for some gamblers, effective form of intervention against problem gambling. As one study suggests, self-exclusion may have similar outcomes to counseling and may reduce harm in the short-term. Additional research has indicated that self-excluded persons also engage in treatment, self-help groups, or other forms of support experience more positive outcome than those who do not. This research suggests that self-exclusion programs that serve as a gateway to treatment are most successful for individuals harmed by problem gambling. Research has also indicated that problem gamblers appear to be more receptive to self-exclusion mitigation strategies when compared to self-led efforts to seek professional help.²⁶ Ultimately, self-exclusion has

²³ M.G.L., Ch. 23K, § 18(6)

²⁴ See e.g., Ohio Regulation 3772-12-06.

²⁵ Nerilee Hing, Barry Tolchard, Elaine Nuske & Louise Holdsworth, *A Process Evaluation of a Self-Exclusion Program: A Qualitative Investigation from the Perspective of Excluders and Non-Excluders*, 12 INTERNATIONAL JOURNAL OF MENTAL HEALTH AND ADDICTION 509, 510 (2014), [10.1007/s11469-014-9482-5](https://doi.org/10.1007/s11469-014-9482-5).

²⁶ Hing, *supra* note 5, at 510.

transitioned from a “punitive” enforcement model to one that aims to provide individual assistance in order to connect vulnerable persons with counseling and other support services.

The framework for self-exclusion programs varies from state to state, but many states mandate that patrons have the ability to refuse their right to gamble and to expel them from the premises.²⁷ In Kansas, for example, the voluntary exclusion statutes require that each self-exclusion applicant “refrain from visiting gaming facilities, pari-mutuel licensee locations, and fair association race meets.”²⁸ Kansas’ statutes also enable the gaming commission to “prohibit the applicant from entering the premises of all gaming facilities.”

Similarly, Massachusetts enables a person to be placed on a self-exclusion list by “acknowledging that the person is a problem gambler and by agreeing that, during any period of voluntary exclusion, the person shall not collect any winnings or recover any losses.”²⁹ Massachusetts also prohibits gaming establishments from marketing “to persons on any excluded persons list,” and requires gaming establishments to deny access to complimentary credits. Ultimately, Massachusetts identifies voluntary self-exclusion as “one means to help address problem gambling behavior or deter an individual with family, religious, or other personal concerns from entering . . . a gaming establishment.”³⁰

Various challenges interfere with the effectiveness of self-exclusion. First, the number of gambling facilities within a jurisdiction may make the enforcement of self-exclusion impractical; if alternative facilities can be easily accessed, the effectiveness of self-exclusion may be compromised. Notably, statutorily required training may not sufficiently prepare officials responsible for self-exclusion enforcement.³¹ The diversity of socioeconomic and psychological conditions among voluntary self-excluders may require responsive enforcement mechanisms. Furthermore, the need to apply for placement on a self-exclusion list within a gaming facility may compromise the integrity of the process, thereby deterring potential self-excluders from participating.

Individual compliance poses another well-documented challenge to the effectiveness of self-exclusion programs. For example, one study determined that more than half of the participants for whom self-exclusion was still in effect had returned to a casino or breached their contracts by the six-month follow-up interview. Additionally, a study of self-excluded individuals in Missouri found similar breaches, indicating that the benefits of the program were attributable more to the act of enrollment than to enforcement. This research has led to the frequent conclusion that responsibility for self-exclusion lies with both the gaming industry and the self-excluding individual.

²⁷ Regulatory Management Counselors, *Comparative Governance and Regulatory Structure of Gaming Regulations Related to Expanded Legalized Gaming Activities in the Commonwealth of Virginia* (Aug. 5, 2019), at 160 (hereinafter *Comparative Governance Report*).

²⁸ *Id.* at 161.

²⁹ *Id.* at 169.

³⁰ *Id.* at 171.

³¹ Hing, *supra* note 5, at 511.

In conclusion, voluntary self-exclusion programs may reduce the urge to gamble and increase the perception of control over personal behavior.³² While self-exclusion alone cannot substitute for dedicated treatment, it provides an external control mechanism that may limit problem gambling and encourage voluntary excluders to seek professional help.

Tracking Behavioral Characteristics

In an effort to predict the likelihood that a patron will experience harm from gambling and to introduce preventative interventions before the onset of such problems, gaming jurisdictions have implemented systems to track player behavioral characteristics. These behavioral tracking systems are based on algorithms of play. Implementation strategies vary with the form of gaming: whereas in online gaming environments tracking procedures benefit from access to all player transaction information, in brick-and-mortar environments, the strategy is often designed around player tracking systems (e.g., Players Clubs) that depend upon an individual patron's participation.

Research on the effectiveness of tracking frameworks has produced informative findings. Based on analysis of player habits, studies have suggested that efforts to promote responsible gaming should be tailored to each type of gambling offered at a gaming location, rather than adhering to a general mitigation program. By studying behaviors and thoughts patrons use to control the amount they gamble, such as attempts to set a budget or to seek help, research has identified characteristics that could be used to develop prevention and early intervention programs for problem gamblers. Research dedicated to tracking the behavioral characteristics of online gamblers has determined that patrons who engaged in more than two types of gambling within their first month of play, with high variability of wagers, were more likely to benefit from responsible gaming programs.

The study of behavioral characteristics remains a highly-variable task. Given the limitations inherent in the use of personalized player data, there remains a lack of definitive evidence of any behavioral algorithm that can accurately predict patterns of gambling disorder.

Setting Gambling Limits

The ability to set gambling limits, a process also known as pre-commitment, allows gamblers to predetermine the amount of time or money they are permitted to devote to gambling activities before play begins. Depending on the gaming venue or website, spending limits can include deposit, play, loss, win, bet, and time limits.

Research on the effectiveness of pre-determined gambling limits has demonstrated mixed outcomes and has illustrated positive and negative results of this mitigation technique. Studies have indicated that requiring individuals to set such limits may reduce overall money spent on gambling, but evidence is still lacking to suggest that this spending reduction occurred in individuals who were experiencing gambling-related harms, or that gambling-related harm was reduced. Furthermore, research has indicated that voluntary money limit setting was more effective

³² Robert Ladouceur, Caroline Sylvain & Patrick Gosselin, *Self-Exclusion Program: A Longitudinal Evaluation Study*, 23 J. GAMBLING STUDIES 85, 85 (2007), [10.1007/s10899-006-9032-6](https://doi.org/10.1007/s10899-006-9032-6).

than time limits in reducing problem gambling behavior. While self-limiting has been found to reduce the variety of games played and the number of bets placed, gambling limits have not been found to reduce the amount wagered per bet. Additionally, research has indicated that pre-commitment may have little effect on decreasing gambling expenditures, especially among those who are intent on continued gambling and who are likely to find methods of circumventing gambling limits.

Finally, the emergence of GameSense, a program that employs in-house responsible gaming information centers or advisors, and other limit-setting programs like PlayMyWay, signal that the future direction of gambling mitigation plans is likely to employ gambling limits. Further research will be required to produce evidence that supports the effectiveness of pre-commitment initiatives.

Responsible Gaming-Oriented Game Features

This harm minimization technique involves the modification to the structure or operation of games to assist patrons in making informed choices about their gambling activity, and to encourage responsible gaming behavior. While research on this mitigation strategy is often focused on the use of warning messages, select studies have explored the use of additional modifications, such as slowing down the rate of play, posting clocks around gambling facilities, and offering “play money” modes.

A threshold study evaluating the effectiveness of five game features (messages, bank meters, clocks, demo mode, and charity donations) found that most participants were aware of at least one feature, but that only a small portion actually utilized the features. Further research concluded that, when compared to warning messages that appear on the periphery of a screen, messages that appear in the middle of a screen are more frequently recalled and considered more useful. Patrons in one study also identified a cash display as helpful to controlling gambling activities.

The research on responsible gaming-oriented game features has provided varying insights on the effectiveness of such features. While evidence confirming the efficacy of responsible game features is mixed, little research has shown that game features reduce gambling-related harm in a real-world setting.

Employee Training

Training of gaming facility employees in responsible gaming is a nearly universal practice. Some states require that this training include instruction on the complex question of how to identify problem gamblers on the gaming floor. Other states provide for in-depth education on the nature and symptoms of problem gambling.³³ With this training, employees of gambling facilities can better serve patrons who may be identified as problem gamblers by providing information about problem gambling programs. Delaware, for example, requires that the rules for state lottery games provide “procedures for the display and presentation of messages concerning responsible gaming and the regulations, procedures and training for identification of and assistance to compulsive gamblers.”³⁴

While few studies exist that explore the effectiveness of employee training programs, research has determined that there is considerable disparity in employee ability to accurately identify problem gambling behavior among patrons. Studies indicate that employee training can improve employee knowledge of responsible gambling, however, there is limited evidence that this enhanced understanding enables employees to more accurately identify patrons with a gambling disorder.

Additional obstacles to the effectiveness of employee training are found in the difficulty, awkwardness, and uncertainty present in the act of confronting a patron. Studies have indicated that gaming facility employees often experience difficulty when approaching patrons due to uncertain estimations of a patron’s potential problems or in an attempt to avoid causing a patron embarrassment.

Ultimately, the spectrum of harm from problem gambling manifests differently from state to state. As a result, the role of employee training may vary with the extent of a state’s understanding of the gambling problems its residents face.

Public Health

By understanding gambling and its potential impacts on public health, policymakers and health practitioners alike can work to minimize gambling’s negative impacts, while promoting its potential benefits. Today, public health perspectives are not limited to the biological and behavioral dimensions of gambling. Rather, a contemporary public health perspective can also target the social and economic determinants of gambling, such as income, employment, and poverty. Four principles have emerged as the basis for a public health framework on gambling: (1) scientific research is the foundation of public health knowledge, (2) public health knowledge is derived from population-based observations, (3) health initiatives are proactive (i.e., health promotion and prevention are primary, while treatment is secondary), and (4) public health is balanced and considers both the costs and benefits of gambling. This framework can stimulate a

³³ Mississippi employee training: MGC Regs. Title 13, Part 3, Rule 10.6

³⁴ Delaware employee training: 19 Del. C. § 4805(a)(29).

better understanding of gambling, further elucidate the determinants of problem gambling, and indicate a range of intervention strategies.

Throughout the past decade, publicly-funded problem gambling services have received increased support in the United States. The total number of states that reported publicly-funded problem gambling services increased from 37 in 2010 to 40 in 2016, and the total amount of public funding allocated to problem gambling services increased from \$60.6 million in 2013 to \$73.0 million in 2016. Among the states that provided funding, the most commonly supported services were problem gambling awareness programs, counselor training, helplines, and problem gambling treatment. Despite the continued growth of problem gambling efforts throughout the United States, in 2016, about one quarter of one percent of people who needed problem gambling treatment received publicly-funded care from a gambling treatment specialist.

Public Education and Informed Choice

Across gaming jurisdictions worldwide, governments and gaming providers have recognized the importance of providing patrons sufficient information to make informed decisions about their gambling. While individuals retain the ultimate responsibility over their gambling choices and level of participation, optimal decision-making depends significantly on the availability of reliable and comprehensive information. This concept of the “informed decision” is pervasive in systems of law and economics and remains an essential component of effective problem gambling mitigation efforts.

Several environmental factors may influence gambling behavior simultaneously, making it difficult to determine the local impact of any one factor. Advertising to promote problem gambling awareness, for example, has attempted to influence gambling behavior and reduce gambling-related harm. Various studies have concluded that the impact of advertising is not likely to be overt, and it may be difficult to measure the impact of advertising efforts to promote problem gambling awareness.

States may require that casinos post signs and/or offer brochures identifying the risks of gambling, signs of gambling disorder, the odds of casino games and/or toll-free phone numbers and other resources for assistance. Common practices among the states include requirements that gambling facilities ensure their advertisements display problem gambling help-line phone numbers. Additionally, some states, like Maryland, require that radio, television, and video advertisements contain a gambling assistance message.³⁵

Some states provide regulations that specifically address risk-related advertisements for internet and mobile gaming. Delaware, for example, mandates that internet lottery websites include advertisements for and links to information for treatment, education, and assistance of compulsive

³⁵ Maryland advertising requirements. COMAR 36.03.06.03(B)(5).

gamblers and their families.³⁶ Similarly, West Virginia requires online sportsbooks and mobile gambling applications to display links to responsible gaming resources.³⁷

Gaming jurisdictions have acknowledged that different messaging approaches may work better for different groups. One Canadian study prospectively detailed the most effective messaging approach for different styles of gaming. For casual gamblers (new and occasional gamblers), programs that enhance gambling literacy, including key safeguards and main risk factors, are essential. Frequent gamblers (i.e., those that gamble at least once per month, but not weekly) need a deeper understanding of how gambling works, including information on house edge, randomness, and independence of events. Finally, the study concluded that intensive gamblers (i.e., those who gamble weekly or more often) need to be informed of their play activity, offered self-assessment tools that draw attention to the consequences of their gaming habits, and made aware of the options available for help in addressing gambling-related problems.

Additional Mitigation Strategies

In addition to the main mitigation techniques discussed above, various jurisdictions also employ additional strategies to promote healthy gambling practices. These strategies include restrictions on alcohol, treatment and research funding, and casino credit restrictions along with bet limits.

Restrictions on Alcohol

Several states require casinos to limit alcoholic beverage service on the gaming floor, or to limit access to gambling services for patrons who are visibly intoxicated. The extent of restrictions on the sale of alcoholic beverages varies across different states. Some states, like Michigan and Kansas do not impose any restriction on alcohol service in gaming facilities. Other states, however, like Massachusetts and Maryland limit the time and place of alcohol sales.

Many states that restrict alcohol service mandate that gambling facilities refuse to sell or serve alcohol to patrons that appear intoxicated, or are younger than 21-years old.³⁸ Maryland, for example, requires that video lottery licensees prevent intoxicated individuals from playing video lottery or table games and prohibit intoxicated individuals from entering areas where such games are located. Maryland further restricts alcohol service by prohibiting licensed operators from providing complimentary alcoholic beverages.³⁹

As a further restriction on alcohol service in gambling facilities, Massachusetts requires gambling facilities to obtain a gaming beverage license in order to serve alcohol on the premises of such a facility.⁴⁰ The sale of alcohol must adhere to the conditions of the issued gaming beverage license, which may be imposed on such license “in the interest of the integrity of gaming and/or public

³⁶ Delaware advertising requirements. 29 Del. C. § 4826.

³⁷ West Virginia advertising requirements. WV CSR § 179-9-13.4.

³⁸ See e.g., 4 Del. C § 706; Md. Code Ann., State Govt. Law, § 9-1A-24(c)(1); 205 CMR 136.02.

³⁹ COMAR 36.03.10.09(A)(2)

⁴⁰ M.G.L. Ch. 23K, § 26.

health, welfare, or safety.”⁴¹ Massachusetts further requires that gaming licensees promulgate a system of internal controls to monitor the sale of alcohol. At minimum, such a system must include procedures to (1) ensure proper training of employees involved in the service of alcoholic beverages, (2) prevent serving alcoholic beverages to underage or visibly intoxicated individuals, (3) ensure that visibly intoxicated or impaired patrons are not permitted to play slot machines or table games, and (4) ensure that alcohol is properly secured and stored.⁴² In addition, Massachusetts prohibits the sale of alcohol between 2:00AM and 4:00AM to patrons who are not in the gaming area and not actively engaged in gambling.⁴³

Restrictions on the sale of alcohol play a significant role in the gambling regulations of several states. While the extent of such restrictions may vary, the motivation to promote public health and welfare remains widely relevant.

Treatment and Research Funding

States may implement financial commitments to support treatment for problem gamblers, education services concerning problem gambling, and research to advance responsible gaming and prevent problem gambling. Most states that implement such commitments earmark certain state revenues from gaming for these programs.

Pursuant to advancing public health efforts, Massachusetts assesses an annual fee in proportion to the number of gaming positions at each gaming establishment. This fee is meant to cover the costs of public health services and programs dedicated to addressing problems associated with compulsive gambling.⁴⁴ Monies within the Fund may be expended to assist social service programs that address gambling prevention, substance abuse services, and educational campaigns to mitigate the potential addictive nature of gambling.⁴⁵ Massachusetts also imposes upon each gaming licensee a requirement to provide on-site space for independent substance abuse, compulsive gambling, and mental health counseling services.⁴⁶

Efforts in other states pursue a more targeted approach, focusing treatment funding specifically on problem gambling, rather than on addictive behavior in general. Kansas, for example, established the Problem Gambling and Addictions Grant Fund to provide assistance for the treatment of “persons diagnosed as suffering from pathological gambling.”⁴⁷

The scope of research efforts varies from state to state. Massachusetts has established an annual research agenda to study the social and economic effects of gaming in the State and to obtain

⁴¹ 205 CMR 136.02.

⁴² Massachusetts alcohol service restriction: 205 CMR 138.12.

⁴³ Massachusetts alcohol service restriction: 205 CMR 136.07(7)(i).

⁴⁴ Massachusetts research statutes. M.G.L., Ch. 23K, § 56(e).

⁴⁵ M.G.L., Ch. 23K, § 58.

⁴⁶ M.G.L., Ch. 23K, § 21.

⁴⁷ Kansas problem gambling treatment statutes. K.S.A. §79-4805(c)(1).

scientific information relative to neuroscience, psychology, sociology, epidemiology, and etiology of gambling.⁴⁸ Similarly, Michigan reserves a significant portion of the monies within its Compulsive Gambling Prevention Fund for, among other things, “research, and evaluation of pathological gamblers and their families.”⁴⁹

The majority of states have implemented treatment and research funding provisions to make gaming as healthy for participating individuals, and the environment around them, as possible.

Casino Credit Restrictions and Bet Limits

Some state laws aim to protect patrons from betting more than they can afford to lose by banning casinos from offering credit advances and limiting bet amounts. Methods to limit credit advances include both patron-driven efforts, such as voluntarily placing one’s name on a credit exclusion list, and facility efforts, including policies and procedures that limit those patrons to whom a gambling facility may issue credit.

Generally, the procedures established by states aim to ensure that a gaming facility does not extend credit to patrons beyond an amount that those patrons lack a reasonable ability to repay. Regulations may range from broad mandates to gaming operators to exercise caution and good judgment in extending credit⁵⁰, to more specific rules that identify groups to whom credit should be limited. As an example of targeted restrictions, Massachusetts requires that a gaming licensee’s policies prevent the extension of credit to patrons who self-identify as problem gamblers, place themselves on a voluntary credit suspension list, or are on public assistance.⁵¹

While the use of credit restrictions as a mitigation tool may vary across states, the desired effect of such restrictions and limitations remains similar. The promotion of safe gambling habits through credit restrictions and bet limits emerges as a primary goal of many states.

⁴⁸ Massachusetts research statutes. M.G.L., 23K, § 71.

⁴⁹ Michigan problem gambling research statutes. MCL 432.253.

⁵⁰ Delaware credit restrictions. 10 Del. Admin. Code 204-6.1.10.

⁵¹ Massachusetts credit restrictions. 205 CMR 138.43(1)(d).

APPENDIX B: CASINOS AND CRIME

The social and community impacts of gaming development have been extensively studied. In many areas research findings have been inconclusive and thus considerable resources continue to be devoted to researching possible negative impacts given the unique nature of gaming compared to other commercial enterprises.

A number of broad studies of the social and economic impact of casinos have been conducted in the United States. In the late 1990s, prompted by the expansion of casinos throughout the United States, mainly in the form of riverboat casinos, Native American casinos, and racetrack slot parlors, Congress set up the National Gambling Impact Study Commission (NGISC). Its findings were released in 1999.

The Commission retained the National Research Council (NRC) to review the existing research on the socio-economic impacts of casino development. The NRC concluded that the existing research on the subject was inadequate:

The NRC project involved a review of all existing and relevant studies by representatives of a variety of scientific fields. In the end, NRC recommended that further study be initiated. Study of the benefits and costs of gambling “is still in its infancy.” Lamenting past studies that utilized “methods so inadequate as to invalidate their conclusions,” the absence of “systematic data,” the substitution of “assumptions for the missing data,” the lack of testing of assumptions, “haphazard” applications of estimations in one study by another, the lack of clear identification of the costs and benefits to be studied, and many other problems, NRC concluded the situation demands a “need for more objective and extensive analysis of the economic impact that gambling has on the economy.”⁵²

The Commission then retained the National Opinion Research Center (NORC) to undertake said “objective and extensive analysis” concerning impacts. The NORC came to the following conclusion:

First, the casino effect is not statistically significant for any of the bankruptcy or crime outcome measures..... This is not to say that there is no casino-related crime or the like; rather, these effects are either small enough as not to be noticeable in the general wash of the statistics, or whatever problems that are created along these lines when a casino is built may be countered by other effects.⁵³

Despite the NGISC’s authoritative findings, some researchers continue to claim that casinos cause crime.⁵⁴ However, there are three major flaws in much of this research:

⁵² National Gambling Impact Study, Chapter 7. 1999. *Gambling’s Impact on People and Places*.

⁵³ The National Gambling Impact Study Commission, “National Gambling Impact Study” (1999).

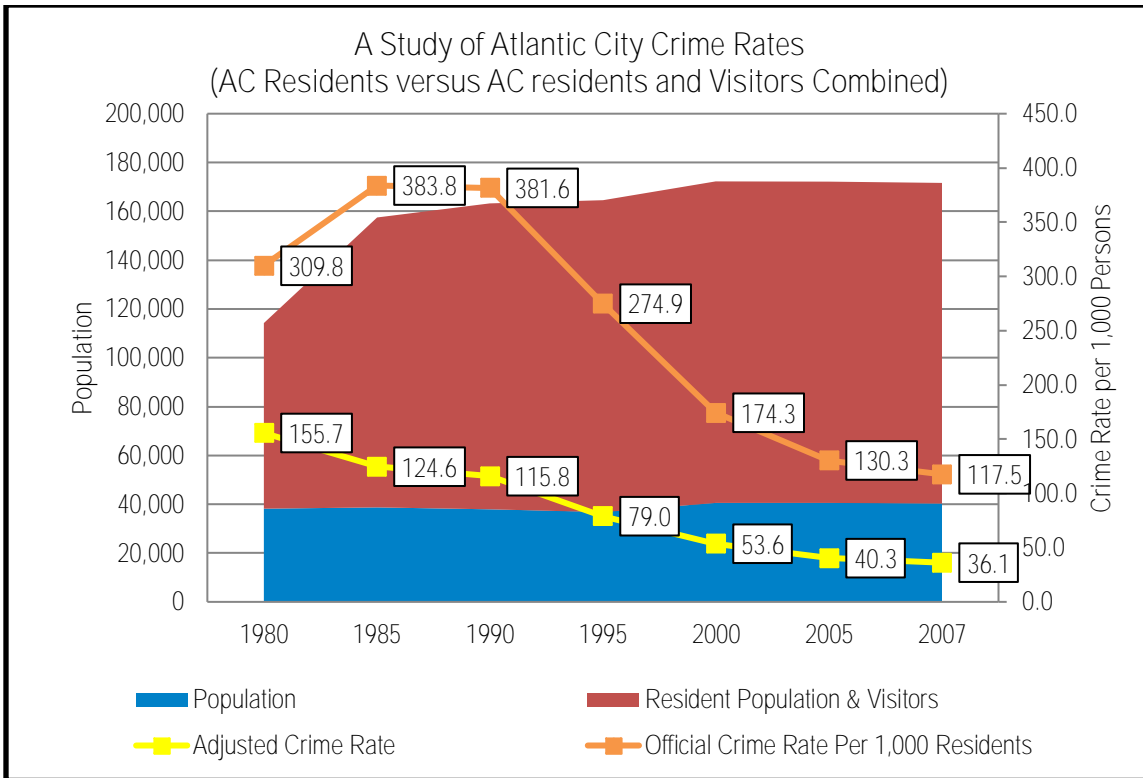
⁵⁴ See Grinols and NBER discussion below.

1. Much of the research that attributes an increase in crime to casinos has ignored the temporary population increases brought about by casino visitation. When crime rates are calculated not accounting for the influx of visitors, there appears to be an increase in crime. While this may be true in absolute terms, it radically overestimates the increase in likelihood of residents being victims of crime.
2. Further to #1, some research applies crimes such as on-site thefts of casino visitors to the local population, leading to an invalid increase in the local crime rate.
3. The crimes rates are not studied over a sufficient period of time and therefore temporary increases or long term trends attributable to more primary causal factors are not always recognized or are misinterpreted.

One of the earliest examples of flawed research is related to Atlantic City. The **number** of crimes tripled after casinos opened in 1978, and some researchers applied the increase to the local resident population, which in the resulting invalid calculation resulted in a tripling of the crime **rate**. However, most of the increase related to thefts within the casinos, which did not impact the local population. A valid calculation of the crime rate has to include the visitation base.

In fact, there has been a **decreased** chance of being a victim of crime since casinos were developed in Atlantic City. Factors likely include an increase in casino employment and law enforcement resources, safer infrastructure with well-lit garages, and an increase in general tourism activity. According to more recent data supplemented to the study completed by Margolis et al,⁵⁵ this decline in crime rates per 1,000 residents continued through 2007 to a rate of 36.1 per thousand residents. The chart below illustrates the crime rate trends from 1980 to 2007.

⁵⁵ Margolis, J. & Altheimer & Gray. (December 1997). "Casinos and crime: An analysis of the evidence." American Gaming Association. <http://www.americangaming.org/assets/files/studies/Crime.pdf> . The Innovation Group.



The Rappaport Institute for Greater Boston and the John F. Kennedy School of Economics at Harvard University (Baxandall and Sacerdote 2005) in a national, county-level study of Native American casinos found a slight decrease in crime rates after casinos opened. The analysis included all California casinos in existence in the 1990s. From their total sample of 156 casino counties, the Rappaport study isolated out 57 counties with large casinos and relatively low population and nine counties with both large casinos and large populations to see if there were statistical differences in terms of community impacts. The following table shows their results:

Table 65: Rappaport Study Results

	All Casino- Counties ¹	Counties with Large- Capacity Casinos ²	Populous Casino Counties ³
Population Growth (%)	+5*	8.6	+8.1*
Total Employment (%)	+6.7*	+14.9*	5.7
Unemployment (%)	-0.3	-1.2*	0.5
House Prices	\$5,869	\$8,924	\$7,083
Crime (Per 1,000 People)	-3	-6	-1

*Statistically significant results at 99% confidence interval.

1. Reports how adjusted outcomes in 156 counties that introduced Indian-run casinos during the 1990s differed from the other 2,959 that did not.

2. The effect for 21 counties in the top 10th percentile in terms of number of slot machines (over 1,760).

3. The effect for the 57 casino counties in the top population quartile (over 55,000 residents).

The Rappaport study concluded:

Our analysis shows that while total crime can be expected to increase when casinos open, the increase is due to increased population, not to a casino-created crime wave. Looking at FBI indexed crimes per resident in all [156] counties; we find that introducing a casino is associated with a decrease of 3 reported crimes per 1,000 people. The introduction of a casino, however, had no statistically significant effect on per-capita crime rates in either large-population casino counties or in large-casino counties. The per-capita crime rate in the 9 large-population counties that also hosted large-capacity casinos dropped 9 crimes per 1,000 residents, however.⁵⁶

It is worth noting that the study included two of the largest casinos in the world, Foxwoods and Mohegan Sun. In Ledyard, Connecticut (which hosts the Foxwoods casino), crimes outside the casino increased from 214 in 1991 to 364 in 1998, but in subsequent years, State Police data show that off-casino crimes in Ledyard fell below pre-casino levels. In Montville, Connecticut (host to Mohegan Sun), as with Ledyard, the number of crimes reported “remained relatively constant,” which the authors conclude is “surprising since the sheer increase in activity around these towns might have led to greater crime.”⁵⁷

The study also highlighted results for three counties in southern California: Riverside, San Bernardino, and San Diego. In all three counties, crime decreased relative to the state average. For example, before casino development, Riverside County suffered 22 more crimes per 1,000 residents than the state average. After casino development, the county had just 6 more crimes per 1,000 residents than the state average, a relative decrease of 16 crimes per thousand residents. San Bernardino had a relative decrease of 10 crimes per thousand, and San Diego 9.

Table 66: Rappaport Study California County Results for Crime

	Relative Crime (Before)	Relative Crime (After)	Change in Relative Crime (After - Before)
Riverside, CA	0.022	0.006	-0.016
San Bernardino, CA	0.016	0.006	-0.01
San Diego, CA	0.008	-0.001	-0.009

⁵⁶ IBID. As summarized in their 2008 report, “Betting on the Future: The Economic Impact of Legalized Gambling.”

⁵⁷ Baxandall, P. & B. Sacerdote (January 2005). *The Casino Gamble in Massachusetts: Full Report and Appendices*. Rappaport Institute for Greater Boston, John F. Kennedy School of Economics, Harvard University. Page 14.

In other western jurisdictions, the Montana legislature in 1997 commissioned a study on the video gaming industry. The resulting analysis found no impact on crime rates in Montana:

While gambling may have caused an increase of certain types of crime, Montana’s overall crime rate increase is not any higher than the increases in matched cities with little or no legal gambling. In fact, in almost three-quarters of the specific comparisons carried out, crime rates rose more (or decreased less) in the matched cities than in the Montana cities.

Each of the seven largest Montana cities was matched with an out-of-state city in the region with similar population size, similar population growth rate, similar racial composition, but with little or no legal gambling. The percentage change in crime rates for three indices of crime (total serious crime, property crime, and violent crime) was computed for three time periods... between 1984 and 1994. [The data] illustrate the lack of a systematic pattern in crime rate changes between Montana cities and those in states with little or no gambling. For example, the violent crime rate grew faster in Cheyenne, Wyo., than in Great Falls between 1984 and 1994, yet the index of property crime decreased in Cheyenne while it increased in Great Falls during the same period.⁵⁸

In summary, there is no evidence from gross level data that the advent of casinos has a measurable impact on local crime rates in general, whether in Eastern, Midwestern, or Western jurisdictions. It is highly likely any crimes associated with casinos are either offset by economic benefits or that the level of crime is so small as to be overwhelmed by other factors such as economic trends.

Primary Research from Select Casino Jurisdictions

The figures from the casinos used in the Comparative Analysis Criminal Incidents section, provide a general picture of criminal activity at a casino. Other communities have found lower and higher levels of incidents. For example, figures from the Kenner Police Department note an average of 9 criminal incidents at the Treasure Chest Casino from 2012 to 2014. Attendance at the Treasure Chest Casino in Kenner is over one million annually.

A recent article in The Enterprise provided additional qualitative data from the casinos in this analysis. An officer from the Pittsburgh Police department compared the number of calls to games at the local baseball and football stadiums, “Nothing different than when there’s a ball game,” Luczak said. “I wouldn’t say there’s much change.”⁵⁹

Des Plaines Police Deputy Chief Nick Treantafeles had similar sentiments, “It’s just like any place that serves alcohol,” he said. “You get drunk and disorderly, but their security handles 98 percent of the issues there. We might get called for a fight that gets out of hand. ... It hasn’t put a damper on the services we offer the rest of the community.”⁶⁰

⁵⁸ Montana Gambling Commission Study, 1998, Chapter 8.

⁵⁹ <http://www.enterpriseneews.com/article/20150517/NEWS/150516955/12741/NEWS/?Start=1>

⁶⁰ <http://www.enterpriseneews.com/article/20150517/NEWS/150516955/12741/NEWS/?Start=1>

While specific increase in police staffing varies from community to community, many communities found no need to increase police staffing, as shown below in the examples from Indiana. The Center for Urban Policy and the Environment at Indiana University-Purdue University has prepared 5-year evaluations of riverboat licensees for the Indiana Gaming Commission which contain sections on community impacts. The following bullet points include summaries and excerpts from these reports with respect to police and fire protection.

Casino Aztar:

- The Evansville Police Department reports no increases in crime since the riverboat opening. They do report a drop in crime in 1999 when compared to the previous year.
- “No new police officers or firefighters were added. Traffic control has not been a problem...”

Majestic Star:

- The community purchased 12 police cars with Year 1 incentive payments.
- Gary’s Chief of Police reports no additional criminal activity surrounding the riverboat.

Horseshoe Hammond (formerly Empress Casino Hammond):

- The Hammond Police Department reports crime has fallen in most categories when compared to before the boat opened.

Hollywood (formerly Argosy):

- According to the Lawrenceburg Police Department, casino-related arrests for public intoxication, DWI, and minor theft, as well as traffic accidents in the area have increased slightly each year from 1997 to 2000.
- Lawrenceburg has added two police officers since the boat opened to deal with the increased caseload.

Ameristar (formerly Harrah’s East Chicago):

- According to East Chicago’s police department, no additional criminal activity can be attributed to the riverboat’s presence.
- “Crime in East Chicago has decreased substantially over this time period due to increased cooperation with federal agencies, community policing and increased staffing.”

Blue Chip Casino:

- According to Michigan City’s chief of police, no additional criminal activity can be attributed to Blue Chip’s presence.

On the issue of crime, Jeremy Margolis, who had served as Assistant U.S. Attorney in Chicago, Illinois Inspector General, and Director of the Illinois State Police, found in a 1997 study⁶¹ that the chance of being victim of a crime decreases after casino development. Factors include an increase in employment brought by casinos, increased law enforcement resources, safer infrastructure with well-lit garages, and an increase in general tourism activity.

In testimony before the Pennsylvania Gaming Control Board (PGCB) in 2006, Margolis was asked to give an update of his seminal study. Margolis concluded, based on examining updated crime data from the F.B.I. as well as interviews with the Executive Director of the Illinois Crime Commission, the Illinois State Police, and the Illinois Gaming Board, that the situation is “really unchanged except for the maturation of the industry, the maturation of the regulatory process has probably settled things down more than it had settled when I completed my study in 1997. It’s just not an issue.”⁶²

⁶¹ Margolis, J. (December 1997). “Casinos and crime: An analysis of the evidence.” American Gaming Association.

⁶² PGCG hearing transcript, September 7, 2006, pages 22-23.

APPENDIX C: IMPACT ON LOCAL BUSINESS

Research Results

There is a substantial body of research and case studies demonstrating the impacts that casinos have on surrounding local businesses. There are several important reasons that local businesses benefit from the development of a casino:

- Casino visitors stopping at local retail outlets and restaurants.
- Long-distance patrons staying at area hotels; even in markets with casino hotels, non-casino hotels enjoy boosts in occupancy.
- Casino expenditures on local goods and services put more money into the local economy.

A review of studies of casino impacts on local business shows that casinos can stimulate local economies, resulting in communitywide growth, including in the local food and beverage business and retail businesses. There is little evidence of significant economic substitution after the introduction of new casinos, particularly for casinos in urban areas.

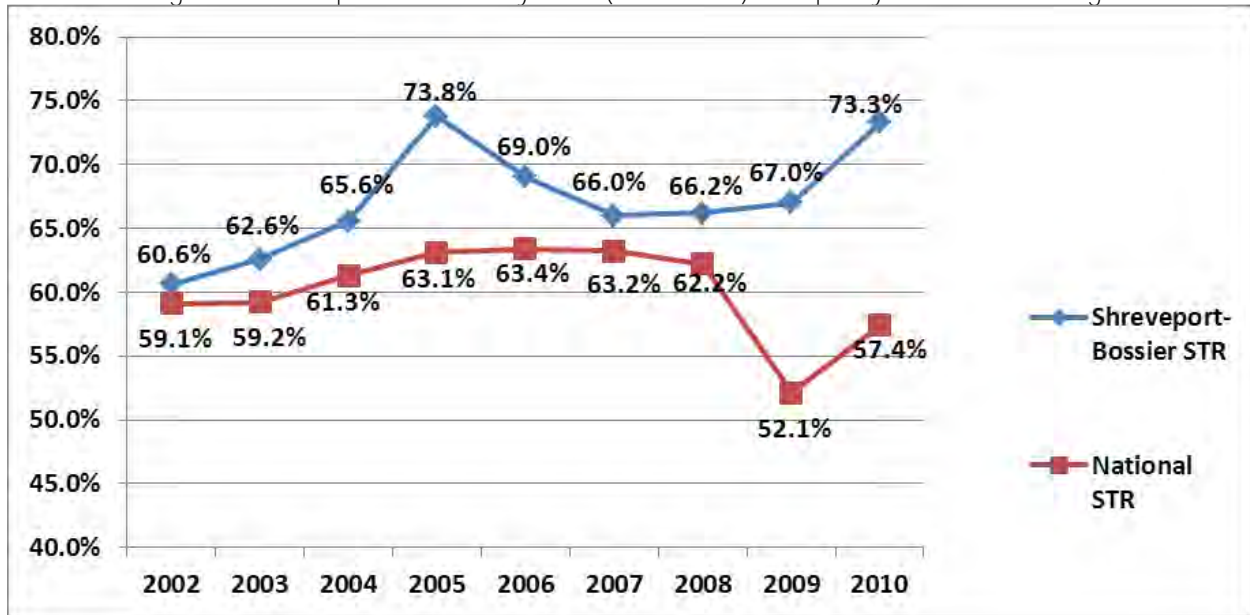
Primary Research

Casino development increases room demand at non-casino hotels even when casino hotels are built. For example, in Shreveport-Bossier City, Louisiana, hotel occupancy rates averaged approximately 60% before casinos were developed beginning in 1994, which is a standard level of occupancy for a small city market without casinos. The Shreveport-Bossier City casino industry was fully developed by 2003 with six casinos featuring 9,500 gaming positions, by which time hotel occupancy in non-casino hotels had risen to 63%, climbing to 74% by 2005.⁶³

The casino industry also helped non-casino hotels in Shreveport-Bossier City weather the impacts of the Great Recession much better than the national hotel market, with hotel occupancy dropping to no lower than 66% compared to the national trough of 52%.

⁶³ Shreveport-Bossier Convention and Tourism Bureau 2011 Lodging Report.

Figure 4: Shreveport-Bossier City Hotel (non-casino) Occupancy v. National Average



Source: Shreveport-Bossier Convention and Tourism Bureau 2011 Lodging Report; Smith Travel Research

Such a boost to non-casino hotel demand results from the overall increased visitation to the area and the overflow from peak periods when casino hotels are fully booked. On the Mississippi Gulf Coast, gaming began in late 1992 and by the early 2000's there were 11 casinos, all but one of which had associated hotels. During this period, occupancy rates in non-casino hotels remained steady at 55% despite a 143% increase in total rooms, including a 60% increase in non-casino hotel rooms.⁶⁴

The overflow effect has been experienced in numerous jurisdictions beyond the Mississippi and Shreveport examples presented above. In fact, third-party developers frequently build new hotels in the vicinity of a casino to take advantage of that overflow, even in remote areas with no other organic sources of demand. For example, an Americas Best Value Inn, a Best Western and a Days Inn were developed next to the Coshatta Casino Resort in Kinder, Louisiana even though the remote casino property has over 950 rooms of its own.

Gaming development on the Mississippi Gulf Coast also boosted retail and restaurant development by local business owners taking advantage of the increased visitation to the area. As the following table shows, between 1991 and 1997 the number of retail and eating and drinking establishments increased in the two counties that host casinos. The increases were an astounding 77 percent for drinking and dining establishments in Hancock County while retail establishments in both counties also increased over this period.

⁶⁴ Source: Mississippi Gaming Commission.

Table 67: Change in Retail and Eating and Drinking Establishments 1991-1997

County	Retail	Eating and drinking places
Harrison County, MS	14.7%	4.2%
Hancock County, MS	6.6%	77.1%

Source: US Census County Business Patterns

On the West Coast, three separate data sources indicate that substantial hotel development at tribal casinos on the outskirts of San Diego (and not within the City) has not negatively impacted local hotels. The Transient Occupancy Tax (TOT) collected by the City of San Diego has grown substantially since recovering from the 2009-10 recession effects, despite the operation of several large rural casino hotels, including an 1,100-room hotel at Harrah’s Rincon, that do not pay the TOT.

Table 68: City of San Diego, CA Transient Occupancy Tax Collections (MMs)

2008	2009	2010	2011	2012	2013	2014	2015	2016	2017
\$160.24	\$136.32	\$128.11	\$139.77	\$150.82	\$157.03	\$170.17	\$186.24	\$202.80	\$221.10

Source: San Diego Tourism Authority

While some of the TOT increase may be related to collection of the TOT at AirBnBs beginning in 2015, hotel occupancy data—which do not include AirBnBs—also increased in the three years from 2014 to 2016. HVS reports that hotel demand and occupancy in the San Diego market “increased steadily from 2010 through 2018, resulting in peak occupancy levels above 78% by year-end 2018.” RevPAR (revenue per available room, a measure of hotel performance) has also seen steady increases since the recession, rising above \$130 for the first time in 2018.⁶⁵

This HVS report is corroborated by STR data for the San Diego market, data that excludes casino properties, as shown in the following table.

⁶⁵ http://hvi.hvs.com/market/united-states/San_Diego; accessed June 2, 2022.

Table 69: San Diego pre-Covid Hotel Trends

	Occupancy %	RevPAR \$	Room Revenue (\$MMs)
2012	73.3	100.27	1,377
2013	74.0	103.22	1,414
2014	76.6	112.00	1,534
2015	78.5	122.23	1,674
2016	79.3	126.62	1,768
2017	79.2	130.47	1,844
2018	80.9	138.44	1,951
2019	78.7	134.84	1,937

Source: STR; The Innovation Group

Secondary Research

Casino development sometimes elicits concern, which research has dispelled, that substitution of consumer spending (the substitution effect) will impact local businesses, especially smaller “mom and pop” retail, restaurant and entertainment businesses. This argument has its origins in long-ago controversies regarding Atlantic City. Clyde Barrow, Director for the Center of Policy Analysis at the University of Massachusetts Dartmouth, traces the Atlantic City “myth” to a misinformation campaign by the Atlantic City Restaurant and Tavern Association “to win more concessions for its members from the city’s casino hotels.”⁶⁶

Research by Kathryn Hashimoto and George Fenich found that contrary to a negative impact, casinos in Atlantic City actually reversed a downward trend:

The number of eating and drinking establishments in Atlantic County was actually declining in the years *prior* to the opening of the first casinos. However, this decline was actually reversed after the first casinos opened, when the number of non-casino eating and drinking places increased from 415 in 1978 to 569 in 1994 (37 percent). Moreover, in the 11 years since the Hashimoto and Fenich study, the number of non-casino eating and drinking places in Atlantic County has continued to increase to 625 (9.8 percent) in 2004 with 9,020 employees (36 percent).⁶⁷

In a review of available literature, the research division of the Federal Reserve Bank of St. Louis concluded in a 2003 report that the results are “mixed” regarding the impacts of casinos on other local businesses. The report references one study that “found that the growth in retail sales tax collections from various industries slowed after the introduction of casino gambling.” However,

⁶⁶ Barrow, Clyde and Mathew Hirshy. “The Persistence of Pseudo-Facts in the U.S. Casino Debate: The Case of Massachusetts” *Gaming Law Review and Economics* Volume 12, Number 4, 2008.

⁶⁷ *Ibid.*

another referenced study from Indiana showed that casino development retained spending by patrons “who would have, without the casino, spent their money outside of the local area.”⁶⁸

Furthermore, there is substantial economic research from throughout the country contradicting the substitution effect. Hashimoto and Fenich’s 1997 research shows that “in jurisdictions from the seashore to the riverfront to rural areas, north and south, east and west, local restaurants tended to thrive after a casino opened nearby.” Furthermore, Hashimoto and Fenich conclude: “When casinos are developed, all aspects of the local food and beverage business increase: the number of establishments increases, the number of people employed increases and payroll increases at an even greater rate than the first two.”⁶⁹

Research conducted in 1996 by Nancy Reeves and Associates for the Mille Lacs Band of Ojibwe, entitled “The Economic Impact of Grand Casino Mille Lacs and Grand Casino Hinckley on Their Surrounding Areas” concluded that:

At least 15 businesses have either opened, expanded, or re-opened since the opening of Grand Casino Mille Lacs. Included are 4 hotels/motels and resorts, 8 restaurants and fast food establishments, 2 gas stations and a go-kart track. Together, these businesses have added an estimated 142 jobs in the area.

With the opening of Grand Casino Hinckley in 1992, the hospitality business in Hinckley was transformed from a rest stop for travelers to a tourist destination. In addition to the casino complex, with its 1,275 jobs, Hinckley has added 11 new businesses and expanded 4 more since 1992, adding 87 new jobs. As is the case in the Mille Lacs area, Hinckley is now a year round destination because of the casino. Also similar to the Mille Lacs situation, the main street businesses in Hinckley have seen increases in customer spending attributed primarily to casino employees living in the area.

The Center for Policy Analysis University of Massachusetts Dartmouth came to similar conclusions analyzing a number of gaming jurisdictions throughout the country. The number of restaurants and retail sales excluding those from casinos increased in Bossier City, Louisiana; Biloxi/Gulfport, Mississippi; Connecticut; Gilpin County, Colorado, and; Tunica County, Mississippi.

There was a net increase of eight restaurants in Bossier City, Louisiana following the introduction of riverboat casinos. The city’s taxable restaurant sales, excluding restaurants in the hotels and casinos, increased by 5 percent in 1994 and by 7 percent in 1995 *after* the introduction of riverboat casinos. In Biloxi/Gulfport, Mississippi, the rate of non-casino

⁶⁸ Thomas A. Garrett, Senior Economist, Federal Reserve Bank of St. Louis, *Casino Gambling in America and Its Economic Impacts*, August 2003.

⁶⁹ George Fenich and Kathryn Hashimoto, “The Effects of Casinos on Local Restaurant Business,” paper presented at the International Conference on Gambling and Risk-Taking, Montreal, 1997.

retail sales growth increased from an average of 3 percent annually (1990-1992) in the years prior to riverboat gambling to 12 percent annually in the years after riverboat gaming began in the locality.

...the number of restaurants in the area surrounding Foxwoods and Mohegan Sun increased from 472 to 506 following the casino's opening, while restaurant employment increased from 5,911 to 6,628 during the same period.... In Gilpin County Colorado, the number of restaurants increased from 31 to 40 after the introduction of casino gaming. In Tunica County, Mississippi, the number of restaurants increased by 13 percent and restaurant employment grew by 9 percent after the introduction of casino gaming in the county.⁷⁰

Similar conclusions have been reached in other studies:

- Even after accounting for substitution effect, economists at the University of Missouri and Washington University concluded that casino gambling in Missouri had a net positive annual impact on Missouri output of \$759 million, corresponding to a continuing higher level of employment of 17,932 jobs generating \$508 million more in personal income.⁷¹
- A multijurisdictional analysis of retail spending found that in Biloxi/Gulfport, Miss., annual retail sales growth rates increased an average of 3 percent per year from 1990 to 1992, the year when casinos were introduced. Between 1993 and 1995, retail sales jumped 13 percent. In Will County, Ill., retail sales growth trailed statewide trends until 1992, when riverboat casinos were introduced in the local economy. But each year between 1992 and 1995, retail sales growth in Will County exceeded the state rate. In Shreveport/Bossier City, La., retail sales increased by more than 10 percent during 1994, the year that riverboat casinos opened, as the region enjoyed the highest retail sales increase in more than a decade.⁷²

In summary, there is a wealth of evidence contradicting the proposition that gaming substitutes for other expenditures. The positive spillover effect on local hotels for one is unequivocally demonstrated in numerous jurisdictions, even in markets where casinos operate hotels for their gaming customers.

⁷⁰ Ibid.

⁷¹ Charles Leven et al., "Casino Gambling and State Economic Development," paper presented at the Regional Science Association, 37th European Congress, Rome, Aug. 26-29, 1997.

⁷² Arthur Andersen, *Economic Impacts of Casino Gaming in the United States, Volume 2: Micro Study* (Washington, D.C.: American Gaming Association, May 1997).

DISCLAIMER

Certain information included in this report contains forward-looking estimates, projections and/or statements. The Innovation Group has based these projections, estimates and/or statements on our current expectations about future events. These forward-looking items include statements that reflect our existing beliefs and knowledge regarding the operating environment, existing trends, existing plans, objectives, goals, expectations, anticipations, results of operations, future performance and business plans.

Further, statements that include the words "may," "could," "should," "would," "believe," "expect," "anticipate," "estimate," "intend," "plan," "project," or other words or expressions of similar meaning have been utilized. These statements reflect our judgment on the date they are made and we undertake no duty to update such statements in the future.

Although we believe that the expectations in these reports are reasonable, any or all of the estimates or projections in this report may prove to be incorrect. To the extent possible, we have attempted to verify and confirm estimates and assumptions used in this analysis. However, some assumptions inevitably will not materialize as a result of inaccurate assumptions or as a consequence of known or unknown risks and uncertainties and unanticipated events and circumstances, which may occur. Consequently, actual results achieved during the period covered by our analysis will vary from our estimates and the variations may be material. As such, The Innovation Group accepts no liability in relation to the estimates provided herein.

Appendix E

Expanded Regulatory and
Environmental Setting

Expanded Regulatory and Environmental Setting

INTRODUCTION

This document summarizes the framework of laws and regulations relevant to the Project Site and actions outlined in the Environmental Assessment (EA). Sections are organized by resource category, and while most regulations discussed within the document are described here, this list is not comprehensive and is limited to the primary regulations relevant to the analysis within the EA. Additionally, because the Project Site currently consists of Native allotment land, it is not generally subject to State or local laws or regulations.

LAND RESOURCES – EA SECTION 3.2

Regulatory Setting

Federal

Clean Water Act

The Clean Water Act (CWA) prohibits sediment and erosion discharge into navigable waters of the United States and establishes water quality goals. The Water Resources Board requires a Construction General Permit if a project disturbs one or more acres of soil. A site-specific Stormwater Pollution Prevention Plan (SWPPP) is required under this permit. For more information on the CWA and the Water Resources Board, see **Water Resources – Section 3.3 of the EA** below.

Surface Mining Control and Reclamation Act (SMCRA) of 1977

The SMCRA regulates the environmental effects of coal mining in the United States. It is the first federal environmental statute to regulate a specific industry rather than a specific type of pollution. The Act prohibits surface coal mining within the boundaries of any unit of the National Park System. There is a program for regulating active coal mines and one for reclaiming abandoned mine lands.

State and Local

Seismic Hazards Mapping Act

The Seismic Hazards Mapping Act was enacted in 1990 to protect the public from the effects of strong ground shaking, liquefaction, landslides, ground failure, or other hazards caused by earthquakes. This act requires a state geologist to delineate various seismic hazard zones and requires cities, counties, and other local permitting agencies to regulate certain development projects within the portions of those zones where they have jurisdiction. Before a development permit is granted by a city, county or other local permitting agency for a site within a seismic hazard zone, a geotechnical investigation must be conducted, and appropriate mitigation measures must be incorporated into the project's design.

