

APPENDICES

Eklutna Native Village Gaming Facility EA

Appendix A

Traffic Impact Analysis Report

Traffic Impact Study Report

Eklutna Native Village Gaming Facility

October 10, 2024

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Executive Summary

This report summarizes the results of the Traffic Impact Study (TIS) conducted for the proposed Eklutna Native Village Gaming Facility development located on Township 12 North, Range 1 West, Section 5, Lot 66, Seward Meridian and Township 15 North, Range 1 West, Section 5, Lots 64 and 67. The project site is also located within the Indian Base and Meridian in the Anchorage B-7 NW United States Geological Survey (USGS) 7.5' quadrangle map. The project will be located on Birchwood Spur Road, on the south side of the Alaska Railroad crossing, within the Municipality of Anchorage, Alaska. The project currently has two site circulation Options that will be analyzed, Option 1, and Option 2. Option 1 will consist of a single parking lot with dedicated inbound and outbound access with circulation. Option 2 will consist of a parking lot with one approach accessible by inbound and outbound access. The different options will not change the projected vehicle trips. Trip Generation will be based on the facility size and will remain consistent for all three site circulation options. The Gaming Facility itself would consist of a single building with a 33,000 square foot gaming floor, restrooms, support space etc., totaling 56,000 square feet. The planned staffing for the facility will be 30 employees per shift for three shifts, so 90 employees per day who will serve a planned 3,000 patrons daily on average.

The purpose of this report is to provide summaries of impacts of the development on traffic in the surrounding area and to provide recommendations on site circulation, consistent with the State of Alaska Department of Transportation requirements.

The following study intersections were selected based on their proximity to the project site and major throughfares in the area, as well as the availability of existing traffic volume data:

1. Birchwood Spur Road and Birchwood Loop Road (One-Way Stop)
2. Birchwood Loop Road and Pilots road (One-Way Stop)
3. Birchwood Loop Road and Glenn Highway (AK1) Southbound on/off ramp (Stop on Off Ramp)
4. Birchwood Loop Road and Glenn Highway (AK1) Northbound on/off ramp (Stop on Off Ramp)
5. Birchwood Loop Road and Old Glenn Highway (One-Way Stop)

Project Trip Generation

Pannone Engineering Services developed estimated project trip generation for the proposed project based on published trip generation rates from the Institute of Transportation Engineers (ITE) publication Trip Generation (11th Edition), Land Use 473 (Casino).

Based on number of gaming stations (1,000 gaming stations).

- Per the ITE the proposed project is expected to generate 8,010 weekday trips and 10,550 Saturday trips.
- The am peak hour volume on a weekday is expected to generate 400 trips, 228 entering and 172 exiting.
- The 4pm-6pm one hour (peak hour of adjacent street traffic) volume on a weekday is expected to generate 460 trips, 235 entering and 225 exiting.
- The pm peak hour volume on a weekday is expected to generate 590 trips, 307 entering and 283

exiting.

- The Saturday peak hour volume is expected to generate 620 trips, 335 entering and 285 exiting.

Existing Conditions

The State of Alaska defaults to USDOT/ Federal Highways Administration on many issues including Level of Service (LOS) designations. The USDOT refers to the most recent addition of the Highway Capacity Manual. During this scenario, all of the study intersections operate within LOS A (Free flow, with low volumes and high speeds.) as defined by the HCM.

The project location is on a road with a functional classification of “Minor Collector” road as defined by ADOT, and published at <https://akdot.maps.arcgis.com/apps/mapviewer/index.html?webmap=8d34059bbfed4fada20a4fdc2a138aca>. It is located approximately 1 mile from the end of the road at a small local airport with no scheduled passenger service, with access additionally to a small residential neighborhood and a shooting range. The traffic passing the development along this segment has an existing Annual Average Daily Traffic (AADT) of 1,140 trips per day. From the project to the south/southeast in the direction of typical access it is approximately 0.5 miles to the intersection of Birchwood Spur Road and N. Birchwood Loop Road. The ‘T’ intersection allows uninterrupted traffic flow from Birchwood Spur Road onto N Birchwood Loop Road, with a stop sign controlled N Birchwood Loop Road continuing to the West (AADT 415). The 45mph speed limit road (AADT 1,456-2,625) follows rolling terrain and curves past stop sign controlled local roads accumulating additional residential traffic for one additional mile until it reaches Pilots Road and the Glenn Highway. Pilots Road (AADT 701) accesses N Birchwood Loop Road from the west at a ‘T’ intersection with a one way stop for Pilots Road, and a left turn lane for northbound N Birchwood Loop Road access. Approximately 280 feet to the southeast of the Pilots Road intersection is the intersection with the southbound Glenn Highway on/off ramp. The southbound Glenn Highway off ramp (AADT 961) is controlled by a stop sign. The southbound Glenn Highway on ramp (AADT 1,610) is accessed by a right turn lane from the southbound N Birchwood Loop Road traffic and by a left turn lane from the northbound N Birchwood Loop Road traffic. An additional 450 feet south on N Birchwood Loop Road (AADT 4,402) is the intersection with the northbound Glenn Highway on/off ramps. The northbound Glenn Highway off ramp (AADT 1,826) is controlled by a stop sign. The northbound Glenn Highway on ramp (AADT 1,438) is accessed from N Birchwood Loop Road through a southbound left turn lane and a northbound right turn lane.

Existing Conditions plus Post Development Project Conditions

During the post-development scenario the trips generated would not meet Service Level D under the ITE volume based on gross floor area (using only gaming area). N Birchwood Loop and associated study intersections would have to be upgraded under this estimate.

During the post-development scenario the trips generated would meet Service Level D on the roadway, based on number of gaming stations.

Site Circulation Options

Option 1: Option 1 will consist of a single parking lot with dedicated inbound and outbound access with circulation. Nearly all traffic accessing the site will access from the south on Birchwood Spur Road. The inbound access would route on a one way access into the site accessed by a right turn from Birchwood Spur Road eastward through the 50' Right-of-Way along the south side of the lot. Outbound traffic would leave the site from a one way access on the west side of the lot, left turning directly onto Birchwood Spur Road.

Option 2: Option 1 will consist of a parking lot with one approach accessible by inbound and outbound access. The access would be located on the west lot line of the development, with access directly onto Birchwood Spur Road.

1.0 Introduction

Project Description

This report summarizes/will summarize the results of the TIS being conducted for the proposed Gaming Facility project located on Birchwood Spur Road, on the south side of the Alaska Railroad crossing, near the Eklutna Native Village, within the Municipality of Anchorage, Alaska. There is one proposed project alternative that consists of a Gaming Facility with a 33,000 square foot gaming floor and support area with a total area of 58,000 square feet. Access will be through Birchwood Spur Road.

This chapter discusses the TIS purpose, project study area, and analysis scenarios. Figure 1 shows the study area, project location, study intersections, and study segments that were analyzed. Figure 2 and Figure 3 show Site circulation options. Figures 6-8 show existing available traffic counts. Figures 9-11 show the existing lane configurations of the study intersections.

Study Purpose

The purpose of this report is to provide summaries of changes in Vehicle Trips and traffic impacts on the surrounding transportation system with the proposed project. The Alaska DOT has not published criteria on impact thresholds for evaluating Vehicle Trips, PES is using Level of Service as defined by the Federal Highway Administration with a threshold of C/D for peak traffic hours.

Study Intersections

PES is evaluating traffic conditions at five study intersections during the a.m. and p.m. peak hours for a typical weekday, as well as the Saturday peak period to account for the “recreational” nature of the project. Data collection efforts included measuring existing traffic counts and utilizing material available through Alaska Department of Transportation.

1. Birchwood Spur Road and Birchwood Loop Road (One-Way Stop)
2. Birchwood Loop Road and Pilots road (One-Way Stop)
3. Birchwood Loop Road and Glenn Highway (AK1) Southbound on/off ramp (Stop on Off Ramp)
4. Birchwood Loop Road and Glenn Highway (AK1) Northbound on/off ramp (Stop on Off Ramp)
5. Birchwood Loop Road and Old Glenn Highway (One-Way Stop)

Study Scenarios

- Existing Conditions – This scenario evaluates the study intersections based on existing traffic volumes, lane geometry and traffic controls.
- Existing conditions plus additional project vehicle traffic – This scenario evaluates the study intersections based on existing traffic volumes with the addition of the proposed project traffic, lane geometry and traffic controls remaining existing.
- Existing Conditions plus Additional Project Vehicle Traffic with Roadway Improvements – This scenario evaluates the study intersections based on existing traffic volumes with the addition of the proposed development traffic and recommended roadway improvements in the study area.

Figure 1: Study area, Project location, Study Intersections, and Study Segments that were analyzed.

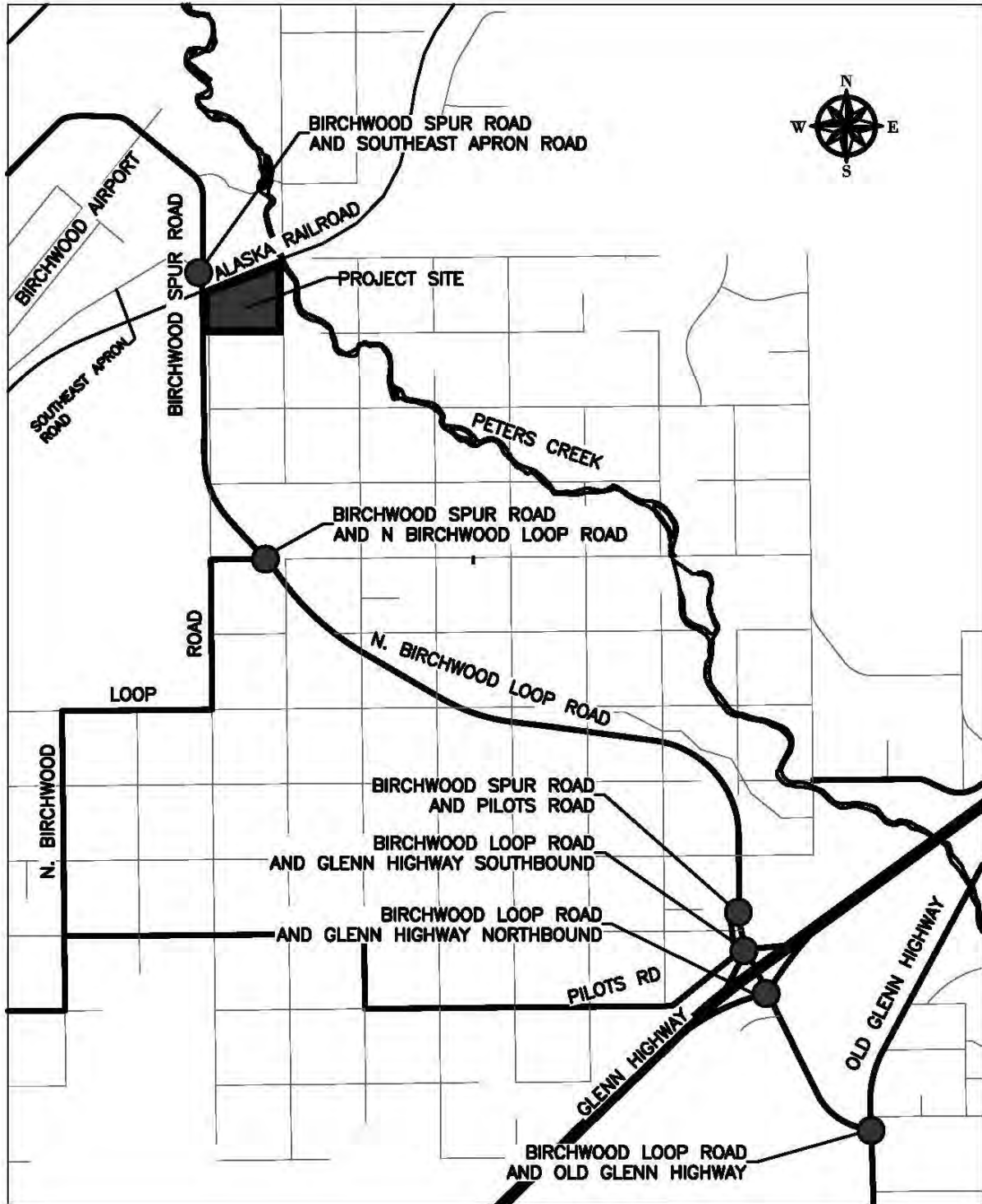


Figure 2: Site Circulation Option 1

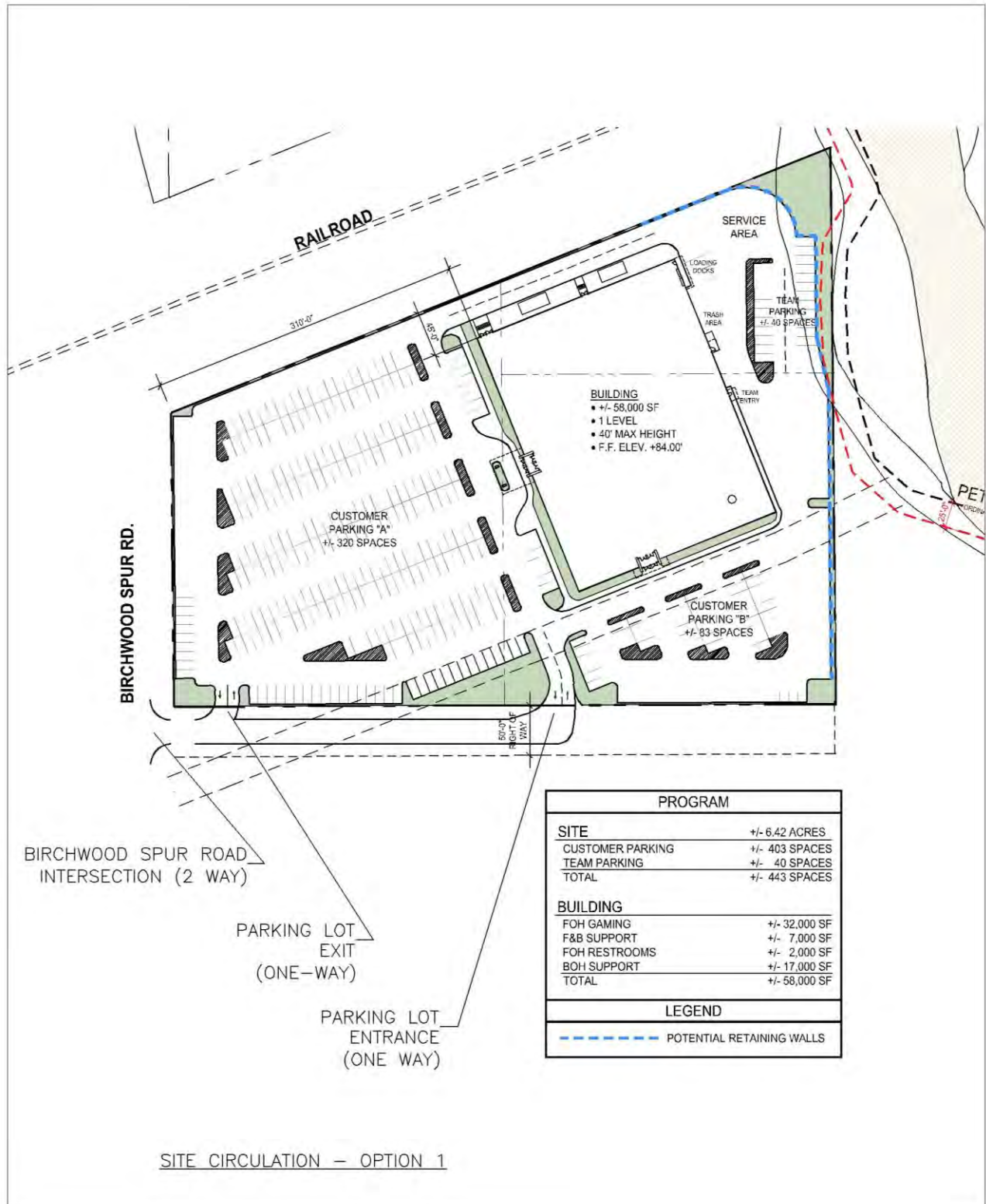
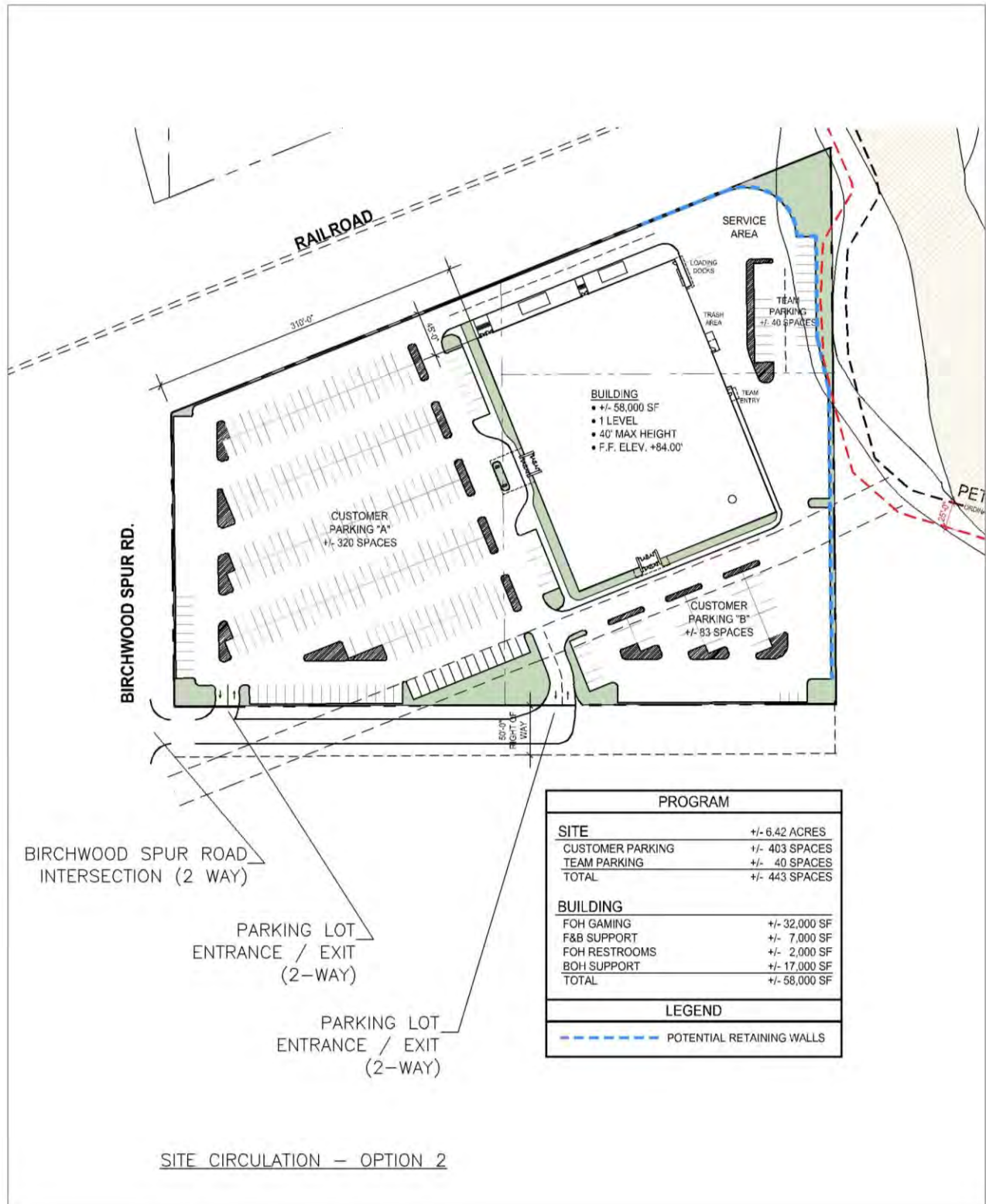


Figure 3: Site Circulation Option 2



2.0 Study Methodology

Traffic impacts related to the proposed project were evaluated for Level of Service based on Average Annual Daily Trips and Peak Hourly Trips for select days and times.

Level of Service

LOS is a quantitative stratification of a performance measure or measures that represent quality of service, measured on an A-F scale, with LOS A representing the best operating conditions from the travelers perspective and LOS F the worst. There are three classes of two-lane highways defined in the USDOT FHA Simplified Highway Capacity Manual. The study area is most closely defined as Class III. A Class III two-lane highway is defined as *“Class III two-lane highways serve moderately developed areas. They may be portions of a Class I or Class II highway that pass through small towns or developed recreational areas. Local traffic often mixes with through traffic on these segments, and the number of unsignalized driveways and cross-streets is noticeably higher than in a purely rural area. Class III highways can include longer roadway segments passing through more spread-out recreational areas, also with increased roadside densities. Such segments are often accompanied by reduced speed limits that reflect the higher activity level.”*

Level of Service Description Percent of Free-Flow Speed for Class III Two-Lane Highways

The LOS on Class III two lane road is based on Percent of Free-Flow Speed (PFFS). PFFS was found by dividing the average travel speed (ATS) by the free flow speed (FFS).

- LOS A PFFS >91.7
- LOS B PFFS >83.3-91.7
- LOS C PFFS >75.0-83.3
- LOS D PFFS >66.7-75.0
- LOS E PFFS <=66.7
- LOS F Demand greater than capacity.

The confining factors on a Class III are lane and shoulder width, access point density, grades, truck traffic, and no passing zones. Free Flow Speed (FFS) measured on N Birchwood Loop through the study section was 40.4mph. The generalized service volume to maintain LOS D per HCM on a Class III two-lane highway is 13,900 trips per day through rolling terrain under optimal conditions. The capacity in either direction is 1,700 vehicles per hour (vph), maintaining service level D. The PFFS at the existing and existing plus project conditions flow was found to be 99+ from the project site to just northwest of the Glenn Highway interchange. All two-lane segments will maintain LOS A.

Unsignalized Intersections

There are 13 minor unsignalized intersections between the project site and the Glenn Highway On/Off ramps. The intersections were each analyzed using Turn Lane Warrant tables found in the “1967 Highway Record 211 (Highway Research Board)” and the 1985 “National Cooperative Research Program Report 279”, both used in the development of the HCM. The tables were developed by “consensus of traffic engineers, based on the study of existing intersections” and output maximum volumes in vehicles per hour (vph) that can turn without increasing the probability of bringing an arriving through vehicle to

a stop at a rate more than 0.020 at the design speed of 50 mph. See figures 4 and 5. Additionally, all intersections were analyzed for intersection capacity with CAPX “Capacity Analysis for Planning of Junctions” software provided by U.S. Department of Transportation Federal Highway Administration.

Level of Service Description Volume-to-Capacity Ratio for Unsignalized Intersections

Level of Service is determined with: $S = sf / (1 + a(v/c)^b)$

S: Level of Service

Sf: Free Flow Speed

V: Volume

C: Capacity

a: 0.15

b: 4

LOS A S=0.0 to 0.2

LOS B S=0.2 to 0.4

LOS C S=0.4 to 0.7

LOS D S=0.7 to 0.8

LOS E S=0.8 to 1.0

LOS F S>1

Source: Highway Capacity Manual 6th Ed., Chapter 20 (Transportation Research Board, 2010)

Figure 4: Right Turn Warrant Table

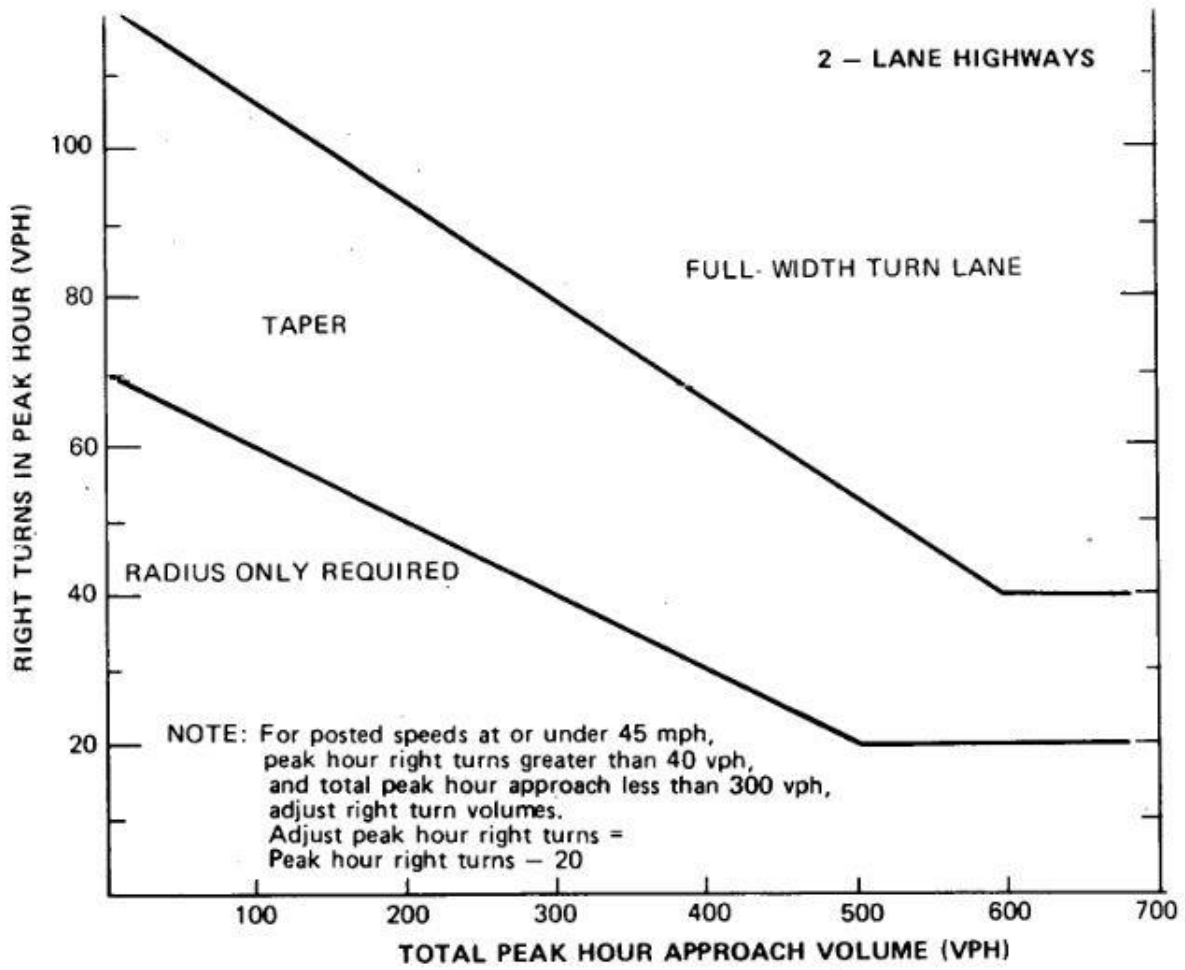


Figure 5: Left Turn Warrant Tables

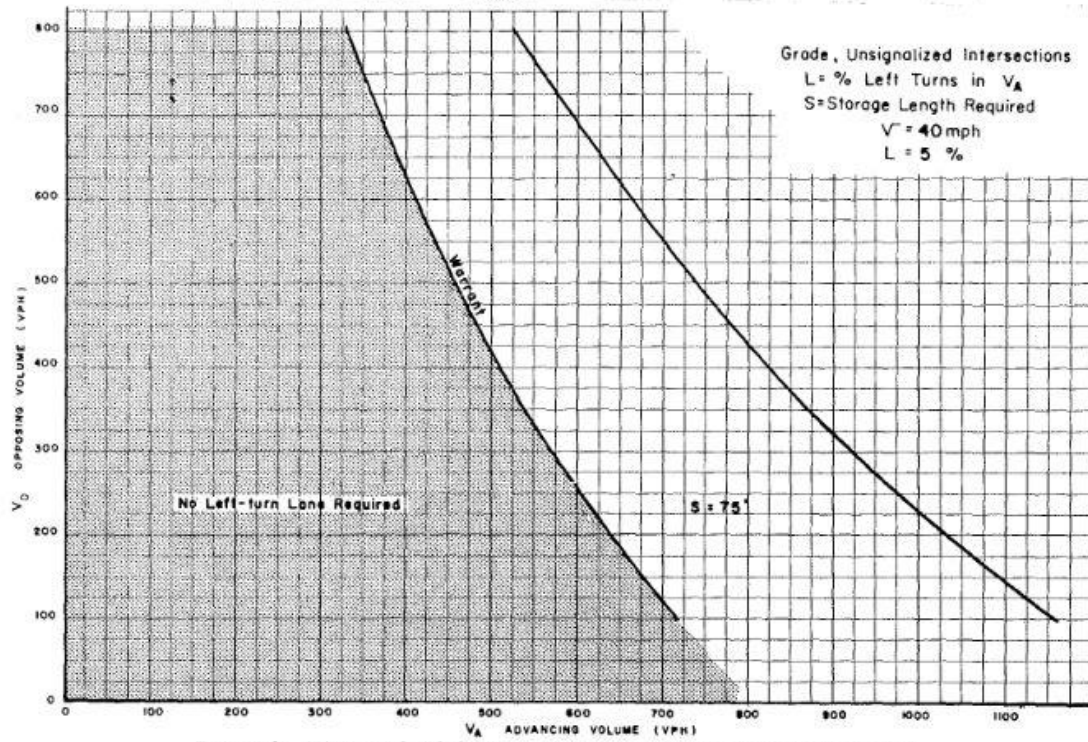


Figure 2. Warrant for left-turn storage lanes on two-lane highways.

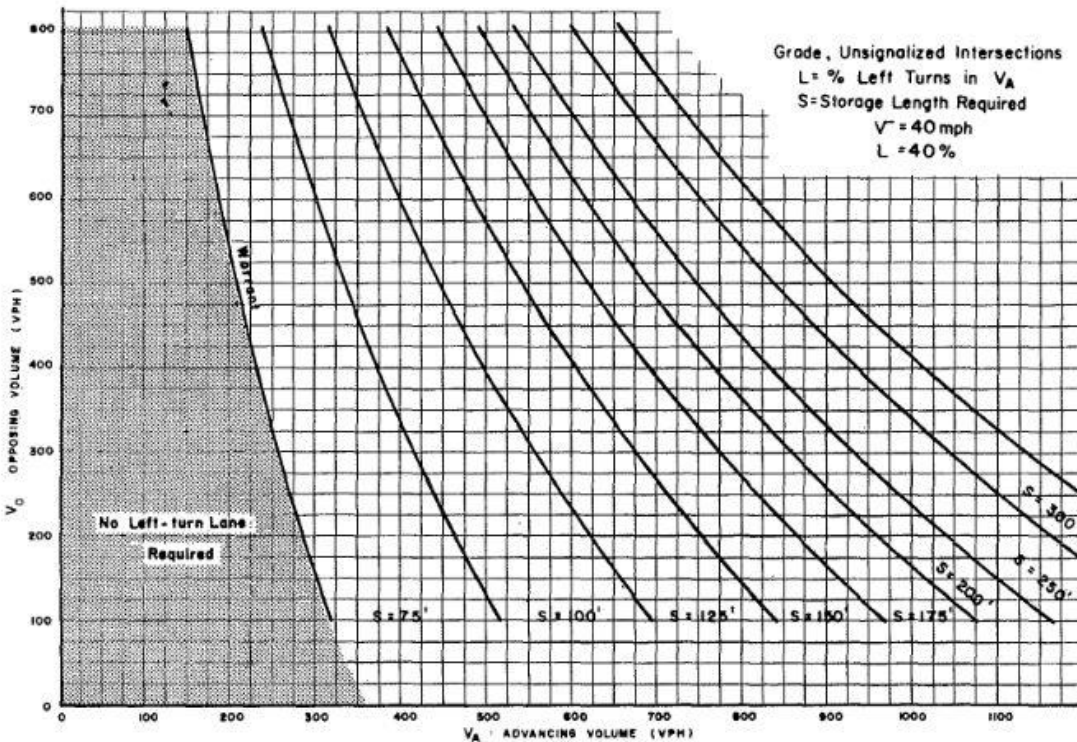


Figure 7. Warrant for left-turn storage lanes on two-lane highways.

3.0 Existing Conditions

This Section describes existing traffic volumes and operating conditions at the study intersections, including the results of the LOS calculations.

Existing Traffic Conditions

PES evaluated existing traffic conditions at selected study intersections and study segments during the a.m. and p.m. peak hours on a typical weekday, and during the peak hours on a typical Saturday. Intersection turning movement counts were estimated based on measured volumes on each roadway. Alaska DOT record average annual daily traffic (AADT) counts available for the year 2020 were used in the initial version of this report for each study segment, and updated with AADT counts from Alaska DOT collected in the year 2012, and AADT collected by PES from August 23, 2024 to September 20, 2024. The AADT counts are shown on Figures 6-8. Peak hour traffic counts from 2024 are shown on figures 9-11. Additional traffic volumes are available at <https://akdot.maps.arcgis.com/home/webmap/viewer.html?webmap=7c1e1029fdb64d7a86449d55ef05e21c> dating back annually to 2012. The existing traffic volume has been stable for a number of years.

Existing Alaska Railroad Crossing

The existing railroad crossing adjacent to the site is owned/ operated by a low volume railroad. The railroad does not exceed one train per hour and during the off-peak season may only see one or two trains per day. The effect on traffic will be insignificant.

Figure 6: Average Annual Daily Traffic Volumes (Alaska DOT Record Data 2012)

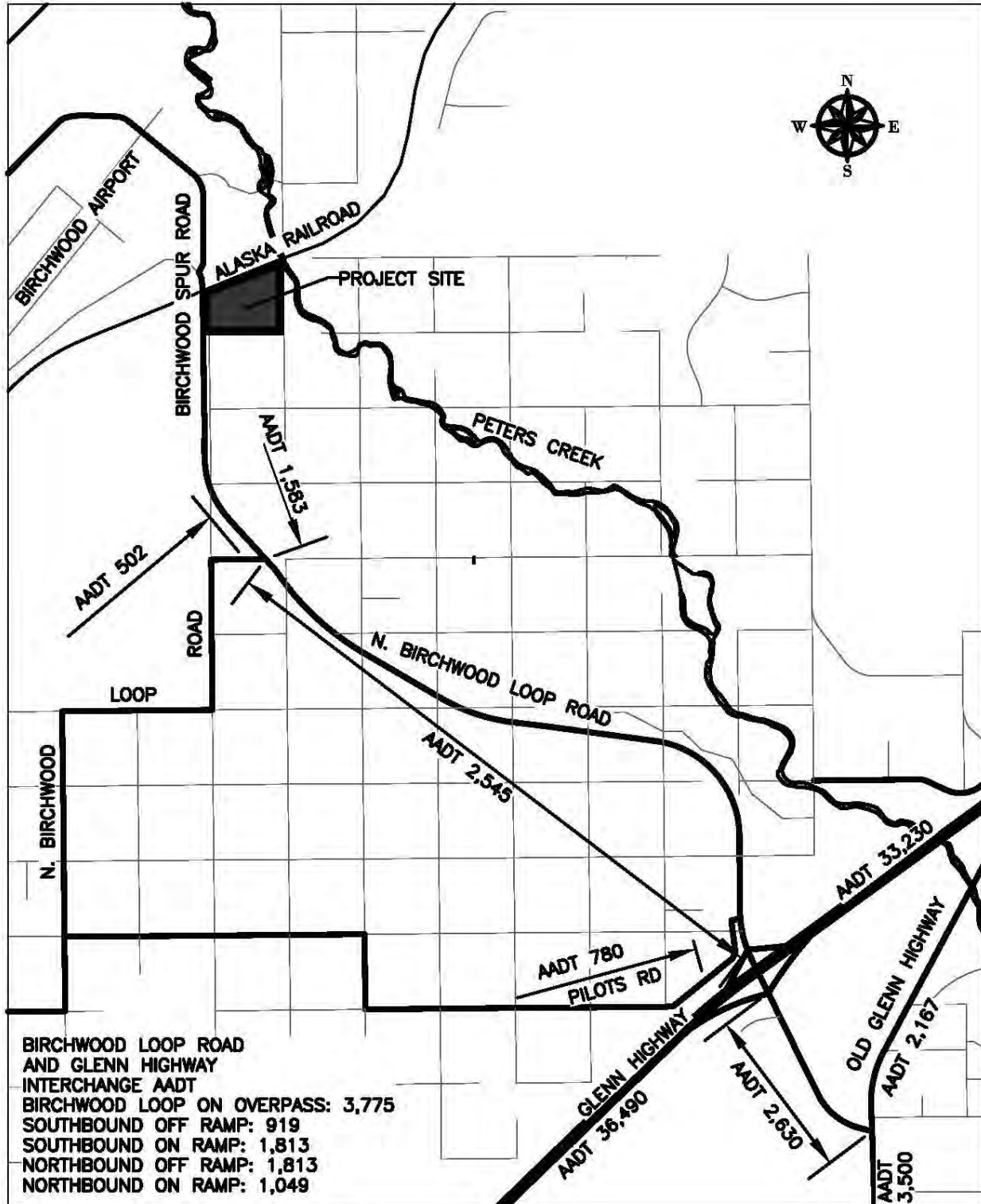


Figure 7: Average Annual Daily Traffic Volumes (Alaska DOT Record Data 2020)

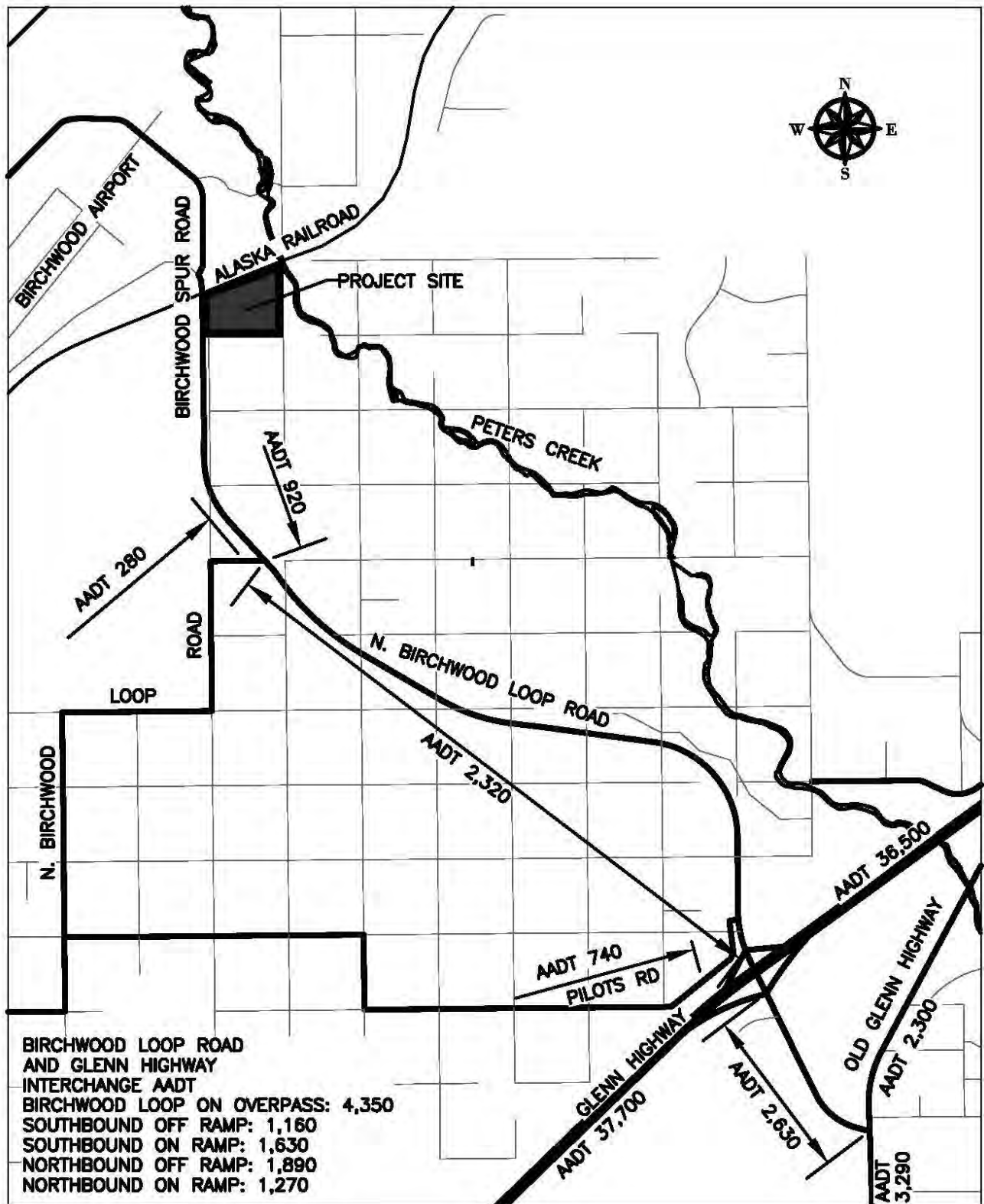


Figure 8: Average Annual Daily Traffic Volumes (August 23 to September 20, 2024)

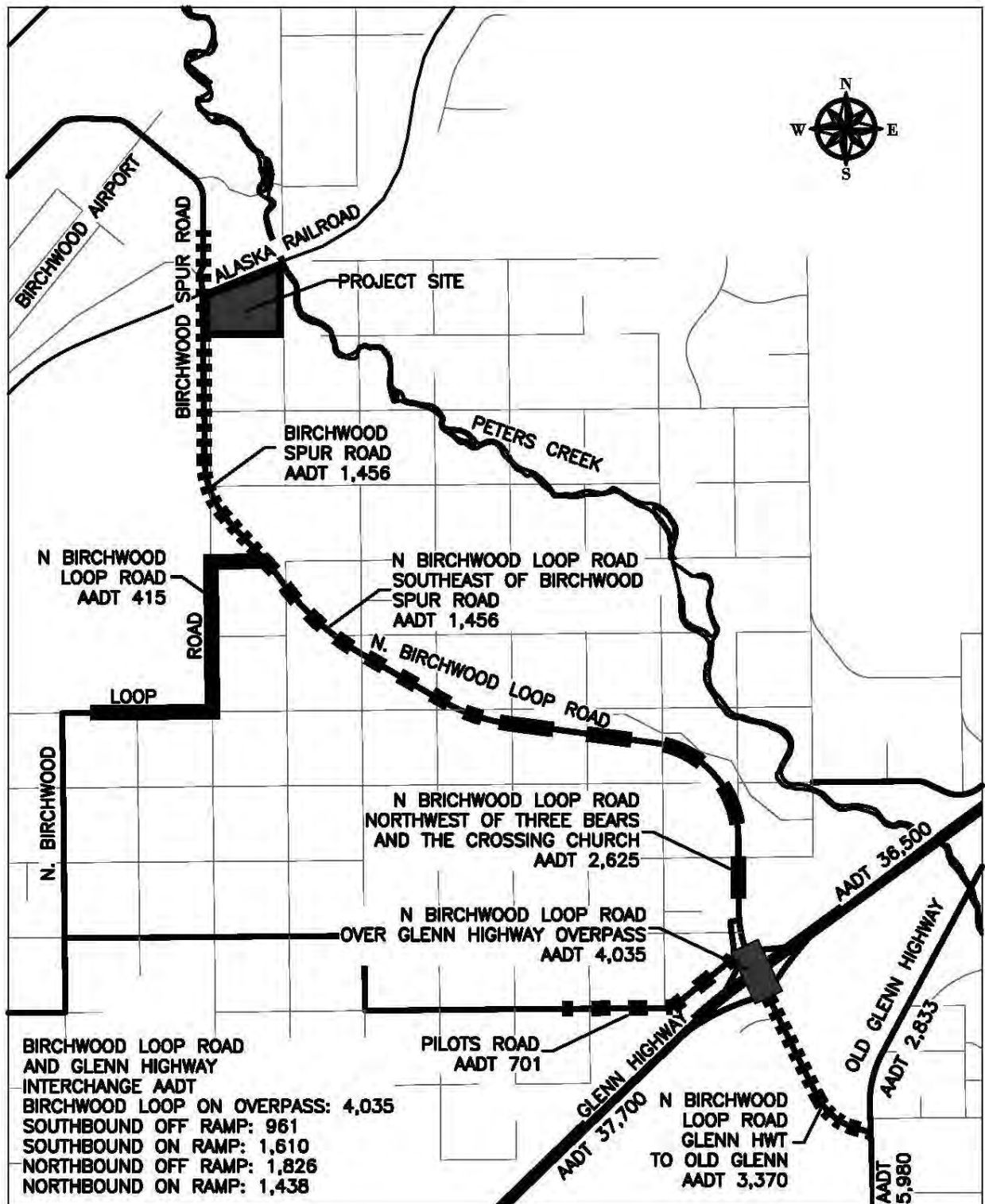


Figure 9: Project Lane Geometry Existing Conditions

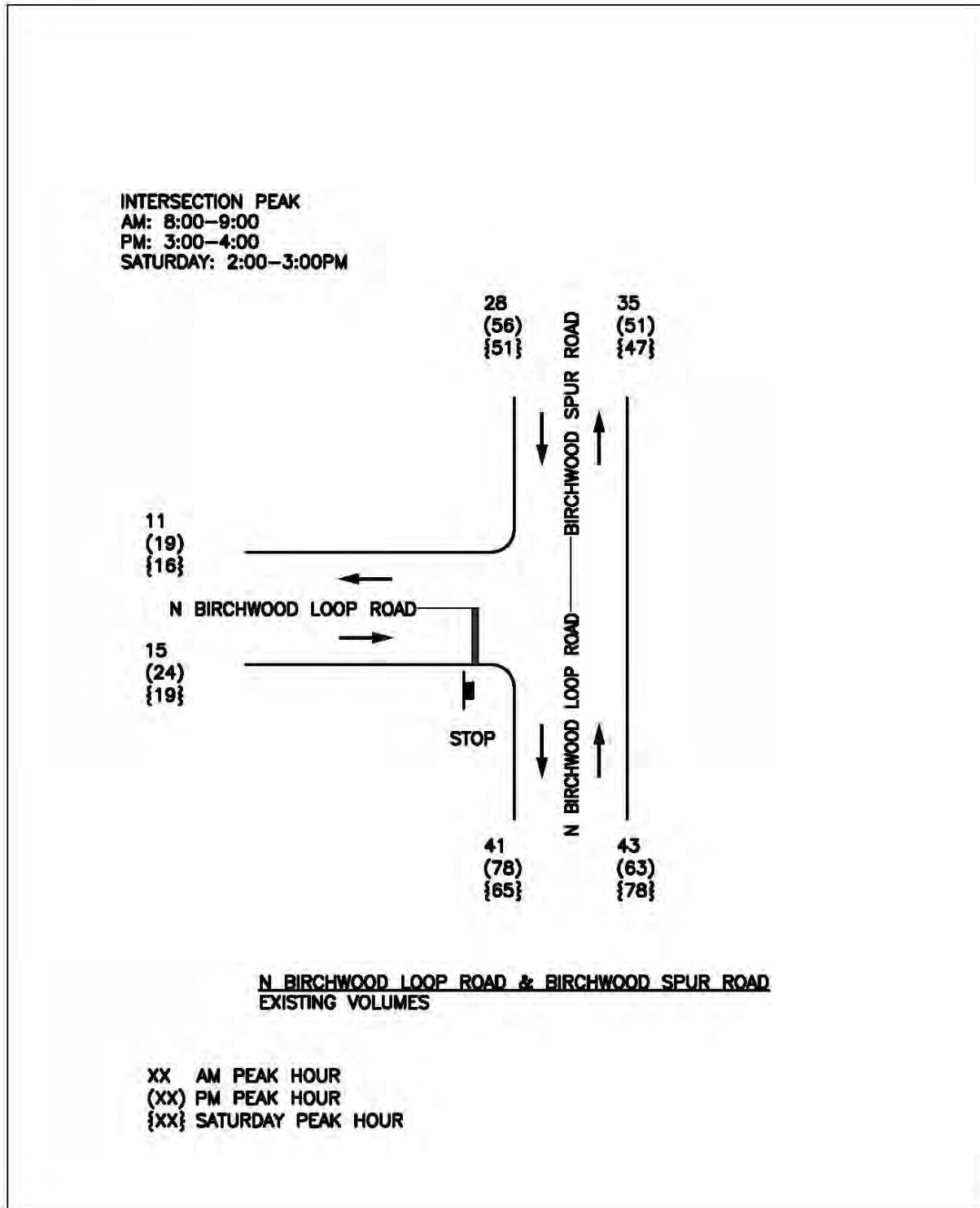


Figure 10: Project Lane Geometry Existing Conditions

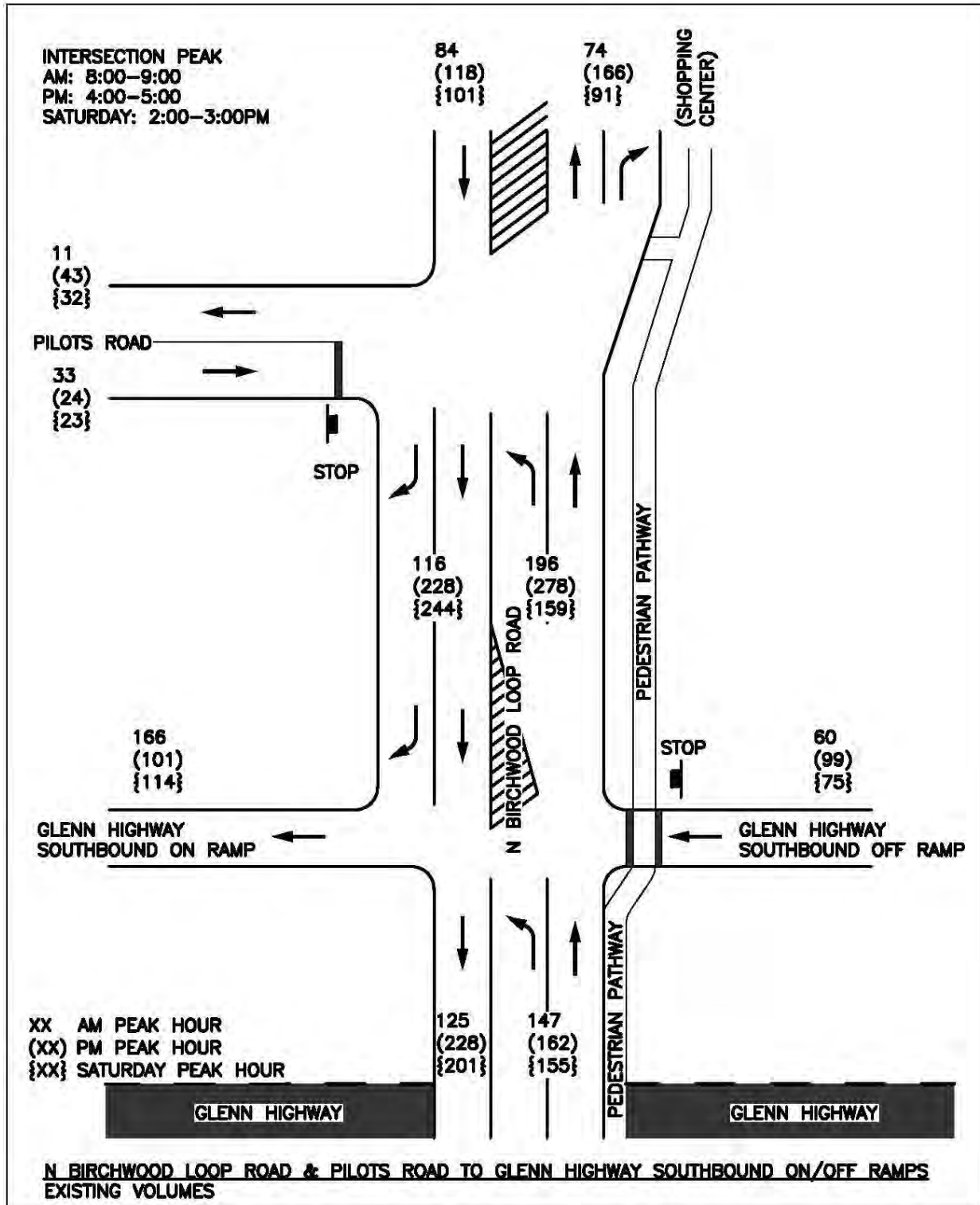
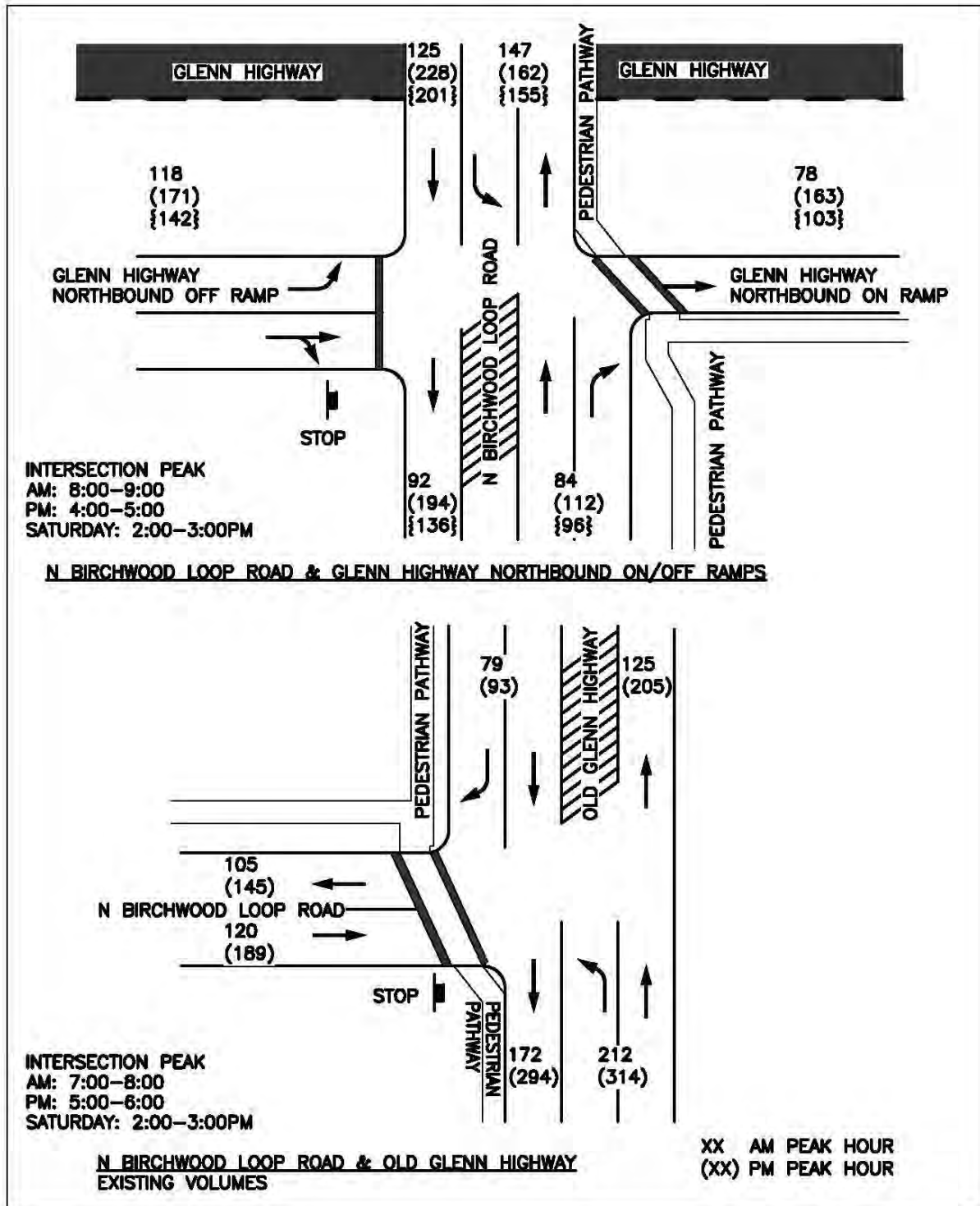


Figure 11: Project Lane Geometry Existing Conditions



Intersection Level of Service Analysis – Existing Conditions

This scenario evaluates the study intersections based on adjacent existing traffic volumes, and existing lane geometry and traffic controls, as described above. Peak hour traffic was observed through the study intersections from August 23, 2024 to September 20, 2024. Turning volumes are in the range of 5% or less of total intersection volumes at all minor intersections (those north and west of the highway interchange). Based on the generally light traffic in the area all study intersections currently operate within LOS A. The intersections are all low volume with peak hour right turns and left turns below the right turn lane and left turn lane warrant level with an advancing and opposing vehicle rate of 450vph and a turning volume of <5%.

The highway interchange with the Glenn Highway was analyzed using the same tables. A left turn lane with advancing and opposing traffic at a volume of 450vph each, and a turning volume of 40% of vehicles well require a turn lane with a queueing lane of 100 feet. The existing queueing lanes range in length from 100 feet to 185 feet. While the warrant tables above are a good indicator of capacity the intersections were also analyzed per the HCM below.

The PFFS on each segment of the study area was 99+ equating to LOS A.

The busiest intersection in the study area was analyzed with existing conditions in CAPX and found to have a Volume-to-Capacity Ratio (VC) of 0.0 to 0.1, and a LOS of A. The queueing length was calculated to by PES with the equation below (Section 4.0) and was found to be less than 1 vehicle at the peak hours. See Figures 12-13 for CAPX output for the Glenn Highway Southbound Ramps and N Birchwood Loop intersection.

Volume to Capacity Ratios and Level of Service:

Birchwood Spur Road and Birchwood Loop Road	VC: ≤ 0.1	LOS: A
Birchwood Loop Road and Pilots road	VC: ≤ 0.1	LOS: A
Birchwood Loop Road and Glenn Highway (AK1) Southbound on/off ramp	VC: ≤ 0.1	LOS: A
Birchwood Loop Road and Glenn Highway (AK1) Northbound on/off ramp	VC: ≤ 0.1	LOS A
Birchwood Loop Road and Old Glenn Highway	VC: ≤ 0.1	LOS A

Figure 12: Glenn Highway Southbound Ramps and N Birchwood Loop Intersection CAPX output Existing Conditions

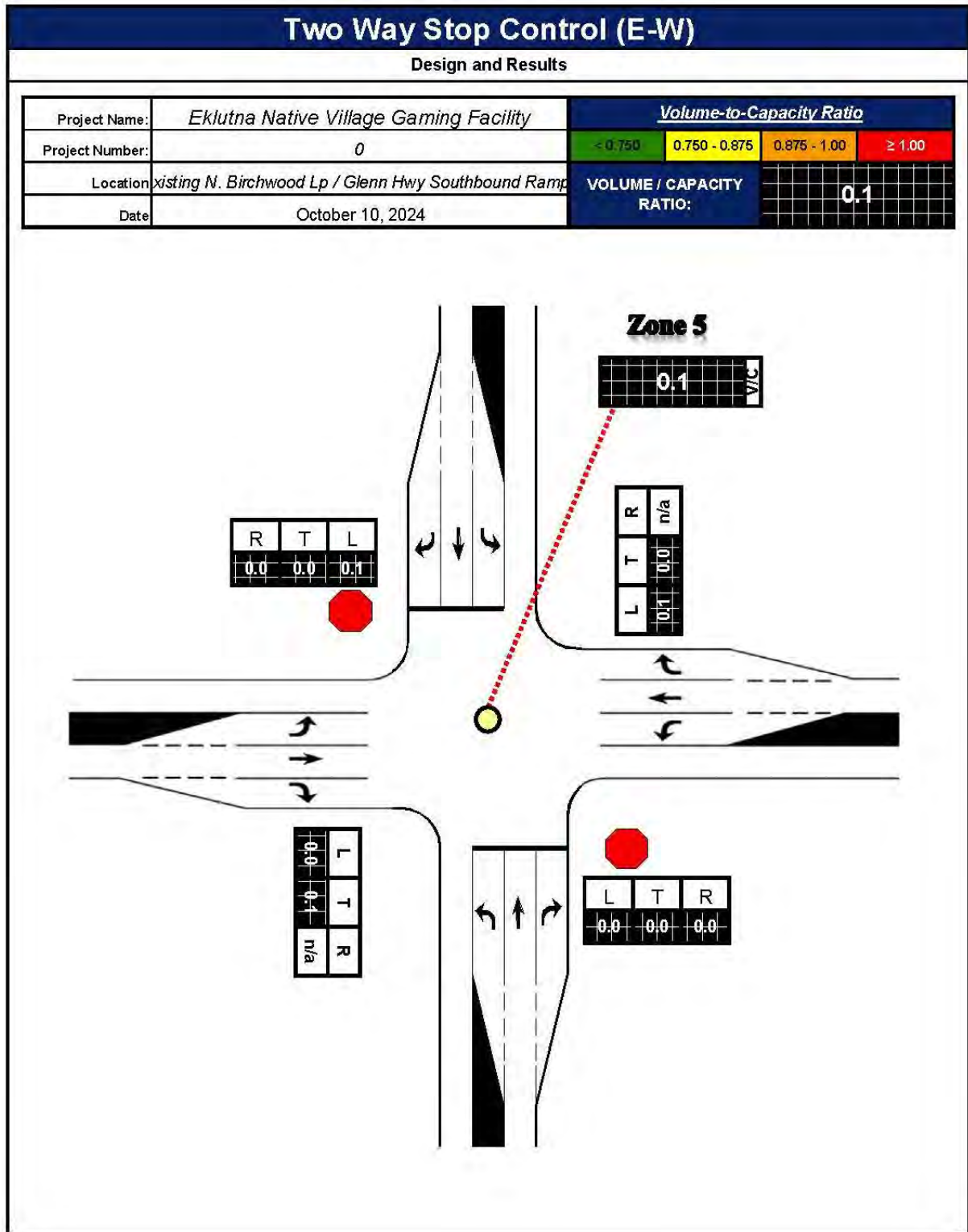
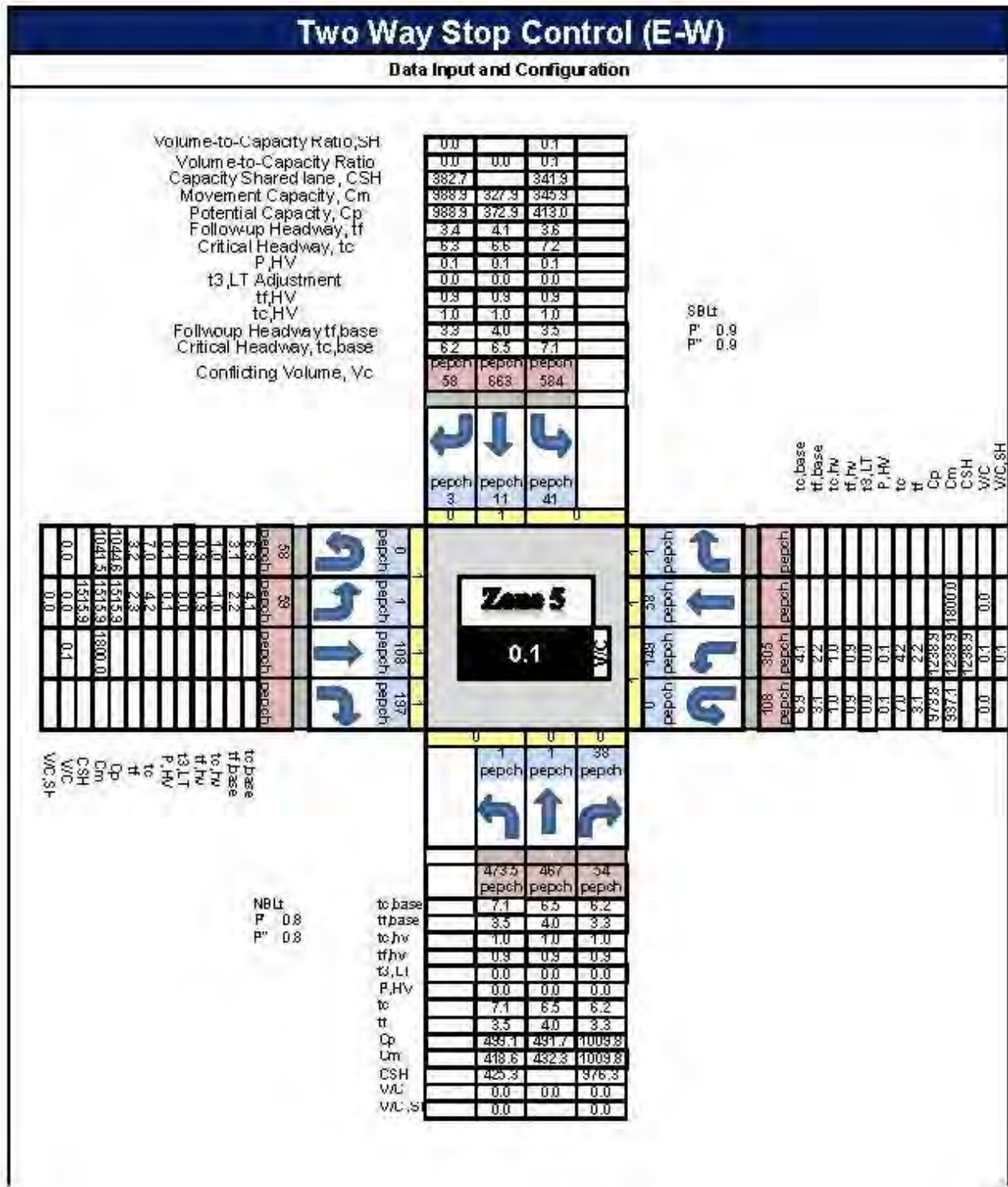


Figure 13: Glenn Highway Southbound Ramps and N Birchwood Loop Intersection CAPX output Existing Conditions



4.0 Existing Conditions Plus Project Conditions

This analysis scenario presents the impacts of the proposed project at the study intersections and surrounding roadway system. This scenario evaluates existing conditions with the addition of traffic from the proposed project. The proposed project will develop a currently vacant lot and construct a Gaming Facility with 33,000 square feet of gaming area and 1,000 gaming positions.

Project Trip Generation

Pannone Engineering Services developed estimated project trip generation for the proposed project based on a combination of published trip generation rates from the Institute of Transportation Engineers (ITE) publication Trip Generation (11th Edition), Land Use 473 (Casino).

Based on number of gaming stations (1,000 gaming stations) per the ITE the proposed project is expected to generate-

- 8,010 weekday trips
- 10,550 Saturday trips
- The am peak hour volume on a weekday is expected to generate 400 trips, 228 entering and 172 exiting.
- The 4pm-6pm one hour volume on a weekday is expected to generate 460 trips, 235 entering and 225 exiting
- The pm peak hour volume on a weekday is expected to generate 590 trips, 307 entering and 283 exiting.
- The Saturday peak hour volume is expected to generate 620 trips, 335 entering and 285 exiting.

Project Trip Distribution and Assignment

Trip distribution is a process that determines in what proportion vehicles would be expected to travel between the project site and various destinations outside the project study area. Assignment determines the various routes that vehicles would take from the project site to each destination using the calculated trip distribution. Trip distribution assumptions for the proposed development project were developed based on the distribution of the population served by the development. The site is served by one major throughfare with a local population small enough to be considered negligible. The distribution assumption is as follows:

- 40 percent to/from Glenn Highway (AK1) to the North
- 60 percent to/from Glenn Highway (AK1) to the South

Intersection Level of Service Analysis – Existing Plus Project Conditions

The study intersections were analyzed for level of service, turn lane warrant, and queueing length. The intersection LOS analysis results for Existing plus Project Conditions are shown for Saturday Peak Hour, the busiest time on the route. Each intersection was analyzed and assigned a volume/capacity ratio. Turn lane warrant was analyzed for each intersection and lane queueing calculations were performed for turn lanes. Traffic volumes collected in 2024 plus trips generated above were used for analysis. PES used CAPX to generate a volume/capacity ratio and a maximum capacity for each intersection.

For required queueing length the following equation was used per the HCM.

$$Q_{95} \sim \left[\frac{V_x}{C_{mx}} - 1 + \sqrt{\left(\frac{V_x}{C_{mx}} - 1 \right)^2 + \frac{\left(\frac{3600}{C_{mx}} \right) \left(\frac{V_x}{C_{mx}} \right)}{150T}} \right] \left(\frac{C_{mx}}{3,600} \right)$$

Where

Q_{95} = 95th percentile queue (veh),

V_x = flow rate for movement x (veh/h),

C_{mx} = capacity of movement X (veh/h), and

T = analysis time period (0.25h for a 15-min period) (h).

See Figures 14-16 for 2024 traffic counts plus project conditions at peak hours.