Department of Natural Resources, Division of Agriculture Soil Conservation Program and Plant Materials Center

Provides plant science expertise and technical assistance to government agencies, contractors, land users, and the general public for the protection of soil resources. Provides professional advice and guidance with Storm Water Pollution Prevention Plans (SWPPP), site specific erosion and sediment control measures, and soils analysis interpretation (ADNR, 2024b).

Department of Natural Resources, Geological and Geophysical Surveys

Provides information related to oil and mineral discoveries. Investing in geologic information is necessary for the discovery and commercialization of the state's undiscovered oil, gas, coal, and mineral resources, and in protecting Alaskans from geologic hazards. It is an investment in future mines, oil and gas production, State revenue, jobs, and a sound economy (ADNR, 2024c).

Municipality of Anchorage Code

Contains the municipalities regulations including those related to sediment and erosion control, seismic design standards, and setbacks to surface waters.

Chugiak-Eagle River Comprehensive Plan

The update is for the 1993 comprehensive plan. The Municipal code requires that a comprehensive plan be reviewed and revised every 20 years. It also requires a re-evaluation of the major trends and policies of a comprehensive plan 10 years from the time of its initial adoption. The plan provides population, housing and employment projections for 2025, updated goals and strategies for development, land use development, and public facilities and services (Municipality of Anchorage, 2006). It also identifies measures to prevent flooding, minimize erosion, assure safety, and prevent eroded material from entering waterways.

Alaska Department of Environmental Conservation CGP

The Construction General Permit authorizes stormwater discharges from large and small construction-related activities resulting in a land disturbance of greater than or equal to one acre where those discharges enter waters of the United States through a stormwater conveyance system or a municipal separate storm sewer system. The 2021 CGP became effective on February 1, 2021 and expires on January 31, 2026 (ADEC, 2024b).

Environmental Setting

Geological Setting

The region's geologic history has been influenced by tectonic activities associated with the Pacific and North American plates. These tectonic activities continue to fuel earthquakes along the Aleutian Trench and associated fault systems. The local geology is characterized as an unconsolidated landform derived from glacial and periglacial deposits that consist of end, lateral, and ground moraine, and outwash. These glacial deposits are prevalent throughout the area, contributing to the region's well-drained soils and diverse geomorphological features. As the glaciers retreated, they also carved out surrounding valleys and fjords, sculpting the regional terrain. Locally, the Elmendorf Moraine is composed of glacial till, which includes a mix of clay, sand, gravel, and boulders that were deposited by retreating glaciers. The moraine marks the furthest advance of the glaciers during the last glacial maximum and lies just to the east of the project site at the base of the Chugach Mountains.

Topography

The Project Site is relatively flat land with sloping topography towards Peters Creek. Elevations on-site range from approximately 73 to 88 feet amsl, with the majority of the site sloping gradually to the north. There is a bench cutting across the northwest corner of the property that drops about 8 feet and then flattens out toward the bank of Peters Creek. The highest portion of the site is the southeast corner at an elevation of 88 feet amsl and the lowest portion of the site is the northwest corner at an elevation of 73 feet amsl. Typical slopes across the site are around 1 to 3 percent. Peters Creek runs along the east side of the site.

Soil Types and Characteristics

The Project Site contains three soil types: Cryorthents and Urban Land, 5 to 20 percent slopes; Kashwitna-Kichatna complex, 0 to 3 percent slopes; and Moose River-Niklason complex, occasionally flooded, 0 to 3 percent slopes (NRCS, 2024). The hydrologic soil group is a classification based on the runoff potential of the soils when thoroughly wet, which is defined by the Natural Resource Conservation Service (NRCS) as being under the conditions of maximum yearly wetness (NRCS, 2007). Soils are grouped into four classes that grade from A to D, with A being coarse-grained soils with high infiltration and low runoff potential and D being mostly fine-grained clays with extremely slow infiltration and high runoff potential. The soils on the Project Site have a hydrologic rating of B, indicating the soils have a moderately low runoff potential when wet and a moderate infiltration rate. Soils with this rating are typically moderately deep to deep, moderately well to well drained, and have moderately fine to moderately coarse textures (NRCS, 2024).

Saturated hydraulic conductivity [Ksat] is a quantitative measurement for the movement of water through saturated soil or the ease with which pores in a saturated soil transmit water. Ksat is a factor in determining the hydrologic soil group and is often used in the design of water and wastewater disposal features such as percolation ponds and septic systems. Ksat measures transport only in a vertical direction under completely saturated conditions.

Soil erosion is the wearing and removal of soil materials from the ground surface and the transportation of these soil materials resulting in deposition elsewhere. Mechanisms of soil erosion include stormwater runoff and wind as well as human activities. Factors that influence erosion include physical properties of the soil, topography, annual rainfall, and peak intensity. Soils on the Project Site transmit water at moderately high to high rates, this indicates that the Project Site has low to medium surface runoff potential. The Project Site has a depth to water table of above 80 inches in the majority of the project site and a depth of 18 to 30 inches in the portion containing the Moose River-Niklason complex soils.

Corrosivity pertains to a soil-induced electrochemical or chemical reaction that corrodes concrete or steel. The soils on the Project Site have no rating or a moderate risk of corrosion to concrete and corrosion to steel (NRCS, 2024). Expansive soils may increase in volume when water is absorbed and may shrink when dried, as expansive soils are largely comprised of clays. The property of expansion is measured using linear extensibility. Expansive soils are of concern because they can cause building foundations to rise during the rainy season and fall during the dry season, causing structural distortion. The soils on the Project Site have mapped low linear extensibility ratings and therefore are not considered to be expansive soils.

Liquefaction occurs when loose, saturated, uniformly graded, fine-grained sand, and relatively cohesionless soil deposits temporarily lose strength from seismic shaking. The primary factors controlling the onset of liquefaction include intensity and duration of strong ground motion, characteristics of subsurface soil, on-site stress conditions, and the depth to groundwater.

The Project site was found to be in Zone 3 seismic zone, which has a moderate ground failure susceptibility (Municipality of Anchorage, 2022).

WATER RESOURCES – EA SECTION 3.3

Regulatory Setting

Federal

Executive Order 11988

Executive Order (EO) 11988 requires federal agencies to avoid to the extent possible the long and short-term adverse impacts associated with the occupancy and modification of floodplains and to avoid direct and indirect support of floodplain development wherever there is a practicable alternative. Specifically, EO 11988 states that agencies shall first determine whether the proposed action will occur in a floodplain. EO 11988 defines a floodplain as an area that has a one percent or greater chance of flooding in any given year. Second, if an agency proposes to allow an action to be located in a floodplain, the agency shall consider alternatives to avoid adverse effects and incompatible development in the floodplains. If the only practicable alternative action requires siting in a floodplain, the agency shall minimize potential harm to or within the floodplain.

Clean Water Act

CWA (33 U.S. Code [USC] § 1251-1376), as amended by the Water Quality Act of 1987, is the major federal legislation governing water quality. The objective of the CWA is “to restore and maintain the chemical, physical, and biological integrity of the Nation’s waters.” The U.S. Environmental Protection Agency (USEPA) is delegated as the administrative agency under the CWA. Relevant sections of the CWA are as follows.

- Sections 303 and 304 provide for water quality standards, criteria, and guidelines. Section 303(d) requires states to identify impaired off-Reservation water bodies, rank these impaired bodies based on severity of contamination and uses for the waters, and develop water quality management strategies, usually in the form of total maximum daily loads for the contaminant(s) of concern.
- Section 401 (Water Quality Certification) requires an applicant for any federal permit that proposes an activity that may result in a discharge to Waters of the U.S., to obtain certification from the USEPA for tribal land activities, or the state for off-Reservation activities, that the discharge will comply with other provisions of the CWA.
- Section 402 establishes the National Pollutant Discharge Elimination System (NPDES), a permitting system for the discharge of any pollutant (except for dredged or fill material) into Waters of the U.S. Each NPDES permit contains limits on concentrations of pollutants discharged to surface waters to prevent degradation of water quality and protect beneficial uses.

The Federal Antidegradation Policy was adopted as part of the 1972 amendments to the CWA. Federal policy (Code of Federal Regulations [CFR], Title 40, Part 131.12) specifies that each state must develop, adopt, and retain an anti-degradation policy to protect the minimum level of off-Reservation surface water quality necessary to support existing uses. Each state must also develop procedures to implement the anti-degradation policy through water quality management processes. Each state anti-degradation policy must include implementation methods consistent with the provisions outlined in 40 CFR § 131.12.

General NPDES Permit for Construction

In 1990, an amendment to the CWA directed the NPDES permitting program to address non-point source pollution from construction activities. Construction activities include clearing, grading, excavation, stockpiling, and reconstructing existing facilities involving removal and replacement of existing foundations or other hardscapes. Construction projects disturbing one or more acres of soil must be covered under the NPDES Construction General Permit process. For tribal projects on land held in trust by the federal government, the Tribe proposing the project must apply for coverage under the USEPA's NPDES Construction General Permit. Project proponents are required to submit to the USEPA a complete Notice of Intent (NOI) to comply with the permit. A complete NOI package consists of an NOI form, site map, and fee. The USEPA's NPDES Construction General Permit also requires the development and implementation of a SWPPP. The SWPPP contains a site map showing the construction site perimeter, existing and proposed buildings, lots and roadways, stormwater collection and discharge points, general topography both before and after construction, and drainage patterns across the site. The SWPPP must list Best Management Practices (BMP) that will be implemented during construction and operation to address stormwater runoff rates and quality. SWPPP BMPs include the following categories:

- Site planning considerations, such as preservation of existing vegetation;
- Vegetation stabilization through methods such as seeding and planting;
- Physical stabilization through use of dust control and stabilization measures;
- Diversion of runoff by utilizing earth dikes and temporary drains and swales;
- Velocity reduction through measures such as slope roughening/terracing; and
- Sediment trapping/filtering through use of silt fences, straw bales and sandbag filters, and sediment traps and basins.

Safe Drinking Water Act

Under the mandate of the Safe Drinking Water Act, the USEPA sets legally enforceable National Primary Drinking Water Regulations (primary standards) that apply to public water systems. These standards are established to protect human health by limiting the levels of contaminants in drinking water. The USEPA also defines National Secondary Drinking Water Regulations (secondary standards) for contaminants that cause cosmetic and aesthetic effects, but not for health effects. The USEPA recommends that these secondary standards be met but does not require systems to comply with them.

The USEPA does not oversee the construction and permitting of groundwater wells, but requires that public health standards, such as an effectively installed sanitary seal, are in place, and recommends that water systems be installed to meet California Department of Public Health Standards. The USEPA will also primarily establish monitoring and operational requirements, which will typically be specific to the project area.

Both primary and secondary drinking water standards are expressed as either Maximum Contaminant Levels, which define the highest level of a contaminant allowed in drinking water, or Maximum Contaminant Level Goals, which define the level of a contaminant below which there is no known or expected risk to health. Monitoring requirements typically include total coliform, nitrate, inorganic chemicals, volatile organic chemicals, non-volatile synthetic organic chemicals, secondary drinking water standard constituents, and general chemistry (including alkalinity, hardness, and minerals). The frequency of sampling varies and may be reduced over time.

Federal Emergency Management Agency

The Disaster Relief Act of 1974 as amended by the Robert T. Stafford Disaster Relief and Emergency Assistance Act of 1988 created the Federal Emergency Management Agency (FEMA), which is responsible for determining flood elevations and floodplain boundaries based on U.S. Army Corps of Engineers (USACE) studies. FEMA is also responsible for distributing Flood Insurance Rate Maps, which are used in the National Flood Insurance Program. These maps identify the locations of special flood hazard areas, including 100-year floodplains.

State and Local

Alaska State Statutes

The Alaska State Statutes contain several regulations relating to waters within the state and within three miles of shore. This includes protections of riparian areas, such as allowable uses. Alaska Statutes also protect drinking water sources by prohibiting actions that would impact groundwater wells. The statutes also contain an anti-degradation policy that identifies water quality maintenance actions.

Alaska Water Use Act (AS 46.15)

The Alaska Water Use Act governs the allocation and use of all surface and groundwater in the state. It designates water as a public resource belonging to Alaska's residents. The Act requires individuals and entities to obtain permits from the Alaska Department of Natural Resources (DNR) for any significant water use.

Municipality of Anchorage Municipal Code

Title 15 of the Municipality of Anchorage Municipal Code identifies the Municipality's environmental protection regulations, including watershed regulations and building codes. Title 21 regulates planning and zoning and provides standards for stormwater management planning and water pollution control, design standards for setbacks to surface waters and encroachments into such setbacks.

Clean Water Act Section 401

The state CGP applies to construction related activities within the state that disturb one or more acres of land, including larger projects that cumulatively disturb more than one acre of land. Project proponents must submit a Notice of Intent to obtain coverage under an Alaska Pollutant Discharge Elimination System CGP and must implement stormwater BMPs protective of water quality throughout the life of construction activities. A CGP SWPPP is also required as part of coverage under the CGP.

AIR QUALITY – EA SECTION 3.4

Regulatory Setting

Federal

Clean Air Act of 1970

The Clean Air Act (CAA; 42 USC Chapter 85) is the federal legislation for the protection of air quality. The CAA gives the USEPA authority to regulate air quality by promulgating standards and levels for air quality and enforcing those standards and levels on federal, state, and tribal land.

The CAA requires the USEPA to regulate hazardous air pollutants, which are those pollutants that are

known or suspected to cause cancer or other serious health effects, such as reproductive effects or birth defects, or adverse environmental effects. Certain air pollutants, either directly or in reaction with other pollutants, have been recognized to cause notable health problems and consequential damage to the environment due to elevated concentrations in the atmosphere. Such pollutants have been identified and regulated as part of the overall endeavor to prevent further deterioration and facilitate improvement in air quality.

The Federal CAA of 1970, as amended, establishes air quality standards for several critical air pollutants (CAPs): ozone (O₃), carbon monoxide (CO), particulate matter (PM), nitrogen dioxide (NO₂), sulfur dioxide (SO₂), and lead (Pb). These pollutants are termed “criteria” pollutants because the USEPA has established specific concentration threshold criteria based upon specific medical evidence of health effects or visibility reduction, soiling, nuisance, and other forms of damage. These National Ambient Air Quality Standards (NAAQS) are divided into primary standards and secondary standards. Primary standards are designed to protect the public health and secondary standards are intended to protect the public welfare from effects such as visibility reduction, soiling, nuisance, and other forms of damage. NAAQS and California Ambient air quality standards (CAAQS) are presented in **Table 1**.

Table 1: Ambient Air Quality Standards

Pollutant	Averaging Time	Standard (parts per million)		Standard (mcg per cubic meter)		Violation Criteria	
		Alaska AAQS	NAAQS	Alaska AAQS	NAAQS	Alaska AAQS	NAAQS
O ₃	8 hours	0.070	0.070	N/A	137	If 3-year average of the annual 4th high daily maximum exceeds standard	If exceeded on more than 3 days in 3 years
CO	8 hours	N/A	9	10,000	10,000	If exceeded on more than 1 day per year	If exceeded on more than 1 day per year
	1 hour	N/A	35	40,000	40,000	If exceeded on more than 1 day per year	If exceeded on more than 1 day per year
NO ₂	Annual average	N/A	0.053	100	100	If exceeded	If exceeded
	1 hour	N/A	0.100	188	188	If exceeded	N/A
SO ₂	Annual arithmetic mean	N/A	0.030	80	79	If exceeded	If exceeded
	24 hours	N/A	0.14	365	N/A	If exceeded on more than 1 day per year	If exceeded on more than 1 day per year
	1 hour (primary)	N/A	0.075	196	196	If 3-year average of the annual 99th-percentile exceeds standard	If 3-year average of the annual 99th-percentile exceeds standard
	3 hours (secondary)	N/A	0.5	1,300	N/A	If exceeded on more than 1 day per year	If exceeded on more than 1 day per year
PM ₁₀	24 hours	N/A	N/A	150	150	If exceeded on more than 1 day per year	If exceeded on more than 1 day per year
PM _{2.5}	Annual arithmetic mean (primary)	N/A	N/A	12	12	If 3-year average exceeds standards	If exceeded
	Annual arithmetic mean (secondary)	N/A	N/A	N/A	15	N/A	If exceeded
	24 hours	N/A	N/A	35	35	If 3-year average exceeds standards	If exceeded on more than 1 day per year
Lead	Rolling 3-month	N/A	N/A	0.15	0.15	If 3-month average exceeds	If exceeded

Pollutant	Averaging Time	Standard (parts per million)		Standard (mcg per cubic meter)		Violation Criteria	
		Alaska AAQS	NAAQS	Alaska AAQS	NAAQS	Alaska AAQS	NAAQS
	Avg.					standards	
Ammonia	8 hours	N/A	No Federal Standard	2100	No Federal Standard	If exceeded on more than 1 day per year	N/A

Source: ADEC, 2022; USEPA, 2024

Areas are designated attainment, nonattainment, or maintenance by the USEPA depending on whether the area is below or exceed the established NAAQS. Nonattainment areas must take steps towards attainment within a specific period of time. Once an area reaches attainment for particular criteria pollutant, then the area is re-designated attainment or maintenance. The CAA places most of the responsibility on states to achieve compliance with the NAAQS. States, municipal statistical areas, and counties that contain areas of nonattainment are required to develop a State Implementation Plan (SIP), which outlines policies and procedures designed to bring the state into compliance with the NAAQS.

Ozone

Photochemical reactions involving reactive organic gases (ROG)/volatile organic compounds (VOC) and nitrogen oxides (NO_x) resulting from the incomplete combustion of fossil fuels are the largest source of ground-level O₃. Because photochemical reaction rates depend on the intensity of ultraviolet light and air temperature, O₃ is primarily a summer air pollution problem. As a photochemical pollutant, O₃ is formed only during daylight hours under appropriate conditions. However, it is destroyed throughout the day and night. O₃ is considered a regional pollutant as the reactions forming it take place over time and are often most noticeable downwind from the sources of the emissions.

Particulate Matter 2.5

Particle pollution is a mixture of microscopic solids and liquid droplets suspended in air. This pollution, also known as PM_{2.5}, is made up of a number of components, including acids (such as nitrates and sulfates), organic chemicals, metals, soil or dust particles, and allergens (such as fragments of pollen or mold spores). The size of particles is directly linked to their potential for causing health problems. Particles smaller than 2.5 µm pose the greatest problems because they can be inhaled deep into the lungs. Exposure to such particles can affect respiratory system function.

Carbon Monoxide

CO is not readily dispersed throughout the atmosphere; therefore, it is considered a localized air quality issue as it is close to the emission source. CO emissions generally cause an acute (short-term) health threat. CO is a pollutant of concern at major signalized intersections (greater than 100,000 vehicles per day) that exhibit prolonged vehicle idling times.

Hazardous Air Pollutants

In addition to the above-listed CAPs, Hazardous Air Pollutants (HAP) are a group of chemical pollutants which can cause adverse effects to human health and/or the environment. Haps are also known as toxic air pollutants or air toxics. HAPs are a list of over 188 airborne chemicals developed by the USEPA. Sources of HAPs include industrial processes, such as petroleum refining and chrome plating operations;

commercial operations, such as gasoline stations and dry cleaners; cigarette smoke; and motor vehicle exhaust. Cars and trucks release at least 40 different HAPs. The most important, in terms of health risk, are diesel particulate matter (DPM), benzene, formaldehyde, 1,3-butadiene, and acetaldehyde. Health effects of HAPs can include cancer, birth defects, and neurological damage.

HAPs are less pervasive in the urban atmosphere than CAPs but are linked to short-term (acute) or long-term (chronic or carcinogenic) adverse human health effects. The majority of the estimated health risk from HAPs can be attributed to relatively few compounds. The most important HAPs are found in DPM. Diesel engines emit a complex mixture of air pollutants, composed of gaseous and solid material. Diesel exhaust contains a variety of harmful gases and over 40 other cancer-causing substances, and the visible emissions in diesel exhaust are PM that includes carbon particles or “soot.”

Exposure to DPM is a health hazard, particularly to children whose lungs are still developing and the elderly who may have other serious health problems.

Federal General Conformity

Under the General Conformity Rule, updated in 2010, the lead agency with respect to a federal action is required to demonstrate that the proposed federal action conforms to the applicable SIP before the action is taken. There are two phases to a demonstration of general conformity.

- The Conformity Review process, which entails an initial review of the federal action to assess whether a full conformity determination is necessary
- The Conformity Determination process, which requires that a proposed federal action be demonstrated to conform to the applicable SIP

The Conformity Review requires the lead agency to compare estimated emissions to the applicable general conformity levels (40 CFR 93.153 [b][1] and [2]), which these can be seen in **Table 2** and **Table 3**. If the emission estimates from step one is below the applicable threshold(s), then a general conformity determination is not necessary and the full Conformity Determination is not required. If emission estimates are greater than the applicable threshold(s), the lead agency must conduct a Conformity Determination.

Table 2: 40 CFR 93.153 [b][1] Emission Rates for Nonattainment Areas (NAAs)

Pollutant	Tons per Year
Ozone (VOC's or NOX)	
Serious NAA's	50
Severe NAA's	25
Extreme NAA's	10
Other ozone NAA's outside ozone transport region	100
Other ozone NAA's inside an ozone transport region	
VOC	50
NOx	100
Carbon Monoxide: all maintenance areas	100
SO ₂ or NO ₂ : All NAAs	100
PM ₁₀ :	
Moderate NAA's	100

Serious NAAs	70
PM2.5 (direct emissions, SO2, NOX, VOC, and Ammonia)	
Moderate NAA's	100
Serious NAAs	70
PD: all NAA's	25

Table 3: 40 CFR 93.153 [b][2] Emission Rates for Maintenance Areas

Pollutant	Tons per Year
Ozone (NOX), SO ₂ or NO ₂ :	
All maintenance areas	100
Ozone (VOC's)	
Maintenance areas inside an ozone transport region	50
Maintenance areas outside an ozone transport region	100
Carbon monoxide: All maintenance areas	100
PM ₁₀ : All maintenance areas	100
PM _{2.5} (direct emissions, SO ₂ , NOx, VOC, and Ammonia)	100
All maintenance areas	100
Pb: All maintenance areas	25

Federal Class I Areas

Title 1, Part C of the CAA was established in part to preserve, protect, and enhance the air quality in national parks, national wilderness areas, national monuments, national seashores, and other areas of special national or regional natural, recreational, scenic, or historic value. The CAA designates all international parks, national wilderness areas, and memorial parks larger than 5,000 acres and national parks larger than 6,000 acres as "Class I areas." The CAA prevents significant deterioration of air quality in Class I areas under the Prevention of Significant Deterioration (PSD) Program. The PSD Program protects Class I areas by allowing only a small increment of air quality deterioration in these areas by requiring assessment of potential impacts on air quality related values of Class I areas.

Any major source of emissions within 100 kilometers (62.1 miles) from a federal Class I area is required to conduct a pre-construction review of air quality impacts on the area(s). A "major source" for the PSD Program is defined as a facility that will emit (from direct stationary sources) 250 tons per year (tpy) of regulated pollutant. For certain industries, these requirements apply to facilities that emit (through direct stationary sources) 100 tpy or more of a regulated pollutant. Mobile sources (e.g., vehicle emissions) are by definition not stationary sources and are therefore not subject to the PSD program.

Tribal New Source Review

The Tribal Minor New Source Review (NSR) permitting program was established by the USEPA under the

CAA. The minor NSR program applies to both new minor sources and minor modifications to both major and minor projects in attainment and nonattainment areas. NSR programs must comply with the standards and control strategies of the Tribal Implementation Plan (TIP) or SIP. If there is not an applicable SIP or TIP, the USEPA issues permits and implements the program. A General Permit under the minor NSR program would be required on tribal land if stationary source allowable emissions of regulated pollutants would exceed the thresholds presented in 40 CFR 49.153, Table 1 (presented in **Table 4**). This General Permit serves as a preconstruction permit containing limitations and other restrictions specifying the construction, modification, and operation of a minor source. The applicability of Tribal NSR is made on a source's potential to emit (PTE). For emergency generators, the USEPA has determined that 500 hours per year should be assumed as a reasonable and realistic "worst-case" estimate on a PTE basis (ADEC, 2024).

Table 4: Tribal Minor New Source Review Thresholds

Pollutant	Emissions Thresholds for Nonattainment Areas (tpy)	Emissions Thresholds for Attainment Areas (tpy)
NO _x	5	10
ROG	2	5
PM	5	10
PM ₁₀	1	5
PM _{2.5}	0.6	3
CO	5	10
SO ₂	5	10
Pb	0.1	0.1

Source: 40 CFR 49.153.

Climate Change

Secretarial Order (SO) 3399

On February 19, 2021, Secretary of the Interior Deb Haaland issued Secretarial Order (SO) 3399 to prioritize action on climate change throughout the Department and to restore transparency and integrity in the Department's decision-making processes. SO 3399 specifies that when considering the impact of GHG emissions from a proposed action, Bureaus/Offices should use appropriate tools, methodologies, and resources available to quantify GHG emissions and compare GHG quantities across alternatives. SO 3399 acknowledges that identifying the interactions between climate change and the environmental impacts of a proposed action in NEPA documents can help decision makers identify opportunities to reduce GHG emissions, improve environmental outcomes, and contribute to protecting communities from the climate crisis.

NEPA Guidance on Consideration of GHG Emissions and Climate Change (2023)

On January 9, 2023, the Council on Environmental Quality issued National Environmental Policy Act Guidance on Consideration of Greenhouse Gas Emissions and Climate Change (88 Fed. Reg. 1196). This interim guidance directs agencies to consider the potential effects of a proposed action on climate change and the effects of climate change on a proposed action and its environmental impacts.

CEQ recommends that agencies quantify a proposed action's projected GHG emissions for the expected lifetime of the action and provide additional context for GHG emissions, including the use of the best

available social cost of GHG (SC–GHG) estimates, to translate climate impacts into the more accessible metric of dollars. This guidance does not propose a specific, quantitative threshold of significance; however, it states that agencies should consider the potential for mitigation measures to reduce or mitigate GHG emissions and climate change effects when those measures are reasonable and consistent with achieving the purpose and need for the proposed action. CEQ recommends that agencies explain how the proposed action and alternatives would help meet or detract from achieving relevant climate action goals and commitments, including federal goals, international agreements, state or regional goals, Tribal goals, agency-specific goals, or others as appropriate.

State and Local

Alaska Department of Environmental Conservation (ADEC)

ADEC is a state agency responsible for protecting Alaska's environment and public health through a range of regulatory and enforcement activities. Established in 1970, ADEC's primary mission is to ensure that Alaska's natural resources, including air, water, and land, are managed in a way that maintains their health and sustainability for current and future generations.

The ADEC Division of Air quality manages air quality in the State through a comprehensive program that includes monitoring, regulation, and enforcement. The department operates a network of air quality monitoring stations to measure pollutants like particulate matter, ozone, nitrogen dioxide, sulfur dioxide, and carbon monoxide. ADEC develops and implements the SIP, ensuring compliance with NAAQS established by the USEPA. It also issues permits for industrial and other pollution sources, sets emission limits, and enforces regulations to maintain air quality.

Alaska Administrative Code (AAC) Title 18, Chapter 50

AAC Title 18, Chapter 50, governs the State's air quality control regulations. This chapter establishes standards, permitting requirements, and enforcement mechanisms to manage and mitigate air pollution in Alaska. It covers a broad range of topics, including the classification of air quality areas, the establishment of emission limits for various pollutants, and the procedures for obtaining necessary permits for activities that might affect air quality. The regulations in this chapter also address compliance monitoring, reporting requirements, and the roles and responsibilities of state agencies in enforcing air quality standards. Additionally, Chapter 50 outlines specific measures to protect air quality in areas designated as nonattainment for certain pollutants, ensuring that industrial, commercial, and residential activities do not compromise the state's air quality objectives. Alaska's ambient air quality standards are summarized in **Table 1** above.

Alaska State Statutes 46.03 and 46.14

Title 46 of the Alaska State Statutes, which addresses Water, Air, Energy, and Environmental Conservation, includes several chapters dedicated to air quality management and control. Chapter 3 establishes the regulatory framework for environmental protection in Alaska, focusing on air and water quality. It grants the Alaska DEC the authority to set and enforce air quality standards, issue permits for pollution sources, and ensure regulatory compliance. The chapter details the DEC's responsibilities for monitoring air quality, inspecting facilities, and implementing pollution control measures. Chapter 14 further specifies the state's air quality management program, emphasizing the importance of clean air for public health and the

environment. It empowers the DEC to develop and enforce air quality regulations, issue permits, and manage both major and minor pollution sources, while also outlining procedures for compliance, monitoring, and enforcement.

Alaska State Implementation Plan (SIP)

The Alaska SIP is a comprehensive strategy required under the CAA to ensure that the state meets and maintains federal air quality standards. It outlines the measures and regulations Alaska will use to control emissions from various sources, including industrial facilities, transportation, and residential areas. The SIP includes detailed plans for monitoring air quality, setting emission limits, and enforcing compliance through state and local agencies, such as the Alaska DEC. It also emphasizes public involvement by providing opportunities for stakeholder input on air quality plans and regulations. For areas not meeting federal standards, the SIP provides strategies to achieve compliance and maintain air quality over time.

State of Alaska Priority Sustainable Energy Action Plan

The State of Alaska Priority Sustainable Energy Action Plan is a strategic framework aimed at guiding the state towards a sustainable and resilient energy future. It emphasizes the development and integration of renewable energy sources like wind, solar, hydroelectric, and biomass to reduce reliance on fossil fuels and leverage Alaska's natural resources. The plan prioritizes energy efficiency and conservation across residential, commercial, and public sectors by promoting energy-efficient technologies and practices. It also focuses on upgrading and modernizing energy infrastructure to enhance reliability and resilience, especially in remote areas. Economic and community development is a key aspect, with the plan aiming to create jobs, support local entrepreneurship, and provide affordable energy solutions. To facilitate these goals, the plan outlines the need for supportive policies and streamlined regulations, ensuring collaborative efforts among state agencies and stakeholders.

BIOLOGICAL RESOURCES – EA SECTION 3.5

Regulatory Setting

Federal

Federal Endangered Species Act

The Federal Endangered Species Act (FESA) protects species that are at risk of extinction and provides for the conservation of the ecosystems on which they depend. The U.S. Fish & Wildlife Service (USFWS) and the National Oceanic and Atmosphere Administration, Fisheries Service (NOAA Fisheries) share responsibility for implementing FESA. Generally, USFWS manages terrestrial and freshwater species, while NOAA Fisheries is responsible for marine and anadromous species. Section 9 (§ 1538) prohibits the "take" of a listed species by anyone, including private individuals and state and local agencies. Threatened and endangered species on the federal list (50 CFR Sections 17.11 and 17.12) are protected from take, which is defined as direct or indirect harm. If "take" of a listed species is incidental to an otherwise lawful activity, this triggers the need for consultation under Section 7 of the FESA for federal agencies.

Pursuant to the requirements of the FESA, a federal agency reviewing a proposed project within its jurisdiction must determine whether any federally listed species may be present on a proposed project site and whether a proposed project will have a potentially significant impact upon such species. Under the FESA, habitat loss is considered to be an impact to the species. In addition, the agency is required to determine whether a project is likely to jeopardize the continued existence of any species that is proposed

for listing under the FESA or to result in the destruction or adverse modification of critical habitat proposed to be designated for such species (16 USC Section 1536[3], [4]).

Migratory Bird Treaty Act

Migratory birds are protected under the federal Migratory Bird Treaty Act (MBTA) of 1918 (16 USC 703-711). The MBTA makes it unlawful to take, possess, buy, sell, purchase, or barter any migratory bird listed under 50 CFR 10, including feathers or other parts, nests, eggs, or products, except as allowed by implementing regulations (50 CFR 21). The direct injury or death of a migratory bird due to construction activities or other construction-related disturbance that causes nest abandonment, nestling abandonment, or forced fledging would be considered take under federal law. As such, project-related disturbances must be reduced or eliminated during the nesting season. The general nesting season extends from February 15 to July 15 for southeast Alaska in forest/woodland areas (USFWS, 2024c).

Bald and Golden Eagle Protection Act

The Bald and Golden Eagle Protection Act was originally enacted in 1940 to protect bald eagles and was later amended to include golden eagles (16 USC Subsection 668-668). This act prohibits take, possession, and commerce of bald and golden eagles and associated parts, feathers, nests, or eggs with limited exceptions. The definition of take is the same as the definition under the FESA.

Clean Water Act - Sections 404 and 401

Any project that involves discharge of dredged or fill material into jurisdictional Waters of the U.S. must first obtain authorization from the USACE, under Section 404 of the CWA. Projects requiring a 404 permit under the CWA also require a Section 401 certification from the USEPA for tribal land. The agency also administers the NPDES general permits for construction activities disturbing one acre or more.

Effective September 8, 2023, the USEPA and the USACE have issued a new final rule in the Code of Federal Regulations to conform the definition of ‘waters of the United States’ to the 2023 Supreme Court’s May 25, 2023 decision in Sackett vs. EPA. Under the new final rule, tributaries and wetlands must have a continuous surface connection to navigable waterways to be considered jurisdictional under the Clean Water Act. Only those relatively permanent, standing, or continuously flowing bodies of water meet the current definition.

In certain states where litigation regarding this definition is ongoing, the pre-2015 definition of waters of the U.S. is in effect. Alaska is one of these states and currently operates under the pre- 2015 regulatory regime (USACE, 2023).

Magnuson-Stevens Act and Sustainable Fisheries Act

The Magnuson–Stevens Fishery Conservation and Management Act (Magnuson-Stevens Act) is the primary law that governs marine fisheries management in U.S. federal waters. First passed in 1976, the Magnuson-Stevens Act fosters the long-term biological and economic sustainability of marine fisheries. Its objectives include: preventing overfishing; rebuilding overfished stocks; increasing long-term economic and social benefits; ensuring a safe and sustainable supply of seafood; and protecting habitat that fish need to spawn, breed, feed, and grow to maturity.

The Sustainable Fisheries Act of 1996 (Public Law 104-297) amended the Magnuson-Stevens Act to establish new requirements for fishery management councils to identify and describe Essential Fish Habitat (EFH) and to protect, conserve, and enhance EFH for the benefit of fisheries. EFH is defined as

those waters and substrate necessary to fish for spawning, breeding, feeding, or growth to maturity. The Sustainable Fisheries Act also established a federal EFH consultation process that advises federal agencies to avoid, minimize, mitigate, or otherwise offset adverse effects on EFH. Consultation is required if a federal agency has authorized, funded, or undertaken part or all of a proposed activity and the action will adversely affect EFH. An adverse effect includes direct or indirect physical, chemical, or biological alternations to waters or substrate, species and their habitat, quality and/or quantity of EFH, or other ecosystem components. If a federal agency determines that an action will not adversely affect EFH, and NOAA Fisheries agrees, no consultation is required. Fishery management councils can designate Habitat Areas of Particular Concern, specific areas within EFH that have extremely important ecological functions and/or are especially vulnerable to degradation.

Alaska National Interests Land Conservation Act (ANILCA)

Is a protected and regulated federal law under Title VIII. It gives priority to subsistence hunting, fishing and gathering for harvesting fish and wildlife by rural residents over recreational/sport and commercial users on federal land (BIA, 2024).

State and Local

Alaska Department of Fish and Game, Subsistence Regulations

The state manages subsistence use of fish and wildlife on State public lands and privately-held Tribal lands (including Alaska Native Corporation lands and Native allotments). Alaska State law (16.05.094) directs the Division of Subsistence to compile data and conduct studies on all aspects of subsistence hunting and fishing from residents of the State, evaluate impacts, make recommendations, and quantify amounts and nutritional value of the resource.

Alaska Forest Resources and Practices Act (FRPA)

This act governs how timber harvesting, reforestation, and timber access occur on state, private, and municipal land. Timber harvesting does not require a permit from ADF&G Habitat Section, but any activities related to commercial or personal use timber harvest that occur in fish-bearing waterbodies will require a fish habitat permit. The FRPA is designed to protect fish habitat and water quality and ensure prompt reforestation of forestland while providing for a healthy timber industry. The ADNR division of forestry is responsible for oversight of timber harvest under the FRPA (ADFG, 2024).

Alaska Wetland Program Plan

Under the Alaska Department of Environmental Conservation, this plan establishes a strategic statewide plan for assessing the state's wetlands, compiles science-based information to identify wetland functions and values, and develops a framework for identifying, evaluating, and implementing efficiencies in wetland regulatory programs within the state (ADEC, 2015).

Alaska Statute 16.20.190 Determining Endangered Species

Under the Alaska Department of Fish and Game. A species or subspecies of fish and wildlife is considered endangered when the commissioner of fish and game determines its numbers have decreased to such an extent as to indicate its continued existence is threatened. The following must be considered:

- Destruction, drastic modification, or severe curtailment of its habitat

- Overutilization for commercial or sporting purposes
- Effect on it of disease or predation
- Other natural or man-made factors affecting its continued existence.

Once a determination is made, the commissioner must publish a list of the species or subspecies of fish and wildlife that are endangered. Every two years reviews must be made to determine any changes in the status (AS 16.20.190).

State Wildlife Action Plan (SWAP)

The United States Congress created the State Wildlife Grants (SWG) program in 2000. The program provides critical funding to every state and territory to plan and implement proactive conservation actions to prevent the nation’s fish and wildlife from becoming endangered. Funding is provided to the states through the USFWS. Using funding, each state and territory develops a SWAP.

These plans conserve priority fish and wildlife species and their habitats, by identifying species with important conservation needs and offer a set of actions to address key threats. The plans also provide a voluntary, non-regulatory alternative to the federal listing process. The plan fulfills the 10-year revision requirement of the grant. This plan is an update to the original 2006 plan (ADFG, 2015).

CULTURAL AND PALEONTOLOGICAL RESOURCES – EA SECTION 3.6

Regulatory Setting

Federal

National Historic Preservation Act

Section 106 of the National Historic Preservation Act (NHPA), as amended, and its implementing regulations found in 36 CFR Part 800 require federal agencies to identify cultural resources that may be affected by actions involving federal lands, funds, or permitting. The BIA must comply with Section 106 for the proposed business lease approval. The significance of the resources must be evaluated using established criteria outlined in 36 CFR 60.4, as described below. If a resource is determined to be a historic property, Section 106 of the NHPA requires that effects of the federal undertaking on the resource be determined. A historic property is defined as:

...any prehistoric or historic district, site, building, structure or object included in, or eligible for inclusion in the National Register of Historic Places, including artifacts, records, and material remains related to such a property... (NHPA Sec. 301[5])

Section 106 of the NHPA prescribes specific criteria for determining whether a project would adversely affect a historic property, as defined in 36 CFR 800.5. An impact is considered adverse when prehistoric or historic archaeological sites, structures, or objects that are listed on or eligible for listing in the National Register of Historic Places (NRHP) are subjected to the following:

- Physical destruction of or damage to all or part of the property;
- Alteration of a property;

- Removal of the property from its historic location;
- Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance;
- Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property's significant historic features;
- Neglect of a property that causes its deterioration; and
- Transfer, lease, or sale of the property out of federal control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance.

If the historic property will be adversely affected by the undertaking, then prudent and feasible measures to resolve adverse impacts must be taken. The State Historic Preservation Office must be provided an opportunity to review and comment on these measures prior to project implementation.

National Register of Historic Places

The eligibility of a resource for listing in the NRHP is determined by evaluating the resource using criteria defined in 36 CFR § 60.4 as follows. The quality of significance in American history, architecture, archaeology, and culture is present in districts, sites, buildings, structures, and objects of state and local importance that possess integrity of location, design, setting, materials, workmanship, feeling, association, and:

- A) That are associated with events that have made a significant contribution to the broad patterns of our history;
- B) That are associated with the lives of persons significant in our past;
- C) That embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or
- D) That have yielded, or may be likely to yield, information important to prehistory or history.

Sites younger than 50 years, unless of exceptional importance, are not eligible for listing in the NRHP.

In addition to meeting at least one of the criteria listed above, the property must also retain enough integrity to enable it to convey its historic significance. The NRHP recognizes seven aspects or qualities that, in various combinations, define integrity. These seven elements of integrity are location, design, setting, materials, workmanship, feeling, and association. To retain integrity a property will always possess several, and usually most, of these aspects.

While most historic buildings and many historic archaeological properties are significant because of their association with important events, people, or styles (Criteria A, B, and C), the significance of most prehistoric and some historic-period archaeological properties is usually assessed under Criterion D. Criterion D stresses the importance of the information contained in an archaeological site rather than its intrinsic value as a surviving example of a type or its historical association with an important person or event. It places importance not on physical appearance but rather on information potential.

Native American Graves Protection and Repatriation Act

The Native American Graves Protection and Repatriation Act (NAGPRA), 25 USC 3001 et seq., provides a process for museums and federal agencies to return Native American cultural items – human remains, funerary objects, sacred objects, or objects of cultural patrimony – to lineal descendants, and culturally

affiliated Indian tribes and Native Hawaiian organizations. NAGPRA includes provisions for unclaimed and culturally unidentifiable Native American cultural items, intentional and inadvertent discovery of Native American cultural items on federal and Tribal lands, and penalties for noncompliance and illegal trafficking.

Archaeological Resources Protection Act of 1979

The Archaeological Resources Protection Act of 1979 (ARPA; Public Law 96-95; 16 USC 470aa-mm) provides for the protection of archaeological resources and sites that are on public and Indian lands and fosters increased cooperation and exchange of information between governmental authorities, the professional archaeological community, and private individuals having collections of archaeological resources and data that were obtained before October 31, 1979. ARPA also provides penalties for noncompliance and illegal trafficking.

Paleontological Resources Preservation Act

Paleontological resources are defined as the traces or remains of prehistoric plants and animals. Such remains often appear as fossilized or petrified skeletal matter, imprints, or endocasts, and reside in sedimentary rock layers. Paleontological resources are considered important for their scientific and educational value. Fossil remains of vertebrates are considered significant. Invertebrate fossils are considered significant if they function as index fossils. Index fossils are those that appear in the fossil record for a relatively short and known period of time. This allows geologists to interpret the age range of the geological formations in which they are found. The Paleontological Resources Preservation subtitle of the Omnibus Public Land Management Act, 16 USC 470aaa to aaa-11 requires the U.S. Department of Agriculture (USDA) and the U.S. Department of the Interior to issue implementation regulations to provide for the preservation, management, and protection of paleontological resources on federal lands and ensure that these resources are available for current and future generations to enjoy as part of America's national heritage.

State

Alaska Department of Natural Resources, Office of History and Archaeology

They serve as Alaska's State Historic Preservation Office. The office administers programs authorized by the NHPA and the Alaska Historic Preservation Act. They work with local governments, the public, and not for profit organizations to identify, preserve, protect, and interpret the state's cultural, historic, and archaeological resources.

Alaska Historic Preservation Plan

The plan intends to guide the activities and priorities of agencies and organizations involved in preservation throughout the state. The plan establishes ways the preservation community in Alaska can work to achieve common goals. Currently, this plan is being updated for the next ten years, 2025 to 2034 (OHA, 2018).

Alaska Historic Preservations Act of 1971

The Act serves to preserve and protect the historic, prehistoric, and archeological resources of Alaska from loss, desecration, and destruction so that the scientific, historic, and cultural heritage embodied in these resources may pass undiminished to future generations.

Environmental Setting

Prehistoric and Archaeological Overview

The prehistory of south-central Alaska is understood as the geographic region of Beringia, which stretches from Pacific Russia on the west through Alaska, and to northwestern Canada on the east. The earliest known archaeological sites in Alaska date back to 14,150 years before present (B.P.) from the Tanana River Valley, approximately 200 miles north of the Project Site. Subsequent to the earliest Diuktai tradition, hunters of the American Paleoarctic left behind two distinct tool assemblages known as the Nenana and Denali complexes, which continue to fuel inquiry about the Alaska's earliest inhabitants. Denali complexes are lithic assemblages that represent a variant of the American Paleoarctic tradition found in the interior of Alaska, which are dated 12,500 to 9,000 years B.P. Archaeological sites along Cook Inlet include sites at Beluga Point, Fish Creek, and Hewitt Lake which have the earliest occupation dates in the region, with a human presence dating between 10,000 and 7,500 B.P.

The Northern Archaic period, 6,000 to 3,000 B.P. reflects subsistence strategies in upland areas. Individuals moved upriver from the coast between 3,000 to 1,500 B.P. during the Kachemak tradition. The distinct signature of the Dena'ina has been documented in the Upper Cook Inlet Basin around 1,000 B.P. and into the Late Prehistoric period.

Dena'ina Cultural Overview

Traditionally, the Dena'ina people occupied the region surrounding Cook Inlet and spoke the Athabaskan language. The Project Site lies within the K'enaht'ana territory, the indigenous people of Nuti (Knik Arm) who are members of the Eklutna Native Village. The Dena'ina are originally from the interior, but integrated marine resources into their lifestyle upon arriving to Cook Inlet. They are the most populous of the Alaska Athabaskans, and their settlement patterns are seasonally dynamic. Eklutna was a primary village on the east Knik Arm. Families would spend winters there and then move through the landscapes along the Knik Arm during warmer months. The Dena'ina use subsistence practices including gathering, fishing, and hunting. They used birch and spruce trees for food, firewood, and raw material for snowshoes, boxes, sleds etc. Peeling of bark on birch and spruce trees results in culturally modified trees. These practices are occasionally reflected in the archaeological record.

Prior to the 20th century, three societies existed among the Dena'ina people. The Village of Eklutna was in the Susitna society. Each society was distinguished by social customs and subsistence. White settlement accelerated when Anchorage was founded in 1915, with the primary focus on the Alaska Railroad. Before WWII, two military bases were established in Dena'ina land which had an influx of new people to the region. The Tribe has been included on all lists of "Alaska Native entities recognized and eligible to receive services" that have been published by the Department of the Interior, beginning with the first such list published in 1982. Most recently, NVE was included in the January 8, 2024 list of Alaska tribal entities published at 89 Fed. Reg. 944, 947.

Historic Overview

During the early contact period, Danish navigators in Russian service explored the waters between Kamchatka Peninsula and North America, where they spotted land on July 15 and 16, 1741. In 1743, the earliest phase of Russian occupation was in Alaska, in which fur-trading became popular. James Cook enticed others to enter the fur-trade. His voyage spent weeks charting the coastline, interacting with the native people, and documenting the natural environment.

Russian presence in Alaska continued to grow throughout the late 18th century, with settlements spread throughout Alaska, which left a lasting legacy on the cultural and geopolitical landscape of the region. There weren't any settlements on the Knik Arm, but a trading relationship was established with the Dena'ina in the 1790s. By the mid-19th century, Russian influence in Cook Inlet began to lessen due to depletion of resources and increases competition with American and British traders. Alaska was transferred to the United States in 1867 via the Treaty of Cession, which marked the end of Russian claims in the area. The formal transfer of Alaska occurred on October 18, 1867. After the purchase, Alaska was under military rule, which lasted for a decade. The Klondike Gold Rush of 1896 increased the population and economic activity. Other economic resources besides gold included coal, fur trading, fishing and timber. The Alaska Railroad was completed in 1923 and changed the Cook Inlet region, allowing other industries more accessibility. Two military bases near Anchorage were established due to Europe at war in 1940 for use in the defense of North America. In 2010, those bases were consolidated and are now operated as Joint Base Elmendorf-Richardson.

The Glenn Highway was established as a strategic transportation route to improve military logistics and connectivity between Anchorage and the interior regions of the State.

By 1945, the majority of the road was completed and fostered post-war economic development and settlement in the region. In 1971, Eklutna, Inc. was established as part of the Alaska Native Claims Settlement Act. The native corporation represents one of 12 regional Alaska native corporations that were created to settle indigenous land claims and promote economic development. Eklutna, Inc. was granted title to approximately 90,000 acres of land in which the corporation was able to diversify their business practices while preserving cultural heritage and supporting the local native community.

Paleontological Resources

Paleontological resources are the fossilized remains of plants and animals, including vertebrates, invertebrates, and fossils of microscopic plants and animals (microfossils). The age and abundance of fossils depends on the location, topographic setting, and particular geologic formation in which they are found.

SOCIOECONOMIC CONDITIONS – EA SECTION 3.7

Regulatory Setting

Executive Order 12898

Executive Order 12898, *Federal Actions to Address Environmental Justice in Minority and Low-Income Populations*, as amended, directs federal agencies to develop an Environmental Justice Strategy that identifies and addresses disproportionately high and adverse human health or environmental effects of their programs, policies, and activities on minority populations and low-income populations. The CEQ has oversight responsibility of the federal government's compliance with EO 12898 and NEPA. The CEQ, in consultation with the USEPA and other agencies, has developed guidance to assist federal agencies with their NEPA procedures so that environmental justice concerns are effectively identified and addressed.

The document *Final Guidance for Incorporating Environmental Justice Concerns in EPA's NEPA Compliance Analyses* provides the following direction on how to analyze the impacts of actions on low-income and minority populations:

Under NEPA, the identification of a disproportionately high and adverse human health or environmental effect on a low-income population, minority population, or Indian tribe does not preclude a proposed agency action from going forward, nor does it necessarily compel a conclusion that a proposed action is environmentally unsatisfactory. Rather, the identification of such an effect should heighten agency attention to alternatives (including alternative sites), mitigation strategies, monitoring needs, and preferences expressed by the affected community or population. (USEPA, 1998)

As previously stated, according to guidance from the CEQ (1997) and USEPA (1998), agencies should consider the composition of the affected area, to determine whether minority populations, low-income populations, or Indian tribes are present in the area affected by a proposed action and, if so, whether there may be disproportionately high and adverse environmental effects to those populations. Communities may be considered “minority” under the executive order if one of the following characteristics apply.

- The cumulative percentage of minorities within a census tract is greater than 50 percent (primary method of analysis); or
- The cumulative percentage of minorities within a census tract is less than 50 percent, but the percentage of minorities is meaningfully greater than the minority population percentage in the general population or other appropriate unit of geographic analysis (secondary method of analysis).

According to USEPA, either the county or the state can be used when considering the scope of the “general population.” A definition of “meaningfully greater” is not given by the CEQ or USEPA, although the latter has noted that any affected area that has a percentage of minorities above the state’s percentage is a potential minority community and any affected area with a minority percentage double that of the state’s is a definite minority community under EO 12898. Communities may be considered “low-income” under the EO if one of the following characteristics applies.

- The median household income for a census tract is below the poverty line (primary method of analysis); or
- Other indications are present that indicate a low-income community is present within the census tract (secondary method of analysis).

Executive Order 14096

EO 14096, issued in April of 2023, amends certain provisions of EO 12898, and includes the following:

- Provides a broader definition of potentially disadvantaged communities.
- Explicitly expands definition of potentially disadvantaged communities to include persons with a Tribal affiliation and disabled persons;
- Requires Federal Agencies to fulfill environmental justice reporting requirements and prepare strategic plans; and
- Describes additional reporting and notification requirements related to toxic spills.

Environmental Setting Environmental Justice Screening Tools

The U.S. Federal Government has several tools that can be used to assess high-resolution environmental

and demographic information for locations in the U.S. and compare their selected locations to the rest of the state, USEPA region, or the nation. These tools can help identify areas with people of color and/or low-income populations, potential environmental quality issues, or a combination of environmental and demographic indicators that are greater than usual.

TRANSPORTATION/CIRCULATION – EA SECTION 3.8

Regulatory Setting

Federal

Department of Transportation (DOT)

The mission of the DOT is to ensure a fast, safe, efficient, accessible and convenient transportation system that meets national interests and enhances quality of life.

Organizations within the DOT include the Federal Highway Administration (FHWA), the Federal Aviation Administration, the National Highway Traffic Safety Administration, the Federal Transit Administration, the Federal Railroad Administration, and the Maritime Administration. The FHWA supports State and local governments in the design, construction, and maintenance of the Nation’s highway system (Federal Aid Highway Program) and various federally and tribal owned lands (Federal Lands Highway Program). Alaska Route 1 is a federal highway within the vicinity of the Project Site.

State and Local

Alaska Department of Transportation and Public Facilities (DOT&PF)

The DOT&PF is the principal agency in the state for the planning, construction, maintenance, and operation of the highway system. The DOT&PF is charged with carrying out a highway program that provides for a common defense to the United States and the state, a network of highways linking together cities and communities throughout the state (thereby contributing to the development of commerce and industry in the state and aiding the extraction and utilization of its resources), and otherwise improve the economic and general welfare of the people of the state.

Anchorage Metropolitan Area Transportation Solutions (AMATS)

AMATS is the designated metropolitan planning organization (MPO) that has overseen planning and programming of the Federal Highway Trust Fund dollars designated for the Anchorage Bowl, Chugiak, and Eagle River since 1976. The 2050 Metropolitan Transportation Plan (MTP) was released by AMATS in February 2024. The MTP is the primary tool used by AMATS to plan for long-range transportation needs and recommend solutions based on anticipated funding availability over a minimum 20-year horizon.

The MTP includes the whole transportation system: streets, sidewalks and pathways, trails, public transit, freeways, highways, and freight mobility. It is also required to address congestion management for a multimodal system and air quality standards and be based on land uses described in the current Comprehensive Plan and Land Use Plans for Anchorage and Chugiak-Eagle River.

LAND USE – EA SECTION 3.9

Regulatory Setting

Federal

Farmland Protection Policy Act

The Farmland Protection Policy Act (FPPA) is intended to minimize the impact federal programs have on the unnecessary and irreversible conversion of farmland to nonagricultural uses. It assures that federal programs are administered in a manner that is compatible with state and local units of government, and private programs and policies to protect farmland (7 U.S.C. § 4201). The Natural Resource Conservation Service (NRCS) is responsible for the implementation of the FPPA and categorizes farmland in a number of ways. These categories include prime farmland, farmland of statewide importance, and unique farmland. Prime farmland is considered to have the best possible features to sustain long-term productivity. Farmland of statewide importance includes farmland similar to prime farmland, but with minor shortcomings, such as greater slopes or less ability to store soil moisture. Unique farmland is characterized by inferior soils and, depending on climate, generally needs irrigation.

The NRCS fulfills the directives of the Soil and Water Conservation Act (16 USC § 2001-2009) by identifying significant areas of concern for the protection of national resources. NRCS uses a land evaluation and site assessment system to establish a Farmland Conversion Impact Rating (FCIR) score. The FCIR is completed on form AD-1006. The FCIR form has two components: land evaluation, which rates soil quality up to 100 points, and the site assessment, which measures other factors that affect the property's viability up to 160 points.

The total FCIR score is used as an indicator for the project's sponsor to consider alternative sites if the potential adverse impacts on the farmland exceed the allowable level; however, the FPPA does not require federal agencies to alter projects to avoid or minimize farmland conversion. Sites receiving a combined score of less than 160 (out of 260 possible points) do not require further evaluation. For sites with a combined score greater than 160 points, at least two other alternatives are required to be considered and the alternative with the lowest number of points selected unless there are other overriding considerations.

Federal Aviation Regulation

In accordance with 14 CFR 77, which provides requirements, standards, and processes for determining obstructions to air navigation, the Federal Aviation Administration's (FAA's) primary objective is to promote air safety and the efficient use of the navigable airspace. In furthering this mission, the FAA conducts aeronautical studies based on information provided on FAA Form 7460-1, Notice of Proposed Construction or Alteration, by proponents of construction or development in the vicinity of airports. Developers must file Form 7460-1 with the FAA at least 45 days prior to construction if any of the following parameters are met:

- Proposed structure(s) will exceed 200 feet above ground level;
- Proposed structure(s) will be in proximity to an airport and will exceed the slope ratio;
- Proposed structure(s) involves construction of a traverseway (i.e., highway, railroad, waterway, etc.) and once adjusted upward with the appropriate vertical distance would exceed a standard of 77.9(a) or (b);
- Proposed structure(s) will emit frequencies, and do/does not meet the conditions of the FAA Colocation Policy;
- Proposed structure(s) will be in an instrument approach area and might exceed part 77 Subpart

- C;
- Proposed structure(s) will be in proximity to a navigation facility and may impact the assurance of navigation signal reception;
- Proposed structure(s) will be on an airport or heliport; or
- Filing has been requested by the FAA.

State and Local

Alaska Department of Transportation and Public Facilities

Alaska's Department of Transportation and Public Services (DOT&PF) is responsible for the operation and maintenance of many state-owned airports and ensure these facilities meet safety, security, and operational standards. The DOT&PF develop and implement aviation plans that guide the growth and development of airports, including updates to the Alaska Aviation System Plan (AASP) which outlines priorities and infrastructure needs. The department ensures that airport operations comply with Federal Aviation Administration (FAA) regulations and standards and coordinate with the FAA on safety inspections, certification, and funding.

The DOT&PF works to enhance airport safety and security measures. This includes maintaining infrastructure, implementing safety programs, and ensuring compliance with federal security requirements. The department enforces airport zoning regulations to manage land use around airports. These regulations are designed to prevent incompatible land uses that could affect airport operations or safety. ADOT&PF collaborates with local governments and planning agencies to integrate airport needs into broader land use and development plans and work to ensure that local zoning ordinances are compatible with airport operations.

Alaska Department of Natural Resources

The Department of Natural Resources manages agricultural land through various programs, including leasing state-owned lands for farming and ranching. They oversee the allocation and use of these lands to ensure they are utilized effectively for agricultural purposes. The department supports agricultural development through initiatives that promote farming and ranching activities. They work to enhance the productivity of agricultural lands and may provide resources or assistance to farmers and ranchers. The Department of Natural Resources develops land use plans that identify suitable areas for agricultural development. These plans help to guide the location and expansion of agricultural activities while considering environmental and community impacts. Furthermore, they ensure that agricultural practices comply with state regulations and environmental standards. This includes monitoring and managing issues related to soil conservation, water use, and land reclamation.

Anchorage Municipal Code

The Anchorage Municipal Code designates land use through a combination of zoning regulations, land use plans, and specific ordinances. These regulations are intended to guide development, ensure compatibility between land uses, and protect community interests. The Code divides Anchorage into various zoning districts, each with specific land use regulations. These districts are designed to control the type and intensity of land uses in different areas of the city. Common zoning categories include residential, commercial, industrial, and mixed-use districts. Each zoning district specifies permitted and conditional uses. Permitted uses are those allowed by right within the district, while conditional uses require additional review and approval, often through a public hearing process. Chapter 10 of the Municipal Code provides the standards and regulations for zoning and development in the Chugiak-Eagle River area where

the Project Site is located.

Anchorage 2040 Land Use Plan

The Anchorage 2040 Land Use Plan is a comprehensive framework designed to guide the growth and development in Anchorage, Alaska. It addresses land use by outlining strategies to accommodate the city's projected population increase while preserving its unique character and natural environment. The plan emphasizes sustainable development, promoting a mix of residential, commercial, and recreational spaces to foster vibrant, walkable neighborhoods. It incorporates guidelines for efficient land use, such as encouraging infill development and revitalizing underutilized areas, to reduce urban sprawl. Additionally, the plan addresses infrastructure needs, transportation connectivity, and environmental conservation, aiming to create a balanced and resilient urban ecosystem that aims to foster a resilient urban environment that supports both community well-being and economic growth.

Municipality of Anchorage Ordinance No. 2020-137

The Municipality of Anchorage Ordinance No. 2020-137, enacted in December 2020, includes key provisions that directly influence the Eklutna area through the adjustments outlined in sections 1.70.010 and 1.70.020 of the Anchorage Municipal Code.

Section 1.70.010 establishes the framework for updating land use policies to better align with the city's evolving housing needs, which includes addressing development challenges in the Eklutna region. Section 1.70.020 specifically addresses the designation and utilization of land for various purposes, paving the way for increased residential density and infrastructure improvements in Eklutna. These sections aim to facilitate more effective and sustainable land use in Eklutna, promoting development that supports both affordable housing goals and broader regional growth objectives. By revising these regulatory frameworks, Ordinance No. 2020-137 helps integrate Eklutna into the city's comprehensive growth strategy, ensuring that the area can contribute to and benefit from Anchorage's future development.

Chugiak River Comprehensive Update 2006

The Chugiak River Comprehensive Plan is a strategic document designed to guide the development and preservation of the Chugiak River area, focusing on sustainable land use practices. This plan outlines a vision for balancing growth with environmental stewardship, emphasizing the need to protect natural resources while accommodating community needs.

Birchwood Community Council Bylaws

The Birchwood Community Council Bylaws serve as the governing framework for the Birchwood Community Council, outlining the structure, responsibilities, and operational procedures of the council. These bylaws establish the council's mission to represent and advocate for the interests of the Birchwood community, ensuring effective local governance and community engagement.

Birchwood Airport Master Plan

The Birchwood Airport Master Plan outlines how land adjacent to the airport will be utilized to support aviation operations while balancing the needs of the community and environmental considerations. The plan addresses how different land uses—such as commercial, industrial, and recreational—can coexist with airport activities, ensuring that growth in these areas does not negatively impact airport functions or nearby residential neighborhoods.

Environmental Setting

Surrounding Land Uses and Zoning

The Project Site is located within the unincorporated community of Chugiak within the municipality of Anchorage and is part of the Birchwood Community Council. The Project Site is zoned and designated as light-industrial and low-density residential in the Anchorage Municipal Code. The generalized zoning district for light industrial is intended for private and public industrial uses including manufacturing, storage, wholesale, and distribution operation, as well as commercial uses that support and/or are compatible with industrial uses (Municipality of Anchorage, 2017). The generalized zoning district for low density residential is intended for residential development with up to one dwelling per acre, while protecting physical and environmental features (Municipality of Anchorage, 2017). The Project Site is included in the Chugiak-Eagle River Comprehensive Plan Update revised December 2006 (Comprehensive Plan). The Project Site was not included in the Anchorage 2040 Land Use Plan. The Project Site is within the boundaries of the Birchwood Community Council, a non-profit, voluntary, self-governing association. The Comprehensive Plan identifies vacant areas as unsuitable, marginally suitable, or suitable for development, with the Project Site categorized as suitable for development. The goal of economic development in the Comprehensive Plan is to promote economic growth that utilizes the area's resources while supporting urban, suburban, and rural lifestyles and providing employment opportunities and a variety of goods and services (Comprehensive Plan, 2006).

The Project Site is bordered by Birchwood Spur Road to the west, the Alaska Railroad to the north, residential properties to the east, and commercial properties to the south. Land zoned to the west of the Project Site is zoned for heavy industrial use and contains warehouses and commercial developments. The Alaska Railroad borders the northern portion of the Project Site. Land to the north is zoned for light industrial uses and contains vacant land. Birchwood Airport is located to the northwest of the Project Site and discussed further below. Land to the east of the Project Site is zoned for low density residential and contains residential and vacant land. Land to the southwest is zoned for light industrial and contains commercial developments, and land to the southeast is zoned for low density residential and contains single family homes.

The nearest airport is Birchwood Airport located approximately 750-feet northwest of the Project Site. The airport is within the jurisdiction of the Anchorage Department of Transportation, which manages municipal airports and transportation infrastructure in the area. The Project Site is within an area of height restriction as delineated by the airport height zoning map prepared February 8, 1984 by the State's Department of Transportation and Public Facilities and adopted for the Municipality of Anchorage designations. The Municipality establishes airport height limitations to be no greater than thirty-five feet above ground elevation as outlined in Title 21, Chapter 21.65.

The Project Site is bordered to the north by the Alaska Railroad, which runs in an east-west direction. The Alaska Railroad is a state-owned railroad system that provides freight and passenger services. The railroad specializes in freight services related to lumber, heavy machinery, rebar, pipe, and hazardous material (Alaska Railroad, 2020). A 1.68-acre easement for the railroad occurs along the northern portion of the Project Site. This easement allows railroad personnel to access the area for construction, operation, and maintenance activities.

Agriculture

The U.S. Department of Agriculture (USDA) conducts a state-by-state census of agriculture every five

years. The National Agriculture Statistical Service collects census data from a list of all known potential agriculture operators. The census reports on various statistics relating to crop yields, farm acreage, and farm economics. According to the 2022 Census of Agriculture, a total of 39,532 acres in the Anchorage area are used for farming purposes (USDA, 2022).

PUBLIC SERVICES AND UTILITIES – EA SECTION 3.10

Regulatory Setting

Federal

Safe Drinking Water Act and Clean Water Act

See **Water Resources – Section 3.3 of the EA** above.

Public Law 280

Public Law 280 was enacted in 1953 to grant certain states criminal jurisdiction over Indians on reservations in addition to permitting civil litigation under tribal or federal court jurisdiction to be handled by state courts.

The states mandated to assume criminal and civil jurisdiction over federal Indian lands are Alaska, California, Minnesota, Nebraska, Oregon, and Wisconsin, although certain tribal lands are exempt, including Metlakatla Indian Community on the Annette Island Reserve, Red Lake Reservation, and Warm Springs Reservation. In addition to these states, other states elected to assume full or partial responsibility, including Arizona, Florida, Idaho, Iowa, Montana, Nevada, North Dakota and Utah. The federal government relinquished all special criminal jurisdictions over Indian offenders and victims in these states. However, Public Law 280 does not grant states the following regulatory powers over lands held in federal trust or tribes:

- Federally guaranteed fishing, tribal hunting, and trapping rights;
- Fundamental tribal governmental functions, such as domestic relations and tribal enrollment; and
- Authority to impose state taxes.

Due to the one-sided process that imposed state jurisdiction on tribes and the failure to recognize tribal sovereignty and tribal self-determination, Public Law 280 was opposed by Indian Nations from its enactment. Subsequent acts of Congress, court decisions, and state actions to retrocede (or give back) jurisdiction back to the federal government have mitigated some of the effects of the 1953 law and strengthened tribes' jurisdiction over civil and criminal matters on their reservations.

State and Local

Alaska State Regulations

Alaska State Regulations contains several regulations relating to utilities and public services. AS 42 houses regulations related to utilities, including cost for service, prohibition against acts damaging utilities, relocation of utilities to accommodate development, and establishment of utility boards. AS 13 AAC 85 houses regulations related to law enforcement, including basic standards for police officers and training

requirements. Several state regulations are also applicable to emergency medical services, including emergency care service requirements, personnel training and facility licensing requirements, medical transport standards, and others.

Municipality of Anchorage Municipal Code

The Municipality of Anchorage Municipal Code contains several regulations relating to utilities and public services. Title 26 houses utility-related codes, including regulations related to utility costs, dedication of easements, inspections, construction coordination, and general rules and provisions. Title 29 specifically identifies regulations related to public schools. Additionally, Title 16 identifies regulations related to emergency response, including designation of emergency response vehicles and responsibilities of municipal emergency medical service personnel (Chapter 16.95). Several regulations exist related to law enforcement personnel, including fees for police services and general authorities of emergency response personnel.

NOISE – EA SECTION 3.11

Regulatory Setting

Federal Highway Administration Construction Noise Abatement Criteria

The Federal Highway Administration (FHWA) Construction Noise Handbook (2006) provides guidance with respect to the development of construction noise level thresholds. Based on that guidance and estimated ambient noise levels in the Project Site vicinity, the criteria in **Table 5** were developed for use in evaluating the significance of construction noise impacts.

Table 5: Federal Construction Noise Thresholds

Noise Receptor Locations and Land Uses	Daytime (7 am – 6 pm)	Nighttime (10 pm – 7 am)
Noise-Sensitive Locations (residences, institutions, hotels, etc.)	90 L _{max}	80 L _{max}
Commercial Areas (businesses, offices, stores, etc.)	None	None
Industrial Areas (factories, plants, etc.)	None	None

Source: FHWA, 2006

Federal Noise Abatement Criteria

23 CFR 772 establishes Noise Abatement Criteria (NAC) for various land uses that have been categorized based upon activity. The FHWA NAC is based on noise generated from peak traffic hour noise levels, and land uses are categorized based on their sensitivity to noise as indicated in **Table 6**. Sensitive receptors with the potential to be impacted by the project alternatives primarily consist of residential land uses; thus, the Category B noise standard (67 dBA L_{eq}) would apply to those uses.

Vibration Standards

The effects of groundborne vibrations typically cause only a nuisance to people, but at extreme vibration levels, damage to buildings may occur. Although groundborne vibration can be felt outdoors, it is typically an annoyance only indoors, where the associated effects of the building shaking can be notable. Groundborne noise is an effect of groundborne vibration and only exists indoors since it is produced from noise radiated from the motion of the walls and floors of a room and may consist of the rattling of windows or dishes on shelves.

Peak particle velocity (PPV) is often used to measure vibration. PPV is the maximum instantaneous peak (inches per second) of the vibration signal. The PPV levels are used to estimate L_v or VdB levels (vibration decibels with a reference velocity of one micro-inch per second). Scientific studies have shown that human responses to vibration vary by the source of vibration, which is either continuous or transient. Continuous sources of vibration include construction while transient sources include truck movements. Generally, the thresholds of perception and annoyance are higher for transient sources than for continuous sources. **Table 7** summarizes the Federal Transit Administration’s (FTA) guideline vibration damage criteria for various structural categories. As shown therein, buildings extremely susceptible to vibration damage could be damaged if vibration levels exceed 90 VdB. Additionally, although humans have a perceptibility threshold of 65 VdB, the threshold for irritation does not occur until approximately 80 VdB (FTA, 2018).

Table 6: Federal Noise Abatement Criteria Hourly A-Weighted Sound Level Decibels¹

Activity Category	Activity Criteria Leq (h), dBA	Evaluation Location	Activity Category Description
A	57	Exterior	Lands on which serenity and quiet are of extraordinary significance and serve an important public need and where the preservation of those qualities is essential if the area is to continue to serve its intended purpose.
B	67	Exterior	Residential.
C	67	Exterior	Active sport areas, amphitheatres, auditoriums, campgrounds, cemeteries, daycare centers, hospitals, libraries, medical facilities, parks, picnic areas, places of worship, playgrounds, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, recreation areas, Section 4(f) sites, schools, television studios, trails and trail crossings.
D	52	Interior	Auditoriums, daycare centers, hospitals, libraries, medical facilities, places of worship, public meeting rooms, public or non-profit institutional structures, radio studios, recording studios, schools, and television studios.
E ¹	72	Exterior	Hotels, motels, offices, restaurants/bars, and other developed lands, properties or activities not included in A–D or F.
F	–	–	Agriculture, airports, bus yards, emergency services, industrial, logging, maintenance facilities, shipyards, utilities (water resources, water treatment, electricity), and warehousing.
G	–	–	Undeveloped lands that are not permitted.

Source: 23 CFR 772

¹ Includes undeveloped lands permitted for this activity category.

Table 7: Construction Vibration Damage Criteria

Building Category	Approximate PPV (in/sec)	Approximate Lv (VdB)
Reinforced-concrete, steel, or timber (no plaster)	0.5	102
Engineered concrete and masonry (no plaster)	0.3	98
Non-engineered timber and masonry buildings	0.2	94
Buildings extremely susceptible to vibration damage	0.12	90

Source: FTA, 2018

State and Local

Anchorage Municipal Noise Ordinance

The primary purpose of the Chapter 15.70 Noise Control ordinance is to protect public health and welfare by regulating and minimizing excessive noise and vibration within the municipality. It establishes detailed noise level limits for various settings (residential, commercial, industrial) and times of the day to prevent noise disturbances and protect public health and quality of life. It also includes provisions for different sources of noise such as construction sites, motor vehicles, and public activities, outlining permissible noise levels and operational restrictions during specified times. Examples of key sections are provided below:

- 15.70.060 Prohibited acts and conditions.
 - 3. Construction. No person shall operate or cause the operation of any tools or equipment used in construction, drilling, repair, alteration, or demolition.
 - a) So that the sound therefrom creates a noise disturbance across a residential real property boundary or within a noise-sensitive zone between the hours of 10:00 pm and 6:00 am during the construction season, between the hours of 10:00 pm and 7:00 am during other months, or at any time on Sundays or state holidays;
 - b) So that the sound level therefrom exceeds an Leq of 80 dB(A) at or within a residential real property boundary or within a noises sensitive zone during any one hour of the daily period from 6:00 am to 10:00 pm during the construction season or from 7:00 am to 10:00 pm during other months; or
 - c) So that the sound level therefrom exceeds an Leq of 80 dB(A) at or within a commercial or industrial real property boundary during any one hour of the daily period from 6:00 am to 10:00 pm during the construction season or from 7:00 am to 10:00 pm during other months.
 - 7. Loading and unloading. No person shall load, unload, open, close or otherwise handle boxes, crates, containers, building materials, garbage cans or similar objects between the hours of 10:00 pm and 7:00 am the following day in such a manner as to cause a noise disturbance across a residential real property boundary or within a noise-sensitive zone.
 - 11. Noise-sensitive zones. No person shall create or cause the creation of any sound within any noise-sensitive zone designated pursuant to Section 15.70.040.A.7 so as to disrupt the activities normally conducted within the zone, provided that conspicuous signs are displayed indicating the presence of the zone. No person shall create or cause the creation of any sound within any noise-sensitive zone designated pursuant to Section 15.70.040.A.7 containing a hospital, nursing home or similar activity so as to interfere with the functions of such activity or disturb or annoy the patients in the activity, provided that conspicuous signs are displayed indicating the presence of the zone.

- 19. Vibration. No person shall operate or cause the operation of any device that produces low-frequency, inaudible sound that creates vibration above the vibration perception threshold of any individual within a residential real property boundary or within a noise-sensitive zone between the hours of 10:00 pm and 7:00 am the following day.
- 15.70.080 Property line noise emission standards.
 - A. Except as otherwise provided in subsections B and C of this section, no person shall operate or cause to be operated on private property any source of sound in such a manner as to create a sound level that exceeds limits set forth for the receiving land use category in **Table 8** when measured at or within the property boundary of the receiving land use.
 - C. The provisions of subsections A and B of this section shall not apply to the unamplified human voice and activities covered by Sections 15.70.060.B.1, pertaining to aircraft and airport operations, 15.70.060.B.3, pertaining to construction, 15.70.060.B.4, pertaining to domestic power tools, 15.70.060.B.5, pertaining to emergency signaling devices, 15.70.060.B.6, pertaining to explosives, firearms and similar devices, 15.70.060.B.9, pertaining to motorboats, 15.70.060.B.14, pertaining to public service utilities, 15.70.060.B.16, pertaining to stationary nonemergency signaling devices, 15.70.090.A, pertaining to motor vehicles operating on public rights-of-way, 15.70.090.D, pertaining to refuse collection vehicles, and 15.70.090.E, pertaining to snow removal vehicles on public rights-of-way.

Table 8: Sound Levels by Receiving Land Use

Receiving Land Use Category	Time	Sound Level Limit (dB(A))
Residential area	7:00 am--10:00 pm	60
	10:00 pm-- 7:00 am	50
Commercial area	7:00 am--10:00 pm	70
	10:00 pm-- 7:00 am	60
Industrial area	At all times	80

Environmental Setting

Acoustical Background and Terminology

Acoustics is the science of sound. Sound may be thought of as mechanical energy of a vibrating object transmitted by pressure waves through a medium to human (or animal) ears. If the pressure variations occur frequently enough (at least 20 times per second), then they can be heard and are called sound. The number of pressure variations per second is called the frequency of sound and is expressed as cycles per second or Hertz (Hz).

Noise is a subjective reaction to different types of sounds. Noise is typically defined as (airborne) sound that is loud, unpleasant, unexpected or undesired, and may therefore be classified as a more specific group of sounds. Perceptions of sound and noise are highly subjective from person to person.

Measuring sound directly in terms of pressure would require a very large and awkward range of numbers. To avoid this, the decibel scale was devised. The decibel scale uses the hearing threshold (20 micropascals), as a point of reference, defined as 0 dB. Other sound pressures are then compared to this

reference pressure, and the logarithm is taken to keep the numbers in a practical range. The decibel scale allows a million-fold increase in pressure to be expressed as 120 dB, and changes in levels (dB) correspond closely to human perception of relative loudness.

The perceived loudness of sounds is dependent upon many factors, including sound pressure level and frequency content. However, within the usual range of environmental noise levels, perception of loudness is relatively predictable, and can be approximated by A-weighted sound levels. There is a strong correlation between A-weighted sound levels (expressed as dBA) and the way the human ear perceives sound. For this reason, the A-weighted sound level has become the standard tool of environmental noise assessment.

The decibel scale is logarithmic, not linear. In other words, two sound levels 10-dB apart differ in acoustic energy by a factor of 10. When the standard logarithmic decibel is A-weighted, an increase of 10-dBA is generally perceived as a doubling in loudness. For example, a 70-dBA sound is half as loud as an 80-dBA sound, and twice as loud as a 60-dBA sound.

Community noise is commonly described in terms of the ambient noise level, which is defined as the all-encompassing noise level associated with a given environment. A common statistical tool is the average, or equivalent, sound level (L_{eq}), which corresponds to a steady-state A-weighted sound level containing the same total energy as a time varying signal over a given time period (usually one hour). The L_{eq} is the foundation of the composite noise descriptor, L_{dn} , and shows very good correlation with community response to noise.

The day/night average level (DNL or L_{dn}) is based upon the average noise level over a 24-hour day, with a +10-decibel weighing applied to noise occurring during nighttime (10:00 pm to 7:00 am) hours. The nighttime penalty is based upon the assumption that people react to nighttime noise exposures as though they were twice as loud as daytime exposures. Because L_{dn} represents a 24-hour average, it tends to disguise short-term variations in the noise environment.

Table 9 lists several examples of the noise levels associated with common situations.

Table 9: Typical Noise Levels

Common Outdoor Activities	Noise Level (dBA)	Common Indoor Activities
	110	Rock Band
Jet Fly-over at 300 m (1,000 ft.)	100	
Gas Lawn Mower at 1 m (3 ft.)	90	
Diesel Truck at 15 m (50 ft.), at 80 km/hr. (50 mph)	80	Food Blender at 1 m (3 ft.) Garbage Disposal at 1 m (3 ft.)
Noisy Urban Area, Daytime Gas Lawn Mower, 30 m (100 ft.)	70	Vacuum Cleaner at 3 m (10 ft.)
Commercial Area Heavy Traffic at 90 m (300 ft.)	60	Normal Speech at 1 m (3 ft.)
Quiet Urban Daytime	50	Large Business Office Dishwasher in Next Room
Quiet Urban Nighttime	40	Theater, Large Conference Room (Background)
Quiet Suburban Nighttime	30	Library
Quiet Rural Nighttime	20	Bedroom at Night, Concert Hall (Background)
	10	Broadcast/Recording Studio
Lowest Threshold of Human Hearing	0	Lowest Threshold of Human Hearing

The effects of noise on people can be placed into three categories:

- Subjective effects of annoyance, nuisance, dissatisfaction
- Interference with activities such as speech, sleep, and learning
- Physiological effects such as hearing loss or sudden startling

Environmental noise typically produces effects in the first two categories. Workers in industrial plants can experience noise in the last category. There is no completely satisfactory way to measure the subjective effects of noise or the corresponding reactions of annoyance and dissatisfaction. A wide variation in individual thresholds of annoyance exists and different tolerances to noise tend to develop based on an individual's past experiences with noise. Thus, an important way of predicting a human reaction to a new noise environment is the way it compares to the existing environment to which one has adapted: the so-called ambient noise level. In general, the more a new noise exceeds the previously existing ambient noise level, the less acceptable the new noise will be judged by those hearing it. With regards to increases in A-weighted noise level, the following relationships occur:

- Except in carefully controlled laboratory experiments, a change of 1-dBA cannot be perceived;
- Outside of the laboratory, a 3-dBA change is considered a just-perceivable difference;
- A change in level of at least 5-dBA is required before any noticeable change in human response would be expected; and
- A 10-dBA change is subjectively heard as approximately a doubling in loudness and can cause an adverse response.

Stationary point sources of noise – including stationary mobile sources such as idling vehicles – attenuate (lessen) at a rate of approximately 6-dB per doubling of distance from the source, depending on environmental conditions (i.e. atmospheric conditions and either vegetative or manufactured noise barriers, etc.). Widely distributed noises, such as a large industrial facility spread over many acres or a street with moving vehicles, would typically attenuate at a lower rate.

Vibration Background and Terminology

Vibration is like noise in that it involves a source, a transmission path, and a receiver. While vibration is related to noise, it differs in that noise is generally considered to be pressure waves transmitted through air, whereas vibration usually consists of the excitation of a structure or surface. As with noise, vibration consists of an amplitude and frequency. A person's perception to the vibration will depend on their individual sensitivity to vibration, as well as the amplitude and frequency of the source and the response of the system which is vibrating. Vibration can be measured in terms of acceleration, velocity, or displacement. A common practice is to monitor vibration measures in terms of peak particle velocities in inches per second. Standards pertaining to perception as well as damage to structures have been developed for vibration levels defined in terms of peak particle velocities. Human and structural response to different vibration levels is influenced by a number of factors, including ground type, distance between source and receptor, duration, and the number of perceived vibration events. Table 10 shows the vibration levels which would normally be required to result in damage to structures. The vibration levels are presented in terms of peak particle velocity in inches per second. The threshold for architectural damage to structures is 0.20 in/sec p.p.v. A threshold of 0.20 in/sec p.p.v. is considered to be a reasonable threshold for short-term construction projects.

Table 10: Effects of Vibration on People and Buildings

Peak Particle Velocity (mm/second)	Peak Particle Velocity (in/second)	Human Reaction	Effect on Buildings
0.15 – 0.20	0.006 – 0.019	Threshold of perception; possibility of intrusion	Vibrations unlikely to cause damage of any type
2.0	0.08	Vibrations readily perceptible	Recommended upper level of the vibration to which ruins and ancient monuments should be subjected
2.5	0.10	Level at which continuous vibrations begin to annoy people	Virtually no risk of “architectural” damage to normal buildings
5.0	0.20	Vibrations annoying to people in buildings (this agrees with the levels established for people standing on bridges and subjected to relative short periods of vibrations)	Threshold at which there is a risk of “architectural” damage to normal dwelling-houses with plastered walls and ceilings. Special types of finish such as lining of walls, flexible ceiling treatment, etc., would minimize “architectural” damage
10 - 15	0.4 – 0.6	Vibrations considered unpleasant by people subjected to continuous vibrations and unacceptable to some people walking on bridges	Vibrations at a greater level than normally expected from traffic, but would cause “architectural” damage and possibly minor structural damage

Source: Transportation Related Earthborne Vibrations. Caltrans. TAV-02-01-R9601. February 20, 2002

Sensitive Receptors

Some land uses are considered more sensitive to noise than others due to the amount of noise exposure (in terms of both exposure duration and insulation from noise) and the types of activities typically involved. Residences, schools, libraries, hospitals, and passive recreation areas generally are more sensitive to noise than commercial or industrial land uses. Sensitive noise receptors may also include threatened or endangered noise-sensitive biological species. Noise sensitive land uses are typically given special attention in order to achieve protection from excessive noise.

HAZARDOUS MATERIALS AND HAZARDS – EA SECTION

3.12

Regulatory Setting

Federal

Resource Conservation and Recovery Act

The Resource Conservation and Recovery Act (RCRA) regulates the land disposal of hazardous materials from cradle-to-grave. This means establishing a regulatory framework for the generation, transport, treatment, storage and disposal of hazardous waste. Specifically, Subtitle D of RCRA pertains to non-hazardous solid waste and Subtitle C focuses on hazardous solid waste. A solid waste can consist of solids, liquids and gases, but these must be discarded in order to be considered waste. Additionally, the USEPA has developed regulations to set minimum national technical standards for how disposal facilities should be designed and operated. States issue permits to ensure compliance with USEPA and state regulations. The regulated community is comprised of a diverse group that must comprehend and adhere to RCRA

regulations. These groups can consist of hazardous waste generators, government agencies, small businesses, and gas stations with underground petroleum tanks.

Food, Drug, and Cosmetic Act

Under the federal Food, Drug, and Cosmetic Act, the USEPA sets maximum residue limits, or tolerances, for pesticides residues on food. When the USEPA sets a tolerance level for a food, this is the level deemed safe. In defining safe, this means that, “reasonable certainty that no harm will result from aggregate exposure to the pesticide residue.” When determining a safety finding for a tolerance level, the USEPA considers the toxicity of the pesticide and its break-down products, aggregate exposure to the pesticide in foods and from other sources of exposure if applicable, and any special risks specific to infants and children. If a tolerance is not set for a pesticide residue, a food containing that pesticide residue will be subject to government seizure if deemed appropriate. However, once a tolerance has been established for a pesticide residue, then residue levels below the tolerance will not trigger enforcement actions. If the residue level is detected above that tolerance, then the commodity will be subject to seizure. Some pesticides do not have a set tolerance level as the USEPA may grant exemptions in the cases where the pesticide residue does not pose, under foreseeable situations, a significant dietary risk.

Insecticide, Fungicide, and Rodenticide Act

The federal Insecticide, Fungicide, and Rodenticide Act (FIFRA) addresses the sale, distribution, and labeling of pesticides, as well as the certification and training of pesticide applicators. FIFRA establishes recordkeeping and reporting requirements on certified applicators of restricted use pesticides.

Furthermore, FIFRA imposes storage, disposal, and transportation requirements on registrants and applicants for the registration of pesticides. Pesticide use is regulated through requirements to apply pesticides in a manner consistent with the label. The labeling requirement includes directions for use, warnings, and cautions along with the uses for which the pesticide is registered (e.g., pests and appropriate applications). This includes the specific conditions for the application, mixture, and storage of the pesticide. Additionally, the label must specify a time period for re-entry into an area after the pesticide has been applied, and when crops may be harvested after the application of the pesticide. If a pesticide is used in a manner contrary to specifics on its label, then the use constitutes a violation of the FIFRA.

Hazardous Communication Standard

The Occupational Safety and Health Administration helps ensure employee safety by regulating the handling and use of chemicals in the workplace. For instance, it administers the Hazard Communication Standard (HCS). The HCS ensures safety in the workplace concerning chemicals through requiring information to be provided and understood by workers about the identity and hazards associated with chemicals they may work with. This also requires that chemical manufactures and importers evaluate the hazards associated with the chemicals they create or import, and that these chemicals have proper labels and material safety data sheets concerning their hazards to others (e.g., customers). Downstream of the production, employers who utilize these hazardous chemicals in their workplaces are obligated to have labels and safety data sheets for their workers and to train them on the proper handling of these chemicals.

Hazardous Substances Act

The Consumer Product Safety Commission has a limited role in regulating hazardous substances; it primarily deals with the labeling of consumer products through the federal Hazardous Substances Act

(HSA). HSA only requires products that may at some point be in the presence of people's dwellings to be labeled, including during purchase, storage, or use. These labels must alert consumers of the potential hazards that the product may pose. However, in order for a product to be required for labelling, the product must be toxic, corrosive, flammable/combustible, an irritant, a strong sensitizer, or have the ability to generate pressure through decomposition, heat, or other means. Furthermore, the product must possess the ability to cause severe personal injury or substantial illness during or as a result of any customary or reasonably predictable handling or use, including reasonably foreseeable ingestion by children.

Toxic Substances Control Act

The federal Toxic Substances Control Act (TSCA), as amended by the Frank R. Lautenberg Chemical Safety for the 21st Century Act, permits the USEPA to evaluate the potential risk from novel and existing chemicals and address unacceptable risks chemicals may have on human health and the environment. The USEPA oversees the production, importation, use, and disposal of certain chemicals. This includes the USEPA having the authority to require record keeping, reporting, and test requirements and restrictions associated with certain chemical substances and/or mixtures. However, certain groups of chemicals are excluded from TSCA consideration, including—but not limited to—food, drugs, cosmetics and pesticides. Examples of chemicals included in TSCA consideration are lead paint, asbestos, mercury, formaldehyde, and polychlorinated biphenyls.

Emergency Planning and Community Right-to-Know Act

The federal Emergency Planning and Community Right-to-Know Act (EPCRA) is designed to assist local communities protect public health, safety, and the environment from chemical hazards. The Community Right-to-Know provisions help increase the public's knowledge and access to information on chemicals at individual facilities, their uses, and releases into the environment. The EPCRA also requires industry to report on the storage, usage, and releases of hazardous substances to federal, state, and local governments, and states and communities can use the information gained to improve chemical safety and protect public health and the environment.

National Fire Protection Association Codes and Standards

The National Fire Protection Association (NFPA) publishes more than 300 consensus codes and standards intended to minimize the possibility and effects of fire and other risks, including, but not limited to:

- NFPA 13 Standard for the Installation of Sprinkler Systems
- NFPA 72 National Fire Alarm and Signaling Code
- NFPA 88A Standard for Parking Structures
- NFPA 1660 Standard for Emergency, Continuity, and Crisis Management: Preparedness, Response, and Recovery
- NFPA 1140 Standard for Wildland Fire Protection

State and Local

Alaska Statutes

The Alaska Statutes, particularly Title 41, Chapter 15, address various aspects of wildfire prevention, management, and response with the following being example key provisions:

- Protection of Forested Land (AS 41.15.010 through 41.15.170): This section outlines the state's efforts to protect forested lands from wildfires and other destructive forces. It mandates measures to prevent, manage, and suppress wildfires and establishes the responsibilities of landowners in these efforts.
- Fire Suppression Fund (AS 41.15.200 through 41.15.240): This section creates a special fund within the state treasury, designated specifically for fire suppression activities. The fund is used by the Department of Natural Resources to cover the costs associated with wildfire suppression.
- Permits and Fire Seasons (AS 41.15.050 and AS 41.15.060): These statutes establish the annual fire season from April 1 to August 31, during which additional precautions are required. Permits are necessary for activities that involve fire or that increase fire risk during this period.
- Enforcement Authority (AS 41.15.950): This statute grants enforcement authority to designated peace officers to ensure compliance with wildfire regulations. These officers have the power to arrest individuals who violate wildfire laws and issue citations.

In addition, the Alaska Statutes regulate hazardous materials, focusing on handling, reporting, and response to ensure public and environmental safety with the following being examples of key statutes:

- **Reporting Requirements (AS 29.35.500 through 590):** This section mandates businesses and government agencies handling hazardous chemicals, materials, or wastes to submit regular inventories and reports. It includes specific reporting thresholds for various hazardous substances, such as compressed gases, flammable solids, and extremely hazardous substances.
- **Municipal Responsibilities (AS 29.35.530):** Municipalities that establish hazardous material programs have the authority to enforce reporting requirements and coordinate emergency responses. They can also impose penalties for non-compliance.
- **Placarding (AS 18.70.310):** Businesses handling hazardous materials are required to post warning placards that follow the National Fire Protection Association (NFPA) standards. These placards provide essential information to the public and emergency responders.
- **Title 46: Water, Air, Energy, and Environmental Conservation:** Establishes the legal framework for managing hazardous materials and responding to environmental hazards, such as oil spills and other dangerous substances. Specifically:
 - **AS 46.04** addresses oil and hazardous substance pollution control, including the establishment of contingency plans and the creation of response mechanisms for hazardous material incidents.
 - **AS 46.08** creates the Oil and Hazardous Substance Release Response Fund, which finances the state's efforts in spill prevention, preparedness, and response, including the coordination of HAZMAT response teams. Furthermore, HAZMAT Response Programs fall primarily under Title 46. Alaska has established several statewide programs to manage hazardous material spills and emergencies, such as the Statewide Hazardous Materials Response Team that is crucial for coordinating responses during significant hazmat incidents.

Alaska Interagency Fire Management Plan

Alaska's fire management strategy is coordinated through the Alaska Interagency Fire Management Plan, which provides a framework for wildfire suppression and management across the state's 375 million acres. The plan divides Alaska into four fire protection categories—Critical, Full, Modified, and Limited—to prioritize fire suppression efforts based on the value of resources at risk and fire management objectives. Critical Protection areas, such as population centers and critical infrastructure, receive the highest priority for fire suppression, while Limited Protection zones, which often encompass remote or wilderness areas, allow fires to burn naturally unless they threaten specific resources. The Project Site is located within a Critical Protection zone, which prioritizes aggressive fire suppression efforts due to the

presence of population centers, key infrastructure, and high-value resources. This designation ensures that any wildfire in the vicinity of the project site will be met with the highest level of suppression response to protect life, property, and essential services. (Grabinski, Z. & H. R. McFarland, 2019).

The implementation of this plan involves close collaboration among several federal and state agencies, including the Bureau of Land Management Alaska Fire Service, the Alaska Division of Forestry, and the U.S. Forest Service. These agencies coordinate fire suppression efforts under the Alaska Master Cooperative Wildland Fire Management and Stafford Act Agreement to ensure efficient resource allocation and response across the state. Additionally, the Alaska Wildland Fire Coordinating Group oversees statewide interagency fire management planning, while the Alaska Interagency Coordination Center serves as the central hub for all fire management and suppression activities in Alaska (Grabinski, Z. & H. R. McFarland, 2019).

VISUAL RESOURCES – EA SECTION 3.13

Regulatory Setting

Federal

Wild and Scenic Rivers Act

The Wild and Scenic Rivers Act of 1968 is a federal law that was established to protect selected rivers in the United States that have outstandingly remarkable scenic, recreational, geologic, fish and wildlife, historic, cultural, or other similar values. The Act preserves the unique character of these rivers while also acknowledging their potential for appropriate use and development. It encourages river management that crosses political boundaries and promotes public participation in developing goals for river protection. The National Wild and Scenic Rivers System was created by the Wild and Scenic Rivers Act. River units designated as part of the system are classified and administered three types based on the condition of the river, the amount of development in the river or on the shorelines, and the degree of accessibility by road or trail at the time of designation:

- Wild River Areas: These rivers or sections of rivers are free of impoundments and generally inaccessible except by trail, with watersheds or shorelines essentially primitive and waters unpolluted. These represent vestiges of primitive America.
- Scenic River Areas: These rivers or sections of rivers are free of impoundments, with shorelines or watersheds still largely primitive and shorelines largely undeveloped, but accessible in places by roads.
- Recreational River Areas: These rivers or sections of rivers are readily accessible by road or railroad, may have some development along their shorelines, and may have undergone some impoundment or diversion in the past.

Typically, rivers are added to the system by an act of Congress, but they may also be added by state nomination with the approval of the Secretary of the Interior. Congress initially designated 789 miles of eight rivers as part of the system. Today there are 208 river units with 12,708.8 miles in 40 states and Puerto Rico, administered by federal agencies or by state, local, or tribal governments. Federal agencies are typically the National Park Service, the Bureau of Land Management, the Forest Service, or the Fish and Wildlife Service.

State and Local

Department of Transportation and Public Facilities

The DOT&PF manages scenic and visual resources through a combination of policies, planning processes, and guidelines designed to preserve and enhance the natural beauty of Alaska while accommodating transportation needs. Alaska DOT&PF designates and manages scenic byways to highlight and protect visually significant routes. These programs aim to enhance travelers' experiences while ensuring that transportation infrastructure integrates well with the natural environment.

Anchorage Municipal Code

The Anchorage Municipal Code addressing visual resources by providing guidelines and policies aimed at preserving and enhancing the city's aesthetic and scenic qualities. These policies focus on protecting significant views, maintaining the character of neighborhoods, and ensuring new developments are visually harmonious with their surroundings.

The Municipal Code includes zoning ordinances that regulate the use and appearance of properties within different zones. Some areas of Anchorage are designated for their scenic value, and the AMC includes provisions to protect these views. For example, there may be restrictions on building heights or types of development that could obstruct important scenic vistas.

Additionally, to prevent visual clutter and maintain aesthetic quality, the code has guidelines for signage, including size, placement, and illumination. These regulations are designed to ensure that signs are compatible with their surroundings and do not detract from the visual environment. There are also provisions for the preservation of historic and culturally significant sites, which can include visual and scenic aspects. These regulations help protect landmarks and areas of historical value from inappropriate alterations that could impact their visual integrity.

Chapter 21.12 SIGNS

21.12.070 - Signs in the commercial, industrial, downtown (DT), turnagain arm (TA), transition (TR), and airport (A) districts. The purpose of this chapter is to promote the public health, safety, and welfare by establishing standards and criteria for the construction, installation, maintenance, and operation of signs in the municipality, which are subject to the provisions of this chapter. Some excerpts are included below.

A. *Applicability.* Signs in the commercial and industrial districts, as well as the downtown (DT), transition (TR), and airport (A) districts shall conform to the standards set forth in this section. The standards of this section apply only to the nonresidential parcels in the TA district.

B. *Basic standards for building signs.*

1. *Maximum area of permanent building signs.* Building signs shall conform to the maximum area

limitations set forth in Table 21.12-5.

2. Placement of building signs.

a. The building signs permitted in Table 21.12-5 may be placed on the wall, awnings, canopies, parapets, or be a projecting sign in compliance with subsection 2.b. below.

b. Projecting signs shall be limited to occupants that have a minimum of ten feet of occupant building frontage provided that:

i. All projecting signs shall have a minimum clearance of eight feet from the ground to the bottom of the sign. A projecting sign may be a minimum of six feet from the ground when it is located above a landscaped area or other area that does not permit pedestrian traffic beneath the sign.

ii. Projecting signs shall be placed on the building so that the signs are intended to be viewed by the pedestrians on the abutting street or pedestrian way.

c. Except in a commercial development as described in subsection 1.d. below, no more than two freestanding signs are allowed along any one frontage. Freestanding signs must be separated by a minimum distance of 150 feet. On corner lots, the property frontage on both streets shall be considered when measuring the separation.

d. If a commercial development comprises more than a single platted lot, and one or more of the platted lots or commercial tract lots of the commercial development has frontage on a classified street of class III or greater, then each platted lot or commercial tract lot having a building and frontage of 100 feet or more on the class III or greater street shall be permitted a freestanding, monument style sign, subject to the limitations of this subsection. In addition, if a building central to the commercial development has frontage of less than 100 feet on the class III or greater street, then one freestanding, monument style sign adjacent to each entrance to the property from the class III or greater street shall be permitted subject to the limitations of this subsection. The height limitation shall be eight feet, excluding the base; the total area of each sign shall not exceed 120 square feet; pole signs are prohibited; and each sign shall comply with all other requirements of this chapter. Signs permitted by this subsection are in addition to all other signage within the commercial development permitted by this chapter including freestanding signs along frontage on classified streets of less than class III and building signs.

G. Menu boards and ordering screens. In addition to other allowed building or freestanding signs, restaurants with accessory drive-through service and food and beverage kiosks may have menu boards, ordering screens, and clearance bars.

H. Unified sign plan. To recognize and accommodate irregular site shapes (which are typically characterized by narrow lot frontages resulting in some buildings with large setbacks and limited visibility to a public street) multiple contiguous lots and/or tracts may be considered as a single site for the purposes of determining the size, number, and placement of freestanding signs permitted pursuant to this section. Solely for the purposes of this section:

1. The number and area of the freestanding signs permitted, pursuant to Table 21.12-6, shall be determined based on size and frontage of the multiple properties being considered as a single parcel.

2. After a unified sign plan has been approved by the municipality and a permit has been issued, the sign rights or limitations shall be recorded with the state district recorder's office as a deed restriction.

3. Revocation of a unified sign plan must be approved by all participants in the plan; all signs on the affected properties must be in compliance with this code before the plan can be revoked.

4. This subsection shall not be interpreted as authorizing the erection or maintenance of any sign or display within 660 feet of the nearest edge of the right-of-way and visible from the main-traveled way of an interstate, primary, or secondary highway, or the erection or maintenance of any sign or display beyond 660 feet of the nearest edge of the right-of-way of the main traveled way of an interstate, primary, or secondary highway with the purpose of the message displayed being read from that travel way, in a manner that would conflict with the provisions of AS 19.25.075—19.25.180.

I. *Display of commercial flags.* In the districts listed in Table 21.12-5, a maximum of three flagpoles may be erected on any parcel provided that:

1. A maximum of three commercial flags may be displayed simultaneously.

2. The maximum length of the flag pole shall be 30 feet.

3. The total maximum size of all commercial flags displayed shall not exceed 120 square feet. Subject to the total maximum size of commercial flags, a commercial or non-commercial organization may display alongside a national or governmental flag, one organizational flag not larger than the national or governmental flag.

4. The corporate or commercial flag may only display the name, trademark, or logo of the business on the parcel and such flag may not be used for other business or advertising purposes.

J. *Temporary signs.* Temporary signs in nonresidential districts are permitted pursuant to Table 21.12-7.

Anchorage 2040 Land Use Plan

The Anchorage 2040 Land Use Plan includes guidelines that aim to protect these visual assets from the impacts of urbanization. It promotes land use practices that ensure new developments are sensitive to the surrounding viewsheds, such as maintaining scenic vistas and implementing design standards that complement the natural environment. The Anchorage 2040 Land Use Plan seeks to preserve the city of Anchorage and adjacent neighborhood's visual character while accommodating growth.