The left turn lane from the Northbound Glenn Highway Off Ramp onto N Birchwood Loop has a 4 car queue under the existing plus project conditions Saturday Peak Hour Volume. The existing left turn lane has a length of 130 feet, and will accommodate this. The Southbound Glenn Highway Off Ramp has a combined left-through-right lane and a queueing length of 2 cars. No other existing turn lane in the study area had a queueing length of greater than one vehicle with added project conditions. None of the intersections without existing turn lanes had a queueing length. See intersection analysis CAPX output figures 17-26.

Volume to Capacity Ratios and Level of Service:

Birchwood Spur Road and Birchwood Loop Road Saturday Peak Hour	VC: 0.2 LOS: A	
Birchwood Loop Road and Pilots road Saturday Peak Hour	VC: 0.3 LOS: B	
Birchwood Loop Road and Glenn Highway (AK1) Southbound on/off ramp Saturday Peak Hour	VC: 0.4 LOS: B	*SB L-T-R lane Queue 2 veh.
Birchwood Loop Road and Glenn Highway (AK1) Northbound on/off ramp Saturday Peak Hour	VC: 0.7 LOS C/D	*NB Left Turn Queue 5 veh.
Birchwood Loop Road and Old Glenn Highway Saturday Peak Hour	VC: 0.2 LOS	

Figure 14: Project Lane Geometry Existing Conditions Plus Project Conditions

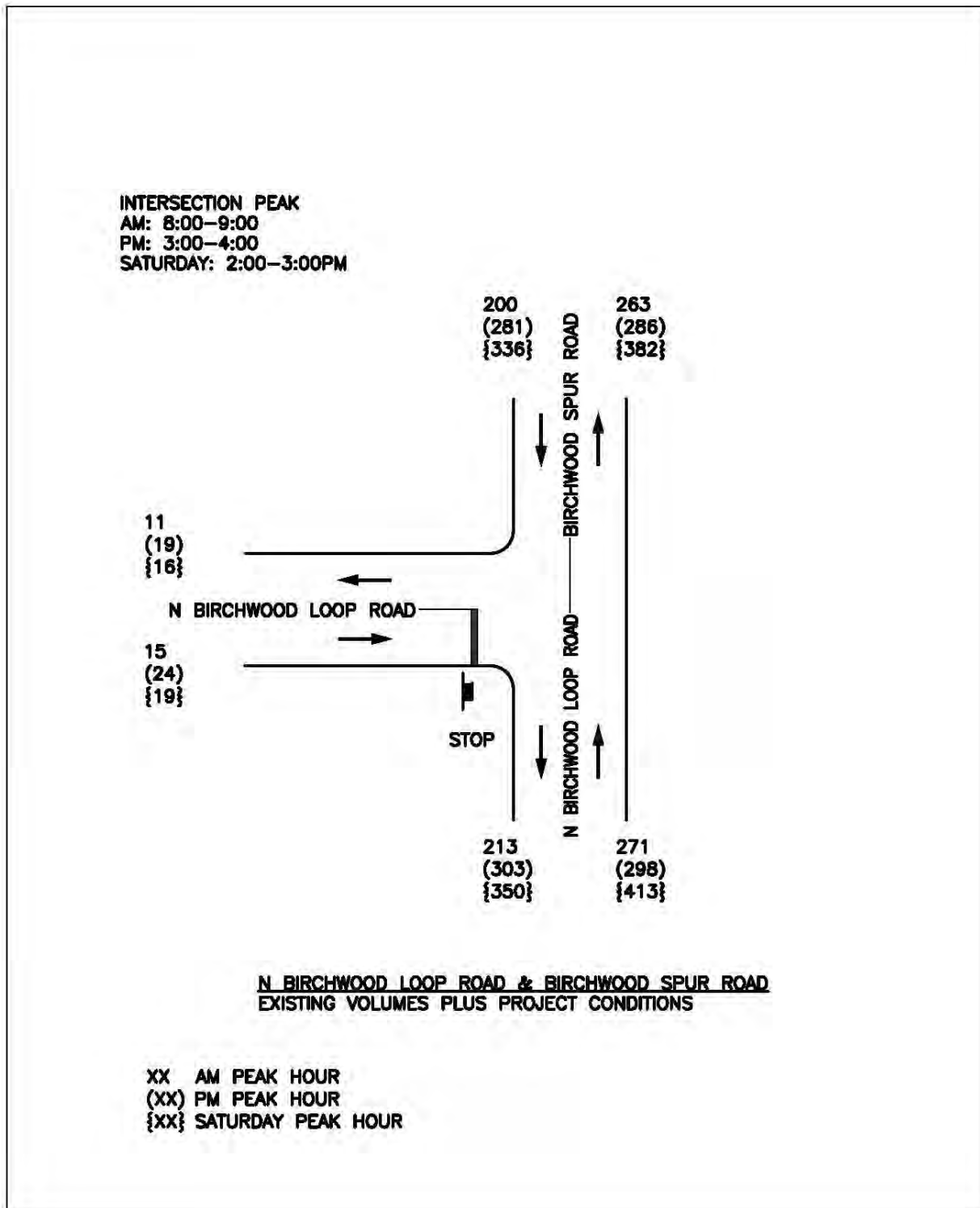


Figure 15: Project Lane Geometry Existing Conditions Plus Project Conditions

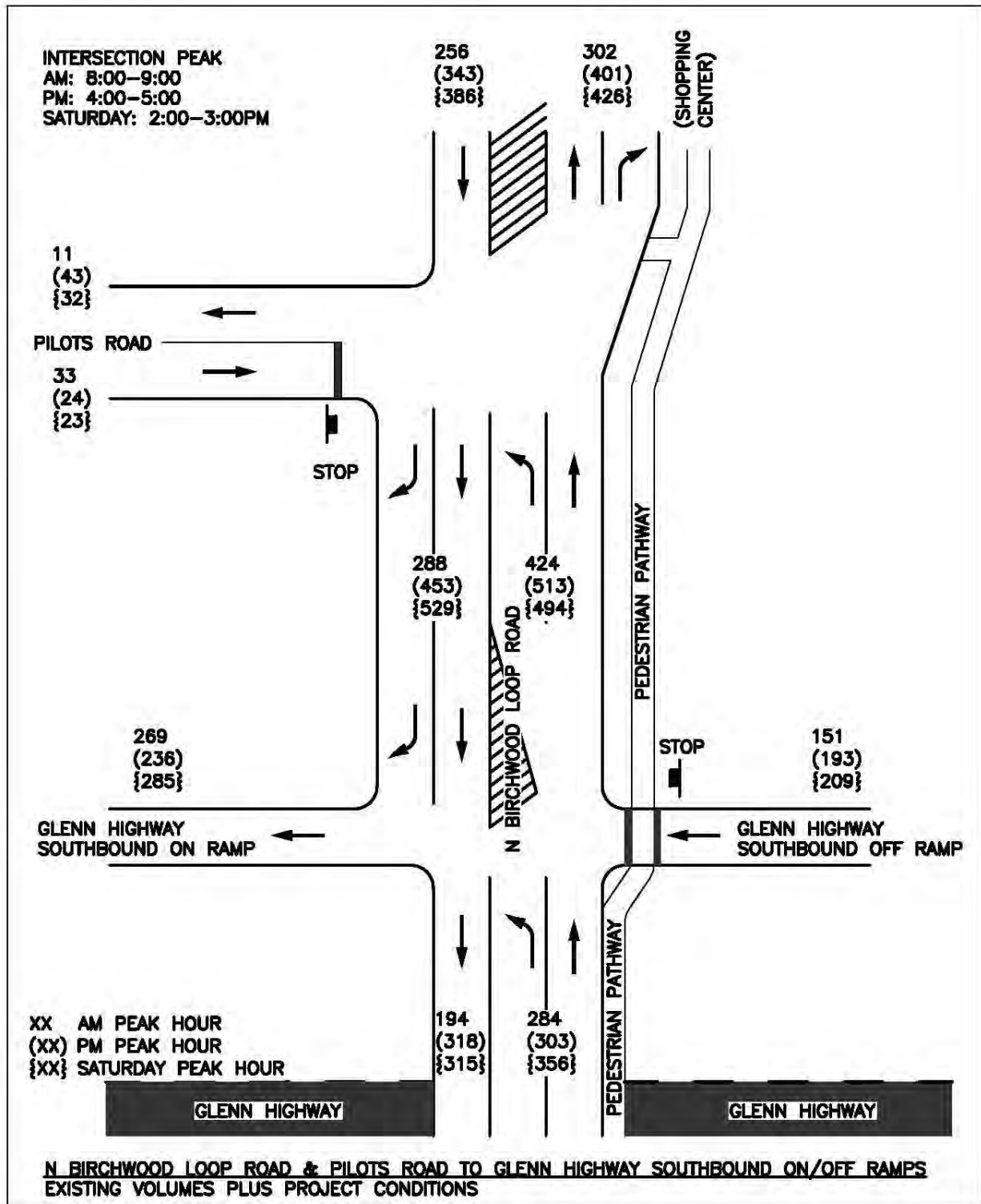


Figure 16: Project Lane Geometry Existing Conditions Plus Project Conditions

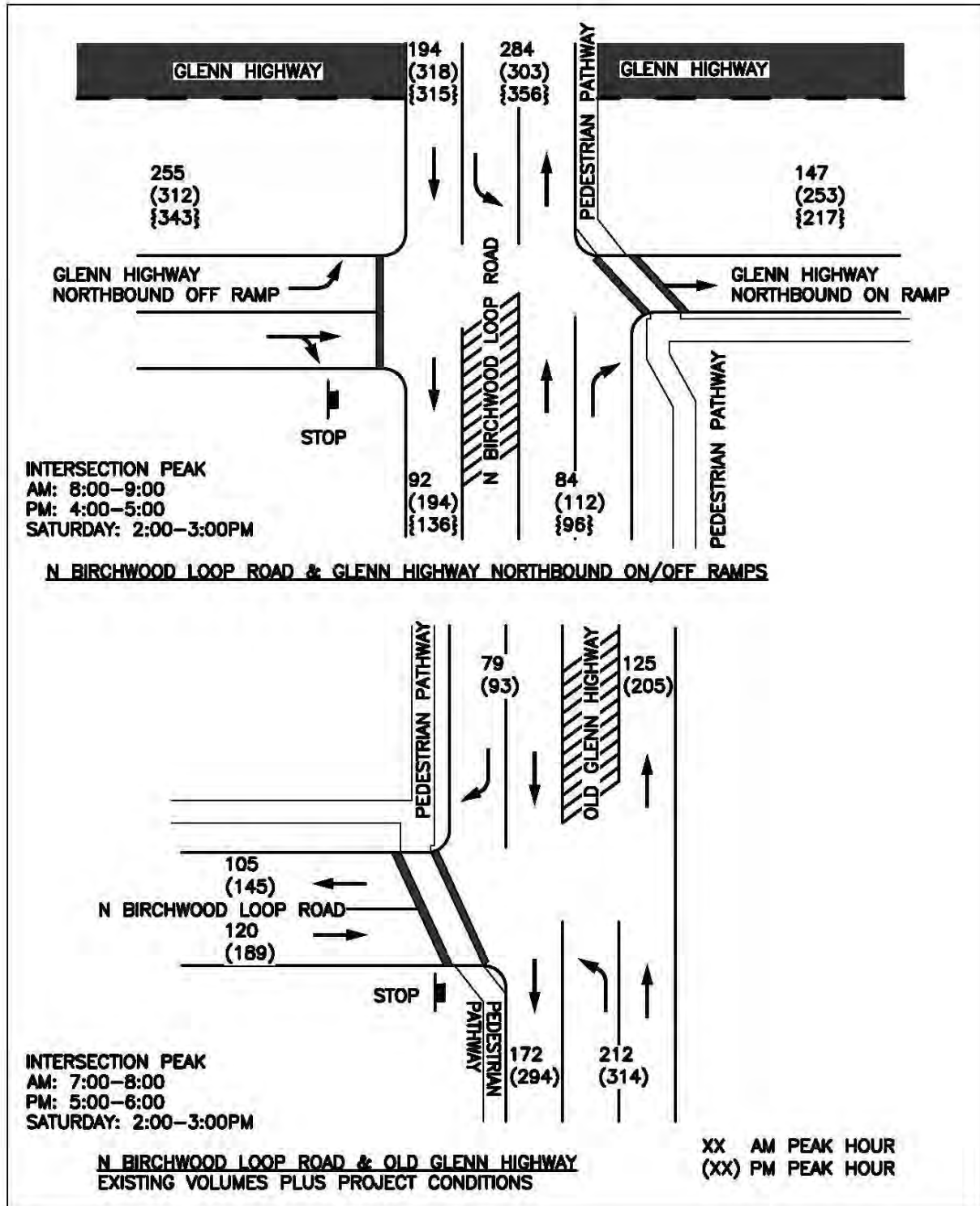


Figure 17: N Birchwood Loop and Birchwood Spur Road Intersection CAPX output Existing Plus Project Conditions

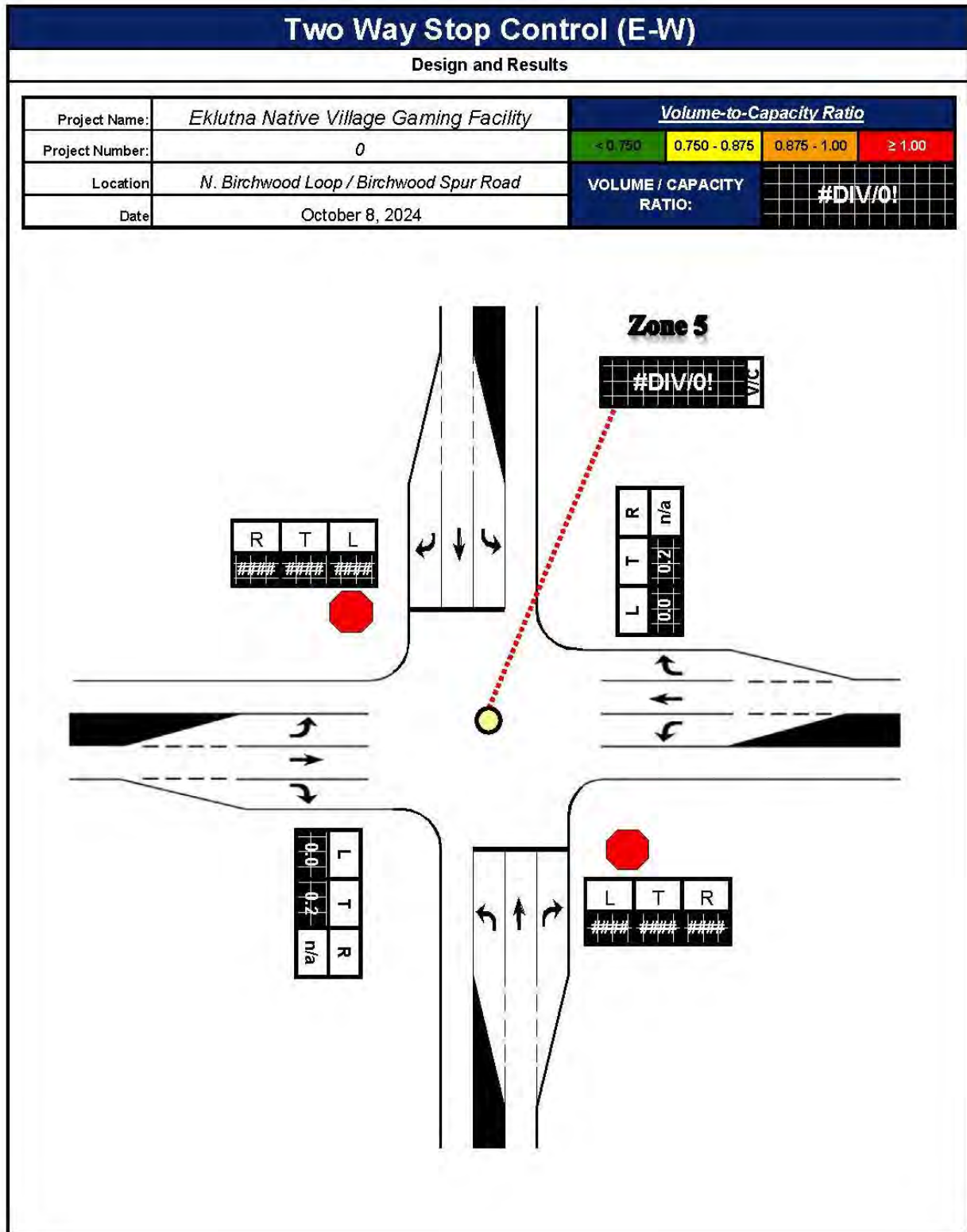


Figure 18: N Birchwood Loop and Birchwood Spur Road Intersection
 CAPX output Existing Plus Project Conditions

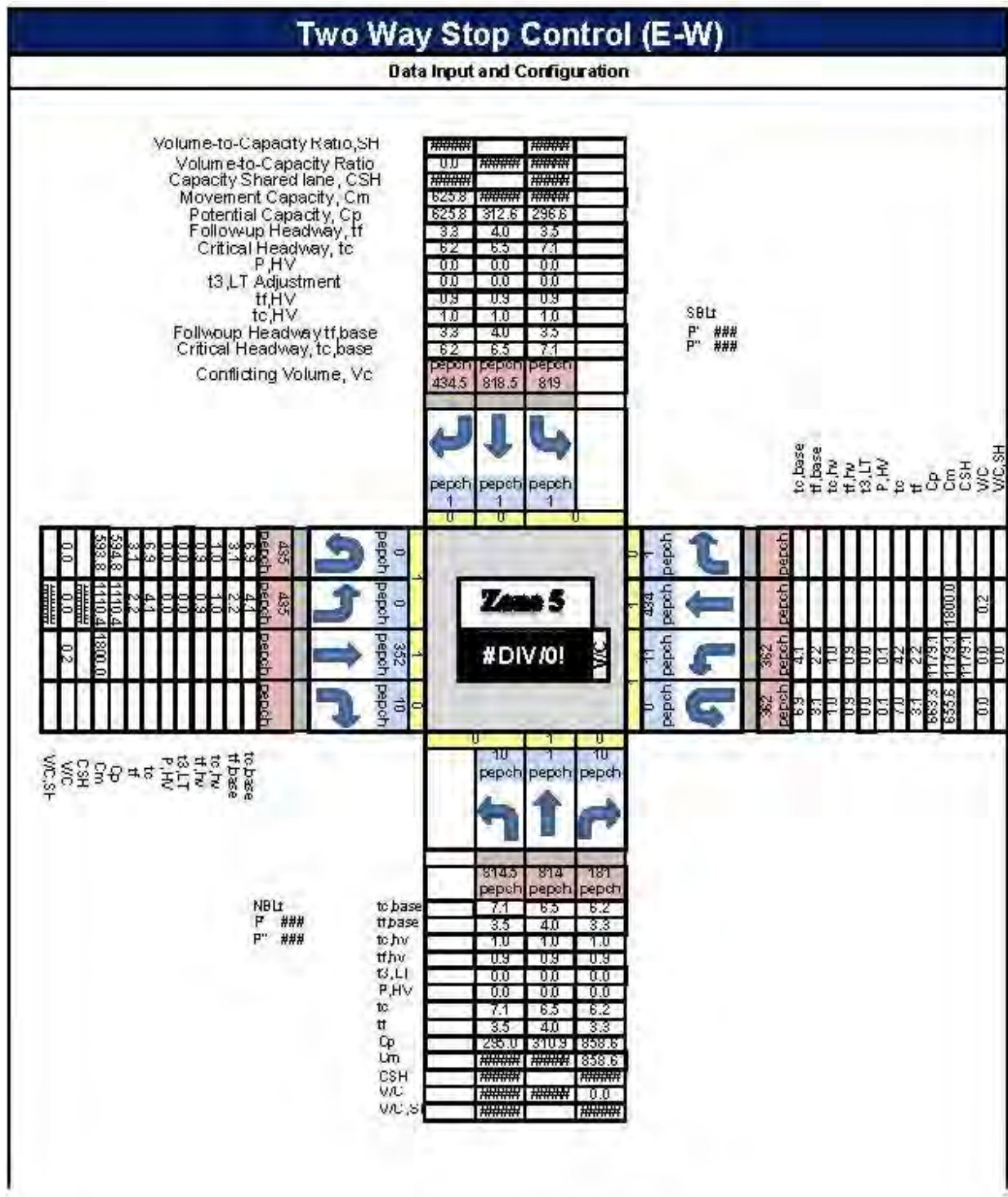


Figure 19: N Birchwood Loop and Pilots Road Intersection CAPX output Existing Plus Project Conditions

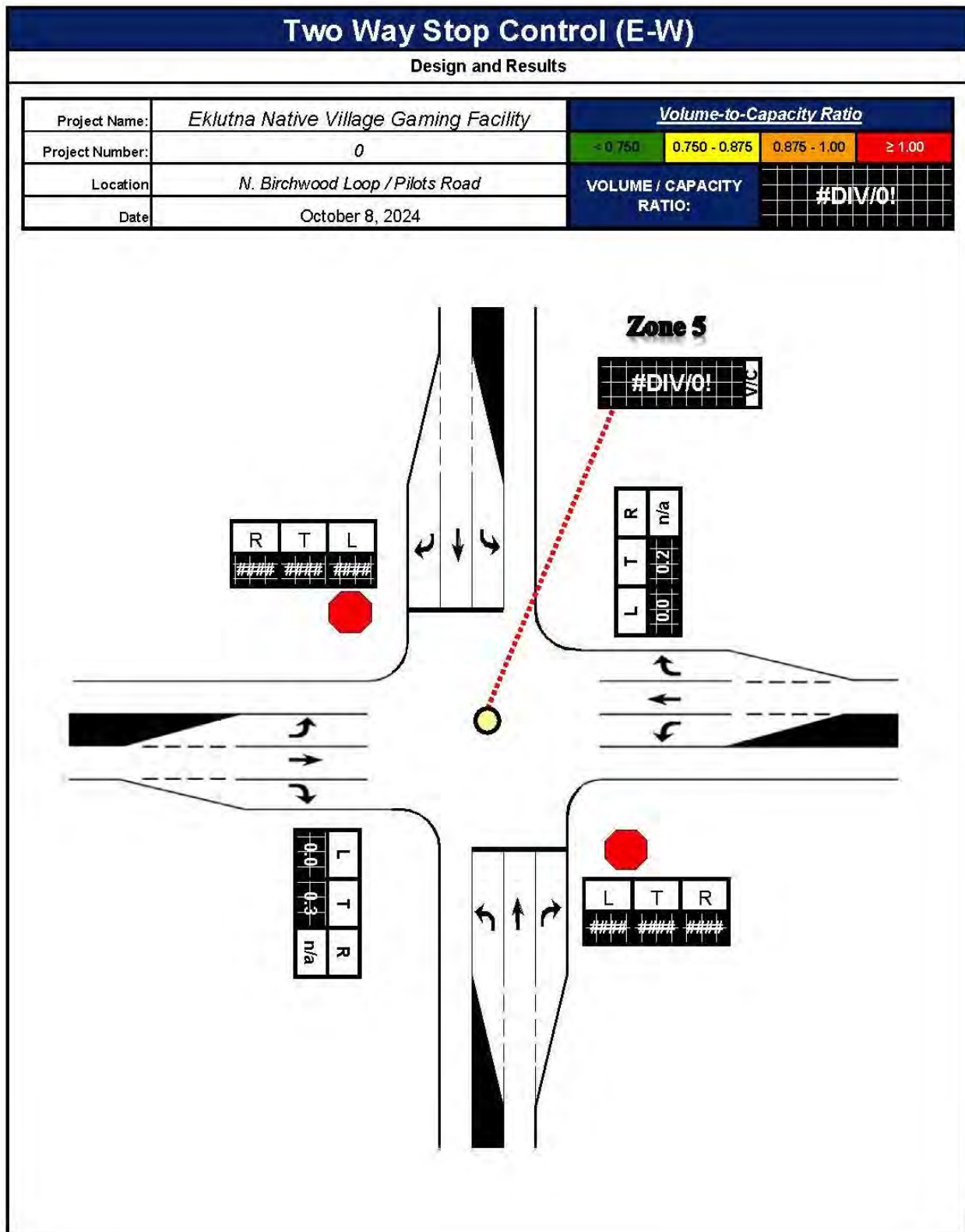


Figure 20: N Birchwood Loop and Pilots Road Intersection CAPX output Existing Plus Project Conditions

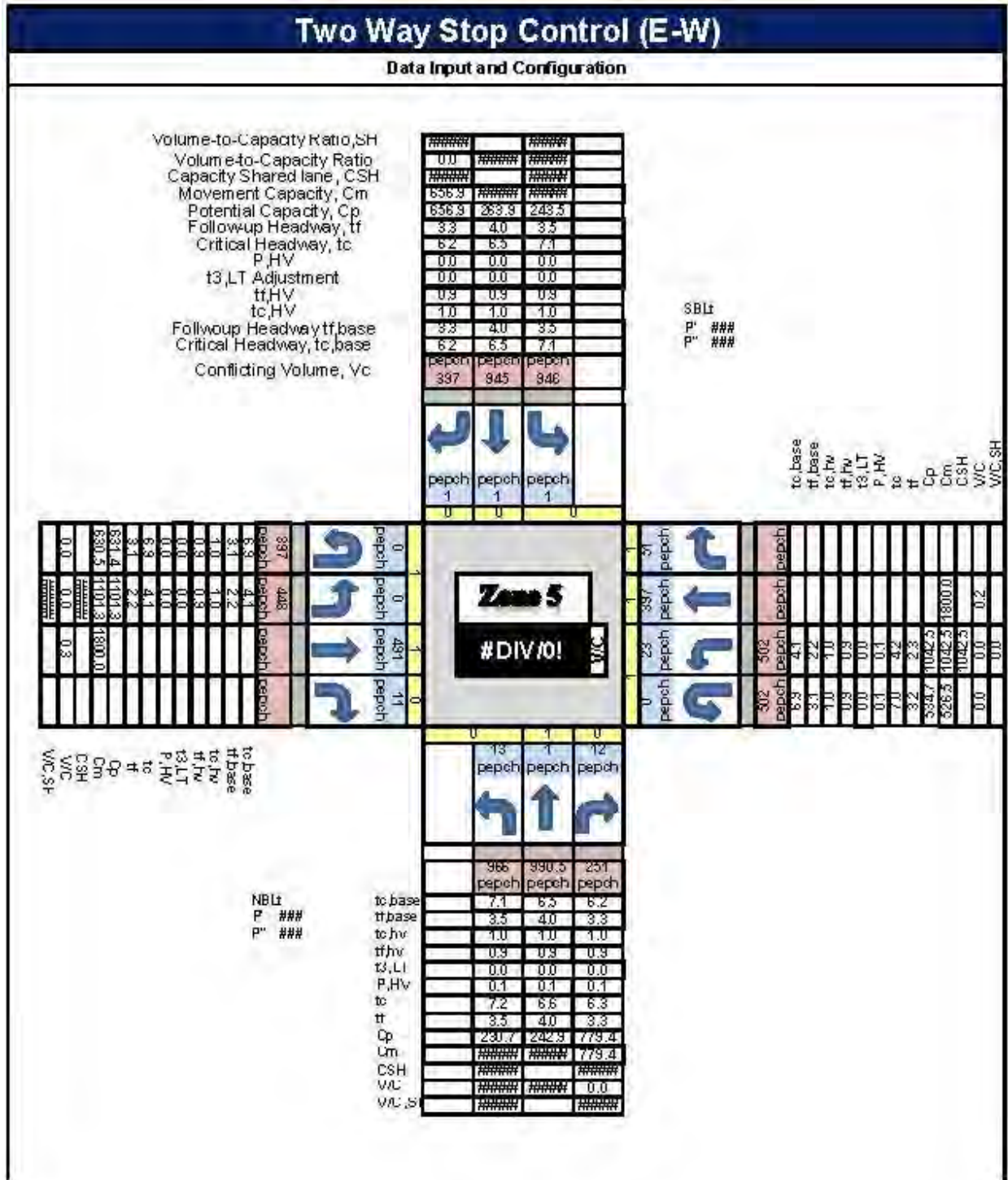


Figure 21: N Birchwood Loop and Southbound Glenn Highway Ramps Intersection CAPX output Existing Plus Project Conditions

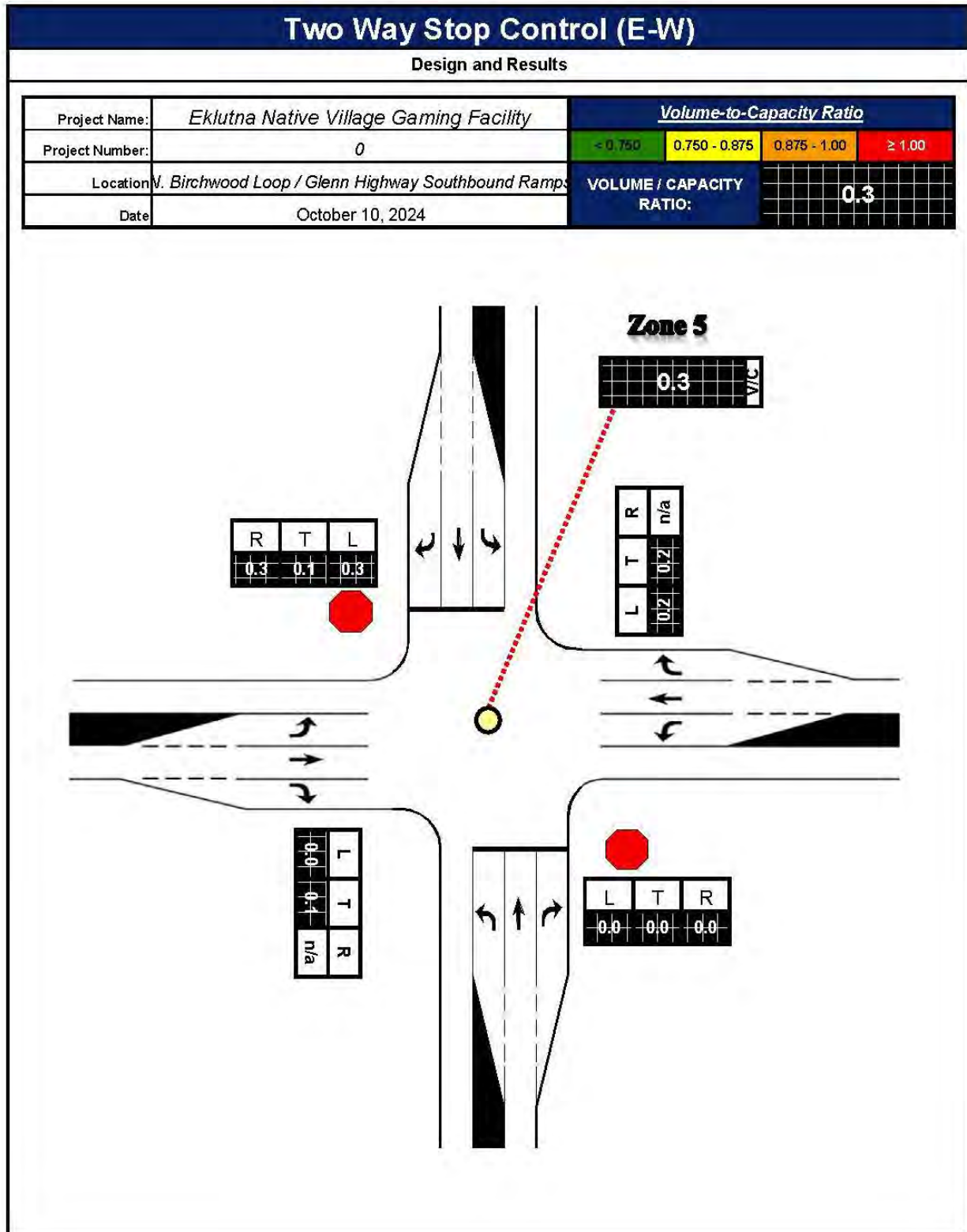


Figure 23: N Birchwood Loop and Northbound Glenn Highway Ramps Intersection CAPX output Existing Plus Project Conditions

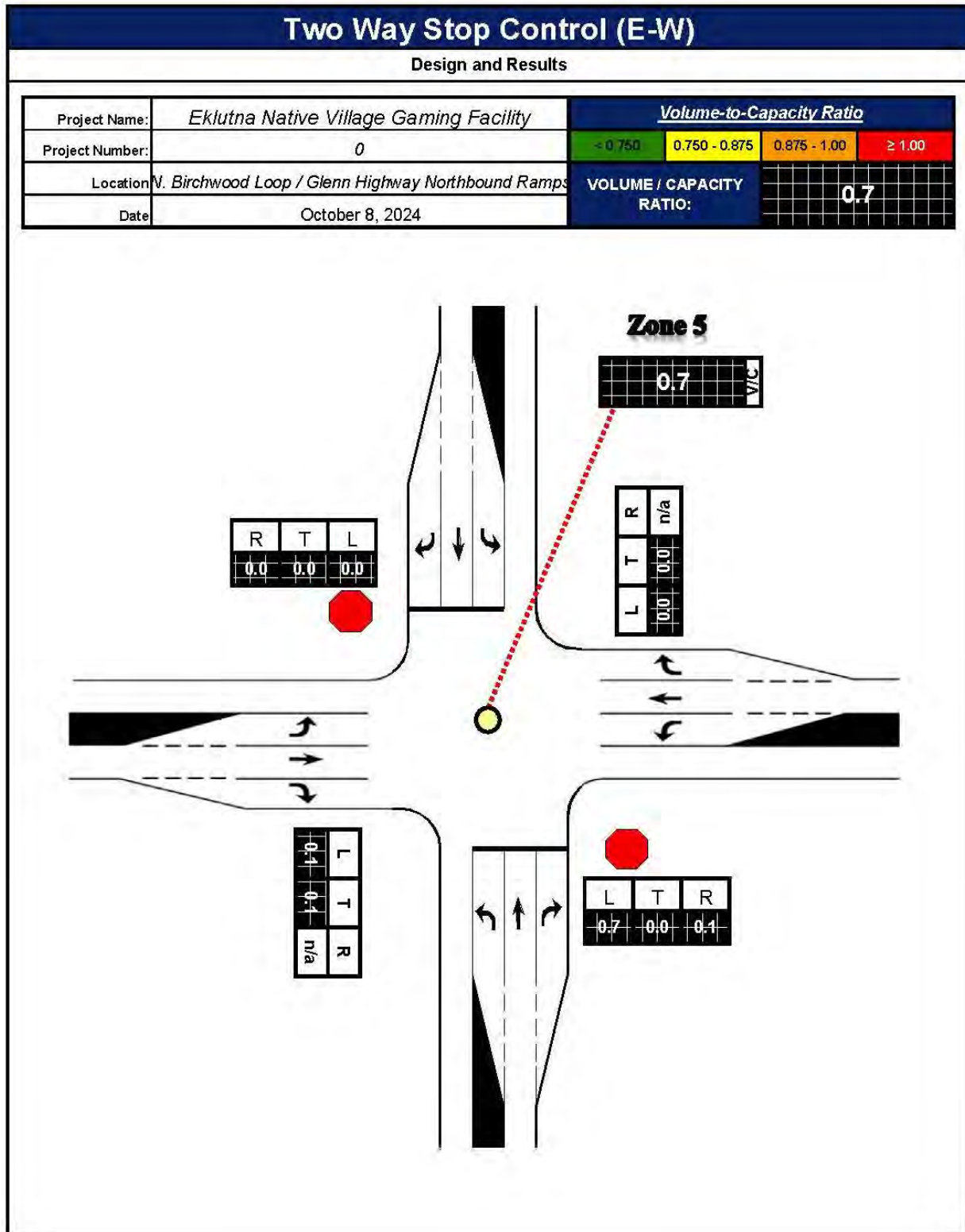


Figure 24: N Birchwood Loop and Northbound Glenn Highway Ramps Intersection CAPX output Existing Plus Project Conditions

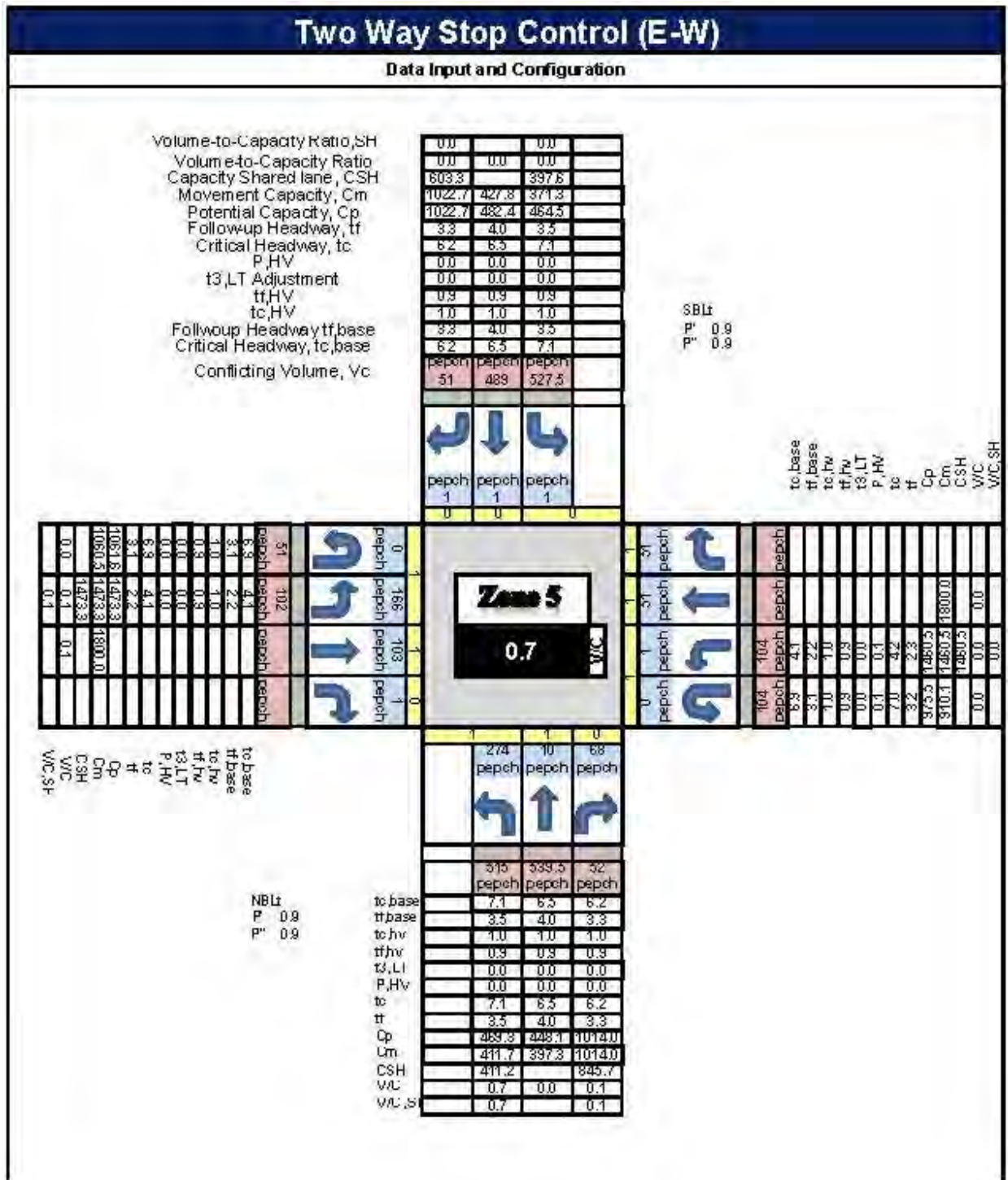
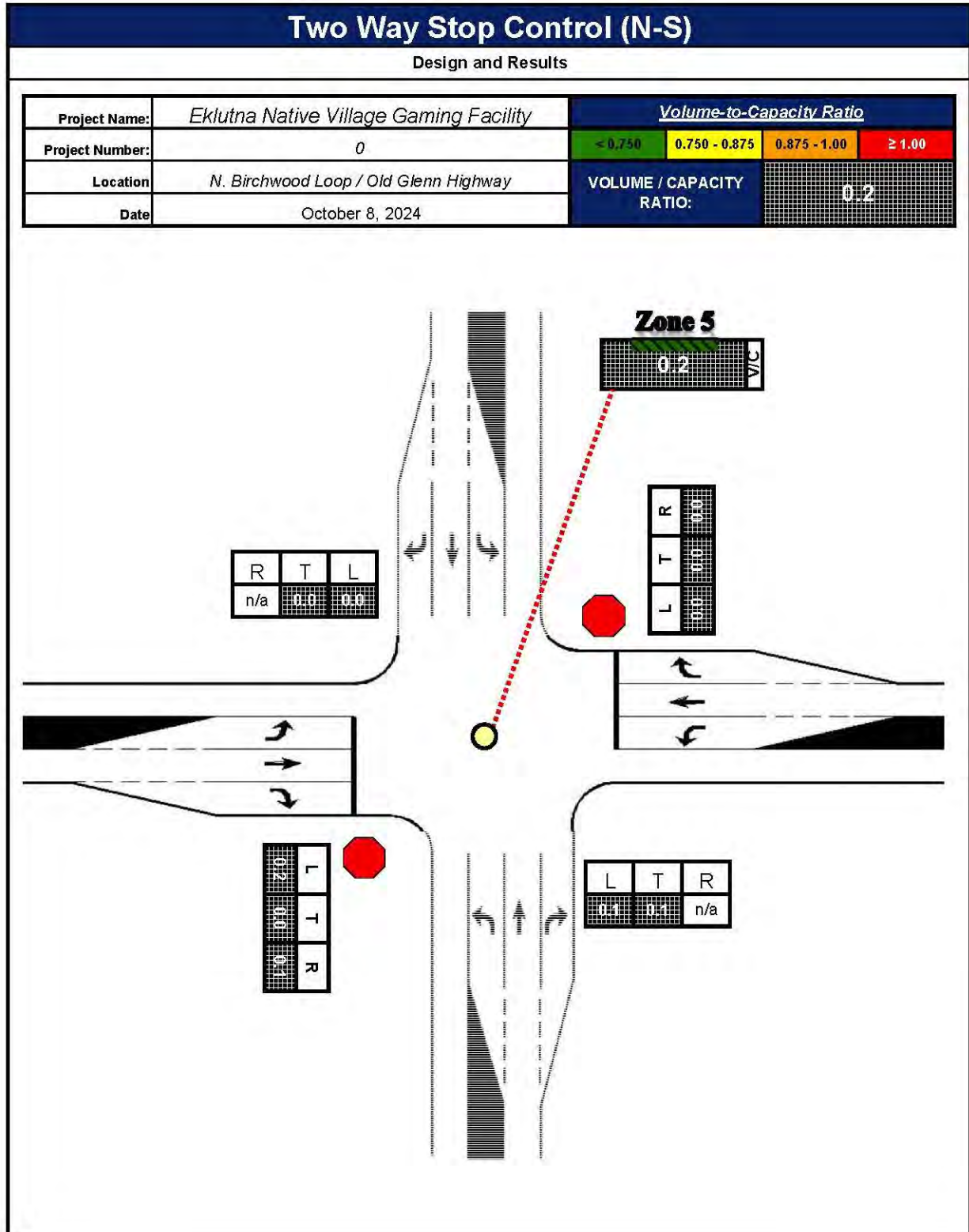


Figure 25: S Birchwood Loop and Old Glenn Highway Intersection CAPX output Existing Plus Project Conditions



Opening Year and Cumulative Horizon Year

The traffic in the subject area has been steady and has not increased in recent years. The current “2050 Metropolitan Transportation Plan” forecasts the total population growth in the Chugiak – Eagle River area to increase by 3% from the year 2019 to the year 2050. A 3% increase in traffic will not add to the Existing plus Project Conditions service level. The cumulative setting includes growth and development envisioned in the Anchorage 2040 Land Use Plan and Chugiak-Eagle River Comprehensive Plan. The Chugiak-Eagle River Comprehensive Plan assumes an approximate growth rate of 3 percent per year. The Anchorage 2040 Land Use Plan forecasts an average annual population growth rate of at least 0.8 percent between 2015 and 2040. The Anchorage 2040 Land Use Plan reflects Chugiak-Eagle River Comprehensive Plan’s anticipated growth at a somewhat higher rate than the rest of the Municipality of Anchorage, such that the Chugiak-Eagle River area, which includes the Project Site, is presumed to accommodate 15 percent of the Municipality’s overall population by 2040.

Recommendations and Improvements

The existing infrastructure supporting traffic in the study area will be sufficient to meet the additional traffic from the facility, but will be near the limit for maintaining the current LOS. Currently the roads have excess capacity, presumably to accommodate future population growth in the area. The peak hour trips will approximately double in the project area. PES recommends consulting with the Alaska DOT in regards to their requirements for future capacity and potential costs to recover that capacity through traffic control and/or reconfiguration specifically in the area surrounding the Glenn Highway Interchange. Specifically the On Ramps and Off Ramps at the interchange with generated additional traffic will reduce the LOS from A to C/D at the intersection.

5.0 Site Circulation Review

Proposed Site Circulation Review

Parking Requirements

The project site is in Native Allotment Land, so it is not governed by state or local agencies or jurisdictions. Thus, Anchorage Municipal Codes AMC etc. do not apply. The project will be constructed to IFC, IBC, etc. With approximately 400 customer parking spaces available, a recreational parking generation rate would require 348 parking spaces based on the gross floor area.

Option 1 (Recommended Option): Option 1 will consist of a single parking lot with dedicated inbound and outbound access with circulation. Nearly all traffic accessing the site will access from the south on Birchwood Spur Road. The access to the site would be through the 50' Right-Of-Way along the south side of the lot with dedicated one-way inbound and outbound access from the ROW to the parking lot. This is the preferred option due to the ease of circulation (separating the inbound and outbound traffic, and the lack of a pedestrian railroad crossing).

Option 2: Option 2 will consist of a single parking lot with two way access. Nearly all traffic accessing the site will access from the south on Birchwood Spur Road. The access to the site would be through the 50' Right-Of-Way along the south side of the lot with two two-way accesses to the lot.

The Project is located on a lot with approximately 6.4 acres of space available. Circulation requirements set by the International Fire Code (IFC) will be met and will have an effect on the layout of the site. Per IFC the building must be accessible by a fire lane 26' wide with turning space available for a fire truck to maneuver on the lot. AMC does not apply to the site because it is in Native Allotment Land but for reference, but may be followed to be consistent with local codes. AMC 21.07 requires that a lot with over 200 parking spaces contain a landscaped area equivalent to 10% of the paved area on the lot and while this is not a requirement it is assumed that this will be met. Additionally, a landscaped perimeter would be required under municipal code. Depending on zoning of the subject lot to be developed and the surrounding area the landscape perimeter buffer would be required to be 8-feet to 15-feet wide.

Appendix: Raw Traffic Data

For Project: Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:24:31 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 8/23/2024 1:00:00 PM through 8/29/2024 10:59:59 AM

85th Percentile Speed 46 MPH

85th Percentile Vehicles 3595

Max Speed 74 MPH on 8/28/2024 8:52:06 PM

Total Vehicles 4229

AADT: 714

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	600	604
AM Peak 8:00 AM	45	40
PM Peak 3:00 PM	64	62

Speed

Speed Limit: 45

85th Percentile Speed: 46

50th Percentile Speed: 40

10 MPH Pace Interval: 35.0 MPH to 45.0 MPH

Average Speed: 40.31

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	125	123	137	21	81	91	119
% over limit	16.4	15.9	16.9	12.7	16.5	15.4	18.7
Avg Speeder	48.0	49.4	48.9	49.2	48.8	48.8	48.6
Avg Speed	40.0	40.5	40.2	39.8	40.4	40.2	40.8

Class Counts

	Number	%
VEH_SM	10	0.2
VEH_MED	3999	94.6
VEH_LG	220	5.2
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/23/2024 2:00:00 PM	44.0	52	61	59	52.1	13.1%	41.0
8/23/2024 3:00:00 PM	45.0	42	50	57	50.3	16.0%	41.6
8/23/2024 4:00:00 PM	45.0	58	68	47	46.6	10.3%	39.6
8/23/2024 5:00:00 PM	47.0	52	61	54	48.2	26.2%	41.9
8/23/2024 6:00:00 PM	46.0	44	52	51	47.1	19.2%	39.6
8/23/2024 7:00:00 PM	47.0	59	69	56	49.9	21.7%	40.6
8/23/2024 8:00:00 PM	46.0	42	50	51	48.0	18.0%	40.6
8/23/2024 9:00:00 PM	43.0	31	37	47	46.3	8.1%	39.3
8/23/2024 10:00:00 PM	43.0	13	15	47	47.0	6.7%	38.1
8/23/2024 11:00:00 PM	46.0	14	16	51	48.0	18.8%	39.5
8/24/2024 12:00:00 AM	38.0	10	12	52	52.0	8.3%	36.3
8/24/2024 1:00:00 AM	45.0	3	3	45	0.0	0.0%	41.7
8/24/2024 2:00:00 AM	43.0	4	5	52	52.0	20.0%	42.2
8/24/2024 3:00:00 AM	48.0	2	2	48	48.0	50.0%	44.5
8/24/2024 4:00:00 AM	35.0	4	5	35	0.0	0.0%	32.6
8/24/2024 5:00:00 AM	37.0	3	3	37	0.0	0.0%	35.7
8/24/2024 6:00:00 AM	**No Data**						
8/24/2024 7:00:00 AM	44.0	1	1	44	0.0	0.0%	44.0
8/24/2024 8:00:00 AM	45.0	12	14	49	48.0	14.3%	40.4
8/24/2024 9:00:00 AM	45.0	14	17	50	49.3	17.6%	40.0
8/24/2024 10:00:00 AM	47.0	31	37	53	48.4	21.6%	42.2
8/24/2024 11:00:00 AM	47.0	26	30	56	48.8	26.7%	41.7
8/24/2024 12:00:00 PM	46.0	37	43	50	48.4	16.3%	40.5
8/24/2024 1:00:00 PM	46.0	44	52	52	49.3	17.3%	40.9
8/24/2024 2:00:00 PM	46.0	37	43	58	49.7	16.3%	41.0
8/24/2024 3:00:00 PM	46.0	37	44	51	47.9	20.5%	40.0
8/24/2024 4:00:00 PM	45.0	50	59	54	48.8	10.2%	40.6
8/24/2024 5:00:00 PM	43.0	31	37	48	48.0	2.7%	38.5
8/24/2024 6:00:00 PM	46.0	30	35	51	48.0	20.0%	41.8
8/24/2024 7:00:00 PM	44.0	42	49	54	50.7	6.1%	39.0
8/24/2024 8:00:00 PM	47.0	39	46	51	48.8	19.6%	39.8
8/24/2024 9:00:00 PM	46.0	20	24	53	49.0	20.8%	39.3
8/24/2024 10:00:00 PM	43.0	15	18	52	49.5	11.1%	38.4
8/24/2024 11:00:00 PM	41.0	8	10	49	48.0	20.0%	36.8
8/25/2024 12:00:00 AM	39.0	10	12	47	47.0	8.3%	36.6
8/25/2024 1:00:00 AM	39.0	4	5	43	0.0	0.0%	38.2
8/25/2024 2:00:00 AM	38.0	4	5	39	0.0	0.0%	34.6
8/25/2024 3:00:00 AM	42.0	3	3	42	0.0	0.0%	37.7
8/25/2024 4:00:00 AM	35.0	2	2	35	0.0	0.0%	27.5
8/25/2024 5:00:00 AM	40.0	3	3	40	0.0	0.0%	37.3
8/25/2024 6:00:00 AM	**No Data**						
8/25/2024 7:00:00 AM	**No Data**						
8/25/2024 8:00:00 AM	44.0	8	9	49	49.0	11.1%	39.3
8/25/2024 9:00:00 AM	46.0	17	20	51	48.4	25.0%	41.2
8/25/2024 10:00:00 AM	46.0	19	22	56	49.5	18.2%	41.6
8/25/2024 11:00:00 AM	47.0	20	24	55	48.6	33.3%	43.2
8/25/2024 12:00:00 PM	45.0	29	34	52	48.6	14.7%	40.9

Incoming Summary
 Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

8/25/2024 1:00:00 PM	45.0	46	54	51	47.7	13.0%	40.6
8/25/2024 2:00:00 PM	45.0	50	59	59	48.0	15.3%	40.6
8/25/2024 3:00:00 PM	48.0	66	78	63	49.1	26.9%	41.9
8/25/2024 4:00:00 PM	45.0	48	57	54	48.2	10.5%	41.4
8/25/2024 5:00:00 PM	46.0	42	49	50	48.4	16.3%	40.0
8/25/2024 6:00:00 PM	46.0	46	54	56	48.0	22.2%	40.9
8/25/2024 7:00:00 PM	47.0	33	39	59	49.4	28.2%	42.2
8/25/2024 8:00:00 PM	47.0	36	42	53	47.9	28.6%	41.8
8/25/2024 9:00:00 PM	46.0	26	30	53	49.0	16.7%	39.0
8/25/2024 10:00:00 PM	45.0	22	26	53	50.7	11.5%	38.9
8/25/2024 11:00:00 PM	44.0	8	9	48	48.0	11.1%	40.9
8/26/2024 12:00:00 AM	42.0	11	13	47	47.0	7.7%	37.9
8/26/2024 1:00:00 AM	42.0	7	8	57	57.0	12.5%	37.4
8/26/2024 2:00:00 AM	41.0	4	5	44	0.0	0.0%	39.4
8/26/2024 3:00:00 AM	46.0	2	2	46	46.0	50.0%	45.0
8/26/2024 4:00:00 AM	**No Data**						
8/26/2024 5:00:00 AM	**No Data**						
8/26/2024 6:00:00 AM	41.0	1	1	41	0.0	0.0%	41.0
8/26/2024 7:00:00 AM	49.0	14	16	52	49.5	25.0%	43.9
8/26/2024 8:00:00 AM	43.0	16	19	51	51.0	5.3%	40.6
8/26/2024 9:00:00 AM	44.0	25	29	57	51.5	6.9%	40.1
8/26/2024 10:00:00 AM	45.0	34	40	50	47.0	15.0%	38.4
8/26/2024 11:00:00 AM	46.0	43	51	57	47.9	27.5%	40.2
8/26/2024 12:00:00 PM	46.0	40	47	52	47.4	21.3%	39.8
8/26/2024 1:00:00 PM	46.0	42	49	56	49.3	16.3%	40.6
8/26/2024 2:00:00 PM	45.0	45	53	55	49.3	13.2%	40.8
8/26/2024 3:00:00 PM	44.0	44	52	56	49.1	13.2%	40.0
8/26/2024 4:00:00 PM	45.0	67	79	53	48.2	13.9%	40.1
8/26/2024 5:00:00 PM	46.0	53	62	51	47.0	16.1%	39.5
8/26/2024 6:00:00 PM	44.0	54	64	52	47.6	10.9%	38.7
8/26/2024 7:00:00 PM	46.0	45	53	51	47.4	20.8%	40.3
8/26/2024 8:00:00 PM	47.0	36	42	54	47.8	38.1%	42.2
8/26/2024 9:00:00 PM	42.0	28	33	46	46.0	3.0%	38.0
8/26/2024 10:00:00 PM	46.0	29	34	48	47.0	17.6%	40.6
8/26/2024 11:00:00 PM	44.0	12	14	47	47.0	7.1%	37.6
8/27/2024 12:00:00 AM	41.0	8	10	46	46.0	10.0%	38.5
8/27/2024 1:00:00 AM	35.0	4	5	41	0.0	0.0%	34.6
8/27/2024 2:00:00 AM	50.0	3	3	50	50.0	33.3%	37.0
8/27/2024 3:00:00 AM	36.0	2	2	36	0.0	0.0%	34.5
8/27/2024 4:00:00 AM	37.0	2	2	37	0.0	0.0%	32.5
8/27/2024 5:00:00 AM	35.0	3	3	35	0.0	0.0%	28.0
8/27/2024 6:00:00 AM	**No Data**						
8/27/2024 7:00:00 AM	46.0	10	12	52	47.5	33.3%	43.6
8/27/2024 8:00:00 AM	46.0	23	27	51	48.2	18.5%	42.1
8/27/2024 9:00:00 AM	47.0	38	45	57	49.5	31.1%	42.3
8/27/2024 10:00:00 AM	45.0	36	42	51	48.0	14.3%	39.3
8/27/2024 11:00:00 AM	45.0	27	32	50	47.6	15.6%	39.6
8/27/2024 12:00:00 PM	44.0	37	43	52	49.2	11.6%	41.0

Incoming Summary
 Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

8/27/2024 1:00:00 PM	45.0	37	43	62	52.8	11.6%	40.2
8/27/2024 2:00:00 PM	44.0	48	57	50	47.8	7.0%	40.4
8/27/2024 3:00:00 PM	45.0	37	44	56	50.1	15.9%	41.5
8/27/2024 4:00:00 PM	49.0	40	47	56	51.3	21.3%	40.9
8/27/2024 5:00:00 PM	47.0	45	53	54	48.8	22.6%	41.9
8/27/2024 6:00:00 PM	47.0	50	59	52	49.6	16.9%	39.3
8/27/2024 7:00:00 PM	44.0	65	77	55	48.7	13.0%	39.7
8/27/2024 8:00:00 PM	46.0	70	82	57	50.2	15.9%	41.7
8/27/2024 9:00:00 PM	46.0	39	46	52	47.9	17.4%	40.9
8/27/2024 10:00:00 PM	45.0	26	30	51	48.5	6.7%	40.4
8/27/2024 11:00:00 PM	42.0	8	10	47	47.0	10.0%	37.5
8/28/2024 12:00:00 AM	44.0	8	9	59	59.0	11.1%	36.8
8/28/2024 1:00:00 AM	39.0	4	5	41	0.0	0.0%	37.2
8/28/2024 2:00:00 AM	37.0	3	4	61	61.0	25.0%	40.0
8/28/2024 3:00:00 AM	34.0	1	1	34	0.0	0.0%	34.0
8/28/2024 4:00:00 AM	38.0	1	1	38	0.0	0.0%	38.0
8/28/2024 5:00:00 AM	47.0	3	3	47	47.0	33.3%	36.3
8/28/2024 6:00:00 AM	**No Data**						
8/28/2024 7:00:00 AM	46.0	10	12	48	46.7	25.0%	41.4
8/28/2024 8:00:00 AM	47.0	19	22	49	47.8	22.7%	41.8
8/28/2024 9:00:00 AM	46.0	48	56	53	48.4	16.1%	39.9
8/28/2024 10:00:00 AM	45.0	37	43	52	49.0	14.0%	38.2
8/28/2024 11:00:00 AM	45.0	33	39	49	48.0	15.4%	40.4
8/28/2024 12:00:00 PM	46.0	31	37	53	49.3	18.9%	41.4
8/28/2024 1:00:00 PM	46.0	43	51	54	48.7	19.6%	41.2
8/28/2024 2:00:00 PM	44.0	36	42	50	47.6	11.9%	38.5
8/28/2024 3:00:00 PM	45.0	55	65	53	49.1	13.8%	40.9
8/28/2024 4:00:00 PM	44.0	54	64	54	48.4	10.9%	39.0
8/28/2024 5:00:00 PM	46.0	65	76	62	49.4	15.8%	40.3
8/28/2024 6:00:00 PM	46.0	60	70	54	48.5	18.6%	39.1
8/28/2024 7:00:00 PM	47.0	51	60	56	48.5	28.3%	42.6
8/28/2024 8:00:00 PM	47.0	54	64	64	49.8	20.3%	41.5
8/28/2024 9:00:00 PM	46.0	35	41	74	51.4	17.1%	40.3
8/28/2024 10:00:00 PM	46.0	25	29	49	47.6	17.2%	40.6
8/28/2024 11:00:00 PM	42.0	12	14	48	48.0	7.1%	39.3
8/29/2024 12:00:00 AM	38.0	10	12	44	0.0	0.0%	35.3
8/29/2024 1:00:00 AM	39.0	1	1	39	0.0	0.0%	39.0
8/29/2024 2:00:00 AM	40.0	3	3	40	0.0	0.0%	33.7
8/29/2024 3:00:00 AM	44.0	3	3	44	0.0	0.0%	38.3
8/29/2024 4:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
8/29/2024 5:00:00 AM	**No Data**						
8/29/2024 6:00:00 AM	**No Data**						
8/29/2024 7:00:00 AM	44.0	9	11	61	56.5	18.2%	42.5
8/29/2024 8:00:00 AM	44.0	8	10	52	50.0	20.0%	42.1
8/29/2024 9:00:00 AM	45.0	43	51	52	48.4	13.7%	40.3
8/29/2024 10:00:00 AM	45.0	39	46	58	48.0	15.2%	40.2
8/29/2024 11:00:00 AM	44.0	33	39	50	48.7	7.7%	38.0

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/24/2024 12:00:00 AM	46.0	417	491	59	48.8	16.5%	40.4
8/25/2024 12:00:00 AM	46.0	501	589	58	48.8	15.4%	40.2
8/26/2024 12:00:00 AM	46.0	541	637	63	48.6	18.7%	40.8
8/27/2024 12:00:00 AM	46.0	649	763	57	48.0	16.4%	40.0
8/28/2024 12:00:00 AM	46.0	657	773	62	49.4	15.9%	40.5
8/29/2024 12:00:00 AM	46.0	689	811	74	48.9	16.9%	40.2
8/29/2024 10:59:59 AM	45.0	140	165	61	49.2	12.7%	39.8

Incoming Weekly Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	3	5	0	4	0
1 - 2	*	*	*	*	*	5	5	0	5	0
2 - 3	*	*	*	*	*	2	3	0	2.5	0
3 - 4	*	*	*	*	*	5	2	0	3.5	0
4 - 5	*	*	*	*	*	3	3	0	3	0
5 - 6	*	*	*	*	*	0	0	0	0	0
6 - 7	*	*	*	*	*	1	0	0	0.5	0
7 - 8	*	*	*	*	*	14	9	0	11.5	0
8 - 9	*	*	*	*	*	17	20	0	18.5	0
9 - 10	*	*	*	*	*	37	22	0	29.5	0
10 - 11	*	*	*	*	*	30	24	0	27	0
11 - 12	*	*	*	*	*	43	34	0	38.5	0
12 - 13	*	*	*	*	*	52	54	0	53	0
13 - 14	*	*	*	*	61	43	59	61	51	44
14 - 15	*	*	*	*	50	44	78	50	61	45
15 - 16	*	*	*	*	68	59	57	68	58	44.2
16 - 17	*	*	*	*	61	37	49	61	43	46.4
17 - 18	*	*	*	*	52	35	54	52	44.5	45.3
18 - 19	*	*	*	*	69	49	39	69	44	47
19 - 20	*	*	*	*	50	46	42	50	44	45.2
20 - 21	*	*	*	*	37	24	30	37	27	42.7
21 - 22	*	*	*	*	15	18	26	15	22	43
22 - 23	*	*	*	*	16	10	9	16	9.5	46
23 - 24	*	*	*	*	12	12	13	12	12.5	38
Totals	0	0	0	0	491	589	637			
% of Total	0%	0%	0%	0%	28.6%	34.3%	37.1%			

Incoming Weekly Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	8	5	5	1	*	*	*	4.75	0	38.75
1 - 2	5	3	4	3	*	*	*	3.75	0	42
2 - 3	2	2	1	3	*	*	*	2	0	40
3 - 4	0	2	1	1	*	*	*	1	0	34.33
4 - 5	0	3	3	0	*	*	*	1.5	0	41
5 - 6	1	0	0	0	*	*	*	0.25	0	41
6 - 7	16	12	12	11	*	*	*	12.75	0	46.05
7 - 8	19	27	22	10	*	*	*	19.5	0	45
8 - 9	29	45	56	51	*	*	*	45.25	0	45.18
9 - 10	40	42	43	46	*	*	*	42.75	0	45
10 - 11	51	32	39	39	*	*	*	40.25	0	44.98
11 - 12	47	43	37	*	*	*	*	42.33	0	45.23
12 - 13	49	43	51	*	*	*	*	47.67	0	45.5
13 - 14	53	57	42	*	*	*	*	50.67	0	44.2
14 - 15	52	44	65	*	*	*	*	53.67	0	44.63
15 - 16	79	47	64	*	*	*	*	63.33	0	45.73
16 - 17	62	53	76	*	*	*	*	63.67	0	45.6
17 - 18	64	59	70	*	*	*	*	64.33	0	45.33
18 - 19	53	77	60	*	*	*	*	63.33	0	45.57
19 - 20	42	82	64	*	*	*	*	62.67	0	46.5
20 - 21	33	46	41	*	*	*	*	40	0	44.23
21 - 22	34	30	29	*	*	*	*	31	0	45.43
22 - 23	14	10	14	*	*	*	*	12.67	0	42.5
23 - 24	10	9	12	*	*	*	*	10.33	0	41
Totals	763	773	811	165	0	0	0			
% of Total	30.37%	30.77%	32.29%	6.57%	0%	0%	0%			

Incoming Monthly Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	8	5	5	1	*	3	5	4.75	4	38.75
1 - 2	5	3	4	3	*	5	5	3.75	5	42
2 - 3	2	2	1	3	*	2	3	2	2.5	40
3 - 4	0	2	1	1	*	5	2	1	3.5	34.33
4 - 5	0	3	3	0	*	3	3	1.5	3	41
5 - 6	1	0	0	0	*	0	0	0.25	0	41
6 - 7	16	12	12	11	*	1	0	12.75	0.5	46.05
7 - 8	19	27	22	10	*	14	9	19.5	11.5	45
8 - 9	29	45	56	51	*	17	20	45.25	18.5	45.18
9 - 10	40	42	43	46	*	37	22	42.75	29.5	45
10 - 11	51	32	39	39	*	30	24	40.25	27	44.98
11 - 12	47	43	37	*	*	43	34	42.33	38.5	45.23
12 - 13	49	43	51	*	*	52	54	47.67	53	45.5
13 - 14	53	57	42	*	61	43	59	53.25	51	44.15
14 - 15	52	44	65	*	50	44	78	52.75	61	44.72
15 - 16	79	47	64	*	68	59	57	64.5	58	45.35
16 - 17	62	53	76	*	61	37	49	63	43	45.8
17 - 18	64	59	70	*	52	35	54	61.25	44.5	45.32
18 - 19	53	77	60	*	69	49	39	64.75	44	45.92
19 - 20	42	82	64	*	50	46	42	59.5	44	46.18
20 - 21	33	46	41	*	37	24	30	39.25	27	43.85
21 - 22	34	30	29	*	15	18	26	27	22	44.82
22 - 23	14	10	14	*	16	10	9	13.5	9.5	43.38
23 - 24	10	9	12	*	12	12	13	10.75	12.5	40.25
Totals	763	773	811	165	491	589	637			
% of Total	18.04%	18.28%	19.18%	3.9%	11.61%	13.93%	15.06%			

Incoming Weekly Speeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	41.67	38.2	0	39.5	0
1 - 2	*	*	*	*	*	42.2	34.6	0	38.4	0
2 - 3	*	*	*	*	*	44.5	37.67	0	40.4	0
3 - 4	*	*	*	*	*	32.6	27.5	0	31.14	0
4 - 5	*	*	*	*	*	35.67	37.33	0	36.5	0
5 - 6	*	*	*	*	*	0	0	0	0	0
6 - 7	*	*	*	*	*	44	0	0	44	0
7 - 8	*	*	*	*	*	40.36	39.33	0	39.96	0
8 - 9	*	*	*	*	*	40	41.2	0	40.65	0
9 - 10	*	*	*	*	*	42.19	41.64	0	41.98	0
10 - 11	*	*	*	*	*	41.67	43.17	0	42.33	0
11 - 12	*	*	*	*	*	40.53	40.94	0	40.71	0
12 - 13	*	*	*	*	*	40.88	40.59	0	40.74	0
13 - 14	*	*	*	*	41.11	41.02	40.63	41.11	40.79	44
14 - 15	*	*	*	*	41.6	40.05	41.94	41.6	41.25	45
15 - 16	*	*	*	*	39.59	40.64	41.35	39.59	40.99	44.2
16 - 17	*	*	*	*	41.93	38.49	39.98	41.93	39.34	46.4
17 - 18	*	*	*	*	39.6	41.8	40.85	39.6	41.22	45.3
18 - 19	*	*	*	*	40.61	39.02	42.18	40.61	40.42	47
19 - 20	*	*	*	*	40.62	39.78	41.81	40.62	40.75	45.2
20 - 21	*	*	*	*	39.27	39.25	39	39.27	39.11	42.7
21 - 22	*	*	*	*	38.13	38.44	38.92	38.13	38.73	43
22 - 23	*	*	*	*	39.5	36.8	40.89	39.5	38.74	46
23 - 24	*	*	*	*	36.33	36.58	37.92	36.33	37.28	38
Totals	0	0	0	0	40.4	40.2	40.8			
% of Total	0%	0%	0%	0%	33.28%	33.11%	33.61%			