Chugiak-Eagle Creek Comprehensive Plan Update 2006

The Chugiak River Comprehensive Plan incorporates guidelines aimed at protecting and showcasing its natural landscapes. It proposes land use practices and development standards that maintain the visual integrity of key natural features, such as river vistas, forested areas, and mountainous backdrops. The plan encourages design approaches that blend with the natural environment, such as maintaining visual buffers and restricting intrusive structures.

TIMBER HARVESTING - EA SECTION 3.14

Regulatory Setting

Federal

25 CFR Part 163- General Forestry Regulations

Regulations regarding Indian Forest Land and the management activities. The regulations have forest management planning and sustained yield management, harvesting restrictions, and enterprise operations.

Forest and Rangeland Renewable Resources Planning Act of 1974 (16 USC 1600 to 1614)

This Act was passed in response to the growing tension between the timber industry and environmentalists in the late 1960s and the early 1970s. It authorizes long-range planning by the U.S. Forest Service to protect, develop, and enhance the productivity and other values of forest resources.

Indian Forest Management Handbook

The handbook, 53 IAM 4, provides information, procedures, and processes to prepare and administer the harvesting of Indian trust forest products through timber cutting permits or free-use harvesting without a permit when volumes and values are sufficiently small where formal contracts are deemed unnecessary (BIA, 2015).

Harvest of Forest Products Manual

Documents the BIA's policy, responsibilities, and standards for the authorized harvest of forest products on Indian land. It is an updated manual that combines the separate policy chapters on contract sales of forest products (53 IAM 3) and Permit of Sales of Forest Products (53 IAM 4) [BIA, 2018].

State and local

Forest Resources Practices Act

This Act (AS 41.17) governs how timber harvesting, reforestation, and timber access occur on state, private, and municipal lands. The Forest Resources Practices Act (FRPA) was adopted in 1978, with major revisions in 1990. FRPA standards are tailored to three regions, consisting of coastal forest (Region I), southcentral transitional forest (Region II), and interior boreal forest (Region III). The Project Site is located in Region II (ADNR, 2015). The FRPA is designed to protect fish habitat and water quality and ensure prompt reforestation of forest land while providing for a healthy timber harvest industry (ADNR, 2024d).

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Appendix F

Air Quality Modeling Output Data

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 1a
Alternative A - Vehicle Miles Traveled per Year

Market Areas	Trip Generation (average daily trips) ²	Trip Distribution	Average Distance (miles) ¹	Alternative A	
				Trips/Year	VMT/Year (miles)
Anchorage City Center	8736	100%	20	3,188,640	63,772,800
Water Deliveries ³			8	793	6,344
				3,189,433	63,779,144

¹ Trip lengths based on weighted average of distance to population centers.

Sources: Market Demand Analysis Casino - Anchorage Alaska, July 2015

² The proposed project is expected to generate 8,010 total weekday trips and 10,550 Saturday trips.

Sources: Traffic Impact Analysis Report Eklutna Village Native Casino, October, 2024

³ Water demand from the Water and Wastewater Study, November 2024. Assumed water truck capacity of 4,000 gallons.

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 2a
2026 Mobile Operations Criteria Pollutant and GHG Emissions

	Alternative A
vmt/yr	63,779,144
Criteria Pollutant Emissions (tpy)	
NOx	31.57
VOC	10.98
SO ₂	0.12
CO	339.21
PM _{2.5}	1.05
PM ₁₀	3.67
Greenhouse Gas¹	
CO ₂	29749.5
CH ₄	2.7
N ₂ O	1.6
CO ₂ e	30279.3

¹ GHG emissions shown in metric tonnes.
Source: MOVES4

Table 2b
2040 Mobile Operations Criteria Pollutant and GHG Emissions

	Alternative A
vmt/yr	63,779,144
Criteria Pollutant Emissions (tpy)	
NOx	10.86
VOC	5.75
SO ₂	0.09
CO	150.48
PM _{2.5}	0.64
PM ₁₀	3.21
Greenhouse Gas¹	
CO ₂	21759.32
CH ₄	1.82
N ₂ O	1.35
CO ₂ e	22209.32

¹ GHG emissions shown in metric tonnes.
Source: MOVES4

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 3a
 2026 Operational Mobile Annual Average
 Emission Factors

Criteria Pollutant	grams per mile
NOx	0.43
VOC	0.12
SO ₂	0.002
CO	4.26
PM _{2.5}	0.01
PM ₁₀	0.05
Greenhouse Gases	
CO ₂	460.39
CH ₄	0.04
N ₂ O	0.02
CO ₂ e	468.08

Source: MOVES4

Table 3b
 2026 Operational Start Annual Average
 Emission Factors

Criteria Pollutant	grams per start
NOx	0.45
VOC	0.68
SO ₂	0.000
CO	11.30
PM _{2.5}	0.02
PM ₁₀	0.02
Greenhouse Gases	
CO ₂	121.06
CH ₄	0.10
N ₂ O	0.03
CO ₂ e	133.50

Source: MOVES4

Table 3c
 2040 Operational Mobile Annual Average
 Emission Factors

Criteria Pollutant	grams per mile
NOx	0.14
VOC	0.06
SO ₂	0.001
CO	1.83
PM _{2.5}	0.01
PM ₁₀	0.04
Greenhouse Gas	
CO ₂	337.24
CH ₄	0.03
N ₂ O	0.020
CO ₂ e	343.86

Source: MOVES4

Table 3d
 2040 Operational Start Annual Average
 Emission Factors

Criteria Pollutant	grams per start
NOx	0.29
VOC	0.43
SO ₂	0.000
CO	6.13
PM _{2.5}	0.01
PM ₁₀	0.02
Greenhouse Gas	
CO ₂	78.46
CH ₄	0.05
N ₂ O	0.02
CO ₂ e	87.16

Source: MOVES4

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 4
 Fugitive Dust Emissions from Construction

	Alternatives A and B
Construction Area (acres)	6.42
Duration of Construction (months)	12
On-site cut/fill (1,000 cubic yards)	57.1
PM ₁₀ Emission Factor (tons PM ₁₀ /acre-month)	0.011
On-site cut/fill PM10 Emission Factor (tons PM10/1,000 cubic yards)	0.059
Total PM10 Emissions (tons)	4.22
Total PM_{2.5} Emissions (tons)	2.11

Source: Emission factors from WRAP's Fugitive Dust Handbook (Level 2).

Note: On-site cut/fill estimated based on Grading and Drainage Study, July 2024. PM2.5 conservatively estimated to be half of PM10 emissions.

"10,350 cubic yards of unusable excavation that will need to be hauled off-site (topsoil). Net volume of 11,600 CY of unusable excavation (poor silty material). Net fill of 29,750 cy of materials required."

Eklutna Native Village Gaming Hall Project

Air Quality Emissions Calculations

Table 5
Alternatives A & B - Construction Emissions

Construction Equipment ¹	Horsepower	Load Factor	Hours in Use ² (hours/day)	Emission Factors (g/bhp/hr) ⁶						Emission (tons/year)					
				CO	VOC	NOx	SO ₂	PM ₁₀	PM _{2.5}	CO	VOC	NOx	SO ₂	PM ₁₀	PM _{2.5}
Demolition															
2 Rubber Tired Dozers	367	0.4	8	2.9	0.371	3.51	0.005	0.154	0.142	0.08	0.01	0.09	0.00	0.00	0.00
3 Excavators	36	0.38	8	4.21	0.402	3.45	0.005	0.107	0.098	0.02	0.00	0.01	0.00	0.00	0.00
1 Concrete/Industrial Saw	33	0.73	8	4.35	0.439	3.63	0.007	0.101	0.093	0.01	0.00	0.01	0.00	0.00	0.00
Employee Trips (miles) ³		3,000		4.24	0.12	0.21	0.00	0.04	0.01	0.01	0.00	0.00	0.00	0.00	0.00
Hauling Trips (miles) ⁴		200		3.65	0.26	1.24	0.00	0.11	0.04	0.00	0.00	0.00	0.00	0.00	0.00
Site Preparation															
3 Rubber Tired Dozers	367	0.4	8	2.9	0.371	3.51	0.005	0.154	0.142	0.17	0.02	0.20	0.00	0.01	0.01
4 Tractors/Loaders/Backhoes	84	0.37	8	3.48	0.196	2.01	0.005	0.077	0.071	0.06	0.00	0.03	0.00	0.00	0.00
Employee Trips (miles) ³		7,200		4.24	0.12	0.21	0.00	0.04	0.01	0.03	0.00	0.00	0.00	0.00	0.00
Hauling Trips (miles) ⁸		728		3.65	0.26	1.24	0.00	0.11	0.04	0.00	0.00	0.00	0.00	0.00	0.00
Site Grading															
1 Grader	148	0.41	8	3.42	0.34	2.86	0.01	0.16	0.15	0.05	0.01	0.05	0.00	0.00	0.00
1 Excavator	36	0.38	8	4.21	0.40	3.45	0.01	0.11	0.10	0.02	0.00	0.01	0.00	0.00	0.00
3 Tractors/Loaders/Backhoes	84	0.37	8	3.48	0.20	2.01	0.01	0.08	0.07	0.09	0.00	0.05	0.00	0.00	0.00
1 Rubber Tired Dozer	367	0.4	8	2.90	0.37	3.51	0.01	0.15	0.14	0.11	0.01	0.14	0.00	0.01	0.01
Employee Trips (miles) ³		12,000		4.24	0.12	0.21	0.00	0.04	0.01	0.06	0.00	0.00	0.00	0.00	0.00
Hauling Trips (miles) ⁴		68,000		3.65	0.26	1.24	0.00	0.11	0.04	0.27	0.02	0.09	0.00	0.01	0.00
Fugitive Dust														4.22	2.11
Construction															
3 Forklift	82	0.20	8	3.60	0.27	2.55	0.01	0.14	0.12	0.28	0.02	0.20	0.00	0.01	0.01
1 Generator Set	14	0.74	8	2.87	0.54	4.35	0.01	0.18	0.16	0.05	0.01	0.07	0.00	0.00	0.00
1 Crane	367	0.29	7	1.66	0.20	1.95	0.01	0.08	0.07	0.25	0.03	0.29	0.00	0.01	0.01
1 Welder	46	0.45	8	4.52	0.50	3.68	0.01	0.11	0.10	0.15	0.02	0.12	0.00	0.00	0.00
3 Tractor/Loaders/Backhoes	84	0.37	7	3.48	0.20	2.01	0.01	0.08	0.07	0.45	0.03	0.26	0.00	0.01	0.01
Employee Trips (miles) ³		96,000		4.24	0.12	0.21	0.00	0.04	0.01	0.45	0.01	0.02	0.00	0.00	0.00
Vendor Trips (miles) ⁵		40,000		4.11	0.13	0.24	0.00	0.05	0.01	0.18	0.01	0.01	0.00	0.00	0.00
Hauling Trips (miles) ⁴		68,000		3.65	0.26	1.24	0.00	0.11	0.04	0.27	0.02	0.09	0.00	0.01	0.00
Paving															
2 Pavers	81	0.42	8	3.45	0.25	2.65	0.01	0.14	0.13	0.03	0.00	0.02	0.00	0.00	0.00
2 Paving Equipment	89	0.36	8	3.42	0.20	2.22	0.01	0.09	0.08	0.03	0.00	0.02	0.00	0.00	0.00
2 Rollers	36	0.38	8	4.11	0.57	3.68	0.01	0.17	0.15	0.01	0.00	0.01	0.00	0.00	0.00
Employee Trips (miles) ³		6,000		4.24	0.12	0.21	0.00	0.04	0.01	0.03	0.00	0.00	0.00	0.00	0.00
Architectural Coating															
1 Air Compressor	37	0.48	6	4.85	0.545	3.76	0.007	0.117	0.107	0.01	0.00	0.01	0.00	0.00	0.00
Employee Trips (miles) ³		2,000		4.24	0.12	0.21	0.00	0.04	0.01	0.01	0.00	0.00	0.00	0.00	0.00
Coating ⁷		0.0116									0.75				
Total Project Construction Emissions										3.17	0.98	1.82	0.00	4.31	2.18

Sources:

¹ Construction equipment list and quantity from CalEEMod. Emission factors for equipment from CalEEMod (grams/mile).

² Hours per normal work day from CalEEMod.

³ Based on 20 mile trip length. Worker trip numbers from CalEEMod. On-Road Emission Factors from MOVES4 for Passenger Truck (31). (ex. # worker trips * 2 one-way trips * construction phase length * 20 miles)

⁴ Based on 20 mile trip length. Hauling trip numbers from CalEEMod. On Road Emission Factors from MOVES4 for Single-Unit Short Haul Truck (52)

⁵ Based on 20 mile trip length. Vendor trip numbers from CalEEMod. On Road Emission Factors from MOVES4 for Light Commercial Truck (32)

⁶ Off-Road Emission factors provided from California Air Resources Board OFFROAD2011 emission factors, as sourced from CalEEMod Default Data Tables; On-Road Emission Factors from MOVES4

⁷ Paved area from CalEEMod.

⁸ Hauling trips for tree removal. Based on 7 mile trip length. 171 cords of trees observed on Project Site. Assumed 3.375 cords/haul trip (16-CY truck capacity).

Eklutna Native Village Gaming Hall Project

Air Quality Emissions Calculations

Table 6
Alternatives A & B - Construction GHG Emissions

Construction Equipment ¹	Horsepower	Load Factor	Hours in Use ² (hours/day)	Emission Factors		Emission (metric tons) CO ₂ e
				(g/bhp/hr) ⁶		
				CO ₂	CH ₄	
Demolition						
2 Rubber Tired Dozers	367	0.4	8	532.00	0.02	12.51
3 Excavators	36	0.38	8	587.00	0.02	1.93
1 Concrete/Industrial Saw	33	0.73	8	575.00	0.02	1.11
Employee Trips (total miles) ³	3,000			389.25	0.02	1
Hauling Trips (total miles) ⁴	200			930.98	0.20	0
Site Preparation						
3 Rubber Tired Dozers	367	0.4	8	532.00	0.02	28.14
4 Tractors/Loaders/Backhoes	84	0.37	8	530.00	0.02	7.91
Employee Trips (total miles) ³	7,200			389.25	0.02	3
Hauling Trips (total miles) ⁷	728			930.98	0.20	1
Site Grading						
1 Grader	148	0.41	8	531.00	0.02	7.74
1 Excavator	36	0.38	8	587.00	0.02	1.93
3 Tractors/Loaders/Backhoes	84	0.37	8	530.00	0.02	11.87
1 Rubber Tired Dozer	367	0.4	8	532.00	0.02	18.76
Employee Trips (total miles) ³	12,000			389.25	0.02	5
Hauling Trips (total miles) ⁴	68,000			930.98	0.20	66
Construction						
3 Forklifts	82	0.2	8	527.00	0.02	37.37
1 Generator Set	14	0.74	8	568.00	0.02	8.48
1 Crane	367	0.29	7	528.00	0.02	70.88
1 Welder	46	0.45	8	568.00	0.02	16.95
3 Tractors/Loaders/Backhoes	84	0.37	7	530.00	0.02	62.33
Employee Trips (total miles) ³	96,000			389.25	0.02	38
Vendor Trips (total miles) ⁵	40,000			429.87	0.02	17
Hauling Trips (total miles) ⁴	68,000			930.98	0.20	66
Paving						
2 Pavers	81	0.42	8	527.00	0.02	4.31
2 Paving Equipment	89	0.36	8	528.00	0.02	4.06
2 Rollers	36	0.38	8	587.00	0.02	1.93
Employee Trips (total miles) ⁴	6,000			389.25	0.02	2
Architectural Coating						
1 Air Compressor	37	0.48	6	567.00	0.02	0.91
Employee Trips (total miles) ³	2,000			389.25	0.02	392.16
Construction GHG Emissions						497

Source:

¹ Construction equipment list and quantity from CalEEMod. Emission factors for equipment from CalEEMod (grams/mile).

² Hours per normal work day

³ Based on 20 mile trip length. Worker trip numbers from CalEEMod. On-Road Emission Factors from MOVES4 for Passenger Truck (31). (ex. # worker trips * 2 one-way trips * construction phase length * 20 miles)

⁴ Based on 20 mile trip length. Hauling trip numbers from CalEEMod. On Road Emission Factors from MOVES4 for Single-Unit Short Haul Truck (52)

⁵ Based on 20 mile trip length. Vendor trip numbers from CalEEMod. On Road Emission Factors from MOVES4 for Light Commercial Truck (32)

⁶ Off-Road Emission factors provided from California Air Resources Board OFFROAD2011 emission factors, as sourced from CalEEMod Default Data Tables; On-Road Emission Factors from MOVES4

⁸ Hauling trips associated with tree removal. Based on 7 mile trip length. 171 cords of trees observed on Project Site. Assumed 3.375 cords/haul trip (16-CY truck capacity).

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 7a
 Alternative A

Pollutant/GHG	MMscf/year	Emission Factors (lb/MMscf)	Conversion factor (lb/tons)	Emissions (tons)
VOC	2.33	5.5	0.0005	0.01
NO _x	2.33	0.64	0.0005	0.00
CO	2.33	84	0.0005	0.10
SO ₂	2.33	0.6	0.0005	0.00
PM ₁₀	2.33	7.6	0.0005	0.01
PM _{2.5}	2.33	7.6	0.0005	0.01
Greenhouse Gas			lb/MT	MT
CO ₂	2.33	120,000	0.00045	126

Stationary Sources include boilers, stoves, heating units, and other equipment.

Source: AP 42, Tables 1.4-1 and 1.4-2 (EPA, 1998), USEIA, 2022.

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 8a Indirect GHG Emissions
 Alternative A

Sources	Emission Factors			Use	Emissions (MT of CO ₂ e)
	CO ₂	CH ₄	N ₂ O		
	lbs of/MWh			MWh	
Electricity	995.8	0.107	0.015	1,256	571.22
	MT of CO ₂ e/MT of Solid Waste			MT of Solid Waste	
Solid Waste	0.503			52.92	26.62
	MT of CO ₂ e/Million Gallons			Million Gallons	
Water/Wastewater	6.428			0.00	0.00
Total					598

Sources: Electricity based on U.S. Energy Information Administration, 2018 Commercial Buildings Energy Consumption Survey.
 Solid Waste value from CalEEMod for Quality Resturaunt land use

Potential To Emit Calculator for Boilers and Emergency Engines

7/1/2016

This spreadsheet helps estimate a facility's potential to emit. It is provided for the convenience of the permitted community. Emission factor sources are subject to revision or correction. It is the permittee's responsibility to determine their emissions. The permittee should consult with the reviewing authority to determine the appropriateness of this calculator for its source.

If you have one or more of the following units that are exempt from the Indian Country Minor NSR Program, please contact your EPA Regional office before you use this calculator to determine whether you need to obtain a minor NSR permit: Internal combustion engines used for landscaping purposes; Emergency generators, designed solely for the purpose of providing electrical power during power outages; in nonattainment areas classified as Serious or lower, the total maximum manufacturer's site-rated hp of all units shall be below 500; in attainment areas, the total maximum manufacturer's site-rated hp of all units shall be below 1,000; Stationary internal combustion engines with a manufacturer's site-rated hp of less than 5; and Furnaces or boilers used for space heating that use only gaseous fuel, with a total maximum heat input (i.e., from all units combined) of in nonattainment areas classified as Serious or lower, 5 MMBtu/hr or less; in nonattainment areas classified as Severe or Extreme, 2 MMBtu/hr or less; and in attainment areas, 10 MMBtu/hr or less.

Directions: Enter the facility's information below.

Write the letter "Y" or "N" next to each fuel type to indicate that the facility does or does not burn that type of fuel.

For **Total Boiler Heat Input** - only add together the heat input of those units that are rated at least 10.0 MMBtu/hr or larger, unless in a severe or extreme ozone nonattainment area. Include all units 2.0 MMBtu/hr and greater in severe or extreme ozone nonattainment areas.

For **Total Small Boilers and Heaters Input** - only add together the heat input of those boilers and heaters that are rated at less than 10.0 MMBtu/hr, unless in a severe or extreme ozone nonattainment area. Include all units less than 2.0 MMBtu/hr in severe or extreme ozone nonattainment areas.

For **Emergency Generators/Engines** - this calculator does not calculate emissions from non-emergency (prime) engines. If you have non-emergency engines please contact your reviewing authority

Facility Profile*

Total Boiler Heat Input -	0	(MMBtu/hr)	Total of boilers 10.0 MMBtu/hr and greater, unless in extreme ozone nonattainment area then include 2.0 MMBtu/hr and greater.
Fuels Used			
Natural Gas-	Y	(Y or N)	
LPG	N	(Y or N)	Sulfur %
Liquid Fuel (distillate, diesel, etc.)	N	(Y or N)	0.0015 Default = 0.0015
Total Small Boilers and Heaters Input -	0	(MMBtu/hr)	Total of boilers less than 10.0 MMBtu/hr, unless in extreme ozone nonattainment area then less than 2.0 MMBtu/hr.
Fuels Used			
Natural Gas	N	(Y or N)	
LPG	N	(Y or N)	Sulfur %
Liquid Fuel (distillate, diesel, etc.)	N	(Y or N)	0.0015 Default = 0.0015
Emergency Generator/Engines -	680	(hp)	
Fuels Used			
Diesel-	Y	(Y or N)	
Gasoline	N	(Y or N)	Sulfur %
Natural Gas/LPG	N	(Y or N)	0.0015 Default = 0.0015

Converter

Insert Value:			
Convert MMscf/yr to MMBtu/yr	345	MMscf	351900 MMBtu/yr
			40,17123 MMBtu/hr

Potential To Emit Calculator for Boilers

7/1/2016

Emissions from Emergency Generator/Engine - Criteria Pollutants

Engine Size: 680 hp

Purple values are pulled from other worksheet

Diesel Used: Y

Blue values are results

Worst Case PTE (ton/yr)

Pollutant									
PM	PM ₁₀	PM _{2.5}	SO ₂	NO _x	CO	VOC	CO ₂	Single HAP	Combined HAP
0.12	0.12	0.12	0.00	4.08	0.94	0.12	195.50	0.20	0.65

Engine Type: Diesel Engine (<= 600 hp) Used: N

	Pollutant									
	PM ²	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single Hap	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	2.20E-03	2.20E-03	2.20E-03	2.05E-03	3.10E-02	6.68E-03	2.47E-03	1.15	0.00118	0.0038
PTE (ton/yr)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Note:

1. Emission factors are from Chapter 3.3, Table 3.3-1 (updated 10/96).
2. Assume PM and PM_{2.5} emissions are equal to PM₁₀ emissions.
3. Assume TOC (total organic compounds) emissions equal to VOC emissions.
4. Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Engine Type: Diesel (> 600 hp) Used: Y Sulfur Content: 0.00 %

	Pollutant									
	PM	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single HAP	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	0.0007	0.0007	0.0007	1.21E-05	0.024	5.50E-03	7.05E-04	1.15	0.00118	0.0038
Limited PTE (ton/yr)	0.12	0.12	0.12	0.00	4.08	0.94	0.12	195.50	0.20	0.65

Note:

1. Emission factors are from Chapter 3.4, Tables 3.4-1 and 3.4-2 for Large Stationary Diesel and Dual Fuel Engines (updated 10/96).
2. Assume PM_{2.5} emissions are equal to PM₁₀ emissions.
3. Assume TOC (total organic compounds) emissions equal to VOC emissions.
4. Assume 500 hours/yr of operation for an emergency engine

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Engine Type: Gasoline Used: N

	Pollutant									
	PM ²	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single Hap	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	7.21E-04	7.21E-04	7.21E-04	5.91E-04	0.011	6.96E-03	2.05E-02	1.08E+00	1.18E-03	3.80E-03
PTE (ton/yr)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Note:

1. Emission factors are from Chapter 3.3, Table 3.3-1 (updated 10/96).
2. Assume PM and PM_{2.5} emissions are equal to PM₁₀ emissions.
3. Assume TOC (total organic compounds) emissions equal to VOC emissions.
4. Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Engine Type: Natural Gas/LPG Used: N

	Pollutant									
	PM ²	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single HAP	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	1.67E-04	1.67E-04	1.67E-04	5.06E-06	1.90E-02	3.20E-02	2.55E-04	0.946	0.00045408	0.0006192
PTE (ton/yr)	FALSE	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Note:

1. Emission factors are from Chapter 3.3, Table 3.3-1 (updated 10/96).
2. Assume PM and PM_{2.5} emissions are equal to PM₁₀ emissions.
3. Assume TOC (total organic compounds) emissions equal to VOC emissions.
4. Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Potential To Emit Calculator for Boilers and Emergency Engines

7/1/2016

This spreadsheet helps estimate a facility's potential to emit. It is provided for the convenience of the permitted community. Emission factor sources are subject to revision or correction. It is the permittee's responsibility to determine their emissions. The permittee should consult with the reviewing authority to determine the appropriateness of this calculator for its source.

If you have one or more of the following units that are exempt from the Indian Country Minor NSR Program, please contact your EPA Regional office before you use this calculator to determine whether you need to obtain a minor NSR permit: Internal combustion engines used for landscaping purposes; Emergency generators, designed solely for the purpose of providing electrical power during power outages; in nonattainment areas classified as Serious or lower, the total maximum manufacturer's site-rated hp of all units shall be below 500; in attainment areas, the total maximum manufacturer's site-rated hp of all units shall be below 1,000; Stationary internal combustion engines with a manufacturer's site-rated hp of less than 5; and Furnaces or boilers used for space heating that use only gaseous fuel, with a total maximum heat input (i.e., from all units combined) of in nonattainment areas classified as Serious or lower, 5 MMBtu/hr or less; in nonattainment areas classified as Severe or Extreme, 2 MMBtu/hr or less; and in attainment areas, 10 MMBtu/hr or less.

Directions: Enter the facility's information below.

Write the letter "Y" or "N" next to each fuel type to indicate that the facility does or does not burn that type of fuel.

For **Total Boiler Heat Input** - only add together the heat input of those units that are rated at least 10.0 MMBtu/hr or larger, unless in a severe or extreme ozone nonattainment area. Include all units 2.0 MMBtu/hr and greater in severe or extreme ozone nonattainment areas.

For **Total Small Boilers and Heaters Input** - only add together the heat input of those boilers and heaters that are rated at less than 10.0 MMBtu/hr, unless in a severe or extreme ozone nonattainment area. Include all units less than 2.0 MMBtu/hr in severe or extreme ozone nonattainment areas.

For **Emergency Generators/Engines** - this calculator does not calculate emissions from non-emergency (prime) engines. If you have non-emergency engines please contact your reviewing authority

Facility Profile*				Converter				
Total Boiler Heat Input -	0	(MMBtu/hr)	Total of boilers 10.0 MMBtu/hr and greater, unless in extreme ozone nonattainment area then include 2.0 MMBtu/hr and greater.	Insert Value:				
Fuels Used				Convert MMscf/yr to MMBtu/yr	345	MMscf	351900	MMBtu/yr
Natural Gas-	Y	(Y or N)						
LPG	N	(Y or N)	Sulfur %					
Liquid Fuel (distillate, diesel, etc.)	N	(Y or N)	0.0015	Default = 0.0015			40,17123	MMBtu/hr
Total Small Boilers and Heaters Input -	0	(MMBtu/hr)	Total of boilers less than 10.0 MMBtu/hr, unless in extreme ozone nonattainment area then less than 2.0 MMBtu/hr.					
Fuels Used								
Natural Gas	N	(Y or N)						
LPG	N	(Y or N)	Sulfur %					
Liquid Fuel (distillate, diesel, etc.)	N	(Y or N)	0.0015	Default = 0.0015				
Emergency Generator/Engines -	1220	(hp)						
Fuels Used			Sulfur %					
Diesel-	Y	(Y or N)	0.0015	Default = 0.0015				
Gasoline	N	(Y or N)						
Natural Gas/LPG	N	(Y or N)						

Potential To Emit Calculator for Boilers

7/1/2016

Emissions from Emergency Generator/Engine - Criteria Pollutants

Engine Size: 1220 hp

Purple values are pulled from other worksheet

Diesel Used: Y

Blue values are results

Worst Case PTE (ton/yr)

Pollutant									
PM	PM ₁₀	PM _{2.5}	SO ₂	NO _x	CO	VOC	CO ₂	Single HAP	Combined HAP
0.21	0.21	0.21	0.00	7.32	1.68	0.22	350.75	0.36	1.16

Engine Type: Diesel Engine (<= 600 hp) Used: N

	Pollutant									
	PM ²	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single Hap	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	2.20E-03	2.20E-03	2.20E-03	2.05E-03	3.10E-02	6.68E-03	2.47E-03	1.15	0.00118	0.0038
PTE (ton/yr)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Note:

1. Emission factors are from Chapter 3.3, Table 3.3-1 (updated 10/96).
2. Assume PM and PM_{2.5} emissions are equal to PM₁₀ emissions.
3. Assume TOC (total organic compounds) emissions equal to VOC emissions.
4. Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Engine Type: Diesel (> 600 hp) Used: Y Sulfur Content: 0.00 %

	Pollutant									
	PM	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single HAP	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	0.0007	0.0007	0.0007	1.21E-05	0.024	5.50E-03	7.05E-04	1.15	0.00118	0.0038
Limited PTE (ton/yr)	0.21	0.21	0.21	0.00	7.32	1.68	0.22	350.75	0.36	1.16

Note:

1. Emission factors are from Chapter 3.4, Tables 3.4-1 and 3.4-2 for Large Stationary Diesel and Dual Fuel Engines (updated 10/96).
2. Assume PM_{2.5} emissions are equal to PM₁₀ emissions.
3. Assume TOC (total organic compounds) emissions equal to VOC emissions.
4. Assume 500 hours/yr of operation for an emergency engine

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Engine Type: Gasoline Used: N

	Pollutant									
	PM ²	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single Hap	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	7.21E-04	7.21E-04	7.21E-04	5.91E-04	0.011	6.96E-03	2.05E-02	1.08E+00	1.18E-03	3.80E-03
PTE (ton/yr)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Note:

1. Emission factors are from Chapter 3.3, Table 3.3-1 (updated 10/96).
2. Assume PM and PM_{2.5} emissions are equal to PM₁₀ emissions.
3. Assume TOC (total organic compounds) emissions equal to VOC emissions.
4. Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Engine Type: Natural Gas/LPG Used: N

	Pollutant									
	PM ²	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single HAP	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	1.67E-04	1.67E-04	1.67E-04	5.06E-06	1.90E-02	3.20E-02	2.55E-04	0.946	0.00045408	0.0006192
PTE (ton/yr)	FALSE	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Note:

1. Emission factors are from Chapter 3.3, Table 3.3-1 (updated 10/96).
2. Assume PM and PM_{2.5} emissions are equal to PM₁₀ emissions.
3. Assume TOC (total organic compounds) emissions equal to VOC emissions.
4. Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Eklutna Native Village Gaming Hall Project
Social Cost of Greenhouse Gas Calculations

GHG/Cost per metric ton	Alternative A		
		Tons	Cost
Construction (2025-2026)			
CO ₂ e	\$56	692	\$38,752
Operation (2026)			
CO ₂ e	\$56	31,416	\$1,759,296
Operation (2040)			
CO ₂ e	\$73	23,346	\$1,704,258
Lifetime			
CO ₂ e		701,072	\$51,166,492

Costs from IWG, 2021 (3% discount rate)

Appendix G
Biological Assessment



United States Department of the Interior

BUREAU OF INDIAN AFFAIRS

Alaska Regional Office
3601 C Street, Suite 1200
Anchorage, Alaska 99503

IN REPLY REFER TO
E620

Ms. Sierra Franks
Protected Species Biologist, Section 7 Coordinator
National Marine Fisheries Service, Alaska Regional Office
P.O. Box 21668
709 West 9th Street, Room 323
Juneau, AK 99802

Dear Ms. Franks:

The Bureau of Indian Affairs (BIA) received and, is reviewing a request for approvals required for implementation of the proposed Eklutna Native Village (Tribe) Gaming Facility Project (Proposed Action), as described below. The BIA requests an expedited informal consultation under section 7(a)(2) of the Endangered Species Act (ESA) for the Proposed Action. The BIA determined the Proposed Action may affect, but is not likely to adversely affect, federally listed fish species. Our supporting analysis is provided below and, in greater detail the Biological Assessment (**Attachment 1**). We request your written concurrence with our determinations.

The BIA determined the Proposed Action will have no effect on federally listed: Whales, pinnipeds, reptiles, marine invertebrates, Critical Habitat, or Essential Fish Habitat (EFH). The Action Area does not contain suitable habitat for these species, nor any designated Critical Habitat or Essential Fish Habitat. Whales, pinnipeds, reptiles, and marine invertebrates will not be discussed further in this consultation.

Description of the Action Area

The Project Area is defined as: 6.37-acre property to be developed under the Proposed Action; and includes all areas that may be directly or indirectly impacted by the Proposed Action (see **Figures 1-3 of Attachment 1**). The Action Area includes the Project Area, as well as a portion of Peters Creek, adjacent to-but outside the Project Area, and a downstream portion of Peters Creek. Over a half-mile of vegetated creek exists between the Project Area and its discharge point at the Knik Arm. Knik Arm and waters draining into the ocean are not evaluated.

The approximately 6.37-acre Project Area is located in the unincorporated community of Chugiak, within the municipality of Anchorage, Alaska. The Project Area consists of three parcels: (Assessor's Parcel Numbers 05108101000, 05108102000, and 05108115000) currently owned by members of the Eklutna Native Village, as Native allotment land under the Alaskan Native Allotment Act (ANAA). The Project Area is located off Birchwood Spur Road. The Alaska Railroad runs along the northern boundary of the Project Area, and the Birchwood Airport lies to the west. Other surrounding land uses include rural residential and industrial.

The Action Area is located within Section 5 of Township 15 North, Range 1 West, within the Seward Meridian, in the Anchorage B-7 NW United States Geological Survey (USGS) 7.5' quadrangle map. Elevations on the Action Area range between approximately 73 feet to 88 feet above mean sea level (amsl), with the majority of the site sloping gradually to the north. There is a bench cutting across the northwest corner of the property which drops approximately 8 feet, then flattens out towards the adjacent bank of Peters Creek.

Description of the Proposed Action

The Proposed Action involves the development of a Class II Gaming Facility by the Tribe; on a 6.37-acre portion of a restricted fee Native Allotment owned by members of the Tribe (Action Area). The Proposed Action consists of the following components: 1) BIA approval of a business lease by tribal members who own the restricted fee Native Allotment to the tribal government for operation of a proposed gaming facility within the Action Area; 2) development by the Tribe of a gaming facility and associated infrastructure on the Action Area; and 3) potential NIGC approval of a Gaming Management Agreement for operation of the gaming facility. Construction is estimated to commence in March of 2025 and would continue for a period of approximately 13 - 15 months.

A Site Plan is included as **Figure 4 of Attachment 1**. The Proposed Action includes a 58,000 square foot (sf) building, with paved surface parking and supporting infrastructure. The gaming facility consists of one-story (approximately 40 feet high) and approximately 32,000 square feet of gaming floor, and approximately 26,000 square feet of support and circulation space. Proposed loading docks to accommodate deliveries will be located behind the building. The gaming facility is proposed to be open for up to 24 hours a day, 7 days a week.

Access to the Action Area is currently provided via a driveway and dedicated right-of-way along the southern boundary of the property connecting to Birchwood Spur Road. The driveway will be improved and paved to serve as the primary access for vehicles ingress and egress to the Project Area (Figure 4). Development will include 443 paved surface parking spaces to accommodate employees and patrons. Exterior signage will be compatible with the building architecture, as well as the natural characteristics of the site. Exterior lighting will be strategically positioned around the building to minimize off-site glare. Downcast lighting and LED bulbs will be used in the landscaped and parking areas to minimize off-site scatter. The parking lot and areas surrounding the building will be enhanced by landscaping which incorporate plants native to the region.

There are no municipal connections for potable water supply or wastewater treatment within the Project Area. Water will be supplied via a proposed groundwater well, and a proposed on-site wastewater treatment and disposal facility. The Proposed Action will have an average water demand and wastewater generation rate of 8,681 gpd. Wastewater will be treated and disposed of through the proposed on-site septic facilities (appropriately sized). Septic facilities will include: On-site grease interceptor tank, 10,000 – 15,000-gallon septic tank, a treatment tank, an advanced secondary treatment system, and a septic drainage field that will be installed below the paved parking areas, within the Project Area.

While there are no surface waters within the Project Area, Peters Creek is adjacent to - but outside the eastern border. All development activities will be set back a minimum of 25 feet from Peters Creek, and no discharges of stormwater or wastewater to the Creek will occur. The proposed septic drain field will be located more than 200 feet from Peters Creek (see Figure 4 of **Attachment 1**).

A Grading and drainage study has been prepared and is an attachment to the Biological Assessment (**Attachment 1**). Development will require retaining walls on the north and east sides of the Action Area, and the northeast corner will be raised by three to seven feet above the existing grade due, to the steep drop off towards Peters Creek.

Following construction, impervious areas would total approximately 6.37 acres. Trenching and excavation are proposed to create stormwater detention and associated drainage infrastructure to provide volume control, treatment, and rate control. Stormwater would be collected and treated consistent with the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0 (Municipality of Anchorage, 2017). Vegetated swales would be established in the parking areas to filter runoff and, storm drainage catch basins would be located at low points on the Project Area to direct runoff to buried infiltration beds to provide volume control, treatment, and rate control (See **Attachment 1, Attachment A**). The stormwater collection and treatment system would be designed to accommodate a 24-hour, 100-year flood event with off-site runoff rates modeled to be equal to or less than existing rates.

Project-Specific Best Management Practices

The following best management practices (BMPs) related to biological resources have been included as part of the Proposed Action:

- Proposed facilities would be constructed generally consistent with the AMC and in accordance with the IBC, (including electrical, mechanical, plumbing, fire protection, and seismic standards).
- Construction and design would follow standard engineering practices related to grading and soil suitability.
- Paving and ground disturbance would be set back from Peters Creek by at least 25 feet, in accordance with the AMC (AMC 21.45.210).
- Proposed storm drainage catch basins would be located at low points on the Action Area to direct runoff to buried infiltration beds.
- Stormwater collection and treatment system would be designed to accommodate a 24-hour 100-year flood event with off-site runoff rates modeled to be equal to or less than existing rates.
- Stormwater collection and treatment facilities shall be designed with excess capacity to offset lost floodplain capacity from development within the 500-year floodplain. This capacity shall be in excess of modeled sizing, compliant with the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0. Coverage under the National Pollutant Discharge Elimination System (NPDES) Construction General Permit (CGP) shall be obtained from the U.S. Environmental Protection Agency (USEPA) for construction site runoff during the construction phase, in compliance with the Clean Water Act (CWA). Conditions of the NPDES General Construction Permit shall be adhered to.
- A Stormwater Pollution Prevention Plan (SWPPP) shall be prepared, implemented, and maintained throughout the construction phase, consistent with CGP requirements. The SWPPP shall include BMPs to minimize stormwater effects to water quality during construction. These will include, but not be limited to, the following:
 - Temporary erosion control measures (such as silt fences, fiber rolls, staked straw bales, temporary re-vegetation, rock bag dams, erosion control blankets, and sediment traps) shall be employed, as needed for disturbed areas.
 - Construction activities shall be scheduled to minimize land disturbance during peak runoff periods, to the extent feasible.
 - Disturbed areas shall be paved, re-vegetated, and/or stabilized following construction.

- A spill prevention and countermeasure plan shall be developed identifying proper storage, collection, and disposal measures for potential pollutants (such as fuel, fertilizers, pesticides, etc.) used on-site.
- Petroleum products shall be stored, handled, used and disposed of properly in accordance with provisions of the CWA (33 USC §§ 1251 to 1387).
- Construction materials shall be stored, covered, and isolated to prevent runoff loss and contamination of surface and groundwater.
- Fuel and vehicle maintenance areas shall be limited to the Action Area.
- To minimize dust generation during construction, soil shall be wetted down with water prior to ground disturbance as needed.
- Trash storage areas for receptacles will be designed to minimize stormwater runoff contact with disposed solid waste. Receptacles shall contain lids and shall be placed on impervious pavement. Receptacles along with signs encouraging use of trash receptacles will be placed in common areas to reduce littering.

Federally Listed Species and Critical Habitat/EFH

The National Marine Fisheries Service “NMFS” website was reviewed to determine appropriate species under NMFS jurisdiction that warrant evaluation. As discussed above and in **Attachment 1**, the BIA determined the Proposed Action will have no effect on federally listed: Whales, pinnipeds, reptiles, and marine invertebrates, as there is no suitable habitats within the Action Area. Therefore, whales, pinnipeds, reptiles, and marine invertebrates will not be discussed further in this consultation. A list of fish species provided by NMFS that may occur within the Alaska Region geographic area is included below.

Fish:

- Green Sturgeon (Southern DPS)(*Acipenser medirostris*): threatened
- Hood Canal Summer-run Chum Salmon (*Oncorhynchus keta*): threatened
- Lower Columbia River Coho Salmon (*Oncorhynchus kisutch*): threatened
- Lower Columbia River Steelhead (*Oncorhynchus mykiss*): threatened
- Middle Columbia River Steelhead (*Oncorhynchus mykiss*): threatened
- Snake River Basin Steelhead (*Oncorhynchus mykiss*): threatened
- Upper Columbia River Steelhead (*Oncorhynchus mykiss*): threatened
- Upper Willamette River Steelhead (*Oncorhynchus mykiss*): threatened
- Snake River Sockeye Salmon (*Oncorhynchus nerka*): endangered
- Lower Columbia River Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Puget Sound Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Snake River Fall Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Snake River Spring/Summer-run Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Upper Columbia River Spring Chinook Salmon (*Oncorhynchus tshawytscha*): endangered
- Upper Willamette River Chinook Salmon (*Oncorhynchus tshawytscha*): threatened

Federally listed fish species identified on the NMFS list are anadromous species that are reared in freshwater and spend their adult life in saltwater. Adults return to their natal freshwaters to spawn. Federally listed anadromous species do not spawn in the Alaska Region, but they may occur in Alaskan waters during the marine stages of their life cycle. Threats to these species often consist of loss of habitat, especially spawning habitat, degradation of habitat, overfishing or bycatch, and competition with invasive species.

There is no habitat for anadromous fish species in the Project Area. Peters Creek however, is hydrologically connected to the Project Area and located within 25 feet of construction activities. It is

therefore considered within the Action Area. Peters Creek is listed in the Alaska Department of Fish and Game (ADFG) – Fish Distribution Database (FDD) as an anadromous stream (#247-50-10160) showing the occurrence of rearing Coho salmon (*Oncorhynchus kisutch*), and presence of Chinook (King) salmon (*Oncorhynchus tshawytscha*) and pink salmon (*Oncorhynchus gorbuscha*). Fishing reports show Chinook salmon, coho salmon, and steelhead have occurred inland of the Knik Arm, and that Coho salmon have been caught in Peters Creek (FishBrain, 2024).

Fish species included on the NMFS list are largely specific evolutionary significant units (ESUs). ESU's are, by definition, taxonomic identifications below the species level; however, these ESUs are considered to be a "distinct population segment" and, thus a "species" under the Endangered Species Act.

While Peters Creek is known to provide habitat for certain species of anadromous fishes, the federally listed ESU species identified do not spawn in Alaskan waters, they are unlikely to occur within the inland freshwaters of Alaska. While the ADFG database and fishing reports do not specify whether the species observed in Peters Creek were the federally listed ESU species identified, it is conservatively assumed some individuals of these species may be infrequent visitors to inland waters in close proximity to marine waters. While Peters Creek is not considered spawning habitat for federally listed species, impacts to individuals that may transiently occur within Peters Creek were assessed.

Effects of the Action

Construction

Paving and ground disturbance would be set back a minimum of 25 feet from Peters Creek, consistent with the setbacks in the Anchorage Municipal Code (AMC 21.45.210). **Alternative A** has been designed to have stable slopes following construction, including the placement of a retaining wall along the Peters Creek setback to stabilize an area with steep slopes that would otherwise be prone to erosion. As part of the Proposed Action, conditions of the NPDES Construction General Permit would be adhered to. Additionally, a Stormwater Pollution Prevention Plan (SWPPP) would be prepared, implemented, and maintained throughout the construction phase. The SWPPP would include BMPs discussed above to minimize on- and off-site stormwater effects to water quality during construction and ensure that water quality thresholds designated by the USEPA to protect the environment are not exceeded. With consideration of project BMPs, construction activities associated with the Proposed Action may affect, but is not likely to adversely affect, federally listed fish species.

Operations

The operation of the Proposed Action will result in an increase in impervious surfaces within the Project Area and, will result in stormwater runoff over these surfaces, including building footprints and parking stalls. Stormwater runoff over these surfaces has the potential to carry pollutants into surface water resources. Specifically, recent research has found 6PPD-quinone, a contaminant found in runoff from roadways, to be a major contributor to pre-spawning mortality in Coho salmon (Tian et al. 2021), suggesting that other fishes may similarly be impacted.

The Proposed Action will utilize the design guidelines found in the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0 (Municipality of Anchorage, 2017). Per the design criteria, the Proposed Action would be considered a large project and would be subject to water quality treatments, including bioretention, infiltration basins, vegetated swales, chamber systems, filter strips, and others. Conceptual stormwater designs include a mixture of vegetated swales and infiltration basins. Research suggests that methods such as vegetated swales are effective at sequestering 6PPD-quinone, which otherwise has a tendency to float. Other BMPs identified with potential to reduce 6PPD-quinone concentration include particle filtering to capture larger tire debris, flow control BMPs, and

source control BMPs (McIntyre et al., 2023; Ecology, 2022). As stormwater would be treated within the Project Area prior to discharge, and may be wholly infiltrated within the Project Area, no appreciable levels of 6PPD-quinone or other pollutants would enter Peters Creek from the Project Area.

An on-site septic system would be constructed to replace the existing septic and leech field system. The septic system would be registered with the USEPA under the Underground Injection Control (UIC) program as a Class V injection well and designed and installed consistent with the ADEC standards (ADEC 18 AAC 72 Wastewater Disposal, 18 AAC 72.530(e)(2)). By proper design and installation of the on-site wastewater system consistent with federal, tribal, and local standards and requirements, impacts to water quality from treatment and discharge of wastewater will be less than significant.

In consideration of project BMPs, the Proposed Action may affect, but is not likely to adversely affect federally listed fish.

Critical Habitat and EFH

There are no NMFS designated or proposed Critical Habitat within the Action Area. The nearest Critical Habitat is designated for Cook Inlet DPS beluga whale within the Knik Arm. The Knik Arm is over 0.5 miles of vegetated streambank downstream of the Project Area and is considered outside of the Action Area. The Proposed Action will have no effect on NMFS Critical Habitat.

Similarly, there are no NMFS EFH within the Action Area. The nearest EFH is in the Knik Arm for Alaska plaice, pink salmon, chum salmon, Chinook salmon, sockeye salmon, coho salmon, dover sole, yellowfin sole, and northern rockeye sole. The EFH is located out of the Action Area and over a half-mile downstream of the Project Area. The Proposed Action would not result in indirect water quality effects to aquatic habitat in Peters Creek, which is hydrologically connected to Knik Arm. Therefore, the Proposed Action would have no effect on NMFS EFH.

Conclusion

Based on the analysis that all effects of the Proposed Action will be insignificant and/or discountable, we have determined the Proposed Action is not likely to adversely affect any listed species or Critical Habitat under NMFS jurisdiction. We used the best scientific and commercial data available to complete this analysis. We request your written concurrence with this determination.

Please direct any questions regarding this letter to Harrilene Yazzie, Regional Environmental Scientist, via email at Harrilene.Yazzie@bia.gov, or by phone at 907-271-4004.

Sincerely,

For Jolene John
Regional Director

Enclosure

Biological Assessment

- Grading and Drainage Study
- Attachment A
- References

BIOLOGICAL ASSESSMENT



Eklutna Native Village Gaming Facility Project

Chugiak, AK | December 2024

Prepared For:

Bureau of Indian Affairs
Alaska Regional Office
3601 C Street Suite 1200
Anchorage, AK 99503



BIOLOGICAL ASSESSMENT

Eklutna Native Village Gaming Facility Project

Chugiak, AK | December 2024

Prepared For:

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- Attachment A Grading and Drainage Report
- Attachment B NMFS Species List
- Attachment C Species Observed
- Attachment D Site Photographs

Section 1 | Introduction

This Biological Assessment (BA) has been prepared to address the potential effects of the proposed Eklutna Native Village Gaming Facility Project (Proposed Action) on federally listed species. The Proposed Action includes the development and operation of a gaming facility and associated infrastructure within an approximately 6.37-acre property located in the unincorporated community of Chugiak within the municipality of Anchorage, Alaska (Project Area).

This BA is intended to facilitate consultation with the National Oceanic and Atmospheric Administration's (NOAA) National Marine Fisheries Service (NMFS) and has been prepared in accordance with legal requirements set forth under Section 7 of the Endangered Species Act (ESA; 16 USC 1536 (c)) and the Magnuson-Stevens Fishery Conservation and Management Act (Section 305) concerning effects of the Proposed Action. The purpose of this BA is to evaluate the Proposed Action in sufficient detail to determine the extent to which it may affect federally listed species, critical habitat, and/or Essential Fish Habitat (EFH) under the jurisdiction of NMFS. For the purposes of this BA, federally listed species include species that are federally listed as endangered or threatened, species that are formally proposed for listing, and species that are listed as candidate species under the ESA.

To fulfill its purpose, this BA:

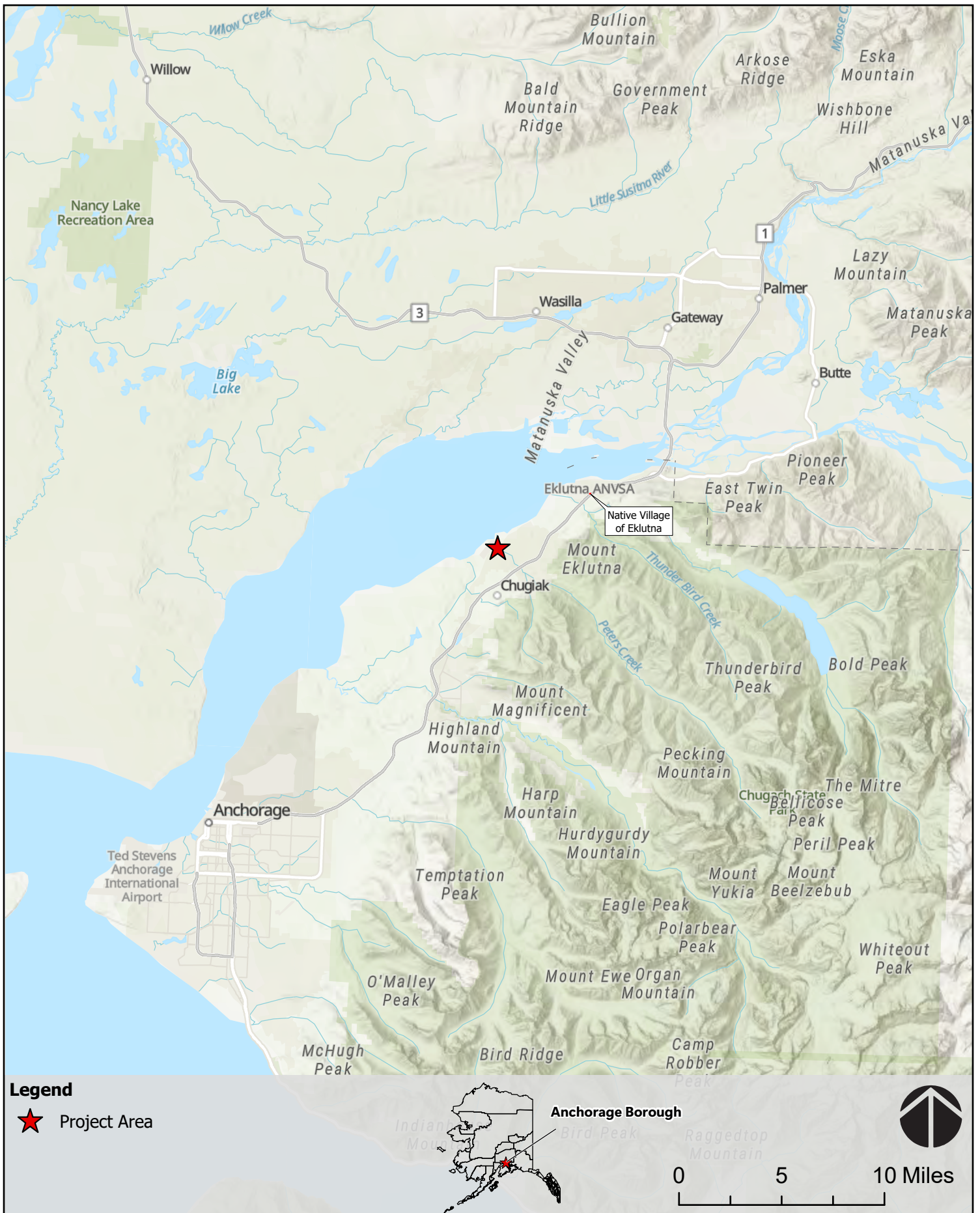
- Characterizes the habitat types present within the areas that may be impacted by the Proposed Action;
- Evaluates the potential for the occurrence of federally endangered, threatened, proposed, or candidate species under the ESA;
- Assesses the potential for the Proposed Action to adversely impact federally listed species, critical habitat, or EFH; and
- Recommends mitigation measures designed to avoid or minimize project-related impacts.

1.1 DESCRIPTION OF THE ACTION AREA

For the purpose of this report, the Project Area is defined as the 6.37-acre property that will be developed under the Proposed Action, and the Action Area includes all areas that may be directly or indirectly impacted by the Proposed Action. The Action Area includes the Project Area, as well as the portion of Peters Creek adjacent to the Project Area, and the downstream portion of Peters Creek. As over a half mile of vegetated creek exists between the Project Area and its discharge point at the Knik Arm, Knik Arm and waters into the ocean are not evaluated (**Figures 1, 2, and 3**).

1.2 PURPOSE OF THE PROPOSED ACTION

The federal Proposed Actions include the Bureau of Indian Affairs (BIA) approval of a business lease by members of the Tribe to the Eklutna Native Village tribal government for the development of a gaming facility on the Project Area and the potential approval of a timber harvest permit. The purpose and need for the Proposed Actions is to facilitate tribal self-sufficiency, self-determination, and economic development, thus satisfying the principal goal of IGRA as articulated in 25 USC § 2701.



Kenai Peninsula Borough, Matanuska-Susitna Borough GIS, Municipality of Anchorage, State of Alaska, Esri, TomTom, Garmin, SafeGraph, FAO, METI/NASA, USGS, EPA, NPS, USFWS, Esri, USGS

FIGURE 1
REGIONAL LOCATION



Kenai Peninsula Borough, Matanuska-Susitna Borough GIS, Municipality of Anchorage, State of Alaska, Esri, TomTom, Garmin, SafeGraph, METI/NASA, USGS, EPA, NPS, USDA, USFWS

FIGURE 2
SITE AND VICINITY



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FIGURE 3
AERIAL PHOTOGRAPH

1.3 PROJECT LOCATION

The approximately 6.37-acre Project Area is in the unincorporated community of Chugiak within the municipality of Anchorage, Alaska. The Project Area consists of three parcels; Assessor's Parcel Numbers 05108101000, 05108102000, and 05108115000 that are currently owned by members of the Eklutna Native Village as Native allotment land under the Alaskan Native Allotment Act (ANAA). The Project Area is located off Birchwood Spur Road. The Alaska Railroad runs along the northern boundary of the Project Area, and the Birchwood Airport lies to the west. Other surrounding land uses include rural residential and industrial. The regional location is shown in **Figure 1**.

The Action Area is within Section 5 of Township 15 North, Range 1 West, Seward Base and Meridian, in the Anchorage B-7 NW U.S. Geological Service (USGS) quadrangle (**Figure 2**). An aerial image is provided in **Figure 3**. Elevations on the Action Area range from approximately 73 to 88 feet above mean sea level (amsl), with the majority of the site sloping gradually to the north. There is a bench cutting across the northwest corner of the property that drops about 8 feet then flattens out towards the adjacent bank of Peters Creek. Description of the Proposed Action

1.4 PROPOSED ACTION

Alternative A consists of the following components: 1) BIA approval of a business lease by members of the Tribe to the tribal government for operation of a proposed gaming facility within the Project Area; 2) development by the Tribe of a gaming facility and associated infrastructure on the Project Area; 3) the harvest and sale of timber resources on the Project Area; and 4) potential NIGC approval of a Gaming Management Agreement for operation of the gaming facility.

1.4.1 Development

A site plan is included as **Figure 4**. The Proposed Action includes a 58,000 square foot (sf) building with paved surface parking and supporting infrastructure. The gaming facility would consist of one-story (approximately 40 feet high) and would include approximately 32,000 square feet of gaming floor and approximately 26,000 square feet of support and circulation space. Proposed loading docks to accommodate deliveries would be located behind the building. The gaming facility would be open for up to 24 hours a day, 7 days a week.

Access to the Action Area is currently provided via a driveway and dedicated right-of-way along the southern boundary of the property that connects to Birchwood Spur Road. This driveway would be paved and improved to serve as the primary access for vehicles entering and exiting the Project Area (**Figure 4**). Development would include 443 paved surface parking spaces to accommodate employees and patrons.

Exterior signage would be compatible with the building architecture as well as the natural characteristics of the site. Exterior lighting would be strategically positioned around the building to minimize off-site glare. Downcast lighting and LED bulbs would be used in the landscaped and parking areas to minimize off-site scatter. The parking lot and areas around the building would be enhanced by landscaping that would incorporate plants native to the region.

There are no municipal connections for potable water supply or wastewater treatment within the Project Area. Therefore, water will be supplied via a proposed groundwater well, and an on-site wastewater treatment and disposal facility will be constructed.

1.4.2 Construction

Construction would involve grading, earthwork, paving, land clearing, and timber harvest. Construction would generally involve grubbing and clearing, grading and paving using heavy-duty and light-duty equipment, trenching for utilities, and construction of buildings. Construction equipment would consist of scrapers/earthmovers, wheeled or tracked bulldozers and loaders, dump trucks, and concrete trucks. Equipment and materials would be staged within the Action Area. Proposed facilities would be constructed generally consistent with applicable Tribal codes and in accordance with the International Building Code (IBC), including electrical, mechanical, plumbing, fire protection, and seismic standards. An indoor fire suppression system would be installed. Construction is estimated to commence in 2025 and would continue for a period of approximately 14 months. Paving and ground disturbance would be set back from Peters Creek by at least 25 feet, in accordance with the AMC (AMC 21.45.210).

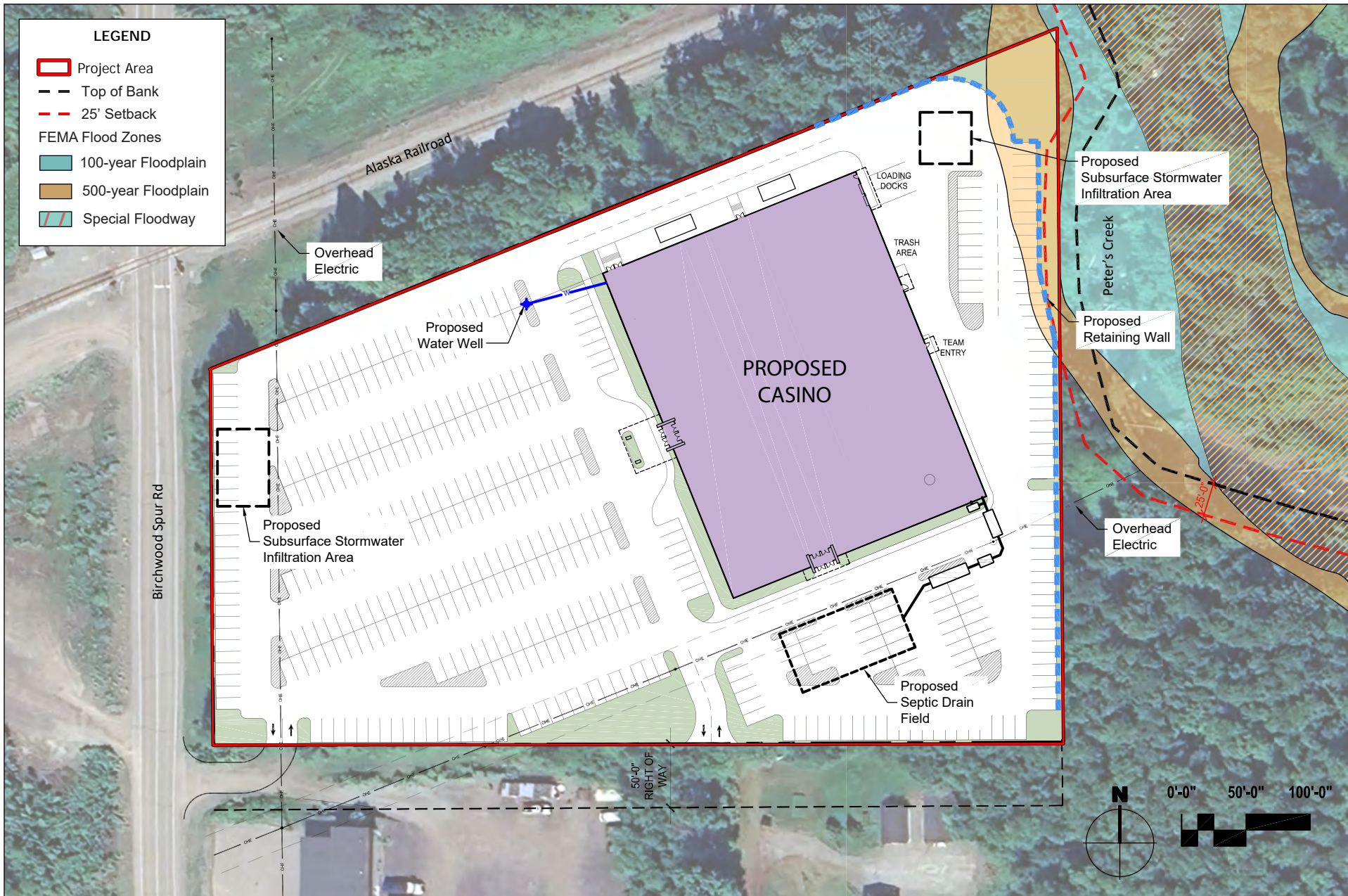
1.4.3 Timber Removal and Clearing

The Action Area consists largely of undeveloped forested land. Development would require timber removal and land clearing within the Action Area. Tree species within the Action Area are dominated by birch (*Betula* sp.), specifically, paper birch (*Betula papyrifera*). Other deciduous trees within the Action Area include alder (*Alnus* sp.) and poplar (*Populus* sp.). Spruce, including white spruce (*Picea glauca*) and red spruce (*Picea rubens*), comprise a minor component of the tree canopy. A few trees would be retained in the proposed parking and landscaping areas and along the perimeter of the Action Area. Merchantable trees would be donated to the Tribe to be utilized for construction of community facilities or sold as timber. Clearing activities would result in leftover vegetation and wood debris, such as stumps. The leftover vegetation and wood debris would be chipped on-site and utilized where feasible and as dictated by the quality of chips. A timber harvest permit may be required through the BIA.

1.4.4 Grading and Drainage

A grading and drainage study has been prepared and is included as **Attachment A**. Development would require retaining walls on the north and east sides of the Action Area, and the northeast corner would be raised by three to seven feet above the existing grade due to the steep drop off towards Peters Creek. The topmost foot of soil would be stripped across the Action Area to remove tree roots, organics, and topsoil/peat. This would equate to approximately 10,350 cubic yards (CY) of unusable material, which would be hauled off-site. Approximately 15,800 CY would be excavated. Of that, 4,200 CY would be reused across the Action Area to fill the lower areas. This would result in a net volume of 11,600 CY of unusable excavation. Approximately 20,500 CY of classified material would be imported for the pavement section.

Following construction, impervious areas would total approximately 6.37 acres. Trenching and excavation would occur to create stormwater detention and associated drainage infrastructure to provide volume control, treatment, and rate control. Stormwater would be collected and treated consistent with the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0 (Municipality of Anchorage, 2017). Vegetated swales would be established within the parking areas to filter runoff and storm drainage catch basins would be located at low points on the Project Area to direct runoff to buried infiltration beds to provide volume control, treatment, and rate control (**Appendix B**). The stormwater collection and treatment system would be designed to accommodate a 24-hour 100-year flood event with off-site runoff rates modeled to be equal to or less than existing rates.



Source: Marnell Companies, 6/5/2024, Acorn Environmental, 8/28/2024

FIGURE 4
PROPOSED PROJECT AREA SITE PLAN

1.4.5 Best Management Practices

The following best management practices (BMPs) related to biological resources have been included as part of the Proposed Action:

- Proposed facilities would be constructed generally consistent with the AMC and in accordance with the IBC, including electrical, mechanical, plumbing, fire protection, and seismic standards.
- Construction and design would follow standard engineering practices related to grading and soil suitability.
- Paving and ground disturbance would be set back from Peters Creek by at least 25 feet, in accordance with the AMC (AMC 21.45.210).
- Proposed storm drainage catch basins would be located at low points on the Action Area to direct runoff to buried infiltration beds.
- Stormwater collection and treatment system would be designed to accommodate a 24-hour 100-year flood event with off-site runoff rates modeled to be equal to or less than existing rates.
- Stormwater collection and treatment facilities shall be designed with excess capacity to offset lost floodplain capacity from development within the 500-year floodplain. This capacity shall be in excess of modeled sizing compliant with the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0. Coverage under the National Pollutant Discharge Elimination System (NPDES) Construction General Permit (CGP) shall be obtained from the U.S. Environmental Protection Agency (USEPA) for construction site runoff during the construction phase in compliance with the Clean Water Act (CWA). Conditions of the NPDES General Construction Permit shall be adhered to. A Stormwater Pollution Prevention Plan (SWPPP) shall be prepared, implemented, and maintained throughout the construction phase consistent with CGP requirements. The SWPPP shall include BMPs to minimize stormwater effects to water quality during construction. These will include, but not be limited to, the following:
 - Temporary erosion control measures (such as silt fences, fiber rolls, staked straw bales, temporary re-vegetation, rock bag dams, erosion control blankets, and sediment traps) shall be employed as needed for disturbed areas.
 - Construction activities shall be scheduled to minimize land disturbance during peak runoff periods to the extent feasible.
 - Disturbed areas shall be paved, re-vegetated, and/or stabilized following construction.
 - A spill prevention and countermeasure plan shall be developed that identifies proper storage, collection, and disposal measures for potential pollutants (such as fuel, fertilizers, pesticides, etc.) used on-site.
 - Petroleum products shall be stored, handled, used, and disposed of properly in accordance with provisions of the CWA (33 USC §§ 1251 to 1387).
 - Construction materials shall be stored, covered, and isolated to prevent runoff loss and contamination of surface and groundwater.
 - Fuel and vehicle maintenance areas shall be limited to the Action Area.
 - To minimize dust generation during construction, soil shall be wetted down with water prior to ground disturbance as needed.
 - Trash storage areas for receptacles will be designed to minimize stormwater runoff contact with disposed solid waste. Receptacles shall contain lids and shall be placed on impervious pavement. Receptacles along with signs encouraging use of trash receptacles will be placed in common areas to reduce littering.

Section 2 | Study Methods

2.1 LITERATURE REVIEW

Prior to conducting biological resources surveys within the Action Area, the following sources were reviewed:

- U.S. Geological Survey (USGS) 7.5-minute Anchorage B-7 NW and NE topographic quadrangles;
- Color aerial photography of the Action Area and vicinity;
- U.S. Fish and Wildlife Service (USFWS) National Wetland Inventory Mapper (USFWS, 2024a; **Figure 5**);
- Federal Emergency Management Agency (FEMA) flood map (**Figure 6**);
- NOAA Critical Habitat and EFH mapper (NMFS, 2024a, b);
- NOAA Species Directory (NMFS, 2024c);
- NRCS Soil report (NRCS, 2024); and
- The USFWS Information for Planning and Consultation (USFWS, 2024b). No species were returned on the USFWS list.

The NMFS website was reviewed to determine the appropriate species under NMFS jurisdiction that warrant evaluation within this BA. The Action Area falls within the Alaska Region. **Attachment A** includes the list of species provided by NMFS that may occur within the Alaska Region geographic area. The following federally listed species were identified in the NMFS list included in **Attachment A** and are evaluated herein:

Whales:

- Bowhead Whale (*Balaena mysticetus*): endangered
- Sei Whale (*Balaenoptera borealis*): endangered
- Blue Whale (*Balaenoptera musculus*): endangered
- Fin Whale (*Balaenoptera physalus*): endangered
- Cook Inlet Distinct Population Segment (DPS) Beluga Whale (*Delphinapterus leucas*): endangered
- Western North Pacific DPS Gray Whale (*Eschrichtius robustus*): endangered
- North Pacific Right Whale (*Eubalaena japonica*): endangered
- Mexico DPS Humpback Whale (*Megaptera novaeangliae*): threatened
- Western North Pacific DPS Humpback Whale (*Megaptera novaeangliae*): endangered
- Sperm Whale (*Physeter macrocephalus*): endangered

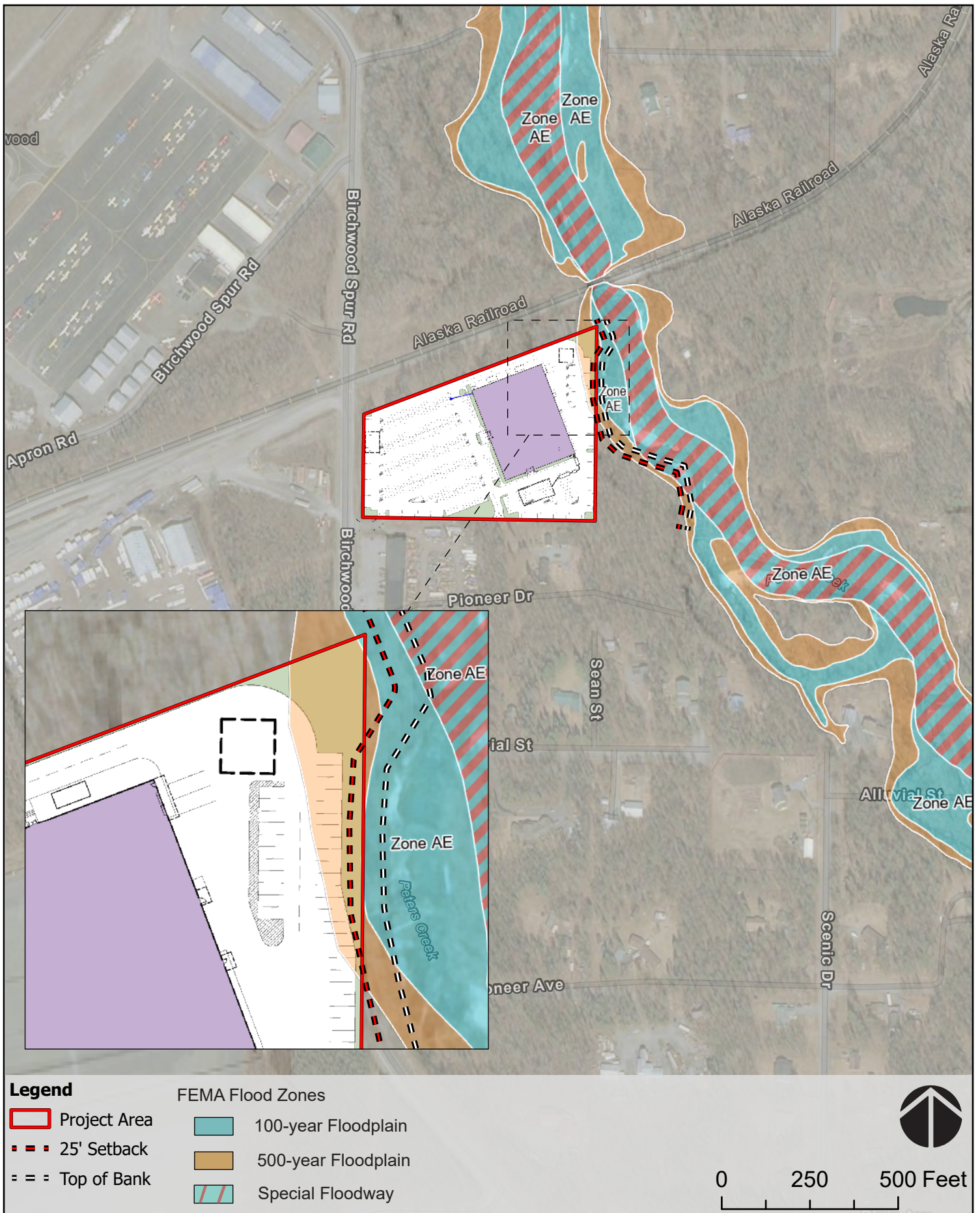
Pinnipeds:

- Arctic Ringed Seal (*Phoca hispida hispida*): threatened
- Beringia DPS, Bearded Seal (*Erignathus barbatus nauticus*): threatened
- Western DPS Steller Sea Lion (*Eumetopias jubatus*): endangered



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FIGURE 5
NATIONAL WETLAND INVENTORY



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FIGURE 6
FEMA FLOOD MAP

Reptiles:

- Loggerhead Sea Turtle (*Caretta caretta*): threatened
- Green Sea Turtle (*Chelonia mydas*): threatened
- Leatherback Sea Turtle (*Dermochelys coriacea*): endangered
- Olive Ridley Sea Turtle (*Lepidochelys olivacea*): threatened

Fish:

- Green Sturgeon (Southern DPS)(*Acipenser medirostris*): threatened
- Hood Canal Summer-run Chum Salmon (*Oncorhynchus keta*): threatened
- Lower Columbia River Coho Salmon (*Oncorhynchus kisutch*): threatened
- Lower Columbia River Steelhead (*Oncorhynchus mykiss*): threatened
- Middle Columbia River Steelhead (*Oncorhynchus mykiss*): threatened
- Snake River Basin Steelhead (*Oncorhynchus mykiss*): threatened
- Upper Columbia River Steelhead (*Oncorhynchus mykiss*): threatened
- Upper Willamette River Steelhead (*Oncorhynchus mykiss*): threatened
- Snake River Sockeye Salmon (*Oncorhynchus nerka*): endangered
- Lower Columbia River Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Puget Sound Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Snake River Fall Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Snake River Spring/Summer-run Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Upper Columbia River Spring Chinook Salmon (*Oncorhynchus tshawytscha*): endangered
- Upper Willamette River Chinook Salmon (*Oncorhynchus tshawytscha*): threatened

Marine Invertebrates:

- Sunflower sea star (*Pycnopodia helianthoides*): proposed threatened

Species listed as Proposed Threatened are not subject to Section 7 consultation. Therefore, effects to sunflower sea star are not formally addressed within this report, however, it is noted that suitable habitat is not located within the Action Area for this species.

It is noted by NMFS that the fish listed above spawn on the west coast of the lower contiguous 48 states but may occur in Alaskan waters during the marine phase of their life cycles.

2.2 BIOLOGICAL RESOURCES SURVEY

A biological resources survey was conducted within the Project Area on July 16-18, 2024. The survey also covered the portion of Peters Creek adjacent to the Project Area and downstream, as walkable. Peters Creek was observed to be immediately adjacent to the Project Area at its closest point. The survey consisted of walking transects throughout the Project Area to identify plant and animal species, characterize habitat types and vegetation communities, and assess the potential for habitats to support federally listed species. Habitat requirements for each federally listed species were assessed and compared to the type and quality of habitats observed during the biological resources survey of the Project Area.

2.3 DEFINITION OF POTENTIAL EFFECT DETERMINATIONS

Five possible determinations exist regarding a project's effects on species listed under the ESA. These determinations are as follows:

- **No effect:** The appropriate conclusion when it is determined that a project would not affect a listed species or designated critical habitat.
- **May affect but is not likely to adversely affect:** The appropriate finding when effects on ESA-protected species are expected to be discountable, insignificant, or completely beneficial.
- **May affect and is likely to adversely affect:** The appropriate finding if any adverse effect to listed species may occur as a direct or indirect result of a project or its interrelated or interdependent actions and the effect is not discountable, insignificant, or beneficial. In the event the overall effect of the project is beneficial to a ESA-protected species but is also likely to cause some adverse effects, then the project "is likely to adversely affect" the listed species. If incidental take is anticipated to occur as a result of a project, an "is likely to adversely affect" determination should be made.
- **Is likely to jeopardize the continued existence of a listed species or result in the destruction or adverse modification of critical habitat:** The appropriate determination when the action agency or NMFS identify situations where a project is likely to jeopardize the species or adversely modify critical habitat. Jeopardy occurs when a project is likely to directly or indirectly appreciably reduce the likelihood of both the survival and recovery of a protected species in the wild by reducing their reproduction, numbers, or distribution. Destruction or adverse modification of critical habitat is a direct or indirect alteration that appreciably diminishes the value of critical habitat for the survival or recovery of a listed species. Such alterations include, but are not limited to, alterations adversely modifying any of those physical or biological features that were the basis for determining the habitat to be critical.
- **Is likely to jeopardize a proposed species or adversely modify proposed critical habitat:** The appropriate conclusion if a project is likely to jeopardize the continued existence of a proposed species or adversely modify proposed critical habitat.

Section 3 | Environmental Setting

3.1 HABITAT TYPES

Habitat types in the Action Area consist of boreal forest, ruderal/developed, and Peters Creek. These habitat types are described below and are shown in **Figure 7**. A list of species observed is included as **Attachment B**, and representative photos are provided in **Attachment C**.

3.1.1 Boreal Forest (5.13 acres)

Boreal forest within the Action Area was dominated by birch trees, specifically paper birch. Other deciduous trees within the boreal forest included alders and poplars. Spruce, including white spruce and red spruce, comprise a minor component of the tree canopy. The shrub layer is generally absent from the understory and was limited to young trees and scattered willows (*Salix* sp.). The forest floor contained a dense mixture of species that left little to no ground visibility. In some areas, ground cover was dominated by Devil's club (*Oplopanax horridus*) or cow parsley (*Anthriscus sylvestris*). Other commonly occurring species included lady fern (*Athyrium filix*), fireweed (*Chamaenerion angustifolium*), meadow horsetail (*Equisetum pratense*), prickly rose (*Rosa acicularis*), raspberry (*Rubus idaeus*), and claspleaf twisted stalk (*Streptopus amplexifolius*). Tracks from use of ATVs on the Action Area were observed.

3.1.2 Ruderal/Developed (1.24 acres)

Ruderal/developed areas included those portions of the Action Area where human use and activity have converted habitats from their natural condition. A dirt access drive was observed leading to a cleared area previously used as a residential site. Residential utilities (well, septic) were observed. Patches of this habitat were observed along the access drive where heavy machinery had been used to clear piled solid waste. Vegetation in these areas was generally absent. An electrical transmission line was also observed crossing the southern portion of the Action Area. Vegetation had been cleared along this row and was limited to ground cover cropped low. Finally, a cleared area is maintained along the northern boundary of the Action Area associated with the adjacent off-site railroad.

3.1.3 Peters Creek

Peters Creek runs along and just outside the eastern boundary of the Project Area. Peters Creek is identified as the Riverine feature on the NWI map provided as **Figure 5**. A sliver of the floodplain from Peters Creek overlaps with the Project Area, as shown in **Figure 6**. There is potential for runoff to enter Peters Creek from the Project Area. Therefore, Peters Creek adjacent to the Project Area to its terminal at Knik Arm is considered part of the Action Area. Peters Creek along the Project Area was observed during the survey. Flows were fast and relatively consistent across the width and length of the creek that was observed. Observed substrate was dominated by gravel to fist-sized cobble intermixed with larger, smooth river rock. Sandy substrate made up a minor component of the visible bed and bank. Water depth appeared relatively shallow where depth was visible. It is likely depth is relatively shallow across the width given the relatively consistent flow observed. The banks consisted of a steep drop across most of the observed area, with the exception of approximately 20 feet of bank with a more terraced, sloping topography. This is consistent with the described conditions along the totality of the creek's length, which is described as swift, shallow, and rocky (Alaska Outdoors Supersite, n.d.).



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FIGURE 7
HABITAT TYPES

Section 4 | Results

This section provides an account of species identified in the NMFS list for the region as well as a discussion of the potential for each species to occur in the Action Area and the potential impacts.

4.1 EFFECTS ON FEDERALLY LISTED SPECIES

4.1.1 Whales

The NMFS list included as **Attachment A** identifies 10 federally listed whale species with the potential to occur within the Alaska Region. Peters Creek is the only surface water resource within the Action Area. As discussed above, Peters Creek is a shallow freshwater body and therefore would not provide habitat for whales. Thus, federally listed whales do not have the potential to occur within the Action Area, and the Proposed Action would have no effect on federally listed whales.

4.1.2 Pinnipeds

The NMFS list included as **Attachment A** identifies three federally listed pinniped species with the potential to occur within the Alaska Region. Peters Creek is the only surface water resource within the Action Area. As discussed above, Peters Creek is a shallow freshwater body and therefore would not provide habitat for pinnipeds. Thus, federally listed pinnipeds do not have the potential to occur within the Action Area, and the Proposed Action would have no effect on federally listed pinnipeds.

4.1.3 Reptiles

The NMFS list included as **Attachment A** identifies four federally listed reptile species with the potential to occur within the Alaska Region. All of these species are sea turtles. As discussed above, Peters Creek is the only surface water resource within the Action Area and is a shallow freshwater body. Peters Creek therefore would not provide habitat for sea turtles. Thus, federally listed reptiles do not have the potential to occur within the Action Area, and the Proposed Action would have no effect on federally listed reptiles.

4.1.4 Fish

Federally listed fish species identified on the NMFS list in **Attachment A** are anadromous species that are reared in freshwater and spend their adult life in saltwater. Adults return to their natal freshwaters to spawn. As noted in **Attachment A** and above, federally listed anadromous species do not spawn in the Alaska Region, but they may occur in Alaskan waters during the marine stages of their life cycle. Threats to these species often consist of loss of habitat, especially spawning habitat, degradation of habitat, overfishing or bycatch, and competition with invasive species.

There is no habitat for anadromous fish species in the Project Area. Peters Creek, however, is hydrologically connected to the Project Area and located within 25 feet of construction activities and is therefore considered within the Action Area. Peter's Creek is listed in the Alaska Department of Fish and Game (ADFG) – Fish Distribution Database (FDD) as an anadromous stream (#247-50-10160) showing the occurrence of rearing Coho salmon, and presence of Chinook (King) salmon and pink salmon (ADFG, 2024 a). Further, fishing reports show that Chinook salmon, coho salmon, and steelhead have occurred inland of the Knik Arm, and that coho salmon have been caught in Peters Creek (FishBrain, 2024).

Fish species included in **Attachment A** are largely specific evolutionary significant units (ESUs). ESU's are, by definition, taxonomic identifications below the species level; however, these ESUs are considered to be a "distinct population segment" and thus a "species" under the Endangered Species Act. While Peters Creek is known to provide habitat for certain species of anadromous fishes, the federally listed ESU species identified in **Attachment A** do not spawn in Alaskan waters and therefore are unlikely to occur within the inland freshwaters of Alaska.

While the ADFG database and fishing reports do not specify whether the species observed in Peters Creek were the federally listed ESU species identified in **Attachment A**, it is conservatively assumed that at least some individuals of these species may be infrequent visitors to inland waters in close proximity to marine waters. Therefore, while Peters Creek is not considered spawning habitat for federally listed species, impacts to individuals that may transiently occur within Peters Creek are assessed herein.

Potential Impacts

Construction

As part of the Proposed Action, a SWPPP would be required and would be reviewed and approved by the USEPA prior to construction. Adherence to the SWPPP and a list of SWPPP BMPs are discussed in **Section 1.4.1**. The SWPPP will ensure that water quality thresholds designated by the USEPA to protect the environment are not exceeded and thus will prevent significant adverse effects to Peters Creek during construction of the Proposed Action.

Operation

Operation of the Proposed Action would result in an increase in impervious surfaces within the Project Area and would result in stormwater runoff over these surfaces, including building footprints and parking stalls. Stormwater runoff over these surfaces has the potential to carry pollutants into surface water resources. Specifically, recent research has found 6PPD-quinone, a contaminant found in runoff from roadways, to be a major contributor to pre-spawning mortality in coho salmon (Tian et al. 2021), suggesting that other fishes may similarly be impacted. The effects of this contaminant on other species, including Chinook salmon and steelhead are still being studied, but some of the early studies indicate that first flush storm events are lethal to juvenile coho, steelhead, and Chinook salmon (French et al. 2022). However, the concentration at which 6PPD-quinone in stormwater may have toxic effects on those species is still being evaluated. Another common component of stormwater runoff is copper, which can impair the olfactory system of salmonids and hinder their predator avoidance behavior (Sandahl et al., 2007).

Research has shown that in addition to juvenile coho, both juvenile steelhead (*Oncorhynchus mykiss*) and juvenile Chinook salmon (*Oncorhynchus tshawytscha*) are also vulnerable to untreated stormwater containing 6PPD-quinone; however, the effects are not as immediate and instead of mortality occurring within a few hours of exposure (up to 4 hours for coho salmon), it can take one or two days for steelhead and Chinook salmon to display mortality (French et al., 2022). A recent study looked at the sensitivity of early life stage (~ three weeks post swim-up [newly feeding] coho salmon and Chinook salmon) and found that juvenile coho salmon are three orders of magnitude more sensitive to 6PPD-quinone than Chinook salmon (Lo et al., 2023). This study found only 7.1 percent survival for coho salmon exposed to the lowest concentration treatment group, while Chinook salmon showed 61.4 percent survival in the highest concentration treatment group (Lo et al., 2023).

While not showing the degree of sensitivity to stormwater runoff containing 6PPD-quinone as coho salmon, juvenile steelhead and Chinook do appear to exhibit direct mortality, although at higher concentrations and with longer response times (Tien et al., 2021; French et al., 2022; Lo et al., 2023; and Brinkman et al., 2022).

As discussed in **Section 1**, the Proposed Action would utilize the design guidelines as found in the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0 (Municipality of Anchorage, 2017). Per the design criteria, the Proposed Action would be considered a large project and would be subject to water quality treatments, including bioretention, infiltration basins, vegetated swales, chamber systems, filter strips, and others. Conceptual stormwater designs include a mixture of vegetated swales and infiltration basins. Research suggests that methods such as vegetated swales are effective at sequestering 6PPD-quinone, which otherwise has a tendency to float. Other BMPs identified with potential to reduce 6PPD-quinone concentration includes particle filtering to capture larger tire debris, flow control BMPs, and source control BMPs (McIntyre et al., 2023; Ecology, 2022).

As stormwater would be treated within the Project Area prior to discharge, and may be wholly infiltrated within the Project Area, no appreciable levels of 6PPD-quinone or other pollutants would enter Peters Creek from the Project Area. With consideration of project BMPs, the Proposed Action may affect, but is not likely to adversely affect federally listed fish.

Recommended Impact Avoidance, Minimization, or Mitigation Measures

None.

4.1.5 Marine Invertebrates

The NMFS list included as **Attachment A** identifies one federally listed marine invertebrate with the potential to occur within the Alaska Region. Peters Creek is the only surface water resource within the Action Area. As discussed above, Peters Creek is a shallow freshwater body and therefore would not provide habitat for marine invertebrates. Thus, federally listed marine invertebrates do not have the potential to occur within the Action Area, and the Proposed Action would have no effect on federally listed marine invertebrates.

4.2 EFFECTS ON CRITICAL HABITAT AND EFH

There is no NMFS designated or proposed critical habitat within the Action Area (NMFS, 2024a). The nearest critical habitat is designated for Cook Inlet DPS beluga whale within the Knik Arm, over a half mile downstream of the Project Area. As discussed in **Section 1.1**, the Knik Arm is over 0.5 miles of vegetated streambank downstream of the Project Area and is considered outside of the Action Area. Therefore, the Proposed Action would have no effect on NMFS critical habitat.

Similarly, there is no NMFS EFH within the Action Area (NMFS, 2024b). The nearest EFH is within the Knik Arm for Alaska plaice, pink salmon, chum salmon, Chinook salmon, sockeye salmon, coho salmon, dover sole, yellowfin sole, and northern rockeye sole. This EFH is located out of the Action Area and over a half mile downstream of the Project Area. As discussed above in Section 4.1.4, the Proposed Action would not result in indirect water quality effects to aquatic habitat in Peters Creek, which is hydrologically connected to Knik Arm. Therefore, the Proposed Action would have no effect on NMFS EFH.

Section 5 | Interrelated and Interdependent Effects

Interrelated and interdependent actions associated with the Proposed Action are those actions that have no independent utility and/or are dependent on the Proposed Action for their justification. Interrelated and interdependent actions that will likely occur as a result of the Proposed Action include the construction of access improvements to the Project Area.

Implementation of site access improvements would fall within roadside shoulders with ruderal vegetation and no surface waters, where significant biological resources do not occur and suitable habitat for species listed under the ESA would not occur. Additionally, these actions would be subject to the BMPs listed in Section 1.4.1, including erosion control, adherence to necessary permitting, and other standard measures protective of water quality. Therefore, interrelated and interdependent impacts to federally listed species would not occur.

Section 6 | Cumulative Effects

Cumulative effects resulting from the Proposed Action could occur if other developments along Peters Creek occurred and resulting in adverse impacts to federally listed species in combination with the Proposed Action. Property alongside Peters Creek from its discharge at Knik Arm upstream for approximately three miles is developed, predominantly with rural residential properties. These parcels contain minimal paved surfaces and a significant amount of tree canopy and permeable ground (**Figure 3**). There are no known future developments along Peters Creek in the vicinity of the Action Area. Therefore, cumulative impacts would not be incurred.

Section 7 | Conclusions and Determination

This Biological Assessment is respectfully submitted to NMFS for review and concurrence on the determinations presented herein and summarized below.

Federally listed Whales: The Proposed Action will have no effect on these species for the following reason:

- There is no potential for these species to occur within the Action Area.

Federally listed Pinnipeds: The Proposed Action will have no effect on these species for the following reason:

- There is no potential for these species to occur within the Action Area.

Federally listed Reptiles: The Proposed Action will have no effect on these species for the following reason:

- There is no potential for these species to occur within the Action Area.

Federally listed Fish: The Proposed Action will may affect but is not likely to adversely affect these species for the following reason:

- Potential for these species to occur is low and limited to transient individuals.
- Suitable habitat will not be converted.
- Indirect impacts to water quality would be avoided or minimized by construction and operational stormwater BMPs.

Federally listed Marine Invertebrates: The Proposed Action will have no effect on these species for the following reason:

- There is no potential for these species to occur within the Action Area.

NMFS Critical Habitat and EFH: The Proposed Action will have no effect on NMFS critical habitat or EFH for the following reason:

- There is no critical habitat or EFH within the Action Area.

Section 8 | References

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Section 9 | Qualifications of Preparers

9.1.1 Kelli Raymond, B.S.

Ms. Raymond holds a B.S. in Animal Biology with a focus on Wildlife Ecology. She has approximately 10 years of experience collecting field data and preparing environmental assessments. Ms. Raymond has worked in several states across the U.S. performing biological resources surveys, including plant surveys, wetland delineations, and wildlife utilization monitoring. She also has experience live handling numerous wildlife species, including fish, migratory birds, and big game. Ms. Raymond is experienced in the preparation of Biological Assessments and Section 7 consultation with both the USFWS and NMFS under the federal Endangered Species Act.

9.1.2 Kimberly Fuchs, B.S.

Ms. Fuchs holds a B.S. in Biology with a specialization in Marine Biology and a minor in Psychology. She also has an A.S. in Biology, A.S. in Math and Science, A.S. in Psychology, A.A. in Liberal Arts and A.A. in General Studies. Ms. Fuchs has over 20 years of experience in environmental research, field experience, permitting, and preparing environmental documents. She has worked in several states across the U.S., including Alaska, California, Texas, and Oklahoma performing biological field studies including wetland delineations, break up studies, bathymetry, marine mammal observations, fisheries, riparian assessments, and water quality data. She has also performed protected species observer/fisheries observer duties in the Bering Sea, Gulf of Alaska, and Beaufort Seas. Ms. Fuchs is experienced in the preparation of Biological Assessments, Biological Opinions, and Section 7 consultation with both the USFWS and NMFS under the Endangered Species Act.

Attachment A
Grading and Drainage Report

Attachment B
NMFS Species List



Endangered, Threatened, and Candidate Species in Alaska

Species under NOAA Fisheries Authority deemed endangered, threatened, and/or candidate species and residing in the Alaska region.

Whales

Common Name	Scientific Name	Status	Critical Habitat in AK?
Bowhead Whale	<i>Balaena mysticetus</i>	Endangered	No
Sei Whale	<i>Balaenoptera borealis</i>	Endangered	No
Blue Whale	<i>Balaenoptera musculus</i>	Endangered	No
Fin Whale	<i>Balaenoptera physalus</i>	Endangered	No
Cook Inlet DPS Beluga Whale	<i>Delphinapterus leucas</i>	Endangered	Yes
Western North Pacific DPS Gray Whale	<i>Eschrichtius robustus</i>	Endangered	No
North Pacific Right Whale	<i>Eubalaena japonica</i>	Endangered	Yes
Mexico DPS Humpback Whale	<i>Megaptera novaeangliae</i>	Threatened	Yes
Western North Pacific DPS Humpback Whale	<i>Megaptera novaeangliae</i>	Endangered	Yes
Sperm Whale	<i>Physeter macrocephalus</i>	Endangered	No

Pinnipeds

Common Name	Scientific Name	Status	Critical Habitat in AK?
Arctic Ringed Seal	<i>Phoca hispida hispida</i>	Threatened	Yes
Beringia DPS, Bearded Seal	<i>Erignathus barbatus nauticus</i>	Threatened	Yes
Western DPS Steller Sea Lion	<i>Eumetopias jubatus</i>	Endangered	Yes

Reptiles

Common Name	Scientific Name	Status	Critical Habitat in AK?
Loggerhead Sea Turtle	<i>Caretta caretta</i>	Threatened	No
Green Sea Turtle	<i>Chelonia mydas</i>	Threatened	No
Leatherback Sea Turtle	<i>Dermochelys coriacea</i>	Endangered	No
Olive Ridley Sea Turtle	<i>Lepidochelys olivacea</i>	Threatened	No

Fish

Common Name	Scientific Name	Status	Critical Habitat in AK?
Green Sturgeon (Southern DPS)*	<i>Acipenser medirostris</i>	Threatened	No

Hood Canal Summer-run Chum Salmon*	<i>Oncorhynchus keta</i>	Threatened	No
Lower Columbia River Coho Salmon*	<i>Oncorhynchus kisutch</i>	Threatened	No
Lower Columbia River Steelhead*	<i>Oncorhynchus mykiss</i>	Threatened	No
Middle Columbia River Steelhead*	<i>Oncorhynchus mykiss</i>	Threatened	No
Snake River Basin Steelhead*	<i>Oncorhynchus mykiss</i>	Threatened	No
Upper Columbia River Steelhead*	<i>Oncorhynchus mykiss</i>	Threatened	No
Upper Willamette River Steelhead*	<i>Oncorhynchus mykiss</i>	Threatened	No
Snake River Sockeye Salmon*	<i>Oncorhynchus nerka</i>	Endangered	No
Lower Columbia River Chinook Salmon*	<i>Oncorhynchus tshawytscha</i>	Threatened	No
Puget Sound Chinook Salmon*	<i>Oncorhynchus tshawytscha</i>	Threatened	No
Snake River Fall Chinook Salmon*	<i>Oncorhynchus tshawytscha</i>	Threatened	No
Snake River Spring/Summer-run Chinook Salmon*	<i>Oncorhynchus tshawytscha</i>	Threatened	No
Upper Columbia River Spring Chinook Salmon*	<i>Oncorhynchus tshawytscha</i>	Endangered	No
Upper Willamette River Chinook Salmon*	<i>Oncorhynchus tshawytscha</i>	Threatened	No

*These species spawn on the West Coast of the Lower 48, but may occur in Alaskan waters during the marine phase of their life cycles

Marine Invertebrates

Common Name	Scientific Name	Status	Critical Habitat in AK?
Sunflower sea star	<i>Pycnopodia helianthoides</i>	Proposed Threatened	No

More Information

- › [ESA Species Directory \(searchable\)](#)
- › [Section 7 Consultations in Alaska](#)
- › [Expedited Informal Consultation Process in Alaska](#)
- › [Endangered Species Directory](#)
- › [ESA Petition and Listing Process](#)
- › [Endangered Species Conservation](#)

Last updated by [Alaska Regional Office](#) on 01/10/2024

Attachment C
Species Observed

Species Observed During the Biological Survey

Common Name	Scientific Name
PLANTS	
Yarrow	<i>Achillea millefolium</i>
Gray alder	<i>Alnus incana</i>
Green alder	<i>Alnus viridis</i>
Eastern bluestar	<i>Amsonia tabernaemontana</i>
Cow parsley	<i>Anthriscus sylvestris</i>
Lady fern	<i>Athyrium filix</i>
Yellow birch	<i>Betula alleghaniensis</i>
Black birch	<i>Betula lenta</i>
Red birch	<i>Betula occidentalis</i>
Paper birch	<i>Betula papyrifera</i>
Canadian bluejoint	<i>Calamagrostis canadensis</i>
Fireweed	<i>Chamaenerion angustifolium</i>
Bunchberry dogwood	<i>Cornus canadensis</i>
Meadow horsetail	<i>Equisetum pratense</i>
European ash	<i>fraxinus excelsior</i>
Wintergreen	<i>Gaultheria procumbens</i>
Yellow avens	<i>Geum aleppicum</i>
Cudweed	<i>Gnaphalium spp.</i>
Oak fern	<i>Gymnocarpium dryopteris</i>
Cow parsnip	<i>Heracleum maximum</i>
Oxeye daisy	<i>Leucanthemum vulgare</i>
Devils club	<i>Oplopanax horridus</i>
White spruce	<i>Picea glauca</i>
Red spruce	<i>Picea rubens</i>
Balsam poplar	<i>Populus balsamifera</i>
Black poplar	<i>Populus nigra</i>
Alder buckthorn	<i>Rhamnus frangula</i>
Arctic rattlebox	<i>Rhinanthus minor ssp. groenlandicus</i>
Labrador tea	<i>Rhododendron groenlandicum</i>
Red currant	<i>Ribes rubrum</i>
Prickly rose	<i>Rosa acicularis</i>
Raspberry	<i>Rubus idaeus</i>
Bebbs willow	<i>Salix bebbiana</i>
Willow	<i>Salix sp.</i>
Interrupted clubmoss	<i>Spinulum annotinum</i>
Claspleaf twisted stalk	<i>Streptopus amplexifolius</i>
Dandelion	<i>Taraxacum</i>
White clover	<i>Trifolium repens</i>
Highbush cranberry	<i>Viburnum trilobum</i>

American vetch	<i>Vicia americana</i>
ANIMALS	
Moose	<i>Alces alces</i>
Dog	<i>Canis lupus familiaris</i>
Mosquito	<i>Culicidae sp.</i>
Dark-eyed junco	<i>Junco hyemalis</i>
American robin	<i>Turdus migratorius</i>
Black bear	<i>Ursus americanus</i>

Attachment D

Representative Photographs



Representative photo of ruderal/developed habitat with boreal forest in the background.



View of Peters Creek looking upstream from terraced streambank.



View of Peters Creek looking downstream from terraced streambank. Large woody debris observed.



View looking across Peters Creek showing eroded drop off.

Appendix H

Cultural Resources Inventory and
Evaluation

(CONFIDENTIAL – AVAILABLE UPON REQUEST)

SHPO Consultation Request



United States Department of the Interior

BUREAU OF INDIAN AFFAIRS

Alaska Regional Office
3601 C Street, Suite 1200
Anchorage, Alaska 99503

IN REPLY REFER TO
[DECRM/6200]

8/23/2024

Judith Bittner
State Historic Preservation Officer
Office of History & Archaeology
550 West 7th Avenue
Anchorage, AK 99501

Dear Ms. Bittner:

The Bureau of Indian Affairs, Alaska Region (BIA) received a request for the development of the Eklutna Native Village Gaming Facility Project on the Olga Ondola's Alaska Native Allotment (AA-55026). The survey and report were conducted by Mike Taggart, RPA, from Acorn Environmental, and submitted to the BIA Archeology department for assessment. The BIA Archeology Department has reviewed the report, and determined that it was sufficient, and support the conclusion that the Undertaking would have a No Historic Properties Affected [36 CFR §800.4(d)(1)].