Incoming Weekly Speeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	37.38	34.6	37.2	39	*	*	*	36.68	*	38.75
1 - 2	39.4	37	40	33.67	*	*	*	37.93	*	42
2 - 3	45	34.5	34	38.33	*	*	*	38.5	*	40
3 - 4	0	32.5	38	28	*	*	*	32.75	*	34.33
4 - 5	0	28	36.33	0	*	*	*	32.17	*	41
5 - 6	41	0	0	0	*	*	*	41	*	41
6 - 7	43.88	43.58	41.42	42.55	*	*	*	42.94	*	46.05
7 - 8	40.63	42.15	41.82	42.1	*	*	*	41.68	*	45
8 - 9	40.14	42.27	39.89	40.31	*	*	*	40.64	*	45.18
9 - 10	38.38	39.29	38.23	40.17	*	*	*	39.05	*	45
10 - 11	40.2	39.56	40.36	38.03	*	*	*	39.58	*	44.98
11 - 12	39.79	41.02	41.35	*	*	*	*	40.66	*	45.23
12 - 13	40.59	40.16	41.18	*	*	*	*	40.67	*	45.5
13 - 14	40.77	40.44	38.52	*	*	*	*	40.03	*	44.2
14 - 15	40.02	41.52	40.89	*	*	*	*	40.78	*	44.63
15 - 16	40.06	40.91	39.05	*	*	*	*	39.93	*	45.73
16 - 17	39.53	41.87	40.26	*	*	*	*	40.47	*	45.6
17 - 18	38.72	39.32	39.11	*	*	*	*	39.05	*	45.33
18 - 19	40.28	39.73	42.58	*	*	*	*	40.78	*	45.57
19 - 20	42.21	41.66	41.48	*	*	*	*	41.72	*	46.5
20 - 21	37.97	40.87	40.27	*	*	*	*	39.87	*	44.23
21 - 22	40.65	40.43	40.62	*	*	*	*	40.57	*	45.43
22 - 23	37.64	37.5	39.29	*	*	*	*	38.21	*	42.5
23 - 24	38.5	36.78	35.25	*	*	*	*	36.74	*	41
Totals	40	40.5	40.2	39.7	0	0	0			
% of Total	24.94%	25.25%	25.06%	24.75%	0%	0%	0%			

Incoming Monthly Speeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	Aug 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	37.38	34.6	37.2	39	*	41.67	38.2	36.68	39.5	38.75
1 - 2	39.4	37	40	33.67	*	42.2	34.6	37.93	38.4	42
2 - 3	45	34.5	34	38.33	*	44.5	37.67	38.5	40.4	40
3 - 4	0	32.5	38	28	*	32.6	27.5	32.75	31.14	34.33
4 - 5	0	28	36.33	0	*	35.67	37.33	32.17	36.5	41
5 - 6	41	0	0	0	*	0	0	41	0	41
6 - 7	43.88	43.58	41.42	42.55	*	44	0	42.94	44	46.05
7 - 8	40.63	42.15	41.82	42.1	*	40.36	39.33	41.68	39.96	45
8 - 9	40.14	42.27	39.89	40.31	*	40	41.2	40.64	40.65	45.18
9 - 10	38.38	39.29	38.23	40.17	*	42.19	41.64	39.05	41.98	45
10 - 11	40.2	39.56	40.36	38.03	*	41.67	43.17	39.58	42.33	44.98
11 - 12	39.79	41.02	41.35	*	*	40.53	40.94	40.66	40.71	45.23
12 - 13	40.59	40.16	41.18	*	*	40.88	40.59	40.67	40.74	45.5
13 - 14	40.77	40.44	38.52	*	41.11	41.02	40.63	40.34	40.79	44.15
14 - 15	40.02	41.52	40.89	*	41.6	40.05	41.94	40.98	41.25	44.72
15 - 16	40.06	40.91	39.05	*	39.59	40.64	41.35	39.84	40.99	45.35
16 - 17	39.53	41.87	40.26	*	41.93	38.49	39.98	40.83	39.34	45.8
17 - 18	38.72	39.32	39.11	*	39.6	41.8	40.85	39.16	41.22	45.32
18 - 19	40.28	39.73	42.58	*	40.61	39.02	42.18	40.74	40.42	45.92
19 - 20	42.21	41.66	41.48	*	40.62	39.78	41.81	41.49	40.75	46.18
20 - 21	37.97	40.87	40.27	*	39.27	39.25	39	39.73	39.11	43.85
21 - 22	40.65	40.43	40.62	*	38.13	38.44	38.92	40.23	38.73	44.82
22 - 23	37.64	37.5	39.29	*	39.5	36.8	40.89	38.59	38.74	43.38
23 - 24	38.5	36.78	35.25	*	36.33	36.58	37.92	36.63	37.28	40.25
Totals	40	40.5	40.2	39.7	40.4	40.2	40.8			
% of Total	14.19%	14.37%	14.27%	14.09%	14.34%	14.27%	14.48%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	45	39	0	42	0
1 - 2	*	*	*	*	*	43	38	0	40.5	0
2 - 3	*	*	*	*	*	48	42	0	45	0
3 - 4	*	*	*	*	*	34.5	35	0	34.75	0
4 - 5	*	*	*	*	*	37	40	0	38.5	0
5 - 6	*	*	*	*	*	0	0	0	0	0
6 - 7	*	*	*	*	*	44	0	0	44	0
7 - 8	*	*	*	*	*	45	44	0	44.5	0
8 - 9	*	*	*	*	*	45	46	0	45.5	0
9 - 10	*	*	*	*	*	46.3	46	0	46.15	0
10 - 11	*	*	*	*	*	47	46.5	0	46.75	0
11 - 12	*	*	*	*	*	46	45	0	45.5	0
12 - 13	*	*	*	*	*	46	44.5	0	45.25	0
13 - 14	*	*	*	*	44	45.5	45	44	45.25	44
14 - 15	*	*	*	*	45	45.7	47.5	45	46.6	45
15 - 16	*	*	*	*	44.2	44.4	44.5	44.2	44.45	44.2
16 - 17	*	*	*	*	46.4	43	46	46.4	44.5	46.4
17 - 18	*	*	*	*	45.3	46	45.7	45.3	45.85	45.3
18 - 19	*	*	*	*	47	43.3	46.7	47	45	47
19 - 20	*	*	*	*	45.2	47	46.5	45.2	46.75	45.2
20 - 21	*	*	*	*	42.7	46	46	42.7	46	42.7
21 - 22	*	*	*	*	43	43	44.5	43	43.75	43
22 - 23	*	*	*	*	46	41	44	46	42.5	46
23 - 24	*	*	*	*	38	39	42	38	40.5	38
Totals	0	0	0	0	486.8	1010.7	964.4			
% of Total	0%	0%	0%	0%	19.77%	41.05%	39.17%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	42	35	39	39	*	*	*	38.75	0	38.75
1 - 2	41	50	37	40	*	*	*	42	0	42
2 - 3	46	36	34	44	*	*	*	40	0	40
3 - 4	0	37	38	28	*	*	*	34.33	0	34.33
4 - 5	0	35	47	0	*	*	*	41	0	41
5 - 6	41	0	0	0	*	*	*	41	0	41
6 - 7	49	45.7	45.5	44	*	*	*	46.05	0	46.05
7 - 8	43	46	47	44	*	*	*	45	0	45
8 - 9	43.5	47	45.5	44.7	*	*	*	45.18	0	45.18
9 - 10	45	45	45	45	*	*	*	45	0	45
10 - 11	45.9	45	45	44	*	*	*	44.98	0	44.98
11 - 12	45.8	43.9	46	*	*	*	*	45.23	0	45.23
12 - 13	46	44.5	46	*	*	*	*	45.5	0	45.5
13 - 14	44.8	43.8	44	*	*	*	*	44.2	0	44.2
14 - 15	44	45	44.9	*	*	*	*	44.63	0	44.63
15 - 16	44.5	49	43.7	*	*	*	*	45.73	0	45.73
16 - 17	45.2	46.3	45.3	*	*	*	*	45.6	0	45.6
17 - 18	43.5	46.5	46	*	*	*	*	45.33	0	45.33
18 - 19	45.8	44	46.9	*	*	*	*	45.57	0	45.57
19 - 20	47	46	46.5	*	*	*	*	46.5	0	46.5
20 - 21	41.5	45.2	46	*	*	*	*	44.23	0	44.23
21 - 22	46	44.3	46	*	*	*	*	45.43	0	45.43
22 - 23	43.5	42	42	*	*	*	*	42.5	0	42.5
23 - 24	41	44	38	*	*	*	*	41	0	41
Totals	975	1006.2	1004.3	372.7	0	0	0			
% of Total	29.03%	29.96%	29.91%	11.1%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	42	35	39	39	*	45	39	38.75	42	38.75
1 - 2	41	50	37	40	*	43	38	42	40.5	42
2 - 3	46	36	34	44	*	48	42	40	45	40
3 - 4	0	37	38	28	*	34.5	35	34.33	34.75	34.33
4 - 5	0	35	47	0	*	37	40	41	38.5	41
5 - 6	41	0	0	0	*	0	0	41	0	41
6 - 7	49	45.7	45.5	44	*	44	0	46.05	44	46.05
7 - 8	43	46	47	44	*	45	44	45	44.5	45
8 - 9	43.5	47	45.5	44.7	*	45	46	45.18	45.5	45.18
9 - 10	45	45	45	45	*	46.3	46	45	46.15	45
10 - 11	45.9	45	45	44	*	47	46.5	44.98	46.75	44.98
11 - 12	45.8	43.9	46	*	*	46	45	45.23	45.5	45.23
12 - 13	46	44.5	46	*	*	46	44.5	45.5	45.25	45.5
13 - 14	44.8	43.8	44	*	44	45.5	45	44.15	45.25	44.15
14 - 15	44	45	44.9	*	45	45.7	47.5	44.72	46.6	44.72
15 - 16	44.5	49	43.7	*	44.2	44.4	44.5	45.35	44.45	45.35
16 - 17	45.2	46.3	45.3	*	46.4	43	46	45.8	44.5	45.8
17 - 18	43.5	46.5	46	*	45.3	46	45.7	45.32	45.85	45.32
18 - 19	45.8	44	46.9	*	47	43.3	46.7	45.92	45	45.92
19 - 20	47	46	46.5	*	45.2	47	46.5	46.18	46.75	46.18
20 - 21	41.5	45.2	46	*	42.7	46	46	43.85	46	43.85
21 - 22	46	44.3	46	*	43	43	44.5	44.82	43.75	44.82
22 - 23	43.5	42	42	*	46	41	44	43.38	42.5	43.38
23 - 24	41	44	38	*	38	39	42	40.25	40.5	40.25

Summary of Violators

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	27	37.5	1	57.0
01:00:00	25	38.1	3	54.3
02:00:00	13	39.2	2	47.0
03:00:00	11	31.7	0	0.0
04:00:00	12	34.3	1	47.0
05:00:00	1	41.0	0	0.0
06:00:00	52	43.0	13	49.3
07:00:00	101	41.3	16	48.5
08:00:00	218	40.6	40	49.0
09:00:00	230	39.8	37	48.2
10:00:00	215	40.3	44	48.2
11:00:00	204	40.7	34	48.4
12:00:00	249	40.7	39	49.3
13:00:00	315	40.5	40	49.3
14:00:00	333	41.1	61	49.2
15:00:00	374	40.2	47	48.7
16:00:00	338	40.4	59	48.4
17:00:00	334	39.7	59	48.2
18:00:00	347	40.7	67	48.9
19:00:00	326	41.3	72	48.8
20:00:00	211	39.6	29	48.9
21:00:00	152	39.8	19	48.2
22:00:00	73	38.6	9	47.8
23:00:00	68	36.9	5	50.2

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/23/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	13:00	0	0	0	0	7	15	30	3	4	2	0	0	0	0	0	0	0	0	61	41.1	44	34 to 44	77.0	8	13.1	0	57	4	
8/23/2024	14:00	0	0	0	1	3	12	22	8	3	1	0	0	0	0	0	0	0	0	50	41.6	45	38 to 48	78.0	8	16.0	1	43	6	
8/23/2024	15:00	0	0	1	0	12	15	29	11	0	0	0	0	0	0	0	0	0	0	68	39.6	44.2	34 to 44	75.0	7	10.3	1	62	5	
8/23/2024	16:00	0	0	0	1	2	18	20	15	5	0	0	0	0	0	0	0	0	0	61	41.9	46.4	37 to 47	78.7	16	26.2	0	55	6	
8/23/2024	17:00	0	0	4	0	2	14	22	9	1	0	0	0	0	0	0	0	0	0	52	39.6	45.3	36 to 46	75.0	10	19.2	0	48	4	
8/23/2024	18:00	0	0	0	3	12	15	22	10	5	2	0	0	0	0	0	0	0	0	69	40.6	47	36 to 46	60.9	15	21.7	0	65	4	
8/23/2024	19:00	0	0	0	3	5	9	22	8	3	0	0	0	0	0	0	0	0	0	50	40.6	45.2	37 to 47	72.0	9	18.0	0	48	2	
8/23/2024	20:00	0	0	0	1	3	14	15	4	0	0	0	0	0	0	0	0	0	0	37	39.3	42.7	33 to 43	81.1	3	8.1	0	36	1	
8/23/2024	21:00	0	1	0	0	1	7	4	2	0	0	0	0	0	0	0	0	0	0	15	38.1	43	35 to 45	80.0	1	6.7	0	15	0	
8/23/2024	22:00	0	0	0	2	0	6	5	2	1	0	0	0	0	0	0	0	0	0	16	39.5	46	34 to 44	68.8	3	18.8	0	16	0	
8/23/2024	23:00	0	0	0	1	3	6	1	0	1	0	0	0	0	0	0	0	0	0	12	36.3	38	31 to 41	83.3	1	8.3	0	11	1	
24 Hr Summary		0	1	5	12	50	131	192	72	23	5	0	0	0	0	0	0	0	0	491	40.4	46	36 to 46	70.7	81	16.5	2	456	33	

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/24/2024	00:00	0	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	3	41.7	45	35 to 45	100.0	*	*	0	3	0	
8/24/2024	01:00	0	0	0	0	0	2	2	0	1	0	0	0	0	0	0	0	0	0	5	42.2	43	33 to 43	80.0	1	20.0	0	5	0	
8/24/2024	02:00	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	44.5	48	38 to 48	100.0	1	50.0	0	2	0	
8/24/2024	03:00	0	0	0	1	2	2	0	0	0	0	0	0	0	0	0	0	0	0	5	32.6	34.5	25 to 35	100.0	*	*	0	5	0	
8/24/2024	04:00	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3	35.7	37	27 to 37	100.0	*	*	0	3	0	
8/24/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	06:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	44	44	34 to 44	100.0	*	*	0	1	0	
8/24/2024	07:00	0	0	0	0	1	5	5	3	0	0	0	0	0	0	0	0	0	0	14	40.4	45	33 to 43	78.6	2	14.3	0	13	1	
8/24/2024	08:00	0	0	0	1	0	9	3	3	1	0	0	0	0	0	0	0	0	0	17	40	45	35 to 45	76.5	3	17.6	0	17	0	
8/24/2024	09:00	0	0	0	0	3	6	15	11	2	0	0	0	0	0	0	0	0	0	37	42.2	46.3	37 to 47	78.4	8	21.6	0	37	0	
8/24/2024	10:00	0	0	0	1	2	8	9	8	1	1	0	0	0	0	0	0	0	0	30	41.7	47	37 to 47	70.0	8	26.7	0	29	1	
8/24/2024	11:00	0	0	0	1	4	15	15	6	2	0	0	0	0	0	0	0	0	0	43	40.5	46	34 to 44	76.7	7	16.3	0	42	1	
8/24/2024	12:00	0	0	1	1	3	13	21	8	5	0	0	0	0	0	0	0	0	0	52	40.9	46	35 to 45	73.1	9	17.3	0	52	0	
8/24/2024	13:00	0	0	0	1	3	13	17	6	2	1	0	0	0	0	0	0	0	0	43	41	45.5	34 to 44	76.7	7	16.3	0	42	1	
8/24/2024	14:00	0	0	0	0	7	14	12	9	2	0	0	0	0	0	0	0	0	0	44	40	45.7	35 to 45	63.6	9	20.5	0	43	1	
8/24/2024	15:00	0	0	0	0	2	26	20	9	2	0	0	0	0	0	0	0	0	0	59	40.6	44.4	35 to 45	86.4	6	10.2	0	58	1	
8/24/2024	16:00	0	0	0	1	11	7	15	3	0	0	0	0	0	0	0	0	0	0	37	38.5	43	33 to 43	75.7	1	2.7	0	37	0	
8/24/2024	17:00	0	0	0	0	4	7	13	9	2	0	0	0	0	0	0	0	0	0	35	41.8	46	38 to 48	77.1	7	20.0	0	35	0	
8/24/2024	18:00	0	0	0	1	8	17	20	2	1	0	0	0	0	0	0	0	0	0	49	39	43.3	34 to 44	81.6	3	6.1	1	46	2	
8/24/2024	19:00	0	0	0	2	7	13	14	7	3	0	0	0	0	0	0	0	0	0	46	39.8	47	33 to 43	65.2	9	19.6	0	42	4	
8/24/2024	20:00	0	0	0	0	5	9	5	3	2	0	0	0	0	0	0	0	0	0	24	39.3	46	31 to 41	75.0	5	20.8	0	24	0	
8/24/2024	21:00	0	0	0	1	4	6	5	1	1	0	0	0	0	0	0	0	0	0	18	38.4	43	33 to 43	72.2	2	11.1	0	18	0	
8/24/2024	22:00	0	0	0	2	1	3	2	2	0	0	0	0	0	0	0	0	0	0	10	36.8	41	25 to 35	50.0	2	20.0	0	10	0	
8/24/2024	23:00	0	0	0	1	3	6	1	1	0	0	0	0	0	0	0	0	0	0	12	36.6	39	28 to 38	75.0	1	8.3	0	12	0	
24 Hr Summary		0	0	1	14	71	184	197	93	27	2	0	0	0	0	0	0	0	0	589	40.2	46	35 to 45	69.9	91	15.4	1	576	12	

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/25/2024	00:00	0	0	0	0	0	4	1	0	0	0	0	0	0	0	0	0	0	0	5	38.2	39	33 to 43	100.0	*	*	0	5	0	
8/25/2024	01:00	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	5	34.6	38	29 to 39	100.0	*	*	0	5	0	
8/25/2024	02:00	0	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3	37.7	42	32 to 42	100.0	*	*	0	3	0	
8/25/2024	03:00	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	27.5	35	10 to 20	50.0	*	*	0	2	0	
8/25/2024	04:00	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	3	37.3	40	30 to 40	100.0	*	*	0	3	0	
8/25/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	07:00	0	0	0	0	2	3	3	1	0	0	0	0	0	0	0	0	0	0	9	39.3	44	34 to 44	66.7	1	11.1	0	9	0	
8/25/2024	08:00	0	0	0	0	2	6	6	4	2	0	0	0	0	0	0	0	0	0	20	41.2	46	34 to 44	65.0	5	25.0	0	20	0	
8/25/2024	09:00	0	0	0	0	2	7	6	6	0	1	0	0	0	0	0	0	0	0	22	41.6	46	36 to 46	77.3	4	18.2	0	22	0	
8/25/2024	10:00	0	0	0	0	0	6	9	6	2	1	0	0	0	0	0	0	0	0	24	43.2	46.5	37 to 47	79.2	8	33.3	0	24	0	
8/25/2024	11:00	0	0	0	0	2	11	15	5	1	0	0	0	0	0	0	0	0	0	34	40.9	45	34 to 44	79.4	5	14.7	0	31	3	
8/25/2024	12:00	0	0	0	0	5	16	24	8	1	0	0	0	0	0	0	0	0	0	54	40.6	44.5	34 to 44	77.8	7	13.0	0	53	1	
8/25/2024	13:00	0	0	0	0	8	17	21	12	0	1	0	0	0	0	0	0	0	0	59	40.6	45	37 to 47	76.3	9	15.3	0	58	1	
8/25/2024	14:00	0	0	0	0	7	23	23	20	4	0	1	0	0	0	0	0	0	0	78	41.9	47.5	39 to 49	67.9	21	26.9	0	78	0	
8/25/2024	15:00	0	0	0	0	1	16	28	11	1	0	0	0	0	0	0	0	0	0	57	41.4	44.5	35 to 45	87.7	6	10.5	0	54	3	
8/25/2024	16:00	1	0	0	0	4	20	12	9	3	0	0	0	0	0	0	0	0	0	49	40	46	35 to 45	73.5	8	16.3	0	47	2	
8/25/2024	17:00	0	0	1	1	1	21	17	11	0	2	0	0	0	0	0	0	0	0	54	40.9	45.7	36 to 46	81.5	12	22.2	0	54	0	
8/25/2024	18:00	0	0	0	0	2	13	11	10	1	2	0	0	0	0	0	0	0	0	39	42.2	46.7	37 to 47	76.9	11	28.2	0	39	0	
8/25/2024	19:00	0	0	0	0	2	15	11	11	3	0	0	0	0	0	0	0	0	0	42	41.8	46.5	38 to 48	81.0	12	28.6	0	41	1	
8/25/2024	20:00	0	0	0	3	6	4	12	3	2	0	0	0	0	0	0	0	0	0	30	39	46	38 to 48	60.0	5	16.7	0	30	0	
8/25/2024	21:00	0	0	0	1	5	9	6	3	2	0	0	0	0	0	0	0	0	0	26	38.9	44.5	34 to 44	65.4	3	11.5	0	25	1	
8/25/2024	22:00	0	0	0	0	0	3	5	1	0	0	0	0	0	0	0	0	0	0	9	40.9	44	34 to 44	88.9	1	11.1	0	8	1	
8/25/2024	23:00	0	0	0	0	5	3	4	1	0	0	0	0	0	0	0	0	0	0	13	37.9	42	32 to 42	84.6	1	7.7	0	13	0	
24 Hr Summary		1	0	2	5	58	203	216	122	22	7	1	0	0	0	0	0	0	0	637	40.8	46	36 to 46	72.1	119	18.7	0	624	13	

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/26/2024	00:00	0	0	1	0	2	2	2	0	0	1	0	0	0	0	0	0	0	0	8	37.4	42	30 to 40	62.5	1	12.5	0	8	0	
8/26/2024	01:00	0	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5	39.4	41	34 to 44	100.0	*	*	0	5	0	
8/26/2024	02:00	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	45	46	36 to 46	100.0	1	50.0	0	2	0	
8/26/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	05:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	41	41	31 to 41	100.0	*	*	0	1	0	
8/26/2024	06:00	0	0	0	0	0	2	9	3	2	0	0	0	0	0	0	0	0	0	16	43.9	49	39 to 49	87.5	4	25.0	0	16	0	
8/26/2024	07:00	0	0	0	0	1	5	11	1	1	0	0	0	0	0	0	0	0	0	19	40.6	43	35 to 45	89.5	1	5.3	0	19	0	
8/26/2024	08:00	0	0	0	0	3	10	13	2	0	1	0	0	0	0	0	0	0	0	29	40.1	43.5	34 to 44	89.7	2	6.9	0	29	0	
8/26/2024	09:00	0	0	0	5	6	10	11	7	1	0	0	0	0	0	0	0	0	0	40	38.4	45	37 to 47	65.0	6	15.0	0	37	3	
8/26/2024	10:00	1	1	1	2	2	12	15	15	1	1	0	0	0	0	0	0	0	0	51	40.2	45.9	36 to 46	70.6	14	27.5	1	44	6	
8/26/2024	11:00	0	0	1	1	8	10	16	10	1	0	0	0	0	0	0	0	0	0	47	39.8	45.8	37 to 47	66.0	10	21.3	0	41	6	
8/26/2024	12:00	0	0	1	1	2	17	19	6	2	1	0	0	0	0	0	0	0	0	49	40.6	46	35 to 45	75.5	8	16.3	0	41	8	
8/26/2024	13:00	0	0	1	1	3	16	20	10	1	1	0	0	0	0	0	0	0	0	53	40.8	44.8	35 to 45	77.4	7	13.2	0	49	4	
8/26/2024	14:00	0	0	0	1	4	20	19	6	1	1	0	0	0	0	0	0	0	0	52	40	44	34 to 44	78.8	7	13.5	1	46	5	
8/26/2024	15:00	0	0	0	1	6	29	30	11	2	0	0	0	0	0	0	0	0	0	79	40.1	44.5	34 to 44	79.7	11	13.9	0	76	3	
8/26/2024	16:00	0	0	1	2	4	25	18	11	1	0	0	0	0	0	0	0	0	0	62	39.5	45.2	36 to 46	75.8	10	16.1	0	56	6	
8/26/2024	17:00	1	1	1	1	6	24	23	6	1	0	0	0	0	0	0	0	0	0	64	38.7	43.5	34 to 44	78.1	7	10.9	0	63	1	
8/26/2024	18:00	0	0	1	0	6	18	14	13	1	0	0	0	0	0	0	0	0	0	53	40.3	45.8	36 to 46	69.8	11	20.8	0	53	0	
8/26/2024	19:00	0	0	0	0	1	15	10	13	3	0	0	0	0	0	0	0	0	0	42	42.2	47	37 to 47	69.0	16	38.1	0	42	0	
8/26/2024	20:00	0	0	1	0	5	13	13	1	0	0	0	0	0	0	0	0	0	0	33	38	41.5	34 to 44	84.8	1	3.0	0	33	0	
8/26/2024	21:00	0	0	0	0	3	12	12	7	0	0	0	0	0	0	0	0	0	0	34	40.6	46	37 to 47	85.3	6	17.6	0	34	0	
8/26/2024	22:00	0	0	0	0	5	5	3	1	0	0	0	0	0	0	0	0	0	0	14	37.6	43.5	34 to 44	78.6	1	7.1	0	14	0	
8/26/2024	23:00	0	0	0	0	1	6	2	1	0	0	0	0	0	0	0	0	0	0	10	38.5	41	31 to 41	80.0	1	10.0	0	10	0	
24 Hr Summary		2	2	9	15	68	254	264	125	18	6	0	0	0	0	0	0	0	0	763	40	46	34 to 44	71.6	125	16.4	2	719	42	

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/27/2024	00:00	0	0	0	0	3	1	1	0	0	0	0	0	0	0	0	0	0	0	5	34.6	35	25 to 35	80.0	*	*	0	5	0	
8/27/2024	01:00	0	0	0	1	0	1	0	0	1	0	0	0	0	0	0	0	0	0	3	37	50	25 to 35	66.7	1	33.3	0	3	0	
8/27/2024	02:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34.5	36	26 to 36	100.0	*	*	0	2	0	
8/27/2024	03:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	32.5	37	27 to 37	100.0	*	*	0	2	0	
8/27/2024	04:00	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	28	35	15 to 25	66.7	*	*	0	3	0	
8/27/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	06:00	0	0	0	0	0	3	4	4	1	0	0	0	0	0	0	0	0	0	12	43.6	45.7	36 to 46	83.3	4	33.3	0	12	0	
8/27/2024	07:00	0	0	0	0	2	5	12	7	1	0	0	0	0	0	0	0	0	0	27	42.1	46	36 to 46	77.8	5	18.5	1	25	1	
8/27/2024	08:00	0	0	0	3	2	6	18	11	3	2	0	0	0	0	0	0	0	0	45	42.3	47	37 to 47	71.1	14	31.1	0	42	3	
8/27/2024	09:00	0	0	0	2	6	13	13	6	2	0	0	0	0	0	0	0	0	0	42	39.3	45	34 to 44	71.4	6	14.3	0	37	5	
8/27/2024	10:00	0	0	0	1	5	8	12	5	1	0	0	0	0	0	0	0	0	0	32	39.6	45	34 to 44	65.6	5	15.6	0	29	3	
8/27/2024	11:00	0	0	0	0	1	17	20	3	2	0	0	0	0	0	0	0	0	0	43	41	43.9	34 to 44	86.0	5	11.6	0	35	8	
8/27/2024	12:00	0	0	0	1	4	16	15	4	1	1	1	0	0	0	0	0	0	0	43	40.2	44.5	34 to 44	76.7	5	11.6	0	38	5	
8/27/2024	13:00	0	0	0	0	2	20	27	7	1	0	0	0	0	0	0	0	0	0	57	40.4	43.8	35 to 45	89.5	4	7.0	0	53	4	
8/27/2024	14:00	0	0	0	0	3	14	16	9	0	2	0	0	0	0	0	0	0	0	44	41.5	45	35 to 45	77.3	7	15.9	0	43	1	
8/27/2024	15:00	0	0	0	2	6	13	14	5	5	2	0	0	0	0	0	0	0	0	47	40.9	49	33 to 43	63.8	10	21.3	1	38	8	
8/27/2024	16:00	0	0	0	0	4	13	22	9	5	0	0	0	0	0	0	0	0	0	53	41.9	46.3	36 to 46	75.5	12	22.6	1	51	1	
8/27/2024	17:00	0	0	3	4	6	13	19	8	6	0	0	0	0	0	0	0	0	0	59	39.3	46.5	35 to 45	61.0	10	16.9	1	54	4	
8/27/2024	18:00	0	0	0	2	10	25	28	9	2	1	0	0	0	0	0	0	0	0	77	39.7	44	36 to 46	74.0	10	13.0	0	72	5	
8/27/2024	19:00	0	0	0	0	7	20	37	12	4	2	0	0	0	0	0	0	0	0	82	41.7	46	35 to 45	75.6	13	15.9	0	81	1	
8/27/2024	20:00	0	0	1	0	2	19	12	10	2	0	0	0	0	0	0	0	0	0	46	40.9	45.2	36 to 46	82.6	8	17.4	0	45	1	
8/27/2024	21:00	0	0	0	0	3	8	14	4	1	0	0	0	0	0	0	0	0	0	30	40.4	44.3	35 to 45	83.3	2	6.7	0	28	2	
8/27/2024	22:00	0	0	0	0	5	1	3	1	0	0	0	0	0	0	0	0	0	0	10	37.5	42	32 to 42	70.0	1	10.0	0	10	0	
8/27/2024	23:00	0	1	0	0	3	3	1	0	0	1	0	0	0	0	0	0	0	0	9	36.8	44	28 to 38	66.7	1	11.1	0	9	0	
24 Hr Summary		0	1	5	18	75	222	288	114	38	11	1	0	0	0	0	0	0	0	773	40.5	46	35 to 45	71.3	123	15.9	4	717	52	

Incoming Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/28/2024	00:00	0	0	0	0	2	2	1	0	0	0	0	0	0	0	0	0	0	0	5	37.2	39	31 to 41	100.0	*	*	0	5	0	
8/28/2024	01:00	0	0	0	0	2	1	0	0	0	0	1	0	0	0	0	0	0	0	4	40	37	27 to 37	75.0	1	25.0	0	4	0	
8/28/2024	02:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	34	34	24 to 34	100.0	*	*	0	1	0	
8/28/2024	03:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	38	38	28 to 38	100.0	*	*	0	1	0	
8/28/2024	04:00	0	1	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	3	36.3	47	37 to 47	66.7	1	33.3	0	3	0	
8/28/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	06:00	0	0	0	1	0	2	5	4	0	0	0	0	0	0	0	0	0	0	12	41.4	45.5	36 to 46	75.0	3	25.0	0	12	0	
8/28/2024	07:00	0	0	0	0	1	5	9	7	0	0	0	0	0	0	0	0	0	0	22	41.8	47	35 to 45	72.7	5	22.7	0	21	1	
8/28/2024	08:00	0	0	0	5	5	13	21	9	3	0	0	0	0	0	0	0	0	0	56	39.9	45.5	37 to 47	69.6	9	16.1	0	54	2	
8/28/2024	09:00	0	0	3	2	3	16	11	5	3	0	0	0	0	0	0	0	0	0	43	38.2	45	35 to 45	67.4	6	14.0	0	37	6	
8/28/2024	10:00	0	0	0	0	5	13	14	7	0	0	0	0	0	0	0	0	0	0	39	40.4	45	34 to 44	76.9	6	15.4	0	39	0	
8/28/2024	11:00	0	0	0	0	4	10	14	6	3	0	0	0	0	0	0	0	0	0	37	41.4	46	35 to 45	70.3	7	18.9	0	33	4	
8/28/2024	12:00	0	0	0	0	6	13	18	11	3	0	0	0	0	0	0	0	0	0	51	41.2	46	36 to 46	70.6	10	19.6	0	47	4	
8/28/2024	13:00	0	0	0	1	11	14	10	5	1	0	0	0	0	0	0	0	0	0	42	38.5	44	33 to 43	76.2	5	11.9	1	36	5	
8/28/2024	14:00	0	0	0	0	7	18	22	14	4	0	0	0	0	0	0	0	0	0	65	40.9	44.9	35 to 45	75.4	9	13.8	0	57	8	
8/28/2024	15:00	0	0	0	1	13	17	25	6	2	0	0	0	0	0	0	0	0	0	64	39	43.7	34 to 44	67.2	7	10.9	0	57	7	
8/28/2024	16:00	0	0	0	2	10	20	29	11	3	0	1	0	0	0	0	0	0	0	76	40.3	45.3	34 to 44	71.1	12	15.8	0	69	7	
8/28/2024	17:00	0	0	3	3	8	21	20	11	4	0	0	0	0	0	0	0	0	0	70	39.1	46	34 to 44	61.4	13	18.6	0	64	6	
8/28/2024	18:00	0	0	0	1	1	13	23	19	1	2	0	0	0	0	0	0	0	0	60	42.6	46.9	38 to 48	80.0	17	28.3	0	57	3	
8/28/2024	19:00	0	1	0	0	9	12	26	12	3	0	1	0	0	0	0	0	0	0	64	41.5	46.5	39 to 49	70.3	13	20.3	0	62	2	
8/28/2024	20:00	0	0	0	2	6	11	14	6	1	0	0	0	1	0	0	0	0	0	41	40.3	46	33 to 43	68.3	7	17.1	0	41	0	
8/28/2024	21:00	0	0	0	0	3	8	12	6	0	0	0	0	0	0	0	0	0	0	29	40.6	46	36 to 46	75.9	5	17.2	0	29	0	
8/28/2024	22:00	0	0	0	0	1	8	3	2	0	0	0	0	0	0	0	0	0	0	14	39.3	42	35 to 45	85.7	1	7.1	0	14	0	
8/28/2024	23:00	0	0	0	1	6	3	2	0	0	0	0	0	0	0	0	0	0	0	12	35.3	38	28 to 38	75.0	*	*	0	12	0	
24 Hr Summary		0	2	6	19	104	221	280	142	31	2	3	0	1	0	0	0	0	0	811	40.2	46	35 to 45	67.0	137	16.9	1	755	55	

Incoming Histogram

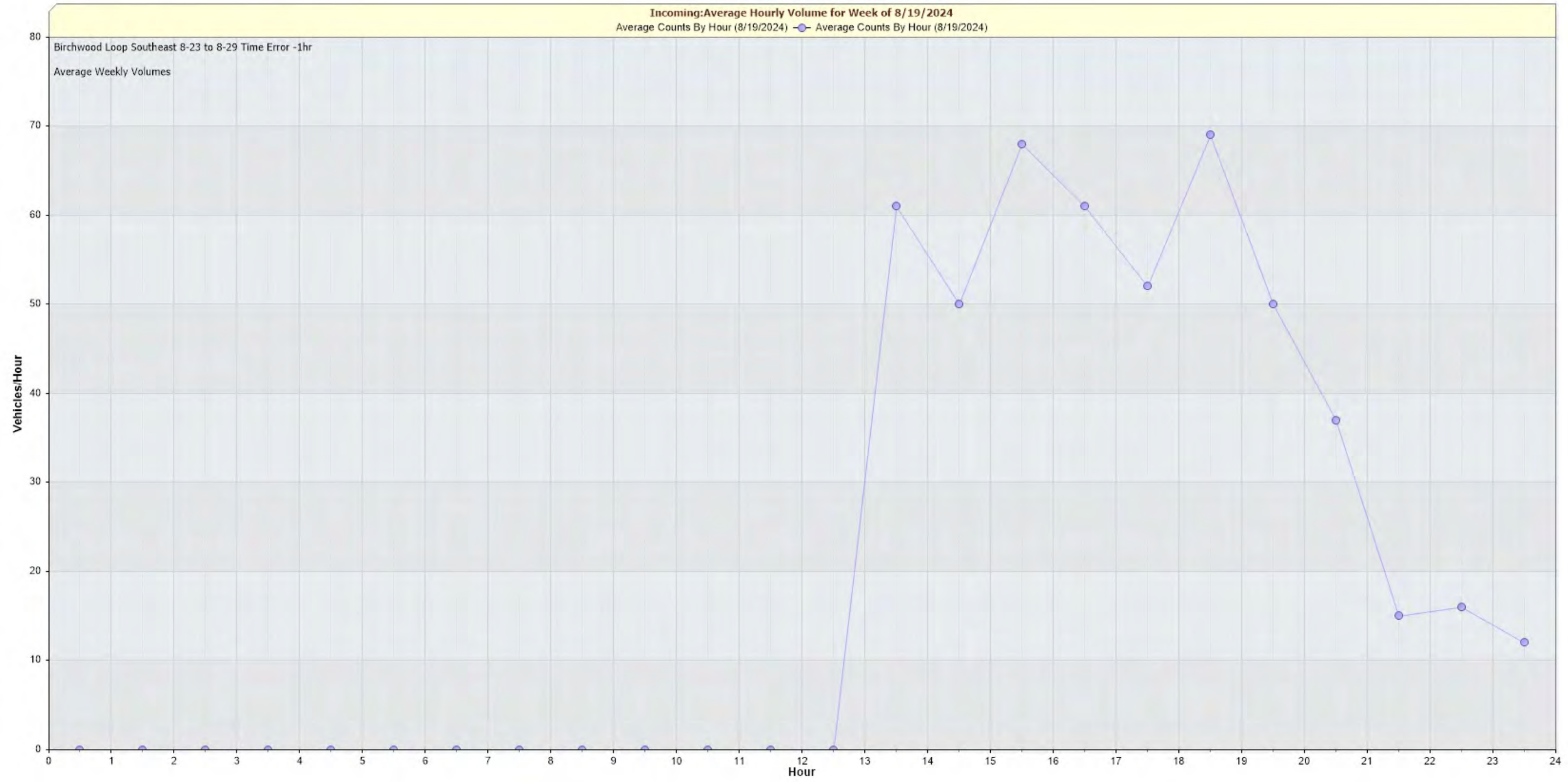
Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	39	39	29 to 39	100.0	*	*	0	1	0
8/29/2024	01:00	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	33.7	40	23 to 33	66.7	*	*	0	2	1
8/29/2024	02:00	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	38.3	44	28 to 38	66.7	*	*	0	3	0
8/29/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	0	1
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	0	0	0	0	1	3	5	0	1	0	1	0	0	0	0	0	0	0	0	11	42.5	44	34 to 44	81.8	2	18.2	0	11	0
8/29/2024	07:00	0	0	0	0	1	2	5	1	1	0	0	0	0	0	0	0	0	0	0	10	42.1	44	38 to 48	80.0	2	20.0	0	9	1
8/29/2024	08:00	0	0	0	1	6	11	23	8	2	0	0	0	0	0	0	0	0	0	0	51	40.3	44.7	35 to 45	72.5	7	13.7	0	49	2
8/29/2024	09:00	0	0	0	3	4	12	15	11	0	1	0	0	0	0	0	0	0	0	0	46	40.2	45	36 to 46	78.3	7	15.2	0	41	5
8/29/2024	10:00	0	0	1	0	9	19	4	4	2	0	0	0	0	0	0	0	0	0	0	39	38	44	29 to 39	71.8	3	7.7	0	36	3
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	1	6	23	49	54	24	6	1	1	0	0	0	0	0	0	0	0	165	39.8	45	36 to 46	70.3	21	12.7	0	152	13

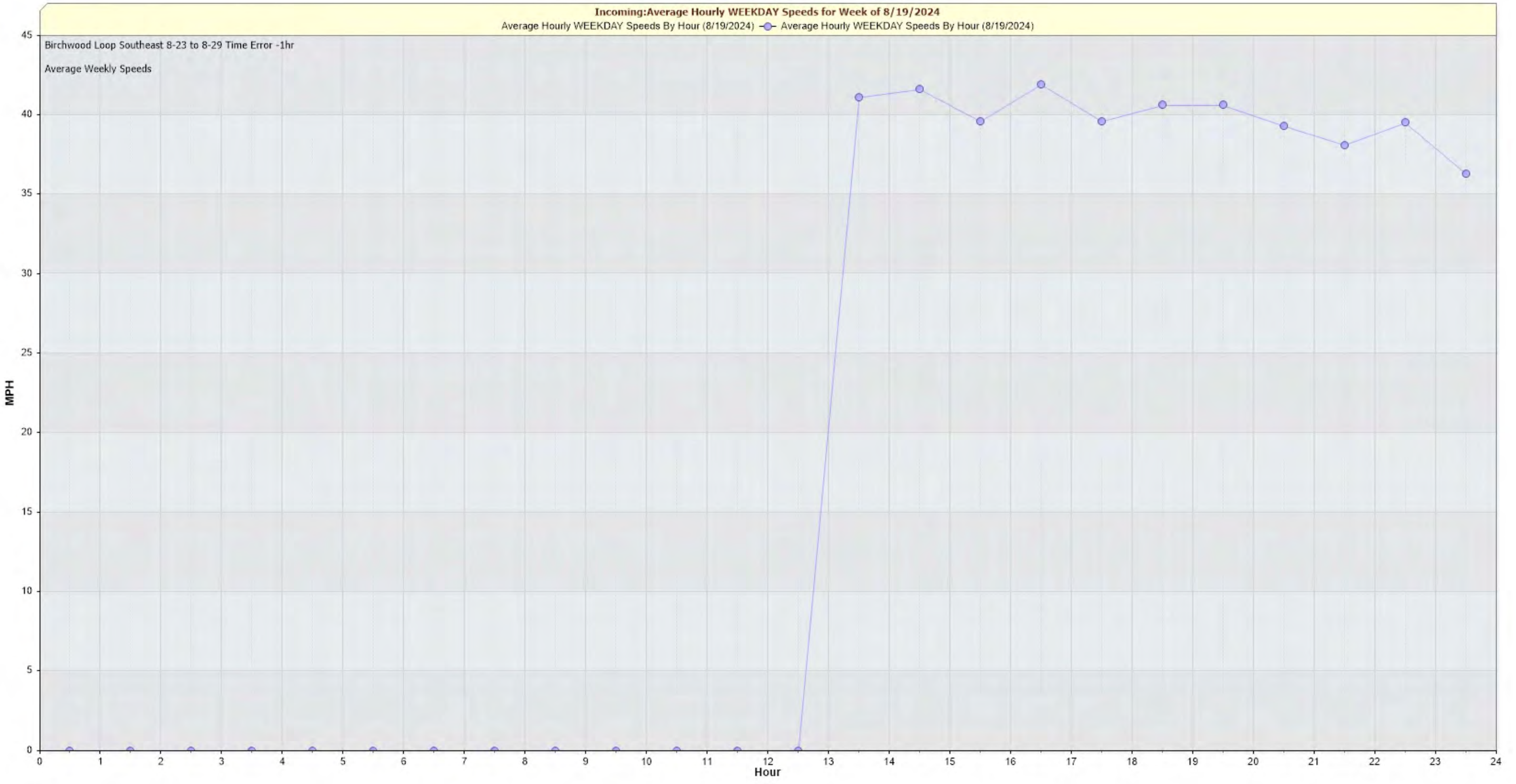
Incoming: Average Hourly Volume for Week of 8/19/2024
Average Counts By Hour (8/19/2024) — Average Counts By Hour (8/19/2024)

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr
Average Weekly Volumes



Incoming:Average Hourly WEEKDAY Speeds for Week of 8/19/2024
Average Hourly WEEKDAY Speeds By Hour (8/19/2024) — Average Hourly WEEKDAY Speeds By Hour (8/19/2024)

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr
Average Weekly Speeds

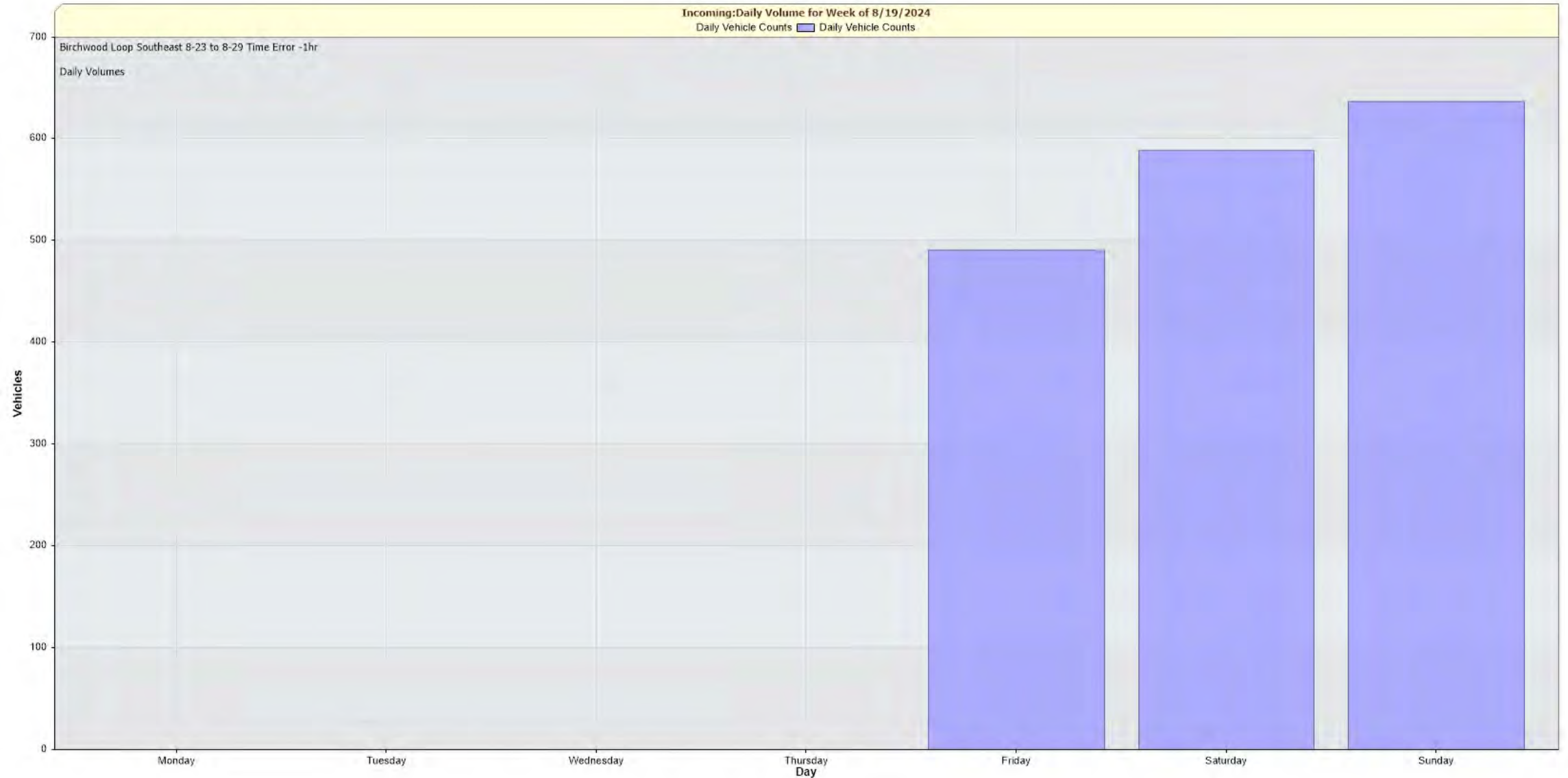


Incoming:Daily Volume for Week of 8/19/2024

Daily Vehicle Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

Daily Volumes



For Project: Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:24:31 PM

Speed Intervals: 1 MPH

Time Intervals: Instant

Traffic Report From: 8/23/2024 1:00:00 PM through 8/29/2024 10:59:59 AM

85th Percentile Speed: 45 MPH

85th Percentile Vehicles: 3730

Max Speed: 79 MPH on 8/25/2024 4:35:30 PM

Total Vehicles: 4388

AADT: 741

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	626	626
AM Peak	9:00 AM 40	37
PM Peak	4:00 PM 77	73

Speed

Speed Limit: 45

85th Percentile Speed: 45

50th Percentile Speed: 38

10 MPH Pace Interval: 35.0 MPH to 45.0 MPH

Average Speed: 37.79

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	90	88	95	15	87	73	87
% over limit	11.9	10.9	11.6	10.0	14.6	11.9	13.5
Avg Speeder	48.2	48.4	48.6	48.7	48.4	48.3	48.9
Avg Speed	37.6	37.6	37.8	35.3	38.3	37.5	38.7

Class Counts

	Number	%
VEH_SM	8	0.2
VEH_MED	4155	94.7
VEH_LG	225	5.1
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/23/2024 2:00:00 PM	45.0	37	43	53	49.8	11.6%	37.4
8/23/2024 3:00:00 PM	46.0	50	59	52	47.8	20.3%	39.7
8/23/2024 4:00:00 PM	45.0	69	81	53	48.5	14.8%	37.7
8/23/2024 5:00:00 PM	44.0	90	106	57	48.7	9.4%	38.3
8/23/2024 6:00:00 PM	46.0	62	73	52	47.9	16.4%	38.7
8/23/2024 7:00:00 PM	46.0	60	71	58	48.9	16.9%	39.4
8/23/2024 8:00:00 PM	45.0	48	57	55	48.4	15.8%	39.0
8/23/2024 9:00:00 PM	44.0	33	39	52	48.3	7.7%	37.1
8/23/2024 10:00:00 PM	47.0	20	24	52	48.3	29.2%	38.2
8/23/2024 11:00:00 PM	43.0	25	29	48	46.7	10.3%	34.8
8/24/2024 12:00:00 AM	44.0	13	15	49	47.5	13.3%	37.9
8/24/2024 1:00:00 AM	41.0	7	8	48	48.0	12.5%	34.3
8/24/2024 2:00:00 AM	46.0	3	3	46	46.0	33.3%	38.7
8/24/2024 3:00:00 AM	41.0	3	4	42	0.0	0.0%	34.8
8/24/2024 4:00:00 AM	41.0	2	2	41	0.0	0.0%	40.5
8/24/2024 5:00:00 AM	37.0	2	2	37	0.0	0.0%	33.5
8/24/2024 6:00:00 AM	**No Data**						
8/24/2024 7:00:00 AM	37.0	2	2	37	0.0	0.0%	33.0
8/24/2024 8:00:00 AM	46.0	8	9	47	46.5	22.2%	38.3
8/24/2024 9:00:00 AM	42.0	10	12	44	0.0	0.0%	36.8
8/24/2024 10:00:00 AM	43.0	20	24	47	46.7	12.5%	37.6
8/24/2024 11:00:00 AM	44.0	27	32	48	47.0	9.4%	37.7
8/24/2024 12:00:00 PM	45.0	33	39	49	47.2	12.8%	37.6
8/24/2024 1:00:00 PM	44.0	44	52	54	48.3	11.5%	36.2
8/24/2024 2:00:00 PM	45.0	46	54	57	50.1	14.8%	38.2
8/24/2024 3:00:00 PM	42.0	41	48	47	46.5	8.3%	36.6
8/24/2024 4:00:00 PM	44.0	55	65	49	47.0	7.7%	37.6
8/24/2024 5:00:00 PM	45.0	50	59	50	47.5	13.6%	38.1
8/24/2024 6:00:00 PM	43.0	37	44	52	49.0	6.8%	38.3
8/24/2024 7:00:00 PM	44.0	34	40	51	48.5	9.8%	38.6
8/24/2024 8:00:00 PM	47.0	23	27	55	49.7	22.2%	36.4
8/24/2024 9:00:00 PM	40.0	26	31	49	47.7	9.7%	35.0
8/24/2024 10:00:00 PM	43.0	21	25	55	50.0	12.0%	37.2
8/24/2024 11:00:00 PM	42.0	12	14	46	46.0	14.3%	37.6
8/25/2024 12:00:00 AM	49.0	13	15	60	51.2	40.0%	43.6
8/25/2024 1:00:00 AM	29.0	3	4	53	53.0	25.0%	34.8
8/25/2024 2:00:00 AM	57.0	1	1	57	57.0	100.0%	57.0
8/25/2024 3:00:00 AM	35.0	2	2	35	0.0	0.0%	33.5
8/25/2024 4:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
8/25/2024 5:00:00 AM	38.0	3	4	43	0.0	0.0%	35.5
8/25/2024 6:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
8/25/2024 7:00:00 AM	**No Data**						
8/25/2024 8:00:00 AM	43.0	6	7	44	0.0	0.0%	37.6
8/25/2024 9:00:00 AM	47.0	5	6	47	47.0	33.3%	38.0
8/25/2024 10:00:00 AM	46.0	17	20	54	48.0	20.0%	37.7
8/25/2024 11:00:00 AM	41.0	24	28	50	48.0	7.1%	36.7
8/25/2024 12:00:00 PM	44.0	27	32	52	50.7	9.4%	39.3

Outgoing Summary
 Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

8/25/2024 1:00:00 PM	45.0	37	44	57	51.2	13.6%	40.6
8/25/2024 2:00:00 PM	46.0	36	42	54	49.6	19.0%	38.9
8/25/2024 3:00:00 PM	46.0	47	55	53	47.8	21.8%	39.6
8/25/2024 4:00:00 PM	44.0	43	51	52	48.5	11.8%	38.1
8/25/2024 5:00:00 PM	45.0	60	70	79	52.1	10.0%	39.2
8/25/2024 6:00:00 PM	43.0	46	54	51	47.8	7.4%	37.2
8/25/2024 7:00:00 PM	45.0	64	75	52	48.1	12.0%	39.2
8/25/2024 8:00:00 PM	46.0	40	47	53	47.8	25.5%	39.7
8/25/2024 9:00:00 PM	44.0	39	46	52	48.3	8.7%	37.2
8/25/2024 10:00:00 PM	46.0	19	22	49	47.3	18.2%	38.7
8/25/2024 11:00:00 PM	43.0	15	18	45	0.0	0.0%	37.7
8/26/2024 12:00:00 AM	45.0	11	13	53	50.0	15.4%	39.5
8/26/2024 1:00:00 AM	34.0	2	2	34	0.0	0.0%	32.0
8/26/2024 2:00:00 AM	39.0	2	2	39	0.0	0.0%	32.5
8/26/2024 3:00:00 AM	28.0	2	2	28	0.0	0.0%	27.5
8/26/2024 4:00:00 AM	34.0	2	2	34	0.0	0.0%	33.0
8/26/2024 5:00:00 AM	19.0	1	1	19	0.0	0.0%	19.0
8/26/2024 6:00:00 AM	34.0	1	1	34	0.0	0.0%	34.0
8/26/2024 7:00:00 AM	45.0	5	6	52	52.0	16.7%	38.8
8/26/2024 8:00:00 AM	47.0	14	16	52	47.6	31.3%	40.1
8/26/2024 9:00:00 AM	44.0	22	26	52	49.5	7.7%	37.2
8/26/2024 10:00:00 AM	46.0	31	37	53	50.4	18.9%	37.6
8/26/2024 11:00:00 AM	43.0	45	53	47	46.3	5.7%	36.8
8/26/2024 12:00:00 PM	44.0	32	38	49	48.0	7.9%	36.7
8/26/2024 1:00:00 PM	44.0	44	52	48	46.7	11.5%	37.2
8/26/2024 2:00:00 PM	44.0	42	49	49	46.8	8.2%	37.9
8/26/2024 3:00:00 PM	46.0	45	53	52	48.2	17.0%	39.1
8/26/2024 4:00:00 PM	42.0	58	68	55	49.8	7.4%	36.4
8/26/2024 5:00:00 PM	46.0	68	80	58	49.0	17.5%	38.4
8/26/2024 6:00:00 PM	45.0	60	70	51	47.6	12.9%	38.6
8/26/2024 7:00:00 PM	45.0	64	75	50	47.1	10.7%	37.3
8/26/2024 8:00:00 PM	46.0	39	46	51	47.8	17.4%	38.8
8/26/2024 9:00:00 PM	44.0	18	21	46	46.0	9.5%	36.5
8/26/2024 10:00:00 PM	41.0	18	21	49	48.5	9.5%	37.3
8/26/2024 11:00:00 PM	42.0	19	22	43	0.0	0.0%	37.5
8/27/2024 12:00:00 AM	45.0	12	14	50	48.5	14.3%	39.0
8/27/2024 1:00:00 AM	42.0	3	4	47	47.0	25.0%	38.3
8/27/2024 2:00:00 AM	26.0	2	2	26	0.0	0.0%	22.0
8/27/2024 3:00:00 AM	33.0	3	4	36	0.0	0.0%	32.5
8/27/2024 4:00:00 AM	41.0	2	2	41	0.0	0.0%	37.5
8/27/2024 5:00:00 AM	35.0	2	2	35	0.0	0.0%	31.0
8/27/2024 6:00:00 AM	40.0	1	1	40	0.0	0.0%	40.0
8/27/2024 7:00:00 AM	48.0	10	12	54	51.0	25.0%	37.9
8/27/2024 8:00:00 AM	41.0	14	17	46	46.0	5.9%	36.3
8/27/2024 9:00:00 AM	47.0	27	32	54	49.5	18.8%	37.7
8/27/2024 10:00:00 AM	44.0	34	40	48	47.0	10.0%	35.8
8/27/2024 11:00:00 AM	42.0	22	26	49	47.0	11.5%	34.4
8/27/2024 12:00:00 PM	42.0	37	43	48	48.0	2.3%	37.0

Outgoing Summary
 Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

8/27/2024 1:00:00 PM	45.0	45	53	54	48.8	15.1%	40.0
8/27/2024 2:00:00 PM	44.0	41	48	49	48.0	10.4%	37.3
8/27/2024 3:00:00 PM	45.0	42	49	53	47.8	12.2%	38.0
8/27/2024 4:00:00 PM	42.0	44	52	48	47.0	7.7%	35.5
8/27/2024 5:00:00 PM	46.0	55	65	53	48.8	21.5%	38.1
8/27/2024 6:00:00 PM	44.0	66	78	55	49.3	9.0%	38.4
8/27/2024 7:00:00 PM	45.0	55	65	53	48.7	13.8%	37.8
8/27/2024 8:00:00 PM	45.0	49	58	50	47.6	8.6%	38.0
8/27/2024 9:00:00 PM	42.0	26	30	47	47.0	3.3%	36.8
8/27/2024 10:00:00 PM	44.0	63	74	49	47.5	5.4%	39.1
8/27/2024 11:00:00 PM	41.0	36	42	51	47.5	9.5%	38.6
8/28/2024 12:00:00 AM	43.0	8	10	52	50.5	20.0%	36.3
8/28/2024 1:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
8/28/2024 2:00:00 AM	30.0	1	1	30	0.0	0.0%	30.0
8/28/2024 3:00:00 AM	36.0	2	2	36	0.0	0.0%	35.5
8/28/2024 4:00:00 AM	**No Data**						
8/28/2024 5:00:00 AM	35.0	5	6	37	0.0	0.0%	29.8
8/28/2024 6:00:00 AM	38.0	2	2	38	0.0	0.0%	34.5
8/28/2024 7:00:00 AM	42.0	9	11	47	46.5	18.2%	38.5
8/28/2024 8:00:00 AM	44.0	14	16	49	49.0	6.3%	37.3
8/28/2024 9:00:00 AM	45.0	29	34	53	50.8	14.7%	37.4
8/28/2024 10:00:00 AM	44.0	30	35	53	48.6	14.3%	37.6
8/28/2024 11:00:00 AM	44.0	34	40	52	49.3	7.5%	36.6
8/28/2024 12:00:00 PM	44.0	30	35	46	46.0	2.9%	38.0
8/28/2024 1:00:00 PM	42.0	42	49	50	49.5	4.1%	37.4
8/28/2024 2:00:00 PM	43.0	36	42	58	50.3	9.5%	36.2
8/28/2024 3:00:00 PM	43.0	47	55	50	47.7	10.9%	37.7
8/28/2024 4:00:00 PM	45.0	63	74	59	49.9	13.5%	36.6
8/28/2024 5:00:00 PM	43.0	50	59	51	49.3	11.9%	36.6
8/28/2024 6:00:00 PM	45.0	63	74	52	48.6	13.5%	39.1
8/28/2024 7:00:00 PM	46.0	65	76	51	47.6	19.7%	40.1
8/28/2024 8:00:00 PM	45.0	46	54	52	47.7	13.0%	38.5
8/28/2024 9:00:00 PM	46.0	48	57	49	47.2	17.5%	38.2
8/28/2024 10:00:00 PM	44.0	42	50	50	47.8	10.0%	38.2
8/28/2024 11:00:00 PM	43.0	27	32	45	0.0	0.0%	37.9
8/29/2024 12:00:00 AM	45.0	14	16	54	53.0	12.5%	37.8
8/29/2024 1:00:00 AM	40.0	6	7	47	47.0	14.3%	34.3
8/29/2024 2:00:00 AM	53.0	2	2	53	53.0	50.0%	43.5
8/29/2024 3:00:00 AM	41.0	2	2	41	0.0	0.0%	33.0
8/29/2024 4:00:00 AM	**No Data**						
8/29/2024 5:00:00 AM	**No Data**						
8/29/2024 6:00:00 AM	36.0	2	2	36	0.0	0.0%	34.0
8/29/2024 7:00:00 AM	41.0	7	8	41	0.0	0.0%	34.5
8/29/2024 8:00:00 AM	45.0	14	17	49	47.0	17.6%	38.5
8/29/2024 9:00:00 AM	46.0	26	31	51	49.2	18.8%	37.4
8/29/2024 10:00:00 AM	42.0	42	50	51	48.5	4.0%	33.9
8/29/2024 11:00:00 AM	38.0	26	31	52	49.0	6.5%	33.8

Outgoing Summary
Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/24/2024 12:00:00 AM	45.0	507	597	58	48.4	14.6%	38.3
8/25/2024 12:00:00 AM	44.0	519	611	60	48.3	11.9%	37.5
8/26/2024 12:00:00 AM	45.0	547	643	79	48.9	13.5%	38.7
8/27/2024 12:00:00 AM	45.0	643	757	58	48.2	11.9%	37.6
8/28/2024 12:00:00 AM	44.0	688	809	55	48.4	10.9%	37.6
8/29/2024 12:00:00 AM	45.0	698	821	59	48.6	11.6%	37.8
8/29/2024 10:59:59 AM	45.0	128	150	53	48.7	10.0%	35.3

Outgoing Weekly Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	8	4	0	6	0
1 - 2	*	*	*	*	*	3	1	0	2	0
2 - 3	*	*	*	*	*	4	2	0	3	0
3 - 4	*	*	*	*	*	2	1	0	1.5	0
4 - 5	*	*	*	*	*	2	4	0	3	0
5 - 6	*	*	*	*	*	0	1	0	0.5	0
6 - 7	*	*	*	*	*	2	0	0	1	0
7 - 8	*	*	*	*	*	9	7	0	8	0
8 - 9	*	*	*	*	*	12	6	0	9	0
9 - 10	*	*	*	*	*	24	20	0	22	0
10 - 11	*	*	*	*	*	32	28	0	30	0
11 - 12	*	*	*	*	*	39	32	0	35.5	0
12 - 13	*	*	*	*	*	52	44	0	48	0
13 - 14	*	*	*	*	43	54	42	43	48	44.5
14 - 15	*	*	*	*	59	48	55	59	51.5	45.8
15 - 16	*	*	*	*	81	65	51	81	58	45
16 - 17	*	*	*	*	106	59	70	106	64.5	43.5
17 - 18	*	*	*	*	73	44	54	73	49	45.3
18 - 19	*	*	*	*	71	40	75	71	57.5	45.3
19 - 20	*	*	*	*	57	27	47	57	37	45
20 - 21	*	*	*	*	39	31	46	39	38.5	43.5
21 - 22	*	*	*	*	24	25	22	24	23.5	47
22 - 23	*	*	*	*	29	14	18	29	16	43
23 - 24	*	*	*	*	15	15	13	15	14	44
Totals	0	0	0	0	597	611	643			
% of Total	0%	0%	0%	0%	32.25%	33.01%	34.74%			

Outgoing Weekly Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	2	4	1	7	*	*	*	3.5	0	36.25
1 - 2	2	2	1	2	*	*	*	1.75	0	37
2 - 3	2	4	2	2	*	*	*	2.5	0	34.5
3 - 4	2	2	0	0	*	*	*	1	0	37.5
4 - 5	1	2	6	0	*	*	*	2.25	0	29.67
5 - 6	1	1	2	2	*	*	*	1.5	0	37
6 - 7	6	12	11	8	*	*	*	9.25	0	43.88
7 - 8	16	17	16	17	*	*	*	16.5	0	44.12
8 - 9	26	32	34	31	*	*	*	30.75	0	45.38
9 - 10	37	40	35	50	*	*	*	40.5	0	43.88
10 - 11	53	26	40	31	*	*	*	37.5	0	41.62
11 - 12	38	43	35	*	*	*	*	38.67	0	43.23
12 - 13	52	53	49	*	*	*	*	51.33	0	43.67
13 - 14	49	48	42	*	*	*	*	46.33	0	43.2
14 - 15	53	49	55	*	*	*	*	52.33	0	44.43
15 - 16	68	52	74	*	*	*	*	64.67	0	42.73
16 - 17	80	65	59	*	*	*	*	68	0	44.7
17 - 18	70	78	74	*	*	*	*	74	0	44.3
18 - 19	75	65	76	*	*	*	*	72	0	44.97
19 - 20	46	58	54	*	*	*	*	52.67	0	44.8
20 - 21	21	30	57	*	*	*	*	36	0	43.73
21 - 22	21	74	50	*	*	*	*	48.33	0	42.63
22 - 23	22	42	32	*	*	*	*	32	0	41.67
23 - 24	14	10	16	*	*	*	*	13.33	0	44.33
Totals	757	809	821	150	0	0	0			
% of Total	29.84%	31.89%	32.36%	5.91%	0%	0%	0%			

Outgoing Monthly Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	Aug 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	2	4	1	7	*	8	4	3.5	6	36.25
1 - 2	2	2	1	2	*	3	1	1.75	2	37
2 - 3	2	4	2	2	*	4	2	2.5	3	34.5
3 - 4	2	2	0	0	*	2	1	1	1.5	37.5
4 - 5	1	2	6	0	*	2	4	2.25	3	29.67
5 - 6	1	1	2	2	*	0	1	1.5	0.5	37
6 - 7	6	12	11	8	*	2	0	9.25	1	43.88
7 - 8	16	17	16	17	*	9	7	16.5	8	44.12
8 - 9	26	32	34	31	*	12	6	30.75	9	45.38
9 - 10	37	40	35	50	*	24	20	40.5	22	43.88
10 - 11	53	26	40	31	*	32	28	37.5	30	41.62
11 - 12	38	43	35	*	*	39	32	38.67	35.5	43.23
12 - 13	52	53	49	*	*	52	44	51.33	48	43.67
13 - 14	49	48	42	*	43	54	42	45.5	48	43.52
14 - 15	53	49	55	*	59	48	55	54	51.5	44.78
15 - 16	68	52	74	*	81	65	51	68.75	58	43.3
16 - 17	80	65	59	*	106	59	70	77.5	64.5	44.4
17 - 18	70	78	74	*	73	44	54	73.75	49	44.55
18 - 19	75	65	76	*	71	40	75	71.75	57.5	45.05
19 - 20	46	58	54	*	57	27	47	53.75	37	44.85
20 - 21	21	30	57	*	39	31	46	36.75	38.5	43.68
21 - 22	21	74	50	*	24	25	22	42.25	23.5	43.72
22 - 23	22	42	32	*	29	14	18	31.25	16	42
23 - 24	14	10	16	*	15	15	13	13.75	14	44.25
Totals	757	809	821	150	597	611	643			
% of Total	17.25%	18.44%	18.71%	3.42%	13.61%	13.92%	14.65%			