Additionally, it has come to the attention of the BIA Archeology Department that the Eklutna Tribe has begun work on the project location and has cleared land and conducted some ground disturbance work for preparation of the proposed construction. BIA Forestry visited the property and gave the BIA Archeology department photographs and GIS data to show the extent of the disturbance (Attachment 2). While this work has begun prior to your Concurrence or a NEPA evaluation, however office suggests that there likely was no damage to cultural resources as the report was completed and submitted to our office prior to the work.

With our department determining that the Eklutna Native Village Gaming Facility Project on the Olga Ondola's Alaska Native Allotment (AA-55026) will result in a **No Historic Properties Affected**, and we request concurrence with your office. If you have any further questions or concerns, feel free to contact our office at joseph.sparaga@bia.gov, or at (907) 271-4003.

Sincerely,

Joseph Sparaga
Regional Archeologist

Appendix I

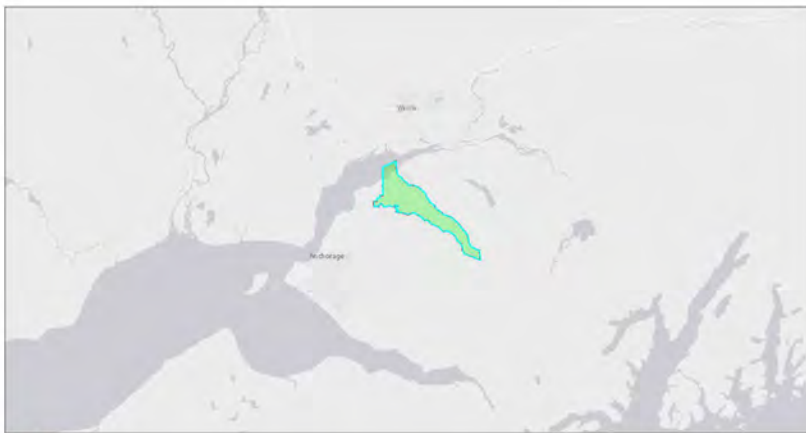
EJScreen Community Report

EJScreen Community Report

This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

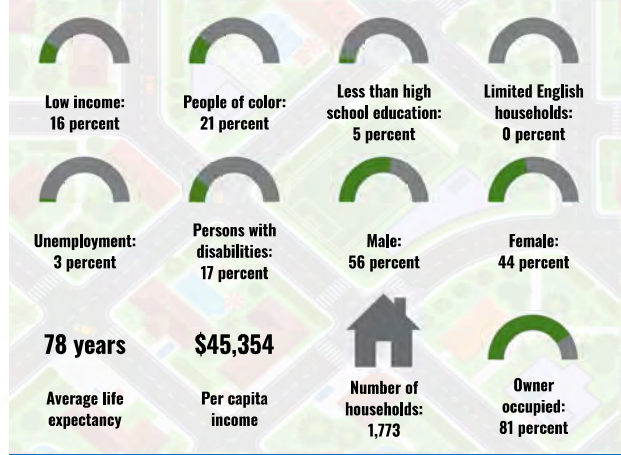
Anchorage, AK

Tract: 02020000102
 Population: 4,323
 Area in square miles: 70.90



August 7, 2024
 Project 1
 1:1,155,581
 0 5 10 20 40 km
Map: HERE, NPS, Esri, HERE, DeLorme, USGS, EPA

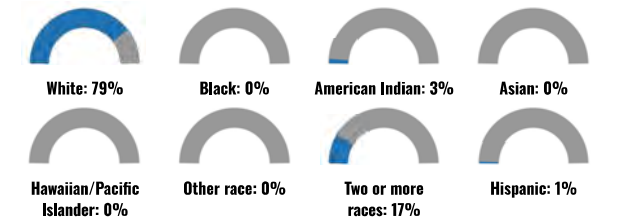
COMMUNITY INFORMATION



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	100%

BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

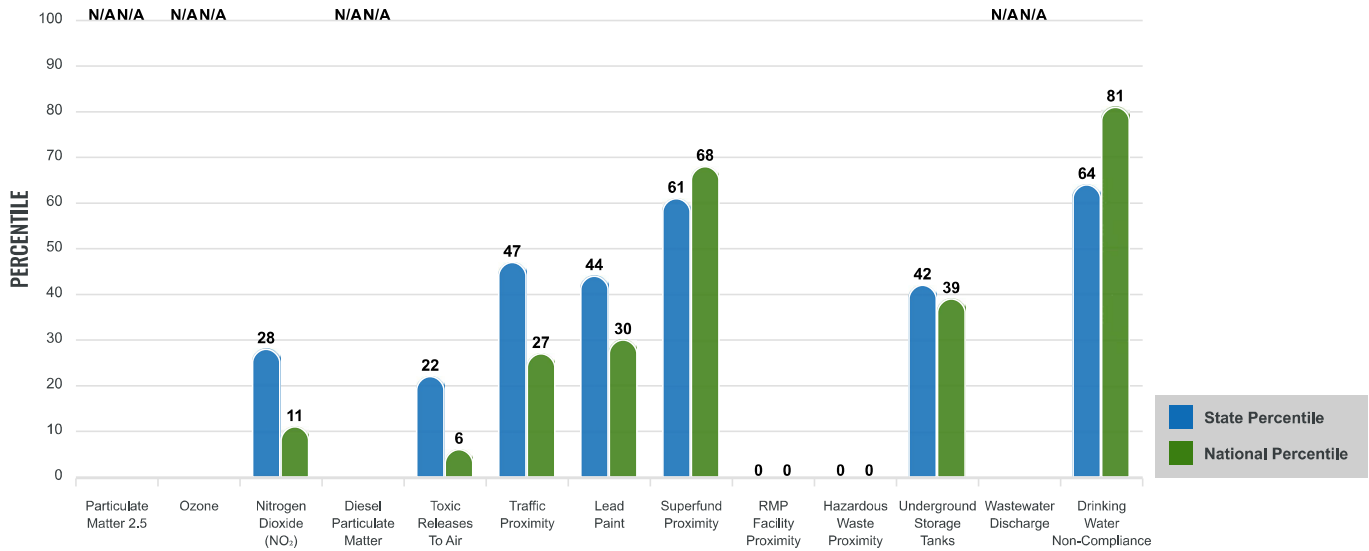
Environmental Justice & Supplemental Indexes

The environmental justice and supplemental indexes are a combination of environmental and socioeconomic information. There are thirteen EJ indexes and supplemental indexes in EJScreen reflecting the 13 environmental indicators. The indexes for a selected area are compared to those for all other locations in the state or nation. For more information and calculation details on the EJ and supplemental indexes, please visit the [EJScreen website](#).

EJ INDEXES

The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator.

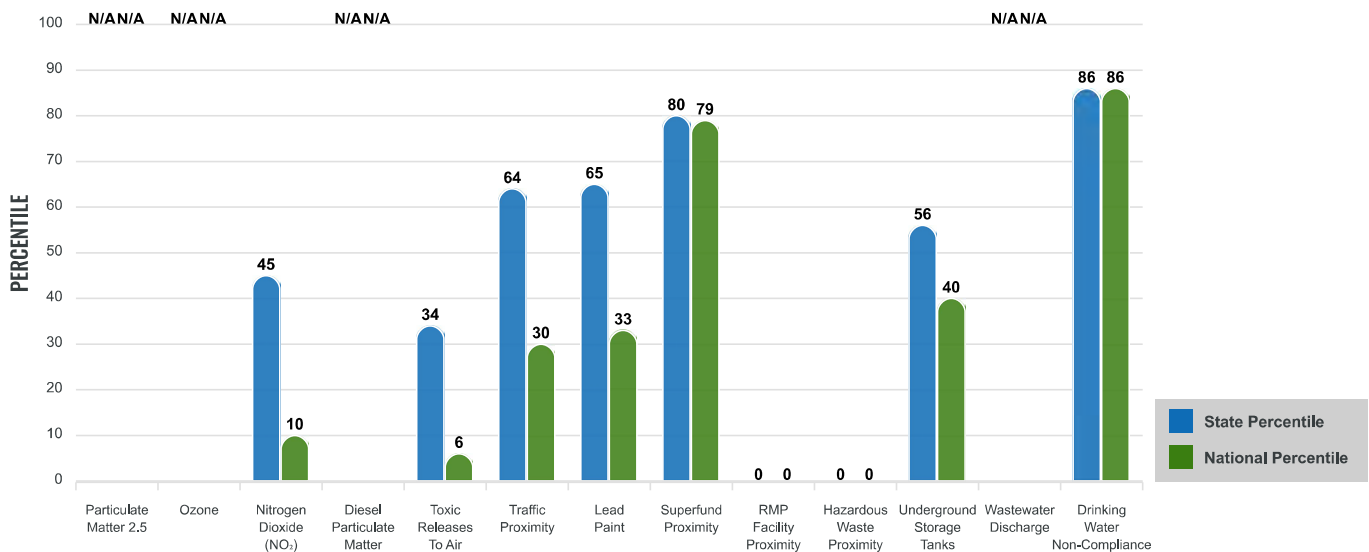
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020000102

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO ₂) (ppbv)	3	5.6	34	7.8	6
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	3	76	26	4,600	6
Traffic Proximity (daily traffic count/distance to road)	250,000	590,000	55	1,700,000	28
Lead Paint (% Pre-1960 Housing)	0.084	0.087	69	0.3	33
Superfund Proximity (site count/km distance)	0.47	0.57	78	0.39	82
RMP Facility Proximity (facility count/km distance)	0	0.32	0	0.57	0
Hazardous Waste Proximity (facility count/km distance)	0	0.21	0	3.5	0
Underground Storage Tanks (count/km ²)	0.21	2.7	48	3.6	37
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	14	5.4	92	2.2	97
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.71	N/A	N/A	1.34	27
Supplemental Demographic Index USA	1.45	N/A	N/A	1.64	44
Demographic Index State	0.93	1.65	18	N/A	N/A
Supplemental Demographic Index State	1.44	1.48	55	N/A	N/A
People of Color	21%	43%	20	40%	38
Low Income	16%	25%	31	30%	30
Unemployment Rate	3%	7%	31	6%	43
Limited English Speaking Households	0%	2%	0	5%	0
Less Than High School Education	5%	7%	51	11%	39
Under Age 5	4%	6%	32	5%	42
Over Age 64	18%	14%	76	18%	59

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	1
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	25
Air Pollution	0
Brownfields	1
Toxic Release Inventory	0

Other community features within defined area:

Schools	4
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	No
Impaired Waters	No

Selected location contains American Indian Reservation Lands*	Yes
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	Yes

Report for Tract: 02020000102

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	20%	19%	65	20%	60
Heart Disease	4.3	4.7	44	5.8	22
Asthma	9.5	10	26	10.3	28
Cancer	5.9	5.5	67	6.4	38
Persons with Disabilities	16.6%	13.2%	80	13.7%	72

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	3%	18%	20	12%	27
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	5%	12%	32	13%	31
Lack of Health Insurance	13%	12%	60	9%	80
Housing Burden	No	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	Yes	N/A	N/A	N/A	N/A

Report for Tract: 02020000102

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

Knik-Fairview, AK

Tract: 02170000601

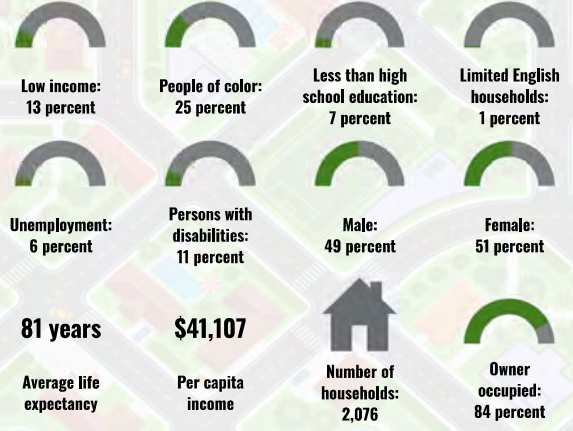
Population: 6,216

Area in square miles: 65.53

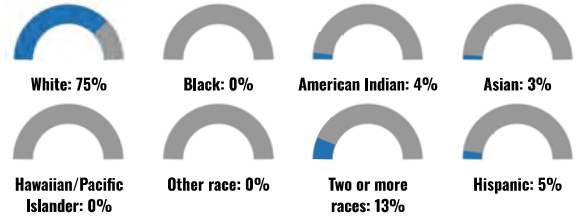


August 7, 2024
 Project 1
 1:1,155,581
 0 5 10 20 40 miles
 Esri, HERE, DeLorme, Swatch, USGS, IGN, GEBCO, GEBCO

COMMUNITY INFORMATION



BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	90%
Spanish	2%
German or other West Germanic	2%
Korean	1%
Tagalog (including Filipino)	2%
Other Asian and Pacific Island	2%
Arabic	1%
Other and Unspecified	1%
Total Non-English	10%

Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

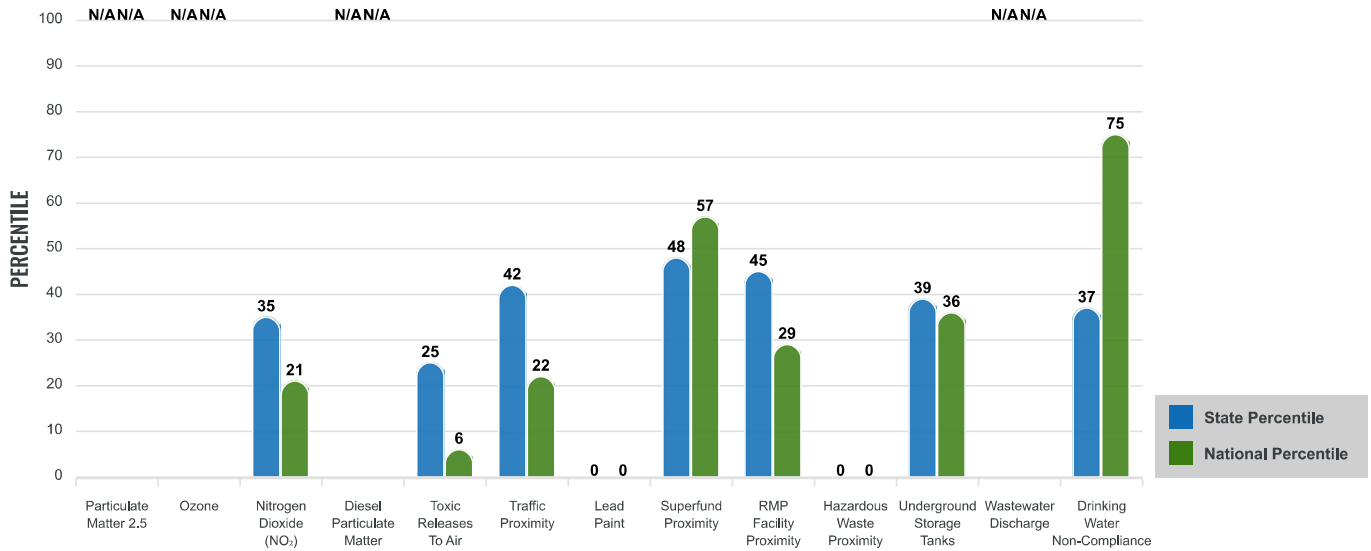
Environmental Justice & Supplemental Indexes

The environmental justice and supplemental indexes are a combination of environmental and socioeconomic information. There are thirteen EJ indexes and supplemental indexes in EJScreen reflecting the 13 environmental indicators. The indexes for a selected area are compared to those for all other locations in the state or nation. For more information and calculation details on the EJ and supplemental indexes, please visit the [EJScreen website](#).

EJ INDEXES

The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator.

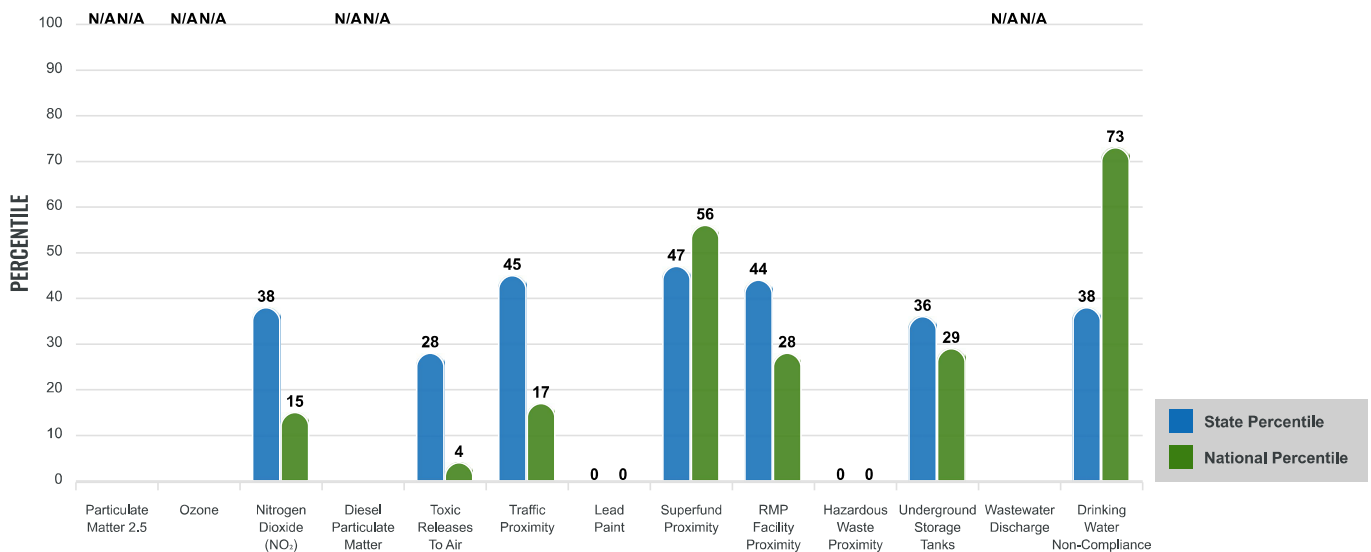
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02170000601

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO ₂) (ppbv)	4.5	5.6	42	7.8	18
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	3.8	76	33	4,600	6
Traffic Proximity (daily traffic count/distance to road)	160,000	590,000	47	1,700,000	22
Lead Paint (% Pre-1960 Housing)	0	0.087	0	0.3	0
Superfund Proximity (site count/km distance)	0.021	0.57	48	0.39	56
RMP Facility Proximity (facility count/km distance)	0.024	0.32	45	0.57	28
Hazardous Waste Proximity (facility count/km distance)	0	0.21	0	3.5	0
Underground Storage Tanks (count/km ²)	0.21	2.7	48	3.6	37
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	1.5	5.4	43	2.2	84
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.71	N/A	N/A	1.34	26
Supplemental Demographic Index USA	1.11	N/A	N/A	1.64	22
Demographic Index State	0.92	1.65	18	N/A	N/A
Supplemental Demographic Index State	1.02	1.48	23	N/A	N/A
People of Color	25%	43%	28	40%	43
Low Income	13%	25%	25	30%	24
Unemployment Rate	6%	7%	55	6%	63
Limited English Speaking Households	1%	2%	61	5%	57
Less Than High School Education	7%	7%	59	11%	44
Under Age 5	8%	6%	72	5%	78
Over Age 64	12%	14%	45	18%	33

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	0
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	14
Air Pollution	0
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	1
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	No
Impaired Waters	Yes

Selected location contains American Indian Reservation Lands*	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	No

Report for Tract: 02170000601

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	17%	19%	30	20%	28
Heart Disease	4.2	4.7	41	5.8	20
Asthma	9.7	10	41	10.3	36
Cancer	5.8	5.5	62	6.4	34
Persons with Disabilities	10.7%	13.2%	27	13.7%	35

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	13%	18%	63	12%	74
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	7%	12%	39	13%	38
Lack of Health Insurance	10%	12%	39	9%	69
Housing Burden	Yes	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	Yes	N/A	N/A	N/A	N/A

Report for Tract: 02170000601

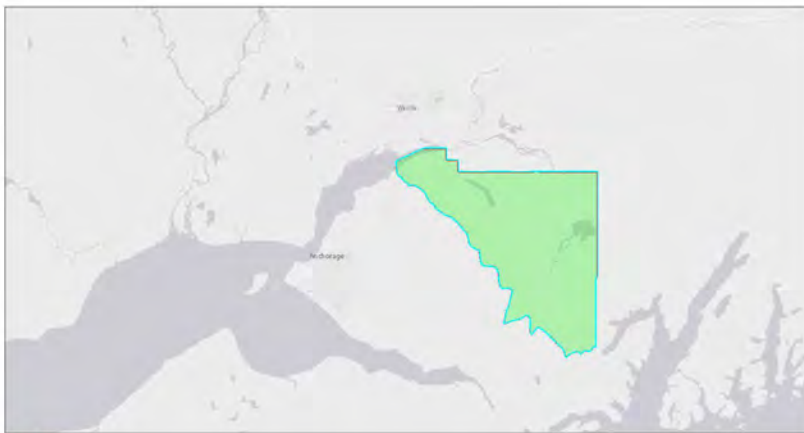
Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

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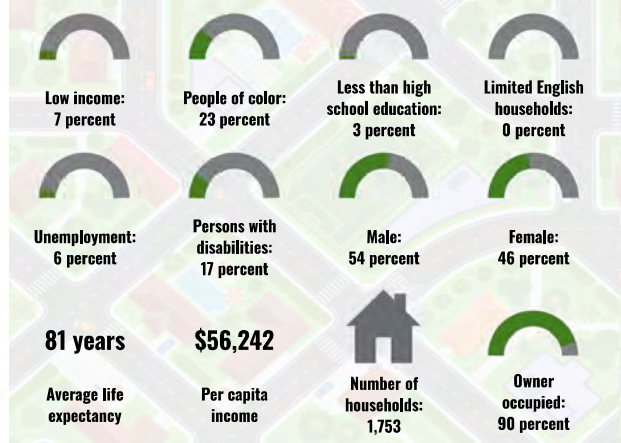
Anchorage, AK

Tract: 02020000101
 Population: 5,370
 Area in square miles: 644.10

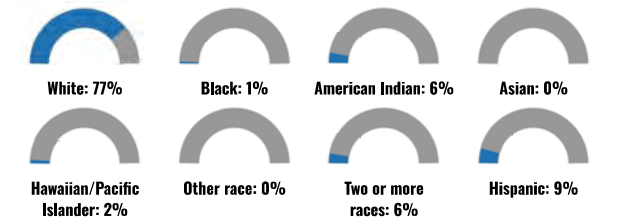


August 7, 2024
 Project 1
 1:1,155,581
 0 5 10 20 40 km
 Esri, HERE, DeLorme, Swire, USGS, IGN, GEBCO, Esri, Google, Swire, Swire

COMMUNITY INFORMATION



BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	91%
Spanish	6%
German or other West Germanic	1%
Other and Unspecified	2%
Total Non-English	9%

Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

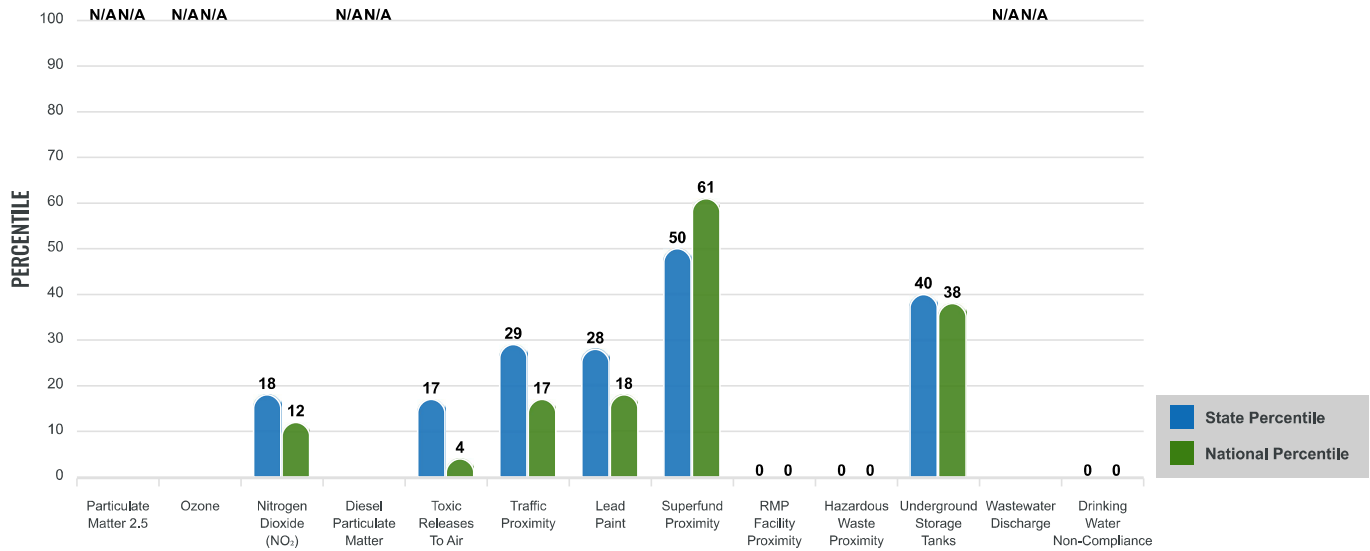
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EJ INDEXES

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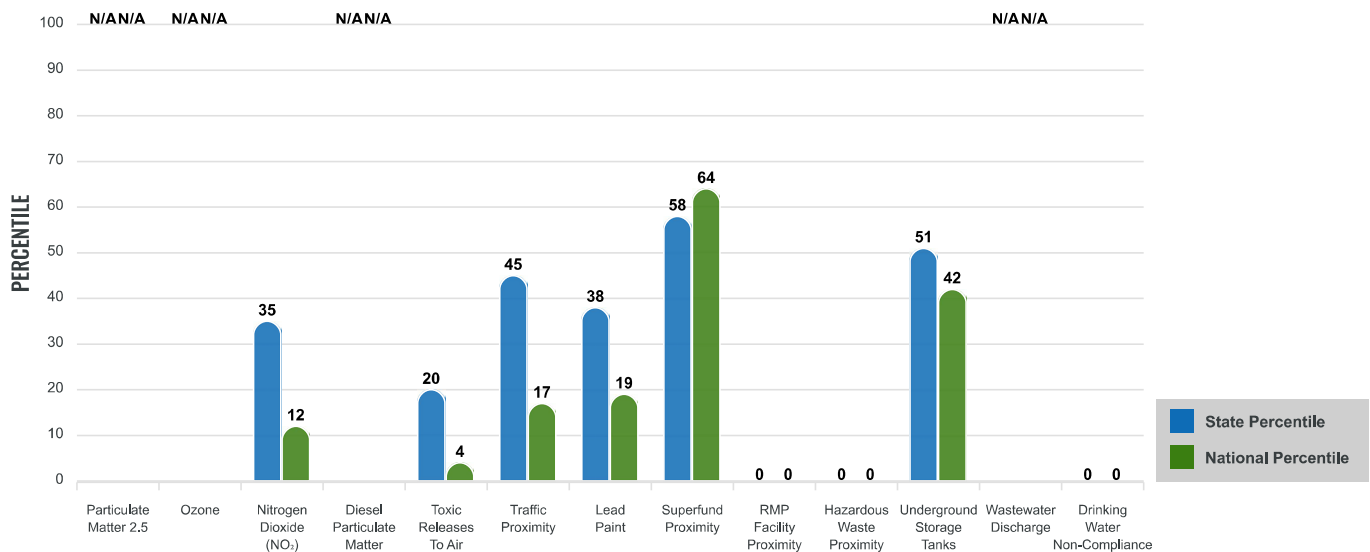
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020000101

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO ₂) (ppbv)	3.4	5.6	35	7.8	9
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	2.2	76	24	4,600	5
Traffic Proximity (daily traffic count/distance to road)	140,000	590,000	45	1,700,000	21
Lead Paint (% Pre-1960 Housing)	0.025	0.087	40	0.3	20
Superfund Proximity (site count/km distance)	0.14	0.57	56	0.39	65
RMP Facility Proximity (facility count/km distance)	0	0.32	0	0.57	0
Hazardous Waste Proximity (facility count/km distance)	0	0.21	0	3.5	0
Underground Storage Tanks (count/km ²)	1.3	2.7	64	3.6	56
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	0	5.4	0	2.2	0
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.52	N/A	N/A	1.34	16
Supplemental Demographic Index USA	1.15	N/A	N/A	1.64	25
Demographic Index State	0.68	1.65	8	N/A	N/A
Supplemental Demographic Index State	1.06	1.48	26	N/A	N/A
People of Color	23%	43%	24	40%	41
Low Income	7%	25%	11	30%	11
Unemployment Rate	7%	7%	62	6%	69
Limited English Speaking Households	0%	2%	59	5%	56
Less Than High School Education	3%	7%	26	11%	23
Under Age 5	7%	6%	63	5%	69
Over Age 64	11%	14%	43	18%	30

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	0
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	5
Air Pollution	0
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	1
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	No
Impaired Waters	No

Selected location contains American Indian Reservation Lands*	Yes
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	Yes

Report for Tract: 02020000101

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	17%	19%	17	20%	23
Heart Disease	3.8	4.7	19	5.8	13
Asthma	9.3	10	15	10.3	23
Cancer	5.5	5.5	51	6.4	30
Persons with Disabilities	17.2%	13.2%	84	13.7%	75

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	4%	18%	30	12%	37
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	7%	12%	40	13%	39
Lack of Health Insurance	8%	12%	24	9%	58
Housing Burden	No	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	No	N/A	N/A	N/A	N/A

Report for Tract: 02020000101

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

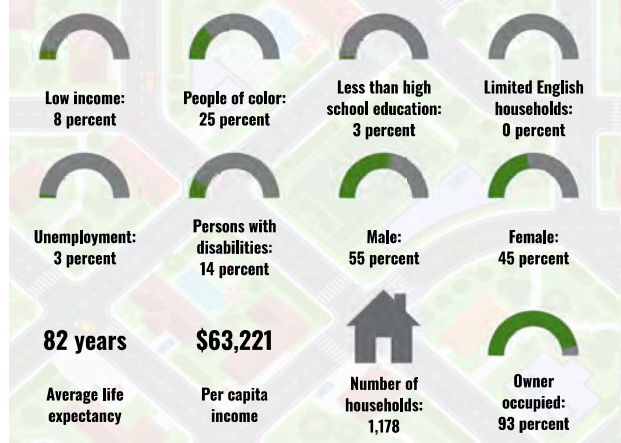
Anchorage, AK

Tract: 02020000204
 Population: 3,450
 Area in square miles: 197.73

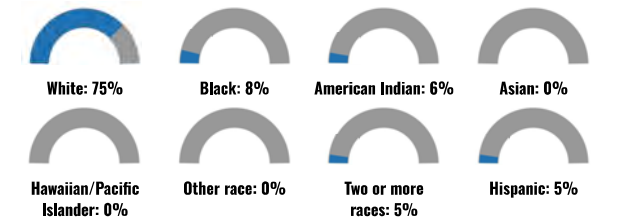


August 7, 2024
 Project 1
 1:1,155,581
 0 5 10 20 40 mi
 0 5 10 20 40 km
Map: HERE, NPS, Esri, HERE, DeLorme, USGS, Swis, SNG, SVA

COMMUNITY INFORMATION



BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	96%
Spanish	3%
Russian, Polish, or Other Slavic	1%
Other and Unspecified	1%
Total Non-English	4%

Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

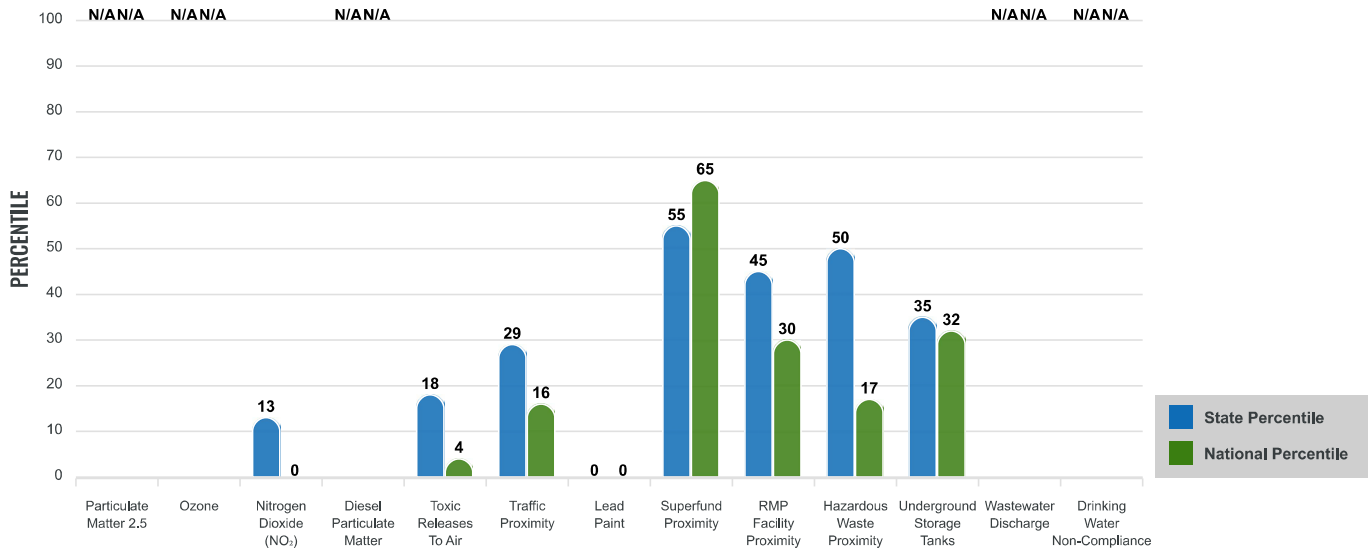
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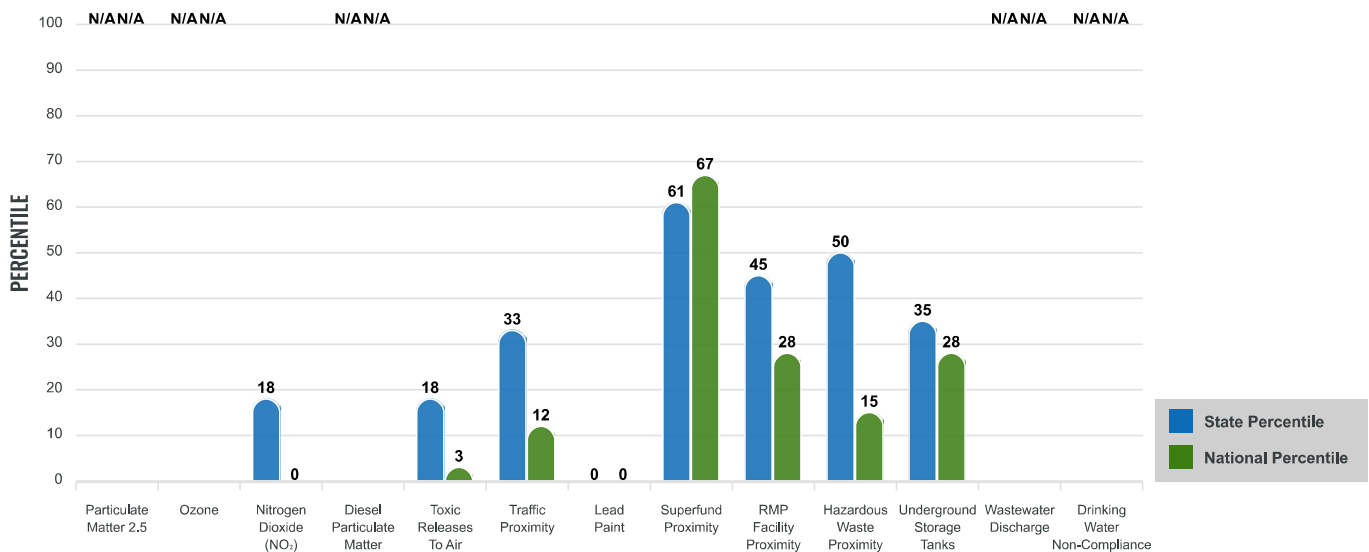
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020000204

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO ₂) (ppbv)	1.1	5.6	24	7.8	0
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	2.3	76	24	4,600	5
Traffic Proximity (daily traffic count/distance to road)	110,000	590,000	40	1,700,000	18
Lead Paint (% Pre-1960 Housing)	0	0.087	0	0.3	0
Superfund Proximity (site count/km distance)	0.57	0.57	82	0.39	85
RMP Facility Proximity (facility count/km distance)	0.041	0.32	45	0.57	28
Hazardous Waste Proximity (facility count/km distance)	0.037	0.21	51	3.5	15
Underground Storage Tanks (count/km ²)	0.03	2.7	39	3.6	29
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	N/A	5.4	N/A	2.2	N/A
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.57	N/A	N/A	1.34	19
Supplemental Demographic Index USA	1.05	N/A	N/A	1.64	19
Demographic Index State	0.74	1.65	10	N/A	N/A
Supplemental Demographic Index State	0.94	1.48	17	N/A	N/A
People of Color	25%	43%	28	40%	43
Low Income	8%	25%	12	30%	13
Unemployment Rate	4%	7%	36	6%	49
Limited English Speaking Households	0%	2%	0	5%	0
Less Than High School Education	3%	7%	27	11%	24
Under Age 5	4%	6%	37	5%	48
Over Age 64	10%	14%	30	18%	23

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	1
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	23
Air Pollution	1
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	1
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	No
Impaired Waters	No

Selected location contains American Indian Reservation Lands*	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	No

Report for Tract: 02020000204

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	16%	19%	15	20%	21
Heart Disease	3.3	4.7	5	5.8	7
Asthma	9.6	10	34	10.3	31
Cancer	5.3	5.5	42	6.4	26
Persons with Disabilities	14.3%	13.2%	65	13.7%	59

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	2%	18%	16	12%	24
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	6%	12%	36	13%	34
Lack of Health Insurance	5%	12%	5	9%	35
Housing Burden	Yes	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	Yes	N/A	N/A	N/A	N/A

Report for Tract: 02020000204

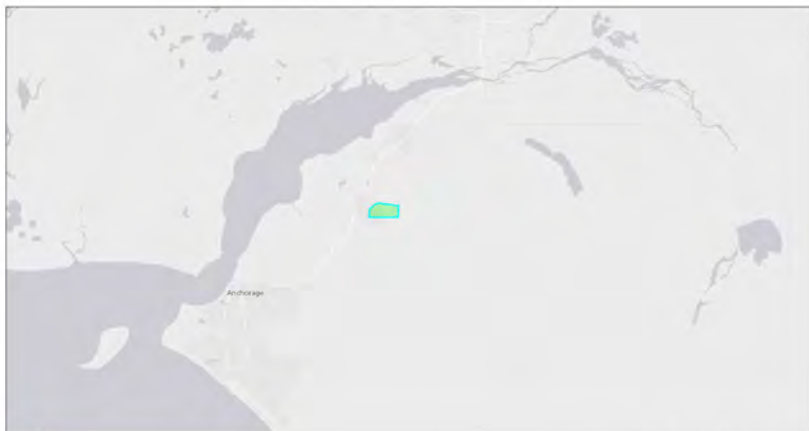
Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

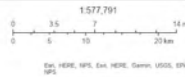
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Anchorage, AK

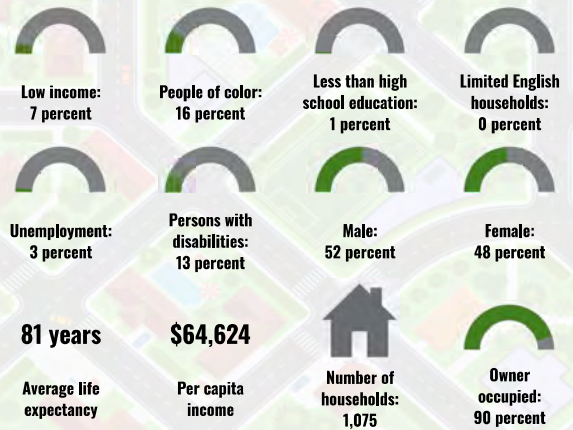
Tract: 02020000206
 Population: 3,137
 Area in square miles: 2.60



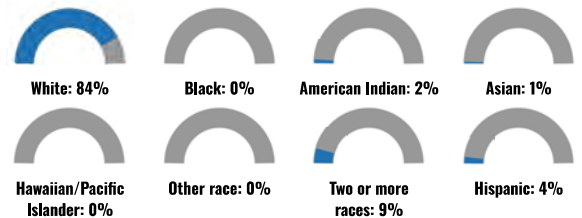
August 7, 2024
 Project 3



COMMUNITY INFORMATION



BREAKDOWN BY RACE



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	95%
Spanish	4%
Total Non-English	5%

BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

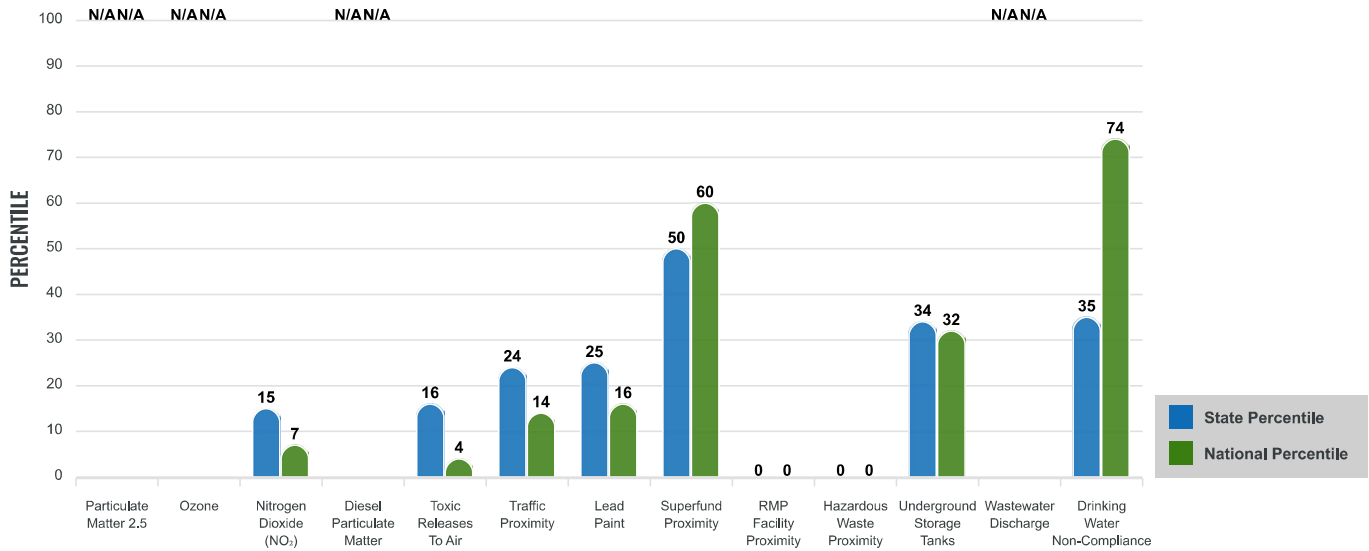
Environmental Justice & Supplemental Indexes

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EJ INDEXES

The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator.

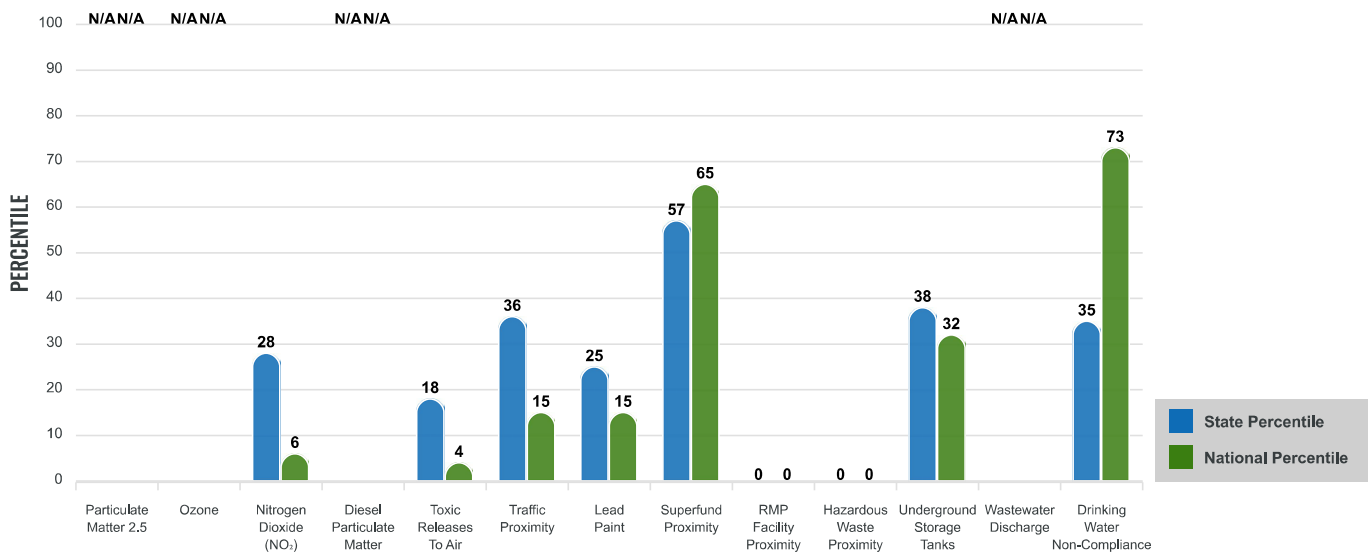
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020000206

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO ₂) (ppbv)	3.4	5.6	35	7.8	9
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	3.3	76	28	4,600	6
Traffic Proximity (daily traffic count/distance to road)	160,000	590,000	47	1,700,000	22
Lead Paint (% Pre-1960 Housing)	0.011	0.087	30	0.3	16
Superfund Proximity (site count/km distance)	0.29	0.57	69	0.39	76
RMP Facility Proximity (facility count/km distance)	0	0.32	0	0.57	0
Hazardous Waste Proximity (facility count/km distance)	0	0.21	0	3.5	0
Underground Storage Tanks (count/km ²)	0.33	2.7	51	3.6	40
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	0.41	5.4	37	2.2	75
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.41	N/A	N/A	1.34	10
Supplemental Demographic Index USA	1	N/A	N/A	1.64	16
Demographic Index State	0.54	1.65	3	N/A	N/A
Supplemental Demographic Index State	0.84	1.48	11	N/A	N/A
People of Color	16%	43%	9	40%	31
Low Income	7%	25%	11	30%	12
Unemployment Rate	3%	7%	31	6%	44
Limited English Speaking Households	0%	2%	0	5%	0
Less Than High School Education	1%	7%	14	11%	13
Under Age 5	8%	6%	72	5%	77
Over Age 64	14%	14%	54	18%	42

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	0
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	3
Air Pollution	0
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	0
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	Yes
Impaired Waters	No

Selected location contains American Indian Reservation Lands*	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	No

Report for Tract: 02020000206

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	17%	19%	24	20%	26
Heart Disease	3.2	4.7	3	5.8	6
Asthma	9.2	10	12	10.3	20
Cancer	5	5.5	34	6.4	22
Persons with Disabilities	12.9%	13.2%	50	13.7%	50

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	5%	18%	36	12%	42
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	4%	12%	27	13%	27
Lack of Health Insurance	2%	12%	1	9%	13
Housing Burden	No	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	No	N/A	N/A	N/A	N/A

Report for Tract: 02020000206

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

Anchorage, AK

Tract: 02020000202

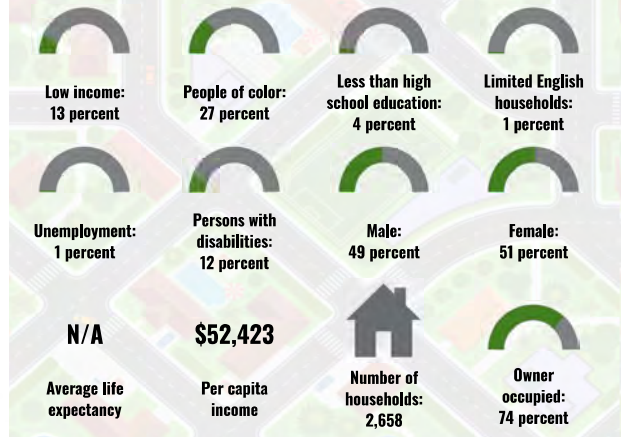
Population: 7,097

Area in square miles: 3.55

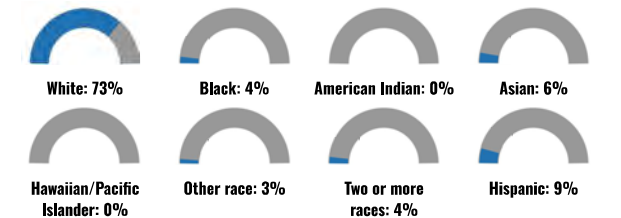


August 7, 2024
Project 1
1:577,791
0 3.5 7 14 mi
0 5 10 20 km
Sat. HERE, HERE, Sat. HERE, Sat. HERE, UCC, OPR, SRS

COMMUNITY INFORMATION



BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	91%
Spanish	3%
German or other West Germanic	1%
Korean	2%
Tagalog (including Filipino)	1%
Other Asian and Pacific Island	1%
Total Non-English	9%

Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

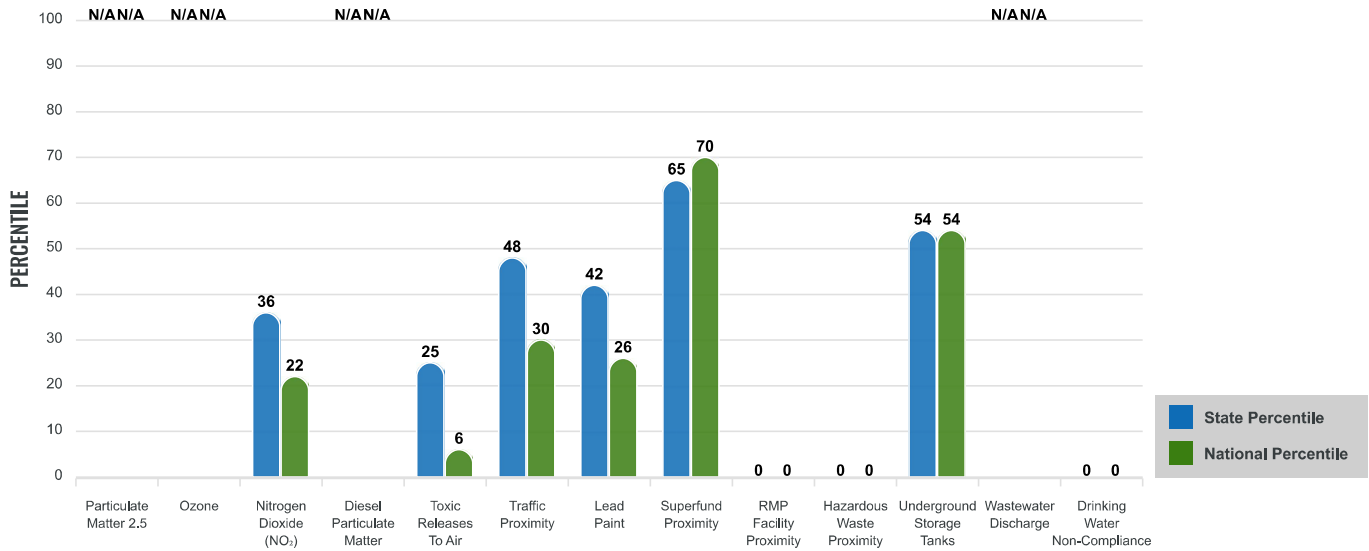
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EJ INDEXES

The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator.

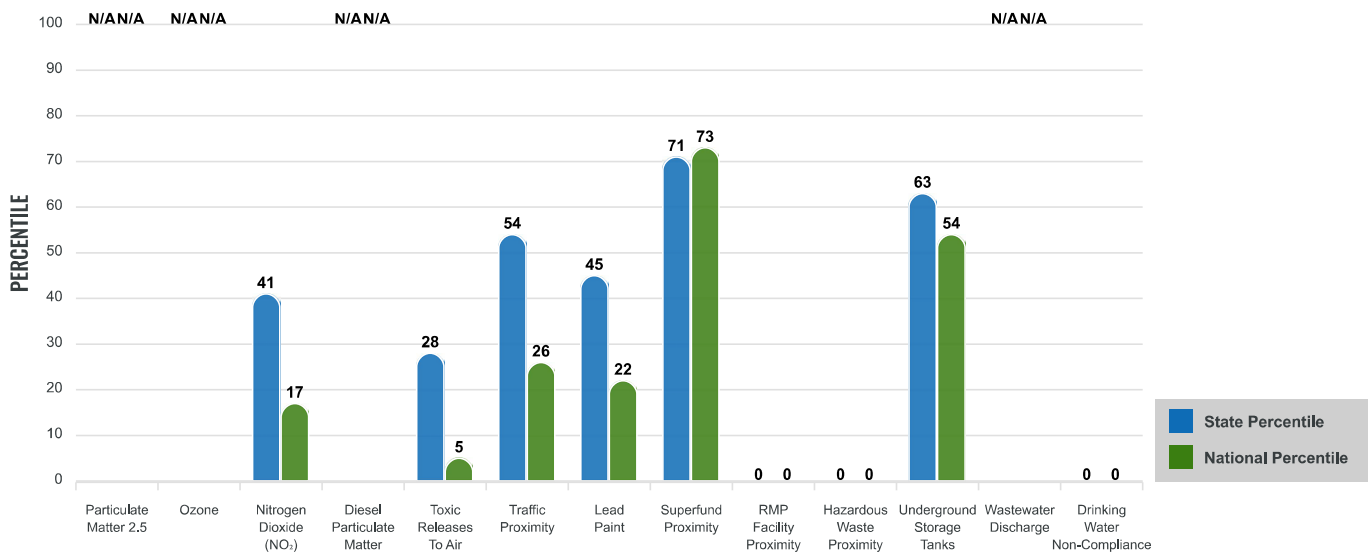
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020000202

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO ₂) (ppbv)	4.5	5.6	42	7.8	19
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	3.6	76	31	4,600	6
Traffic Proximity (daily traffic count/distance to road)	300,000	590,000	57	1,700,000	30
Lead Paint (% Pre-1960 Housing)	0.047	0.087	55	0.3	26
Superfund Proximity (site count/km distance)	0.74	0.57	85	0.39	88
RMP Facility Proximity (facility count/km distance)	0	0.32	0	0.57	0
Hazardous Waste Proximity (facility count/km distance)	0	0.21	0	3.5	0
Underground Storage Tanks (count/km ²)	2.6	2.7	73	3.6	67
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	0	5.4	0	2.2	0
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.72	N/A	N/A	1.34	27
Supplemental Demographic Index USA	1.14	N/A	N/A	1.64	25
Demographic Index State	0.94	1.65	19	N/A	N/A
Supplemental Demographic Index State	1.07	1.48	26	N/A	N/A
People of Color	27%	43%	31	40%	45
Low Income	13%	25%	24	30%	23
Unemployment Rate	1%	7%	18	6%	24
Limited English Speaking Households	1%	2%	65	5%	59
Less Than High School Education	4%	7%	39	11%	32
Under Age 5	4%	6%	36	5%	46
Over Age 64	15%	14%	60	18%	46

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	0
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	10
Air Pollution	0
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	2
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	Yes
Impaired Waters	No

Selected location contains American Indian Reservation Lands*	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	No

Report for Tract: 02020000202

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	N/A	19%	N/A	20%	N/A
Heart Disease	3.7	4.7	14	5.8	12
Asthma	9.4	10	20	10.3	27
Cancer	5.5	5.5	51	6.4	30
Persons with Disabilities	12.4%	13.2%	47	13.7%	47

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	9%	18%	52	12%	60
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	3%	12%	23	13%	23
Lack of Health Insurance	7%	12%	20	9%	55
Housing Burden	No	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	No	N/A	N/A	N/A	N/A

Report for Tract: 02020000202

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

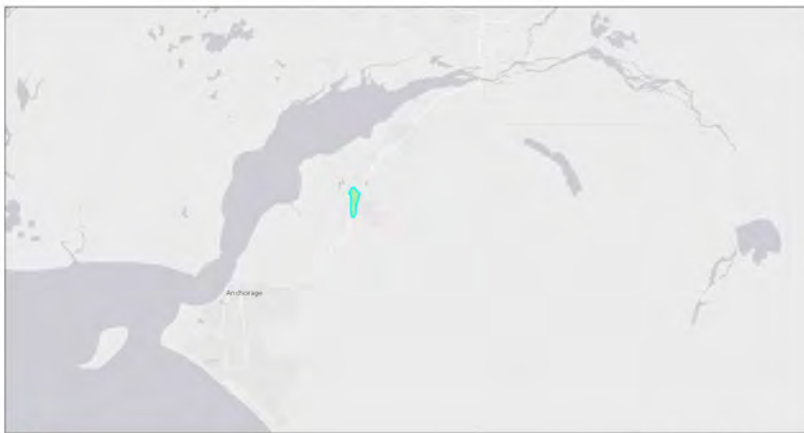
This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

Anchorage, AK

Tract: 02020000201

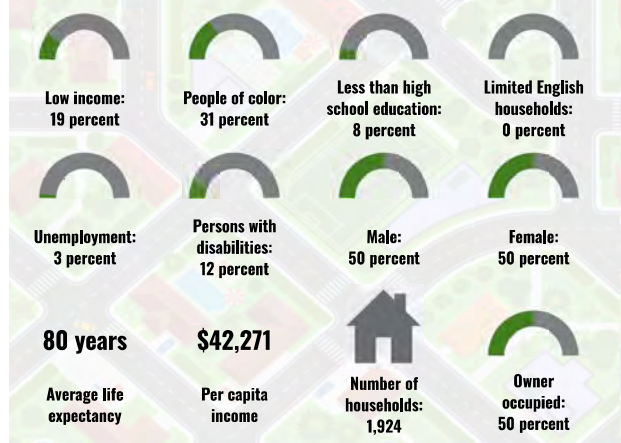
Population: 4,728

Area in square miles: 1.32

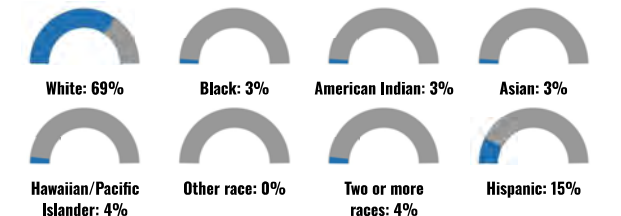


August 7, 2024
Project 3
1:577,791
0 3.5 7 14 mi
0 5 10 20 km
Data: HERE, HERE, Esri, HERE, DeLorme, USGS, EPA, Swisstopo

COMMUNITY INFORMATION



BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	90%
Spanish	5%
French, Haitian, or Cajun	1%
German or other West Germanic	1%
Other Asian and Pacific Island	1%
Other and Unspecified	1%
Total Non-English	10%

Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

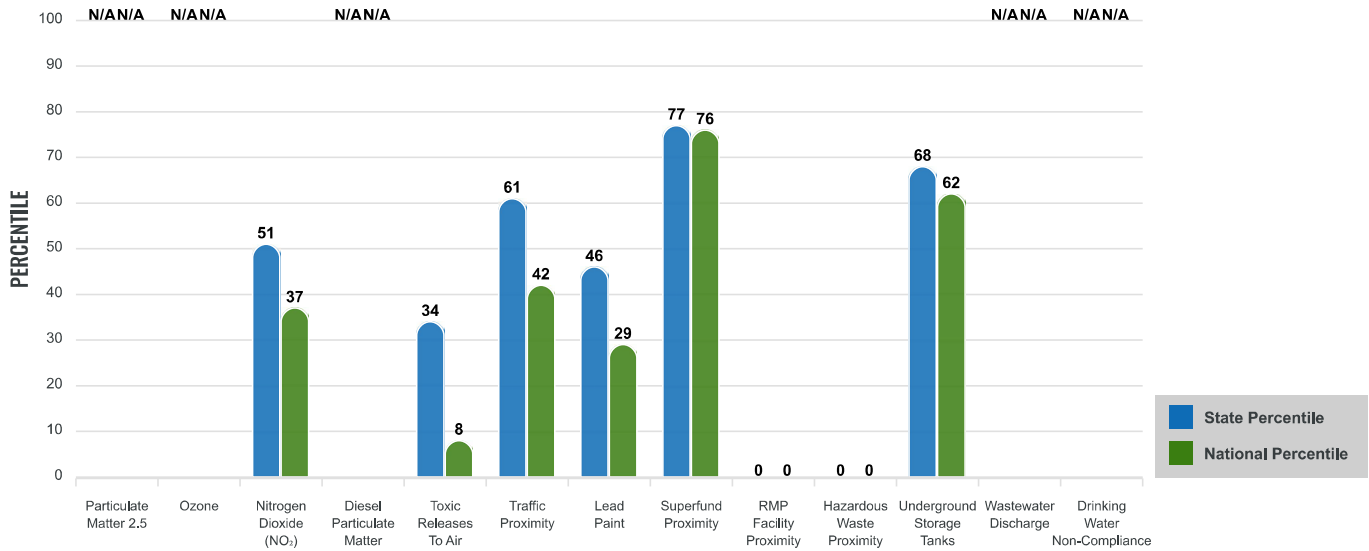
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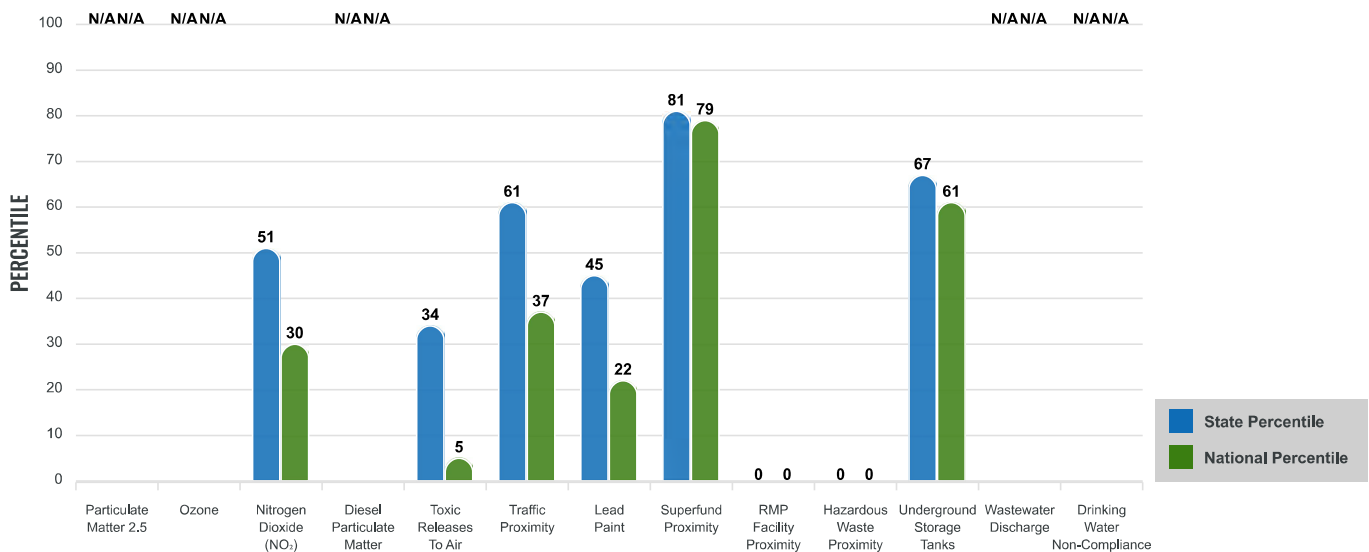
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020000201

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO ₂) (ppbv)	5.8	5.6	50	7.8	31
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	3.8	76	33	4,600	6
Traffic Proximity (daily traffic count/distance to road)	470,000	590,000	61	1,700,000	38
Lead Paint (% Pre-1960 Housing)	0.031	0.087	44	0.3	22
Superfund Proximity (site count/km distance)	3	0.57	94	0.39	97
RMP Facility Proximity (facility count/km distance)	0	0.32	0	0.57	0
Hazardous Waste Proximity (facility count/km distance)	0	0.21	0	3.5	0
Underground Storage Tanks (count/km ²)	3.1	2.7	75	3.6	70
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	N/A	5.4	N/A	2.2	N/A
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.93	N/A	N/A	1.34	39
Supplemental Demographic Index USA	1.24	N/A	N/A	1.64	31
Demographic Index State	1.21	1.65	35	N/A	N/A
Supplemental Demographic Index State	1.19	1.48	34	N/A	N/A
People of Color	31%	43%	38	40%	49
Low Income	19%	25%	40	30%	36
Unemployment Rate	4%	7%	39	6%	52
Limited English Speaking Households	0%	2%	0	5%	0
Less Than High School Education	8%	7%	66	11%	49
Under Age 5	8%	6%	74	5%	78
Over Age 64	7%	14%	19	18%	16

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Sites reporting to EPA within defined area:

Superfund	2
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	8
Air Pollution	1
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	2
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	Yes
Impaired Waters	No

Selected location contains American Indian Reservation Lands*	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	Yes

Report for Tract: 02020000201

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	18%	19%	36	20%	36
Heart Disease	3	4.7	1	5.8	5
Asthma	9.8	10	50	10.3	39
Cancer	4.2	5.5	7	6.4	10
Persons with Disabilities	11.7%	13.2%	38	13.7%	42

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	6%	18%	42	12%	50
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	10%	12%	54	13%	51
Lack of Health Insurance	10%	12%	35	9%	68
Housing Burden	Yes	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	Yes	N/A	N/A	N/A	N/A

Report for Tract: 02020000201

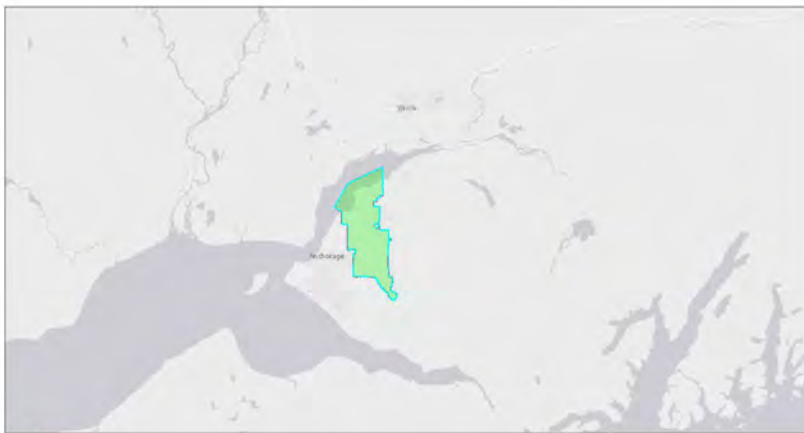
Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

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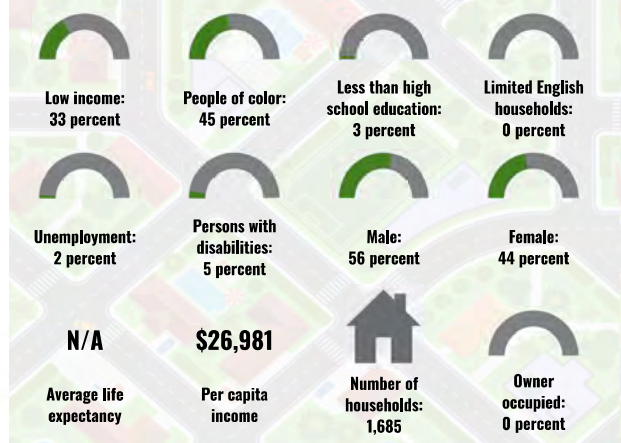
Anchorage, AK

Tract: 02020980200
 Population: 7,857
 Area in square miles: 117.52

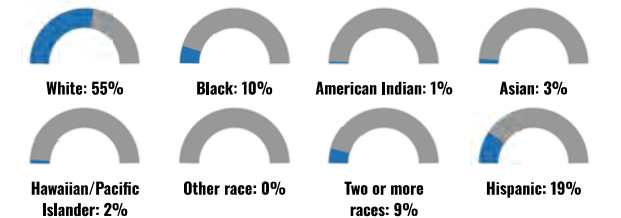


August 7, 2024
 Project 1
 1:1,155,581
 0 5 10 20 40 mi
 0 5 10 20 40 km
Map: HERE, NPS, Esri, HERE, DeLorme, USGS, Swis, SNG

COMMUNITY INFORMATION



BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	85%
Spanish	9%
French, Haitian, or Cajun	1%
Russian, Polish, or Other Slavic	1%
Tagalog (including Filipino)	2%
Other Asian and Pacific Island	1%
Other and Unspecified	1%
Total Non-English	15%

Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

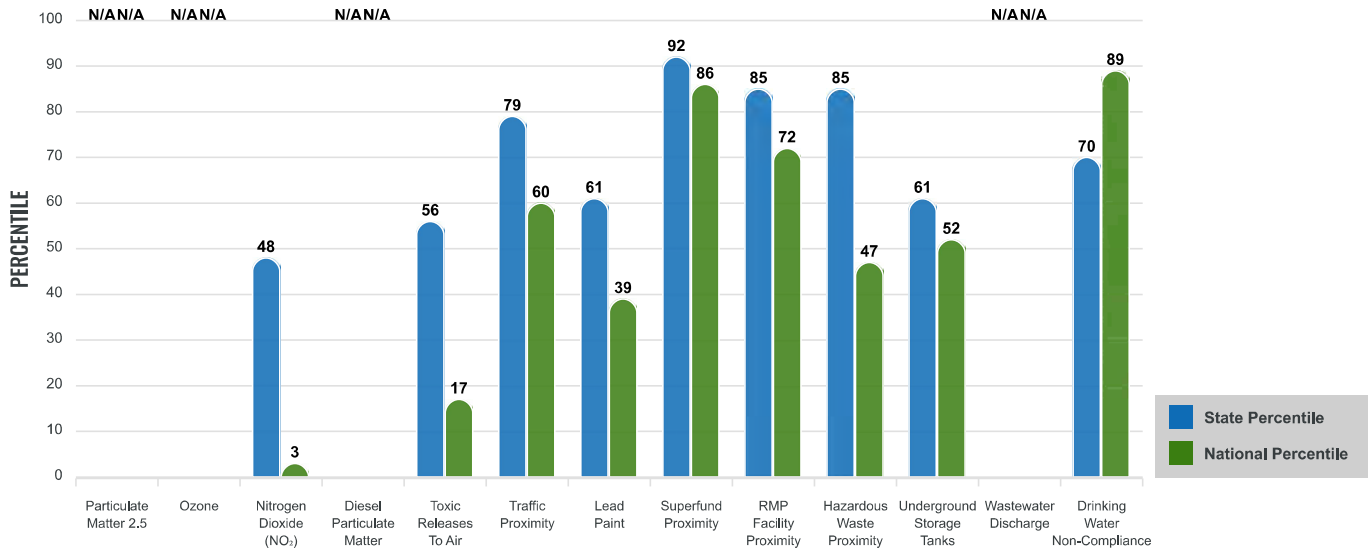
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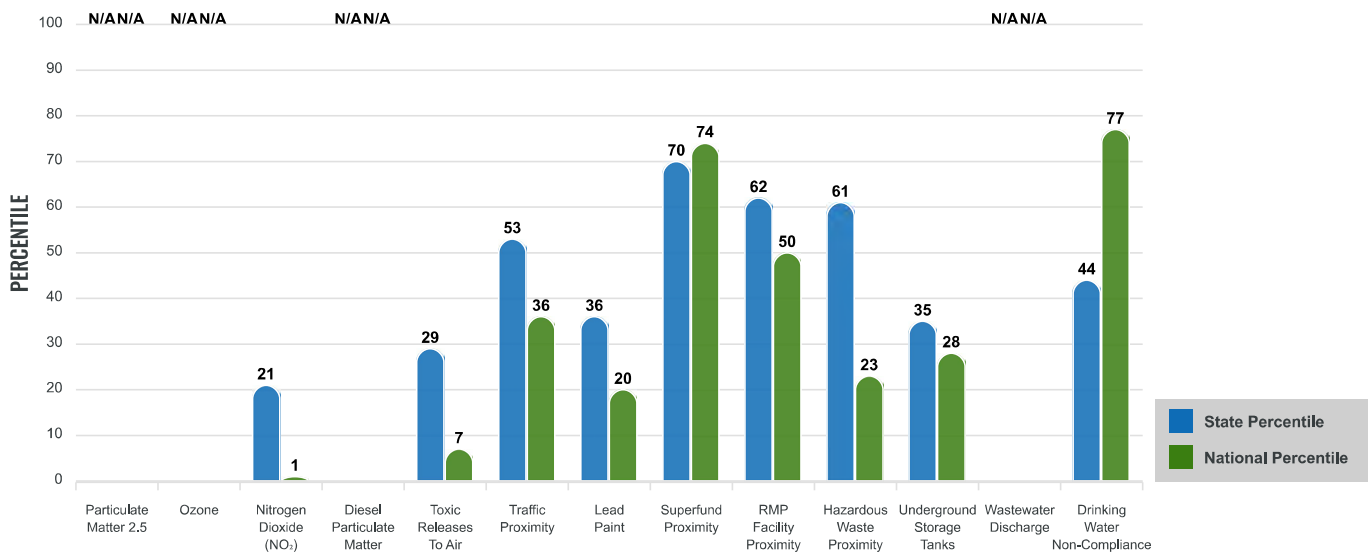
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020980200

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO ₂) (ppbv)	2	5.6	29	7.8	1
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	9.9	76	40	4,600	9
Traffic Proximity (daily traffic count/distance to road)	690,000	590,000	65	1,700,000	46
Lead Paint (% Pre-1960 Housing)	0.034	0.087	46	0.3	23
Superfund Proximity (site count/km distance)	12	0.57	99	0.39	99
RMP Facility Proximity (facility count/km distance)	0.55	0.32	83	0.57	66
Hazardous Waste Proximity (facility count/km distance)	0.35	0.21	80	3.5	29
Underground Storage Tanks (count/km ²)	0.043	2.7	41	3.6	30
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	2	5.4	50	2.2	85
SOCIOECONOMIC INDICATORS					
Demographic Index USA	1.48	N/A	N/A	1.34	62
Supplemental Demographic Index USA	1.04	N/A	N/A	1.64	18
Demographic Index State	1.92	1.65	69	N/A	N/A
Supplemental Demographic Index State	0.89	1.48	13	N/A	N/A
People of Color	45%	43%	59	40%	62
Low Income	33%	25%	71	30%	60
Unemployment Rate	9%	7%	75	6%	80
Limited English Speaking Households	0%	2%	0	5%	0
Less Than High School Education	3%	7%	31	11%	27
Under Age 5	12%	6%	91	5%	92
Over Age 64	2%	14%	1	18%	3

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	2
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	25
Air Pollution	1
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	2
Hospitals	2
Places of Worship	0

Other environmental data:

Air Non-attainment	No
Impaired Waters	Yes

Selected location contains American Indian Reservation Lands*	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	No

Report for Tract: 02020980200

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	N/A	19%	N/A	20%	N/A
Heart Disease	1	4.7	0	5.8	0
Asthma	9.5	10	26	10.3	28
Cancer	1.4	5.5	0	6.4	0
Persons with Disabilities	4.9%	13.2%	1	13.7%	3

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	9%	18%	53	12%	61
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	8%	12%	44	13%	43
Lack of Health Insurance	1%	12%	0	9%	2
Housing Burden	Yes	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	Yes	N/A	N/A	N/A	N/A

Report for Tract: 02020980200

Report produced August 7, 2024 using EJScreen Version 2.3

Appendix J

FAA No Hazard Determination



Mail Processing Center
 Federal Aviation Administration
 Southwest Regional Office
 Obstruction Evaluation Group
 10101 Hillwood Parkway
 Fort Worth, TX 76177

Aeronautical Study No.
 2024-AAL-517-OE

Issued Date: 09/23/2024

Brenda Hewitt
 Native Village of Eklutna
 26339 Eklutna Village Rd
 Chugiak, AK 99567

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure: Commercial Use Building Casino
 Location: Anchorage, AK
 Latitude: 61-25-04.90N NAD 83
 Longitude: 149-29-34.10W
 Heights: 80 feet site elevation (SE)
 40 feet above ground level (AGL)
 120 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
- Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 03/23/2026 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2024-AAL-517-OE.

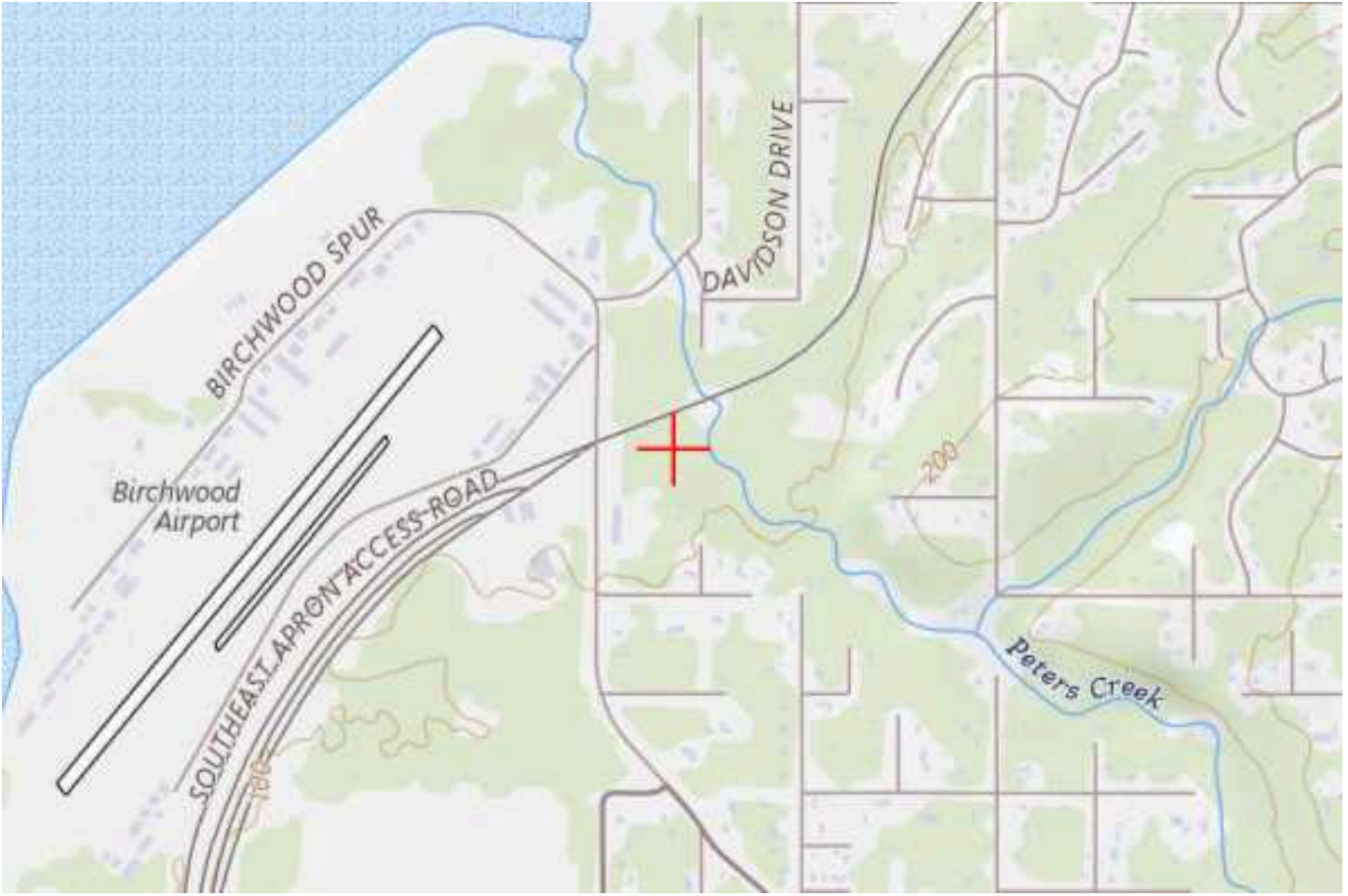
Signature Control No: 631200128-634004900

(DNE)

Paul Holmquist
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2024-AAL-517-OE





The FAA is currently experiencing delays in processing off-airport aeronautical studies. These delays are currently resulting in an approximate 15 additional days in processing time. The FAA will continue to work aeronautical studies on a first come, first served basis. Please take this possible delay into consideration when determining when to submit your case. If your submitted aeronautical study requires priority and 60 days has elapsed since submission, please contact the OEG Specialist for your state with the rationale for your request and it will be reviewed for escalation. The issue causing these delays is actively being mitigated and is expected to be resolved around August.

Notice Criteria Tool

Notice Criteria Tool - Desk Reference Guide V_2018.2.0

The requirements for filing with the Federal Aviation Administration for proposed structures vary based on a number of factors: height, proximity to an airport, location, and frequencies emitted from the structure, etc. For more details, please reference [CFR Title 14 Part 77.9](#).

You must file with the FAA at least 45 days prior to construction if:

- your structure will exceed 200ft above ground level
- your structure will be in proximity to an airport and will exceed the slope ratio
- your structure involves construction of a traverseway (i.e. highway, railroad, waterway etc...) and once adjusted upward with the appropriate vertical distance would exceed a standard of 77.9(a) or (b)
- your structure will emit frequencies, and does not meet the conditions of the [FAA Co-location Policy](#)
- your structure will be in an instrument approach area and might exceed part 77 Subpart C
- your proposed structure will be in proximity to a navigation facility and may impact the assurance of navigation signal reception
- your structure will be on an airport or heliport
- filing has been requested by the FAA

If you require additional information regarding the filing requirements for your structure, please identify and contact the appropriate FAA representative using the [Air Traffic Areas of Responsibility map](#) for Off Airport construction, or contact the [FAA Airports Region / District Office](#) for On Airport construction.

The tool below will assist in applying Part 77 Notice Criteria.

* **Structure Type:** BUILDING | Commercial Use Building ▼
Please select structure type and complete location point information.

Latitude: 61 Deg 25 M 05.0 S N ▼

Longitude: 149 Deg 29 M 35.7 S W ▼

Horizontal Datum: NAD83 ▼

Site Elevation (SE): 80 (nearest foot)

Structure Height : 40 (nearest foot)

Is structure on airport: No Yes

Results

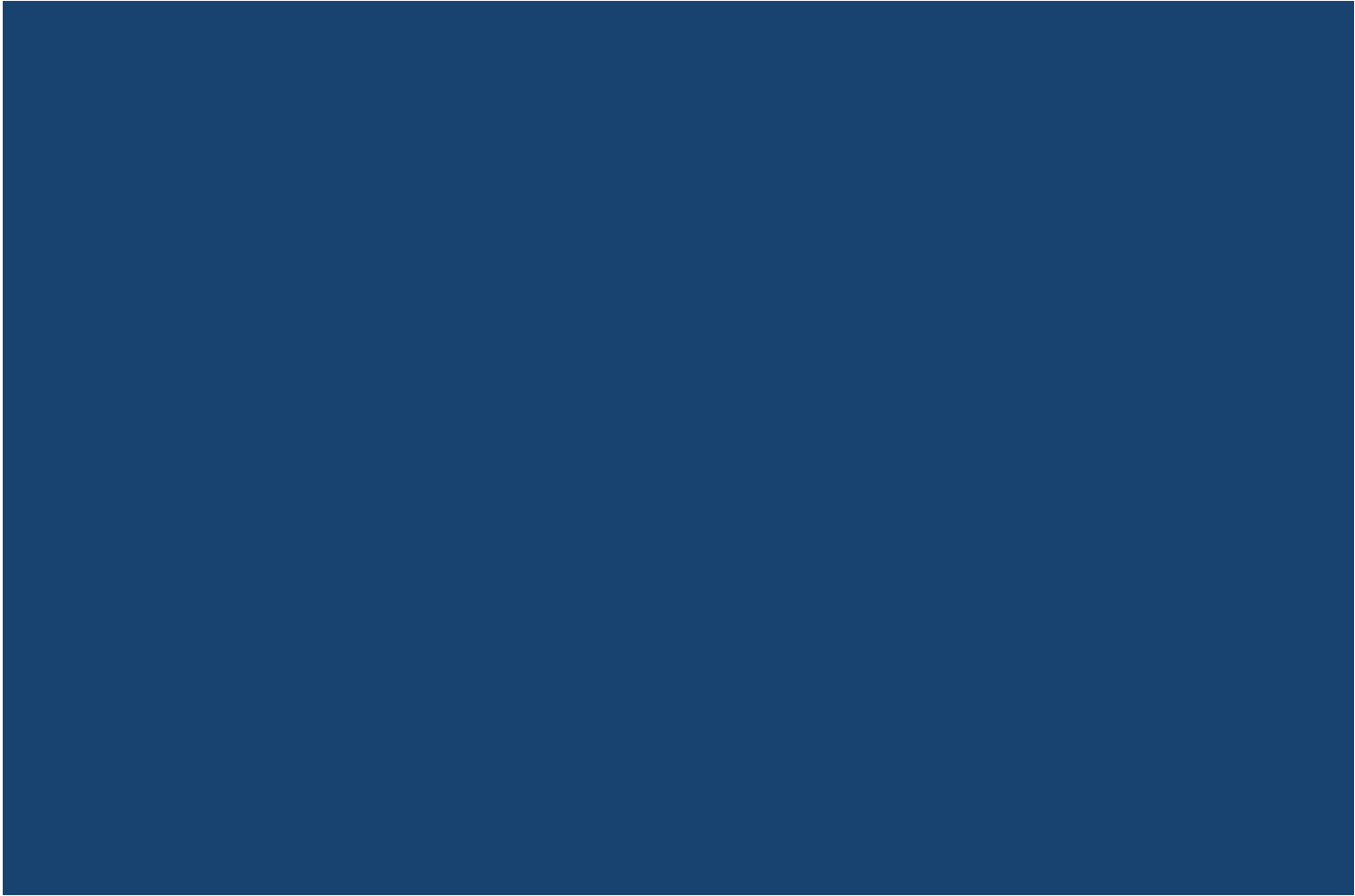
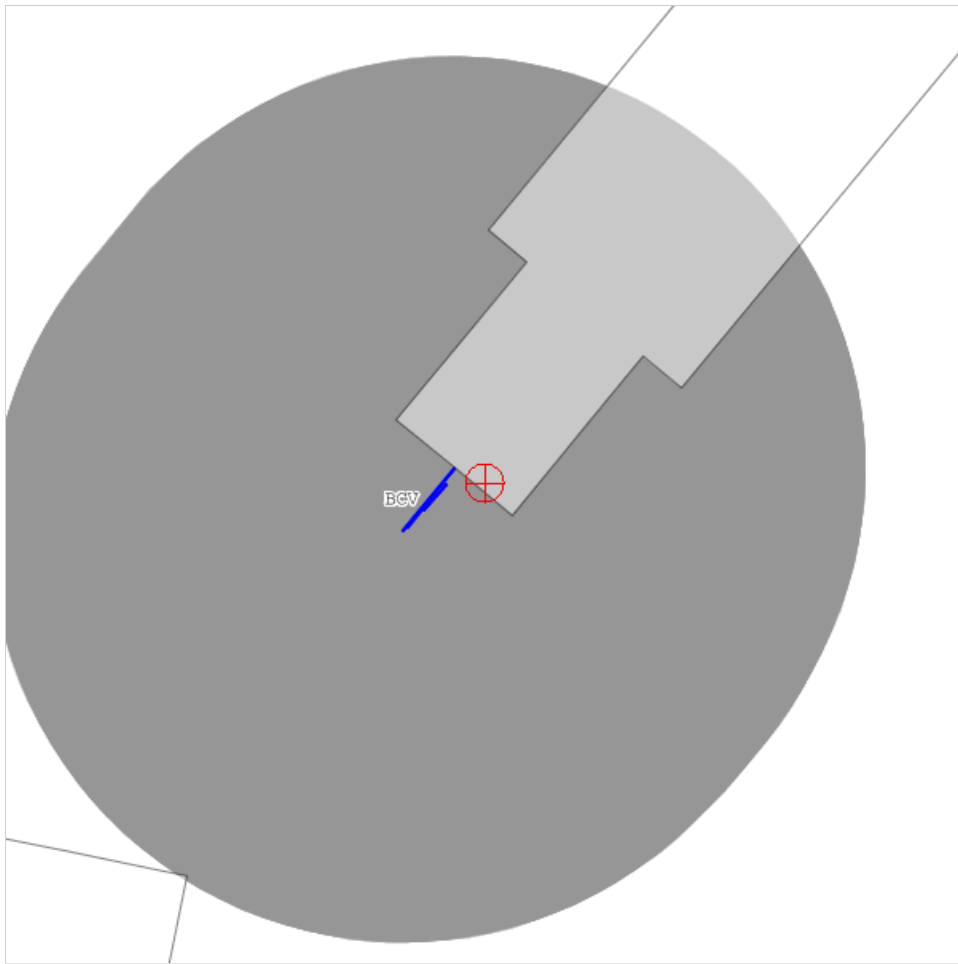
You exceed the following Notice Criteria:

Your proposed structure exceeds an instrument approach area by 52 feet and aeronautical study is needed to determine if it will exceed a standard of subpart C of 14CFR Part 77. The FAA, in accordance with 77.9, requests that you file.

Your proposed structure is in proximity to a navigation facility and may impact the assurance of navigation signal reception. The FAA, in accordance with 77.9, requests that you file.

77.9(b) by 36 ft. The nearest airport is BCV, and the nearest runway is 02L/20R.

The FAA requests that you file





Federal Aviation Administration

The FAA is currently experiencing delays in processing off-airport aeronautical studies. These delays are currently resulting in an approximate 15 additional days in processing time. The FAA will continue to work aeronautical studies on a first come, first served basis. Please take this possible delay into consideration when determining when to submit your case. If your submitted aeronautical study requires priority and 60 days has elapsed since submission, please contact the OEG Specialist for your state with the rationale for your request and it will be reviewed for escalation. The issue causing these delays is actively being mitigated and is expected to be resolved around August.

<< OE

Notice of Proposed Construction or Alteration - Off Airport

Add a New Case (Off Airport) - Desk Reference Guide V_2018.2.1

Add a New Case (Off Airport) for Wind Turbines - Met Towers (with WT Farm) - WT-Barge Crane - Desk Reference Guide V_2018.2.1

The Description of Location field can only be 300 characters long
The Description of Proposal field can only be 300 characters long

Form containing sections: Sponsor (person, company, etc. proposing this action), Construction / Alteration Information, Structure Summary, Structure Details, Proposed Frequency Bands, and Additional Location(s).

I hereby certify that all of the above statements made by me are true, complete, and correct to the best of my knowledge. In addition, I agree to mark and/or light the structure in accordance with established marking and lighting standards as necessary.



Kenai Peninsula Borough, Matanuska-Susitna Borough GIS, Municipality of Anchorage, State of Alaska, Esri, TomTom, Garmin, SafeGraph, METI/NASA, USGS, EPA, NPS, USDA, USFWS


Exhibit A - PROPOSED PROJECT SITE AND VICINITY



Legend

- Project Site
- Ondola Allotment
- County Parcels

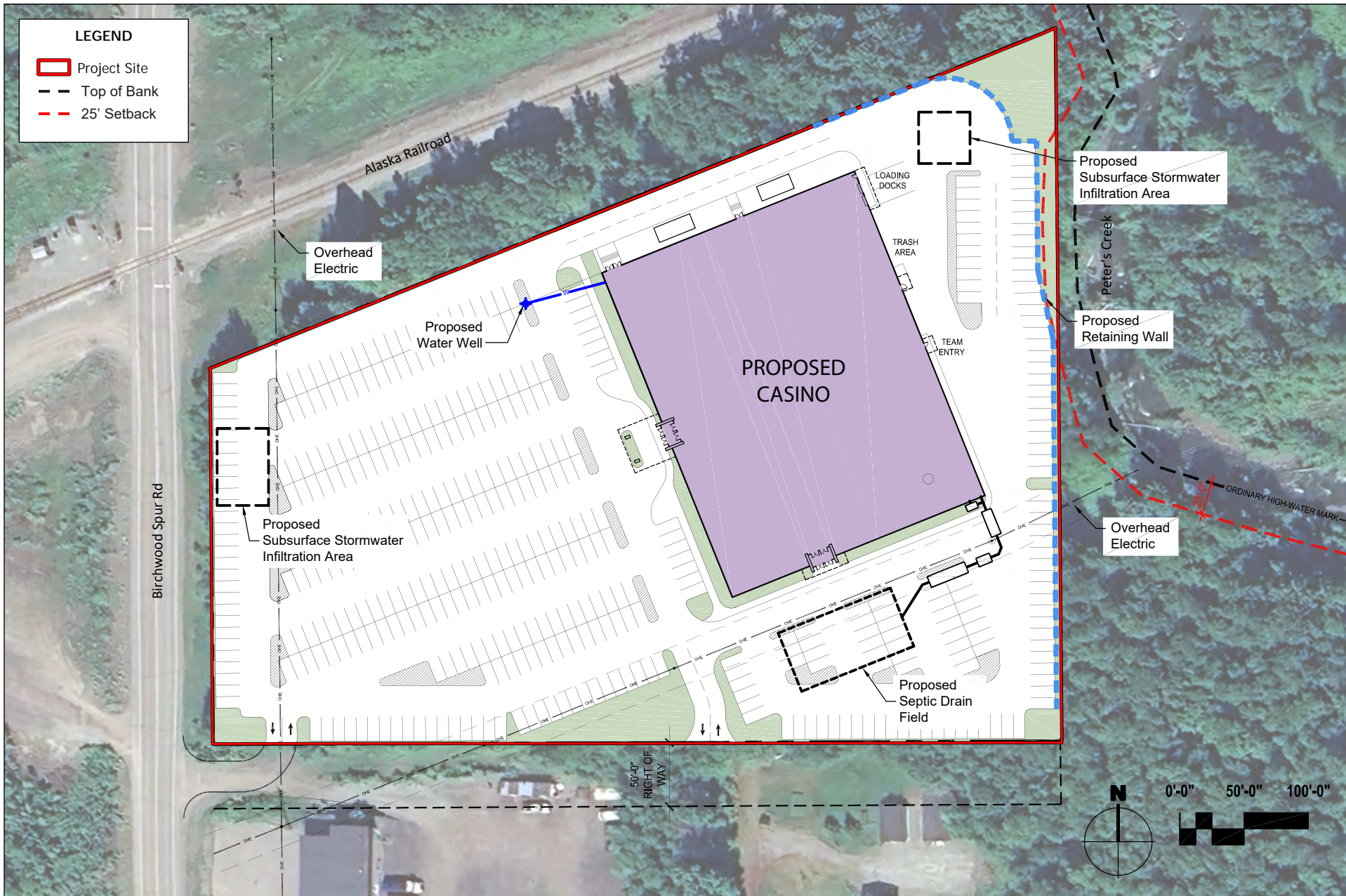
Alluvial St
Image © 2024 Airbus



0 200 400 Feet

Kenai Peninsula Borough, Matanuska-Susitna Borough GIS, Municipality of Anchorage, State of Alaska, © OpenStreetMap, Microsoft, Esri, TomTom, Garmin, SafeGraph, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, USFWS

EXHIBIT B
AERIAL PHOTOGRAPH



Source: Marnell Companies, 6/5/2024, Acorn Environmental, 8/26/2024

EXHIBIT C -
PROPORSE CASINO
SITE PLAN



Federal Aviation Administration

The FAA is currently experiencing delays in processing off-airport aeronautical studies. These delays are currently resulting in an approximate 15 additional days in processing time. The FAA will continue to work aeronautical studies on a first come, first served basis. Please take this possible delay into consideration when determining when to submit your case. If your submitted aeronautical study requires priority and 60 days has elapsed since submission, please contact the OEG Specialist for your state with the rationale for your request and it will be reviewed for escalation. The issue causing these delays is actively being mitigated and is expected to be resolved around August.

<< OE/AAA

Project Submission Success
Project Name: NATIV-000878409-24

Project NATIV-000878409-24 has been submitted successfully to the FAA.

Your filing is assigned Aeronautical Study Number (ASN):
2024-AAL-517-OE

Please refer to the assigned ASN on all future inquiries regarding this filing.

Please return to the system at a later date for status updates.

It is the responsibility of each e-filer to exercise due diligence to determine if coordination of the proposed construction or alteration is necessary with their state aviation department. Please use the link below to contact your state aviation department to determine their requirements:
[State Aviation Contacts](#)

To ensure e-mail notifications are delivered to your inbox please add noreply@faa.gov to your address book. Notifications sent from this address are system generated FAA e-mails and replies to this address will NOT be read or forwarded for review. Each system generated e-mail will contain specific FAA contact information in the text of the message.

Appendix K

Hazardous Materials Due Diligence Memo

Eklutna Native Village Casino Project

Hazardous Materials Due Diligence Memo

Prepared For: Eklutna Native Village
Prepared By: Acorn Environmental
Date: August 29, 2024

Introduction

This memorandum has been prepared to summarize results of a hazardous materials due diligence investigation conducted for the Eklutna Native Village Casino Project. The Subject Property, approximately 6.37-acres, consists of portions of three restricted fee Native allotment parcels (Assessor’s Parcel Numbers 05108101000, 05108102000, and 05108115000). The Subject Property is situated off Birchwood Spur Road with the Alaska Railroad to the north and the Birchwood Airport to the west, and is within Section 5 of Township 15 North, Range 1 West within the Seward Meridian, and is within the Anchorage B-7 NW United States Geological Survey 7.5’ quadrangle map

Site Reconnaissance

A reconnaissance survey of the Subject Property was conducted by Acorn Environmental from July 15 to 19, 2024 to assess the Subject Property for potential hazardous materials issues. **Table 1**, below, summarizes the current and past uses of the Subject Property in addition to the current uses of the adjoining properties.

Table 1: Current and Past Uses of the Subject Property and Adjacent Properties

Current/Past Uses	Description
Project Site (Current and Past)	
Current Use of Subject Property	Vacant forestland with cleared areas
Past Use of Subject Property	Single-family residential development
Existing Structures on Subject Property	None
Adjacent Properties (Current)	
Current Use of Adjoining Property (North)	Alaska Railroad Corporation railway and right of way
Current Use of Adjoining Property (West)	Birchwood Spur Roadway - adjoining property associated with the railroad
Current Use of Adjoining Property (East)	Peters Creek
Current Use of Adjoining Property (South)	Commercial facility (maintenance yard) and residential homes

Solid waste was observed throughout the Subject Property, including household debris such as tarps, wood, sheeting, pallets, broken glass bottles, cardboard, an oven, and other common household items. Ongoing cleanup activities were ongoing at the Subject Property during the site reconnaissance.

Two groundwater wells were noted on the Subject Property. One well showed signs of neglect and non-operation; it is assumed to be abandoned or decommissioned. The second well is presumed to have previously served the historic residence that was formerly onsite. One septic system was identified on the Subject Property, and septic and trash odors were noted.

Electrical overhead service lines in the southern part of the Subject Property were noted. One overhead transformer was observed along this utility line; however, no leaks or stains were observed.

No above ground or underground storage tanks/associated piping, pits, ponds, or lagoons of liquid were observed on the Subject Property. Recent rainfall created multiple areas of standing water however no sheen or odors were evident. Several empty and rusted 55-gallon steel drums were found partially buried in four locations around the Subject Property. These 55-gallon drums had no labels to indicate the contents. No evidence of impaired vegetation was noted in the immediate vicinity of these empty 55-gallon drums. No stained soil or pavement was observed.

Database Searches

A database search of the Project Site and surrounding area was conducted in August 2024 by NetrOnline for records of hazardous materials generation, storage, or contamination (**Attachment 1**). Applicable federal, State, and local databases were searched in addition to the Alaska Department of Environmental Conservation (DEC) Contaminated Site's Contaminated Sites portal for researching recorded cleanups and spills (DEC, 2024; EPA, 2024). Databases were searched for listings up to one mile from a point roughly equivalent to the center of the Subject Property.

Five leaking underground storage tank (LUST) sites were noted on the DEC database within a 1-mile radius of the Subject Property. All five sites have been remediated and closed by the ADEC (**Attachment 1**).