Outgoing Weekly Speeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	34.25	34.75	0	34.42	0
1 - 2	*	*	*	*	*	38.67	57	0	43.25	0
2 - 3	*	*	*	*	*	34.75	33.5	0	34.33	0
3 - 4	*	*	*	*	*	40.5	35	0	38.67	0
4 - 5	*	*	*	*	*	33.5	35.5	0	34.83	0
5 - 6	*	*	*	*	*	0	29	0	29	0
6 - 7	*	*	*	*	*	33	0	0	33	0
7 - 8	*	*	*	*	*	38.33	37.57	0	38	0
8 - 9	*	*	*	*	*	36.83	38	0	37.22	0
9 - 10	*	*	*	*	*	37.62	37.7	0	37.66	0
10 - 11	*	*	*	*	*	37.66	36.71	0	37.22	0
11 - 12	*	*	*	*	*	37.64	39.31	0	38.39	0
12 - 13	*	*	*	*	*	36.21	40.59	0	38.22	0
13 - 14	*	*	*	*	37.51	38.22	38.9	37.51	38.52	44.5
14 - 15	*	*	*	*	39.68	36.56	39.58	39.68	38.17	45.8
15 - 16	*	*	*	*	37.65	37.58	38.14	37.65	37.83	45
16 - 17	*	*	*	*	38.29	38.08	39.2	38.29	38.69	43.5
17 - 18	*	*	*	*	38.71	38.32	37.19	38.71	37.69	45.3
18 - 19	*	*	*	*	39.41	38.6	39.23	39.41	39.01	45.3
19 - 20	*	*	*	*	38.96	36.41	39.72	38.96	38.51	45
20 - 21	*	*	*	*	37.13	35	37.24	37.13	36.34	43.5
21 - 22	*	*	*	*	38.21	37.24	38.73	38.21	37.94	47
22 - 23	*	*	*	*	34.76	37.64	37.72	34.76	37.69	43
23 - 24	*	*	*	*	37.93	43.6	39.46	37.93	41.68	44
Totals	0	0	0	0	38.3	37.5	38.7			
% of Total	0%	0%	0%	0%	33.45%	32.75%	33.8%			

Outgoing Weekly Speeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	32	38.25	29	34.29	*	*	*	34.71	*	36.25
1 - 2	32.5	22	30	43.5	*	*	*	32.29	*	37
2 - 3	27.5	32.5	35.5	33	*	*	*	32.2	*	34.5
3 - 4	33	37.5	0	0	*	*	*	35.25	*	37.5
4 - 5	19	31	29.83	0	*	*	*	28.89	*	29.67
5 - 6	34	40	34.5	34	*	*	*	35.17	*	37
6 - 7	38.83	37.92	38.55	34.5	*	*	*	37.51	*	43.88
7 - 8	40.06	36.29	37.31	38.47	*	*	*	38.02	*	44.12
8 - 9	37.19	37.66	37.38	37.42	*	*	*	37.42	*	45.38
9 - 10	37.62	35.78	37.6	33.9	*	*	*	36.01	*	43.88
10 - 11	36.79	34.42	36.58	33.84	*	*	*	35.71	*	41.62
11 - 12	36.68	36.95	37.97	*	*	*	*	37.17	*	43.23
12 - 13	37.23	40.04	37.41	*	*	*	*	38.25	*	43.67
13 - 14	37.92	37.33	36.21	*	*	*	*	37.2	*	43.2
14 - 15	39.11	37.98	37.65	*	*	*	*	38.25	*	44.43
15 - 16	36.44	35.46	36.58	*	*	*	*	36.23	*	42.73
16 - 17	38.39	38.09	36.64	*	*	*	*	37.79	*	44.7
17 - 18	38.59	38.45	39.14	*	*	*	*	38.72	*	44.3
18 - 19	37.28	37.78	40.09	*	*	*	*	38.42	*	44.97
19 - 20	38.8	38.02	38.52	*	*	*	*	38.42	*	44.8
20 - 21	36.48	36.8	38.18	*	*	*	*	37.46	*	43.73
21 - 22	37.33	39.07	38.22	*	*	*	*	38.52	*	42.63
22 - 23	37.45	38.62	37.91	*	*	*	*	38.11	*	41.67
23 - 24	39	36.3	37.81	*	*	*	*	37.85	*	44.33
Totals	37.6	37.6	37.8	35.3	0	0	0			
% of Total	25.35%	25.35%	25.49%	23.8%	0%	0%	0%			

Outgoing Monthly Speeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	32	38.25	29	34.29	*	34.25	34.75	34.71	34.42	36.25
1 - 2	32.5	22	30	43.5	*	38.67	57	32.29	43.25	37
2 - 3	27.5	32.5	35.5	33	*	34.75	33.5	32.2	34.33	34.5
3 - 4	33	37.5	0	0	*	40.5	35	35.25	38.67	37.5
4 - 5	19	31	29.83	0	*	33.5	35.5	28.89	34.83	29.67
5 - 6	34	40	34.5	34	*	0	29	35.17	29	37
6 - 7	38.83	37.92	38.55	34.5	*	33	0	37.51	33	43.88
7 - 8	40.06	36.29	37.31	38.47	*	38.33	37.57	38.02	38	44.12
8 - 9	37.19	37.66	37.38	37.42	*	36.83	38	37.42	37.22	45.38
9 - 10	37.62	35.78	37.6	33.9	*	37.62	37.7	36.01	37.66	43.88
10 - 11	36.79	34.42	36.58	33.84	*	37.66	36.71	35.71	37.22	41.62
11 - 12	36.68	36.95	37.97	*	*	37.64	39.31	37.17	38.39	43.23
12 - 13	37.23	40.04	37.41	*	*	36.21	40.59	38.25	38.22	43.67
13 - 14	37.92	37.33	36.21	*	37.51	38.22	38.9	37.27	38.52	43.52
14 - 15	39.11	37.98	37.65	*	39.68	36.56	39.58	38.64	38.17	44.78
15 - 16	36.44	35.46	36.58	*	37.65	37.58	38.14	36.65	37.83	43.3
16 - 17	38.39	38.09	36.64	*	38.29	38.08	39.2	37.96	38.69	44.4
17 - 18	38.59	38.45	39.14	*	38.71	38.32	37.19	38.72	37.69	44.55
18 - 19	37.28	37.78	40.09	*	39.41	38.6	39.23	38.67	39.01	45.05
19 - 20	38.8	38.02	38.52	*	38.96	36.41	39.72	38.56	38.51	44.85
20 - 21	36.48	36.8	38.18	*	37.13	35	37.24	37.37	36.34	43.68
21 - 22	37.33	39.07	38.22	*	38.21	37.24	38.73	38.48	37.94	43.72
22 - 23	37.45	38.62	37.91	*	34.76	37.64	37.72	37.34	37.69	42
23 - 24	39	36.3	37.81	*	37.93	43.6	39.46	37.87	41.68	44.25
Totals	37.6	37.6	37.8	35.3	38.3	37.5	38.7			
% of Total	14.31%	14.31%	14.38%	13.43%	14.57%	14.27%	14.73%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	41	29	0	35	0
1 - 2	*	*	*	*	*	46	57	0	51.5	0
2 - 3	*	*	*	*	*	41	35	0	38	0
3 - 4	*	*	*	*	*	41	35	0	38	0
4 - 5	*	*	*	*	*	37	38	0	37.5	0
5 - 6	*	*	*	*	*	0	29	0	29	0
6 - 7	*	*	*	*	*	37	0	0	37	0
7 - 8	*	*	*	*	*	46	43	0	44.5	0
8 - 9	*	*	*	*	*	41.5	46.5	0	44	0
9 - 10	*	*	*	*	*	43	45.3	0	44.15	0
10 - 11	*	*	*	*	*	43.5	41	0	42.25	0
11 - 12	*	*	*	*	*	44.5	43.7	0	44.1	0
12 - 13	*	*	*	*	*	43.8	44.5	0	44.15	0
13 - 14	*	*	*	*	44.5	45	46	44.5	45.5	44.5
14 - 15	*	*	*	*	45.8	42	45.6	45.8	43.8	45.8
15 - 16	*	*	*	*	45	43.5	43.8	45	43.65	45
16 - 17	*	*	*	*	43.5	44.7	44.5	43.5	44.6	43.5
17 - 18	*	*	*	*	45.3	43	43	45.3	43	45.3
18 - 19	*	*	*	*	45.3	43.7	44.7	45.3	44.2	45.3
19 - 20	*	*	*	*	45	47	45.8	45	46.4	45
20 - 21	*	*	*	*	43.5	40	43.2	43.5	41.6	43.5
21 - 22	*	*	*	*	47	43	45.5	47	44.25	47
22 - 23	*	*	*	*	43	42	43	43	42.5	43
23 - 24	*	*	*	*	44	49	45	44	47	44
Totals	0	0	0	0	491.9	988.2	977.1			
% of Total	0%	0%	0%	0%	20.02%	40.22%	39.76%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	34	42	29	40	*	*	*	36.25	0	36.25
1 - 2	39	26	30	53	*	*	*	37	0	37
2 - 3	28	33	36	41	*	*	*	34.5	0	34.5
3 - 4	34	41	0	0	*	*	*	37.5	0	37.5
4 - 5	19	35	35	0	*	*	*	29.67	0	29.67
5 - 6	34	40	38	36	*	*	*	37	0	37
6 - 7	45	48	42	40.5	*	*	*	43.88	0	43.88
7 - 8	46.5	41	44	45	*	*	*	44.12	0	44.12
8 - 9	43.5	47	45	46	*	*	*	45.38	0	45.38
9 - 10	46	43.5	44	42	*	*	*	43.88	0	43.88
10 - 11	42.5	42	44	38	*	*	*	41.62	0	41.62
11 - 12	43.7	42	44	*	*	*	*	43.23	0	43.23
12 - 13	44	45	42	*	*	*	*	43.67	0	43.67
13 - 14	43.3	43.3	43	*	*	*	*	43.2	0	43.2
14 - 15	46	44.5	42.8	*	*	*	*	44.43	0	44.43
15 - 16	42	41.7	44.5	*	*	*	*	42.73	0	42.73
16 - 17	45.4	45.7	43	*	*	*	*	44.7	0	44.7
17 - 18	44.7	43.4	44.8	*	*	*	*	44.3	0	44.3
18 - 19	44.2	44.7	46	*	*	*	*	44.97	0	44.97
19 - 20	45.5	44.2	44.7	*	*	*	*	44.8	0	44.8
20 - 21	44	42	45.2	*	*	*	*	43.73	0	43.73
21 - 22	41	43.2	43.7	*	*	*	*	42.63	0	42.63
22 - 23	41.5	41	42.5	*	*	*	*	41.67	0	41.67
23 - 24	45	43	45	*	*	*	*	44.33	0	44.33
Totals	981.8	1002.2	958.2	381.5	0	0	0			
% of Total	29.54%	30.15%	28.83%	11.48%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	34	42	29	40	*	41	29	36.25	35	36.25
1 - 2	39	26	30	53	*	46	57	37	51.5	37
2 - 3	28	33	36	41	*	41	35	34.5	38	34.5
3 - 4	34	41	0	0	*	41	35	37.5	38	37.5
4 - 5	19	35	35	0	*	37	38	29.67	37.5	29.67
5 - 6	34	40	38	36	*	0	29	37	29	37
6 - 7	45	48	42	40.5	*	37	0	43.88	37	43.88
7 - 8	46.5	41	44	45	*	46	43	44.12	44.5	44.12
8 - 9	43.5	47	45	46	*	41.5	46.5	45.38	44	45.38
9 - 10	46	43.5	44	42	*	43	45.3	43.88	44.15	43.88
10 - 11	42.5	42	44	38	*	43.5	41	41.62	42.25	41.62
11 - 12	43.7	42	44	*	*	44.5	43.7	43.23	44.1	43.23
12 - 13	44	45	42	*	*	43.8	44.5	43.67	44.15	43.67
13 - 14	43.3	43.3	43	*	44.5	45	46	43.52	45.5	43.52
14 - 15	46	44.5	42.8	*	45.8	42	45.6	44.78	43.8	44.78
15 - 16	42	41.7	44.5	*	45	43.5	43.8	43.3	43.65	43.3
16 - 17	45.4	45.7	43	*	43.5	44.7	44.5	44.4	44.6	44.4
17 - 18	44.7	43.4	44.8	*	45.3	43	43	44.55	43	44.55
18 - 19	44.2	44.7	46	*	45.3	43.7	44.7	45.05	44.2	45.05
19 - 20	45.5	44.2	44.7	*	45	47	45.8	44.85	46.4	44.85
20 - 21	44	42	45.2	*	43.5	40	43.2	43.68	41.6	43.68
21 - 22	41	43.2	43.7	*	47	43	45.5	43.72	44.25	43.72
22 - 23	41.5	41	42.5	*	43	42	43	42	42.5	42
23 - 24	45	43	45	*	44	49	45	44.25	47	44.25

Summary of Violators

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	26	34.6	4	48.8
01:00:00	11	36.3	3	52.0
02:00:00	16	33.0	0	0.0
03:00:00	7	36.7	0	0.0
04:00:00	15	31.3	0	0.0
05:00:00	7	34.3	0	0.0
06:00:00	39	37.3	6	49.7
07:00:00	82	38.0	12	47.3
08:00:00	141	37.4	21	49.5
09:00:00	206	36.4	25	48.5
10:00:00	210	36.1	16	47.7
11:00:00	187	37.6	13	48.2
12:00:00	250	38.2	28	48.8
13:00:00	278	37.7	34	49.3
14:00:00	319	38.5	49	47.7
15:00:00	391	37.0	42	48.7
16:00:00	439	38.2	60	49.1
17:00:00	393	38.5	45	48.3
18:00:00	402	38.8	57	48.1
19:00:00	289	38.6	47	48.1
20:00:00	224	37.0	23	47.5
21:00:00	216	38.4	25	48.1
22:00:00	157	37.4	9	46.9
23:00:00	83	39.2	16	50.4

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/23/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	13:00	0	0	3	5	7	9	12	4	3	0	0	0	0	0	0	0	0	0	43	37.5	44.5	33 to 43	53.5	5	11.6	0	41	2	
8/23/2024	14:00	0	0	0	5	7	15	19	12	1	0	0	0	0	0	0	0	0	0	59	39.7	45.8	36 to 46	64.4	12	20.3	0	51	8	
8/23/2024	15:00	0	1	4	7	16	17	23	9	4	0	0	0	0	0	0	0	0	0	81	37.7	45	33 to 43	58.0	12	14.8	0	77	4	
8/23/2024	16:00	0	0	0	11	16	28	38	10	2	1	0	0	0	0	0	0	0	0	106	38.3	43.5	33 to 43	65.1	10	9.4	0	102	4	
8/23/2024	17:00	0	0	2	5	11	20	19	13	3	0	0	0	0	0	0	0	0	0	73	38.7	45.3	36 to 46	61.6	12	16.4	0	70	3	
8/23/2024	18:00	0	0	4	4	6	18	22	13	3	1	0	0	0	0	0	0	0	0	71	39.4	45.3	35 to 45	63.4	12	16.9	2	64	5	
8/23/2024	19:00	0	1	1	4	8	11	21	9	1	1	0	0	0	0	0	0	0	0	57	39	45	34 to 44	61.4	9	15.8	0	55	2	
8/23/2024	20:00	0	0	2	1	11	12	8	4	1	0	0	0	0	0	0	0	0	0	39	37.1	43.5	32 to 42	69.2	3	7.7	0	38	1	
8/23/2024	21:00	0	0	3	3	4	0	6	6	2	0	0	0	0	0	0	0	0	0	24	38.2	47	40 to 50	54.2	7	29.2	0	24	0	
8/23/2024	22:00	0	0	5	2	8	5	5	4	0	0	0	0	0	0	0	0	0	0	29	34.8	43	31 to 41	51.7	3	10.3	0	28	1	
8/23/2024	23:00	0	0	0	1	4	5	3	2	0	0	0	0	0	0	0	0	0	0	15	37.9	44	31 to 41	66.7	2	13.3	0	15	0	
24 Hr Summary		0	2	24	48	98	140	176	86	20	3	0	0	0	0	0	0	0	0	597	38.3	45	36 to 46	57.6	87	14.6	2	565	30	

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/24/2024	00:00	0	0	0	3	2	1	1	1	0	0	0	0	0	0	0	0	0	0	8	34.3	41	26 to 36	75.0	1	12.5	0	8	0	
8/24/2024	01:00	0	0	0	1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	3	38.7	46	36 to 46	66.7	1	33.3	0	3	0	
8/24/2024	02:00	0	0	1	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	4	34.8	41	32 to 42	75.0	*	*	0	4	0	
8/24/2024	03:00	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	40.5	41	31 to 41	100.0	*	*	0	2	0	
8/24/2024	04:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	33.5	37	27 to 37	100.0	*	*	0	1	1	
8/24/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	06:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	33	37	27 to 37	100.0	*	*	0	1	1	
8/24/2024	07:00	0	0	0	2	1	2	0	4	0	0	0	0	0	0	0	0	0	0	9	38.3	46	37 to 47	66.7	2	22.2	0	9	0	
8/24/2024	08:00	0	0	0	1	3	4	4	0	0	0	0	0	0	0	0	0	0	0	12	36.8	41.5	32 to 42	75.0	*	*	0	12	0	
8/24/2024	09:00	0	0	0	2	6	7	5	4	0	0	0	0	0	0	0	0	0	0	24	37.6	43	33 to 43	62.5	3	12.5	0	24	0	
8/24/2024	10:00	0	0	0	4	5	9	11	3	0	0	0	0	0	0	0	0	0	0	32	37.7	43.5	34 to 44	65.6	3	9.4	0	31	1	
8/24/2024	11:00	0	0	3	4	4	9	12	7	0	0	0	0	0	0	0	0	0	0	39	37.6	44.5	36 to 46	61.5	5	12.8	0	39	0	
8/24/2024	12:00	0	0	1	12	9	12	11	6	1	0	0	0	0	0	0	0	0	0	52	36.2	43.8	28 to 38	51.9	6	11.5	0	49	3	
8/24/2024	13:00	0	0	1	5	12	12	15	6	1	2	0	0	0	0	0	0	0	0	54	38.2	45	32 to 42	57.4	8	14.8	0	54	0	
8/24/2024	14:00	0	0	2	5	10	14	13	4	0	0	0	0	0	0	0	0	0	0	48	36.6	42	32 to 42	70.8	4	8.3	0	44	4	
8/24/2024	15:00	0	0	4	4	9	19	21	8	0	0	0	0	0	0	0	0	0	0	65	37.6	43.5	35 to 45	66.2	5	7.7	0	63	2	
8/24/2024	16:00	0	0	2	7	8	15	16	10	1	0	0	0	0	0	0	0	0	0	59	38.1	44.7	32 to 42	61.0	8	13.6	0	57	2	
8/24/2024	17:00	0	0	1	4	5	12	16	5	1	0	0	0	0	0	0	0	0	0	44	38.3	43	35 to 45	70.5	3	6.8	0	44	0	
8/24/2024	18:00	0	0	0	6	5	7	17	4	1	0	0	0	0	0	0	0	0	0	40	38.6	43.7	37 to 47	65.0	4	10.0	0	39	1	
8/24/2024	19:00	0	0	2	6	5	3	5	3	2	1	0	0	0	0	0	0	0	0	27	36.4	47	27 to 37	48.1	6	22.2	0	26	1	
8/24/2024	20:00	0	0	0	7	9	7	5	3	0	0	0	0	0	0	0	0	0	0	31	35	40	25 to 35	64.5	3	9.7	0	30	1	
8/24/2024	21:00	0	0	0	3	8	4	6	3	0	1	0	0	0	0	0	0	0	0	25	37.2	43	33 to 43	60.0	3	12.0	0	24	1	
8/24/2024	22:00	0	0	0	0	4	6	2	2	0	0	0	0	0	0	0	0	0	0	14	37.6	42	30 to 40	78.6	2	14.3	0	14	0	
8/24/2024	23:00	0	0	0	0	2	3	4	4	1	0	1	0	0	0	0	0	0	0	15	43.6	49	39 to 49	66.7	6	40.0	0	15	0	
24 Hr Summary		0	0	17	77	109	148	169	78	8	4	1	0	0	0	0	0	0	0	611	37.5	44	32 to 42	56.3	73	11.9	0	593	18	

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/25/2024	00:00	0	0	0	3	0	0	0	0	1	0	0	0	0	0	0	0	0	0	4	34.8	29	19 to 29	75.0	1	25.0	0	4	0	
8/25/2024	01:00	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	57	57	47 to 57	100.0	1	100.0	0	1	0	
8/25/2024	02:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	33.5	35	25 to 35	100.0	*	*	0	2	0	
8/25/2024	03:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	1	0	
8/25/2024	04:00	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	4	35.5	38	33 to 43	75.0	*	*	0	4	0	
8/25/2024	05:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0	
8/25/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	07:00	0	0	0	1	1	1	4	0	0	0	0	0	0	0	0	0	0	0	7	37.6	43	34 to 44	71.4	*	*	0	7	0	
8/25/2024	08:00	0	0	0	1	2	0	1	2	0	0	0	0	0	0	0	0	0	0	6	38	46.5	24 to 34	50.0	2	33.3	0	6	0	
8/25/2024	09:00	0	0	0	4	4	2	5	4	1	0	0	0	0	0	0	0	0	0	20	37.7	45.3	36 to 46	50.0	4	20.0	0	18	2	
8/25/2024	10:00	0	0	2	3	3	8	10	1	1	0	0	0	0	0	0	0	0	0	28	36.7	41	32 to 42	71.4	2	7.1	0	28	0	
8/25/2024	11:00	0	0	0	4	3	7	15	1	2	0	0	0	0	0	0	0	0	0	32	39.3	43.7	34 to 44	71.9	3	9.4	0	32	0	
8/25/2024	12:00	0	0	0	1	8	8	19	6	0	2	0	0	0	0	0	0	0	0	44	40.6	44.5	34 to 44	68.2	6	13.6	0	42	2	
8/25/2024	13:00	0	0	0	3	11	8	8	8	4	0	0	0	0	0	0	0	0	0	42	38.9	46	31 to 41	54.8	8	19.0	0	40	2	
8/25/2024	14:00	0	0	1	3	6	14	18	10	3	0	0	0	0	0	0	0	0	0	55	39.6	45.6	36 to 46	65.5	12	21.8	1	54	0	
8/25/2024	15:00	0	0	0	9	8	8	19	5	2	0	0	0	0	0	0	0	0	0	51	38.1	43.8	34 to 44	58.8	6	11.8	0	50	1	
8/25/2024	16:00	0	0	0	6	11	16	24	11	1	0	0	0	0	1	0	0	0	0	70	39.2	44.5	35 to 45	65.7	7	10.0	0	69	1	
8/25/2024	17:00	0	0	1	5	12	15	16	4	1	0	0	0	0	0	0	0	0	0	54	37.2	43	34 to 44	61.1	4	7.4	0	54	0	
8/25/2024	18:00	0	0	0	5	14	17	23	13	3	0	0	0	0	0	0	0	0	0	75	39.2	44.7	36 to 46	65.3	9	12.0	0	72	3	
8/25/2024	19:00	0	0	0	3	11	6	14	10	3	0	0	0	0	0	0	0	0	0	47	39.7	45.8	37 to 47	55.3	12	25.5	0	46	1	
8/25/2024	20:00	0	0	0	9	6	13	14	3	1	0	0	0	0	0	0	0	0	0	46	37.2	43.2	34 to 44	60.9	4	8.7	0	46	0	
8/25/2024	21:00	0	0	0	2	3	7	6	4	0	0	0	0	0	0	0	0	0	0	22	38.7	45.5	36 to 46	68.2	4	18.2	0	22	0	
8/25/2024	22:00	0	0	0	4	2	1	9	2	0	0	0	0	0	0	0	0	0	0	18	37.7	43	35 to 45	66.7	*	*	0	18	0	
8/25/2024	23:00	0	0	0	1	3	3	2	3	1	0	0	0	0	0	0	0	0	0	13	39.5	45	27 to 37	53.8	2	15.4	0	12	1	
24 Hr Summary		0	0	4	69	109	138	208	87	24	3	0	0	0	1	0	0	0	0	643	38.7	45	36 to 46	59.4	87	13.5	1	629	13	

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/26/2024	00:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	32	34	24 to 34	100.0	*	*	0	2	0
8/26/2024	01:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	32.5	39	16 to 26	50.0	*	*	0	2	0
8/26/2024	02:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27.5	28	18 to 28	100.0	*	*	0	2	0
8/26/2024	03:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	33	34	24 to 34	100.0	*	*	0	1	1
8/26/2024	04:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	19	19	9 to 19	100.0	*	*	0	1	0
8/26/2024	05:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	34	34	24 to 34	100.0	*	*	0	1	0
8/26/2024	06:00	0	0	0	2	0	0	2	1	1	0	0	0	0	0	0	0	0	0	6	38.8	45	35 to 45	50.0	1	16.7	0	6	0
8/26/2024	07:00	0	0	0	0	6	1	3	5	1	0	0	0	0	0	0	0	0	0	16	40.1	46.5	31 to 41	56.3	5	31.3	0	16	0
8/26/2024	08:00	0	0	2	3	4	6	8	2	1	0	0	0	0	0	0	0	0	0	26	37.2	43.5	34 to 44	57.7	2	7.7	0	23	3
8/26/2024	09:00	0	0	1	5	11	5	5	5	5	0	0	0	0	0	0	0	0	0	37	37.6	46	26 to 36	51.4	7	18.9	0	33	4
8/26/2024	10:00	0	0	1	7	10	15	16	4	0	0	0	0	0	0	0	0	0	0	53	36.8	42.5	34 to 44	66.0	3	5.7	0	47	6
8/26/2024	11:00	0	1	3	1	9	6	13	5	0	0	0	0	0	0	0	0	0	0	38	36.7	43.7	34 to 44	55.3	3	7.9	0	35	3
8/26/2024	12:00	0	1	3	4	6	17	13	8	0	0	0	0	0	0	0	0	0	0	52	37.2	44	36 to 46	65.4	6	11.5	0	46	6
8/26/2024	13:00	0	0	1	2	11	13	17	5	0	0	0	0	0	0	0	0	0	0	49	37.9	43.3	34 to 44	67.3	4	8.2	1	44	4
8/26/2024	14:00	0	0	0	2	12	13	13	11	2	0	0	0	0	0	0	0	0	0	53	39.1	46	31 to 41	58.5	9	17.0	0	48	5
8/26/2024	15:00	0	0	2	10	12	23	13	6	1	1	0	0	0	0	0	0	0	0	68	36.4	42	32 to 42	60.3	5	7.4	0	66	2
8/26/2024	16:00	0	0	2	9	13	19	19	13	4	1	0	0	0	0	0	0	0	0	80	38.4	45.4	36 to 46	55.0	14	17.5	0	76	4
8/26/2024	17:00	0	0	0	6	11	20	21	11	1	0	0	0	0	0	0	0	0	0	70	38.6	44.7	35 to 45	62.9	9	12.9	0	69	1
8/26/2024	18:00	0	0	2	9	15	17	20	11	1	0	0	0	0	0	0	0	0	0	75	37.3	44.2	37 to 47	61.3	8	10.7	0	71	4
8/26/2024	19:00	0	1	1	4	5	10	15	9	1	0	0	0	0	0	0	0	0	0	46	38.8	45.5	35 to 45	58.7	8	17.4	0	46	0
8/26/2024	20:00	0	1	0	4	3	4	6	3	0	0	0	0	0	0	0	0	0	0	21	36.5	44	36 to 46	61.9	2	9.5	0	21	0
8/26/2024	21:00	0	0	0	2	4	10	2	3	0	0	0	0	0	0	0	0	0	0	21	37.3	41	29 to 39	76.2	2	9.5	0	19	2
8/26/2024	22:00	0	0	1	1	2	7	11	0	0	0	0	0	0	0	0	0	0	0	22	37.5	41.5	33 to 43	86.4	*	*	0	22	0
8/26/2024	23:00	0	0	0	1	2	3	4	3	1	0	0	0	0	0	0	0	0	0	14	39	45	35 to 45	64.3	2	14.3	0	14	0
24 Hr Summary		0	5	19	75	141	190	201	105	19	2	0	0	0	0	0	0	0	0	757	37.6	45	35 to 45	56.4	90	11.9	1	711	45

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/27/2024	00:00	0	0	0	1	0	1	1	1	0	0	0	0	0	0	0	0	0	0	4	38.3	42	25 to 35	50.0	1	25.0	0	4	0
8/27/2024	01:00	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	22	26	16 to 26	100.0	*	*	0	2	0
8/27/2024	02:00	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	4	32.5	33	26 to 36	100.0	*	*	0	4	0
8/27/2024	03:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	37.5	41	31 to 41	100.0	*	*	0	2	0
8/27/2024	04:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	31	35	25 to 35	100.0	*	*	0	2	0
8/27/2024	05:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	40	40	30 to 40	100.0	*	*	0	1	0
8/27/2024	06:00	0	0	1	1	2	4	1	1	2	0	0	0	0	0	0	0	0	0	12	37.9	48	28 to 38	58.3	3	25.0	0	11	1
8/27/2024	07:00	0	0	2	1	1	7	5	1	0	0	0	0	0	0	0	0	0	0	17	36.3	41	34 to 44	70.6	1	5.9	0	16	1
8/27/2024	08:00	0	0	0	2	11	9	3	5	2	0	0	0	0	0	0	0	0	0	32	37.7	47	27 to 37	62.5	6	18.8	0	29	3
8/27/2024	09:00	0	0	0	7	13	10	5	5	0	0	0	0	0	0	0	0	0	0	40	35.8	43.5	27 to 37	67.5	4	10.0	0	34	6
8/27/2024	10:00	0	1	2	4	7	4	5	3	0	0	0	0	0	0	0	0	0	0	26	34.4	42	32 to 42	53.8	3	11.5	0	22	4
8/27/2024	11:00	0	0	0	5	8	16	8	6	0	0	0	0	0	0	0	0	0	0	43	37	42	31 to 41	69.8	1	2.3	1	36	6
8/27/2024	12:00	0	0	2	2	4	15	21	6	3	0	0	0	0	0	0	0	0	0	53	40	45	34 to 44	71.7	8	15.1	0	50	3
8/27/2024	13:00	0	0	0	7	10	13	13	5	0	0	0	0	0	0	0	0	0	0	48	37.3	43.3	29 to 39	54.2	5	10.4	0	45	3
8/27/2024	14:00	0	0	0	7	6	17	11	7	1	0	0	0	0	0	0	0	0	0	49	38	44.5	33 to 43	65.3	6	12.2	1	45	3
8/27/2024	15:00	0	0	3	8	12	13	11	5	0	0	0	0	0	0	0	0	0	0	52	35.5	41.7	30 to 40	55.8	4	7.7	0	48	4
8/27/2024	16:00	0	0	4	8	10	14	14	9	6	0	0	0	0	0	0	0	0	0	65	38.1	45.7	36 to 46	50.8	14	21.5	1	59	5
8/27/2024	17:00	0	0	1	3	12	30	23	6	2	1	0	0	0	0	0	0	0	0	78	38.4	43.4	34 to 44	71.8	7	9.0	0	77	1
8/27/2024	18:00	0	0	2	7	12	16	16	10	2	0	0	0	0	0	0	0	0	0	65	37.8	44.7	35 to 45	53.8	9	13.8	0	60	5
8/27/2024	19:00	0	0	0	4	14	11	19	8	2	0	0	0	0	0	0	0	0	0	58	38	44.2	36 to 46	62.1	5	8.6	0	57	1
8/27/2024	20:00	0	0	1	3	5	8	10	3	0	0	0	0	0	0	0	0	0	0	30	36.8	42	31 to 41	66.7	1	3.3	0	29	1
8/27/2024	21:00	0	0	0	5	7	25	30	7	0	0	0	0	0	0	0	0	0	0	74	39.1	43.2	36 to 46	81.1	4	5.4	0	72	2
8/27/2024	22:00	0	0	0	2	5	14	16	4	1	0	0	0	0	0	0	0	0	0	42	38.6	41	31 to 41	78.6	4	9.5	0	42	0
8/27/2024	23:00	0	0	1	3	0	2	2	1	1	0	0	0	0	0	0	0	0	0	10	36.3	43	18 to 28	40.0	2	20.0	0	10	0
24 Hr Summary		0	2	19	83	142	231	216	93	22	1	0	0	0	0	0	0	0	0	809	37.6	44	34 to 44	58.7	88	10.9	3	757	49

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/28/2024	00:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0	
8/28/2024	01:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	30	30	20 to 30	100.0	*	*	0	1	0	
8/28/2024	02:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	35.5	36	26 to 36	100.0	*	*	0	0	2	
8/28/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	04:00	0	0	1	2	1	2	0	0	0	0	0	0	0	0	0	0	0	0	6	29.8	35	25 to 35	66.7	*	*	1	4	1	
8/28/2024	05:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34.5	38	28 to 38	100.0	*	*	0	2	0	
8/28/2024	06:00	0	0	0	1	1	4	3	2	0	0	0	0	0	0	0	0	0	0	11	38.5	42	32 to 42	72.7	2	18.2	0	10	1	
8/28/2024	07:00	0	0	1	2	1	4	6	2	0	0	0	0	0	0	0	0	0	0	16	37.3	44	35 to 45	68.8	1	6.3	0	16	0	
8/28/2024	08:00	0	0	0	4	10	9	5	2	4	0	0	0	0	0	0	0	0	0	34	37.4	45	30 to 40	64.7	5	14.7	0	32	2	
8/28/2024	09:00	0	0	0	5	7	10	8	4	1	0	0	0	0	0	0	0	0	0	35	37.6	44	32 to 42	57.1	5	14.3	0	34	1	
8/28/2024	10:00	0	0	1	5	13	5	10	5	1	0	0	0	0	0	0	0	0	0	40	36.6	44	27 to 37	52.5	3	7.5	0	36	4	
8/28/2024	11:00	0	0	0	3	6	12	9	5	0	0	0	0	0	0	0	0	0	0	35	38	44	35 to 45	71.4	1	2.9	0	33	2	
8/28/2024	12:00	0	0	0	5	9	17	15	2	1	0	0	0	0	0	0	0	0	0	49	37.4	42	32 to 42	71.4	2	4.1	0	43	6	
8/28/2024	13:00	0	0	3	4	12	10	8	4	0	1	0	0	0	0	0	0	0	0	42	36.2	43	31 to 41	64.3	4	9.5	0	36	6	
8/28/2024	14:00	0	0	1	4	12	14	18	4	2	0	0	0	0	0	0	0	0	0	55	37.7	42.8	33 to 43	69.1	6	10.9	0	50	5	
8/28/2024	15:00	0	1	5	7	11	27	11	8	3	1	0	0	0	0	0	0	0	0	74	36.6	44.5	30 to 40	56.8	10	13.5	0	68	6	
8/28/2024	16:00	0	0	1	12	8	16	15	2	5	0	0	0	0	0	0	0	0	0	59	36.6	43	34 to 44	55.9	7	11.9	0	54	5	
8/28/2024	17:00	1	0	2	2	9	20	25	12	3	0	0	0	0	0	0	0	0	0	74	39.1	44.8	35 to 45	67.6	10	13.5	0	71	3	
8/28/2024	18:00	0	0	2	4	12	9	23	24	2	0	0	0	0	0	0	0	0	0	76	40.1	46	38 to 48	67.1	15	19.7	0	71	5	
8/28/2024	19:00	0	0	0	3	11	17	13	9	1	0	0	0	0	0	0	0	0	0	54	38.5	44.7	35 to 45	61.1	7	13.0	0	53	1	
8/28/2024	20:00	0	1	2	3	9	14	16	12	0	0	0	0	0	0	0	0	0	0	57	38.2	45.2	32 to 42	56.1	10	17.5	0	54	3	
8/28/2024	21:00	0	0	0	5	9	14	15	5	2	0	0	0	0	0	0	0	0	0	50	38.2	43.7	33 to 43	64.0	5	10.0	0	48	2	
8/28/2024	22:00	0	0	0	3	5	9	12	3	0	0	0	0	0	0	0	0	0	0	32	37.9	42.5	35 to 45	75.0	*	*	0	32	0	
8/28/2024	23:00	0	0	1	0	5	4	3	1	2	0	0	0	0	0	0	0	0	0	16	37.8	45	28 to 38	56.3	2	12.5	0	15	1	
24 Hr Summary		1	2	20	75	153	220	215	106	27	2	0	0	0	0	0	0	0	0	821	37.8	45	35 to 45	57.9	95	11.6	1	764	56	

Outgoing Histogram

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

from Fri-Aug-23-2024-01-00-PM to Thu-Aug-29-2024-10-59-AM

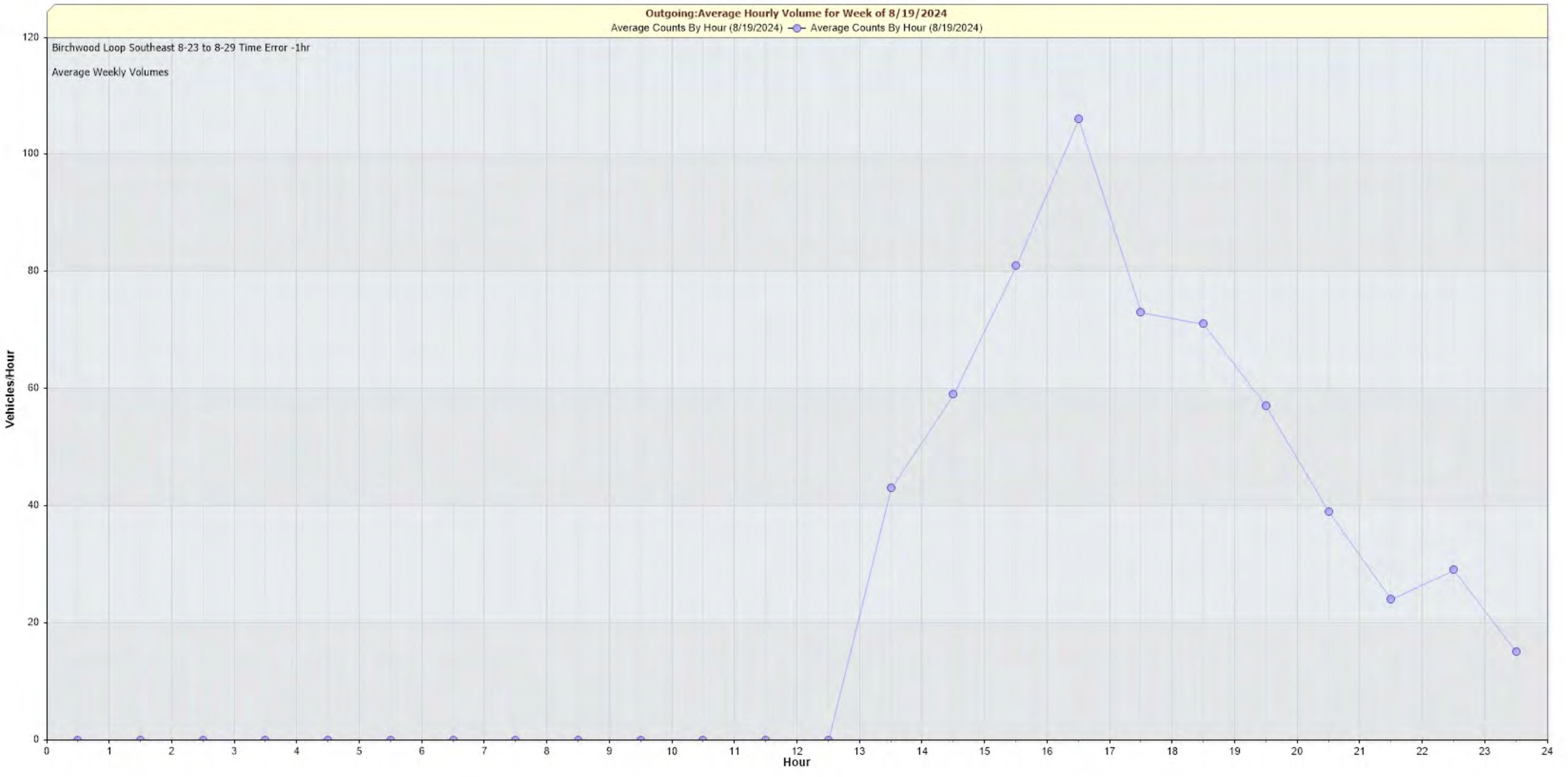
Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	0	0	0	3	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	7	34.3	40	22 to 32	57.1	1	14.3	0	7	0
8/29/2024	01:00	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	2	43.5	53	24 to 34	50.0	1	50.0	0	1	1
8/29/2024	02:00	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	33	41	15 to 25	50.0	*	*	0	1	1
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	34	36	26 to 36	100.0	*	*	0	2	0
8/29/2024	06:00	0	0	1	1	1	3	2	0	0	0	0	0	0	0	0	0	0	0	0	8	34.5	40.5	31 to 41	75.0	*	*	0	7	1
8/29/2024	07:00	0	0	1	2	2	4	3	5	0	0	0	0	0	0	0	0	0	0	0	17	38.5	45	36 to 46	64.7	3	17.6	0	17	0
8/29/2024	08:00	0	0	1	4	10	4	4	5	3	0	0	0	0	0	0	0	0	0	0	31	37.4	46	26 to 36	51.6	6	19.4	0	30	1
8/29/2024	09:00	0	1	5	5	20	6	6	6	1	0	0	0	0	0	0	0	0	0	0	50	33.9	42	30 to 40	58.0	2	4.0	0	42	8
8/29/2024	10:00	1	0	0	7	9	9	3	1	1	0	0	0	0	0	0	0	0	0	0	31	33.8	38	28 to 38	74.2	2	6.5	0	29	2
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		1	1	8	23	45	28	20	18	6	0	0	0	0	0	0	0	0	0	0	150	35.3	45	28 to 38	55.3	15	10.0	0	136	14

Outgoing: Average Hourly Volume for Week of 8/19/2024

Average Counts By Hour (8/19/2024) — Average Counts By Hour (8/19/2024)

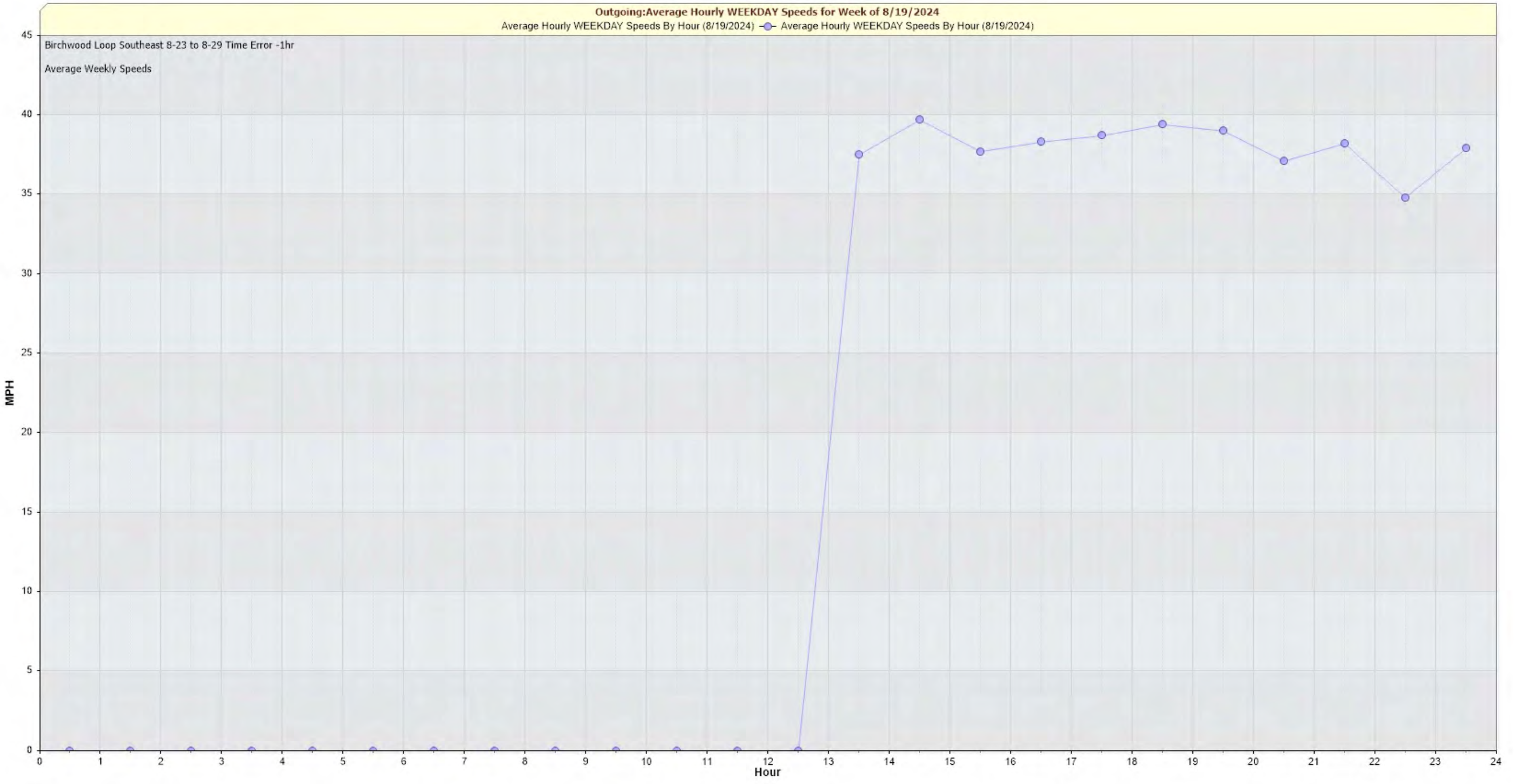
Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 8/19/2024
Average Hourly WEEKDAY Speeds By Hour (8/19/2024)

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr
Average Weekly Speeds

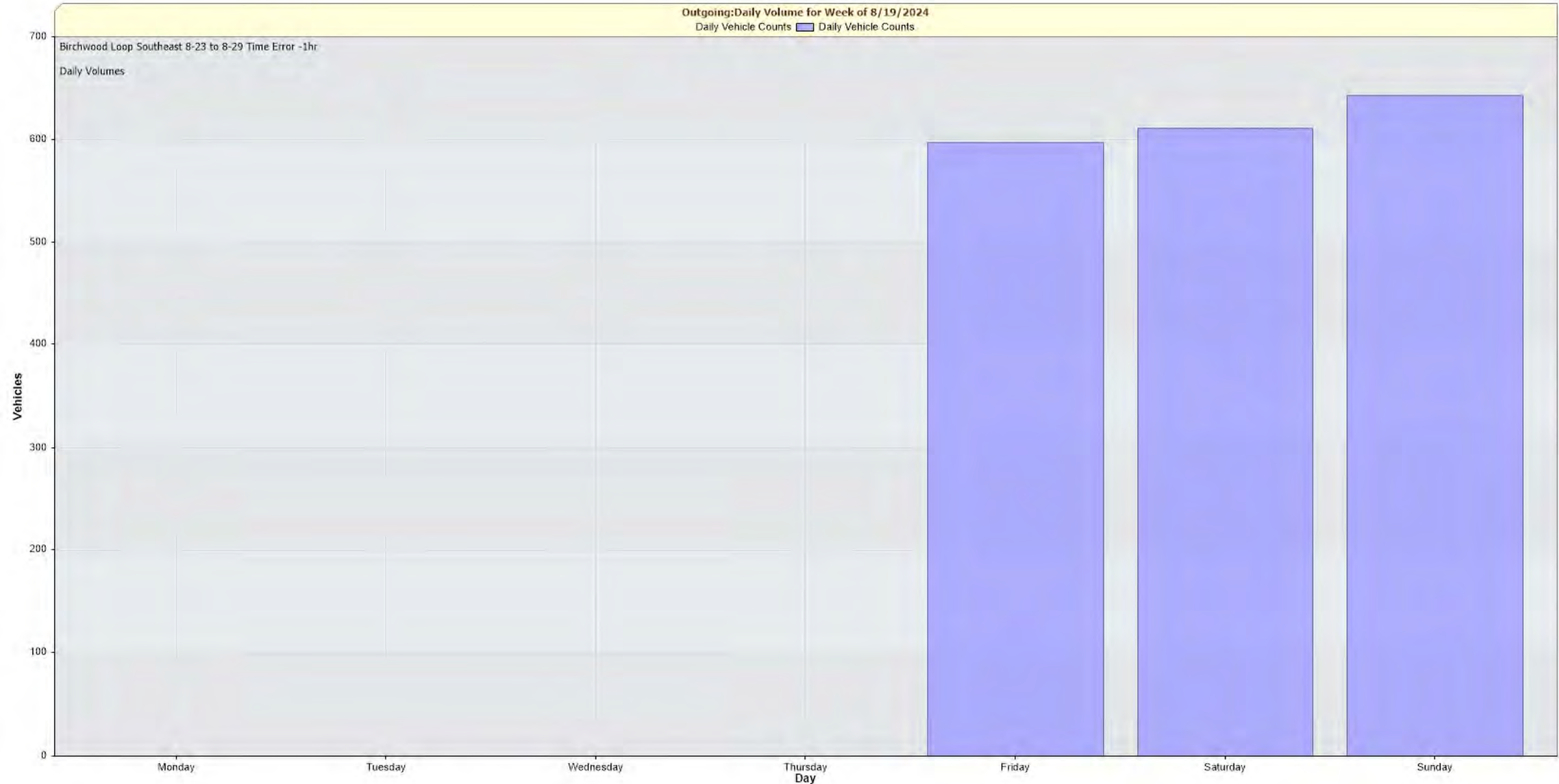


Outgoing:Daily Volume for Week of 8/19/2024

Daily Vehicle Counts

Birchwood Loop Southeast 8-23 to 8-29 Time Error -1hr

Daily Volumes



For Project: Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:27:14 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 8/23/2024 3:00:00 PM through 8/29/2024 12:59:59 PM

85th Percentile Speed 35 MPH

85th Percentile Vehicles 1085

Max Speed 56 MPH on 8/24/2024 3:46:20 PM

Total Vehicles 1276

AADT: 215

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	179	182
AM Peak 11:00 AM	15	13
PM Peak 5:00 PM	23	21

Speed

Speed Limit: 45

85th Percentile Speed: 35

50th Percentile Speed: 29

10 MPH Pace Interval: 25.0 MPH to 35.0 MPH

Average Speed: 28.82

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	1	0	0	4	1
% over limit	0.0	0.0	0.5	0.0	0.0	1.9	0.6
Avg Speeder	0.0	0.0	54.0	0.0	0.0	51.3	50.0
Avg Speed	29.0	29.0	28.8	27.8	29.7	28.6	28.3

Class Counts

	Number	%
VEH_SM	2	0.2
VEH_MED	1233	96.6
VEH_LG	41	3.2
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Incoming Summary
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/23/2024 4:00:00 PM	37.0	14	16	37	0.0	0.0%	27.7
8/23/2024 5:00:00 PM	34.0	14	17	40	0.0	0.0%	27.3
8/23/2024 6:00:00 PM	35.0	21	25	39	0.0	0.0%	29.0
8/23/2024 7:00:00 PM	37.0	20	24	42	0.0	0.0%	30.4
8/23/2024 8:00:00 PM	34.0	14	17	37	0.0	0.0%	30.8
8/23/2024 9:00:00 PM	37.0	18	21	43	0.0	0.0%	30.0
8/23/2024 10:00:00 PM	35.0	14	17	41	0.0	0.0%	32.0
8/23/2024 11:00:00 PM	36.0	14	16	40	0.0	0.0%	30.5
8/24/2024 12:00:00 AM	34.0	6	7	37	0.0	0.0%	29.7
8/24/2024 1:00:00 AM	33.0	16	19	37	0.0	0.0%	27.7
8/24/2024 2:00:00 AM	30.0	3	3	30	0.0	0.0%	26.0
8/24/2024 3:00:00 AM	34.0	3	3	34	0.0	0.0%	29.3
8/24/2024 4:00:00 AM	34.0	2	2	34	0.0	0.0%	31.0
8/24/2024 5:00:00 AM	32.0	2	2	32	0.0	0.0%	29.5
8/24/2024 6:00:00 AM	**No Data**						
8/24/2024 7:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
8/24/2024 8:00:00 AM	**No Data**						
8/24/2024 9:00:00 AM	**No Data**						
8/24/2024 10:00:00 AM	27.0	3	3	27	0.0	0.0%	22.7
8/24/2024 11:00:00 AM	25.0	2	2	25	0.0	0.0%	23.5
8/24/2024 12:00:00 PM	33.0	8	9	37	0.0	0.0%	29.1
8/24/2024 1:00:00 PM	34.0	13	15	36	0.0	0.0%	29.9
8/24/2024 2:00:00 PM	35.0	12	14	36	0.0	0.0%	27.1
8/24/2024 3:00:00 PM	34.0	15	18	36	0.0	0.0%	29.2
8/24/2024 4:00:00 PM	34.0	16	19	56	51.7	15.8%	31.4
8/24/2024 5:00:00 PM	37.0	13	15	40	0.0	0.0%	28.9
8/24/2024 6:00:00 PM	33.0	14	16	39	0.0	0.0%	28.7
8/24/2024 7:00:00 PM	34.0	11	13	35	0.0	0.0%	27.0
8/24/2024 8:00:00 PM	35.0	7	8	35	0.0	0.0%	27.0
8/24/2024 9:00:00 PM	31.0	8	10	34	0.0	0.0%	28.5
8/24/2024 10:00:00 PM	35.0	13	15	40	0.0	0.0%	28.0
8/24/2024 11:00:00 PM	35.0	13	15	37	0.0	0.0%	29.7
8/25/2024 12:00:00 AM	31.0	8	10	50	50.0	10.0%	28.2
8/25/2024 1:00:00 AM	32.0	3	4	42	0.0	0.0%	31.3
8/25/2024 2:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
8/25/2024 3:00:00 AM	33.0	4	5	40	0.0	0.0%	30.8
8/25/2024 4:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
8/25/2024 5:00:00 AM	**No Data**						
8/25/2024 6:00:00 AM	**No Data**						
8/25/2024 7:00:00 AM	21.0	2	2	21	0.0	0.0%	21.0
8/25/2024 8:00:00 AM	**No Data**						
8/25/2024 9:00:00 AM	**No Data**						
8/25/2024 10:00:00 AM	30.0	2	2	30	0.0	0.0%	27.5
8/25/2024 11:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
8/25/2024 12:00:00 PM	29.0	8	9	31	0.0	0.0%	20.7
8/25/2024 1:00:00 PM	32.0	10	12	43	0.0	0.0%	28.2
8/25/2024 2:00:00 PM	38.0	6	7	50	50.0	14.3%	35.0

Incoming Summary
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

8/25/2024 3:00:00 PM	32.0	7	8	37	0.0	0.0%	26.4
8/25/2024 4:00:00 PM	32.0	12	14	33	0.0	0.0%	26.2
8/25/2024 5:00:00 PM	39.0	12	14	42	0.0	0.0%	32.2
8/25/2024 6:00:00 PM	31.0	11	13	32	0.0	0.0%	27.5
8/25/2024 7:00:00 PM	33.0	17	20	36	0.0	0.0%	27.1
8/25/2024 8:00:00 PM	31.0	9	11	37	0.0	0.0%	27.5
8/25/2024 9:00:00 PM	38.0	11	13	39	0.0	0.0%	32.5
8/25/2024 10:00:00 PM	34.0	12	14	37	0.0	0.0%	29.9
8/25/2024 11:00:00 PM	30.0	9	11	35	0.0	0.0%	26.6
8/26/2024 12:00:00 AM	33.0	3	4	34	0.0	0.0%	28.5
8/26/2024 1:00:00 AM	33.0	6	7	36	0.0	0.0%	31.3
8/26/2024 2:00:00 AM	34.0	3	3	34	0.0	0.0%	34.0
8/26/2024 3:00:00 AM	30.0	3	3	30	0.0	0.0%	26.7
8/26/2024 4:00:00 AM	23.0	2	2	23	0.0	0.0%	22.5
8/26/2024 5:00:00 AM	27.0	1	1	27	0.0	0.0%	27.0
8/26/2024 6:00:00 AM	**No Data**						
8/26/2024 7:00:00 AM	**No Data**						
8/26/2024 9:00:00 AM						
8/26/2024 9:00:00 AM	24.0	2	2	24	0.0	0.0%	20.5
8/26/2024 10:00:00 AM	36.0	5	6	40	0.0	0.0%	31.5
8/26/2024 11:00:00 AM	34.0	8	10	35	0.0	0.0%	28.8
8/26/2024 12:00:00 PM	35.0	14	16	40	0.0	0.0%	29.1
8/26/2024 1:00:00 PM	32.0	18	21	35	0.0	0.0%	26.7
8/26/2024 2:00:00 PM	35.0	8	9	38	0.0	0.0%	31.2
8/26/2024 3:00:00 PM	32.0	8	9	37	0.0	0.0%	27.8
8/26/2024 4:00:00 PM	34.0	17	20	37	0.0	0.0%	27.8
8/26/2024 5:00:00 PM	36.0	6	7	37	0.0	0.0%	32.3
8/26/2024 6:00:00 PM	33.0	21	25	38	0.0	0.0%	28.4
8/26/2024 7:00:00 PM	36.0	22	26	38	0.0	0.0%	29.5
8/26/2024 8:00:00 PM	32.0	14	16	40	0.0	0.0%	28.2
8/26/2024 9:00:00 PM	33.0	20	24	37	0.0	0.0%	27.9
8/26/2024 10:00:00 PM	38.0	11	13	44	0.0	0.0%	32.0
8/26/2024 11:00:00 PM	32.0	7	8	37	0.0	0.0%	31.0
8/27/2024 12:00:00 AM	34.0	5	6	37	0.0	0.0%	31.5
8/27/2024 1:00:00 AM	38.0	5	6	40	0.0	0.0%	31.5
8/27/2024 2:00:00 AM	23.0	3	3	23	0.0	0.0%	22.3
8/27/2024 3:00:00 AM	30.0	2	2	30	0.0	0.0%	26.5
8/27/2024 4:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
8/27/2024 5:00:00 AM	34.0	3	3	34	0.0	0.0%	23.0
8/27/2024 6:00:00 AM	**No Data**						
8/27/2024 7:00:00 AM	31.0	1	1	31	0.0	0.0%	31.0
8/27/2024 8:00:00 AM	**No Data**						
8/27/2024 9:00:00 AM	37.0	3	3	37	0.0	0.0%	28.0
8/27/2024 10:00:00 AM	32.0	9	11	34	0.0	0.0%	28.1
8/27/2024 11:00:00 AM	33.0	12	14	34	0.0	0.0%	28.8
8/27/2024 12:00:00 PM	32.0	14	17	39	0.0	0.0%	27.9
8/27/2024 1:00:00 PM	33.0	7	8	34	0.0	0.0%	28.0
8/27/2024 2:00:00 PM	35.0	10	12	39	0.0	0.0%	32.5

Incoming Summary
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

8/27/2024 3:00:00 PM	34.0	6	7	34	0.0	0.0%	30.0
8/27/2024 4:00:00 PM	36.0	13	15	43	0.0	0.0%	28.9
8/27/2024 5:00:00 PM	35.0	9	11	40	0.0	0.0%	32.1
8/27/2024 6:00:00 PM	34.0	20	23	36	0.0	0.0%	28.8
8/27/2024 7:00:00 PM	35.0	17	20	40	0.0	0.0%	29.3
8/27/2024 8:00:00 PM	33.0	10	12	39	0.0	0.0%	30.0
8/27/2024 9:00:00 PM	35.0	20	24	37	0.0	0.0%	28.7
8/27/2024 10:00:00 PM	38.0	12	14	38	0.0	0.0%	27.1
8/27/2024 11:00:00 PM	35.0	10	12	36	0.0	0.0%	30.4
8/28/2024 12:00:00 AM	30.0	7	8	33	0.0	0.0%	27.1
8/28/2024 1:00:00 AM	30.0	5	6	36	0.0	0.0%	27.0
8/28/2024 2:00:00 AM	25.0	3	4	30	0.0	0.0%	24.8
8/28/2024 3:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
8/28/2024 4:00:00 AM	**No Data**						
8/28/2024 5:00:00 AM	**No Data**						
8/28/2024 7:00:00 AM	31.0	2	2	31	0.0	0.0%	27.0
8/28/2024 8:00:00 AM	37.0	2	2	37	0.0	0.0%	34.0
8/28/2024 9:00:00 AM	23.0	2	2	23	0.0	0.0%	23.0
8/28/2024 10:00:00 AM	31.0	3	4	35	0.0	0.0%	27.8
8/28/2024 11:00:00 AM	32.0	14	16	35	0.0	0.0%	27.3
8/28/2024 12:00:00 PM	36.0	9	11	40	0.0	0.0%	33.1
8/28/2024 1:00:00 PM	34.0	13	15	36	0.0	0.0%	27.5
8/28/2024 2:00:00 PM	36.0	9	11	38	0.0	0.0%	32.1
8/28/2024 3:00:00 PM	35.0	8	10	37	0.0	0.0%	31.3
8/28/2024 4:00:00 PM	34.0	12	14	39	0.0	0.0%	30.1
8/28/2024 5:00:00 PM	33.0	7	8	36	0.0	0.0%	29.0
8/28/2024 6:00:00 PM	31.0	17	20	36	0.0	0.0%	27.8
8/28/2024 7:00:00 PM	34.0	20	24	39	0.0	0.0%	26.1
8/28/2024 8:00:00 PM	33.0	9	11	40	0.0	0.0%	28.2
8/28/2024 9:00:00 PM	35.0	13	15	41	0.0	0.0%	30.0
8/28/2024 10:00:00 PM	33.0	10	12	37	0.0	0.0%	27.6
8/28/2024 11:00:00 PM	32.0	6	7	54	54.0	14.3%	32.0
8/29/2024 12:00:00 AM	31.0	7	8	35	0.0	0.0%	29.4
8/29/2024 1:00:00 AM	31.0	4	5	34	0.0	0.0%	30.2
8/29/2024 2:00:00 AM	32.0	4	5	43	0.0	0.0%	30.6
8/29/2024 3:00:00 AM	32.0	3	4	36	0.0	0.0%	32.5
8/29/2024 4:00:00 AM	**No Data**						
8/29/2024 5:00:00 AM	**No Data**						
8/29/2024 8:00:00 AM	33.0	1	1	33	0.0	0.0%	33.0
8/29/2024 9:00:00 AM	22.0	2	2	22	0.0	0.0%	21.5
8/29/2024 10:00:00 AM	33.0	6	7	34	0.0	0.0%	26.9
8/29/2024 11:00:00 AM	35.0	14	16	42	0.0	0.0%	28.0
8/29/2024 12:00:00 PM	32.0	14	17	34	0.0	0.0%	26.8
8/29/2024 1:00:00 PM	31.0	14	17	34	0.0	0.0%	26.6

Incoming Summary
Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/24/2024 12:00:00 AM	36.0	136	160	43	0.0	0.0%	29.7
8/25/2024 12:00:00 AM	34.0	180	212	56	51.3	1.9%	28.6
8/26/2024 12:00:00 AM	34.0	141	166	50	50.0	0.6%	28.3
8/27/2024 12:00:00 AM	35.0	199	234	44	0.0	0.0%	29.0
8/28/2024 12:00:00 AM	35.0	193	227	43	0.0	0.0%	29.0
8/29/2024 12:00:00 AM	34.0	173	203	54	54.0	0.5%	28.8
8/29/2024 12:59:59 PM	33.0	63	74	43	0.0	0.0%	27.8

Incoming Weekly Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	19	4	0	11.5	0
1 - 2	*	*	*	*	*	3	1	0	2	0
2 - 3	*	*	*	*	*	3	5	0	4	0
3 - 4	*	*	*	*	*	2	1	0	1.5	0
4 - 5	*	*	*	*	*	2	0	0	1	0
5 - 6	*	*	*	*	*	0	0	0	0	0
6 - 7	*	*	*	*	*	1	2	0	1.5	0
7 - 8	*	*	*	*	*	0	0	0	0	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	3	2	0	2.5	0
10 - 11	*	*	*	*	*	2	1	0	1.5	0
11 - 12	*	*	*	*	*	9	9	0	9	0
12 - 13	*	*	*	*	*	15	12	0	13.5	0
13 - 14	*	*	*	*	*	14	7	0	10.5	0
14 - 15	*	*	*	*	*	18	8	0	13	0
15 - 16	*	*	*	*	16	19	14	16	16.5	36.3
16 - 17	*	*	*	*	17	15	14	17	14.5	34
17 - 18	*	*	*	*	25	16	13	25	14.5	35
18 - 19	*	*	*	*	24	13	20	24	16.5	36.5
19 - 20	*	*	*	*	17	8	11	17	9.5	33.5
20 - 21	*	*	*	*	21	10	13	21	11.5	37
21 - 22	*	*	*	*	17	15	14	17	14.5	35
22 - 23	*	*	*	*	16	15	11	16	13	35.5
23 - 24	*	*	*	*	7	10	4	7	7	34
Totals	0	0	0	0	160	212	166			
% of Total	0%	0%	0%	0%	29.74%	39.41%	30.86%			

Incoming Weekly Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	7	6	6	5	*	*	*	6	0	33
1 - 2	3	3	4	5	*	*	*	3.75	0	28.5
2 - 3	3	2	1	4	*	*	*	2.5	0	31.75
3 - 4	2	1	0	0	*	*	*	0.75	0	25.5
4 - 5	1	3	0	0	*	*	*	1	0	30.5
5 - 6	0	0	0	0	*	*	*	0	0	0
6 - 7	0	1	2	0	*	*	*	0.75	0	31
7 - 8	0	0	2	1	*	*	*	0.75	0	35
8 - 9	2	3	2	2	*	*	*	2.25	0	26.5
9 - 10	6	11	4	7	*	*	*	7	0	32.88
10 - 11	10	14	16	16	*	*	*	14	0	33.3
11 - 12	16	17	11	17	*	*	*	15.25	0	33.58
12 - 13	21	8	15	17	*	*	*	15.25	0	32.5
13 - 14	9	12	11	*	*	*	*	10.67	0	35.33
14 - 15	9	7	10	*	*	*	*	8.67	0	33.33
15 - 16	20	15	14	*	*	*	*	16.33	0	34.5
16 - 17	7	11	8	*	*	*	*	8.67	0	34.5
17 - 18	25	23	20	*	*	*	*	22.67	0	32.57
18 - 19	26	20	24	*	*	*	*	23.33	0	34.63
19 - 20	16	12	11	*	*	*	*	13	0	32.67
20 - 21	24	24	15	*	*	*	*	21	0	34.33
21 - 22	13	14	12	*	*	*	*	13	0	35.93
22 - 23	8	12	7	*	*	*	*	9	0	32.83
23 - 24	6	8	8	*	*	*	*	7.33	0	31.67
Totals	234	227	203	74	0	0	0			
% of Total	31.71%	30.76%	27.51%	10.03%	0%	0%	0%			

Incoming Monthly Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	7	6	6	5	*	19	4	6	11.5	33
1 - 2	3	3	4	5	*	3	1	3.75	2	28.5
2 - 3	3	2	1	4	*	3	5	2.5	4	31.75
3 - 4	2	1	0	0	*	2	1	0.75	1.5	25.5
4 - 5	1	3	0	0	*	2	0	1	1	30.5
5 - 6	0	0	0	0	*	0	0	0	0	0
6 - 7	0	1	2	0	*	1	2	0.75	1.5	31
7 - 8	0	0	2	1	*	0	0	0.75	0	35
8 - 9	2	3	2	2	*	0	0	2.25	0	26.5
9 - 10	6	11	4	7	*	3	2	7	2.5	32.88
10 - 11	10	14	16	16	*	2	1	14	1.5	33.3
11 - 12	16	17	11	17	*	9	9	15.25	9	33.58
12 - 13	21	8	15	17	*	15	12	15.25	13.5	32.5
13 - 14	9	12	11	*	*	14	7	10.67	10.5	35.33
14 - 15	9	7	10	*	*	18	8	8.67	13	33.33
15 - 16	20	15	14	*	16	19	14	16.25	16.5	34.95
16 - 17	7	11	8	*	17	15	14	10.75	14.5	34.38
17 - 18	25	23	20	*	25	16	13	23.25	14.5	33.17
18 - 19	26	20	24	*	24	13	20	23.5	16.5	35.1
19 - 20	16	12	11	*	17	8	11	14	9.5	32.88
20 - 21	24	24	15	*	21	10	13	21	11.5	35
21 - 22	13	14	12	*	17	15	14	14	14.5	35.7
22 - 23	8	12	7	*	16	15	11	10.75	13	33.5
23 - 24	6	8	8	*	7	10	4	7.25	7	32.25
Totals	234	227	203	74	160	212	166			
% of Total	18.34%	17.79%	15.91%	5.8%	12.54%	16.61%	13.01%			