Of the database results reviewed for the 1-mile radius, only one listed site had an active cleanup status: the Sunset Auto Body Class V Injection Well located at 21442 N Birchwood Loop Road, approximately 0.75 miles to the south of the Subject Property. The Sunset Auto Body injection well system, operational since 1988, consists of two shop floor drains that discharge into a 4.5-foot corrugated steel culvert pit located beneath the facility's office, which leads to a 16-foot perforated pipe drain pit outside the rear of the building. In 2018, soil contamination was identified at the Sunset Auto Body site, triggering a compliance order from the USEPA to close the well. The contaminants detected include chromium, naphthalene, and tetrachloroethylene in the soil beneath the office. These findings prompted further investigation and a series of mitigation efforts. As of 2019, work at the Sunset Auto Body site included a plan to seal the injection well with concrete, remove contaminated soils, and collect confirmation soil samples. The analysis did not detect any not contain detectable concentrations of volatile organic compounds. Groundwater samples analyzed for VOCs on August 13, 2019, from an onsite well on the Sunset Auto Body site, did not contain detectable concentrations of contaminants (DEC, 2024a).

Given the distance to the Subject Property, the ongoing DEC oversight and remediation activities, and the lack of detected contaminants in the water sample on the listed site, the contamination at the Sunset Auto Body site does not present an immediate risk to the Subject Property.

Findings

No visible signs of hazardous materials or petroleum product involvement or contamination were observed during the July 2024 site reconnaissance. The Subject Property was not listed on any regulatory agency database as having previous or current hazardous materials uses or releases, and no listed sites in the vicinity presented a risk of contamination to the Subject Property. Based on the findings of this technical memorandum, no further hazardous materials investigation of the Subject Property is recommended at this time.

References

United States Environmental Protection Agency (USEPA), 2024. Enforcement and Compliance History Online Facility Search Results. Available online at: <https://echo.epa.gov/facilities/facility-search/results>. Accessed August 29, 2024.

Alaska Department of Environmental Conservation (DEC), 2024. Division of Spill Prevention and Response Database of Contaminated Sites. Available online at <https://www.arcgis.com/apps/mapviewer/index.html?webmap=315240bfbaf84aa0b8272ad1cef3cad3>. Accessed August 29, 2024.

DEC, 2024a. Sunset Auto Body Class V Injection Well Site. Available online at <https://dec.alaska.gov/Applications/SPAR/PublicMVC/CSP/SiteReport/26906#documents>. Accessed August 29, 2024.

Site Name: Project Site
Location: NA
Prepared for: Acorn Environmental
Ref: Eklutna
Center Coordinates: 61.417862,-149.493514
Prepared Date: Thu Aug 15 2024 18:01:02 GMT+0000 (Coordinated Universal Time)

ATTACHMENT 1

ENVIRONMENTAL RADIUS REPORT

ASTM E1527-21



2055 E. Rio Salado Pkwy, Tempe, AZ 85381 | 480-967-6752

Summary

Federal	< 1/4	1/4 - 1/2	1/2 - 1
Lists of Federal NPL (Superfund) sites	0	0	0
Lists of Federal Delisted NPL sites	0	0	0
Lists of Federal sites subject to CERCLA removals and CERCLA orders	0	0	0
Lists of Federal CERCLA sites with NFRAP	0	0	0
Lists of Federal RCRA facilities undergoing Corrective Action	0	0	0
Lists of Federal RCRA TSD facilities	0	0	0
Lists of Federal RCRA generators	0	0	0
Federal institutional control/engineering control registries	0	0	0
Federal ERNS list	0	0	0

State	< 1/4	1/4 - 1/2	1/2 - 1
Lists of state and tribal Superfund equivalent sites	0	0	0
Lists of state and tribal hazardous waste facilities	0	0	0
Lists of state and tribal landfills and solid waste disposal facilities	0	0	0
Lists of state and tribal leaking storage tanks	3	2	0
Lists of state and tribal registered storage tanks	0	0	0
State and tribal institutional control/engineering control registries	0	0	0
Lists of state and tribal voluntary cleanup sites	0	2	0
Lists of state and tribal brownfields sites	0	0	0

Other	< 1/4	1/4 - 1/2	1/2 - 1
State and/or tribal lists of sites requiring further investigation / remediation	0	0	0
State list of Significant Environmental Hazards (SEH)	0	0	0
Lists of state and tribal mine sites requiring further investigation and/or remediation	0	0	0
State and/or tribal lists of spills and spill responses	0	0	0
State and/or tribal lists of emergency responses	0	0	0
State and/or tribal lists of dry cleaners	0	0	0
State and/or tribal lists of clandestine laboratory cleanups	0	0	0
State and/or tribal lists of scrap/used tire processing facilities	0	0	0
State and/or tribal lists of underground injection control sites	0	0	0
State and/or tribal listings of permitted drywells	0	0	0
Automobile salvage yards	0	0	0
Livestock Waste Control sites	0	0	0
Controlled Animal Feeding Operations (CAFOs)	0	0	0
State and/or tribal lists of registered aboveground storage tanks (ASTs)	0	0	0
C.A.A. Permitted Facilities	0	0	0
NPDES Permitted Facilities	0	0	0
Onsite Wastewater Treatment sites	0	0	0
State and/or tribal lists of permitted facilities	0	0	0
Resource Conservation and Recovery Act Information (RCRAInfo)	0	0	0
U.S. EPA Enforcement, Compliance History Online (ECHO)	0	0	0
U.S. EPA Underground Storage Tanks (UST)	0	0	0
U.S. EPA Toxic Substances Control Act (TSCA) database	0	0	0
U.S. EPA Toxic Release Inventory System (TRIS)	0	0	0

Lists of Federal NPL (Superfund) sites

The National Priorities List (NPL) is the list of sites of national priority among the known releases or threatened releases of hazardous substances, pollutants, or contaminants throughout the United States and its territories. The NPL is intended primarily to guide the EPA in determining which sites warrant further investigation. The NPL is updated periodically, as mandated by CERCLA.

There were no Federal NPL sites found within a one-mile radius of the target property.

Lists of Federal Delisted NPL sites

The EPA may delete a final NPL site if it determines that no further response is required to protect human health or the environment. Under Section 300.425(e) of the NCP (55 FR 8845, March 8, 1990), a site may be deleted when no further response is appropriate if EPA determines that one of the following criteria has been met: 1) EPA, in conjunction with the state, has determined that responsible parties have implemented all appropriate response action required, 2) EPA, in consultation with the state, has determined that all appropriate Superfund-financed responses under CERCLA have been implemented and that no further response by responsible parties is appropriate, 3) A remedial investigation/feasibility study (RI/FS) has shown that the release poses no significant threat to public health or the environment and, therefore, remedial measures are not appropriate.

There were no Federal Delisted NPL sites found within a half-mile radius of the target property.

Lists of Federal sites subject to CERCLA removals and CERCLA orders

CERCLA identifies the classes of parties liable under CERCLA for the cost of responding to releases of hazardous substances. In addition, CERCLA contains provisions specifying when Federal installations must report releases of hazardous substances and the cleanup procedures they must follow. Executive Order No. 12580, Superfund Implementation, delegates response authorities to EPA and the Coast Guard. Generally, the head of the Federal agency has the delegated authority to address releases at the Federal facilities in its jurisdiction.

There were no Federal sites subject to CERCLA removals and/or orders found within a half-mile radius of the target property.

Lists of Federal CERCLA sites with NFRAP

No Further Remedial Action Planned (NFRAP) is a decision made as part of the Superfund remedial site evaluation process to denote that further remedial assessment activities are not required and that the facility/site does not pose a threat to public health or the environment sufficient to qualify for placement on the National Priorities List (NPL) based on currently available information. These facilities/sites may be re-evaluated if EPA receives new information or learns that site conditions have changed. A NFRAP decision does not mean the facility/site is free of contamination and does not preclude the facility/site from being addressed under another federal, state or tribal cleanup program.

There were no Federal CERCLA sites with No Further Remedial Action Planned (NFRAP) decisions found within a half-mile radius of the target property.

Lists of Federal RCRA facilities undergoing Corrective Action

Corrective action is a requirement under the Resource Conservation and Recovery Act (RCRA) that facilities that treat, store or dispose of hazardous wastes investigate and cleanup hazardous releases into soil, ground water, surface water and air. Corrective action is principally implemented through RCRA permits and orders. RCRA permits issued to TSDFs must include provisions for corrective action as well as financial assurance to cover the costs of implementing those cleanup measures. In addition to the EPA, 44 states and territories are authorized to run the Corrective Action program.

There were no Federal RCRA facilities undergoing corrective action(s) found within a one-mile radius of the target property.

Lists of Federal RCRA TSD facilities

The final link in RCRA's cradle-to-grave concept is the treatment, storage, and disposal facility (TSDF) that follows the generator and transporter in the chain of waste management activities. The regulations pertaining to TSDFs are more stringent than those that apply to generators or transporters. They include general facility standards as well as unit-specific design and operating criteria.

There were no Federal RCRA treatment, storage and disposal facilities (TSDFs) found within a half-mile radius of target property.

Lists of Federal RCRA generators

A generator is any person who produces a hazardous waste as listed or characterized in part 261 of title 40 of the Code of Federal Regulations (CFR). Recognizing that generators also produce waste in different quantities, EPA established three categories of generators in the regulations: very small quantity generators, small quantity generators, and large quantity generators. EPA regulates hazardous waste under the Resource Conservation and Recovery Act (RCRA) to ensure that these wastes are managed in ways that protect human health and the environment. Generators of hazardous waste are regulated based on the amount of hazardous waste they generate in a calendar month, not the size of their business or facility.

There were no Federal RCRA generators found at the target property and/or adjoining properties.

Federal institutional control/engineering control registries

Institutional Controls (IC) are defined as non-engineered and/or legal controls that minimize the potential human exposure to contamination by limiting land or resource use. Whereas, Engineering Controls (EC) consist of engineering measures (e.g. caps, treatment systems, etc.) designed to minimize the potential for human exposure to contamination by either limiting direct contact with contaminated areas or controlling migration of contaminants through environmental media.

There were no Federal institutional or engineering controls found at the target property.

Federal ERNS list

The Emergency Response Notification System (ERNS) is a database used to store information on notification of oil discharges and hazardous substances releases. The ERNS program is a cooperative data sharing effort encompassing the National Response Center (NRC), operated by the US Coast Guard, EPA HQ and EPA regional offices. ERNS data is used to analyze release notifications, track EPA responses and compliance to environmental laws, support emergency planning efforts, and assist decision-makers in developing spill prevention programs.

There were no Federally recorded releases of oil and/or hazardous substances at the target property.

Lists of state and tribal Superfund equivalent sites

In order to maintain close coordination with the states and tribes in the NPL listing decision process, the EPA's policy is to determine the position of states and tribes on sites that EPA is considering for listing. Consistent with this policy, since 1996, it has been the EPA's general practice to seek the state or tribe's position on sites under consideration for NPL listing by submitting a written request to the governor/state environmental agency or tribe. Various states may have their own program for identifying, investigating and cleaning up sites where consequential amounts of hazardous waste may have been disposed that work in conjunction with the EPA's Superfund remedial program.

There were no State and/or tribal Superfund equivalent sites found within a one-mile radius of target property.

Lists of state and tribal hazardous waste facilities

EPA established basic hazardous waste management standards for businesses who produce hazardous waste and categorized three businesses based on the volume of hazardous waste produced in a calendar month. On the federal level, there are three generator categories: large quantity generator, small quantity generator, and conditionally exempt small quantity generator. Some states are authorized to establish generator categories that are different from those that federal EPA set up. State regulatory requirements for generators of hazardous waste may be more stringent than the federal program.

There were no State and/or tribal hazardous waste facilities found within a half-mile radius of the target property.

Lists of state and tribal landfills and solid waste disposal facilities

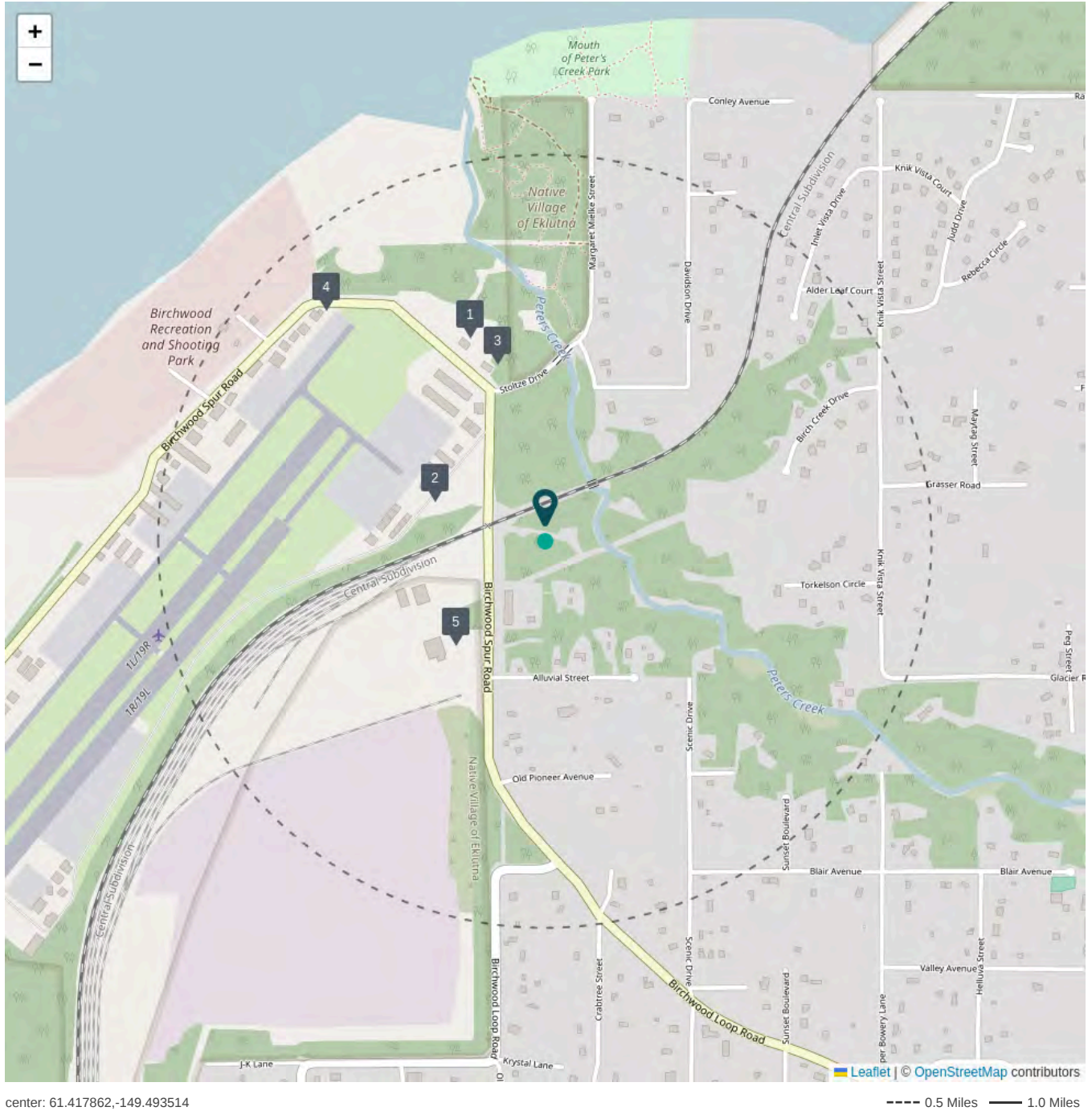
Title 40 of the CFR parts 239 through 259 contain the regulations for non-hazardous solid waste programs set up by the states. EPA has requirements for state solid waste permit programs, guidelines for the processing of solid waste, guidelines for storage and collection of commercial, residential and institutional solid waste, and the criteria for municipal solid waste landfills. State solid waste programs may be more stringent than the federal code requires.

There were no State and/or tribal landfills or solid waste disposal facilities found within a half-mile radius of the target property.

Lists of state and tribal leaking storage tanks

ADEC - LEAKING UNDERGROUND STORAGE TANKS

Alaska's Department of Environmental Conservation (ADEC) Contaminated Sites Program (CSP) maintains a database of all contaminated sites currently undergoing cleanup, sites with cleanups planned, and sites where cleanups have been completed. This data set contains sites where a leaking underground storage tank has been located by the ADEC, and was searched to return all records within a half-mile of the target property.



center: 61.417862,-149.493514

1

ADOTPF - Chugiak Volunteer Fire Station

20651 Birchwood Spur

Hazard ID: 24641

File ID: 2106.26.014

Contaminated Site or LUST: LUST

Site Name: ADOTPF - Chugiak Volunteer Fire Station

Address: 20651 Birchwood Spur

City: Chugiak

Borough: Anchorage

State: AK

ZIP Code: 99567

Status: Cleanup Complete

Staff: No Longer Assigned

Landowner: State of Alaska - ADOTPF DES-Design / Engineering Svcs

Site Type: Unknown

Latitude: 61.421658

Longitude: -149.496513

Horizontal Datum: NAD83

Distance From Center (Miles): 0.2803

Site Source: last updated 08-21-2023 from ADEC-LUST-NEW

2

Aviation North Al Hand Birchwood Airport Lot 19 Block 100

Birchwood Airport

Hazard ID: 24198

File ID: 2106.26.011

Contaminated Site or LUST: LUST

Site Name: Aviation North Al Hand Birchwood Airport Lot 19 Block 100

Address: Birchwood Airport

City: Chugiak

Borough: Anchorage

State: AK

ZIP Code: 99567

Status: Cleanup Complete

Staff: No Longer Assigned

Landowner: Al Hand

Site Type: Unknown

Latitude: 61.418598

Longitude: -149.497877

Horizontal Datum: NAD83

Distance From Center (Miles): 0.1529

Site Source: last updated 08-21-2023 from ADEC-LUST-NEW

3

Civil Air Patrol / Birchwood

Birchwood Airport

Hazard ID: 25083

File ID: 2106.26.010

Contaminated Site or LUST: LUST

Site Name: Civil Air Patrol / Birchwood

Address: Birchwood Airport

City: Chugiak

Borough: Anchorage

State: AK

ZIP Code: 99567

Status: Cleanup Complete

Staff: No Longer Assigned

Landowner: Civil Air Patrol Real Estate Manager

Site Type: Unknown

Latitude: 61.421158

Longitude: -149.495413

Horizontal Datum: NAD83

Distance From Center (Miles): 0.2362

Site Source: last updated 08-21-2023 from ADEC-LUST-NEW

4

Kelly Vrem, Birchwood Airport

Lot 1, Block 500, Birchwood Airport

Hazard ID: 24715

File ID: 2106.26.008

Contaminated Site or LUST: LUST

Site Name: Kelly Vrem, Birchwood Airport

Address: Lot 1, Block 500, Birchwood Airport

City: Chugiak

Borough: Anchorage

State: AK

ZIP Code: 99567

Status: Cleanup Complete

Staff: No Longer Assigned

Landowner: Kelly Vrem

Site Type: Unknown

Latitude: 61.422158

Longitude: -149.502113

Horizontal Datum: NAD83

Distance From Center (Miles): 0.4109

Site Source: last updated 08-21-2023 from ADEC-LUST-NEW

5

Kenai Supply Inc., Birchwood

20850 Birchwood Spur Rd.

Hazard ID: 23342

File ID: 2106.26.013

Contaminated Site or LUST: LUST

Site Name: Kenai Supply Inc., Birchwood

Address: 20850 Birchwood Spur Rd.

City: Chugiak

Borough: Anchorage

State: AK

ZIP Code: 99567

Status: Cleanup Complete

Staff: No Longer Assigned

Landowner: Kenai Supply, Inc.

Site Type: Unknown

Latitude: 61.415898

Longitude: -149.497077

Horizontal Datum: NAD83

Distance From Center (Miles): 0.1796

Site Source: last updated 08-21-2023 from ADEC-LUST-NEW

Lists of state and tribal registered storage tanks

EPA initially issued UST regulations in 1988. In 2015, EPA modified the UST regulation, which was effective October 13, 2015 in Indian Country and states without State Program Approval. EPA recognizes that, because of the size and diversity of the regulated community, state and local governments are in the best position to oversee USTs: 1) State and local authorities are closer to the situation in their domain and are in the best position to set priorities, 2) Subtitle I of the Solid Waste Disposal Act allows state UST programs approved by EPA to operate in lieu of the federal program, 3) the state program approval (SPA) regulations set criteria for states to obtain the authority to operate in lieu of the federal program. State programs must be at least as stringent as EPA's. A complete version of the law that governs USTs can be found in U.S. Code, Title 42, Chapter 82, Subchapter IX.

There were no State and/or tribal registered storage tanks found at subject and adjoining properties.

State and tribal institutional control/engineering control registries

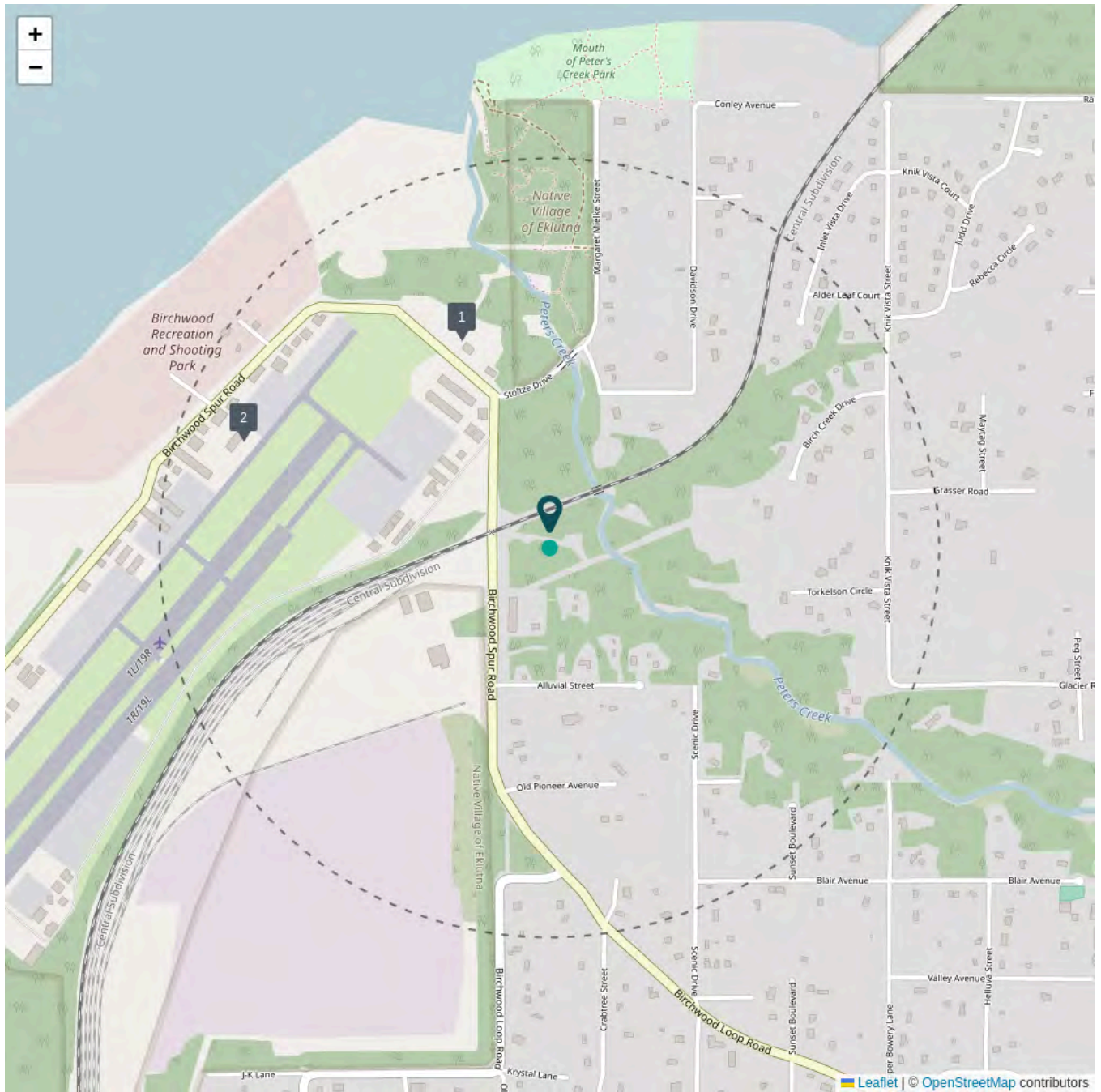
Institutional controls are non-engineered instruments such as administrative and legal controls that help minimize the potential for human exposure to contamination and/or protect the integrity of the remedy. Engineering controls consist of engineering measures (e.g, caps, treatment systems, etc.) designed to minimize the potential for human exposure to contamination by either limiting direct contact with contaminated areas or controlling migration of contaminants through environmental media. It is EPA's expectation that treatment or engineering controls will be used to address principal threat wastes and that groundwater will be returned to its beneficial use whenever practicable.

There were no State and/or tribal institutional and/or engineering controls found filed against the target property.

Lists of state and tribal voluntary cleanup sites

ADEC - CONTAMINATED SITES PROGRAM

Alaska Department of Conservation's (ADECs) Contaminated Sites Program (CSP) protects human health and the environment by managing the cleanup of contaminated soil and groundwater in Alaska. A contaminated site is a location where hazardous substances, including petroleum products, have been improperly disposed. Sites included in the Contaminated Sites Database include: brownfields, RCRA Corrective Action Program sites, Federal Facilities (DOD and NPL), dry cleaners, and Leaking Underground Storage Tank (LUST) release sites. This database was searched to return all contaminated site(s) records within a half-mile of the target property.



center: 61.417862,-149.493514

--- 0.5 Miles — 1.0 Miles

1

ADOT&PF Birchwood Maintenance Station Class V Injection Well

20651 Birchwood Spur Road; Birchwood Airport

Hazard ID: 26242

File ID: 2106.38.011

Program: CSP

Site Name: ADOT&PF Birchwood Maintenance Station Class V Injection Well

Address: 20651 Birchwood Spur Road; Birchwood Airport

City: Chugiak

Borough: Anchorage

State: AK

99627: 99567

Status: Cleanup Complete - Institutional Controls

Staff: IC Unit

Landowner: ADOT&PF CRD & HA Admin (8/19)

Site Type: Airport/Airfield

Latitude: 61.421698

Longitude: -149.496994

Horizontal Datum: WGS84

Distance From Center (Miles): 0.2889

Site Source: last updated 08-21-2023 from ADEC-CSP-NEW

2

Birchwood Airport

Lot 4, Block 500 Birchwood Airport

Hazard ID: 1471

File ID: 2106.38.003

Program: CSP

Site Name: Birchwood Airport

Address: Lot 4, Block 500 Birchwood Airport

City: Chugiak

Borough: Anchorage

State: AK

99627: 99567

Status: Cleanup Complete

Staff: No Longer Assigned

Landowner: First National Bank of Anchorage

Site Type: Unknown

Latitude: 61.419819

Longitude: -149.505419

Horizontal Datum:

Distance From Center (Miles): 0.416

Site Source: last updated 08-21-2023 from ADEC-CSP-NEW

Lists of state and tribal brownfields sites

Since its inception in 1995, EPA's Brownfields and Land Revitalization Program has grown into a proven, results-oriented program that has changed the way communities address and manage contaminated property. The program is designed to empower states, tribes, communities, and other stakeholders to work together to prevent, assess, safely clean up, and sustainably reuse brownfields. Beginning in the mid-1990s, EPA provided small amounts of seed money to local governments that launched hundreds of two-year Brownfields pilot projects and developed guidance and tools to help states, communities and other stakeholders in the cleanup and redevelopment of brownfields sites.

There were no State and/or tribal brownfields sites found within a half-mile radius of the target property.

State and/or tribal lists of sites requiring further investigation / remediation

No records found

State list of Significant Environmental Hazards (SEH)

No records found

Lists of state and tribal mine sites requiring further investigation and/or remediation

No records found

State and/or tribal lists of spills and spill responses

No records found

State and/or tribal lists of emergency responses

No records found

State and/or tribal lists of dry cleaners

No records found

State and/or tribal lists of clandestine laboratory cleanups

No records found

State and/or tribal lists of scrap/used tire processing facilities

No records found

State and/or tribal lists of underground injection control sites

No records found

State and/or tribal listings of permitted drywells

No state and/or tribal permitted drywells were found within a half-mile radius of the target property.

Automobile salvage yards

No records found

Livestock Waste Control sites

No records found

Controlled Animal Feeding Operations (CAFOs)

No records found

State and/or tribal lists of registered aboveground storage tanks (ASTs)

No records found

C.A.A. Permitted Facilities

No records found

NPDES Permitted Facilities

No records found

Onsite Wastewater Treatment sites

No records found

State and/or tribal lists of permitted facilities

No State and/or tribal permitted facilities found within a half-mile of the target property.

Resource Conservation and Recovery Act Information (RCRAInfo)

No records found

U.S. EPA Enforcement, Compliance History Online (ECHO)

No records found

U.S. EPA Underground Storage Tanks (UST)

No records found

U.S. EPA Toxic Substances Control Act (TSCA) database

No records found

U.S. EPA Toxic Release Inventory System (TRIS)

No records found

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Appendix L

Tree Survey Results
and BIA Timber Letter



United States Department of the Interior

BUREAU OF INDIAN AFFAIRS

Alaska Regional Office
3601 C Street, Suite 1200
Anchorage, Alaska 99503

IN REPLY REFER TO
Regional Director – MC 100

Sarah Roubidoux Lawson
Schwabe, Williamson, & Wyatt, P.C.
1420 5th Avenue, Suite 3400
Seattle, Washington 98101

Dear Living Landowners of Allotment 975 A 055026:

This letter serves two purposes, (1) to inform you of the volume and valuation of the forest products resource on Allotment 975 A 055026; and (2) document (a) your decision to waive the valuation of the forest products located on the allotment; and (b) your request to classify the forested acres on Allotment 975 A 055026 as non-forest as outlined in the October 28, 2024 letter from your attorney, Sarah Roubidoux Lawson of Schwabe, Williamson, & Wyatt, P.C., to Fabian Keirn, Regional Forester, Bureau of Indian Affairs (BIA) Alaska Region.

Background.

As part of trust responsibilities to Native allotment owners, BIA established a forest management plan for Cook Inlet Management Unit. In preparation of the forest management plan, our office engaged with the federally recognized Tribes located within the Cook Inlet Management Unit. A letter, dated April 25, 2012, was sent to each Tribe requesting feedback on the preparation of the forest management plan for Native allotments in the unit. The letter also requested the Tribes to post and distribute to the allotment owners an announcement and associated questionnaire. The Forest Management Plan for Native Allotment in the Cook Inlet Management Unit was completed on September 30, 2020.

1. Volume and Valuation.

On October 16, 2024, BIA forestry staff conducted a forest inventory of Allotment 975 A 055026. Gina Ondola and an attorney from Schwabe, Williamson, & Wyatt, P.C. named Molly were present. Forestry staff calculated a volume of 147.4 cords of trees standing on approximately 5.15 acres of primarily paper birch (*Betula papyrifera*) with a small component of white spruce (*Picea glauca*) and black cottonwood (*Populus trichocarpa*). An additional 23.6 cords of birch had already been cleared and decked from an area of 0.51 acres. Cords were used as the measurement tool for firewood. Using the State of Alaska's rate for commercial firewood of \$22.22, there is an estimated value of \$3,800.61 for the 171 cords of firewood on Allotment 975 A 055026.

2. Waiver of Valuation and Request to classify forested acres of Allotment 975 A 055026 as non-forest.

I understand that you wish to waive the \$3,800.61 valuation and utilize the 171 cords for personal use and to classify forested acres of Allotment 975 A 055026 as non-forest to meet your goals and objectives for the allotment.

I base this understanding on (1) the numerous well-attended meetings with BIA staff, Sarah Roubidoux Lawson representing the living landowners, attorneys for the Native Village of Eklutna, and contractors hired by the tribe for an environmental assessment for the proposed gaming operation on Allotment 975 A 055026; (2) the conversation that our forestry staff had directly with Gina Ondola on October 16, 2024, while conducting the forestry inventory; and (3) the October 28, 2024 letter from your attorney, Sarah Roubidoux Lawson of Schwabe, Williamson, & Wyatt, P.C., to Fabian Keirn, Regional Forester, BIA Alaska Region.

I accept your waiver of valuation of the forest products located on your allotment. For the amount already decked, a Non-Timber Forest Products Harvest Permit (BIA Form 5-5332) will not be required. I also accept your request to classify forested acres of Allotment 975 A 055026 as non-forest. The forested acres of Allotment 975 A 055026 will be removed from the Cook Inlet Management Unit catalog of forested acres.

For further information regarding this letter, please contact Fabian Keirn, Regional Forester: fabian.keirn@bia.gov; phone (907) 519-9371.

Sincerely,

Jolene N. John
Regional Director

Enclosure: April 25, 2012 Outreach Letter to the Native Village of Eklutna,
Oct. 28, 2024 Letter from Sarah R. Lawson,

cc: Dorothy Ondola Cook
Sophie Gutierrez
Goergianna Ondola
Jerry Ondola
Diane Buls
Franklin Rosenberg

Tree Survey Results

Eklutna Native Village Gaming Facility Project Tree Inventory Results

Prepared For: Eklutna Native Village
Prepared By: Acorn Environmental
Date: August 27, 2024

Introduction

This memorandum has been prepared to summarize results of the tree survey conducted for the Eklutna Native Village Gaming Facility Project (Proposed Project) in accordance with Timber Harvest Permit requirements of the Bureau of Indian Affairs. The Proposed Project includes the development and operation of a gaming facility and associated infrastructure within an approximately 6.37-acre property located in the unincorporated community of Chugiak within the municipality of Anchorage, Alaska (Project Site). Development will require tree removal and land clearing on the Project Site, which is owned in restricted fee status by tribal members of the Native Village of Eklutna as Native allotment land under the Alaska Native Allotment Act. Some trees will be retained in the proposed parking and landscaping areas, as well as along the perimeter of the Project Site. Clearing activities will result in leftover vegetation and wood debris, such as stumps, which will be chipped on-site and used where feasible, depending on the quality of the chips.

The Project Site consists of portions of three restricted fee Native allotment parcels (Assessor's Parcel Numbers 05108101000, 05108102000, and 05108115000) currently owned by members of the Eklutna Native Village. No tree removal is proposed within the portion of these parcels that is subject to a right of way (ROW) easement for the adjacent Alaska Railroad. The Project Site is situated off Birchwood Spur Road with the Alaska Railroad to the north and the Birchwood Airport to the west, and is within Section 5 of Township 15 North, Range 1 West within the Seward Meridian, and is within the Anchorage B-7 NW United States Geological Survey 7.5' quadrangle map (**Figure 1**).

Survey Methodology

A tree survey was conducted on the Project Site July 16 - July 18, 2024 to approximate the type, number, and size of trees present within the Project Site. Tree size was estimated utilizing approximate diameter at breast height (dbh) and approximate height. Trees with a dbh of five inches or greater were recorded. Height was approximated using a clinometer and dbh was measured using a measuring tape. Due to the density of trees, discrete heights of individual trees were only observable from across roadways or from canopy openings in ruderal/developed areas.

Results

The Project Site primarily consists of undeveloped forestland (**Figure 1**). Tree species on the Project Site are predominantly birch (*Betula* sp.), specifically paper birch (*Betula papyrifera*). Other deciduous trees include alder (*Alnus* sp.) and poplar (*Populus* sp.), while spruce species, such as white spruce (*Picea glauca*) and red spruce (*Picea rubens*), make up a minor component of the canopy. The understory is generally sparse, consisting mainly of young trees and scattered willows (*Salix* sp.). The forest floor features dense ground cover, which reduces ground visibility, with dominant species including Devil’s club (*Oplopanax horridus*) and cow parsley (*Anthriscus sylvestris*). Other common species are lady fern (*Athyrium filix*), fireweed (*Chamaenerion angustifolium*), meadow horsetail (*Equisetum pratense*), prickly rose (*Rosa acicularis*), raspberry (*Rubus idaeus*), and clasp-leaf twisted stalk (*Streptopus amplexifolius*).

Trees within the Project Site generally fell into a handful of similar sizes. Tree size information for the Project Site is presented in **Table 1** and a tree inventory is detailed in **Table 2**. Based on these tables, a total of 1,617 trees were inventoried on the Project Site. This includes a portion of the trees within the Alaska Railway ROW. The actual number of trees on the Project Site, excluding the Alaska Railroad ROW is approximately 1,309 trees.

Table 1: Tree Sizing *

Species	Extra Small	Small	Medium	Large	Extra Large
Birch	dbh: 5.0-6.7" height: 57.1'	dbh: 6.8-9.6" height: 69.4'	dbh: 9.7-12.4" height: 85.2'	dbh: 12.5+" height: 128.8'	dbh: None height: None
Alder	dbh: 5.0-14.3" height: 79.8'	dbh: 14.4-15.3" height: 79.8'	dbh: 15.4-16.2" height: 79.8'	dbh: 16.3+" height: 79.8'	dbh: None height: None
Poplar	dbh: 5.0-5.7" height: 30.7'	dbh: 5.8-8.0" height: 79.0'*	dbh: 8.1-11.1" height: 127.2'	dbh: 11.2-23.6" height: 138.6'*	dbh: 23.6+" height: 150'*
Spruce	dbh: 5.0-5.4" height: 40.4'	dbh: 5.4-6.4" height: 40.4'	dbh: 6.5-7.6" height: 50.3'*	dbh: 7.7+" height: 60.2'	dbh: None height: None

* These heights were approximated based upon averaging between sizing categories or visual approximation as no line of sight was available to allow for accurate clinometer reading

Table 2: Tree Inventory

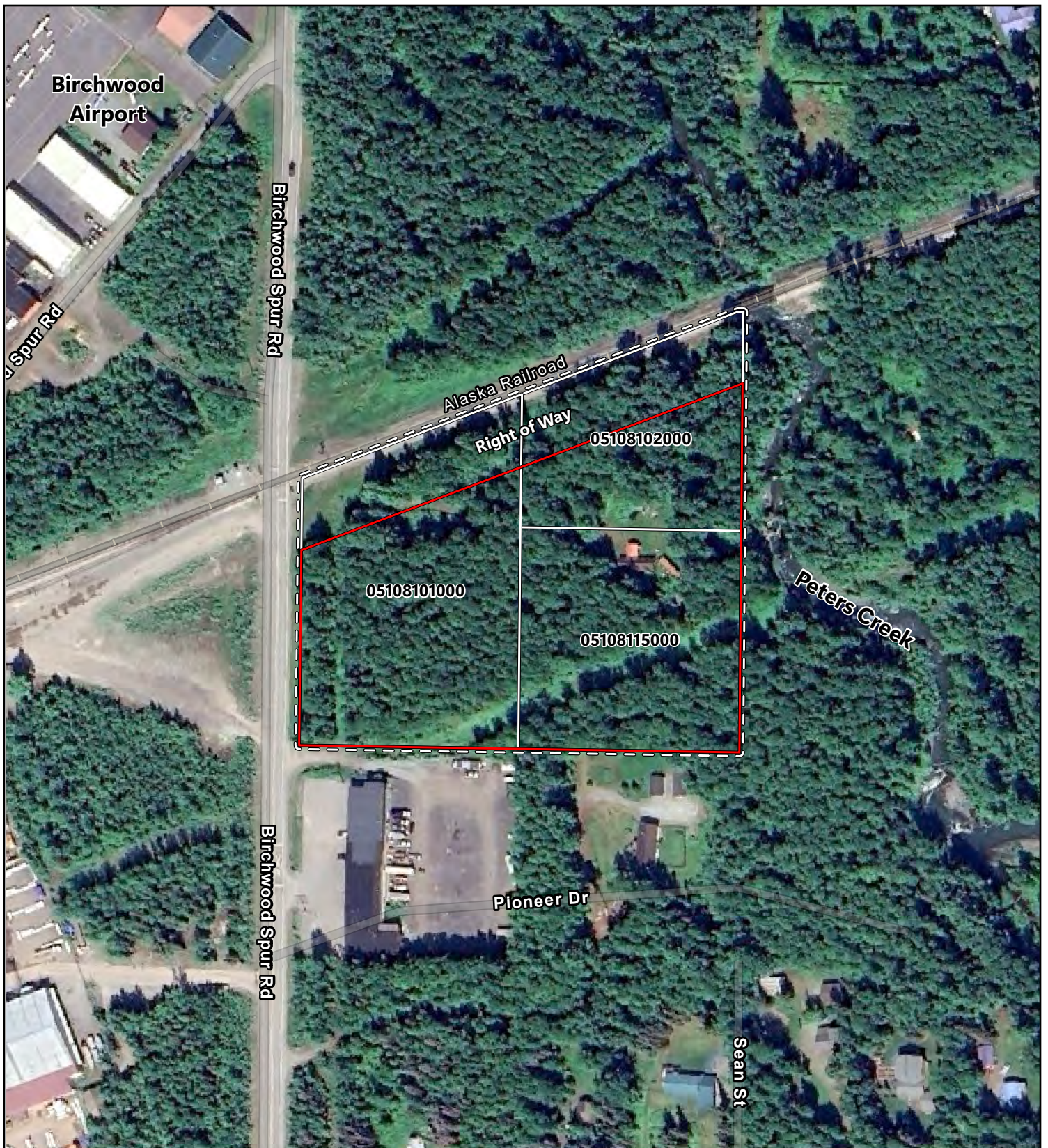
Species	Extra Small	Small	Medium	Large	Extra Large
Birch	269	539	389	200	0
Alder	8	4	1	2	0
Poplar	5	19	22	34	3
Spruce	48	22	28	24	0

References

National Park Service, 2010. Forests of Alaska. Available online at:
<https://npshistory.com/publications/usfs/handbooks/alaska/sec1.htm>. Accessed August 2024.

Resource Development Council, 2024. Alaska's Forest Industry. Available online at:
<https://www.akrdc.org/forestry>. Accessed August 2024.

USDA, 2010. Forests of Alaska. Available online at:
<https://npshistory.com/publications/usfs/handbooks/alaska/sec1.htm>. Accessed August 2024.



Legend

- Project Site
- Ondola Allotment
- County Parcels

Alluvial St
Image © 2024 Airbus



0 200 400 Feet

FIGURE 1
AERIAL PHOTOGRAPH

Appendix M
Tribal Ordinance



Native Village of Eklutna
Tribal Government

ORDINANCE 2024-__

AN ORDINANCE of the Native Village of Eklutna Approving Amendments to the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance

WHEREAS the Native Village of Eklutna (“Tribe”) is a sovereign federally-recognized Indian tribe and is a distinct, independent political community, and as such, is qualified to exercise powers of self-government by reason of its original tribal sovereignty as passed down from its ancestors since time immemorial;

WHEREAS, the Constitution of the Tribe was duly and properly enacted, adopted, and approved in accordance with all applicable Tribal laws, requirements, customs, and traditions by the members of the Tribe on January 27, 1996, by a majority of the qualified voters of the Tribe with a quorum of at least fifteen percent of those entitled to vote (the “Constitution”);

WHEREAS, Article II of the Constitution extends the authority of the Tribe to allotments of tribal members within the traditional lands and waters of the Eklutna people, including the allotment owned by the Ondola family located at Lots 64, 66 and 67, located within Section 5, T15N, R1W, Seward Meridian Alaska, containing approximately 8.05 acres, more or less (“Ondola Allotment”) and subject to a restriction on alienation pursuant to the Alaska Native Allotment Act; and

WHEREAS, Article IX, Section 1(g), (h) and (n) of the Constitution authorizes the Tribal Council to administer tribal assets and tribal economic affairs and enterprises, to promote the peace, safety, health, politics, education and general welfare of the Tribe and to enact ordinances and regulations; and

WHEREAS, the Tribe and Gaming Authority plan to engage in casino gaming under the Indian Gaming Regulatory Act (“IGRA”) and applicable regulations and standards of the National Indian Gaming Commission (“NIGC”) in a facility on the Ondola Allotment;

WHEREAS, on March 22, 2006 the Tribal Council adopted Ordinance No. 2007O-01 the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance regulating gaming on restricted lands within the jurisdiction of the Tribe;

WHEREAS, on April 19, 2024 the Tribal Council enacted the Native Village of Eklutna Gaming

Ordinance of 2024 (“Gaming Ordinance”) to govern gaming on the Ondola Allotment, referred to in the Gaming Ordinance as the Tribe’s Lands, pursuant to IGRA;

WHEREAS, on July 18, 2024, the NIGC determined that the Ondola Allotment constitutes Indian lands eligible for gaming by the Tribe under IGRA and that the Gaming Ordinance complied with IGRA and with applicable NIGC regulations and issued its approval of that Gaming Ordinance;

WHEREAS, the Tribal Council finds it necessary to amend the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance; and

NOW THEREFORE BE IT RESOLVED THAT, the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance shall be amended to read as follows:

EKLUTNA PUBLIC HEALTH AND SAFETY AND ANTI-DISCRIMINATION AT GAMING FACILITY ORDINANCE

Section 101 Definitions

(a) This Ordinance shall be referred to as the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance.

(b) The terms defined in the Native Village of Eklutna Gaming Ordinance of 2024 (“Gaming Ordinance”) shall have the same meaning in this Ordinance.

Section 105 Findings

The Tribal Council makes the following findings:

(a) Section 701 of the Gaming Ordinance requires that the construction, maintenance and operation of the Gaming Facility and the Gaming Operation be conducted in a manner that adequately protects the environment and the public health and safety and complies with the requirements and all applicable health, safety and environmental standards enacted by the Tribe.

(b) Section 801 of the Gaming Ordinance requires the Gaming Facility to secure a facility license for the Gaming Facility in which gaming is operated on Indian lands and establishes environmental, health, and public safety standards for issuance of such license by the Gaming Commission, including that the licensee (i) maintain an “orderly, clean and neat establishment,” (ii) subject the Gaming Facility to patrol by security personnel, (iii) permit inspection by authorized tribal officials and (iv) prohibit discrimination.

(c) The Tribe will continue to provide governmental services to the Ondola Allotment as it has done for decades. The Tribe relies on federal, state and local agencies to provide services that supplement services provided by the Tribe. The Tribe has historically collaborated with federal, state and local agencies regarding governmental services to the Ondola Allotment and the tribal community, and the Tribe will continue such collaboration to ensure the highest quality governmental services.

(d) While Alaska state and local governments have criminal jurisdiction in Indian country, including the Ondola Allotment, pursuant to Public Law 280, they lack civil regulatory jurisdiction.

(e) The Municipality of Anchorage provides police, fire protection and EMS services within municipal limits pursuant to Anchorage Municipal Code (AMC) 27.30.135(C), requiring that “[p]olice protection services shall be provided in the Anchorage Metropolitan Police Service Area,” which encompasses the Ondola Allotment. Chugiak Volunteer Fire and Rescue Company, Inc. provides fire protection and EMS services to the Chugiak area pursuant to AMC 27.30.060(B), requiring that “[f]ire protection shall be provided in the Chugiak Fire Service Area,” which encompasses the Ondola Allotment.

(f) Timber on the Ondola Allotment must be cut in connection with development of the Gaming Facility. The Bureau of Indian Affairs is evaluating whether a permit is required.

Section 110 Purpose

The purpose of this Ordinance is to provide for the protection of the health and safety of the employees, patrons and visitors of any Gaming Facility established pursuant to the Gaming Ordinance.

Section 115 Scope

These provisions shall apply to the Gaming Operation and the Gaming Facility, as defined in Sections 210 and 212 of the Gaming Ordinance of 2024 of the Native Village of Eklutna, on the Ondola Allotment.

Section 120 Building and Safety Standards

(a) The provisions of Title 23 of the Anchorage Municipal Code are adopted as tribal law and shall govern the construction, expansion, modification, and renovation of the Gaming Facility. The Tribal Council shall appoint a building official and such other officials necessary to implement and enforce such provisions.

(b) The provisions of Title 13, Chapter 50 of the Alaska Administrative Code are adopted as tribal law and shall govern the construction, expansion, modification, and renovation of the Gaming Facility. The building official appointed by the Tribal Council pursuant to paragraph (a) shall have authority to carry out the plan review and issue the corresponding approvals specified in 13 AAC 50.027.

(c) The Tribal Council has determined that no additional timber on the Ondola Allotment shall be cut until the Bureau of Indian Affairs either issues a permit for such timber removal or determines that no permit is required.

Section 125 Disabilities

The standards of the Americans with Disabilities Act, 42 U.S.C § 12101 *et seq.* are adopted as tribal law and shall govern any construction, expansion or modification of any gaming facility.

Section 130 Food and Beverage Handling

The provisions of Title 16, Chapter 16.60 of the Anchorage Municipal Code regulating the sale of food are adopted as tribal law governing the Gaming Facility.

Section 135 Water Supply and Water Quality

(a) The Eklutna Gaming Authority shall, consistent with applicable law, ensure adequate water supply to meet the needs of the Gaming Facility either by causing water to be delivered to the Gaming Facility, or by utilizing ground water from a well.

(b) The standards for water quality adopted by the U.S. Environmental Protection Agency pursuant to the Clean Water Act and the Safe Drinking Water Act, including the national primary water drinking regulations and national water quality standards in Title 40 of the Code of Federal Regulations, are adopted as tribal law governing the Gaming Facility.

Section 140 Workplace and Occupational Health and Safety

The standards for workplace and occupational health and safety established by the U.S. Occupational Health and Safety Administration, including 29 CFR parts 1910, 1926 and 1977, are adopted as tribal law governing the Gaming Facility.

Section 145 Public Health and Safety

(a) The Gaming Operation shall comply with applicable federal laws regarding public health and safety.

(b) Prior to commencement of gaming operations at the Gaming Facility, the Tribal Council shall ensure that police and fire protection services are available to the Gaming Facility by (i) entering into a binding agreement with the Municipality of Anchorage and/or Chugiak Volunteer Fire and Rescue Company, Inc. governing the provision of such services, or (ii) the provision of such services by the Tribe.

Section 150 Anti-Discrimination in Employment

(a) The relevant federal and state laws forbidding employers from discriminating in employment on the basis of race, color, religion, national origin, gender, sexual orientation, age or disability are adopted as tribal law governing the Gaming Facility.

(b) Nothing in subsection (a) shall prohibit the gaming operation and other enterprises in the gaming facility from giving preference in employment to Indians according to tribal law.

Section 155 Enforcement

Except to the extent provided otherwise in this Ordinance, the Gaming Commission shall enforce the provisions of this Ordinance pursuant to the Gaming Ordinance.

CERTIFICATION

This Ordinance was read and approved on the __th day of November 2024, by a vote of __ for, __ against, __ abstain and __ absent.

Attested to with our signatures fixed below by:

Aaron Leggett, President

Amanda Adams, Secretary