Incoming Weekly Speeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	27.68	31.25	0	28.3	0
1 - 2	*	*	*	*	*	26	26	0	26	0
2 - 3	*	*	*	*	*	29.33	30.8	0	30.25	0
3 - 4	*	*	*	*	*	31	26	0	29.33	0
4 - 5	*	*	*	*	*	29.5	0	0	29.5	0
5 - 6	*	*	*	*	*	0	0	0	0	0
6 - 7	*	*	*	*	*	29	21	0	23.67	0
7 - 8	*	*	*	*	*	0	0	0	0	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	22.67	27.5	0	24.6	0
10 - 11	*	*	*	*	*	23.5	29	0	25.33	0
11 - 12	*	*	*	*	*	29.11	20.67	0	24.89	0
12 - 13	*	*	*	*	*	29.93	28.17	0	29.15	0
13 - 14	*	*	*	*	*	27.14	35	0	29.76	0
14 - 15	*	*	*	*	*	29.17	26.38	0	28.31	0
15 - 16	*	*	*	*	27.69	31.37	26.21	27.69	29.18	36.3
16 - 17	*	*	*	*	27.29	28.93	32.21	27.29	30.52	34
17 - 18	*	*	*	*	29.04	28.69	27.46	29.04	28.14	35
18 - 19	*	*	*	*	30.42	27	27.1	30.42	27.06	36.5
19 - 20	*	*	*	*	30.76	27	27.55	30.76	27.32	33.5
20 - 21	*	*	*	*	29.95	28.5	32.54	29.95	30.78	37
21 - 22	*	*	*	*	32	28	29.86	32	28.9	35
22 - 23	*	*	*	*	30.5	29.67	26.64	30.5	28.38	35.5
23 - 24	*	*	*	*	29.71	28.2	28.5	29.71	28.29	34
Totals	0	0	0	0	29.7	28.6	28.3			
% of Total	0%	0%	0%	0%	34.3%	33.03%	32.68%			

Incoming Weekly Speeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	31.29	31.5	27	30.2	*	*	*	30.04	*	33
1 - 2	34	22.33	24.75	30.6	*	*	*	28.07	*	28.5
2 - 3	26.67	26.5	35	32.5	*	*	*	29.8	*	31.75
3 - 4	22.5	28	0	0	*	*	*	24.33	*	25.5
4 - 5	27	23	0	0	*	*	*	24	*	30.5
5 - 6	0	0	0	0	*	*	*	*	*	0
6 - 7	0	31	27	0	*	*	*	28.33	*	31
7 - 8	0	0	34	33	*	*	*	33.67	*	35
8 - 9	20.5	28	23	21.5	*	*	*	23.78	*	26.5
9 - 10	31.5	28.09	27.75	26.86	*	*	*	28.46	*	32.88
10 - 11	28.8	28.79	27.25	28	*	*	*	28.13	*	33.3
11 - 12	29.12	27.94	33.09	26.82	*	*	*	28.87	*	33.58
12 - 13	26.67	28	27.53	26.59	*	*	*	27.03	*	32.5
13 - 14	31.22	32.5	32.09	*	*	*	*	32	*	35.33
14 - 15	27.78	30	31.3	*	*	*	*	29.73	*	33.33
15 - 16	27.75	28.87	30.07	*	*	*	*	28.76	*	34.5
16 - 17	32.29	32.09	29	*	*	*	*	31.19	*	34.5
17 - 18	28.36	28.78	27.75	*	*	*	*	28.32	*	32.57
18 - 19	29.54	29.3	26.08	*	*	*	*	28.29	*	34.63
19 - 20	28.19	30	28.18	*	*	*	*	28.74	*	32.67
20 - 21	27.88	28.71	30	*	*	*	*	28.7	*	34.33
21 - 22	32	27.14	27.58	*	*	*	*	28.9	*	35.93
22 - 23	31	30.42	32	*	*	*	*	31	*	32.83
23 - 24	31.5	27.12	29.38	*	*	*	*	29.14	*	31.67
Totals	29	29	28.8	27.8	0	0	0			
% of Total	25.31%	25.31%	25.13%	24.26%	0%	0%	0%			

Incoming Monthly Speeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	31.29	31.5	27	30.2	*	27.68	31.25	30.04	28.3	33
1 - 2	34	22.33	24.75	30.6	*	26	26	28.07	26	28.5
2 - 3	26.67	26.5	35	32.5	*	29.33	30.8	29.8	30.25	31.75
3 - 4	22.5	28	0	0	*	31	26	24.33	29.33	25.5
4 - 5	27	23	0	0	*	29.5	0	24	29.5	30.5
5 - 6	0	0	0	0	*	0	0	0	0	0
6 - 7	0	31	27	0	*	29	21	28.33	23.67	31
7 - 8	0	0	34	33	*	0	0	33.67	0	35
8 - 9	20.5	28	23	21.5	*	0	0	23.78	0	26.5
9 - 10	31.5	28.09	27.75	26.86	*	22.67	27.5	28.46	24.6	32.88
10 - 11	28.8	28.79	27.25	28	*	23.5	29	28.13	25.33	33.3
11 - 12	29.12	27.94	33.09	26.82	*	29.11	20.67	28.87	24.89	33.58
12 - 13	26.67	28	27.53	26.59	*	29.93	28.17	27.03	29.15	32.5
13 - 14	31.22	32.5	32.09	*	*	27.14	35	32	29.76	35.33
14 - 15	27.78	30	31.3	*	*	29.17	26.38	29.73	28.31	33.33
15 - 16	27.75	28.87	30.07	*	27.69	31.37	26.21	28.49	29.18	34.95
16 - 17	32.29	32.09	29	*	27.29	28.93	32.21	29.65	30.52	34.38
17 - 18	28.36	28.78	27.75	*	29.04	28.69	27.46	28.52	28.14	33.17
18 - 19	29.54	29.3	26.08	*	30.42	27	27.1	28.83	27.06	35.1
19 - 20	28.19	30	28.18	*	30.76	27	27.55	29.36	27.32	32.88
20 - 21	27.88	28.71	30	*	29.95	28.5	32.54	29.01	30.78	35
21 - 22	32	27.14	27.58	*	32	28	29.86	29.84	28.9	35.7
22 - 23	31	30.42	32	*	30.5	29.67	26.64	30.81	28.38	33.5
23 - 24	31.5	27.12	29.38	*	29.71	28.2	28.5	29.28	28.29	32.25
Totals	29	29	28.8	27.8	29.7	28.6	28.3			
% of Total	14.41%	14.41%	14.31%	13.82%	14.76%	14.21%	14.07%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	33	32	0	32.5	0
1 - 2	*	*	*	*	*	30	26	0	28	0
2 - 3	*	*	*	*	*	34	33	0	33.5	0
3 - 4	*	*	*	*	*	34	26	0	30	0
4 - 5	*	*	*	*	*	32	0	0	32	0
5 - 6	*	*	*	*	*	0	0	0	0	0
6 - 7	*	*	*	*	*	29	21	0	25	0
7 - 8	*	*	*	*	*	0	0	0	0	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	27	30	0	28.5	0
10 - 11	*	*	*	*	*	25	29	0	27	0
11 - 12	*	*	*	*	*	33	29	0	31	0
12 - 13	*	*	*	*	*	34	32	0	33	0
13 - 14	*	*	*	*	*	34.5	38	0	36.25	0
14 - 15	*	*	*	*	*	33.3	32	0	32.65	0
15 - 16	*	*	*	*	36.3	34	31.5	36.3	32.75	36.3
16 - 17	*	*	*	*	34	37	38.5	34	37.75	34
17 - 18	*	*	*	*	35	33	31	35	32	35
18 - 19	*	*	*	*	36.5	34	32.5	36.5	33.25	36.5
19 - 20	*	*	*	*	33.5	34.5	31	33.5	32.75	33.5
20 - 21	*	*	*	*	37	31	38	37	34.5	37
21 - 22	*	*	*	*	35	35	34	35	34.5	35
22 - 23	*	*	*	*	35.5	35	30	35.5	32.5	35.5
23 - 24	*	*	*	*	34	31	33	34	32	34
Totals	0	0	0	0	316.8	683.3	627.5			
% of Total	0%	0%	0%	0%	19.46%	41.98%	38.55%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	33	38	30	31	*	*	*	33	0	33
1 - 2	34	23	25	32	*	*	*	28.5	0	28.5
2 - 3	30	30	35	32	*	*	*	31.75	0	31.75
3 - 4	23	28	0	0	*	*	*	25.5	0	25.5
4 - 5	27	34	0	0	*	*	*	30.5	0	30.5
5 - 6	0	0	0	0	*	*	*	0	0	0
6 - 7	0	31	31	0	*	*	*	31	0	31
7 - 8	0	0	37	33	*	*	*	35	0	35
8 - 9	24	37	23	22	*	*	*	26.5	0	26.5
9 - 10	36	31.5	31	33	*	*	*	32.88	0	32.88
10 - 11	34	32.5	31.7	35	*	*	*	33.3	0	33.3
11 - 12	35	32	36	31.3	*	*	*	33.58	0	33.58
12 - 13	32	33	34	31	*	*	*	32.5	0	32.5
13 - 14	35	35	36	*	*	*	*	35.33	0	35.33
14 - 15	32	33.5	34.5	*	*	*	*	33.33	0	33.33
15 - 16	34	35.5	34	*	*	*	*	34.5	0	34.5
16 - 17	36	34.5	33	*	*	*	*	34.5	0	34.5
17 - 18	33	33.7	31	*	*	*	*	32.57	0	32.57
18 - 19	36	34.7	33.2	*	*	*	*	34.63	0	34.63
19 - 20	32	33	33	*	*	*	*	32.67	0	32.67
20 - 21	33	35	35	*	*	*	*	34.33	0	34.33
21 - 22	37.5	37.3	33	*	*	*	*	35.93	0	35.93
22 - 23	32	34.5	32	*	*	*	*	32.83	0	32.83
23 - 24	34	30	31	*	*	*	*	31.67	0	31.67
Totals	682.5	726.7	679.4	280.3	0	0	0			
% of Total	28.81%	30.68%	28.68%	11.83%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	33	38	30	31	*	33	32	33	32.5	33
1 - 2	34	23	25	32	*	30	26	28.5	28	28.5
2 - 3	30	30	35	32	*	34	33	31.75	33.5	31.75
3 - 4	23	28	0	0	*	34	26	25.5	30	25.5
4 - 5	27	34	0	0	*	32	0	30.5	32	30.5
5 - 6	0	0	0	0	*	0	0	0	0	0
6 - 7	0	31	31	0	*	29	21	31	25	31
7 - 8	0	0	37	33	*	0	0	35	0	35
8 - 9	24	37	23	22	*	0	0	26.5	0	26.5
9 - 10	36	31.5	31	33	*	27	30	32.88	28.5	32.88
10 - 11	34	32.5	31.7	35	*	25	29	33.3	27	33.3
11 - 12	35	32	36	31.3	*	33	29	33.58	31	33.58
12 - 13	32	33	34	31	*	34	32	32.5	33	32.5
13 - 14	35	35	36	*	*	34.5	38	35.33	36.25	35.33
14 - 15	32	33.5	34.5	*	*	33.3	32	33.33	32.65	33.33
15 - 16	34	35.5	34	*	36.3	34	31.5	34.95	32.75	34.95
16 - 17	36	34.5	33	*	34	37	38.5	34.38	37.75	34.38
17 - 18	33	33.7	31	*	35	33	31	33.17	32	33.17
18 - 19	36	34.7	33.2	*	36.5	34	32.5	35.1	33.25	35.1
19 - 20	32	33	33	*	33.5	34.5	31	32.88	32.75	32.88
20 - 21	33	35	35	*	37	31	38	35	34.5	35
21 - 22	37.5	37.3	33	*	35	35	34	35.7	34.5	35.7
22 - 23	32	34.5	32	*	35.5	35	30	33.5	32.5	33.5
23 - 24	34	30	31	*	34	31	33	32.25	32	32.25

Summary of Violators

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	47	29.2	0	0.0
01:00:00	19	27.6	0	0.0
02:00:00	18	30.0	0	0.0
03:00:00	6	26.8	0	0.0
04:00:00	6	25.8	0	0.0
05:00:00	0	N/A	N/A	N/A
06:00:00	6	26.0	0	0.0
07:00:00	3	33.7	0	0.0
08:00:00	9	23.8	0	0.0
09:00:00	33	27.9	0	0.0
10:00:00	59	28.0	0	0.0
11:00:00	79	28.0	0	0.0
12:00:00	88	27.7	0	0.0
13:00:00	53	31.1	1	50.0
14:00:00	52	29.0	0	0.0
15:00:00	98	28.7	3	51.7
16:00:00	72	30.0	0	0.0
17:00:00	122	28.4	0	0.0
18:00:00	127	28.4	0	0.0
19:00:00	75	28.8	0	0.0
20:00:00	107	29.4	0	0.0
21:00:00	85	29.5	0	0.0
22:00:00	69	29.9	1	54.0
23:00:00	43	29.0	1	50.0

Incoming Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/23/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	15:00	1	1	2	6	3	3	0	0	0	0	0	0	0	0	0	0	0	0	0	16	27.7	36.3	22 to 32	68.8	*	*	1	14	1
8/23/2024	16:00	2	2	1	3	6	2	1	0	0	0	0	0	0	0	0	0	0	0	0	17	27.3	34	26 to 36	64.7	*	*	0	17	0
8/23/2024	17:00	1	0	4	8	7	5	0	0	0	0	0	0	0	0	0	0	0	0	0	25	29	35	22 to 32	72.0	*	*	0	24	1
8/23/2024	18:00	2	0	3	3	9	6	1	0	0	0	0	0	0	0	0	0	0	0	0	24	30.4	36.5	29 to 39	66.7	*	*	0	23	1
8/23/2024	19:00	0	0	0	6	9	2	0	0	0	0	0	0	0	0	0	0	0	0	0	17	30.8	33.5	26 to 36	94.1	*	*	0	17	0
8/23/2024	20:00	1	0	3	7	6	3	1	0	0	0	0	0	0	0	0	0	0	0	0	21	30	37	23 to 33	66.7	*	*	0	20	1
8/23/2024	21:00	0	0	1	2	10	2	2	0	0	0	0	0	0	0	0	0	0	0	0	17	32	35	26 to 36	82.4	*	*	0	17	0
8/23/2024	22:00	0	0	4	2	7	2	1	0	0	0	0	0	0	0	0	0	0	0	0	16	30.5	35.5	23 to 33	68.8	*	*	0	15	1
8/23/2024	23:00	0	1	0	1	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	7	29.7	34	27 to 37	85.7	*	*	0	7	0
24 Hr Summary		7	4	18	38	61	26	6	0	0	0	0	0	0	0	0	0	0	0	160	29.7	36	27 to 37	69.4	*	*	1	154	5	

Incoming Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/24/2024	00:00	0	2	5	3	8	1	0	0	0	0	0	0	0	0	0	0	0	0	19	27.7	33	24 to 34	68.4	*	*	0	19	0	
8/24/2024	01:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26	30	20 to 30	100.0	*	*	0	3	0	
8/24/2024	02:00	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	29.3	34	24 to 34	100.0	*	*	0	2	1	
8/24/2024	03:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	31	34	24 to 34	100.0	*	*	0	2	0	
8/24/2024	04:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	29.5	32	22 to 32	100.0	*	*	0	2	0	
8/24/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	06:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0	
8/24/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	09:00	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	22.7	27	17 to 27	66.7	*	*	0	3	0	
8/24/2024	10:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	23.5	25	15 to 25	100.0	*	*	0	2	0	
8/24/2024	11:00	0	1	0	4	3	1	0	0	0	0	0	0	0	0	0	0	0	0	9	29.1	33	23 to 33	77.8	*	*	0	9	0	
8/24/2024	12:00	0	0	2	4	7	2	0	0	0	0	0	0	0	0	0	0	0	0	15	29.9	34	26 to 36	86.7	*	*	1	14	0	
8/24/2024	13:00	2	0	2	5	2	3	0	0	0	0	0	0	0	0	0	0	0	0	14	27.1	34.5	26 to 36	71.4	*	*	0	14	0	
8/24/2024	14:00	1	1	0	5	10	1	0	0	0	0	0	0	0	0	0	0	0	0	18	29.2	33.3	26 to 36	88.9	*	*	0	17	1	
8/24/2024	15:00	0	1	4	5	6	0	0	1	1	1	0	0	0	0	0	0	0	0	19	31.4	34	21 to 31	57.9	3	15.8	0	19	0	
8/24/2024	16:00	0	1	3	5	3	2	1	0	0	0	0	0	0	0	0	0	0	0	15	28.9	37	21 to 31	73.3	*	*	0	15	0	
8/24/2024	17:00	0	0	2	8	4	2	0	0	0	0	0	0	0	0	0	0	0	0	16	28.7	33	23 to 33	81.3	*	*	0	16	0	
8/24/2024	18:00	0	2	3	3	3	2	0	0	0	0	0	0	0	0	0	0	0	0	13	27	34	25 to 35	61.5	*	*	0	13	0	
8/24/2024	19:00	0	2	0	3	1	2	0	0	0	0	0	0	0	0	0	0	0	0	8	27	34.5	25 to 35	75.0	*	*	0	8	0	
8/24/2024	20:00	0	0	1	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	10	28.5	31	22 to 32	90.0	*	*	0	10	0	
8/24/2024	21:00	1	1	1	6	3	2	1	0	0	0	0	0	0	0	0	0	0	0	15	28	35	26 to 36	73.3	*	*	0	15	0	
8/24/2024	22:00	1	0	0	5	6	3	0	0	0	0	0	0	0	0	0	0	0	0	15	29.7	35	25 to 35	80.0	*	*	0	14	1	
8/24/2024	23:00	0	2	1	3	3	0	0	0	1	0	0	0	0	0	0	0	0	0	10	28.2	31	23 to 33	70.0	1	10.0	0	10	0	
24 Hr Summary		5	14	26	73	67	21	2	1	2	1	0	0	0	0	0	0	0	0	212	28.6	34	25 to 35	70.3	4	1.9	1	208	3	

Incoming Histogram
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/25/2024	00:00	0	0	1	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	4	31.3	32	22 to 32	75.0	*	*	0	4	0	
8/25/2024	01:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	1	0	
8/25/2024	02:00	0	0	0	3	1	0	1	0	0	0	0	0	0	0	0	0	0	0	5	30.8	33	23 to 33	80.0	*	*	0	5	0	
8/25/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	1	0	
8/25/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	06:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	21	21	11 to 21	100.0	*	*	0	2	0	
8/25/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	09:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27.5	30	20 to 30	100.0	*	*	0	2	0	
8/25/2024	10:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0	
8/25/2024	11:00	3	1	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	9	20.7	29	13 to 23	66.7	*	*	0	9	0	
8/25/2024	12:00	0	1	4	3	2	0	2	0	0	0	0	0	0	0	0	0	0	0	12	28.2	32	21 to 31	66.7	*	*	0	11	1	
8/25/2024	13:00	0	0	0	2	1	3	0	0	1	0	0	0	0	0	0	0	0	0	7	35	38	25 to 35	57.1	1	14.3	0	7	0	
8/25/2024	14:00	1	0	1	4	1	1	0	0	0	0	0	0	0	0	0	0	0	0	8	26.4	32	22 to 32	75.0	*	*	0	8	0	
8/25/2024	15:00	1	1	2	7	3	0	0	0	0	0	0	0	0	0	0	0	0	0	14	26.2	31.5	23 to 33	85.7	*	*	0	14	0	
8/25/2024	16:00	0	0	2	4	2	5	1	0	0	0	0	0	0	0	0	0	0	0	14	32.2	38.5	29 to 39	71.4	*	*	0	14	0	
8/25/2024	17:00	0	0	3	5	5	0	0	0	0	0	0	0	0	0	0	0	0	0	13	27.5	31	22 to 32	92.3	*	*	0	13	0	
8/25/2024	18:00	0	1	6	6	5	2	0	0	0	0	0	0	0	0	0	0	0	0	20	27.1	32.5	23 to 33	75.0	*	*	0	20	0	
8/25/2024	19:00	0	0	3	5	2	1	0	0	0	0	0	0	0	0	0	0	0	0	11	27.5	31	19 to 29	72.7	*	*	0	10	1	
8/25/2024	20:00	0	0	0	3	7	3	0	0	0	0	0	0	0	0	0	0	0	0	13	32.5	38	29 to 39	84.6	*	*	0	13	0	
8/25/2024	21:00	0	1	1	4	6	2	0	0	0	0	0	0	0	0	0	0	0	0	14	29.9	34	27 to 37	78.6	*	*	0	14	0	
8/25/2024	22:00	1	1	0	5	3	1	0	0	0	0	0	0	0	0	0	0	0	0	11	26.6	30	25 to 35	81.8	*	*	0	11	0	
8/25/2024	23:00	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28.5	33	24 to 34	75.0	*	*	0	4	0	
24 Hr Summary		6	6	28	59	43	18	5	0	1	0	0	0	0	0	0	0	0	0	166	28.3	34	23 to 33	66.3	1	0.6	0	164	2	

Incoming Histogram
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/26/2024	00:00	0	0	0	2	4	1	0	0	0	0	0	0	0	0	0	0	0	0	7	31.3	33	26 to 36	100.0	*	*	0	7	0	
8/26/2024	01:00	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3	34	34	24 to 34	100.0	*	*	0	3	0	
8/26/2024	02:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26.7	30	20 to 30	100.0	*	*	0	3	0	
8/26/2024	03:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	22.5	23	13 to 23	100.0	*	*	0	2	0	
8/26/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	27	27	17 to 27	100.0	*	*	0	1	0	
8/26/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	08:00	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	20.5	24	14 to 24	100.0	*	*	0	2	0	
8/26/2024	09:00	0	1	0	0	2	2	1	0	0	0	0	0	0	0	0	0	0	0	6	31.5	36	30 to 40	83.3	*	*	0	6	0	
8/26/2024	10:00	0	1	1	4	2	2	0	0	0	0	0	0	0	0	0	0	0	0	10	28.8	34	25 to 35	80.0	*	*	0	8	2	
8/26/2024	11:00	0	1	2	5	5	2	1	0	0	0	0	0	0	0	0	0	0	0	16	29.1	35	22 to 32	75.0	*	*	0	15	1	
8/26/2024	12:00	0	4	3	6	7	1	0	0	0	0	0	0	0	0	0	0	0	0	21	26.7	32	22 to 32	66.7	*	*	0	21	0	
8/26/2024	13:00	0	0	1	2	3	3	0	0	0	0	0	0	0	0	0	0	0	0	9	31.2	35	28 to 38	88.9	*	*	0	9	0	
8/26/2024	14:00	0	0	3	3	2	1	0	0	0	0	0	0	0	0	0	0	0	0	9	27.8	32	22 to 32	77.8	*	*	0	9	0	
8/26/2024	15:00	2	1	2	4	8	3	0	0	0	0	0	0	0	0	0	0	0	0	20	27.8	34	25 to 35	65.0	*	*	0	19	1	
8/26/2024	16:00	0	0	0	1	4	2	0	0	0	0	0	0	0	0	0	0	0	0	7	32.3	36	27 to 37	85.7	*	*	0	7	0	
8/26/2024	17:00	0	0	8	6	9	2	0	0	0	0	0	0	0	0	0	0	0	0	25	28.4	33	22 to 32	76.0	*	*	0	24	1	
8/26/2024	18:00	1	2	3	4	9	7	0	0	0	0	0	0	0	0	0	0	0	0	26	29.5	36	27 to 37	69.2	*	*	0	22	4	
8/26/2024	19:00	0	3	0	6	6	0	1	0	0	0	0	0	0	0	0	0	0	0	16	28.2	32	23 to 33	75.0	*	*	0	16	0	
8/26/2024	20:00	0	1	4	10	6	3	0	0	0	0	0	0	0	0	0	0	0	0	24	27.9	33	22 to 32	70.8	*	*	0	24	0	
8/26/2024	21:00	0	0	2	1	7	2	1	0	0	0	0	0	0	0	0	0	0	0	13	32	37.5	28 to 38	76.9	*	*	0	13	0	
8/26/2024	22:00	0	0	0	3	4	1	0	0	0	0	0	0	0	0	0	0	0	0	8	31	32	27 to 37	100.0	*	*	0	8	0	
8/26/2024	23:00	0	0	0	2	3	1	0	0	0	0	0	0	0	0	0	0	0	0	6	31.5	34	24 to 34	83.3	*	*	0	6	0	
24 Hr Summary		3	15	33	61	85	33	4	0	0	0	0	0	0	0	0	0	0	0	234	29	35	25 to 35	66.7	*	*	0	225	9	

Incoming Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/27/2024	00:00	0	0	1	2	1	1	1	0	0	0	0	0	0	0	0	0	0	0	6	31.5	38	24 to 34	66.7	*	*	0	6	0	
8/27/2024	01:00	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	22.3	23	13 to 23	100.0	*	*	0	3	0	
8/27/2024	02:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	26.5	30	20 to 30	100.0	*	*	0	2	0	
8/27/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	0	1	
8/27/2024	04:00	1	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	23	34	12 to 22	66.7	*	*	0	3	0	
8/27/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	06:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	31	31	21 to 31	100.0	*	*	0	1	0	
8/27/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	08:00	0	0	2	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	28	37	14 to 24	66.7	*	*	0	3	0	
8/27/2024	09:00	0	1	1	4	5	0	0	0	0	0	0	0	0	0	0	0	0	0	11	28.1	31.5	24 to 34	90.9	*	*	0	10	1	
8/27/2024	10:00	0	1	3	0	10	0	0	0	0	0	0	0	0	0	0	0	0	0	14	28.8	32.5	23 to 33	78.6	*	*	0	13	1	
8/27/2024	11:00	0	3	3	2	6	3	0	0	0	0	0	0	0	0	0	0	0	0	17	27.9	32	22 to 32	58.8	*	*	0	17	0	
8/27/2024	12:00	0	0	2	3	3	0	0	0	0	0	0	0	0	0	0	0	0	0	8	28	33	24 to 34	75.0	*	*	0	7	1	
8/27/2024	13:00	0	0	0	4	4	4	0	0	0	0	0	0	0	0	0	0	0	0	12	32.5	35	26 to 36	91.7	*	*	0	12	0	
8/27/2024	14:00	0	0	0	3	4	0	0	0	0	0	0	0	0	0	0	0	0	0	7	30	33.5	24 to 34	100.0	*	*	0	7	0	
8/27/2024	15:00	1	0	3	4	3	3	1	0	0	0	0	0	0	0	0	0	0	0	15	28.9	35.5	22 to 32	66.7	*	*	0	15	0	
8/27/2024	16:00	0	0	1	1	6	2	1	0	0	0	0	0	0	0	0	0	0	0	11	32.1	34.5	25 to 35	81.8	*	*	0	11	0	
8/27/2024	17:00	0	3	2	5	11	2	0	0	0	0	0	0	0	0	0	0	0	0	23	28.8	33.7	25 to 35	73.9	*	*	0	19	4	
8/27/2024	18:00	0	3	2	4	6	4	1	0	0	0	0	0	0	0	0	0	0	0	20	29.3	34.7	25 to 35	65.0	*	*	0	16	4	
8/27/2024	19:00	0	0	2	3	5	2	0	0	0	0	0	0	0	0	0	0	0	0	12	30	33	23 to 33	75.0	*	*	0	12	0	
8/27/2024	20:00	0	2	4	7	5	6	0	0	0	0	0	0	0	0	0	0	0	0	24	28.7	35	27 to 37	70.8	*	*	0	23	1	
8/27/2024	21:00	0	2	3	5	1	3	0	0	0	0	0	0	0	0	0	0	0	0	14	27.1	37.3	17 to 27	71.4	*	*	0	14	0	
8/27/2024	22:00	0	1	1	2	5	3	0	0	0	0	0	0	0	0	0	0	0	0	12	30.4	34.5	26 to 36	83.3	*	*	0	12	0	
8/27/2024	23:00	0	0	2	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	8	27.1	30	23 to 33	100.0	*	*	0	8	0	
24 Hr Summary		2	16	37	54	80	34	4	0	0	0	0	0	0	0	0	0	0	0	227	29	35	25 to 35	65.6	*	*	0	214	13	

Incoming Histogram
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/28/2024	00:00	0	1	1	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	6	27	30	20 to 30	66.7	*	*	0	6	0	
8/28/2024	01:00	0	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	24.8	25	20 to 30	100.0	*	*	0	4	0	
8/28/2024	02:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	1	0	
8/28/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/28/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/28/2024	06:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27	31	21 to 31	100.0	*	*	0	2	0	
8/28/2024	07:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	37	27 to 37	100.0	*	*	0	2	0	
8/28/2024	08:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	23	23	13 to 23	100.0	*	*	0	2	0	
8/28/2024	09:00	0	0	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	4	27.8	31	25 to 35	75.0	*	*	0	4	0	
8/28/2024	10:00	1	2	1	5	6	1	0	0	0	0	0	0	0	0	0	0	0	0	16	27.3	31.7	22 to 32	75.0	*	*	0	16	0	
8/28/2024	11:00	0	0	0	2	6	2	1	0	0	0	0	0	0	0	0	0	0	0	11	33.1	36	26 to 36	81.8	*	*	0	11	0	
8/28/2024	12:00	0	1	5	4	3	2	0	0	0	0	0	0	0	0	0	0	0	0	15	27.5	34	24 to 34	73.3	*	*	0	13	2	
8/28/2024	13:00	0	0	0	3	4	4	0	0	0	0	0	0	0	0	0	0	0	0	11	32.1	36	26 to 36	81.8	*	*	0	10	1	
8/28/2024	14:00	0	0	0	3	4	3	0	0	0	0	0	0	0	0	0	0	0	0	10	31.3	34.5	25 to 35	90.0	*	*	0	10	0	
8/28/2024	15:00	0	0	0	9	3	2	0	0	0	0	0	0	0	0	0	0	0	0	14	30.1	34	24 to 34	85.7	*	*	0	14	0	
8/28/2024	16:00	0	0	2	2	3	1	0	0	0	0	0	0	0	0	0	0	0	0	8	29	33	23 to 33	87.5	*	*	0	7	1	
8/28/2024	17:00	0	0	4	11	4	1	0	0	0	0	0	0	0	0	0	0	0	0	20	27.8	31	22 to 32	80.0	*	*	0	19	1	
8/28/2024	18:00	3	1	7	4	8	1	0	0	0	0	0	0	0	0	0	0	0	0	24	26.1	33.2	20 to 30	50.0	*	*	0	23	1	
8/28/2024	19:00	1	0	2	3	3	1	1	0	0	0	0	0	0	0	0	0	0	0	11	28.2	33	25 to 35	63.6	*	*	0	11	0	
8/28/2024	20:00	0	1	1	5	5	2	1	0	0	0	0	0	0	0	0	0	0	0	15	30	35	25 to 35	73.3	*	*	0	15	0	
8/28/2024	21:00	1	0	2	3	5	1	0	0	0	0	0	0	0	0	0	0	0	0	12	27.6	33	23 to 33	66.7	*	*	0	12	0	
8/28/2024	22:00	0	0	0	4	2	0	0	0	1	0	0	0	0	0	0	0	0	0	7	32	32	22 to 32	85.7	1	14.3	0	7	0	
8/28/2024	23:00	0	0	1	2	4	1	0	0	0	0	0	0	0	0	0	0	0	0	8	29.4	31	21 to 31	87.5	*	*	0	8	0	
24 Hr Summary		6	6	32	64	65	26	3	0	1	0	0	0	0	0	0	0	0	0	203	28.8	34	25 to 35	68.0	1	0.5	0	197	6	

Incoming Histogram

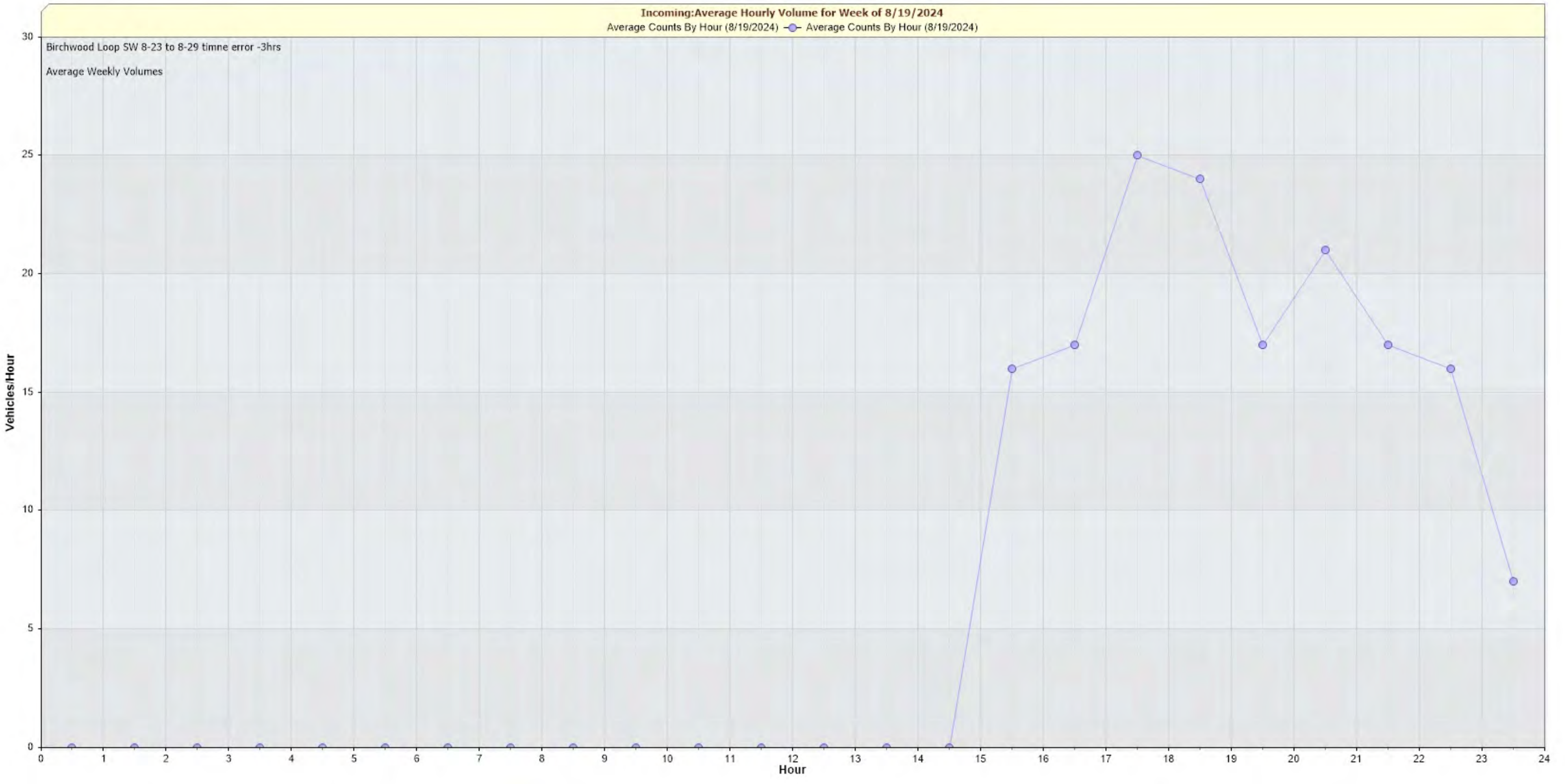
Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	0	0	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	5	30.2	31	24 to 34	100.0	*	*	0	5	0	
8/29/2024	01:00	0	0	1	2	1	0	1	0	0	0	0	0	0	0	0	0	0	0	5	30.6	32	22 to 32	80.0	*	*	0	5	0	
8/29/2024	02:00	0	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	0	0	4	32.5	32	26 to 36	100.0	*	*	0	4	0	
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	33	33	23 to 33	100.0	*	*	0	1	0	
8/29/2024	08:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	21.5	22	12 to 22	100.0	*	*	0	2	0	
8/29/2024	09:00	0	2	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	7	26.9	33	24 to 34	71.4	*	*	0	7	0	
8/29/2024	10:00	3	0	1	3	6	2	1	0	0	0	0	0	0	0	0	0	0	0	16	28	35	26 to 36	68.8	*	*	0	16	0	
8/29/2024	11:00	1	1	3	4	8	0	0	0	0	0	0	0	0	0	0	0	0	0	17	26.8	31.3	24 to 34	70.6	*	*	0	16	1	
8/29/2024	12:00	0	2	3	8	4	0	0	0	0	0	0	0	0	0	0	0	0	0	17	26.6	31	24 to 34	76.5	*	*	0	15	2	
8/29/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		4	5	10	21	29	3	2	0	0	0	0	0	0	0	0	0	0	0	74	27.8	33	24 to 34	68.9	*	*	0	71	3	

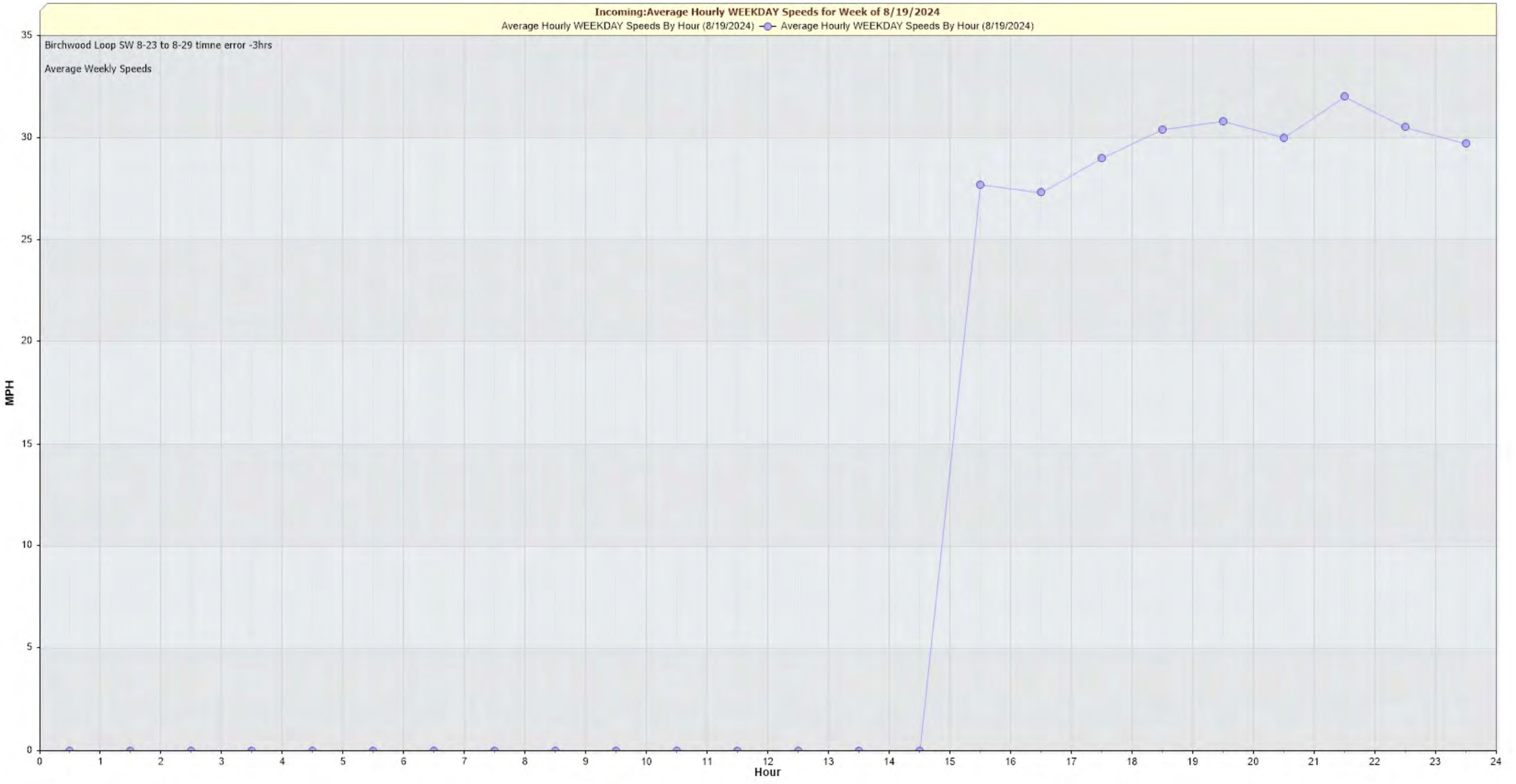
Incoming: Average Hourly Volume for Week of 8/19/2024
Average Counts By Hour (8/19/2024) — Average Counts By Hour (8/19/2024)

Birchwood Loop SW 8-23 to 8-29 time error -3hrs
Average Weekly Volumes



Incoming: Average Hourly WEEKDAY Speeds for Week of 8/19/2024
Average Hourly WEEKDAY Speeds By Hour (8/19/2024) — Average Hourly WEEKDAY Speeds By Hour (8/19/2024)

Birchwood Loop SW 8-23 to 8-29 time error -3hrs
Average Weekly Speeds



Incoming: Daily Volume for Week of 8/19/2024

Daily Vehicle Counts

Birchwood Loop SW 8-23 to 8-29 time error -3hrs

Daily Volumes

Vehicles

250
200
150
100
50
0

Monday

Tuesday

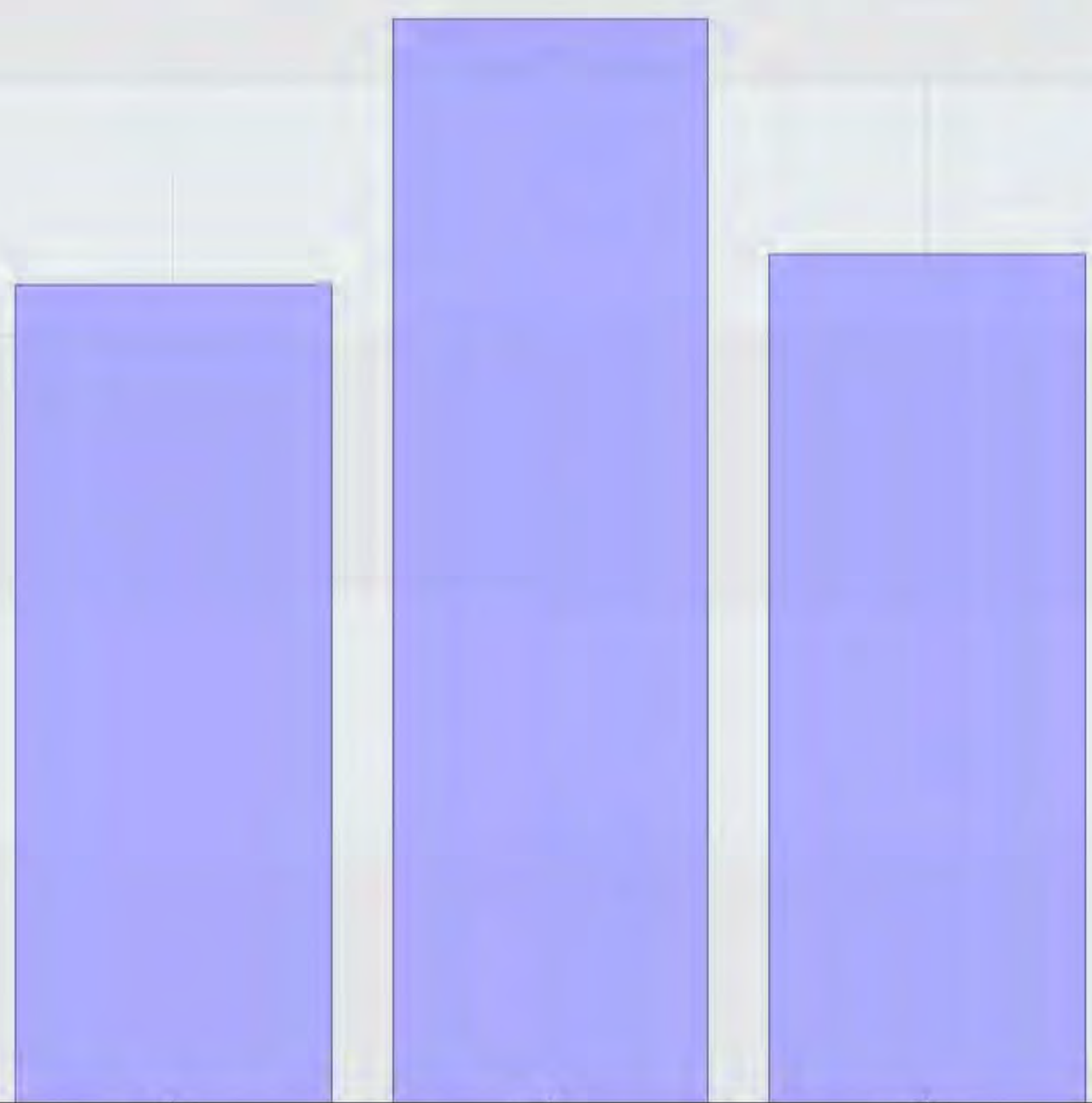
Wednesday

Thursday
Day

Friday

Saturday

Sunday



For Project: Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:27:14 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 8/23/2024 3:00:00 PM through 8/29/2024 12:59:59 PM

85th Percentile Speed 27 MPH

85th Percentile Vehicles 1005

Max Speed 41 MPH on 8/23/2024 8:10:36 PM

Total Vehicles 1182

AADT: 199

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	167	168
AM Peak 10:00 AM	11	8
PM Peak 7:00 PM	22	20

Speed

Speed Limit: 45

85th Percentile Speed: 27

50th Percentile Speed: 23

10 MPH Pace Interval: 18.0 MPH to 28.0 MPH

Average Speed: 23.29

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	0	0	0	0	0
% over limit	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speeder	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speed	23.2	23.7	23.3	23.3	24.1	22.6	22.7

Class Counts

	Number	%
VEH_SM	22	1.9
VEH_MED	1137	96.2
VEH_LG	23	1.9
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Outgoing Summary
Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/23/2024 4:00:00 PM	24.0	12	14	29	0.0	0.0%	22.2
8/23/2024 5:00:00 PM	29.0	11	13	35	0.0	0.0%	25.1
8/23/2024 6:00:00 PM	26.0	14	17	31	0.0	0.0%	24.2
8/23/2024 7:00:00 PM	27.0	14	16	28	0.0	0.0%	23.6
8/23/2024 8:00:00 PM	26.0	16	19	31	0.0	0.0%	23.9
8/23/2024 9:00:00 PM	28.0	26	31	41	0.0	0.0%	24.7
8/23/2024 10:00:00 PM	28.0	18	21	32	0.0	0.0%	24.2
8/23/2024 11:00:00 PM	26.0	15	18	32	0.0	0.0%	23.7
8/24/2024 12:00:00 AM	27.0	8	10	31	0.0	0.0%	25.6
8/24/2024 1:00:00 AM	27.0	8	10	28	0.0	0.0%	24.9
8/24/2024 2:00:00 AM	25.0	4	5	27	0.0	0.0%	22.6
8/24/2024 3:00:00 AM	19.0	1	1	19	0.0	0.0%	19.0
8/24/2024 4:00:00 AM	25.0	2	2	25	0.0	0.0%	23.5
8/24/2024 5:00:00 AM	**No Data**						
8/24/2024 6:00:00 AM	20.0	1	1	20	0.0	0.0%	20.0
8/24/2024 7:00:00 AM	17.0	1	1	17	0.0	0.0%	17.0
8/24/2024 8:00:00 AM	**No Data**						
8/24/2024 9:00:00 AM	**No Data**						
8/24/2024 10:00:00 AM	23.0	2	2	23	0.0	0.0%	22.5
8/24/2024 11:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
8/24/2024 12:00:00 PM	22.0	3	4	23	0.0	0.0%	20.5
8/24/2024 1:00:00 PM	22.0	3	3	22	0.0	0.0%	20.3
8/24/2024 2:00:00 PM	22.0	8	10	26	0.0	0.0%	21.2
8/24/2024 3:00:00 PM	24.0	14	16	26	0.0	0.0%	20.2
8/24/2024 4:00:00 PM	24.0	9	11	32	0.0	0.0%	23.5
8/24/2024 5:00:00 PM	26.0	12	14	30	0.0	0.0%	23.5
8/24/2024 6:00:00 PM	26.0	14	16	33	0.0	0.0%	23.8
8/24/2024 7:00:00 PM	26.0	14	16	28	0.0	0.0%	22.4
8/24/2024 8:00:00 PM	24.0	7	8	25	0.0	0.0%	21.4
8/24/2024 9:00:00 PM	28.0	18	21	30	0.0	0.0%	23.6
8/24/2024 10:00:00 PM	25.0	16	19	27	0.0	0.0%	21.8
8/24/2024 11:00:00 PM	26.0	10	12	34	0.0	0.0%	23.2
8/25/2024 12:00:00 AM	25.0	9	11	27	0.0	0.0%	23.5
8/25/2024 1:00:00 AM	24.0	8	9	25	0.0	0.0%	21.7
8/25/2024 2:00:00 AM	25.0	7	8	26	0.0	0.0%	22.3
8/25/2024 3:00:00 AM	28.0	3	3	28	0.0	0.0%	27.0
8/25/2024 4:00:00 AM	21.0	1	1	21	0.0	0.0%	21.0
8/25/2024 5:00:00 AM	**No Data**						
8/25/2024 6:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
8/25/2024 7:00:00 AM	20.0	1	1	20	0.0	0.0%	20.0
8/25/2024 8:00:00 AM	**No Data**						
8/25/2024 9:00:00 AM	**No Data**						
8/25/2024 10:00:00 AM	14.0	1	1	14	0.0	0.0%	14.0
8/25/2024 11:00:00 AM	17.0	1	1	17	0.0	0.0%	17.0
8/25/2024 12:00:00 PM	15.0	1	1	15	0.0	0.0%	15.0
8/25/2024 1:00:00 PM	35.0	3	3	35	0.0	0.0%	28.7
8/25/2024 2:00:00 PM	24.0	3	4	25	0.0	0.0%	22.3

Outgoing Summary
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

8/25/2024 3:00:00 PM	25.0	8	10	28	0.0	0.0%	23.2
8/25/2024 4:00:00 PM	26.0	14	16	30	0.0	0.0%	22.1
8/25/2024 5:00:00 PM	29.0	11	13	34	0.0	0.0%	24.5
8/25/2024 6:00:00 PM	23.0	8	9	24	0.0	0.0%	19.4
8/25/2024 7:00:00 PM	27.0	12	14	31	0.0	0.0%	24.1
8/25/2024 8:00:00 PM	26.0	14	17	30	0.0	0.0%	22.4
8/25/2024 9:00:00 PM	29.0	12	14	36	0.0	0.0%	24.1
8/25/2024 10:00:00 PM	28.0	12	14	28	0.0	0.0%	23.4
8/25/2024 11:00:00 PM	25.0	10	12	27	0.0	0.0%	22.0
8/26/2024 12:00:00 AM	24.0	9	11	29	0.0	0.0%	21.9
8/26/2024 1:00:00 AM	25.0	5	6	26	0.0	0.0%	23.8
8/26/2024 2:00:00 AM	25.0	7	8	25	0.0	0.0%	22.4
8/26/2024 3:00:00 AM	24.0	6	7	25	0.0	0.0%	19.9
8/26/2024 4:00:00 AM	26.0	3	3	26	0.0	0.0%	24.7
8/26/2024 5:00:00 AM	**No Data**						
8/26/2024 6:00:00 AM	**No Data**						
						
8/26/2024 10:00:00 AM	25.0	2	2	25	0.0	0.0%	22.0
8/26/2024 11:00:00 AM	25.0	7	8	25	0.0	0.0%	20.5
8/26/2024 12:00:00 PM	29.0	14	16	30	0.0	0.0%	22.4
8/26/2024 1:00:00 PM	25.0	9	11	35	0.0	0.0%	23.5
8/26/2024 2:00:00 PM	25.0	7	8	28	0.0	0.0%	22.5
8/26/2024 3:00:00 PM	25.0	6	7	26	0.0	0.0%	21.0
8/26/2024 4:00:00 PM	22.0	5	6	22	0.0	0.0%	20.3
8/26/2024 5:00:00 PM	26.0	9	11	31	0.0	0.0%	24.9
8/26/2024 6:00:00 PM	27.0	16	19	30	0.0	0.0%	24.5
8/26/2024 7:00:00 PM	26.0	24	28	35	0.0	0.0%	22.6
8/26/2024 8:00:00 PM	26.0	21	25	39	0.0	0.0%	24.0
8/26/2024 9:00:00 PM	27.0	20	24	38	0.0	0.0%	24.2
8/26/2024 10:00:00 PM	30.0	14	16	41	0.0	0.0%	24.6
8/26/2024 11:00:00 PM	27.0	11	13	27	0.0	0.0%	23.3
8/27/2024 12:00:00 AM	25.0	8	9	27	0.0	0.0%	23.6
8/27/2024 1:00:00 AM	26.0	6	7	26	0.0	0.0%	24.0
8/27/2024 2:00:00 AM	25.0	5	6	29	0.0	0.0%	24.2
8/27/2024 3:00:00 AM	24.0	4	5	24	0.0	0.0%	21.2
8/27/2024 4:00:00 AM	22.0	1	1	22	0.0	0.0%	22.0
8/27/2024 5:00:00 AM	**No Data**						
8/27/2024 6:00:00 AM	**No Data**						
8/27/2024 7:00:00 AM	15.0	1	1	15	0.0	0.0%	15.0
8/27/2024 8:00:00 AM	**No Data**						
8/27/2024 9:00:00 AM	**No Data**						
8/27/2024 10:00:00 AM	28.0	3	3	28	0.0	0.0%	23.7
8/27/2024 11:00:00 AM	24.0	9	11	32	0.0	0.0%	21.9
8/27/2024 12:00:00 PM	26.0	9	11	31	0.0	0.0%	23.9
8/27/2024 1:00:00 PM	24.0	3	3	24	0.0	0.0%	21.7
8/27/2024 2:00:00 PM	24.0	7	8	24	0.0	0.0%	22.1
8/27/2024 3:00:00 PM	25.0	5	6	27	0.0	0.0%	24.2
8/27/2024 4:00:00 PM	28.0	9	11	35	0.0	0.0%	24.9

Outgoing Summary
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

8/27/2024 5:00:00 PM	25.0	8	10	33	0.0	0.0%	24.8
8/27/2024 6:00:00 PM	26.0	10	12	27	0.0	0.0%	23.0
8/27/2024 7:00:00 PM	29.0	13	15	36	0.0	0.0%	24.6
8/27/2024 8:00:00 PM	28.0	20	23	31	0.0	0.0%	23.9
8/27/2024 9:00:00 PM	27.0	20	23	29	0.0	0.0%	24.4
8/27/2024 10:00:00 PM	28.0	14	16	34	0.0	0.0%	24.7
8/27/2024 11:00:00 PM	25.0	10	12	29	0.0	0.0%	23.5
8/28/2024 12:00:00 AM	24.0	8	9	29	0.0	0.0%	23.2
8/28/2024 1:00:00 AM	26.0	7	8	27	0.0	0.0%	23.0
8/28/2024 2:00:00 AM	26.0	5	6	27	0.0	0.0%	23.8
8/28/2024 3:00:00 AM	26.0	3	4	27	0.0	0.0%	26.0
8/28/2024 4:00:00 AM	25.0	2	2	25	0.0	0.0%	24.0
8/28/2024 5:00:00 AM	**No Data**						
8/28/2024 6:00:00 AM	**No Data**						
8/28/2024 7:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
8/28/2024 8:00:00 AM	**No Data**						
8/28/2024 9:00:00 AM	22.0	3	3	22	0.0	0.0%	20.3
8/28/2024 10:00:00 AM	23.0	3	4	26	0.0	0.0%	22.3
8/28/2024 11:00:00 AM	27.0	11	13	28	0.0	0.0%	22.7
8/28/2024 12:00:00 PM	26.0	7	8	27	0.0	0.0%	22.0
8/28/2024 1:00:00 PM	24.0	9	11	25	0.0	0.0%	22.8
8/28/2024 2:00:00 PM	26.0	6	7	31	0.0	0.0%	23.7
8/28/2024 3:00:00 PM	26.0	7	8	26	0.0	0.0%	21.1
8/28/2024 4:00:00 PM	25.0	8	10	26	0.0	0.0%	23.7
8/28/2024 5:00:00 PM	25.0	8	10	28	0.0	0.0%	24.1
8/28/2024 6:00:00 PM	25.0	12	14	27	0.0	0.0%	23.1
8/28/2024 7:00:00 PM	26.0	13	15	28	0.0	0.0%	23.3
8/28/2024 8:00:00 PM	28.0	18	21	29	0.0	0.0%	23.5
8/28/2024 9:00:00 PM	23.0	6	7	25	0.0	0.0%	22.3
8/28/2024 10:00:00 PM	28.0	14	16	31	0.0	0.0%	24.3
8/28/2024 11:00:00 PM	26.0	15	18	38	0.0	0.0%	23.8
8/29/2024 12:00:00 AM	27.0	10	12	32	0.0	0.0%	23.7
8/29/2024 1:00:00 AM	31.0	7	8	32	0.0	0.0%	25.6
8/29/2024 2:00:00 AM	29.0	8	9	33	0.0	0.0%	25.2
8/29/2024 3:00:00 AM	**No Data**						
8/29/2024 4:00:00 AM	24.0	1	1	24	0.0	0.0%	24.0
8/29/2024 5:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
8/29/2024 6:00:00 AM	**No Data**						
8/29/2024 7:00:00 AM	**No Data**						
8/29/2024 9:00:00 AM	22.0	2	2	22	0.0	0.0%	22.0
8/29/2024 10:00:00 AM	22.0	1	1	22	0.0	0.0%	22.0
8/29/2024 11:00:00 AM	26.0	10	12	29	0.0	0.0%	21.0
8/29/2024 12:00:00 PM	25.0	8	10	28	0.0	0.0%	22.8
8/29/2024 1:00:00 PM	25.0	12	14	32	0.0	0.0%	22.8

Outgoing Summary
Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/24/2024 12:00:00 AM	28.0	135	159	41	0.0	0.0%	24.1
8/25/2024 12:00:00 AM	26.0	156	184	34	0.0	0.0%	22.6
8/26/2024 12:00:00 AM	27.0	139	163	36	0.0	0.0%	22.7
8/27/2024 12:00:00 AM	26.0	193	227	41	0.0	0.0%	23.2
8/28/2024 12:00:00 AM	27.0	164	193	36	0.0	0.0%	23.7
8/29/2024 12:00:00 AM	27.0	168	198	38	0.0	0.0%	23.3
8/29/2024 12:59:59 PM	28.0	49	58	33	0.0	0.0%	23.3

Outgoing Weekly Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	10	9	0	9.5	0
1 - 2	*	*	*	*	*	5	8	0	6.5	0
2 - 3	*	*	*	*	*	1	3	0	2	0
3 - 4	*	*	*	*	*	2	1	0	1.5	0
4 - 5	*	*	*	*	*	0	0	0	0	0
5 - 6	*	*	*	*	*	1	1	0	1	0
6 - 7	*	*	*	*	*	1	1	0	1	0
7 - 8	*	*	*	*	*	0	0	0	0	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	2	1	0	1.5	0
10 - 11	*	*	*	*	*	1	1	0	1	0
11 - 12	*	*	*	*	*	4	1	0	2.5	0
12 - 13	*	*	*	*	*	3	3	0	3	0
13 - 14	*	*	*	*	*	10	4	0	7	0
14 - 15	*	*	*	*	*	16	10	0	13	0
15 - 16	*	*	*	*	14	11	16	14	13.5	24
16 - 17	*	*	*	*	13	14	13	13	13.5	28.5
17 - 18	*	*	*	*	17	16	9	17	12.5	26
18 - 19	*	*	*	*	16	16	14	16	15	26.7
19 - 20	*	*	*	*	19	8	17	19	12.5	26
20 - 21	*	*	*	*	31	21	14	31	17.5	27.3
21 - 22	*	*	*	*	21	19	14	21	16.5	28
22 - 23	*	*	*	*	18	12	12	18	12	26
23 - 24	*	*	*	*	10	11	11	10	11	27
Totals	0	0	0	0	159	184	163			
% of Total	0%	0%	0%	0%	31.42%	36.36%	32.21%			

Outgoing Weekly Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	6	7	8	8	*	*	*	7.25	0	26.88
1 - 2	8	6	6	9	*	*	*	7.25	0	26.12
2 - 3	7	5	4	0	*	*	*	4	0	24.5
3 - 4	3	1	2	1	*	*	*	1.75	0	24.25
4 - 5	0	0	0	1	*	*	*	0.25	0	28
5 - 6	0	0	0	0	*	*	*	0	0	0
6 - 7	0	1	1	0	*	*	*	0.5	0	20.5
7 - 8	0	0	0	0	*	*	*	0	0	0
8 - 9	0	0	3	2	*	*	*	1.25	0	22
9 - 10	2	3	4	1	*	*	*	2.5	0	24.5
10 - 11	8	11	13	12	*	*	*	11	0	25.38
11 - 12	16	11	8	10	*	*	*	11.25	0	26.38
12 - 13	11	3	11	14	*	*	*	9.75	0	24.45
13 - 14	8	8	7	*	*	*	*	7.67	0	24.83
14 - 15	7	6	8	*	*	*	*	7	0	25.17
15 - 16	6	11	10	*	*	*	*	9	0	24.83
16 - 17	11	10	10	*	*	*	*	10.33	0	25.33
17 - 18	19	12	14	*	*	*	*	15	0	25.9
18 - 19	28	15	15	*	*	*	*	19.33	0	27
19 - 20	25	23	21	*	*	*	*	23	0	27.1
20 - 21	24	23	7	*	*	*	*	18	0	25.67
21 - 22	16	16	16	*	*	*	*	16	0	28.33
22 - 23	13	12	18	*	*	*	*	14.33	0	25.77
23 - 24	9	9	12	*	*	*	*	10	0	25.33
Totals	227	193	198	58	0	0	0			
% of Total	33.58%	28.55%	29.29%	8.58%	0%	0%	0%			

Outgoing Monthly Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	6	7	8	8	*	10	9	7.25	9.5	26.88
1 - 2	8	6	6	9	*	5	8	7.25	6.5	26.12
2 - 3	7	5	4	0	*	1	3	4	2	24.5
3 - 4	3	1	2	1	*	2	1	1.75	1.5	24.25
4 - 5	0	0	0	1	*	0	0	0.25	0	28
5 - 6	0	0	0	0	*	1	1	0	1	0
6 - 7	0	1	1	0	*	1	1	0.5	1	20.5
7 - 8	0	0	0	0	*	0	0	0	0	0
8 - 9	0	0	3	2	*	0	0	1.25	0	22
9 - 10	2	3	4	1	*	2	1	2.5	1.5	24.5
10 - 11	8	11	13	12	*	1	1	11	1	25.38
11 - 12	16	11	8	10	*	4	1	11.25	2.5	26.38
12 - 13	11	3	11	14	*	3	3	9.75	3	24.45
13 - 14	8	8	7	*	*	10	4	7.67	7	24.83
14 - 15	7	6	8	*	*	16	10	7	13	25.17
15 - 16	6	11	10	*	14	11	16	10.25	13.5	24.62
16 - 17	11	10	10	*	13	14	13	11	13.5	26.12
17 - 18	19	12	14	*	17	16	9	15.5	12.5	25.92
18 - 19	28	15	15	*	16	16	14	18.5	15	26.92
19 - 20	25	23	21	*	19	8	17	22	12.5	26.82
20 - 21	24	23	7	*	31	21	14	21.25	17.5	26.08
21 - 22	16	16	16	*	21	19	14	17.25	16.5	28.25
22 - 23	13	12	18	*	18	12	12	15.25	12	25.82
23 - 24	9	9	12	*	10	11	11	10	11	25.75
Totals	227	193	198	58	159	184	163			
% of Total	19.2%	16.33%	16.75%	4.91%	13.45%	15.57%	13.79%			

Outgoing Weekly Speeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	24.9	21.67	0	23.37	0
1 - 2	*	*	*	*	*	22.6	22.25	0	22.38	0
2 - 3	*	*	*	*	*	19	27	0	25	0
3 - 4	*	*	*	*	*	23.5	21	0	22.67	0
4 - 5	*	*	*	*	*	0	0	0	0	0
5 - 6	*	*	*	*	*	20	25	0	22.5	0
6 - 7	*	*	*	*	*	17	20	0	18.5	0
7 - 8	*	*	*	*	*	0	0	0	0	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	22.5	14	0	19.67	0
10 - 11	*	*	*	*	*	25	17	0	21	0
11 - 12	*	*	*	*	*	20.5	15	0	19.4	0
12 - 13	*	*	*	*	*	20.33	28.67	0	24.5	0
13 - 14	*	*	*	*	*	21.2	22.25	0	21.5	0
14 - 15	*	*	*	*	*	20.19	23.2	0	21.35	0
15 - 16	*	*	*	*	22.21	23.55	22.06	22.21	22.67	24
16 - 17	*	*	*	*	25.08	23.5	24.54	25.08	24	28.5
17 - 18	*	*	*	*	24.18	23.81	19.44	24.18	22.24	26
18 - 19	*	*	*	*	23.62	22.38	24.14	23.62	23.2	26.7
19 - 20	*	*	*	*	23.95	21.38	22.41	23.95	22.08	26
20 - 21	*	*	*	*	24.71	23.62	24.07	24.71	23.8	27.3
21 - 22	*	*	*	*	24.19	21.84	23.36	24.19	22.48	28
22 - 23	*	*	*	*	23.67	23.17	22	23.67	22.58	26
23 - 24	*	*	*	*	25.6	23.55	21.91	25.6	22.73	27
Totals	0	0	0	0	24.1	22.6	22.7			
% of Total	0%	0%	0%	0%	34.73%	32.56%	32.71%			

Outgoing Weekly Speeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	23.83	24	23	25.62	*	*	*	24.14	*	26.88
1 - 2	22.38	24.17	23.83	25.22	*	*	*	23.93	*	26.12
2 - 3	19.86	21.2	26	0	*	*	*	21.81	*	24.5
3 - 4	24.67	22	24	24	*	*	*	24	*	24.25
4 - 5	0	0	0	28	*	*	*	28	*	28
5 - 6	0	0	0	0	*	*	*	*	*	0
6 - 7	0	15	26	0	*	*	*	20.5	*	20.5
7 - 8	0	0	0	0	*	*	*	*	*	0
8 - 9	0	0	20.33	22	*	*	*	21	*	22
9 - 10	22	23.67	22.25	22	*	*	*	22.6	*	24.5
10 - 11	20.5	21.91	22.69	21	*	*	*	21.64	*	25.38
11 - 12	22.44	23.91	22	22.8	*	*	*	22.8	*	26.38
12 - 13	23.55	21.67	22.82	22.79	*	*	*	22.92	*	24.45
13 - 14	22.5	22.12	23.71	*	*	*	*	22.74	*	24.83
14 - 15	21	24.17	21.12	*	*	*	*	21.95	*	25.17
15 - 16	20.33	24.91	23.7	*	*	*	*	23.44	*	24.83
16 - 17	24.91	24.8	24.1	*	*	*	*	24.61	*	25.33
17 - 18	24.53	23	23.14	*	*	*	*	23.69	*	25.9
18 - 19	22.64	24.6	23.33	*	*	*	*	23.33	*	27
19 - 20	24.04	23.91	23.48	*	*	*	*	23.83	*	27.1
20 - 21	24.21	24.39	22.29	*	*	*	*	24.04	*	25.67
21 - 22	24.62	24.69	24.25	*	*	*	*	24.52	*	28.33
22 - 23	23.31	23.5	23.78	*	*	*	*	23.56	*	25.77
23 - 24	23.56	23.22	23.67	*	*	*	*	23.5	*	25.33
Totals	23.2	23.7	23.3	23.3	0	0	0			
% of Total	24.81%	25.35%	24.92%	24.92%	0%	0%	0%			

Outgoing Monthly Speeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	Aug 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	23.83	24	23	25.62	*	24.9	21.67	24.14	23.37	26.88
1 - 2	22.38	24.17	23.83	25.22	*	22.6	22.25	23.93	22.38	26.12
2 - 3	19.86	21.2	26	0	*	19	27	21.81	25	24.5
3 - 4	24.67	22	24	24	*	23.5	21	24	22.67	24.25
4 - 5	0	0	0	28	*	0	0	28	0	28
5 - 6	0	0	0	0	*	20	25	0	22.5	0
6 - 7	0	15	26	0	*	17	20	20.5	18.5	20.5
7 - 8	0	0	0	0	*	0	0	0	0	0
8 - 9	0	0	20.33	22	*	0	0	21	0	22
9 - 10	22	23.67	22.25	22	*	22.5	14	22.6	19.67	24.5
10 - 11	20.5	21.91	22.69	21	*	25	17	21.64	21	25.38
11 - 12	22.44	23.91	22	22.8	*	20.5	15	22.8	19.4	26.38
12 - 13	23.55	21.67	22.82	22.79	*	20.33	28.67	22.92	24.5	24.45
13 - 14	22.5	22.12	23.71	*	*	21.2	22.25	22.74	21.5	24.83
14 - 15	21	24.17	21.12	*	*	20.19	23.2	21.95	21.35	25.17
15 - 16	20.33	24.91	23.7	*	22.21	23.55	22.06	23.02	22.67	24.62
16 - 17	24.91	24.8	24.1	*	25.08	23.5	24.54	24.75	24	26.12
17 - 18	24.53	23	23.14	*	24.18	23.81	19.44	23.82	22.24	25.92
18 - 19	22.64	24.6	23.33	*	23.62	22.38	24.14	23.39	23.2	26.92
19 - 20	24.04	23.91	23.48	*	23.95	21.38	22.41	23.85	22.08	26.82
20 - 21	24.21	24.39	22.29	*	24.71	23.62	24.07	24.28	23.8	26.08
21 - 22	24.62	24.69	24.25	*	24.19	21.84	23.36	24.42	22.48	28.25
22 - 23	23.31	23.5	23.78	*	23.67	23.17	22	23.59	22.58	25.82
23 - 24	23.56	23.22	23.67	*	25.6	23.55	21.91	24.02	22.73	25.75
Totals	23.2	23.7	23.3	23.3	24.1	22.6	22.7			
% of Total	14.24%	14.55%	14.3%	14.3%	14.79%	13.87%	13.93%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	26.5	24	0	25.25	0
1 - 2	*	*	*	*	*	25	25	0	25	0
2 - 3	*	*	*	*	*	19	28	0	23.5	0
3 - 4	*	*	*	*	*	25	21	0	23	0
4 - 5	*	*	*	*	*	0	0	0	0	0
5 - 6	*	*	*	*	*	20	25	0	22.5	0
6 - 7	*	*	*	*	*	17	20	0	18.5	0
7 - 8	*	*	*	*	*	0	0	0	0	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	23	14	0	18.5	0
10 - 11	*	*	*	*	*	25	17	0	21	0
11 - 12	*	*	*	*	*	22	15	0	18.5	0
12 - 13	*	*	*	*	*	22	35	0	28.5	0
13 - 14	*	*	*	*	*	22	24	0	23	0
14 - 15	*	*	*	*	*	23.5	25	0	24.25	0
15 - 16	*	*	*	*	24	24	26	24	25	24
16 - 17	*	*	*	*	28.5	26	29	28.5	27.5	28.5
17 - 18	*	*	*	*	26	26	23	26	24.5	26
18 - 19	*	*	*	*	26.7	25.5	27	26.7	26.25	26.7
19 - 20	*	*	*	*	26	24	25.3	26	24.65	26
20 - 21	*	*	*	*	27.3	28	28.5	27.3	28.25	27.3
21 - 22	*	*	*	*	28	24.8	27.3	28	26.05	28
22 - 23	*	*	*	*	26	26	25	26	25.5	26
23 - 24	*	*	*	*	27	24.5	24	27	24.25	27
Totals	0	0	0	0	239.5	498.8	508.1			
% of Total	0%	0%	0%	0%	19.22%	40.02%	40.77%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	25	25.5	26	31	*	*	*	26.88	0	26.88
1 - 2	24.5	25	26	29	*	*	*	26.12	0	26.12
2 - 3	24	23.5	26	0	*	*	*	24.5	0	24.5
3 - 4	26	22	25	24	*	*	*	24.25	0	24.25
4 - 5	0	0	0	28	*	*	*	28	0	28
5 - 6	0	0	0	0	*	*	*	0	0	0
6 - 7	0	15	26	0	*	*	*	20.5	0	20.5
7 - 8	0	0	0	0	*	*	*	0	0	0
8 - 9	0	0	22	22	*	*	*	22	0	22
9 - 10	25	28	23	22	*	*	*	24.5	0	24.5
10 - 11	24.5	24	27	26	*	*	*	25.38	0	25.38
11 - 12	28.5	26	26	25	*	*	*	26.38	0	26.38
12 - 13	25	24	23.8	25	*	*	*	24.45	0	24.45
13 - 14	25	23.5	26	*	*	*	*	24.83	0	24.83
14 - 15	25	25	25.5	*	*	*	*	25.17	0	25.17
15 - 16	21.7	28	24.8	*	*	*	*	24.83	0	24.83
16 - 17	26	25	25	*	*	*	*	25.33	0	25.33
17 - 18	26.7	26	25	*	*	*	*	25.9	0	25.9
18 - 19	26	29	26	*	*	*	*	27	0	27
19 - 20	26	28	27.3	*	*	*	*	27.1	0	27.1
20 - 21	27	27	23	*	*	*	*	25.67	0	25.67
21 - 22	29.5	28	27.5	*	*	*	*	28.33	0	28.33
22 - 23	26.3	25	26	*	*	*	*	25.77	0	25.77
23 - 24	25	24	27	*	*	*	*	25.33	0	25.33
Totals	486.7	501.5	533.9	232	0	0	0			
% of Total	27.75%	28.59%	30.44%	13.23%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	25	25.5	26	31	*	26.5	24	26.88	25.25	26.88
1 - 2	24.5	25	26	29	*	25	25	26.12	25	26.12
2 - 3	24	23.5	26	0	*	19	28	24.5	23.5	24.5
3 - 4	26	22	25	24	*	25	21	24.25	23	24.25
4 - 5	0	0	0	28	*	0	0	28	0	28
5 - 6	0	0	0	0	*	20	25	0	22.5	0
6 - 7	0	15	26	0	*	17	20	20.5	18.5	20.5
7 - 8	0	0	0	0	*	0	0	0	0	0
8 - 9	0	0	22	22	*	0	0	22	0	22
9 - 10	25	28	23	22	*	23	14	24.5	18.5	24.5
10 - 11	24.5	24	27	26	*	25	17	25.38	21	25.38
11 - 12	28.5	26	26	25	*	22	15	26.38	18.5	26.38
12 - 13	25	24	23.8	25	*	22	35	24.45	28.5	24.45
13 - 14	25	23.5	26	*	*	22	24	24.83	23	24.83
14 - 15	25	25	25.5	*	*	23.5	25	25.17	24.25	25.17
15 - 16	21.7	28	24.8	*	24	24	26	24.62	25	24.62
16 - 17	26	25	25	*	28.5	26	29	26.12	27.5	26.12
17 - 18	26.7	26	25	*	26	26	23	25.92	24.5	25.92
18 - 19	26	29	26	*	26.7	25.5	27	26.92	26.25	26.92
19 - 20	26	28	27.3	*	26	24	25.3	26.82	24.65	26.82
20 - 21	27	27	23	*	27.3	28	28.5	26.08	28.25	26.08
21 - 22	29.5	28	27.5	*	28	24.8	27.3	28.25	26.05	28.25
22 - 23	26.3	25	26	*	26	26	25	25.82	25.5	25.82
23 - 24	25	24	27	*	27	24.5	24	25.75	24.25	25.75

Summary of Violators

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	48	23.8	0	0.0
01:00:00	42	23.5	0	0.0
02:00:00	20	22.5	0	0.0
03:00:00	10	23.6	0	0.0
04:00:00	1	28.0	0	0.0
05:00:00	2	22.5	0	0.0
06:00:00	4	19.5	0	0.0
07:00:00	0	N/A	N/A	N/A
08:00:00	5	21.0	0	0.0
09:00:00	13	21.9	0	0.0
10:00:00	46	21.6	0	0.0
11:00:00	50	22.5	0	0.0
12:00:00	45	23.1	0	0.0
13:00:00	37	22.3	0	0.0
14:00:00	47	21.6	0	0.0
15:00:00	68	22.9	0	0.0
16:00:00	71	24.5	0	0.0
17:00:00	87	23.4	0	0.0
18:00:00	104	23.3	0	0.0
19:00:00	113	23.5	0	0.0
20:00:00	120	24.1	0	0.0
21:00:00	102	23.8	0	0.0
22:00:00	85	23.3	0	0.0
23:00:00	62	23.6	0	0.0

Outgoing Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/23/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	15:00	0	1	11	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	22.2	24	16 to 26	92.9	*	*	0	14	0
8/23/2024	16:00	1	1	2	8	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	13	25.1	28.5	19 to 29	76.9	*	*	1	12	0
8/23/2024	17:00	0	2	6	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17	24.2	26	18 to 28	88.2	*	*	0	16	1
8/23/2024	18:00	0	0	9	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16	23.6	26.7	18 to 28	100.0	*	*	0	16	0
8/23/2024	19:00	0	1	12	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19	23.9	26	19 to 29	89.5	*	*	0	19	0
8/23/2024	20:00	0	4	13	11	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	31	24.7	27.3	18 to 28	87.1	*	*	0	28	3
8/23/2024	21:00	0	1	14	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	21	24.2	28	19 to 29	85.7	*	*	0	21	0
8/23/2024	22:00	0	2	9	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	18	23.7	26	18 to 28	88.9	*	*	0	18	0
8/23/2024	23:00	0	0	4	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	25.6	27	21 to 31	100.0	*	*	0	10	0
24 Hr Summary		1	12	80	55	9	1	1	0	0	0	0	0	0	0	0	0	0	0	159	24.1	28	19 to 29	87.4	*	*	1	154	4	

Outgoing Histogram
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/24/2024	00:00	0	0	5	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	24.9	26.5	18 to 28	100.0	*	*	0	10	0	
8/24/2024	01:00	0	2	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	22.6	25	17 to 27	100.0	*	*	0	5	0	
8/24/2024	02:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	19	19	9 to 19	100.0	*	*	0	1	0	
8/24/2024	03:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	23.5	25	15 to 25	100.0	*	*	0	2	0	
8/24/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	05:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	20	20	10 to 20	100.0	*	*	0	1	0	
8/24/2024	06:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	17	17	7 to 17	100.0	*	*	0	1	0	
8/24/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	09:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	22.5	23	13 to 23	100.0	*	*	0	2	0	
8/24/2024	10:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0	
8/24/2024	11:00	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	20.5	22	13 to 23	100.0	*	*	0	4	0	
8/24/2024	12:00	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	20.3	22	12 to 22	100.0	*	*	0	3	0	
8/24/2024	13:00	0	3	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	21.2	22	16 to 26	100.0	*	*	0	10	0	
8/24/2024	14:00	1	3	11	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16	20.2	23.5	14 to 24	93.8	*	*	1	15	0	
8/24/2024	15:00	0	0	9	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	11	23.5	24	15 to 25	90.9	*	*	1	10	0	
8/24/2024	16:00	0	2	6	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	14	23.5	26	20 to 30	85.7	*	*	0	14	0	
8/24/2024	17:00	0	0	12	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	23.8	26	18 to 28	93.8	*	*	0	16	0	
8/24/2024	18:00	1	2	8	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	16	22.4	25.5	16 to 26	87.5	*	*	0	16	0	
8/24/2024	19:00	0	1	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	21.4	24	15 to 25	100.0	*	*	0	8	0	
8/24/2024	20:00	0	2	12	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	21	23.6	28	19 to 29	90.5	*	*	0	21	0	
8/24/2024	21:00	0	5	8	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19	21.8	24.8	17 to 27	100.0	*	*	0	19	0	
8/24/2024	22:00	0	4	4	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	12	23.2	26	18 to 28	91.7	*	*	1	11	0	
8/24/2024	23:00	0	1	7	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	23.5	24.5	17 to 27	100.0	*	*	0	11	0	
24 Hr Summary		2	30	101	45	6	0	0	0	0	0	0	0	0	0	0	0	0	0	184	22.6	26	18 to 28	90.2	*	*	3	181	0	

Outgoing Histogram
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/25/2024	00:00	0	2	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	21.7	24	15 to 25	100.0	*	*	0	9	0	
8/25/2024	01:00	0	1	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	22.3	25	15 to 25	87.5	*	*	0	8	0	
8/25/2024	02:00	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	27	28	18 to 28	100.0	*	*	0	3	0	
8/25/2024	03:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	21	21	11 to 21	100.0	*	*	0	1	0	
8/25/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	05:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0	
8/25/2024	06:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	20	20	10 to 20	100.0	*	*	0	1	0	
8/25/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	09:00	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	14	14	4 to 14	100.0	*	*	0	1	0	
8/25/2024	10:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	17	17	7 to 17	100.0	*	*	0	1	0	
8/25/2024	11:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	15	15	5 to 15	100.0	*	*	0	1	0	
8/25/2024	12:00	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	28.7	35	17 to 27	66.7	*	*	1	2	0	
8/25/2024	13:00	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	22.3	24	15 to 25	100.0	*	*	0	4	0	
8/25/2024	14:00	0	1	6	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	23.2	25	17 to 27	90.0	*	*	0	10	0	
8/25/2024	15:00	1	3	9	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	22.1	26	17 to 27	87.5	*	*	0	16	0	
8/25/2024	16:00	0	1	6	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	13	24.5	29	17 to 27	76.9	*	*	0	13	0	
8/25/2024	17:00	1	2	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	19.4	23	14 to 24	88.9	*	*	0	8	1	
8/25/2024	18:00	0	2	6	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	14	24.1	27	17 to 27	85.7	*	*	1	13	0	
8/25/2024	19:00	0	3	9	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	17	22.4	25.3	16 to 26	88.2	*	*	1	16	0	
8/25/2024	20:00	0	2	8	3	0	1	0	0	0	0	0	0	0	0	0	0	0	0	14	24.1	28.5	19 to 29	85.7	*	*	0	14	0	
8/25/2024	21:00	0	2	7	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	23.4	27.3	18 to 28	92.9	*	*	1	13	0	
8/25/2024	22:00	0	3	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	22	25	17 to 27	91.7	*	*	0	12	0	
8/25/2024	23:00	1	1	7	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	21.9	24	15 to 25	81.8	*	*	0	11	0	
24 Hr Summary		4	26	85	41	5	2	0	0	0	0	0	0	0	0	0	0	0	0	163	22.7	27	17 to 27	81.6	*	*	4	158	1	

Outgoing Histogram
 Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/26/2024	00:00	0	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	23.8	25	16 to 26	100.0	*	*	0	6	0	
8/26/2024	01:00	0	2	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	22.4	24.5	15 to 25	100.0	*	*	0	8	0	
8/26/2024	02:00	0	3	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	19.9	24	15 to 25	100.0	*	*	0	7	0	
8/26/2024	03:00	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	24.7	26	16 to 26	100.0	*	*	0	3	0	
8/26/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/26/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/26/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/26/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/26/2024	09:00	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	22	25	15 to 25	100.0	*	*	0	2	0	
8/26/2024	10:00	1	2	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	20.5	24.5	15 to 25	87.5	*	*	0	8	0	
8/26/2024	11:00	0	4	9	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	22.4	28.5	14 to 24	81.3	*	*	1	15	0	
8/26/2024	12:00	0	2	6	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	11	23.5	25	17 to 27	90.9	*	*	0	10	1	
8/26/2024	13:00	0	2	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	22.5	25	18 to 28	100.0	*	*	0	8	0	
8/26/2024	14:00	0	3	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	21	25	16 to 26	100.0	*	*	0	7	0	
8/26/2024	15:00	0	2	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	20.3	21.7	12 to 22	100.0	*	*	0	5	1	
8/26/2024	16:00	0	0	6	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	11	24.9	26	21 to 31	100.0	*	*	1	10	0	
8/26/2024	17:00	0	1	11	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	19	24.5	26.7	18 to 28	94.7	*	*	0	19	0	
8/26/2024	18:00	3	3	12	8	1	1	0	0	0	0	0	0	0	0	0	0	0	0	28	22.6	26	17 to 27	78.6	*	*	1	23	4	
8/26/2024	19:00	0	3	10	11	0	1	0	0	0	0	0	0	0	0	0	0	0	0	25	24	26	18 to 28	84.0	*	*	0	25	0	
8/26/2024	20:00	1	2	12	7	1	1	0	0	0	0	0	0	0	0	0	0	0	0	24	24.2	27	18 to 28	83.3	*	*	1	23	0	
8/26/2024	21:00	0	1	9	3	2	0	1	0	0	0	0	0	0	0	0	0	0	0	16	24.6	29.5	20 to 30	87.5	*	*	1	15	0	
8/26/2024	22:00	0	2	5	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13	23.3	26.3	17 to 27	92.3	*	*	0	13	0	
8/26/2024	23:00	0	0	6	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	23.6	25	17 to 27	100.0	*	*	0	9	0	
24 Hr Summary		5	33	112	65	7	4	1	0	0	0	0	0	0	0	0	0	0	0	227	23.2	26	17 to 27	84.6	*	*	5	216	6	

Outgoing Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/27/2024	00:00	0	0	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	24	25.5	16 to 26	100.0	*	*	0	7	0	
8/27/2024	01:00	0	0	3	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	24.2	25	19 to 29	100.0	*	*	0	6	0	
8/27/2024	02:00	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	21.2	23.5	14 to 24	100.0	*	*	0	5	0	
8/27/2024	03:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	22	22	12 to 22	100.0	*	*	0	1	0	
8/27/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	06:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	15	15	5 to 15	100.0	*	*	0	1	0	
8/27/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	09:00	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	23.7	28	18 to 28	100.0	*	*	0	3	0	
8/27/2024	10:00	0	3	6	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	11	21.9	24	15 to 25	90.9	*	*	1	9	1	
8/27/2024	11:00	0	1	7	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	11	23.9	26	18 to 28	90.9	*	*	0	10	1	
8/27/2024	12:00	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	21.7	24	14 to 24	100.0	*	*	0	3	0	
8/27/2024	13:00	0	1	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	22.1	23.5	14 to 24	100.0	*	*	0	7	1	
8/27/2024	14:00	0	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	24.2	25	17 to 27	100.0	*	*	0	6	0	
8/27/2024	15:00	0	1	4	5	0	1	0	0	0	0	0	0	0	0	0	0	0	0	11	24.9	28	19 to 29	81.8	*	*	0	10	1	
8/27/2024	16:00	0	0	7	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	10	24.8	25	21 to 31	90.0	*	*	1	9	0	
8/27/2024	17:00	1	0	7	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	23	26	17 to 27	91.7	*	*	0	12	0	
8/27/2024	18:00	0	1	9	3	1	1	0	0	0	0	0	0	0	0	0	0	0	0	15	24.6	29	16 to 26	80.0	*	*	2	12	1	
8/27/2024	19:00	0	3	10	7	3	0	0	0	0	0	0	0	0	0	0	0	0	0	23	23.9	28	21 to 31	82.6	*	*	1	22	0	
8/27/2024	20:00	0	2	9	12	0	0	0	0	0	0	0	0	0	0	0	0	0	0	23	24.4	27	19 to 29	100.0	*	*	0	23	0	
8/27/2024	21:00	0	1	7	6	2	0	0	0	0	0	0	0	0	0	0	0	0	0	16	24.7	28	18 to 28	87.5	*	*	0	16	0	
8/27/2024	22:00	0	2	4	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	23.5	25	16 to 26	91.7	*	*	0	12	0	
8/27/2024	23:00	0	0	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	23.2	24	19 to 29	100.0	*	*	0	9	0	
24 Hr Summary		1	19	105	56	10	2	0	0	0	0	0	0	0	0	0	0	0	0	193	23.7	27	18 to 28	86.0	*	*	5	183	5	

Outgoing Histogram

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/28/2024	00:00	0	0	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	23	26	17 to 27	100.0	*	*	0	8	0	
8/28/2024	01:00	0	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	23.8	26	17 to 27	100.0	*	*	0	6	0	
8/28/2024	02:00	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26	26	17 to 27	100.0	*	*	0	4	0	
8/28/2024	03:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24	25	15 to 25	100.0	*	*	0	2	0	
8/28/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/28/2024	06:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	1	0	
8/28/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/28/2024	08:00	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	20.3	22	12 to 22	100.0	*	*	0	3	0	
8/28/2024	09:00	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	22.3	23	16 to 26	100.0	*	*	0	4	0	
8/28/2024	10:00	1	1	6	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	13	22.7	27	18 to 28	84.6	*	*	0	11	2	
8/28/2024	11:00	0	2	4	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	22	26	17 to 27	100.0	*	*	0	8	0	
8/28/2024	12:00	0	1	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	11	22.8	23.8	15 to 25	100.0	*	*	0	11	0	
8/28/2024	13:00	0	1	4	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	7	23.7	26	16 to 26	85.7	*	*	0	6	1	
8/28/2024	14:00	0	4	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	8	21.1	25.5	16 to 26	100.0	*	*	0	8	0	
8/28/2024	15:00	0	0	5	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	23.7	24.8	16 to 26	100.0	*	*	0	10	0	
8/28/2024	16:00	0	0	7	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	24.1	25	18 to 28	100.0	*	*	0	9	1	
8/28/2024	17:00	0	1	8	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	14	23.1	25	17 to 27	100.0	*	*	0	14	0	
8/28/2024	18:00	0	1	10	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	15	23.3	26	18 to 28	100.0	*	*	0	14	1	
8/28/2024	19:00	1	3	8	9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	21	23.5	27.3	18 to 28	90.5	*	*	0	21	0	
8/28/2024	20:00	0	1	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	7	22.3	23	15 to 25	100.0	*	*	0	7	0	
8/28/2024	21:00	0	0	9	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	24.3	27.5	18 to 28	93.8	*	*	0	15	1	
8/28/2024	22:00	0	3	9	4	1	1	0	0	0	0	0	0	0	0	0	0	0	0	18	23.8	26	16 to 26	83.3	*	*	1	17	0	
8/28/2024	23:00	0	3	4	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	12	23.7	27	17 to 27	75.0	*	*	0	11	1	
24 Hr Summary		2	23	104	63	5	1	0	0	0	0	0	0	0	0	0	0	0	0	198	23.3	27	18 to 28	91.4	*	*	1	190	7	

Outgoing Histogram

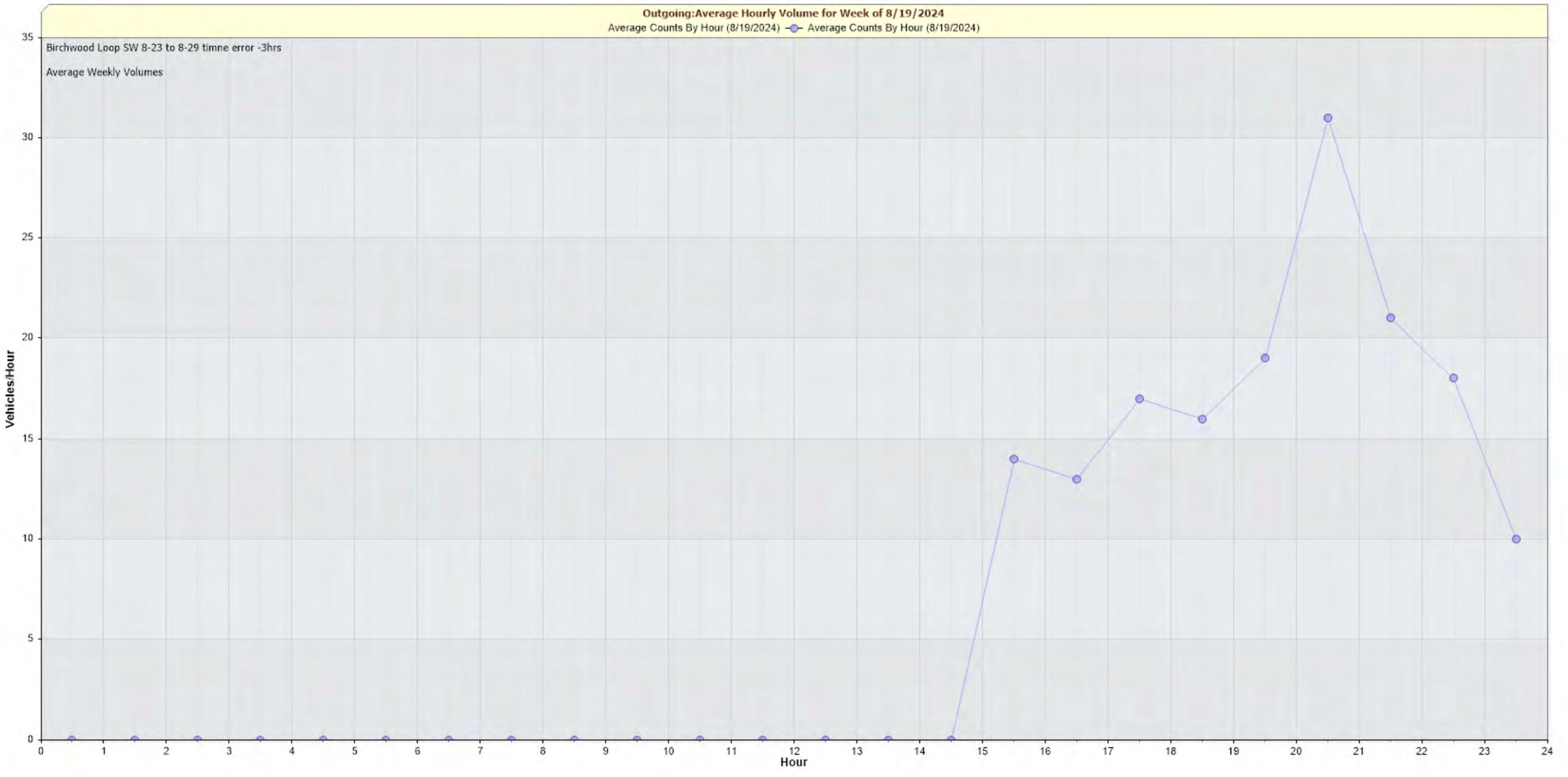
Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-12-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	0	1	2	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	8	25.6	31	22 to 32	87.5	*	*	1	7	0	
8/29/2024	01:00	0	1	2	5	1	0	0	0	0	0	0	0	0	0	0	0	0	0	9	25.2	29	19 to 29	77.8	*	*	0	9	0	
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	24	24	14 to 24	100.0	*	*	0	1	0	
8/29/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0	
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	08:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	22	22	12 to 22	100.0	*	*	0	2	0	
8/29/2024	09:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	22	22	12 to 22	100.0	*	*	0	1	0	
8/29/2024	10:00	3	2	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	12	21	26	18 to 28	66.7	*	*	2	10	0	
8/29/2024	11:00	0	4	2	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	22.8	25	18 to 28	100.0	*	*	0	10	0	
8/29/2024	12:00	0	2	9	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	14	22.8	25	18 to 28	92.9	*	*	0	14	0	
8/29/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/29/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		3	10	21	20	4	0	0	0	0	0	0	0	0	0	0	0	0	0	58	23.3	28	18 to 28	82.8	*	*	3	55	0	

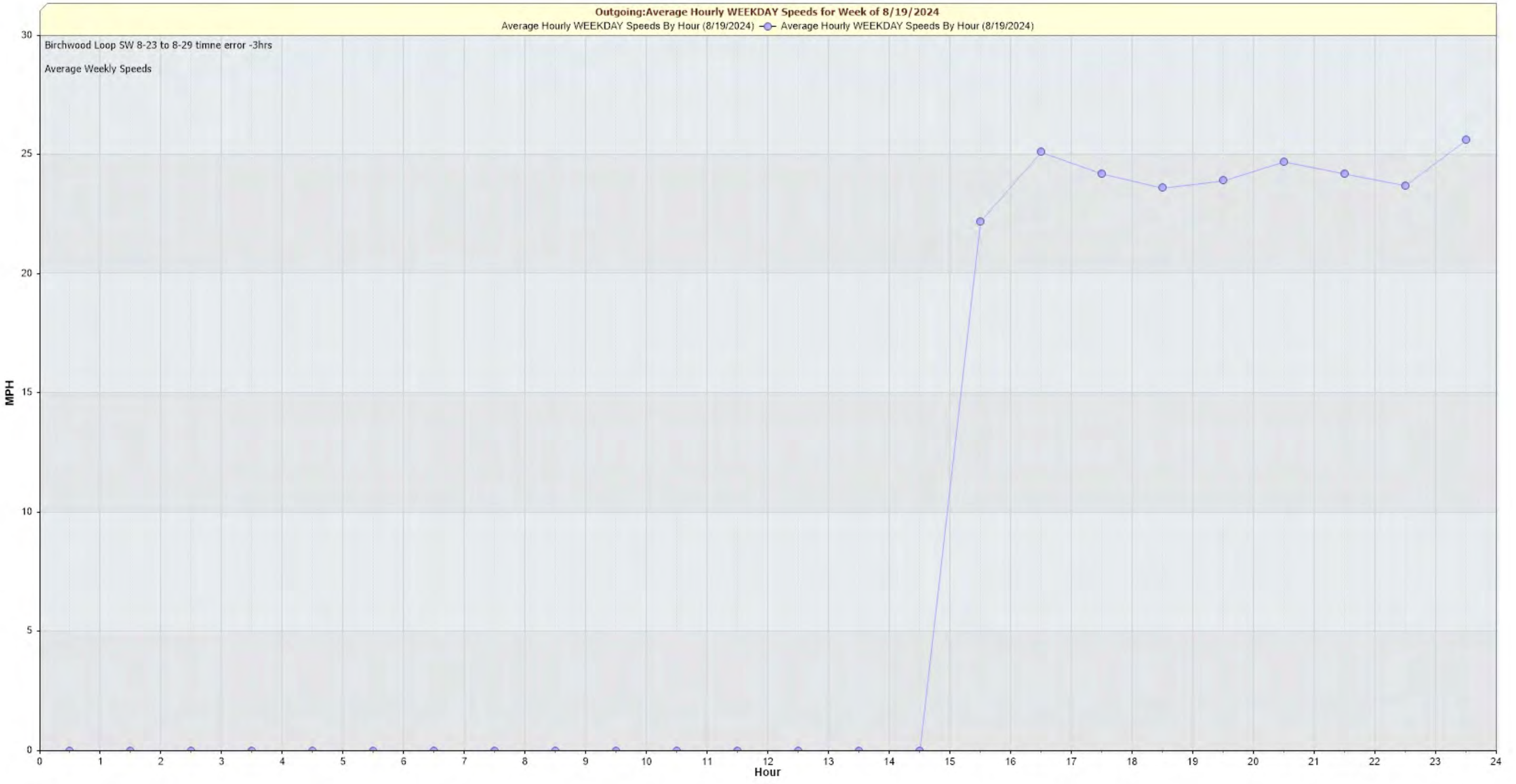
Outgoing: Average Hourly Volume for Week of 8/19/2024
Average Counts By Hour (8/19/2024) — Average Counts By Hour (8/19/2024)

Birchwood Loop SW 8-23 to 8-29 time error -3hrs
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 8/19/2024
Average Hourly WEEKDAY Speeds By Hour (8/19/2024) — Average Hourly WEEKDAY Speeds By Hour (8/19/2024)

Birchwood Loop SW 8-23 to 8-29 time error -3hrs
Average Weekly Speeds



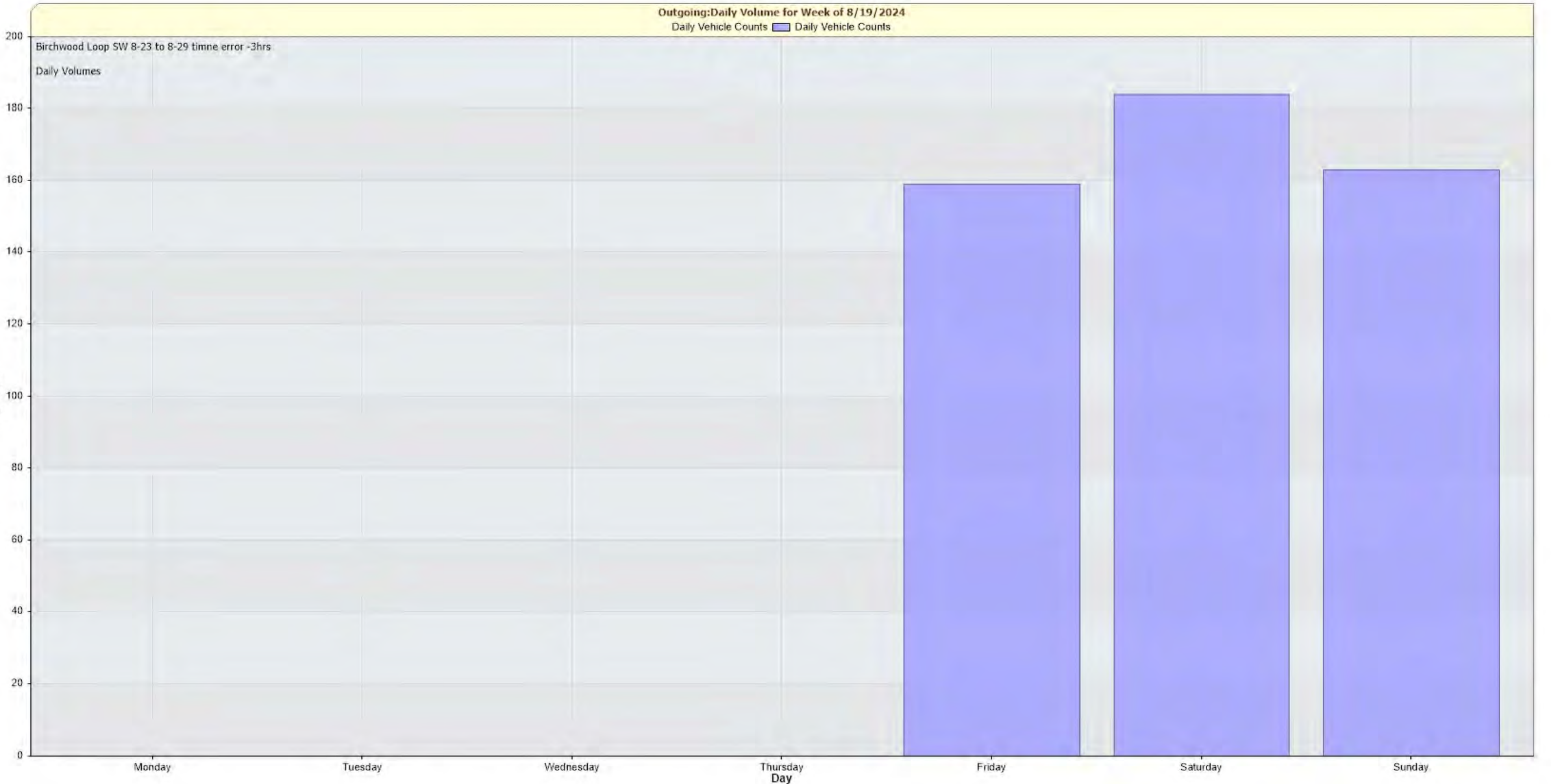
Outgoing:Daily Volume for Week of 8/19/2024

Daily Vehicle Counts

Birchwood Loop SW 8-23 to 8-29 timne error -3hrs

Daily Volumes

Vehicles



For Project: Birchwood Spur 8-23 to 8-29 time error -4hrs
 Project Notes:
 Location/Name: Incoming
 Report Generated: 10/3/2024 3:28:40 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 8/23/2024 3:00:00 PM through 8/29/2024 1:59:59 PM
 85th Percentile Speed: 48 MPH
 85th Percentile Vehicles: 2869
 Max Speed: 61 MPH on 8/26/2024 6:10:01 PM
 Total Vehicles: 3375
 AADT: 566

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	481	482
AM Peak	12:00 AM 36	30
PM Peak	8:00 PM 65	57

Speed

Speed Limit: 45
 85th Percentile Speed: 48
 50th Percentile Speed: 42
 10 MPH Pace Interval: 38.0 MPH to 48.0 MPH
 Average Speed: 41.96

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	159	137	149	38	130	122	165
% over limit	27.9	24.8	22.4	20.7	29.8	26.6	32.4
Avg Speeder	48.4	49.1	48.9	48.6	48.5	48.5	48.8
Avg Speed	41.7	41.9	41.6	40.5	42.8	41.7	42.8

Class Counts

	Number	%
VEH_SM	3	0.1
VEH_MED	3213	95.2
VEH_LG	159	4.7
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/23/2024 4:00:00 PM	44.0	8	9	49	49.0	11.1%	42.6
8/23/2024 5:00:00 PM	47.0	46	54	56	48.8	29.6%	42.7
8/23/2024 6:00:00 PM	49.0	40	47	55	48.9	34.0%	43.1
8/23/2024 7:00:00 PM	48.0	53	62	57	50.6	22.6%	41.8
8/23/2024 8:00:00 PM	47.0	68	80	57	47.7	30.0%	42.9
8/23/2024 9:00:00 PM	47.0	51	60	51	47.9	30.0%	42.7
8/23/2024 10:00:00 PM	48.0	47	55	59	48.4	40.0%	44.1
8/23/2024 11:00:00 PM	48.0	38	45	54	48.5	31.1%	42.7
8/24/2024 12:00:00 AM	46.0	20	24	50	47.2	20.8%	42.0
8/24/2024 1:00:00 AM	49.0	16	19	52	48.7	47.4%	43.7
8/24/2024 2:00:00 AM	45.0	16	19	49	47.3	15.8%	37.8
8/24/2024 3:00:00 AM	44.0	11	13	51	48.5	15.4%	39.6
8/24/2024 4:00:00 AM	43.0	5	6	48	48.0	16.7%	35.5
8/24/2024 5:00:00 AM	47.0	1	1	47	47.0	100.0%	47.0
8/24/2024 6:00:00 AM	42.0	2	2	42	0.0	0.0%	41.5
8/24/2024 7:00:00 AM	40.0	1	1	40	0.0	0.0%	40.0
8/24/2024 8:00:00 AM	37.0	1	1	37	0.0	0.0%	37.0
8/24/2024 9:00:00 AM	**No Data**						
8/24/2024 10:00:00 AM	21.0	1	1	21	0.0	0.0%	21.0
8/24/2024 11:00:00 AM	47.0	5	6	48	47.0	50.0%	41.8
8/24/2024 12:00:00 PM	44.0	8	9	44	0.0	0.0%	39.3
8/24/2024 1:00:00 PM	47.0	15	18	51	47.6	44.4%	41.8
8/24/2024 2:00:00 PM	47.0	16	19	50	48.3	21.1%	41.6
8/24/2024 3:00:00 PM	48.0	26	30	52	47.7	43.3%	43.3
8/24/2024 4:00:00 PM	47.0	33	39	51	47.9	30.8%	41.0
8/24/2024 5:00:00 PM	49.0	31	37	60	49.8	32.4%	43.6
8/24/2024 6:00:00 PM	44.0	31	36	52	49.0	13.9%	40.6
8/24/2024 7:00:00 PM	47.0	43	51	57	48.8	23.5%	42.4
8/24/2024 8:00:00 PM	47.0	37	43	51	47.8	32.6%	42.6
8/24/2024 9:00:00 PM	45.0	33	39	53	48.8	15.4%	41.9
8/24/2024 10:00:00 PM	47.0	29	34	53	48.8	17.6%	41.6
8/24/2024 11:00:00 PM	51.0	14	17	55	51.5	35.3%	43.5
8/25/2024 12:00:00 AM	48.0	14	17	50	48.8	29.4%	40.8
8/25/2024 1:00:00 AM	45.0	15	18	48	47.3	16.7%	41.2
8/25/2024 2:00:00 AM	46.0	8	10	47	46.7	30.0%	41.9
8/25/2024 3:00:00 AM	49.0	13	15	53	50.2	33.3%	42.9
8/25/2024 4:00:00 AM	49.0	1	1	49	49.0	100.0%	49.0
8/25/2024 5:00:00 AM	56.0	2	2	56	56.0	50.0%	44.0
8/25/2024 6:00:00 AM	37.0	2	2	37	0.0	0.0%	36.0
8/25/2024 7:00:00 AM	36.0	1	1	36	0.0	0.0%	36.0
8/25/2024 8:00:00 AM	46.0	2	2	46	46.0	50.0%	41.5
8/25/2024 9:00:00 AM	37.0	1	1	37	0.0	0.0%	37.0
8/25/2024 10:00:00 AM	**No Data**						
8/25/2024 11:00:00 AM	45.0	4	5	46	46.0	20.0%	41.2
8/25/2024 12:00:00 PM	46.0	4	5	51	48.5	40.0%	42.2
8/25/2024 1:00:00 PM	48.0	10	12	55	48.5	50.0%	44.7
8/25/2024 2:00:00 PM	46.0	15	18	50	47.3	22.2%	42.0

Incoming Summary
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

8/25/2024 3:00:00 PM	51.0	19	22	54	49.2	40.9%	45.0
8/25/2024 4:00:00 PM	49.0	33	39	57	49.3	33.3%	43.9
8/25/2024 5:00:00 PM	51.0	26	30	57	49.9	43.3%	43.3
8/25/2024 6:00:00 PM	49.0	40	47	55	49.5	38.3%	43.5
8/25/2024 7:00:00 PM	48.0	37	44	53	48.5	31.8%	42.5
8/25/2024 8:00:00 PM	47.0	49	58	53	48.3	24.1%	41.9
8/25/2024 9:00:00 PM	47.0	37	44	54	48.2	34.1%	42.4
8/25/2024 10:00:00 PM	48.0	52	61	55	48.6	31.1%	42.8
8/25/2024 11:00:00 PM	49.0	31	37	55	48.8	43.2%	44.1
8/26/2024 12:00:00 AM	47.0	31	36	51	49.0	19.4%	41.5
8/26/2024 1:00:00 AM	45.0	17	20	52	50.3	15.0%	42.1
8/26/2024 2:00:00 AM	45.0	12	14	47	47.0	14.3%	41.9
8/26/2024 3:00:00 AM	47.0	9	11	50	47.8	54.5%	43.6
8/26/2024 4:00:00 AM	**No Data**						
8/26/2024 5:00:00 AM	43.0	2	2	43	0.0	0.0%	33.0
8/26/2024 6:00:00 AM	27.0	2	2	27	0.0	0.0%	20.0
8/26/2024 7:00:00 AM	30.0	1	1	30	0.0	0.0%	30.0
8/26/2024 8:00:00 AM	**No Data**						
8/26/2024 9:00:00 AM	36.0	1	1	36	0.0	0.0%	36.0
8/26/2024 10:00:00 AM	47.0	4	5	54	50.5	40.0%	45.4
8/26/2024 11:00:00 AM	47.0	8	10	51	48.8	40.0%	43.2
8/26/2024 12:00:00 PM	46.0	14	17	52	48.3	23.5%	41.1
8/26/2024 1:00:00 PM	51.0	21	25	55	50.8	44.0%	42.5
8/26/2024 2:00:00 PM	46.0	35	41	49	47.4	17.1%	40.9
8/26/2024 3:00:00 PM	47.0	26	30	52	47.5	36.7%	41.1
8/26/2024 4:00:00 PM	47.0	37	44	52	48.5	22.7%	41.1
8/26/2024 5:00:00 PM	46.0	30	35	52	48.1	20.0%	40.9
8/26/2024 6:00:00 PM	48.0	40	47	55	48.3	31.9%	41.2
8/26/2024 7:00:00 PM	45.0	40	47	61	50.0	14.9%	41.3
8/26/2024 8:00:00 PM	47.0	49	58	59	49.0	24.1%	42.3
8/26/2024 9:00:00 PM	47.0	48	57	51	48.0	29.8%	42.3
8/26/2024 10:00:00 PM	48.0	45	53	55	47.7	39.6%	42.7
8/26/2024 11:00:00 PM	47.0	30	35	53	47.8	37.1%	42.4
8/27/2024 12:00:00 AM	47.0	13	15	51	47.8	33.3%	43.2
8/27/2024 1:00:00 AM	45.0	14	17	49	48.3	17.6%	40.3
8/27/2024 2:00:00 AM	41.0	14	17	44	0.0	0.0%	40.1
8/27/2024 3:00:00 AM	48.0	10	12	51	50.0	25.0%	43.4
8/27/2024 4:00:00 AM	49.0	2	2	49	48.0	100.0%	48.0
8/27/2024 5:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
8/27/2024 6:00:00 AM	37.0	2	2	37	0.0	0.0%	35.5
8/27/2024 7:00:00 AM	39.0	2	2	39	0.0	0.0%	36.5
8/27/2024 8:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
8/27/2024 9:00:00 AM	42.0	2	2	42	0.0	0.0%	41.0
8/27/2024 10:00:00 AM	49.0	6	7	53	51.0	28.6%	40.0
8/27/2024 11:00:00 AM	45.0	8	10	45	0.0	0.0%	42.2
8/27/2024 12:00:00 PM	52.0	20	23	56	51.5	34.8%	41.9
8/27/2024 1:00:00 PM	48.0	20	23	53	48.8	26.1%	40.9
8/27/2024 2:00:00 PM	45.0	13	15	52	50.5	13.3%	40.1

Incoming Summary
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

8/27/2024 3:00:00 PM	43.0	28	33	48	47.5	6.1%	40.4
8/27/2024 4:00:00 PM	48.0	42	49	57	48.5	36.7%	43.5
8/27/2024 5:00:00 PM	47.0	31	37	50	48.5	21.6%	41.4
8/27/2024 6:00:00 PM	46.0	36	42	56	48.9	21.4%	41.8
8/27/2024 7:00:00 PM	45.0	26	31	51	49.2	16.1%	42.2
8/27/2024 8:00:00 PM	51.0	37	44	55	50.1	40.9%	44.0
8/27/2024 9:00:00 PM	47.0	65	76	55	48.8	25.0%	41.3
8/27/2024 10:00:00 PM	50.0	35	41	55	49.2	41.5%	43.9
8/27/2024 11:00:00 PM	48.0	38	45	53	48.3	26.7%	42.2
8/28/2024 12:00:00 AM	44.0	18	21	49	47.3	14.3%	41.2
8/28/2024 1:00:00 AM	46.0	57	67	50	47.5	17.9%	42.3
8/28/2024 2:00:00 AM	45.0	32	38	51	48.2	15.8%	41.8
8/28/2024 3:00:00 AM	47.0	5	6	52	48.3	50.0%	44.0
8/28/2024 4:00:00 AM	**No Data**						
8/28/2024 5:00:00 AM	**No Data**						
8/28/2024 6:00:00 AM	43.0	2	2	43	0.0	0.0%	40.5
8/28/2024 7:00:00 AM	**No Data**						
8/28/2024 8:00:00 AM	39.0	3	3	39	0.0	0.0%	36.0
8/28/2024 9:00:00 AM	**No Data**						
8/28/2024 10:00:00 AM	47.0	7	8	49	48.0	25.0%	38.8
8/28/2024 11:00:00 AM	49.0	8	9	51	50.0	22.2%	43.3
8/28/2024 12:00:00 PM	53.0	17	20	56	52.1	35.0%	42.8
8/28/2024 1:00:00 PM	46.0	24	28	57	48.7	25.0%	41.4
8/28/2024 2:00:00 PM	46.0	20	24	56	50.4	20.8%	41.3
8/28/2024 3:00:00 PM	46.0	21	25	47	46.8	20.0%	41.2
8/28/2024 4:00:00 PM	45.0	36	42	53	48.0	14.3%	39.1
8/28/2024 5:00:00 PM	46.0	27	32	57	49.8	18.8%	40.4
8/28/2024 6:00:00 PM	45.0	41	48	51	48.9	14.6%	40.3
8/28/2024 7:00:00 PM	47.0	47	55	57	49.9	21.8%	41.2
8/28/2024 8:00:00 PM	47.0	34	40	52	48.5	25.0%	41.9
8/28/2024 9:00:00 PM	48.0	58	68	54	49.4	26.5%	42.9
8/28/2024 10:00:00 PM	48.0	49	58	54	48.3	37.9%	43.5
8/28/2024 11:00:00 PM	47.0	35	41	52	48.6	22.0%	41.6
8/29/2024 12:00:00 AM	47.0	42	50	51	48.8	20.0%	41.3
8/29/2024 1:00:00 AM	47.0	37	43	54	48.0	23.3%	41.9
8/29/2024 2:00:00 AM	47.0	21	25	50	47.7	28.0%	42.4
8/29/2024 3:00:00 AM	42.0	10	12	53	53.0	8.3%	39.5
8/29/2024 4:00:00 AM	39.0	3	4	50	50.0	25.0%	39.8
8/29/2024 5:00:00 AM	55.0	2	2	55	55.0	50.0%	46.5
8/29/2024 6:00:00 AM	43.0	1	1	43	0.0	0.0%	43.0
8/29/2024 7:00:00 AM	**No Data**						
8/29/2024 8:00:00 AM	**No Data**						
8/29/2024 9:00:00 AM	39.0	1	1	39	0.0	0.0%	39.0
8/29/2024 10:00:00 AM	41.0	5	6	42	0.0	0.0%	35.8
8/29/2024 11:00:00 AM	49.0	8	10	49	48.3	40.0%	44.0
8/29/2024 12:00:00 PM	49.0	18	21	51	48.6	38.1%	41.9
8/29/2024 1:00:00 PM	45.0	30	35	50	48.0	11.4%	38.5
8/29/2024 2:00:00 PM	43.0	20	24	52	49.5	8.3%	37.2

Incoming Summary
Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/24/2024 12:00:00 AM	48.0	371	436	59	48.5	29.8%	42.8
8/25/2024 12:00:00 AM	47.0	389	458	60	48.5	26.6%	41.7
8/26/2024 12:00:00 AM	48.0	434	510	57	48.8	32.4%	42.8
8/27/2024 12:00:00 AM	47.0	484	570	61	48.4	27.9%	41.7
8/28/2024 12:00:00 AM	48.0	470	553	57	49.1	24.8%	41.9
8/29/2024 12:00:00 AM	47.0	564	664	57	48.9	22.4%	41.6
8/29/2024 1:59:59 PM	47.0	156	184	55	48.6	20.7%	40.5

Incoming Weekly Counts
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	19	18	0	18.5	0
1 - 2	*	*	*	*	*	19	10	0	14.5	0
2 - 3	*	*	*	*	*	13	15	0	14	0
3 - 4	*	*	*	*	*	6	1	0	3.5	0
4 - 5	*	*	*	*	*	1	2	0	1.5	0
5 - 6	*	*	*	*	*	2	2	0	2	0
6 - 7	*	*	*	*	*	1	1	0	1	0
7 - 8	*	*	*	*	*	1	2	0	1.5	0
8 - 9	*	*	*	*	*	0	1	0	0.5	0
9 - 10	*	*	*	*	*	1	0	0	0.5	0
10 - 11	*	*	*	*	*	6	5	0	5.5	0
11 - 12	*	*	*	*	*	9	5	0	7	0
12 - 13	*	*	*	*	*	18	12	0	15	0
13 - 14	*	*	*	*	*	19	18	0	18.5	0
14 - 15	*	*	*	*	*	30	22	0	26	0
15 - 16	*	*	*	*	9	39	39	9	39	44
16 - 17	*	*	*	*	54	37	30	54	33.5	47
17 - 18	*	*	*	*	47	36	47	47	41.5	48.5
18 - 19	*	*	*	*	62	51	44	62	47.5	47.7
19 - 20	*	*	*	*	80	43	58	80	50.5	46.3
20 - 21	*	*	*	*	60	39	44	60	41.5	47
21 - 22	*	*	*	*	55	34	61	55	47.5	48
22 - 23	*	*	*	*	45	17	37	45	27	47.3
23 - 24	*	*	*	*	24	17	36	24	26.5	45.3
Totals	0	0	0	0	436	458	510			
% of Total	0%	0%	0%	0%	31.05%	32.62%	36.32%			

Incoming Weekly Counts

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	20	17	67	43	*	*	*	36.75	0	45.42
1 - 2	14	17	38	25	*	*	*	23.5	0	44.42
2 - 3	11	12	6	12	*	*	*	10.25	0	46
3 - 4	0	2	0	4	*	*	*	1.5	0	44
4 - 5	2	1	0	2	*	*	*	1.25	0	41
5 - 6	2	2	2	1	*	*	*	1.75	0	37.5
6 - 7	1	2	0	0	*	*	*	0.75	0	34.5
7 - 8	0	1	3	0	*	*	*	1	0	37
8 - 9	1	2	0	1	*	*	*	1	0	39
9 - 10	5	7	8	6	*	*	*	6.5	0	46
10 - 11	10	10	9	10	*	*	*	9.75	0	47.15
11 - 12	17	23	20	21	*	*	*	20.25	0	49.62
12 - 13	25	23	28	35	*	*	*	27.75	0	47.3
13 - 14	41	15	24	24	*	*	*	26	0	44.88
14 - 15	30	33	25	*	*	*	*	29.33	0	45.33
15 - 16	44	49	42	*	*	*	*	45	0	46.6
16 - 17	35	37	32	*	*	*	*	34.67	0	46.13
17 - 18	47	42	48	*	*	*	*	45.67	0	46.1
18 - 19	47	31	55	*	*	*	*	44.33	0	45.67
19 - 20	58	44	40	*	*	*	*	47.33	0	48
20 - 21	57	76	68	*	*	*	*	67	0	47.27
21 - 22	53	41	58	*	*	*	*	50.67	0	48.23
22 - 23	35	45	41	*	*	*	*	40.33	0	47.07
23 - 24	15	21	50	*	*	*	*	28.67	0	46
Totals	570	553	664	184	0	0	0			
% of Total	28.92%	28.06%	33.69%	9.34%	0%	0%	0%			

Incoming Monthly Counts

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	20	17	67	43	*	19	18	36.75	18.5	45.42
1 - 2	14	17	38	25	*	19	10	23.5	14.5	44.42
2 - 3	11	12	6	12	*	13	15	10.25	14	46
3 - 4	0	2	0	4	*	6	1	1.5	3.5	44
4 - 5	2	1	0	2	*	1	2	1.25	1.5	41
5 - 6	2	2	2	1	*	2	2	1.75	2	37.5
6 - 7	1	2	0	0	*	1	1	0.75	1	34.5
7 - 8	0	1	3	0	*	1	2	1	1.5	37
8 - 9	1	2	0	1	*	0	1	1	0.5	39
9 - 10	5	7	8	6	*	1	0	6.5	0.5	46
10 - 11	10	10	9	10	*	6	5	9.75	5.5	47.15
11 - 12	17	23	20	21	*	9	5	20.25	7	49.62
12 - 13	25	23	28	35	*	18	12	27.75	15	47.3
13 - 14	41	15	24	24	*	19	18	26	18.5	44.88
14 - 15	30	33	25	*	*	30	22	29.33	26	45.33
15 - 16	44	49	42	*	9	39	39	36	39	45.95
16 - 17	35	37	32	*	54	37	30	39.5	33.5	46.35
17 - 18	47	42	48	*	47	36	47	46	41.5	46.7
18 - 19	47	31	55	*	62	51	44	48.75	47.5	46.18
19 - 20	58	44	40	*	80	43	58	55.5	50.5	47.58
20 - 21	57	76	68	*	60	39	44	65.25	41.5	47.2
21 - 22	53	41	58	*	55	34	61	51.75	47.5	48.18
22 - 23	35	45	41	*	45	17	37	41.5	27	47.12
23 - 24	15	21	50	*	24	17	36	27.5	26.5	45.82
Totals	570	553	664	184	436	458	510			
% of Total	16.89%	16.39%	19.67%	5.45%	12.92%	13.57%	15.11%			

Incoming Weekly Speeds

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	43.68	41.22	0	42.49	0
1 - 2	*	*	*	*	*	37.84	41.9	0	39.24	0
2 - 3	*	*	*	*	*	39.62	42.87	0	41.36	0
3 - 4	*	*	*	*	*	35.5	49	0	37.43	0
4 - 5	*	*	*	*	*	47	44	0	45	0
5 - 6	*	*	*	*	*	41.5	36	0	38.75	0
6 - 7	*	*	*	*	*	40	36	0	38	0
7 - 8	*	*	*	*	*	37	41.5	0	40	0
8 - 9	*	*	*	*	*	0	37	0	37	0
9 - 10	*	*	*	*	*	21	0	0	21	0
10 - 11	*	*	*	*	*	41.83	41.2	0	41.55	0
11 - 12	*	*	*	*	*	39.33	42.2	0	40.36	0
12 - 13	*	*	*	*	*	41.83	44.67	0	42.97	0
13 - 14	*	*	*	*	*	41.63	42	0	41.81	0
14 - 15	*	*	*	*	*	43.3	45.05	0	44.04	0
15 - 16	*	*	*	*	42.56	40.97	43.9	42.56	42.44	44
16 - 17	*	*	*	*	42.69	43.62	43.27	42.69	43.46	47
17 - 18	*	*	*	*	43.13	40.56	43.49	43.13	42.22	48.5
18 - 19	*	*	*	*	41.84	42.37	42.45	41.84	42.41	47.7
19 - 20	*	*	*	*	42.91	42.58	41.91	42.91	42.2	46.3
20 - 21	*	*	*	*	42.68	41.87	42.43	42.68	42.17	47
21 - 22	*	*	*	*	44.07	41.59	42.79	44.07	42.36	48
22 - 23	*	*	*	*	42.69	43.47	44.14	42.69	43.93	47.3
23 - 24	*	*	*	*	41.96	40.82	41.5	41.96	41.28	45.3
Totals	0	0	0	0	42.8	41.7	42.8			
% of Total	0%	0%	0%	0%	33.62%	32.76%	33.62%			

Incoming Weekly Speeds

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	42.1	40.29	42.34	41.86	*	*	*	41.93	*	45.42
1 - 2	41.93	40.06	41.76	42.36	*	*	*	41.64	*	44.42
2 - 3	43.64	43.42	44	39.5	*	*	*	42.41	*	46
3 - 4	0	48	0	39.75	*	*	*	42.5	*	44
4 - 5	33	25	0	46.5	*	*	*	36.8	*	41
5 - 6	20	35.5	40.5	43	*	*	*	33.57	*	37.5
6 - 7	30	36.5	0	0	*	*	*	34.33	*	34.5
7 - 8	0	35	36	0	*	*	*	35.75	*	37
8 - 9	36	41	0	39	*	*	*	39.25	*	39
9 - 10	45.4	40	38.75	35.83	*	*	*	39.69	*	46
10 - 11	43.2	42.2	43.33	44	*	*	*	43.18	*	47.15
11 - 12	41.12	41.87	42.75	41.9	*	*	*	41.94	*	49.62
12 - 13	42.48	40.91	41.43	38.54	*	*	*	40.65	*	47.3
13 - 14	40.88	40.13	41.33	37.17	*	*	*	40.02	*	44.88
14 - 15	41.1	40.36	41.16	*	*	*	*	40.84	*	45.33
15 - 16	41.09	43.47	39.14	*	*	*	*	41.35	*	46.6
16 - 17	40.86	41.35	40.38	*	*	*	*	40.88	*	46.13
17 - 18	41.17	41.79	40.33	*	*	*	*	41.07	*	46.1
18 - 19	41.34	42.16	41.18	*	*	*	*	41.47	*	45.67
19 - 20	42.29	44.02	41.9	*	*	*	*	42.72	*	48
20 - 21	42.33	41.25	42.93	*	*	*	*	42.12	*	47.27
21 - 22	42.66	43.85	43.48	*	*	*	*	43.3	*	48.23
22 - 23	42.4	42.2	41.56	*	*	*	*	42.04	*	47.07
23 - 24	43.2	41.24	41.26	*	*	*	*	41.59	*	46
Totals	41.7	41.9	41.6	40.5	0	0	0			
% of Total	25.17%	25.29%	25.11%	24.44%	0%	0%	0%			

Incoming Monthly Speeds

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	42.1	40.29	42.34	41.86	*	43.68	41.22	41.93	42.49	45.42
1 - 2	41.93	40.06	41.76	42.36	*	37.84	41.9	41.64	39.24	44.42
2 - 3	43.64	43.42	44	39.5	*	39.62	42.87	42.41	41.36	46
3 - 4	0	48	0	39.75	*	35.5	49	42.5	37.43	44
4 - 5	33	25	0	46.5	*	47	44	36.8	45	41
5 - 6	20	35.5	40.5	43	*	41.5	36	33.57	38.75	37.5
6 - 7	30	36.5	0	0	*	40	36	34.33	38	34.5
7 - 8	0	35	36	0	*	37	41.5	35.75	40	37
8 - 9	36	41	0	39	*	0	37	39.25	37	39
9 - 10	45.4	40	38.75	35.83	*	21	0	39.69	21	46
10 - 11	43.2	42.2	43.33	44	*	41.83	41.2	43.18	41.55	47.15
11 - 12	41.12	41.87	42.75	41.9	*	39.33	42.2	41.94	40.36	49.62
12 - 13	42.48	40.91	41.43	38.54	*	41.83	44.67	40.65	42.97	47.3
13 - 14	40.88	40.13	41.33	37.17	*	41.63	42	40.02	41.81	44.88
14 - 15	41.1	40.36	41.16	*	*	43.3	45.05	40.84	44.04	45.33
15 - 16	41.09	43.47	39.14	*	42.56	40.97	43.9	41.42	42.44	45.95
16 - 17	40.86	41.35	40.38	*	42.69	43.62	43.27	41.5	43.46	46.35
17 - 18	41.17	41.79	40.33	*	43.13	40.56	43.49	41.59	42.22	46.7
18 - 19	41.34	42.16	41.18	*	41.84	42.37	42.45	41.58	42.41	46.18
19 - 20	42.29	44.02	41.9	*	42.91	42.58	41.91	42.79	42.2	47.58
20 - 21	42.33	41.25	42.93	*	42.68	41.87	42.43	42.25	42.17	47.2
21 - 22	42.66	43.85	43.48	*	44.07	41.59	42.79	43.5	42.36	48.18
22 - 23	42.4	42.2	41.56	*	42.69	43.47	44.14	42.22	43.93	47.12
23 - 24	43.2	41.24	41.26	*	41.96	40.82	41.5	41.67	41.28	45.82
Totals	41.7	41.9	41.6	40.5	42.8	41.7	42.8			
% of Total	14.23%	14.3%	14.2%	13.82%	14.61%	14.23%	14.61%			

Incoming Weekly EightyFifthSpeeds
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	49	45	0	47	0
1 - 2	*	*	*	*	*	45	46	0	45.5	0
2 - 3	*	*	*	*	*	44	49	0	46.5	0
3 - 4	*	*	*	*	*	43	49	0	46	0
4 - 5	*	*	*	*	*	47	56	0	51.5	0
5 - 6	*	*	*	*	*	42	37	0	39.5	0
6 - 7	*	*	*	*	*	40	36	0	38	0
7 - 8	*	*	*	*	*	37	46	0	41.5	0
8 - 9	*	*	*	*	*	0	37	0	37	0
9 - 10	*	*	*	*	*	21	0	0	21	0
10 - 11	*	*	*	*	*	47	45	0	46	0
11 - 12	*	*	*	*	*	43.5	46	0	44.75	0
12 - 13	*	*	*	*	*	46.8	47.5	0	47.15	0
13 - 14	*	*	*	*	*	46.5	45.5	0	46	0
14 - 15	*	*	*	*	*	48	51	0	49.5	0
15 - 16	*	*	*	*	44	46.8	48.5	44	47.65	44
16 - 17	*	*	*	*	47	48.3	51	47	49.65	47
17 - 18	*	*	*	*	48.5	44	49	48.5	46.5	48.5
18 - 19	*	*	*	*	47.7	47	48	47.7	47.5	47.7
19 - 20	*	*	*	*	46.3	47	46.7	46.3	46.85	46.3
20 - 21	*	*	*	*	47	45	47	47	46	47
21 - 22	*	*	*	*	48	46.3	47.5	48	46.9	48
22 - 23	*	*	*	*	47.3	51	48.5	47.3	49.75	47.3
23 - 24	*	*	*	*	45.3	48	47	45.3	47.5	45.3
Totals	0	0	0	0	421.1	1023.2	1069.2			
% of Total	0%	0%	0%	0%	16.75%	40.71%	42.54%			

Incoming Weekly EightyFifthSpeeds
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	45	45	45.5	46.2	*	*	*	45.42	0	45.42
1 - 2	45	41	45	46.7	*	*	*	44.42	0	44.42
2 - 3	47	48	47	42	*	*	*	46	0	46
3 - 4	0	49	0	39	*	*	*	44	0	44
4 - 5	43	25	0	55	*	*	*	41	0	41
5 - 6	27	37	43	43	*	*	*	37.5	0	37.5
6 - 7	30	39	0	0	*	*	*	34.5	0	34.5
7 - 8	0	35	39	0	*	*	*	37	0	37
8 - 9	36	42	0	39	*	*	*	39	0	39
9 - 10	47	49	47	41	*	*	*	46	0	46
10 - 11	47	44.3	49	48.3	*	*	*	47.15	0	47.15
11 - 12	45.5	52	52.5	48.5	*	*	*	49.62	0	49.62
12 - 13	51	47.7	46	44.5	*	*	*	47.3	0	47.3
13 - 14	46	45	45.5	43	*	*	*	44.88	0	44.88
14 - 15	47	43	46	*	*	*	*	45.33	0	45.33
15 - 16	47	47.8	45	*	*	*	*	46.6	0	46.6
16 - 17	45.7	46.7	46	*	*	*	*	46.13	0	46.13
17 - 18	47.3	46	45	*	*	*	*	46.1	0	46.1
18 - 19	45	45	47	*	*	*	*	45.67	0	45.67
19 - 20	46.8	50.5	46.7	*	*	*	*	48	0	48
20 - 21	47	47	47.8	*	*	*	*	47.27	0	47.27
21 - 22	47.2	49.5	48	*	*	*	*	48.23	0	48.23
22 - 23	47	47.2	47	*	*	*	*	47.07	0	47.07
23 - 24	47	44	47	*	*	*	*	46	0	46
Totals	976.5	1065.7	925	536.2	0	0	0			
% of Total	27.87%	30.42%	26.4%	15.31%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	45	45	45.5	46.2	*	49	45	45.42	47	45.42
1 - 2	45	41	45	46.7	*	45	46	44.42	45.5	44.42
2 - 3	47	48	47	42	*	44	49	46	46.5	46
3 - 4	0	49	0	39	*	43	49	44	46	44
4 - 5	43	25	0	55	*	47	56	41	51.5	41
5 - 6	27	37	43	43	*	42	37	37.5	39.5	37.5
6 - 7	30	39	0	0	*	40	36	34.5	38	34.5
7 - 8	0	35	39	0	*	37	46	37	41.5	37
8 - 9	36	42	0	39	*	0	37	39	37	39
9 - 10	47	49	47	41	*	21	0	46	21	46
10 - 11	47	44.3	49	48.3	*	47	45	47.15	46	47.15
11 - 12	45.5	52	52.5	48.5	*	43.5	46	49.62	44.75	49.62
12 - 13	51	47.7	46	44.5	*	46.8	47.5	47.3	47.15	47.3
13 - 14	46	45	45.5	43	*	46.5	45.5	44.88	46	44.88
14 - 15	47	43	46	*	*	48	51	45.33	49.5	45.33
15 - 16	47	47.8	45	*	44	46.8	48.5	45.95	47.65	45.95
16 - 17	45.7	46.7	46	*	47	48.3	51	46.35	49.65	46.35
17 - 18	47.3	46	45	*	48.5	44	49	46.7	46.5	46.7
18 - 19	45	45	47	*	47.7	47	48	46.18	47.5	46.18
19 - 20	46.8	50.5	46.7	*	46.3	47	46.7	47.58	46.85	47.58
20 - 21	47	47	47.8	*	47	45	47	47.2	46	47.2
21 - 22	47.2	49.5	48	*	48	46.3	47.5	48.18	46.9	48.18
22 - 23	47	47.2	47	*	47.3	51	48.5	47.12	49.75	47.12
23 - 24	47	44	47	*	45.3	48	47	45.82	47.5	45.82

Summary of Violators

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	184	42.0	40	48.2
01:00:00	123	41.1	21	47.6
02:00:00	69	42.0	20	49.2
03:00:00	13	39.8	5	48.6
04:00:00	8	39.9	3	52.7
05:00:00	11	35.5	0	0.0
06:00:00	5	35.8	0	0.0
07:00:00	7	37.6	1	46.0
08:00:00	5	38.8	0	0.0
09:00:00	27	39.0	6	49.8
10:00:00	50	42.8	14	48.2
11:00:00	95	41.7	29	50.2
12:00:00	141	41.1	42	49.0
13:00:00	141	40.5	24	48.6
14:00:00	140	42.0	40	47.9
15:00:00	222	41.8	60	48.5
16:00:00	225	42.1	62	49.2
17:00:00	267	41.8	70	48.9
18:00:00	290	41.9	64	49.5
19:00:00	323	42.6	94	48.5
20:00:00	344	42.2	93	48.5
21:00:00	302	43.1	107	48.4
22:00:00	220	42.6	70	48.7
23:00:00	163	41.5	35	48.3

Incoming Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/23/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	15:00	0	0	0	0	0	1	7	1	0	0	0	0	0	0	0	0	0	0	9	42.6	44	39 to 49	100.0	1	11.1	0	9	0	
8/23/2024	16:00	0	0	0	1	2	8	23	15	3	2	0	0	0	0	0	0	0	0	54	42.7	47	39 to 49	74.1	16	29.6	0	52	2	
8/23/2024	17:00	0	0	0	0	3	8	18	12	5	1	0	0	0	0	0	0	0	0	47	43.1	48.5	40 to 50	70.2	16	34.0	0	42	5	
8/23/2024	18:00	0	1	0	0	9	7	26	13	3	3	0	0	0	0	0	0	0	0	62	41.8	47.7	39 to 49	64.5	14	22.6	0	59	3	
8/23/2024	19:00	0	0	0	0	5	12	32	28	2	1	0	0	0	0	0	0	0	0	80	42.9	46.3	39 to 49	85.0	24	30.0	0	77	3	
8/23/2024	20:00	0	0	0	0	2	16	18	21	3	0	0	0	0	0	0	0	0	0	60	42.7	47	37 to 47	80.0	18	30.0	0	58	2	
8/23/2024	21:00	0	0	0	1	0	8	20	20	5	1	0	0	0	0	0	0	0	0	55	44.1	48	38 to 48	78.2	22	40.0	0	53	2	
8/23/2024	22:00	0	1	0	0	2	6	21	12	3	0	0	0	0	0	0	0	0	0	45	42.7	47.3	39 to 49	77.8	14	31.1	0	45	0	
8/23/2024	23:00	0	0	0	0	0	7	10	6	1	0	0	0	0	0	0	0	0	0	24	42	45.3	36 to 46	87.5	5	20.8	0	22	2	
24 Hr Summary		0	2	0	2	23	73	175	128	25	8	0	0	0	0	0	0	0	0	436	42.8	48	39 to 49	75.2	130	29.8	0	417	19	

Incoming Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/24/2024	00:00	0	0	0	0	2	3	4	7	3	0	0	0	0	0	0	0	0	0	19	43.7	49	42 to 52	68.4	9	47.4	0	19	0	
8/24/2024	01:00	0	1	0	4	0	4	4	6	0	0	0	0	0	0	0	0	0	0	19	37.8	45	35 to 45	57.9	3	15.8	0	18	1	
8/24/2024	02:00	0	0	0	1	2	2	6	1	1	0	0	0	0	0	0	0	0	0	13	39.6	44	34 to 44	69.2	2	15.4	0	13	0	
8/24/2024	03:00	0	1	0	0	1	1	2	1	0	0	0	0	0	0	0	0	0	0	6	35.5	43	30 to 40	50.0	1	16.7	0	6	0	
8/24/2024	04:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	47	47	37 to 47	100.0	1	100.0	0	1	0	
8/24/2024	05:00	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	41.5	42	32 to 42	100.0	*	*	0	2	0	
8/24/2024	06:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	40	40	30 to 40	100.0	*	*	0	1	0	
8/24/2024	07:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	37	37	27 to 37	100.0	*	*	0	1	0	
8/24/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	09:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	21	21	11 to 21	100.0	*	*	0	1	0	
8/24/2024	10:00	0	0	0	0	1	1	1	3	0	0	0	0	0	0	0	0	0	0	6	41.8	47	38 to 48	66.7	3	50.0	0	6	0	
8/24/2024	11:00	0	0	0	1	1	1	6	0	0	0	0	0	0	0	0	0	0	0	9	39.3	43.5	34 to 44	77.8	*	*	0	9	0	
8/24/2024	12:00	0	0	0	0	4	2	4	6	2	0	0	0	0	0	0	0	0	0	18	41.8	46.8	37 to 47	66.7	8	44.4	0	18	0	
8/24/2024	13:00	0	0	0	1	1	3	10	3	1	0	0	0	0	0	0	0	0	0	19	41.6	46.5	37 to 47	78.9	4	21.1	0	19	0	
8/24/2024	14:00	0	0	0	0	1	5	11	11	2	0	0	0	0	0	0	0	0	0	30	43.3	48	39 to 49	80.0	13	43.3	0	30	0	
8/24/2024	15:00	0	0	0	2	3	11	10	10	3	0	0	0	0	0	0	0	0	0	39	41	46.8	37 to 47	66.7	12	30.8	0	38	1	
8/24/2024	16:00	0	0	0	0	2	5	14	12	3	0	1	0	0	0	0	0	0	0	37	43.6	48.3	39 to 49	73.0	12	32.4	0	37	0	
8/24/2024	17:00	0	0	0	0	4	12	15	3	2	0	0	0	0	0	0	0	0	0	36	40.6	44	34 to 44	80.6	5	13.9	0	35	1	
8/24/2024	18:00	0	0	0	1	4	7	21	16	1	1	0	0	0	0	0	0	0	0	51	42.4	47	38 to 48	80.4	12	23.5	0	51	0	
8/24/2024	19:00	0	0	0	0	3	7	15	15	3	0	0	0	0	0	0	0	0	0	43	42.6	47	38 to 48	76.7	14	32.6	0	43	0	
8/24/2024	20:00	0	0	1	0	2	5	21	8	2	0	0	0	0	0	0	0	0	0	39	41.9	45	36 to 46	79.5	6	15.4	0	39	0	
8/24/2024	21:00	0	0	0	3	0	7	17	5	2	0	0	0	0	0	0	0	0	0	34	41.6	46.3	37 to 47	82.4	6	17.6	0	33	1	
8/24/2024	22:00	0	0	0	0	2	2	7	1	4	1	0	0	0	0	0	0	0	0	17	43.5	51	38 to 48	58.8	6	35.3	0	16	1	
8/24/2024	23:00	0	0	0	0	5	3	3	5	1	0	0	0	0	0	0	0	0	0	17	40.8	48	33 to 43	58.8	5	29.4	0	17	0	
24 Hr Summary		0	2	2	13	38	82	174	114	30	2	1	0	0	0	0	0	0	0	458	41.7	47	39 to 49	69.7	122	26.6	0	453	5	

Incoming Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/25/2024	00:00	0	0	0	0	2	4	7	5	0	0	0	0	0	0	0	0	0	0	18	41.2	45	38 to 48	83.3	3	16.7	0	17	1	
8/25/2024	01:00	0	0	0	0	0	3	4	3	0	0	0	0	0	0	0	0	0	0	10	41.9	46	37 to 47	90.0	3	30.0	0	10	0	
8/25/2024	02:00	0	0	0	1	0	3	5	4	2	0	0	0	0	0	0	0	0	0	15	42.9	49	35 to 45	60.0	5	33.3	0	15	0	
8/25/2024	03:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	49	49	39 to 49	100.0	1	100.0	0	1	0	
8/25/2024	04:00	0	0	0	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	2	44	56	22 to 32	50.0	1	50.0	0	2	0	
8/25/2024	05:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	36	37	27 to 37	100.0	*	*	0	2	0	
8/25/2024	06:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	36	36	26 to 36	100.0	*	*	0	1	0	
8/25/2024	07:00	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	41.5	46	36 to 46	100.0	1	50.0	0	2	0	
8/25/2024	08:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	37	37	27 to 37	100.0	*	*	0	1	0	
8/25/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	10:00	0	0	0	0	0	2	1	2	0	0	0	0	0	0	0	0	0	0	5	41.2	45	36 to 46	100.0	1	20.0	0	5	0	
8/25/2024	11:00	0	0	0	0	1	0	2	1	1	0	0	0	0	0	0	0	0	0	5	42.2	46	32 to 42	60.0	2	40.0	0	5	0	
8/25/2024	12:00	0	0	0	0	1	1	4	5	0	1	0	0	0	0	0	0	0	0	12	44.7	47.5	38 to 48	83.3	6	50.0	0	11	1	
8/25/2024	13:00	0	0	0	0	0	5	8	4	1	0	0	0	0	0	0	0	0	0	18	42	45.5	36 to 46	88.9	4	22.2	0	18	0	
8/25/2024	14:00	0	0	0	0	1	1	8	8	4	0	0	0	0	0	0	0	0	0	22	45	51	38 to 48	77.3	9	40.9	0	22	0	
8/25/2024	15:00	0	0	0	0	1	6	13	14	4	1	0	0	0	0	0	0	0	0	39	43.9	48.5	39 to 49	71.8	13	33.3	0	38	1	
8/25/2024	16:00	0	1	0	1	1	5	6	10	4	2	0	0	0	0	0	0	0	0	30	43.3	51	42 to 52	60.0	13	43.3	0	27	3	
8/25/2024	17:00	0	1	0	1	0	8	17	13	6	1	0	0	0	0	0	0	0	0	47	43.5	49	39 to 49	70.2	18	38.3	0	46	1	
8/25/2024	18:00	0	0	1	1	2	7	15	14	4	0	0	0	0	0	0	0	0	0	44	42.5	48	40 to 50	72.7	14	31.8	0	44	0	
8/25/2024	19:00	0	0	0	2	3	14	15	20	4	0	0	0	0	0	0	0	0	0	58	41.9	46.7	38 to 48	77.6	14	24.1	0	56	2	
8/25/2024	20:00	0	0	0	0	4	9	14	13	4	0	0	0	0	0	0	0	0	0	44	42.4	47	36 to 46	70.5	15	34.1	0	44	0	
8/25/2024	21:00	0	0	0	1	4	11	20	18	6	1	0	0	0	0	0	0	0	0	61	42.8	47.5	38 to 48	75.4	19	31.1	0	58	3	
8/25/2024	22:00	0	0	0	0	0	7	13	13	3	1	0	0	0	0	0	0	0	0	37	44.1	48.5	39 to 49	75.7	16	43.2	0	37	0	
8/25/2024	23:00	0	0	0	0	2	9	15	6	4	0	0	0	0	0	0	0	0	0	36	41.5	47	35 to 45	75.0	7	19.4	0	36	0	
24 Hr Summary		0	2	1	7	23	100	167	155	47	8	0	0	0	0	0	0	0	0	510	42.8	48	38 to 48	69.4	165	32.4	0	498	12	

Incoming Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/26/2024	00:00	0	0	0	0	2	4	10	2	2	0	0	0	0	0	0	0	0	0	20	42.1	45	34 to 44	75.0	3	15.0	0	20	0	
8/26/2024	01:00	0	0	0	0	0	3	8	3	0	0	0	0	0	0	0	0	0	0	14	41.9	45	37 to 47	92.9	2	14.3	0	14	0	
8/26/2024	02:00	0	0	0	0	1	2	1	5	2	0	0	0	0	0	0	0	0	0	11	43.6	47	40 to 50	72.7	6	54.5	0	10	1	
8/26/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	04:00	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	2	33	43	13 to 23	50.0	*	*	0	2	0	
8/26/2024	05:00	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	20	27	3 to 13	50.0	*	*	0	2	0	
8/26/2024	06:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	30	30	20 to 30	100.0	*	*	0	0	1	
8/26/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	08:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	36	36	26 to 36	100.0	*	*	0	1	0	
8/26/2024	09:00	0	0	0	0	0	0	3	1	1	0	0	0	0	0	0	0	0	0	5	45.4	47	37 to 47	80.0	2	40.0	0	4	1	
8/26/2024	10:00	0	0	0	0	2	1	2	3	2	0	0	0	0	0	0	0	0	0	10	43.2	47	41 to 51	70.0	4	40.0	0	10	0	
8/26/2024	11:00	0	0	0	0	6	0	4	6	1	0	0	0	0	0	0	0	0	0	17	41.1	45.5	39 to 49	58.8	4	23.5	0	15	2	
8/26/2024	12:00	0	1	0	0	4	4	4	4	7	1	0	0	0	0	0	0	0	0	25	42.5	51	42 to 52	52.0	11	44.0	0	22	3	
8/26/2024	13:00	0	0	0	1	1	12	17	10	0	0	0	0	0	0	0	0	0	0	41	40.9	46	35 to 45	78.0	7	17.1	0	37	4	
8/26/2024	14:00	1	0	0	1	4	3	7	13	1	0	0	0	0	0	0	0	0	0	30	41.1	47	38 to 48	70.0	11	36.7	0	28	2	
8/26/2024	15:00	0	0	1	1	0	14	18	7	3	0	0	0	0	0	0	0	0	0	44	41.1	47	38 to 48	77.3	10	22.7	0	39	5	
8/26/2024	16:00	0	0	1	2	2	7	13	8	2	0	0	0	0	0	0	0	0	0	35	40.9	45.7	38 to 48	77.1	7	20.0	0	33	2	
8/26/2024	17:00	0	0	1	2	4	12	10	16	1	1	0	0	0	0	0	0	0	0	47	41.2	47.3	36 to 46	61.7	15	31.9	0	44	3	
8/26/2024	18:00	0	0	0	1	4	12	18	10	1	0	1	0	0	0	0	0	0	0	47	41.3	45	35 to 45	74.5	7	14.9	0	46	1	
8/26/2024	19:00	0	0	0	3	2	12	19	19	2	1	0	0	0	0	0	0	0	0	58	42.3	46.8	37 to 47	75.9	14	24.1	0	58	0	
8/26/2024	20:00	0	0	0	0	5	11	22	14	5	0	0	0	0	0	0	0	0	0	57	42.3	47	38 to 48	75.4	17	29.8	0	56	1	
8/26/2024	21:00	0	1	0	0	2	12	14	21	2	1	0	0	0	0	0	0	0	0	53	42.7	47.2	38 to 48	75.5	21	39.6	0	50	3	
8/26/2024	22:00	0	0	1	0	2	7	11	12	2	0	0	0	0	0	0	0	0	0	35	42.4	47	38 to 48	77.1	13	37.1	0	35	0	
8/26/2024	23:00	0	0	0	1	0	1	5	7	1	0	0	0	0	0	0	0	0	0	15	43.2	47	38 to 48	80.0	5	33.3	0	15	0	
24 Hr Summary		2	2	5	13	42	118	187	161	35	4	1	0	0	0	0	0	0	0	570	41.7	47	38 to 48	69.8	159	27.9	0	541	29	

Incoming Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/27/2024	00:00	0	0	0	1	0	6	6	4	0	0	0	0	0	0	0	0	0	0	17	40.3	45	35 to 45	76.5	3	17.6	0	17	0
8/27/2024	01:00	0	0	0	0	0	7	10	0	0	0	0	0	0	0	0	0	0	0	17	40.1	41	34 to 44	100.0	*	*	0	17	0
8/27/2024	02:00	0	0	0	0	0	1	8	1	2	0	0	0	0	0	0	0	0	0	12	43.4	48	34 to 44	75.0	3	25.0	0	12	0
8/27/2024	03:00	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	48	49	39 to 49	100.0	2	100.0	0	2	0
8/27/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0
8/27/2024	05:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	35.5	37	27 to 37	100.0	*	*	0	2	0
8/27/2024	06:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	36.5	39	29 to 39	100.0	*	*	0	2	0
8/27/2024	07:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	1	0
8/27/2024	08:00	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	41	42	32 to 42	100.0	*	*	0	2	0
8/27/2024	09:00	0	0	0	2	0	1	2	1	1	0	0	0	0	0	0	0	0	0	7	40	49	39 to 49	57.1	2	28.6	0	6	1
8/27/2024	10:00	0	0	0	0	0	2	5	3	0	0	0	0	0	0	0	0	0	0	10	42.2	44.3	35 to 45	100.0	*	*	0	10	0
8/27/2024	11:00	0	0	0	1	5	4	5	3	4	1	0	0	0	0	0	0	0	0	23	41.9	52	30 to 40	47.8	8	34.8	0	18	5
8/27/2024	12:00	0	0	0	1	4	4	6	7	1	0	0	0	0	0	0	0	0	0	23	40.9	47.7	38 to 48	60.9	6	26.1	0	19	4
8/27/2024	13:00	0	0	0	2	1	3	5	3	1	0	0	0	0	0	0	0	0	0	15	40.1	45	35 to 45	66.7	2	13.3	0	12	3
8/27/2024	14:00	0	0	0	0	5	7	18	3	0	0	0	0	0	0	0	0	0	0	33	40.4	43	34 to 44	87.9	2	6.1	1	29	3
8/27/2024	15:00	0	0	0	0	2	6	18	19	3	1	0	0	0	0	0	0	0	0	49	43.5	47.8	38 to 48	79.6	18	36.7	0	44	5
8/27/2024	16:00	0	0	0	1	6	6	13	9	2	0	0	0	0	0	0	0	0	0	37	41.4	46.7	37 to 47	67.6	8	21.6	1	35	1
8/27/2024	17:00	0	0	0	1	3	7	21	7	2	1	0	0	0	0	0	0	0	0	42	41.8	46	36 to 46	71.4	9	21.4	0	40	2
8/27/2024	18:00	0	0	0	0	2	8	11	7	3	0	0	0	0	0	0	0	0	0	31	42.2	45	35 to 45	77.4	5	16.1	0	30	1
8/27/2024	19:00	0	0	0	0	2	8	13	12	7	2	0	0	0	0	0	0	0	0	44	44	50.5	38 to 48	59.1	18	40.9	0	42	2
8/27/2024	20:00	0	0	1	2	10	12	27	18	5	1	0	0	0	0	0	0	0	0	76	41.3	47	37 to 47	64.5	19	25.0	0	73	3
8/27/2024	21:00	0	0	0	0	3	6	13	12	6	1	0	0	0	0	0	0	0	0	41	43.9	49.5	40 to 50	65.9	17	41.5	0	40	1
8/27/2024	22:00	0	0	0	0	4	8	21	9	3	0	0	0	0	0	0	0	0	0	45	42.2	47.2	38 to 48	73.3	12	26.7	0	43	2
8/27/2024	23:00	0	0	0	0	1	4	13	3	0	0	0	0	0	0	0	0	0	0	21	41.2	44	34 to 44	85.7	3	14.3	0	20	1
24 Hr Summary		0	0	1	12	50	103	217	123	40	7	0	0	0	0	0	0	0	0	553	41.9	48	39 to 49	67.1	137	24.8	2	517	34

Incoming Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/28/2024	00:00	0	0	0	0	0	13	37	16	1	0	0	0	0	0	0	0	0	0	67	42.3	45.5	36 to 46	85.1	12	17.9	0	67	0	
8/28/2024	01:00	0	0	0	0	2	7	22	6	1	0	0	0	0	0	0	0	0	0	38	41.8	45	35 to 45	78.9	6	15.8	0	38	0	
8/28/2024	02:00	0	0	0	0	0	1	2	2	1	0	0	0	0	0	0	0	0	0	6	44	47	42 to 52	83.3	3	50.0	0	6	0	
8/28/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	05:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	40.5	43	33 to 43	100.0	*	*	0	1	1	
8/28/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	07:00	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3	36	39	29 to 39	100.0	*	*	0	2	1	
8/28/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	09:00	0	0	1	0	0	3	2	2	0	0	0	0	0	0	0	0	0	0	8	38.8	47	39 to 49	75.0	2	25.0	0	8	0	
8/28/2024	10:00	0	0	0	0	0	2	4	2	1	0	0	0	0	0	0	0	0	0	9	43.3	49	35 to 45	77.8	2	22.2	0	9	0	
8/28/2024	11:00	0	0	0	1	3	4	5	2	3	2	0	0	0	0	0	0	0	0	20	42.8	52.5	33 to 43	50.0	7	35.0	0	18	2	
8/28/2024	12:00	0	0	0	0	4	7	9	6	1	1	0	0	0	0	0	0	0	0	28	41.4	46	36 to 46	71.4	7	25.0	0	22	6	
8/28/2024	13:00	0	0	0	1	5	2	8	6	0	2	0	0	0	0	0	0	0	0	24	41.3	45.5	36 to 46	58.3	5	20.8	0	23	1	
8/28/2024	14:00	0	0	0	2	1	4	8	10	0	0	0	0	0	0	0	0	0	0	25	41.2	46	37 to 47	80.0	5	20.0	0	24	1	
8/28/2024	15:00	0	0	1	0	8	12	13	7	1	0	0	0	0	0	0	0	0	0	42	39.1	45	36 to 46	69.0	6	14.3	0	36	6	
8/28/2024	16:00	0	0	1	0	4	9	12	3	2	1	0	0	0	0	0	0	0	0	32	40.4	46	33 to 43	71.9	6	18.8	0	28	4	
8/28/2024	17:00	0	1	0	2	7	8	18	9	3	0	0	0	0	0	0	0	0	0	48	40.3	45	35 to 45	64.6	7	14.6	0	44	4	
8/28/2024	18:00	0	0	0	1	6	16	16	10	5	1	0	0	0	0	0	0	0	0	55	41.2	47	35 to 45	65.5	12	21.8	0	49	6	
8/28/2024	19:00	0	0	0	0	3	10	14	9	4	0	0	0	0	0	0	0	0	0	40	41.9	46.7	38 to 48	77.5	10	25.0	0	36	4	
8/28/2024	20:00	0	0	0	1	1	14	26	18	8	0	0	0	0	0	0	0	0	0	68	42.9	47.8	38 to 48	75.0	18	26.5	0	65	3	
8/28/2024	21:00	0	0	1	0	2	7	20	23	5	0	0	0	0	0	0	0	0	0	58	43.5	48	40 to 50	77.6	22	37.9	0	56	2	
8/28/2024	22:00	0	0	0	0	4	12	12	11	2	0	0	0	0	0	0	0	0	0	41	41.6	47	38 to 48	73.2	9	22.0	0	40	1	
8/28/2024	23:00	0	1	0	1	2	13	18	12	3	0	0	0	0	0	0	0	0	0	50	41.3	47	35 to 45	72.0	10	20.0	0	49	1	
24 Hr Summary		0	2	4	9	53	147	247	154	41	7	0	0	0	0	0	0	0	0	664	41.6	47	37 to 47	69.4	149	22.4	0	621	43	

Incoming Histogram

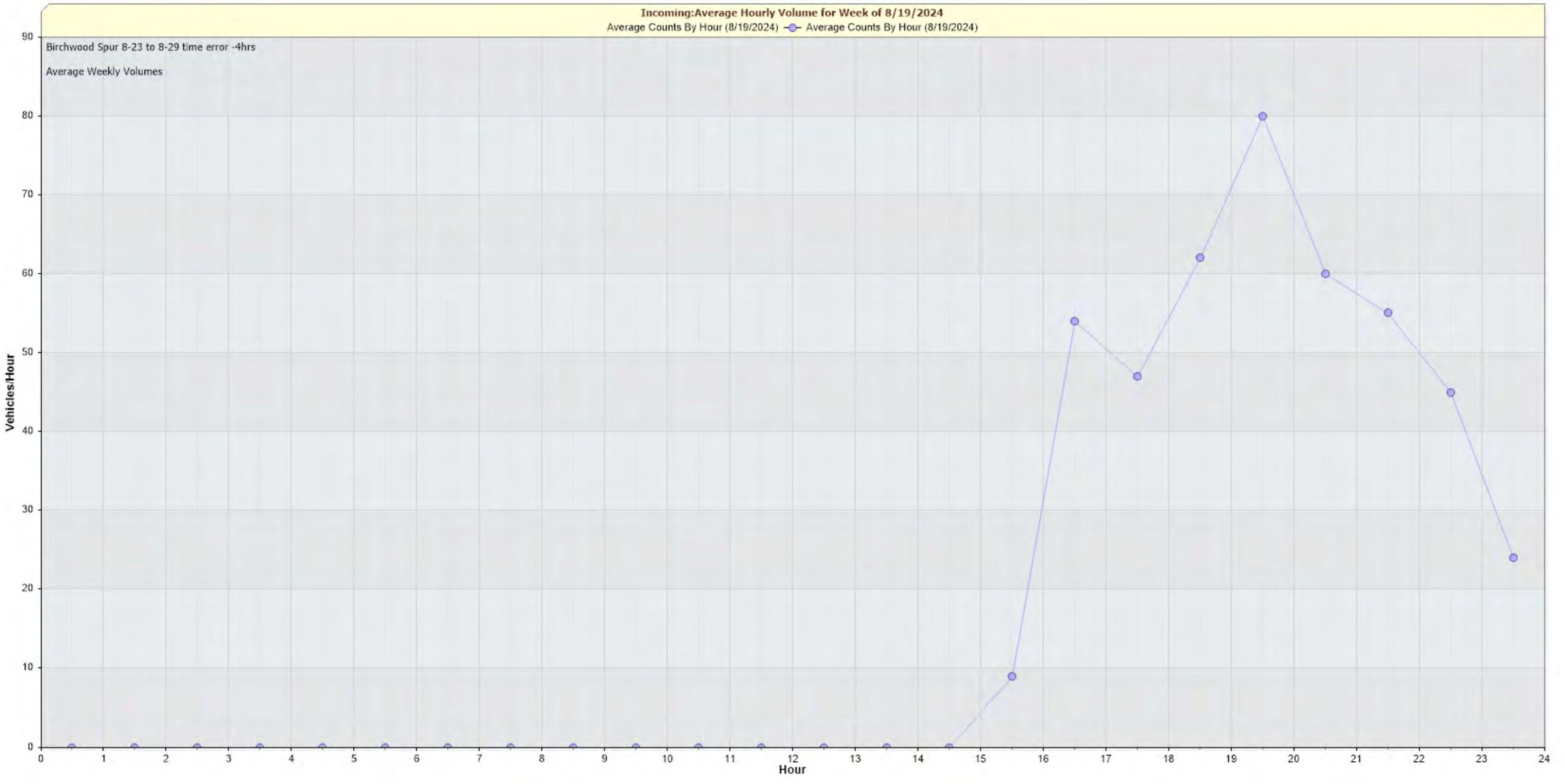
Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	0	0	0	0	2	14	12	13	2	0	0	0	0	0	0	0	0	0	43	41.9	46.2	37 to 47	79.1	10	23.3	0	42	1	
8/29/2024	01:00	0	0	0	0	1	6	9	8	1	0	0	0	0	0	0	0	0	0	25	42.4	46.7	37 to 47	80.0	7	28.0	0	25	0	
8/29/2024	02:00	0	0	0	1	1	2	7	0	1	0	0	0	0	0	0	0	0	0	12	39.5	42	32 to 42	75.0	1	8.3	0	11	1	
8/29/2024	03:00	0	0	0	0	1	2	0	0	1	0	0	0	0	0	0	0	0	0	4	39.8	39	29 to 39	75.0	1	25.0	0	4	0	
8/29/2024	04:00	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2	46.5	55	28 to 38	50.0	1	50.0	0	1	1	
8/29/2024	05:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	43	43	33 to 43	100.0	*	*	0	1	0	
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	39	39	29 to 39	100.0	*	*	0	1	0	
8/29/2024	09:00	0	0	0	2	0	1	3	0	0	0	0	0	0	0	0	0	0	0	6	35.8	41	32 to 42	66.7	*	*	0	5	1	
8/29/2024	10:00	0	0	0	0	0	2	2	6	0	0	0	0	0	0	0	0	0	0	10	44	48.3	39 to 49	80.0	4	40.0	0	10	0	
8/29/2024	11:00	0	0	0	2	1	4	5	7	2	0	0	0	0	0	0	0	0	0	21	41.9	48.5	39 to 49	61.9	8	38.1	0	17	4	
8/29/2024	12:00	0	0	0	1	11	7	10	5	1	0	0	0	0	0	0	0	0	0	35	38.5	44.5	30 to 40	60.0	4	11.4	0	29	6	
8/29/2024	13:00	0	1	0	2	4	8	7	1	1	0	0	0	0	0	0	0	0	0	24	37.2	43	34 to 44	75.0	2	8.3	1	20	3	
8/29/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	1	0	8	21	48	56	40	9	1	0	0	0	0	0	0	0	0	184	40.5	47	37 to 47	63.6	38	20.7	1	166	17	

Incoming: Average Hourly Volume for Week of 8/19/2024
Average Counts By Hour (8/19/2024) — Average Counts By Hour (8/19/2024)

Birchwood Spur 8-23 to 8-29 time error -4hrs
Average Weekly Volumes



Incoming: Average Hourly WEEKDAY Speeds for Week of 8/19/2024
Average Hourly WEEKDAY Speeds By Hour (8/19/2024) — Average Hourly WEEKDAY Speeds By Hour (8/19/2024)

Birchwood Spur 8-23 to 8-29 time error -4hrs
Average Weekly Speeds



Incoming: Daily Volume for Week of 8/19/2024

Daily Vehicle Counts

Daily Vehicle Counts

Birchwood Spur 8-23 to 8-29 time error -4hrs

Daily Volumes

Vehicles

600
500
400
300
200
100
0

Monday

Tuesday

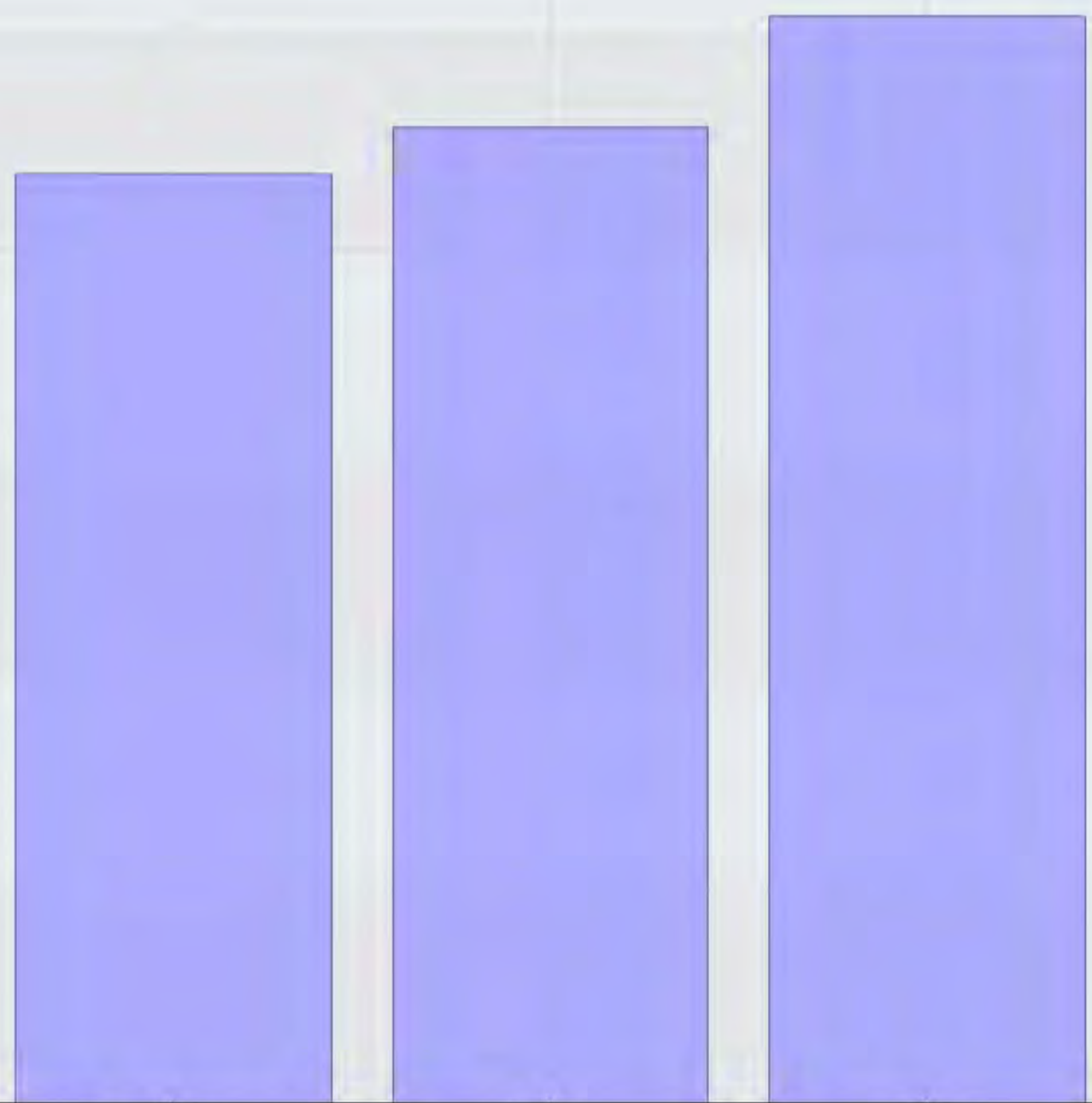
Wednesday

Thursday
Day

Friday

Saturday

Sunday



For Project: Birchwood Spur 8-23 to 8-29 time error -4hrs

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:28:40 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 8/23/2024 3:00:00 PM through 8/29/2024 1:59:59 PM

85th Percentile Speed 46 MPH

85th Percentile Vehicles 2909

Max Speed 71 MPH on 8/28/2024 2:39:33 AM

Total Vehicles 3422

AADT: 574

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	487	488
AM Peak 11:00 AM	39	32
PM Peak 6:00 PM	57	55

Speed

Speed Limit: 45

85th Percentile Speed: 46

50th Percentile Speed: 41

10 MPH Pace Interval: 36.0 MPH to 46.0 MPH

Average Speed: 40.55

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	80	90	111	22	72	80	93
% over limit	13.9	14.0	16.8	11.7	19.5	17.1	18.0
Avg Speeder	48.7	49.0	49.0	49.2	48.6	48.1	48.3
Avg Speed	40.4	40.5	40.5	39.3	41.1	40.5	41.0

Class Counts

	Number	%
VEH_SM	1	0
VEH_MED	3210	93.8
VEH_LG	211	6.2
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/23/2024 4:00:00 PM	46.0	9	11	48	47.0	27.3%	43.6
8/23/2024 5:00:00 PM	45.0	58	68	56	50.3	13.2%	40.5
8/23/2024 6:00:00 PM	47.0	34	40	54	49.3	25.0%	41.4
8/23/2024 7:00:00 PM	45.0	51	60	53	47.6	11.7%	39.8
8/23/2024 8:00:00 PM	47.0	42	50	52	47.8	32.0%	42.0
8/23/2024 9:00:00 PM	45.0	34	40	48	47.0	12.5%	40.7
8/23/2024 10:00:00 PM	49.0	39	46	54	49.3	32.6%	42.4
8/23/2024 11:00:00 PM	46.0	30	35	54	48.4	20.0%	42.0
8/24/2024 12:00:00 AM	44.0	16	19	45	0.0	0.0%	39.6
8/24/2024 1:00:00 AM	44.0	9	11	47	47.0	9.1%	39.3
8/24/2024 2:00:00 AM	44.0	9	11	48	47.0	18.2%	39.5
8/24/2024 3:00:00 AM	38.0	6	7	51	51.0	14.3%	36.4
8/24/2024 4:00:00 AM	46.0	3	3	46	46.0	33.3%	40.3
8/24/2024 5:00:00 AM	40.0	3	4	46	46.0	25.0%	40.5
8/24/2024 6:00:00 AM	45.0	2	2	45	0.0	0.0%	40.5
8/24/2024 7:00:00 AM	32.0	3	4	38	0.0	0.0%	32.3
8/24/2024 8:00:00 AM	35.0	2	2	35	0.0	0.0%	34.0
8/24/2024 9:00:00 AM	**No Data**						
8/24/2024 10:00:00 AM	42.0	1	1	42	0.0	0.0%	42.0
8/24/2024 11:00:00 AM	44.0	10	12	49	47.5	16.7%	40.3
8/24/2024 12:00:00 PM	45.0	14	16	49	48.5	12.5%	38.9
8/24/2024 1:00:00 PM	46.0	30	35	52	47.3	22.9%	42.0
8/24/2024 2:00:00 PM	46.0	25	29	54	49.4	17.2%	40.8
8/24/2024 3:00:00 PM	46.0	36	42	52	48.4	16.7%	41.2
8/24/2024 4:00:00 PM	45.0	37	43	54	48.7	14.0%	40.7
8/24/2024 5:00:00 PM	44.0	30	35	51	48.5	11.4%	40.2
8/24/2024 6:00:00 PM	47.0	28	33	50	47.9	27.3%	39.8
8/24/2024 7:00:00 PM	45.0	40	47	50	47.6	14.9%	41.0
8/24/2024 8:00:00 PM	44.0	19	22	46	46.0	9.1%	40.2
8/24/2024 9:00:00 PM	46.0	26	30	49	47.4	23.3%	42.1
8/24/2024 10:00:00 PM	47.0	28	33	53	49.0	18.2%	39.7
8/24/2024 11:00:00 PM	46.0	24	28	50	48.2	17.9%	40.6
8/25/2024 12:00:00 AM	46.0	14	17	51	49.0	23.5%	40.2
8/25/2024 1:00:00 AM	48.0	8	9	51	49.5	22.2%	40.0
8/25/2024 2:00:00 AM	49.0	2	2	49	48.5	100.0%	48.5
8/25/2024 3:00:00 AM	38.0	5	6	46	46.0	16.7%	38.2
8/25/2024 4:00:00 AM	40.0	3	3	40	0.0	0.0%	34.3
8/25/2024 5:00:00 AM	36.0	5	6	38	0.0	0.0%	32.3
8/25/2024 6:00:00 AM	42.0	3	3	42	0.0	0.0%	37.0
8/25/2024 7:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
8/25/2024 8:00:00 AM	41.0	2	2	41	0.0	0.0%	38.5
8/25/2024 9:00:00 AM	**No Data**						
8/25/2024 10:00:00 AM	**No Data**						
8/25/2024 11:00:00 AM	43.0	7	8	46	46.0	12.5%	39.6
8/25/2024 12:00:00 PM	45.0	16	19	51	48.5	10.5%	39.8
8/25/2024 1:00:00 PM	47.0	20	23	58	53.0	17.4%	42.7
8/25/2024 2:00:00 PM	47.0	18	21	51	48.8	19.0%	43.2

Outgoing Summary
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

8/25/2024 3:00:00 PM	45.0	28	33	53	48.3	12.1%	40.5
8/25/2024 4:00:00 PM	46.0	43	51	51	47.2	17.6%	41.0
8/25/2024 5:00:00 PM	45.0	37	44	48	46.6	11.4%	41.2
8/25/2024 6:00:00 PM	47.0	61	72	59	48.2	26.4%	41.2
8/25/2024 7:00:00 PM	45.0	50	59	51	47.0	10.2%	40.5
8/25/2024 8:00:00 PM	46.0	35	41	52	48.3	17.1%	40.8
8/25/2024 9:00:00 PM	46.0	33	39	53	48.0	23.1%	41.5
8/25/2024 10:00:00 PM	47.0	22	26	53	48.3	23.1%	42.4
8/25/2024 11:00:00 PM	48.0	26	30	53	49.0	26.7%	42.0
8/26/2024 12:00:00 AM	46.0	17	20	52	49.5	20.0%	41.5
8/26/2024 1:00:00 AM	46.0	14	16	51	49.3	18.8%	41.8
8/26/2024 2:00:00 AM	40.0	4	5	45	0.0	0.0%	39.6
8/26/2024 3:00:00 AM	42.0	4	5	44	0.0	0.0%	39.8
8/26/2024 4:00:00 AM	57.0	2	2	57	57.0	50.0%	47.0
8/26/2024 5:00:00 AM	36.0	3	4	42	0.0	0.0%	33.5
8/26/2024 6:00:00 AM	45.0	2	2	45	0.0	0.0%	43.5
8/26/2024 7:00:00 AM	**No Data**						
8/26/2024 8:00:00 AM	**No Data**						
8/26/2024 9:00:00 AM	41.0	1	1	41	0.0	0.0%	41.0
8/26/2024 10:00:00 AM	47.0	14	16	49	47.3	25.0%	42.4
8/26/2024 11:00:00 AM	44.0	14	17	46	46.0	5.9%	40.5
8/26/2024 12:00:00 PM	42.0	23	27	45	0.0	0.0%	39.5
8/26/2024 1:00:00 PM	45.0	24	28	60	52.0	13.8%	39.1
8/26/2024 2:00:00 PM	46.0	37	43	60	49.1	16.3%	41.2
8/26/2024 3:00:00 PM	46.0	33	39	48	47.3	17.9%	39.6
8/26/2024 4:00:00 PM	44.0	37	44	56	48.8	13.6%	40.5
8/26/2024 5:00:00 PM	45.0	42	50	55	50.7	12.0%	40.6
8/26/2024 6:00:00 PM	46.0	35	41	54	49.0	17.1%	40.8
8/26/2024 7:00:00 PM	44.0	60	70	55	48.9	11.4%	39.9
8/26/2024 8:00:00 PM	45.0	38	45	52	47.8	11.1%	40.0
8/26/2024 9:00:00 PM	45.0	33	39	51	47.6	12.8%	39.6
8/26/2024 10:00:00 PM	46.0	26	30	50	47.8	20.0%	40.7
8/26/2024 11:00:00 PM	48.0	29	34	55	49.1	26.5%	43.0
8/27/2024 12:00:00 AM	44.0	16	19	46	46.0	10.5%	39.3
8/27/2024 1:00:00 AM	44.0	20	24	46	46.0	12.5%	40.6
8/27/2024 2:00:00 AM	44.0	8	9	45	0.0	0.0%	38.0
8/27/2024 3:00:00 AM	38.0	3	4	40	0.0	0.0%	37.8
8/27/2024 4:00:00 AM	**No Data**						
8/27/2024 5:00:00 AM	48.0	2	2	48	48.0	50.0%	36.5
8/27/2024 6:00:00 AM	33.0	2	2	33	0.0	0.0%	30.5
8/27/2024 7:00:00 AM	38.0	2	2	38	0.0	0.0%	36.5
8/27/2024 8:00:00 AM	33.0	2	2	33	0.0	0.0%	28.5
8/27/2024 9:00:00 AM	**No Data**						
8/27/2024 10:00:00 AM	46.0	10	12	49	47.3	25.0%	42.3
8/27/2024 11:00:00 AM	45.0	24	28	53	49.0	10.7%	41.3
8/27/2024 12:00:00 PM	46.0	34	40	57	50.3	17.5%	41.1
8/27/2024 1:00:00 PM	44.0	29	34	51	48.8	11.8%	39.8
8/27/2024 2:00:00 PM	44.0	25	29	48	48.0	6.9%	38.6

Outgoing Summary
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

8/27/2024 3:00:00 PM	45.0	32	38	52	49.0	7.9%	40.2
8/27/2024 4:00:00 PM	44.0	34	40	57	52.0	10.0%	39.8
8/27/2024 5:00:00 PM	44.0	44	52	48	47.3	7.7%	39.8
8/27/2024 6:00:00 PM	45.0	32	38	54	51.4	13.2%	40.5
8/27/2024 7:00:00 PM	49.0	36	42	55	50.7	23.8%	41.4
8/27/2024 8:00:00 PM	46.0	38	45	52	47.9	20.0%	42.0
8/27/2024 9:00:00 PM	45.0	34	40	51	48.0	12.5%	39.1
8/27/2024 10:00:00 PM	45.0	48	57	55	48.7	15.8%	40.8
8/27/2024 11:00:00 PM	46.0	60	70	56	49.2	17.1%	41.4
8/28/2024 12:00:00 AM	46.0	28	33	51	47.7	18.2%	41.3
8/28/2024 1:00:00 AM	45.0	20	23	50	48.0	8.7%	40.0
8/28/2024 2:00:00 AM	45.0	5	6	50	50.0	16.7%	37.0
8/28/2024 3:00:00 AM	71.0	3	3	71	71.0	33.3%	45.3
8/28/2024 4:00:00 AM	32.0	1	1	32	0.0	0.0%	32.0
8/28/2024 5:00:00 AM	55.0	2	2	55	55.0	50.0%	40.5
8/28/2024 6:00:00 AM	33.0	1	1	33	0.0	0.0%	33.0
8/28/2024 7:00:00 AM	37.0	1	1	37	0.0	0.0%	37.0
8/28/2024 8:00:00 AM	43.0	3	3	43	0.0	0.0%	38.0
8/28/2024 9:00:00 AM	**No Data**						
8/28/2024 10:00:00 AM	46.0	8	10	65	51.5	40.0%	46.1
8/28/2024 11:00:00 AM	46.0	16	19	47	46.8	21.1%	41.9
8/28/2024 12:00:00 PM	45.0	38	45	51	48.3	13.3%	40.0
8/28/2024 1:00:00 PM	46.0	31	36	51	48.0	16.7%	38.8
8/28/2024 2:00:00 PM	46.0	26	30	49	47.2	16.7%	40.5
8/28/2024 3:00:00 PM	48.0	26	31	53	49.3	22.6%	41.9
8/28/2024 4:00:00 PM	47.0	37	44	54	48.1	22.7%	41.5
8/28/2024 5:00:00 PM	43.0	31	37	53	49.3	8.1%	38.4
8/28/2024 6:00:00 PM	45.0	54	64	53	48.7	14.1%	40.2
8/28/2024 7:00:00 PM	45.0	48	56	57	49.3	14.3%	39.1
8/28/2024 8:00:00 PM	45.0	54	64	51	48.0	10.9%	40.4
8/28/2024 9:00:00 PM	45.0	41	48	53	48.1	14.6%	38.9
8/28/2024 10:00:00 PM	46.0	46	54	53	47.6	25.9%	41.9
8/28/2024 11:00:00 PM	46.0	46	54	69	50.4	18.5%	41.9
8/29/2024 12:00:00 AM	47.0	24	28	66	50.8	21.4%	42.2
8/29/2024 1:00:00 AM	48.0	18	21	51	48.8	23.8%	42.0
8/29/2024 2:00:00 AM	44.0	7	8	45	0.0	0.0%	39.4
8/29/2024 3:00:00 AM	35.0	4	5	44	0.0	0.0%	35.8
8/29/2024 4:00:00 AM	38.0	2	2	38	0.0	0.0%	34.0
8/29/2024 5:00:00 AM	34.0	2	2	34	0.0	0.0%	33.5
8/29/2024 6:00:00 AM	43.0	2	2	43	0.0	0.0%	40.5
8/29/2024 7:00:00 AM	**No Data**						
8/29/2024 8:00:00 AM	**No Data**						
8/29/2024 10:00:00 AM						
8/29/2024 10:00:00 AM	47.0	8	9	56	51.5	22.2%	41.7
8/29/2024 11:00:00 AM	45.0	8	10	51	49.0	20.0%	41.9
8/29/2024 12:00:00 PM	44.0	40	47	50	48.0	8.5%	39.5
8/29/2024 1:00:00 PM	44.0	37	43	58	49.6	11.6%	39.0
8/29/2024 2:00:00 PM	42.0	33	39	52	49.3	10.3%	37.8

Outgoing Summary
Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/24/2024 12:00:00 AM	46.0	314	369	56	48.6	19.5%	41.1
8/25/2024 12:00:00 AM	46.0	397	467	54	48.1	17.1%	40.5
8/26/2024 12:00:00 AM	46.0	440	518	59	48.3	18.0%	41.0
8/27/2024 12:00:00 AM	45.0	490	577	60	48.7	13.9%	40.4
8/28/2024 12:00:00 AM	45.0	547	643	57	49.0	14.0%	40.5
8/29/2024 12:00:00 AM	46.0	561	660	71	49.0	16.8%	40.5
8/29/2024 1:59:59 PM	45.0	160	188	58	49.2	11.7%	39.3

Outgoing Weekly Counts
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	11	9	0	10	0
1 - 2	*	*	*	*	*	11	2	0	6.5	0
2 - 3	*	*	*	*	*	7	6	0	6.5	0
3 - 4	*	*	*	*	*	3	3	0	3	0
4 - 5	*	*	*	*	*	4	6	0	5	0
5 - 6	*	*	*	*	*	2	3	0	2.5	0
6 - 7	*	*	*	*	*	4	1	0	2.5	0
7 - 8	*	*	*	*	*	2	2	0	2	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	1	0	0	0.5	0
10 - 11	*	*	*	*	*	12	8	0	10	0
11 - 12	*	*	*	*	*	16	19	0	17.5	0
12 - 13	*	*	*	*	*	35	23	0	29	0
13 - 14	*	*	*	*	*	29	21	0	25	0
14 - 15	*	*	*	*	*	42	33	0	37.5	0
15 - 16	*	*	*	*	11	43	51	11	47	46
16 - 17	*	*	*	*	68	35	44	68	39.5	44.7
17 - 18	*	*	*	*	40	33	72	40	52.5	46.8
18 - 19	*	*	*	*	60	47	59	60	53	44.6
19 - 20	*	*	*	*	50	22	41	50	31.5	46.8
20 - 21	*	*	*	*	40	30	39	40	34.5	44.8
21 - 22	*	*	*	*	46	33	26	46	29.5	49
22 - 23	*	*	*	*	35	28	30	35	29	45.7
23 - 24	*	*	*	*	19	17	20	19	18.5	43.3
Totals	0	0	0	0	369	467	518			
% of Total	0%	0%	0%	0%	27.25%	34.49%	38.26%			

Outgoing Weekly Counts

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	16	24	23	21	*	*	*	21	0	45.58
1 - 2	5	9	6	8	*	*	*	7	0	43.25
2 - 3	5	4	3	5	*	*	*	4.25	0	46.5
3 - 4	2	0	1	2	*	*	*	1.25	0	42.33
4 - 5	4	2	2	2	*	*	*	2.5	0	43.25
5 - 6	2	2	1	2	*	*	*	1.75	0	38.5
6 - 7	0	2	1	0	*	*	*	0.75	0	37.5
7 - 8	0	2	3	0	*	*	*	1.25	0	38
8 - 9	1	0	0	0	*	*	*	0.25	0	41
9 - 10	16	12	10	9	*	*	*	11.75	0	46.38
10 - 11	17	28	19	10	*	*	*	18.5	0	44.78
11 - 12	27	40	45	47	*	*	*	39.75	0	44.08
12 - 13	28	34	36	43	*	*	*	35.25	0	44.32
13 - 14	43	29	30	39	*	*	*	35.25	0	44.08
14 - 15	39	38	31	*	*	*	*	36	0	45.9
15 - 16	44	40	44	*	*	*	*	42.67	0	44.77
16 - 17	50	52	37	*	*	*	*	46.33	0	43.7
17 - 18	41	38	64	*	*	*	*	47.67	0	44.93
18 - 19	70	42	56	*	*	*	*	56	0	45.77
19 - 20	45	45	64	*	*	*	*	51.33	0	44.93
20 - 21	39	40	48	*	*	*	*	42.33	0	44.67
21 - 22	30	57	54	*	*	*	*	47	0	45.57
22 - 23	34	70	54	*	*	*	*	52.67	0	46.47
23 - 24	19	33	28	*	*	*	*	26.67	0	45.33
Totals	577	643	660	188	0	0	0			
% of Total	27.9%	31.09%	31.91%	9.09%	0%	0%	0%			

Outgoing Monthly Counts

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	16	24	23	21	*	11	9	21	10	45.58
1 - 2	5	9	6	8	*	11	2	7	6.5	43.25
2 - 3	5	4	3	5	*	7	6	4.25	6.5	46.5
3 - 4	2	0	1	2	*	3	3	1.25	3	42.33
4 - 5	4	2	2	2	*	4	6	2.5	5	43.25
5 - 6	2	2	1	2	*	2	3	1.75	2.5	38.5
6 - 7	0	2	1	0	*	4	1	0.75	2.5	37.5
7 - 8	0	2	3	0	*	2	2	1.25	2	38
8 - 9	1	0	0	0	*	0	0	0.25	0	41
9 - 10	16	12	10	9	*	1	0	11.75	0.5	46.38
10 - 11	17	28	19	10	*	12	8	18.5	10	44.78
11 - 12	27	40	45	47	*	16	19	39.75	17.5	44.08
12 - 13	28	34	36	43	*	35	23	35.25	29	44.32
13 - 14	43	29	30	39	*	29	21	35.25	25	44.08
14 - 15	39	38	31	*	*	42	33	36	37.5	45.9
15 - 16	44	40	44	*	11	43	51	34.75	47	45.08
16 - 17	50	52	37	*	68	35	44	51.75	39.5	43.95
17 - 18	41	38	64	*	40	33	72	45.75	52.5	45.4
18 - 19	70	42	56	*	60	47	59	57	53	45.48
19 - 20	45	45	64	*	50	22	41	51	31.5	45.4
20 - 21	39	40	48	*	40	30	39	41.75	34.5	44.7
21 - 22	30	57	54	*	46	33	26	46.75	29.5	46.42
22 - 23	34	70	54	*	35	28	30	48.25	29	46.28
23 - 24	19	33	28	*	19	17	20	24.75	18.5	44.82
Totals	577	643	660	188	369	467	518			
% of Total	16.86%	18.79%	19.29%	5.49%	10.78%	13.65%	15.14%			

Outgoing Weekly Speeds

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	39.27	40	0	39.6	0
1 - 2	*	*	*	*	*	39.45	48.5	0	40.85	0
2 - 3	*	*	*	*	*	36.43	38.17	0	37.23	0
3 - 4	*	*	*	*	*	40.33	34.33	0	37.33	0
4 - 5	*	*	*	*	*	40.5	32.33	0	35.6	0
5 - 6	*	*	*	*	*	40.5	37	0	38.4	0
6 - 7	*	*	*	*	*	32.25	28	0	31.4	0
7 - 8	*	*	*	*	*	34	38.5	0	36.25	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	42	0	0	42	0
10 - 11	*	*	*	*	*	40.33	39.62	0	40.05	0
11 - 12	*	*	*	*	*	38.94	39.84	0	39.43	0
12 - 13	*	*	*	*	*	42.03	42.7	0	42.29	0
13 - 14	*	*	*	*	*	40.79	43.24	0	41.82	0
14 - 15	*	*	*	*	*	41.17	40.52	0	40.88	0
15 - 16	*	*	*	*	43.64	40.74	41.02	43.64	40.89	46
16 - 17	*	*	*	*	40.53	40.2	41.16	40.53	40.73	44.7
17 - 18	*	*	*	*	41.38	39.76	41.15	41.38	40.71	46.8
18 - 19	*	*	*	*	39.77	41.02	40.53	39.77	40.75	44.6
19 - 20	*	*	*	*	42.04	40.23	40.83	42.04	40.62	46.8
20 - 21	*	*	*	*	40.72	42.13	41.46	40.72	41.75	44.8
21 - 22	*	*	*	*	42.39	39.7	42.42	42.39	40.9	49
22 - 23	*	*	*	*	42	40.64	42.03	42	41.36	45.7
23 - 24	*	*	*	*	39.63	40.24	41.5	39.63	40.92	43.3
Totals	0	0	0	0	41.1	40.4	41			
% of Total	0%	0%	0%	0%	33.55%	32.98%	33.47%			

Outgoing Weekly Speeds

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	41.75	40.62	40	42.05	*	*	*	41.02	*	45.58
1 - 2	39.6	38	37	39.38	*	*	*	38.46	*	43.25
2 - 3	39.8	37.75	45.33	35.8	*	*	*	39.12	*	46.5
3 - 4	47	0	32	34	*	*	*	38.8	*	42.33
4 - 5	33.5	36.5	40.5	33.5	*	*	*	35.5	*	43.25
5 - 6	43.5	30.5	33	40.5	*	*	*	37.43	*	38.5
6 - 7	0	36.5	37	0	*	*	*	36.67	*	37.5
7 - 8	0	28.5	38	0	*	*	*	34.2	*	38
8 - 9	41	0	0	0	*	*	*	41	*	41
9 - 10	42.44	42.25	46.1	41.67	*	*	*	43.02	*	46.38
10 - 11	40.53	41.32	41.89	41.9	*	*	*	41.36	*	44.78
11 - 12	39.48	41.1	40.04	39.51	*	*	*	40.06	*	44.08
12 - 13	39.11	39.79	38.81	39.02	*	*	*	39.17	*	44.32
13 - 14	41.21	38.62	40.47	37.77	*	*	*	39.57	*	44.08
14 - 15	39.62	40.16	41.9	*	*	*	*	40.46	*	45.9
15 - 16	40.48	39.75	41.55	*	*	*	*	40.62	*	44.77
16 - 17	40.56	39.79	38.38	*	*	*	*	39.69	*	43.7
17 - 18	40.83	40.47	40.17	*	*	*	*	40.44	*	44.93
18 - 19	39.91	41.4	39.05	*	*	*	*	40	*	45.77
19 - 20	39.96	41.98	40.36	*	*	*	*	40.71	*	44.93
20 - 21	39.59	39.1	38.85	*	*	*	*	39.16	*	44.67
21 - 22	40.73	40.77	41.87	*	*	*	*	41.18	*	45.57
22 - 23	43.03	41.41	41.89	*	*	*	*	41.92	*	46.47
23 - 24	39.32	41.3	42.18	*	*	*	*	41.14	*	45.33
Totals	40.4	40.5	40.5	39.3	0	0	0			
% of Total	25.14%	25.2%	25.2%	24.46%	0%	0%	0%			

Outgoing Monthly Speeds

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	Aug 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	41.75	40.62	40	42.05	*	39.27	40	41.02	39.6	45.58
1 - 2	39.6	38	37	39.38	*	39.45	48.5	38.46	40.85	43.25
2 - 3	39.8	37.75	45.33	35.8	*	36.43	38.17	39.12	37.23	46.5
3 - 4	47	0	32	34	*	40.33	34.33	38.8	37.33	42.33
4 - 5	33.5	36.5	40.5	33.5	*	40.5	32.33	35.5	35.6	43.25
5 - 6	43.5	30.5	33	40.5	*	40.5	37	37.43	38.4	38.5
6 - 7	0	36.5	37	0	*	32.25	28	36.67	31.4	37.5
7 - 8	0	28.5	38	0	*	34	38.5	34.2	36.25	38
8 - 9	41	0	0	0	*	0	0	41	0	41
9 - 10	42.44	42.25	46.1	41.67	*	42	0	43.02	42	46.38
10 - 11	40.53	41.32	41.89	41.9	*	40.33	39.62	41.36	40.05	44.78
11 - 12	39.48	41.1	40.04	39.51	*	38.94	39.84	40.06	39.43	44.08
12 - 13	39.11	39.79	38.81	39.02	*	42.03	42.7	39.17	42.29	44.32
13 - 14	41.21	38.62	40.47	37.77	*	40.79	43.24	39.57	41.82	44.08
14 - 15	39.62	40.16	41.9	*	*	41.17	40.52	40.46	40.88	45.9
15 - 16	40.48	39.75	41.55	*	43.64	40.74	41.02	40.86	40.89	45.08
16 - 17	40.56	39.79	38.38	*	40.53	40.2	41.16	39.97	40.73	43.95
17 - 18	40.83	40.47	40.17	*	41.38	39.76	41.15	40.64	40.71	45.4
18 - 19	39.91	41.4	39.05	*	39.77	41.02	40.53	39.94	40.75	45.48
19 - 20	39.96	41.98	40.36	*	42.04	40.23	40.83	41.04	40.62	45.4
20 - 21	39.59	39.1	38.85	*	40.72	42.13	41.46	39.53	41.75	44.7
21 - 22	40.73	40.77	41.87	*	42.39	39.7	42.42	41.48	40.9	46.42
22 - 23	43.03	41.41	41.89	*	42	40.64	42.03	41.94	41.36	46.28
23 - 24	39.32	41.3	42.18	*	39.63	40.24	41.5	40.85	40.92	44.82
Totals	40.4	40.5	40.5	39.3	41.1	40.4	41			
% of Total	14.27%	14.3%	14.3%	13.88%	14.51%	14.27%	14.48%			

Outgoing Weekly EightyFifthSpeeds
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/19/2024	to	8/25/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/19/2024	8/20/2024	8/21/2024	8/22/2024	8/23/2024	8/24/2024	8/25/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	44	48	0	46	0
1 - 2	*	*	*	*	*	44	49	0	46.5	0
2 - 3	*	*	*	*	*	38	38	0	38	0
3 - 4	*	*	*	*	*	46	40	0	43	0
4 - 5	*	*	*	*	*	40	36	0	38	0
5 - 6	*	*	*	*	*	45	42	0	43.5	0
6 - 7	*	*	*	*	*	32	28	0	30	0
7 - 8	*	*	*	*	*	35	41	0	38	0
8 - 9	*	*	*	*	*	0	0	0	0	0
9 - 10	*	*	*	*	*	42	0	0	42	0
10 - 11	*	*	*	*	*	44	43	0	43.5	0
11 - 12	*	*	*	*	*	45	44.5	0	44.75	0
12 - 13	*	*	*	*	*	45.8	47	0	46.4	0
13 - 14	*	*	*	*	*	46	47	0	46.5	0
14 - 15	*	*	*	*	*	46	44.7	0	45.35	0
15 - 16	*	*	*	*	46	45	45.3	46	45.15	46
16 - 17	*	*	*	*	44.7	44	44.5	44.7	44.25	44.7
17 - 18	*	*	*	*	46.8	47	46.5	46.8	46.75	46.8
18 - 19	*	*	*	*	44.6	45	44.4	44.6	44.7	44.6
19 - 20	*	*	*	*	46.8	43.5	45.5	46.8	44.5	46.8
20 - 21	*	*	*	*	44.8	46	45.8	44.8	45.9	44.8
21 - 22	*	*	*	*	49	46.5	46.5	49	46.5	49
22 - 23	*	*	*	*	45.7	46	47.5	45.7	46.75	45.7
23 - 24	*	*	*	*	43.3	46	46	43.3	46	43.3
Totals	0	0	0	0	411.7	1001.8	960.2			
% of Total	0%	0%	0%	0%	17.34%	42.2%	40.45%			

Outgoing Weekly EightyFifthSpeeds
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	46	43.5	44.8	48	*	*	*	45.58	0	45.58
1 - 2	40	44	45	44	*	*	*	43.25	0	43.25
2 - 3	42	38	71	35	*	*	*	46.5	0	46.5
3 - 4	57	0	32	38	*	*	*	42.33	0	42.33
4 - 5	36	48	55	34	*	*	*	43.25	0	43.25
5 - 6	45	33	33	43	*	*	*	38.5	0	38.5
6 - 7	0	38	37	0	*	*	*	37.5	0	37.5
7 - 8	0	33	43	0	*	*	*	38	0	38
8 - 9	41	0	0	0	*	*	*	41	0	41
9 - 10	46.5	46	46	47	*	*	*	46.38	0	46.38
10 - 11	43.3	44.8	46	45	*	*	*	44.78	0	44.78
11 - 12	42	46	44.5	43.8	*	*	*	44.08	0	44.08
12 - 13	44	44	45.3	44	*	*	*	44.32	0	44.32
13 - 14	45.5	43.5	45.3	42	*	*	*	44.08	0	44.08
14 - 15	45.5	44.2	48	*	*	*	*	45.9	0	45.9
15 - 16	44	44	46.3	*	*	*	*	44.77	0	44.77
16 - 17	44.3	43.8	43	*	*	*	*	43.7	0	43.7
17 - 18	45.5	44.5	44.8	*	*	*	*	44.93	0	44.93
18 - 19	43.8	48.5	45	*	*	*	*	45.77	0	45.77
19 - 20	44.3	46	44.5	*	*	*	*	44.93	0	44.93
20 - 21	44.5	44.5	45	*	*	*	*	44.67	0	44.67
21 - 22	45.7	45	46	*	*	*	*	45.57	0	45.57
22 - 23	48	45.7	45.7	*	*	*	*	46.47	0	46.47
23 - 24	43.7	45.3	47	*	*	*	*	45.33	0	45.33
Totals	977.6	953.3	1043.2	463.8	0	0	0			
% of Total	28.44%	27.73%	30.34%	13.49%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	46	43.5	44.8	48	*	44	48	45.58	46	45.58
1 - 2	40	44	45	44	*	44	49	43.25	46.5	43.25
2 - 3	42	38	71	35	*	38	38	46.5	38	46.5
3 - 4	57	0	32	38	*	46	40	42.33	43	42.33
4 - 5	36	48	55	34	*	40	36	43.25	38	43.25
5 - 6	45	33	33	43	*	45	42	38.5	43.5	38.5
6 - 7	0	38	37	0	*	32	28	37.5	30	37.5
7 - 8	0	33	43	0	*	35	41	38	38	38
8 - 9	41	0	0	0	*	0	0	41	0	41
9 - 10	46.5	46	46	47	*	42	0	46.38	42	46.38
10 - 11	43.3	44.8	46	45	*	44	43	44.78	43.5	44.78
11 - 12	42	46	44.5	43.8	*	45	44.5	44.08	44.75	44.08
12 - 13	44	44	45.3	44	*	45.8	47	44.32	46.4	44.32
13 - 14	45.5	43.5	45.3	42	*	46	47	44.08	46.5	44.08
14 - 15	45.5	44.2	48	*	*	46	44.7	45.9	45.35	45.9
15 - 16	44	44	46.3	*	46	45	45.3	45.08	45.15	45.08
16 - 17	44.3	43.8	43	*	44.7	44	44.5	43.95	44.25	43.95
17 - 18	45.5	44.5	44.8	*	46.8	47	46.5	45.4	46.75	45.4
18 - 19	43.8	48.5	45	*	44.6	45	44.4	45.48	44.7	45.48
19 - 20	44.3	46	44.5	*	46.8	43.5	45.5	45.4	44.5	45.4
20 - 21	44.5	44.5	45	*	44.8	46	45.8	44.7	45.9	44.7
21 - 22	45.7	45	46	*	49	46.5	46.5	46.42	46.5	46.42
22 - 23	48	45.7	45.7	*	45.7	46	47.5	46.28	46.75	46.28
23 - 24	43.7	45.3	47	*	43.3	46	46	44.82	46	44.82

Summary of Violators

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	104	40.8	16	48.3
01:00:00	41	39.2	5	48.2
02:00:00	30	38.3	3	56.0
03:00:00	11	38.0	2	51.5
04:00:00	20	35.6	3	49.7
05:00:00	12	37.8	0	0.0
06:00:00	8	33.4	0	0.0
07:00:00	9	35.1	0	0.0
08:00:00	1	41.0	0	0.0
09:00:00	48	43.0	13	49.2
10:00:00	94	41.1	13	47.6
11:00:00	194	39.9	21	49.0
12:00:00	199	40.1	30	49.0
13:00:00	191	40.2	27	48.7
14:00:00	183	40.6	28	48.4
15:00:00	233	40.9	38	48.4
16:00:00	286	40.2	31	49.1
17:00:00	288	40.7	59	48.8
18:00:00	334	40.2	46	48.7
19:00:00	267	40.9	46	47.8
20:00:00	236	40.2	38	47.7
21:00:00	246	41.3	56	48.5
22:00:00	251	41.8	51	49.2
23:00:00	136	40.9	22	49.0

Outgoing Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/23/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/23/2024	15:00	0	0	0	0	0	2	5	4	0	0	0	0	0	0	0	0	0	0	0	11	43.6	46	38 to 48	100.0	3	27.3	0	11	0
8/23/2024	16:00	0	0	0	1	7	20	28	6	5	1	0	0	0	0	0	0	0	0	0	68	40.5	44.7	33 to 43	75.0	9	13.2	1	62	5
8/23/2024	17:00	0	0	0	1	1	14	12	9	3	0	0	0	0	0	0	0	0	0	0	40	41.4	46.8	35 to 45	70.0	10	25.0	0	34	6
8/23/2024	18:00	0	0	0	1	12	14	21	11	1	0	0	0	0	0	0	0	0	0	0	60	39.8	44.6	37 to 47	73.3	7	11.7	0	55	5
8/23/2024	19:00	0	0	0	0	3	15	13	16	3	0	0	0	0	0	0	0	0	0	0	50	42	46.8	37 to 47	70.0	16	32.0	0	46	4
8/23/2024	20:00	0	0	0	2	3	8	17	10	0	0	0	0	0	0	0	0	0	0	0	40	40.7	44.8	36 to 46	80.0	5	12.5	0	38	2
8/23/2024	21:00	0	0	0	1	7	6	12	13	7	0	0	0	0	0	0	0	0	0	0	46	42.4	49	38 to 48	60.9	15	32.6	0	42	4
8/23/2024	22:00	0	0	0	0	4	4	14	10	3	0	0	0	0	0	0	0	0	0	0	35	42	45.7	37 to 47	80.0	7	20.0	0	34	1
8/23/2024	23:00	0	0	0	0	3	6	9	1	0	0	0	0	0	0	0	0	0	0	0	19	39.6	43.3	34 to 44	84.2	*	*	0	18	1
24 Hr Summary		0	0	0	6	40	89	131	80	22	1	0	0	0	0	0	0	0	0	369	41.1	46	37 to 47	72.4	72	19.5	1	340	28	

Outgoing Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/24/2024	00:00	0	0	0	1	1	3	4	2	0	0	0	0	0	0	0	0	0	0	11	39.3	44	37 to 47	81.8	1	9.1	0	11	0	
8/24/2024	01:00	0	0	0	0	3	1	5	2	0	0	0	0	0	0	0	0	0	0	11	39.5	44	32 to 42	63.6	2	18.2	0	11	0	
8/24/2024	02:00	0	0	0	1	3	2	0	0	1	0	0	0	0	0	0	0	0	0	7	36.4	38	28 to 38	71.4	1	14.3	0	7	0	
8/24/2024	03:00	0	0	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	3	40.3	46	36 to 46	66.7	1	33.3	0	3	0	
8/24/2024	04:00	0	0	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	4	40.5	40	36 to 46	100.0	1	25.0	0	4	0	
8/24/2024	05:00	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	40.5	45	35 to 45	100.0	*	*	0	2	0	
8/24/2024	06:00	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	4	32.3	32	22 to 32	75.0	*	*	0	4	0	
8/24/2024	07:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	35	25 to 35	100.0	*	*	0	2	0	
8/24/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/24/2024	09:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	42	42	32 to 42	100.0	*	*	0	1	0	
8/24/2024	10:00	0	0	0	0	1	5	4	2	0	0	0	0	0	0	0	0	0	0	12	40.3	44	34 to 44	83.3	2	16.7	0	11	1	
8/24/2024	11:00	0	0	0	1	0	10	2	3	0	0	0	0	0	0	0	0	0	0	16	38.9	45	35 to 45	81.3	2	12.5	0	16	0	
8/24/2024	12:00	0	0	0	1	1	7	12	13	1	0	0	0	0	0	0	0	0	0	35	42	45.8	36 to 46	80.0	8	22.9	0	35	0	
8/24/2024	13:00	0	0	0	2	1	10	9	5	2	0	0	0	0	0	0	0	0	0	29	40.8	46	36 to 46	75.9	5	17.2	0	28	1	
8/24/2024	14:00	0	0	0	1	2	12	18	7	2	0	0	0	0	0	0	0	0	0	42	41.2	46	35 to 45	76.2	7	16.7	0	41	1	
8/24/2024	15:00	0	0	0	0	5	8	22	6	2	0	0	0	0	0	0	0	0	0	43	40.7	45	37 to 47	79.1	6	14.0	0	41	2	
8/24/2024	16:00	0	0	0	0	3	12	15	4	1	0	0	0	0	0	0	0	0	0	35	40.2	44	35 to 45	80.0	4	11.4	0	32	3	
8/24/2024	17:00	0	0	2	1	5	6	9	8	2	0	0	0	0	0	0	0	0	0	33	39.8	47	39 to 49	60.6	9	27.3	0	31	2	
8/24/2024	18:00	0	0	0	0	1	17	17	11	1	0	0	0	0	0	0	0	0	0	47	41	45	35 to 45	83.0	7	14.9	0	47	0	
8/24/2024	19:00	0	0	0	0	1	7	12	2	0	0	0	0	0	0	0	0	0	0	22	40.2	43.5	36 to 46	90.9	2	9.1	0	22	0	
8/24/2024	20:00	0	0	0	0	3	5	9	13	0	0	0	0	0	0	0	0	0	0	30	42.1	46	36 to 46	76.7	7	23.3	0	28	2	
8/24/2024	21:00	0	0	2	0	6	7	12	5	1	0	0	0	0	0	0	0	0	0	33	39.7	46.5	34 to 44	60.6	6	18.2	0	31	2	
8/24/2024	22:00	0	0	0	2	2	6	9	8	1	0	0	0	0	0	0	0	0	0	28	40.6	46	35 to 45	67.9	5	17.9	0	27	1	
8/24/2024	23:00	0	0	0	2	1	4	5	3	2	0	0	0	0	0	0	0	0	0	17	40.2	46	36 to 46	64.7	4	23.5	0	16	1	
24 Hr Summary		0	0	4	13	43	126	168	97	16	0	0	0	0	0	0	0	0	0	467	40.5	46	36 to 46	71.9	80	17.1	0	451	16	

Outgoing Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/25/2024	00:00	0	0	0	0	1	4	2	1	1	0	0	0	0	0	0	0	0	0	9	40	48	33 to 43	66.7	2	22.2	0	9	0	
8/25/2024	01:00	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	2	48.5	49	39 to 49	100.0	2	100.0	0	2	0	
8/25/2024	02:00	0	0	0	0	0	5	0	1	0	0	0	0	0	0	0	0	0	0	6	38.2	38	28 to 38	83.3	1	16.7	0	6	0	
8/25/2024	03:00	0	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	3	34.3	40	26 to 36	66.7	*	*	0	3	0	
8/25/2024	04:00	0	0	1	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	6	32.3	36	28 to 38	83.3	*	*	0	6	0	
8/25/2024	05:00	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	3	37	42	32 to 42	100.0	*	*	0	3	0	
8/25/2024	06:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0	
8/25/2024	07:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	38.5	41	31 to 41	100.0	*	*	0	2	0	
8/25/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/25/2024	10:00	0	0	0	0	1	2	4	1	0	0	0	0	0	0	0	0	0	0	8	39.6	43	33 to 43	75.0	1	12.5	0	8	0	
8/25/2024	11:00	0	0	0	0	3	5	7	3	1	0	0	0	0	0	0	0	0	0	19	39.8	44.5	35 to 45	73.7	2	10.5	0	19	0	
8/25/2024	12:00	0	0	0	0	1	7	7	6	0	2	0	0	0	0	0	0	0	0	23	42.7	47	35 to 45	78.3	4	17.4	0	23	0	
8/25/2024	13:00	0	0	0	0	0	2	12	6	1	0	0	0	0	0	0	0	0	0	21	43.2	47	39 to 49	90.5	4	19.0	0	21	0	
8/25/2024	14:00	0	0	0	1	2	11	12	6	1	0	0	0	0	0	0	0	0	0	33	40.5	44.7	37 to 47	84.8	4	12.1	0	30	3	
8/25/2024	15:00	0	0	0	0	3	15	21	11	1	0	0	0	0	0	0	0	0	0	51	41	45.3	37 to 47	82.4	9	17.6	0	49	2	
8/25/2024	16:00	0	0	0	0	1	15	19	9	0	0	0	0	0	0	0	0	0	0	44	41.2	44.5	36 to 46	90.9	5	11.4	0	42	2	
8/25/2024	17:00	0	1	0	1	6	19	21	21	2	1	0	0	0	0	0	0	0	0	72	41.2	46.5	37 to 47	68.1	19	26.4	0	72	0	
8/25/2024	18:00	0	0	0	0	5	16	27	10	1	0	0	0	0	0	0	0	0	0	59	40.5	44.4	35 to 45	81.4	6	10.2	0	58	1	
8/25/2024	19:00	0	0	0	0	5	14	13	7	2	0	0	0	0	0	0	0	0	0	41	40.8	45.5	34 to 44	73.2	7	17.1	0	39	2	
8/25/2024	20:00	0	0	0	1	1	12	13	10	2	0	0	0	0	0	0	0	0	0	39	41.5	45.8	36 to 46	76.9	9	23.1	0	38	1	
8/25/2024	21:00	0	0	0	0	2	4	9	10	1	0	0	0	0	0	0	0	0	0	26	42.4	46.5	39 to 49	73.1	6	23.1	0	26	0	
8/25/2024	22:00	0	0	0	1	2	6	12	6	3	0	0	0	0	0	0	0	0	0	30	42	47.5	36 to 46	70.0	8	26.7	0	30	0	
8/25/2024	23:00	0	0	0	0	2	6	7	3	2	0	0	0	0	0	0	0	0	0	20	41.5	46	35 to 45	70.0	4	20.0	0	20	0	
24 Hr Summary		0	1	1	6	39	148	189	113	18	3	0	0	0	0	0	0	0	0	518	41	46	36 to 46	74.1	93	18.0	0	507	11	

Outgoing Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/26/2024	00:00	0	0	0	0	2	3	5	4	2	0	0	0	0	0	0	0	0	0	16	41.8	46	36 to 46	75.0	3	18.8	0	15	1	
8/26/2024	01:00	0	0	0	0	0	3	1	1	0	0	0	0	0	0	0	0	0	0	5	39.6	40	35 to 45	100.0	*	*	0	4	1	
8/26/2024	02:00	0	0	0	0	1	0	4	0	0	0	0	0	0	0	0	0	0	0	5	39.8	42	32 to 42	80.0	*	*	0	5	0	
8/26/2024	03:00	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2	47	57	27 to 37	50.0	1	50.0	0	2	0	
8/26/2024	04:00	0	0	1	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	4	33.5	36	32 to 42	75.0	*	*	0	4	0	
8/26/2024	05:00	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	43.5	45	35 to 45	100.0	*	*	0	2	0	
8/26/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/26/2024	08:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	41	41	31 to 41	100.0	*	*	0	1	0	
8/26/2024	09:00	0	0	0	0	0	1	11	4	0	0	0	0	0	0	0	0	0	0	16	42.4	46.5	37 to 47	93.8	4	25.0	0	16	0	
8/26/2024	10:00	0	0	0	0	1	3	12	1	0	0	0	0	0	0	0	0	0	0	17	40.5	43.3	34 to 44	94.1	1	5.9	0	17	0	
8/26/2024	11:00	0	0	0	0	2	12	12	1	0	0	0	0	0	0	0	0	0	0	27	39.5	42	35 to 45	92.6	*	*	0	27	0	
8/26/2024	12:00	0	0	0	3	3	8	10	3	1	0	0	0	0	0	0	0	0	0	28	39.1	44	37 to 47	71.4	3	10.7	0	20	8	
8/26/2024	13:00	0	0	0	1	3	11	16	10	1	0	1	0	0	0	0	0	0	0	43	41.2	45.5	36 to 46	76.7	7	16.3	0	38	5	
8/26/2024	14:00	0	0	2	0	6	10	12	9	0	0	0	0	0	0	0	0	0	0	39	39.6	45.5	36 to 46	66.7	7	17.9	0	35	4	
8/26/2024	15:00	0	0	0	0	6	13	18	5	1	1	0	0	0	0	0	0	0	0	44	40.5	44	36 to 46	75.0	6	13.6	0	38	6	
8/26/2024	16:00	0	0	0	3	1	18	19	4	4	1	0	0	0	0	0	0	0	0	50	40.6	44.3	35 to 45	80.0	6	12.0	0	45	5	
8/26/2024	17:00	0	0	0	1	2	14	15	7	2	0	0	0	0	0	0	0	0	0	41	40.8	45.5	35 to 45	75.6	7	17.1	0	38	3	
8/26/2024	18:00	0	0	0	0	5	34	22	7	1	1	0	0	0	0	0	0	0	0	70	39.9	43.8	35 to 45	81.4	8	11.4	0	65	5	
8/26/2024	19:00	0	0	0	2	5	12	18	7	1	0	0	0	0	0	0	0	0	0	45	40	44.3	36 to 46	75.6	5	11.1	0	42	3	
8/26/2024	20:00	0	0	1	0	4	15	12	6	1	0	0	0	0	0	0	0	0	0	39	39.6	44.5	33 to 43	79.5	5	12.8	0	38	1	
8/26/2024	21:00	0	0	0	1	5	7	5	10	2	0	0	0	0	0	0	0	0	0	30	40.7	45.7	36 to 46	66.7	6	20.0	0	30	0	
8/26/2024	22:00	0	0	0	0	2	8	9	11	3	1	0	0	0	0	0	0	0	0	34	43	48	36 to 46	70.6	9	26.5	0	33	1	
8/26/2024	23:00	0	0	0	0	2	9	6	2	0	0	0	0	0	0	0	0	0	0	19	39.3	43.7	36 to 46	89.5	2	10.5	0	17	2	
24 Hr Summary		0	0	4	11	51	183	210	93	19	5	1	0	0	0	0	0	0	0	577	40.4	45	36 to 46	76.1	80	13.9	0	532	45	

Outgoing Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/27/2024	00:00	0	0	0	0	2	8	11	3	0	0	0	0	0	0	0	0	0	0	24	40.6	43.5	36 to 46	91.7	3	12.5	0	24	0	
8/27/2024	01:00	0	0	0	0	3	2	3	1	0	0	0	0	0	0	0	0	0	0	9	38	44	32 to 42	66.7	*	*	0	9	0	
8/27/2024	02:00	0	0	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	0	4	37.8	38	30 to 40	100.0	*	*	0	4	0	
8/27/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	04:00	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	2	36.5	48	15 to 25	50.0	1	50.0	0	2	0	
8/27/2024	05:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30.5	33	23 to 33	100.0	*	*	0	2	0	
8/27/2024	06:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	36.5	38	28 to 38	100.0	*	*	0	2	0	
8/27/2024	07:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28.5	33	23 to 33	100.0	*	*	0	2	0	
8/27/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/27/2024	09:00	0	0	0	0	1	2	5	4	0	0	0	0	0	0	0	0	0	0	12	42.3	46	37 to 47	83.3	3	25.0	0	12	0	
8/27/2024	10:00	0	0	0	0	2	7	12	6	1	0	0	0	0	0	0	0	0	0	28	41.3	44.8	35 to 45	82.1	3	10.7	0	26	2	
8/27/2024	11:00	0	0	1	1	4	6	15	9	2	2	0	0	0	0	0	0	0	0	40	41.1	46	37 to 47	75.0	7	17.5	0	40	0	
8/27/2024	12:00	0	0	0	1	6	6	16	3	2	0	0	0	0	0	0	0	0	0	34	39.8	44	36 to 46	70.6	4	11.8	0	30	4	
8/27/2024	13:00	0	1	0	1	5	4	15	3	0	0	0	0	0	0	0	0	0	0	29	38.6	43.5	34 to 44	72.4	2	6.9	0	27	2	
8/27/2024	14:00	0	0	0	1	2	12	16	6	1	0	0	0	0	0	0	0	0	0	38	40.2	44.2	35 to 45	84.2	3	7.9	0	32	6	
8/27/2024	15:00	0	0	0	2	4	12	16	4	0	2	0	0	0	0	0	0	0	0	40	39.8	44	33 to 43	75.0	4	10.0	0	38	2	
8/27/2024	16:00	0	0	0	0	4	21	20	7	0	0	0	0	0	0	0	0	0	0	52	39.8	43.8	35 to 45	84.6	4	7.7	0	48	4	
8/27/2024	17:00	0	0	0	0	5	13	13	4	3	0	0	0	0	0	0	0	0	0	38	40.5	44.5	33 to 43	76.3	5	13.2	0	36	2	
8/27/2024	18:00	0	0	0	1	3	15	12	6	3	2	0	0	0	0	0	0	0	0	42	41.4	48.5	34 to 44	66.7	10	23.8	0	33	9	
8/27/2024	19:00	0	0	0	0	2	9	22	11	1	0	0	0	0	0	0	0	0	0	45	42	46	37 to 47	77.8	9	20.0	0	45	0	
8/27/2024	20:00	0	0	2	1	4	9	17	6	1	0	0	0	0	0	0	0	0	0	40	39.1	44.5	35 to 45	70.0	5	12.5	0	38	2	
8/27/2024	21:00	0	0	2	1	4	11	25	11	2	1	0	0	0	0	0	0	0	0	57	40.8	45	36 to 46	73.7	9	15.8	0	55	2	
8/27/2024	22:00	0	1	0	1	3	16	33	12	3	1	0	0	0	0	0	0	0	0	70	41.4	45.7	36 to 46	80.0	12	17.1	0	69	1	
8/27/2024	23:00	0	0	0	0	1	11	13	6	2	0	0	0	0	0	0	0	0	0	33	41.3	45.3	36 to 46	84.8	6	18.2	0	32	1	
24 Hr Summary		0	2	6	12	57	169	265	103	21	8	0	0	0	0	0	0	0	0	643	40.5	45	35 to 45	74.0	90	14.0	0	606	37	

Outgoing Histogram

Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/28/2024	00:00	0	0	0	0	3	8	6	5	1	0	0	0	0	0	0	0	0	0	23	40	44.8	35 to 45	78.3	2	8.7	0	21	2	
8/28/2024	01:00	0	0	1	0	2	0	1	1	1	0	0	0	0	0	0	0	0	0	6	37	45	22 to 32	50.0	1	16.7	0	6	0	
8/28/2024	02:00	0	0	0	1	0	0	1	0	0	0	0	0	1	0	0	0	0	0	3	45.3	71	15 to 25	33.3	1	33.3	0	3	0	
8/28/2024	03:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	32	32	22 to 32	100.0	*	*	0	0	1	
8/28/2024	04:00	0	0	0	1	0	0	0	0	0	1	0	0	0	0	0	0	0	0	2	40.5	55	16 to 26	50.0	1	50.0	0	2	0	
8/28/2024	05:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	33	33	23 to 33	100.0	*	*	0	0	1	
8/28/2024	06:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	37	37	27 to 37	100.0	*	*	0	1	0	
8/28/2024	07:00	0	0	0	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	3	38	43	33 to 43	66.7	*	*	0	3	0	
8/28/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/28/2024	09:00	0	0	0	0	0	1	5	3	0	0	0	1	0	0	0	0	0	0	10	46.1	46	36 to 46	80.0	4	40.0	0	10	0	
8/28/2024	10:00	0	0	0	0	1	5	7	6	0	0	0	0	0	0	0	0	0	0	19	41.9	46	37 to 47	89.5	4	21.1	0	18	1	
8/28/2024	11:00	0	0	0	2	4	12	19	6	2	0	0	0	0	0	0	0	0	0	45	40	44.5	34 to 44	73.3	6	13.3	0	44	1	
8/28/2024	12:00	0	0	1	0	8	10	10	5	2	0	0	0	0	0	0	0	0	0	36	38.8	45.3	36 to 46	66.7	6	16.7	0	32	4	
8/28/2024	13:00	0	0	0	0	4	9	10	7	0	0	0	0	0	0	0	0	0	0	30	40.5	45.3	36 to 46	80.0	5	16.7	0	30	0	
8/28/2024	14:00	0	0	0	0	3	9	8	9	2	0	0	0	0	0	0	0	0	0	31	41.9	48	35 to 45	67.7	7	22.6	0	27	4	
8/28/2024	15:00	0	0	0	1	2	13	15	12	1	0	0	0	0	0	0	0	0	0	44	41.5	46.3	38 to 48	77.3	10	22.7	0	40	4	
8/28/2024	16:00	0	0	0	1	10	10	12	3	1	0	0	0	0	0	0	0	0	0	37	38.4	43	33 to 43	75.7	3	8.1	0	31	6	
8/28/2024	17:00	0	0	0	3	8	14	26	11	2	0	0	0	0	0	0	0	0	0	64	40.2	44.8	34 to 44	68.8	9	14.1	0	55	9	
8/28/2024	18:00	0	0	0	3	9	19	15	8	1	1	0	0	0	0	0	0	0	0	56	39.1	45	35 to 45	64.3	8	14.3	0	49	7	
8/28/2024	19:00	0	0	0	2	5	19	25	12	1	0	0	0	0	0	0	0	0	0	64	40.4	44.5	35 to 45	78.1	7	10.9	0	55	9	
8/28/2024	20:00	0	0	0	3	8	15	13	7	2	0	0	0	0	0	0	0	0	0	48	38.9	45	35 to 45	62.5	7	14.6	0	43	5	
8/28/2024	21:00	0	0	0	3	2	6	27	14	2	0	0	0	0	0	0	0	0	0	54	41.9	46	38 to 48	85.2	14	25.9	0	52	2	
8/28/2024	22:00	0	0	1	0	2	11	25	12	1	1	0	1	0	0	0	0	0	0	54	41.9	45.7	35 to 45	75.9	10	18.5	0	52	2	
8/28/2024	23:00	0	0	0	1	2	6	8	9	1	0	0	1	0	0	0	0	0	0	28	42.2	47	38 to 48	75.0	6	21.4	0	28	0	
24 Hr Summary		0	0	3	22	75	168	235	130	20	3	0	3	1	0	0	0	0	0	660	40.5	46	36 to 46	69.8	111	16.8	0	602	58	

Outgoing Histogram

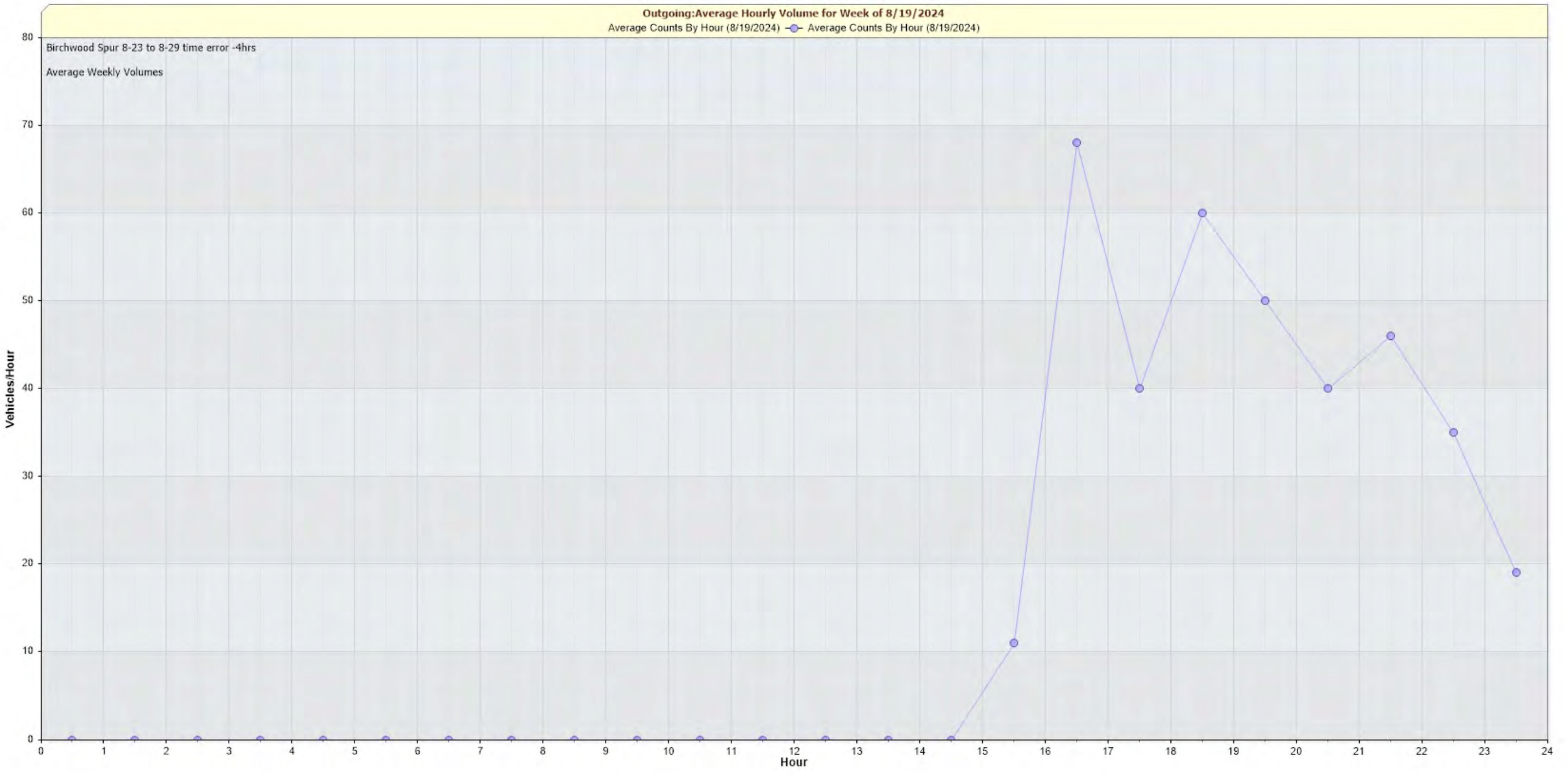
Birchwood Spur 8-23 to 8-29 time error -4hrs

from Fri-Aug-23-2024-03-00-PM to Thu-Aug-29-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	0	0	0	0	1	7	5	6	2	0	0	0	0	0	0	0	0	0	21	42	48	36 to 46	76.2	5	23.8	0	20	1	
8/29/2024	01:00	0	0	0	0	2	2	3	1	0	0	0	0	0	0	0	0	0	0	8	39.4	44	34 to 44	75.0	*	*	0	8	0	
8/29/2024	02:00	0	0	0	0	2	2	1	0	0	0	0	0	0	0	0	0	0	0	5	35.8	35	25 to 35	80.0	*	*	0	4	1	
8/29/2024	03:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	38	28 to 38	100.0	*	*	0	2	0	
8/29/2024	04:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	33.5	34	24 to 34	100.0	*	*	0	1	1	
8/29/2024	05:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	40.5	43	33 to 43	100.0	*	*	0	2	0	
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	0	0	0	0	1	3	3	1	0	1	0	0	0	0	0	0	0	0	9	41.7	47	31 to 41	77.8	2	22.2	0	9	0	
8/29/2024	10:00	0	0	0	0	1	2	4	2	1	0	0	0	0	0	0	0	0	0	10	41.9	45	33 to 43	70.0	2	20.0	0	9	1	
8/29/2024	11:00	0	0	0	2	6	14	19	5	1	0	0	0	0	0	0	0	0	0	47	39.5	43.8	34 to 44	74.5	4	8.5	0	45	2	
8/29/2024	12:00	0	0	1	2	7	9	18	5	0	1	0	0	0	0	0	0	0	0	43	39	44	35 to 45	65.1	5	11.6	0	38	5	
8/29/2024	13:00	0	2	0	2	6	12	13	2	2	0	0	0	0	0	0	0	0	0	39	37.8	42	32 to 42	66.7	4	10.3	0	34	5	
8/29/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	2	1	6	29	53	67	22	6	2	0	0	0	0	0	0	0	0	188	39.3	45	35 to 45	68.1	22	11.7	0	172	16	

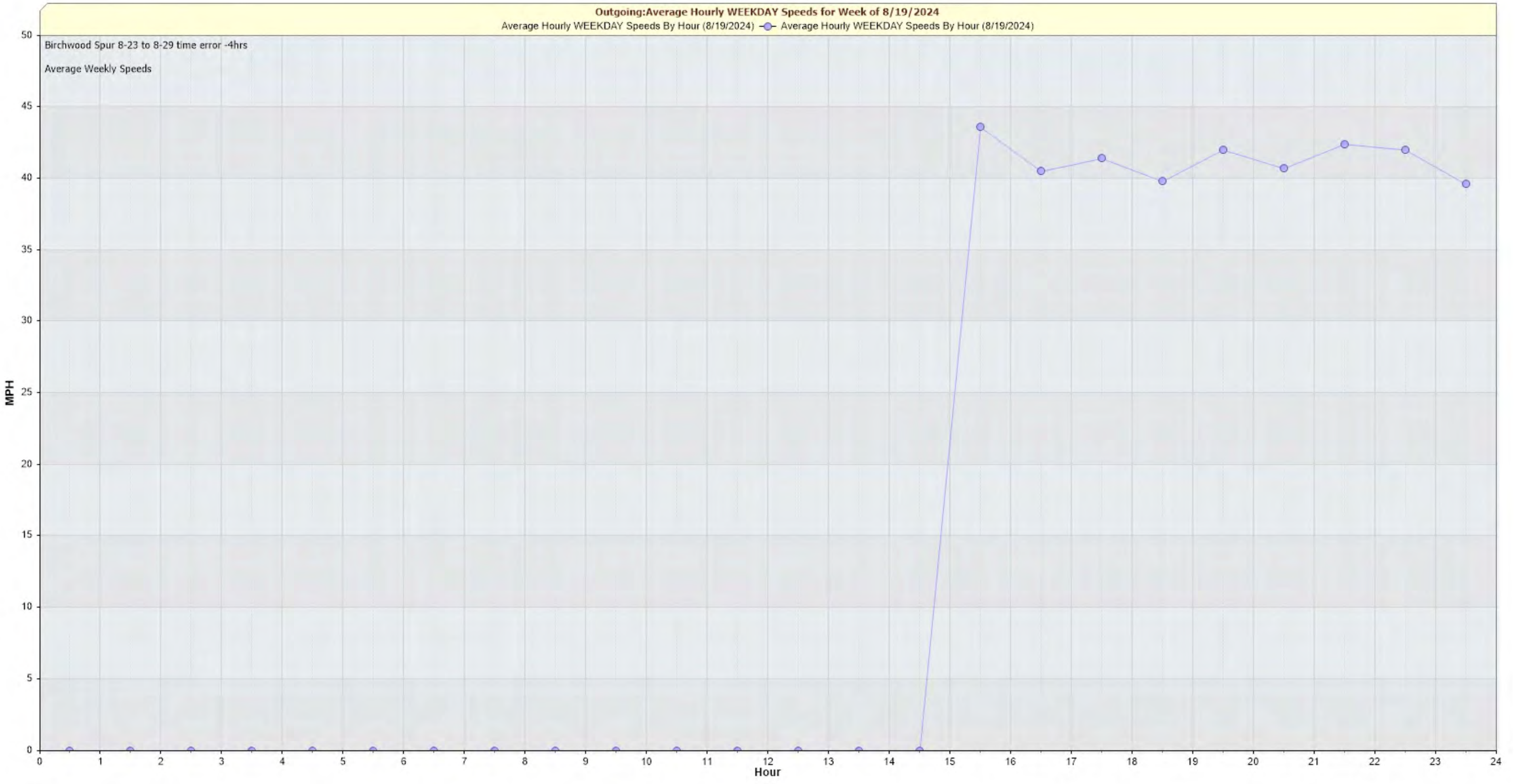
Outgoing: Average Hourly Volume for Week of 8/19/2024
Average Counts By Hour (8/19/2024) — Average Counts By Hour (8/19/2024)

Birchwood Spur 8-23 to 8-29 time error -4hrs
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 8/19/2024
Average Hourly WEEKDAY Speeds By Hour (8/19/2024) — Average Hourly WEEKDAY Speeds By Hour (8/19/2024)

Birchwood Spur 8-23 to 8-29 time error -4hrs
Average Weekly Speeds



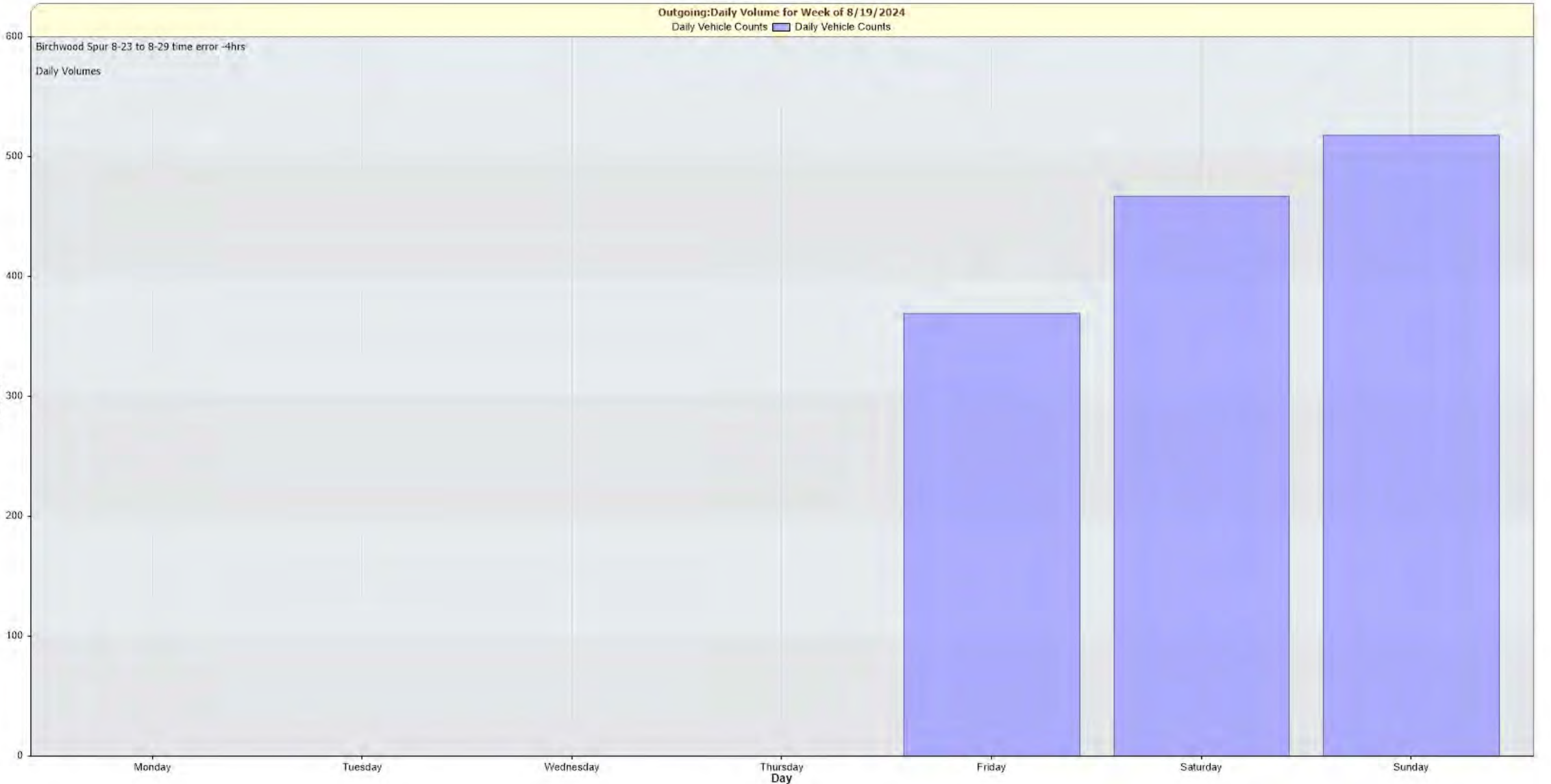
Outgoing:Daily Volume for Week of 8/19/2024

Daily Vehicle Counts

Birchwood Spur 8-23 to 8-29 time error -4hrs

Daily Volumes

Vehicles



Monday

Tuesday

Wednesday

Thursday
Day

Friday

Saturday

Sunday

For Project: Pilots Rd Southeast 8-29 to 9-4 time error -1hr
 Project Notes:
 Location/Name: Incoming
 Report Generated: 10/3/2024 3:30:59 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 8/29/2024 11:00:00 AM through 9/4/2024 10:59:59 AM
 85th Percentile Speed: 34 MPH
 85th Percentile Vehicles: 1642
 Max Speed: 52 MPH on 8/30/2024 7:36:08 AM
 Total Vehicles: 1932
 AADT: 322

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	264	276
AM Peak	8:00 AM 23	20
PM Peak	3:00 PM 27	26

Speed

Speed Limit: 45
 85th Percentile Speed: 34
 50th Percentile Speed: 30
 10 MPH Pace Interval: 24.0 MPH to 34.0 MPH
 Average Speed: 29.62

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	0	0	2	0	0
% over limit	0.0	0.0	0.0	0.0	0.5	0.0	0.0
Avg Speeder	0.0	0.0	0.0	0.0	49.0	0.0	0.0
Avg Speed	29.4	30.0	30.4	29.6	29.9	29.2	29.2

Class Counts

	Number	%
VEH_SM	3	0.2
VEH_MED	1824	94.4
VEH_LG	105	5.4
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/29/2024 12:00:00 PM	27.0	1	1	27	0.0	0.0%	-1.0
8/29/2024 1:00:00 PM	36.0	16	19	38	0.0	0.0%	31.9
8/29/2024 2:00:00 PM	36.0	14	16	37	0.0	0.0%	30.8
8/29/2024 3:00:00 PM	31.0	17	20	33	0.0	0.0%	28.1
8/29/2024 4:00:00 PM	33.0	19	22	35	0.0	0.0%	29.0
8/29/2024 5:00:00 PM	34.0	27	32	42	0.0	0.0%	29.0
8/29/2024 6:00:00 PM	35.0	19	22	41	0.0	0.0%	31.1
8/29/2024 7:00:00 PM	33.0	15	18	38	0.0	0.0%	30.8
8/29/2024 8:00:00 PM	32.0	16	19	40	0.0	0.0%	28.6
8/29/2024 9:00:00 PM	33.0	11	13	36	0.0	0.0%	30.0
8/29/2024 10:00:00 PM	34.0	14	16	36	0.0	0.0%	29.1
8/29/2024 11:00:00 PM	33.0	3	3	33	0.0	0.0%	27.0
8/30/2024 12:00:00 AM	31.0	5	6	35	0.0	0.0%	26.7
8/30/2024 1:00:00 AM	30.0	3	4	32	0.0	0.0%	27.8
8/30/2024 2:00:00 AM	31.0	1	1	31	0.0	0.0%	31.0
8/30/2024 3:00:00 AM	28.0	3	3	28	0.0	0.0%	26.3
8/30/2024 4:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
8/30/2024 5:00:00 AM	31.0	2	2	31	0.0	0.0%	28.0
8/30/2024 6:00:00 AM	32.0	2	2	32	0.0	0.0%	30.0
8/30/2024 7:00:00 AM	43.0	7	8	45	0.0	0.0%	33.8
8/30/2024 8:00:00 AM	32.0	12	14	52	52.0	7.1%	30.6
8/30/2024 9:00:00 AM	33.0	17	20	46	46.0	5.0%	31.1
8/30/2024 10:00:00 AM	32.0	22	26	38	0.0	0.0%	29.3
8/30/2024 11:00:00 AM	35.0	24	28	38	0.0	0.0%	30.2
8/30/2024 12:00:00 PM	36.0	17	20	39	0.0	0.0%	30.1
8/30/2024 1:00:00 PM	31.0	19	22	38	0.0	0.0%	29.0
8/30/2024 2:00:00 PM	33.0	15	18	40	0.0	0.0%	31.1
8/30/2024 3:00:00 PM	38.0	11	13	39	0.0	0.0%	33.0
8/30/2024 4:00:00 PM	31.0	19	22	40	0.0	0.0%	28.1
8/30/2024 5:00:00 PM	34.0	31	36	37	0.0	0.0%	29.3
8/30/2024 6:00:00 PM	35.0	24	28	40	0.0	0.0%	30.6
8/30/2024 7:00:00 PM	32.0	26	30	36	0.0	0.0%	29.4
8/30/2024 8:00:00 PM	33.0	21	25	38	0.0	0.0%	28.8
8/30/2024 9:00:00 PM	35.0	14	17	37	0.0	0.0%	32.2
8/30/2024 10:00:00 PM	33.0	11	13	38	0.0	0.0%	30.5
8/30/2024 11:00:00 PM	31.0	10	12	33	0.0	0.0%	26.7
8/31/2024 12:00:00 AM	26.0	3	3	26	0.0	0.0%	23.3
8/31/2024 1:00:00 AM	27.0	4	5	31	0.0	0.0%	26.4
8/31/2024 2:00:00 AM	19.0	2	2	19	0.0	0.0%	18.5
8/31/2024 3:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
8/31/2024 4:00:00 AM	40.0	1	1	40	0.0	0.0%	40.0
8/31/2024 5:00:00 AM	**No Data**						
8/31/2024 6:00:00 AM	34.0	1	1	34	0.0	0.0%	34.0
8/31/2024 7:00:00 AM	**No Data**						
8/31/2024 8:00:00 AM	28.0	3	4	29	0.0	0.0%	27.5
8/31/2024 9:00:00 AM	36.0	6	7	42	0.0	0.0%	32.1
8/31/2024 10:00:00 AM	32.0	18	21	36	0.0	0.0%	28.9

Incoming Summary
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

8/31/2024 11:00:00 AM	34.0	15	18	40	0.0	0.0%	31.3
8/31/2024 12:00:00 PM	32.0	20	23	34	0.0	0.0%	28.5
8/31/2024 1:00:00 PM	33.0	25	29	34	0.0	0.0%	29.0
8/31/2024 2:00:00 PM	33.0	19	22	38	0.0	0.0%	29.4
8/31/2024 3:00:00 PM	32.0	19	22	34	0.0	0.0%	29.3
8/31/2024 4:00:00 PM	31.0	26	31	38	0.0	0.0%	27.7
8/31/2024 5:00:00 PM	34.0	15	18	39	0.0	0.0%	31.2
8/31/2024 6:00:00 PM	34.0	16	19	36	0.0	0.0%	29.9
8/31/2024 7:00:00 PM	34.0	20	23	37	0.0	0.0%	29.6
8/31/2024 8:00:00 PM	35.0	11	13	40	0.0	0.0%	31.9
8/31/2024 9:00:00 PM	31.0	8	9	34	0.0	0.0%	29.0
8/31/2024 10:00:00 PM	33.0	8	9	33	0.0	0.0%	28.4
8/31/2024 11:00:00 PM	32.0	10	12	36	0.0	0.0%	28.0
9/1/2024 12:00:00 AM	29.0	9	11	32	0.0	0.0%	26.3
9/1/2024 1:00:00 AM	31.0	2	2	31	0.0	0.0%	27.0
9/1/2024 2:00:00 AM	35.0	3	3	35	0.0	0.0%	30.3
9/1/2024 3:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
9/1/2024 4:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
9/1/2024 5:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
9/1/2024 6:00:00 AM	**No Data**						
9/1/2024 7:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
9/1/2024 8:00:00 AM	31.0	3	4	32	0.0	0.0%	29.3
9/1/2024 9:00:00 AM	32.0	8	10	39	0.0	0.0%	29.7
9/1/2024 10:00:00 AM	33.0	9	11	37	0.0	0.0%	28.7
9/1/2024 11:00:00 AM	34.0	17	20	42	0.0	0.0%	31.5
9/1/2024 12:00:00 PM	34.0	25	29	36	0.0	0.0%	29.6
9/1/2024 1:00:00 PM	34.0	23	27	41	0.0	0.0%	28.9
9/1/2024 2:00:00 PM	33.0	20	23	36	0.0	0.0%	28.5
9/1/2024 3:00:00 PM	32.0	20	23	39	0.0	0.0%	29.3
9/1/2024 4:00:00 PM	34.0	15	18	36	0.0	0.0%	28.1
9/1/2024 5:00:00 PM	33.0	18	21	36	0.0	0.0%	28.5
9/1/2024 6:00:00 PM	34.0	24	28	36	0.0	0.0%	29.9
9/1/2024 7:00:00 PM	32.0	21	25	35	0.0	0.0%	29.0
9/1/2024 8:00:00 PM	33.0	14	17	36	0.0	0.0%	29.5
9/1/2024 9:00:00 PM	34.0	11	13	39	0.0	0.0%	31.5
9/1/2024 10:00:00 PM	32.0	16	19	36	0.0	0.0%	27.8
9/1/2024 11:00:00 PM	34.0	5	6	37	0.0	0.0%	29.5
9/2/2024 12:00:00 AM	24.0	3	4	36	0.0	0.0%	24.3
9/2/2024 1:00:00 AM	**No Data**						
9/2/2024 2:00:00 AM	36.0	2	2	36	0.0	0.0%	30.5
9/2/2024 3:00:00 AM	**No Data**						
9/2/2024 4:00:00 AM	**No Data**						
9/2/2024 5:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
9/2/2024 6:00:00 AM	**No Data**						
9/2/2024 7:00:00 AM	35.0	3	4	37	0.0	0.0%	33.5
9/2/2024 8:00:00 AM	31.0	3	4	39	0.0	0.0%	31.0
9/2/2024 9:00:00 AM	31.0	3	3	31	0.0	0.0%	30.3
9/2/2024 10:00:00 AM	34.0	7	8	35	0.0	0.0%	30.6

Incoming Summary
 Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

9/2/2024 11:00:00 AM	32.0	8	9	35	0.0	0.0%	29.8
9/2/2024 12:00:00 PM	33.0	16	19	41	0.0	0.0%	30.1
9/2/2024 1:00:00 PM	32.0	26	31	39	0.0	0.0%	28.5
9/2/2024 2:00:00 PM	35.0	20	24	37	0.0	0.0%	28.6
9/2/2024 3:00:00 PM	34.0	19	22	37	0.0	0.0%	29.1
9/2/2024 4:00:00 PM	32.0	30	35	38	0.0	0.0%	29.0
9/2/2024 5:00:00 PM	35.0	17	20	41	0.0	0.0%	31.5
9/2/2024 6:00:00 PM	33.0	12	14	35	0.0	0.0%	30.4
9/2/2024 7:00:00 PM	32.0	10	12	37	0.0	0.0%	27.7
9/2/2024 8:00:00 PM	34.0	16	19	37	0.0	0.0%	29.7
9/2/2024 9:00:00 PM	33.0	14	16	39	0.0	0.0%	30.5
9/2/2024 10:00:00 PM	31.0	14	16	36	0.0	0.0%	27.5
9/2/2024 11:00:00 PM	28.0	5	6	31	0.0	0.0%	26.8
9/3/2024 12:00:00 AM	37.0	2	2	37	0.0	0.0%	34.0
9/3/2024 1:00:00 AM	32.0	3	4	34	0.0	0.0%	30.0
9/3/2024 2:00:00 AM	**No Data**						
9/3/2024 3:00:00 AM	23.0	1	1	23	0.0	0.0%	23.0
9/3/2024 4:00:00 AM	30.0	1	1	30	0.0	0.0%	30.0
9/3/2024 5:00:00 AM	30.0	2	2	30	0.0	0.0%	29.0
9/3/2024 6:00:00 AM	37.0	2	2	37	0.0	0.0%	32.0
9/3/2024 7:00:00 AM	34.0	11	13	44	0.0	0.0%	32.2
9/3/2024 8:00:00 AM	36.0	11	13	36	0.0	0.0%	31.1
9/3/2024 9:00:00 AM	33.0	26	31	36	0.0	0.0%	29.6
9/3/2024 10:00:00 AM	35.0	29	34	38	0.0	0.0%	31.3
9/3/2024 11:00:00 AM	33.0	19	22	40	0.0	0.0%	31.0
9/3/2024 12:00:00 PM	33.0	23	27	35	0.0	0.0%	29.7
9/3/2024 1:00:00 PM	32.0	22	26	38	0.0	0.0%	29.9
9/3/2024 2:00:00 PM	33.0	13	15	41	0.0	0.0%	29.5
9/3/2024 3:00:00 PM	34.0	21	25	39	0.0	0.0%	29.3
9/3/2024 4:00:00 PM	33.0	25	29	36	0.0	0.0%	29.1
9/3/2024 5:00:00 PM	33.0	19	22	36	0.0	0.0%	28.8
9/3/2024 6:00:00 PM	34.0	19	22	38	0.0	0.0%	29.5
9/3/2024 7:00:00 PM	34.0	25	29	37	0.0	0.0%	29.6
9/3/2024 8:00:00 PM	34.0	19	22	40	0.0	0.0%	30.9
9/3/2024 9:00:00 PM	33.0	15	18	39	0.0	0.0%	29.6
9/3/2024 10:00:00 PM	33.0	7	8	34	0.0	0.0%	31.0
9/3/2024 11:00:00 PM	34.0	5	6	36	0.0	0.0%	30.2
9/4/2024 12:00:00 AM	30.0	2	2	30	0.0	0.0%	28.0
9/4/2024 1:00:00 AM	**No Data**						
9/4/2024 2:00:00 AM	43.0	2	2	43	0.0	0.0%	37.5
9/4/2024 3:00:00 AM	30.0	2	2	30	0.0	0.0%	25.0
9/4/2024 4:00:00 AM	**No Data**						
9/4/2024 5:00:00 AM	33.0	3	3	33	0.0	0.0%	30.0
9/4/2024 6:00:00 AM	31.0	2	2	31	0.0	0.0%	28.5
9/4/2024 7:00:00 AM	34.0	12	14	44	0.0	0.0%	31.3
9/4/2024 8:00:00 AM	35.0	9	11	38	0.0	0.0%	31.5
9/4/2024 9:00:00 AM	34.0	35	41	42	0.0	0.0%	29.9
9/4/2024 10:00:00 AM	35.0	20	24	39	0.0	0.0%	31.0

Incoming Summary
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

9/4/2024 11:00:00 AM	34.0	8	9	39	0.0	0.0%	28.8
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Incoming Summary
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/30/2024 12:00:00 AM	34.0	176	207	42	0.0	0.0%	29.6
8/31/2024 12:00:00 AM	34.0	313	368	52	49.0	0.5%	29.9
9/1/2024 12:00:00 AM	33.0	256	301	42	0.0	0.0%	29.2
9/2/2024 12:00:00 AM	34.0	261	307	42	0.0	0.0%	29.2
9/3/2024 12:00:00 AM	34.0	227	267	41	0.0	0.0%	29.4
9/4/2024 12:00:00 AM	33.0	318	374	44	0.0	0.0%	30.0
9/4/2024 10:59:59 AM	35.0	92	108	44	0.0	0.0%	30.4

Incoming Weekly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	4	5	2	4	3.5	30
1 - 2	*	*	*	*	1	2	3	1	2.5	31
2 - 3	*	*	*	*	3	1	1	3	1	28
3 - 4	*	*	*	*	1	1	1	1	1	28
4 - 5	*	*	*	*	2	0	1	2	0.5	31
5 - 6	*	*	*	*	2	1	0	2	0.5	32
6 - 7	*	*	*	*	8	0	1	8	0.5	43
7 - 8	*	*	*	*	14	4	4	14	4	32
8 - 9	*	*	*	*	20	7	10	20	8.5	33
9 - 10	*	*	*	*	26	21	11	26	16	32
10 - 11	*	*	*	*	28	18	20	28	19	34.7
11 - 12	*	*	*	1	20	23	29	10.5	26	31.25
12 - 13	*	*	*	19	22	29	27	20.5	28	33.15
13 - 14	*	*	*	16	18	22	23	17	22.5	34.25
14 - 15	*	*	*	20	13	22	23	16.5	22.5	34.1
15 - 16	*	*	*	22	22	31	18	22	24.5	32
16 - 17	*	*	*	32	36	18	21	34	19.5	33.75
17 - 18	*	*	*	22	28	19	28	25	23.5	34.75
18 - 19	*	*	*	18	30	23	25	24	24	32.35
19 - 20	*	*	*	19	25	13	17	22	15	32.25
20 - 21	*	*	*	13	17	9	13	15	11	33.75
21 - 22	*	*	*	16	13	9	19	14.5	14	33.5
22 - 23	*	*	*	3	12	12	6	7.5	9	32
23 - 24	*	*	*	6	3	11	4	4.5	7.5	28.5
Totals	0	0	0	207	368	301	307			
% of Total	0%	0%	0%	17.5%	31.11%	25.44%	25.95%			

Incoming Weekly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	0	4	0	*	*	*	*	1.33	0	32
1 - 2	2	0	2	*	*	*	*	1.33	0	39.5
2 - 3	0	1	2	*	*	*	*	1	0	26.5
3 - 4	0	1	0	*	*	*	*	0.33	0	30
4 - 5	1	2	3	*	*	*	*	2	0	30.67
5 - 6	0	2	2	*	*	*	*	1.33	0	34
6 - 7	4	13	14	*	*	*	*	10.33	0	34.33
7 - 8	4	13	11	*	*	*	*	9.33	0	33.77
8 - 9	3	31	41	*	*	*	*	25	0	32.53
9 - 10	8	34	24	*	*	*	*	22	0	34.5
10 - 11	9	22	9	*	*	*	*	13.33	0	33
11 - 12	19	27	*	*	*	*	*	23	0	32.65
12 - 13	31	26	*	*	*	*	*	28.5	0	32
13 - 14	24	15	*	*	*	*	*	19.5	0	33.65
14 - 15	22	25	*	*	*	*	*	23.5	0	34
15 - 16	35	29	*	*	*	*	*	32	0	32.1
16 - 17	20	22	*	*	*	*	*	21	0	33.85
17 - 18	14	22	*	*	*	*	*	18	0	33.5
18 - 19	12	29	*	*	*	*	*	20.5	0	33
19 - 20	19	22	*	*	*	*	*	20.5	0	33.75
20 - 21	16	18	*	*	*	*	*	17	0	32.75
21 - 22	16	8	*	*	*	*	*	12	0	32
22 - 23	6	6	*	*	*	*	*	6	0	31
23 - 24	2	2	*	*	*	*	*	2	0	33.5
Totals	267	374	108	0	0	0	0			
% of Total	35.65%	49.93%	14.42%	0%	0%	0%	0%			

Incoming Monthly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	4	5	*	4	5	30
1 - 2	*	*	*	*	1	2	*	1	2	31
2 - 3	*	*	*	*	3	1	*	3	1	28
3 - 4	*	*	*	*	1	1	*	1	1	28
4 - 5	*	*	*	*	2	0	*	2	0	31
5 - 6	*	*	*	*	2	1	*	2	1	32
6 - 7	*	*	*	*	8	0	*	8	0	43
7 - 8	*	*	*	*	14	4	*	14	4	32
8 - 9	*	*	*	*	20	7	*	20	7	33
9 - 10	*	*	*	*	26	21	*	26	21	32
10 - 11	*	*	*	*	28	18	*	28	18	34.7
11 - 12	*	*	*	1	20	23	*	10.5	23	31.25
12 - 13	*	*	*	19	22	29	*	20.5	29	33.15
13 - 14	*	*	*	16	18	22	*	17	22	34.25
14 - 15	*	*	*	20	13	22	*	16.5	22	34.1
15 - 16	*	*	*	22	22	31	*	22	31	32
16 - 17	*	*	*	32	36	18	*	34	18	33.75
17 - 18	*	*	*	22	28	19	*	25	19	34.75
18 - 19	*	*	*	18	30	23	*	24	23	32.35
19 - 20	*	*	*	19	25	13	*	22	13	32.25
20 - 21	*	*	*	13	17	9	*	15	9	33.75
21 - 22	*	*	*	16	13	9	*	14.5	9	33.5
22 - 23	*	*	*	3	12	12	*	7.5	12	32
23 - 24	*	*	*	6	3	11	*	4.5	11	28.5
Totals	0	0	0	207	368	301	0			
% of Total	0%	0%	0%	23.63%	42.01%	34.36%	0%			

Incoming Monthly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	0	4	0	*	*	*	2	1.33	2	32
1 - 2	2	0	2	*	*	*	3	1.33	3	39.5
2 - 3	0	1	2	*	*	*	1	1	1	26.5
3 - 4	0	1	0	*	*	*	1	0.33	1	30
4 - 5	1	2	3	*	*	*	1	2	1	30.67
5 - 6	0	2	2	*	*	*	0	1.33	0	34
6 - 7	4	13	14	*	*	*	1	10.33	1	34.33
7 - 8	4	13	11	*	*	*	4	9.33	4	33.77
8 - 9	3	31	41	*	*	*	10	25	10	32.53
9 - 10	8	34	24	*	*	*	11	22	11	34.5
10 - 11	9	22	9	*	*	*	20	13.33	20	33
11 - 12	19	27	*	*	*	*	29	23	29	32.65
12 - 13	31	26	*	*	*	*	27	28.5	27	32
13 - 14	24	15	*	*	*	*	23	19.5	23	33.65
14 - 15	22	25	*	*	*	*	23	23.5	23	34
15 - 16	35	29	*	*	*	*	18	32	18	32.1
16 - 17	20	22	*	*	*	*	21	21	21	33.85
17 - 18	14	22	*	*	*	*	28	18	28	33.5
18 - 19	12	29	*	*	*	*	25	20.5	25	33
19 - 20	19	22	*	*	*	*	17	20.5	17	33.75
20 - 21	16	18	*	*	*	*	13	17	13	32.75
21 - 22	16	8	*	*	*	*	19	12	19	32
22 - 23	6	6	*	*	*	*	6	6	6	31
23 - 24	2	2	*	*	*	*	4	2	4	33.5
Totals	267	374	108	0	0	0	307			
% of Total	25.28%	35.42%	10.23%	0%	0%	0%	29.07%			

Incoming Weekly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	27.75	26.4	27	27.75	26.57	30
1 - 2	*	*	*	*	31	18.5	30.33	31	25.6	31
2 - 3	*	*	*	*	26.33	29	28	26.33	28.5	28
3 - 4	*	*	*	*	28	40	26	28	33	28
4 - 5	*	*	*	*	28	0	28	28	28	31
5 - 6	*	*	*	*	30	34	0	30	34	32
6 - 7	*	*	*	*	33.75	0	26	33.75	26	43
7 - 8	*	*	*	*	30.57	27.5	29.25	30.57	28.38	32
8 - 9	*	*	*	*	31.15	32.14	29.7	31.15	30.71	33
9 - 10	*	*	*	*	29.35	28.9	28.73	29.35	28.84	32
10 - 11	*	*	*	*	30.21	31.28	31.45	30.21	31.37	34.7
11 - 12	*	*	*	27	30.15	28.52	29.55	30	29.1	31.25
12 - 13	*	*	*	31.95	29.05	28.97	28.89	30.39	28.93	33.15
13 - 14	*	*	*	30.75	31.06	29.36	28.48	30.91	28.91	34.25
14 - 15	*	*	*	28.1	33	29.27	29.35	30.03	29.31	34.1
15 - 16	*	*	*	29.05	28.09	27.68	28.11	28.57	27.84	32
16 - 17	*	*	*	29.03	29.33	31.17	28.48	29.19	29.72	33.75
17 - 18	*	*	*	31.09	30.64	29.89	29.89	30.84	29.89	34.75
18 - 19	*	*	*	30.83	29.43	29.57	29.04	29.96	29.29	32.35
19 - 20	*	*	*	28.58	28.84	31.92	29.53	28.73	30.57	32.25
20 - 21	*	*	*	30	32.18	29	31.54	31.23	30.5	33.75
21 - 22	*	*	*	29.06	30.46	28.44	27.84	29.69	28.04	33.5
22 - 23	*	*	*	27	26.67	28	29.5	26.73	28.5	32
23 - 24	*	*	*	26.67	23.33	26.27	24.25	25.56	25.73	28.5
Totals	0	0	0	29.6	29.9	29.2	29.2			
% of Total	0%	0%	0%	25.11%	25.36%	24.77%	24.77%			

Incoming Weekly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	0	30	0	*	*	*	*	30	*	32
1 - 2	30.5	0	37.5	*	*	*	*	34	*	39.5
2 - 3	0	23	25	*	*	*	*	24.33	*	26.5
3 - 4	0	30	0	*	*	*	*	30	*	30
4 - 5	29	29	30	*	*	*	*	29.5	*	30.67
5 - 6	0	32	28.5	*	*	*	*	30.25	*	34
6 - 7	33.5	32.23	31.29	*	*	*	*	31.97	*	34.33
7 - 8	31	31.08	31.55	*	*	*	*	31.25	*	33.77
8 - 9	30.33	29.58	29.85	*	*	*	*	29.76	*	32.53
9 - 10	30.62	31.26	31.04	*	*	*	*	31.11	*	34.5
10 - 11	29.78	31	28.78	*	*	*	*	30.23	*	33
11 - 12	30.05	29.74	*	*	*	*	*	29.87	*	32.65
12 - 13	28.48	29.92	*	*	*	*	*	29.14	*	32
13 - 14	28.62	29.47	*	*	*	*	*	28.95	*	33.65
14 - 15	29.09	29.32	*	*	*	*	*	29.21	*	34
15 - 16	29.03	29.1	*	*	*	*	*	29.06	*	32.1
16 - 17	31.45	28.82	*	*	*	*	*	30.07	*	33.85
17 - 18	30.36	29.45	*	*	*	*	*	29.81	*	33.5
18 - 19	27.67	29.55	*	*	*	*	*	29	*	33
19 - 20	29.68	30.86	*	*	*	*	*	30.32	*	33.75
20 - 21	30.5	29.56	*	*	*	*	*	30	*	32.75
21 - 22	27.5	31	*	*	*	*	*	28.67	*	32
22 - 23	26.83	30.17	*	*	*	*	*	28.5	*	31
23 - 24	34	28	*	*	*	*	*	31	*	33.5
Totals	29.4	30	30.4	0	0	0	0			
% of Total	32.74%	33.41%	33.85%	0%	0%	0%	0%			

Incoming Monthly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	27.75	26.4	*	27.75	26.4	30
1 - 2	*	*	*	*	31	18.5	*	31	18.5	31
2 - 3	*	*	*	*	26.33	29	*	26.33	29	28
3 - 4	*	*	*	*	28	40	*	28	40	28
4 - 5	*	*	*	*	28	0	*	28	*	31
5 - 6	*	*	*	*	30	34	*	30	34	32
6 - 7	*	*	*	*	33.75	0	*	33.75	*	43
7 - 8	*	*	*	*	30.57	27.5	*	30.57	27.5	32
8 - 9	*	*	*	*	31.15	32.14	*	31.15	32.14	33
9 - 10	*	*	*	*	29.35	28.9	*	29.35	28.9	32
10 - 11	*	*	*	*	30.21	31.28	*	30.21	31.28	34.7
11 - 12	*	*	*	27	30.15	28.52	*	30	28.52	31.25
12 - 13	*	*	*	31.95	29.05	28.97	*	30.39	28.97	33.15
13 - 14	*	*	*	30.75	31.06	29.36	*	30.91	29.36	34.25
14 - 15	*	*	*	28.1	33	29.27	*	30.03	29.27	34.1
15 - 16	*	*	*	29.05	28.09	27.68	*	28.57	27.68	32
16 - 17	*	*	*	29.03	29.33	31.17	*	29.19	31.17	33.75
17 - 18	*	*	*	31.09	30.64	29.89	*	30.84	29.89	34.75
18 - 19	*	*	*	30.83	29.43	29.57	*	29.96	29.57	32.35
19 - 20	*	*	*	28.58	28.84	31.92	*	28.73	31.92	32.25
20 - 21	*	*	*	30	32.18	29	*	31.23	29	33.75
21 - 22	*	*	*	29.06	30.46	28.44	*	29.69	28.44	33.5
22 - 23	*	*	*	27	26.67	28	*	26.73	28	32
23 - 24	*	*	*	26.67	23.33	26.27	*	25.56	26.27	28.5
Totals	0	0	0	29.6	29.9	29.2	0			
% of Total	0%	0%	0%	33.37%	33.71%	32.92%	0%			

Incoming Monthly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	0	30	0	*	*	*	27	30	27	32
1 - 2	30.5	0	37.5	*	*	*	30.33	34	30.33	39.5
2 - 3	0	23	25	*	*	*	28	24.33	28	26.5
3 - 4	0	30	0	*	*	*	26	30	26	30
4 - 5	29	29	30	*	*	*	28	29.5	28	30.67
5 - 6	0	32	28.5	*	*	*	0	30.25	0	34
6 - 7	33.5	32.23	31.29	*	*	*	26	31.97	26	34.33
7 - 8	31	31.08	31.55	*	*	*	29.25	31.25	29.25	33.77
8 - 9	30.33	29.58	29.85	*	*	*	29.7	29.76	29.7	32.53
9 - 10	30.62	31.26	31.04	*	*	*	28.73	31.11	28.73	34.5
10 - 11	29.78	31	28.78	*	*	*	31.45	30.23	31.45	33
11 - 12	30.05	29.74	*	*	*	*	29.55	29.87	29.55	32.65
12 - 13	28.48	29.92	*	*	*	*	28.89	29.14	28.89	32
13 - 14	28.62	29.47	*	*	*	*	28.48	28.95	28.48	33.65
14 - 15	29.09	29.32	*	*	*	*	29.35	29.21	29.35	34
15 - 16	29.03	29.1	*	*	*	*	28.11	29.06	28.11	32.1
16 - 17	31.45	28.82	*	*	*	*	28.48	30.07	28.48	33.85
17 - 18	30.36	29.45	*	*	*	*	29.89	29.81	29.89	33.5
18 - 19	27.67	29.55	*	*	*	*	29.04	29	29.04	33
19 - 20	29.68	30.86	*	*	*	*	29.53	30.32	29.53	33.75
20 - 21	30.5	29.56	*	*	*	*	31.54	30	31.54	32.75
21 - 22	27.5	31	*	*	*	*	27.84	28.67	27.84	32
22 - 23	26.83	30.17	*	*	*	*	29.5	28.5	29.5	31
23 - 24	34	28	*	*	*	*	24.25	31	24.25	33.5
Totals	29.4	30	30.4	0	0	0	29.2			
% of Total	24.71%	25.21%	25.55%	0%	0%	0%	24.54%			

Incoming Weekly EightyFifthSpeeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	30	27	31	30	29	30
1 - 2	*	*	*	*	31	19	35	31	27	31
2 - 3	*	*	*	*	28	29	28	28	28.5	28
3 - 4	*	*	*	*	28	40	26	28	33	28
4 - 5	*	*	*	*	31	0	28	31	28	31
5 - 6	*	*	*	*	32	34	0	32	34	32
6 - 7	*	*	*	*	43	0	26	43	26	43
7 - 8	*	*	*	*	32	28	31	32	29.5	32
8 - 9	*	*	*	*	33	36	32	33	34	33
9 - 10	*	*	*	*	32	32	33	32	32.5	32
10 - 11	*	*	*	*	34.7	34	34	34.7	34	34.7
11 - 12	*	*	*	27	35.5	31.5	33.5	31.25	32.5	31.25
12 - 13	*	*	*	35.5	30.8	32.3	33.7	33.15	33	33.15
13 - 14	*	*	*	35.5	33	33	33	34.25	33	34.25
14 - 15	*	*	*	30.7	37.5	32	31.5	34.1	31.75	34.1
15 - 16	*	*	*	33	31	30.8	33.5	32	32.15	32
16 - 17	*	*	*	34	33.5	33.7	33	33.75	33.35	33.75
17 - 18	*	*	*	35	34.5	33.3	34	34.75	33.65	34.75
18 - 19	*	*	*	32.7	32	33.7	32	32.35	32.85	32.35
19 - 20	*	*	*	32	32.5	35	32.5	32.25	33.75	32.25
20 - 21	*	*	*	32.5	35	31	34	33.75	32.5	33.75
21 - 22	*	*	*	34	33	32.7	32	33.5	32.35	33.5
22 - 23	*	*	*	33	31	31.5	34	32	32.75	32
23 - 24	*	*	*	31	26	28.8	24	28.5	26.4	28.5
Totals	0	0	0	425.9	780	698.3	724.7			
% of Total	0%	0%	0%	16.2%	29.67%	26.56%	27.57%			

Incoming Weekly EightyFifthSpeeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	0	32	0	*	*	*	*	32	0	32
1 - 2	36	0	43	*	*	*	*	39.5	0	39.5
2 - 3	0	23	30	*	*	*	*	26.5	0	26.5
3 - 4	0	30	0	*	*	*	*	30	0	30
4 - 5	29	30	33	*	*	*	*	30.67	0	30.67
5 - 6	0	37	31	*	*	*	*	34	0	34
6 - 7	35	34	34	*	*	*	*	34.33	0	34.33
7 - 8	31	35.3	35	*	*	*	*	33.77	0	33.77
8 - 9	31	32.6	34	*	*	*	*	32.53	0	32.53
9 - 10	34	35	34.5	*	*	*	*	34.5	0	34.5
10 - 11	32	33	34	*	*	*	*	33	0	33
11 - 12	33	32.3	*	*	*	*	*	32.65	0	32.65
12 - 13	32	32	*	*	*	*	*	32	0	32
13 - 14	34.3	33	*	*	*	*	*	33.65	0	33.65
14 - 15	34	34	*	*	*	*	*	34	0	34
15 - 16	32	32.2	*	*	*	*	*	32.1	0	32.1
16 - 17	34.7	33	*	*	*	*	*	33.85	0	33.85
17 - 18	33	34	*	*	*	*	*	33.5	0	33.5
18 - 19	32	34	*	*	*	*	*	33	0	33
19 - 20	33.5	34	*	*	*	*	*	33.75	0	33.75
20 - 21	33	32.5	*	*	*	*	*	32.75	0	32.75
21 - 22	31	33	*	*	*	*	*	32	0	32
22 - 23	28	34	*	*	*	*	*	31	0	31
23 - 24	37	30	*	*	*	*	*	33.5	0	33.5
Totals	655.5	749.9	308.5	0	0	0	0			
% of Total	38.25%	43.75%	18%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	30	27	*	30	27	30
1 - 2	*	*	*	*	31	19	*	31	19	31
2 - 3	*	*	*	*	28	29	*	28	29	28
3 - 4	*	*	*	*	28	40	*	28	40	28
4 - 5	*	*	*	*	31	0	*	31	0	31
5 - 6	*	*	*	*	32	34	*	32	34	32
6 - 7	*	*	*	*	43	0	*	43	0	43
7 - 8	*	*	*	*	32	28	*	32	28	32
8 - 9	*	*	*	*	33	36	*	33	36	33
9 - 10	*	*	*	*	32	32	*	32	32	32
10 - 11	*	*	*	*	34.7	34	*	34.7	34	34.7
11 - 12	*	*	*	27	35.5	31.5	*	31.25	31.5	31.25
12 - 13	*	*	*	35.5	30.8	32.3	*	33.15	32.3	33.15
13 - 14	*	*	*	35.5	33	33	*	34.25	33	34.25
14 - 15	*	*	*	30.7	37.5	32	*	34.1	32	34.1
15 - 16	*	*	*	33	31	30.8	*	32	30.8	32
16 - 17	*	*	*	34	33.5	33.7	*	33.75	33.7	33.75
17 - 18	*	*	*	35	34.5	33.3	*	34.75	33.3	34.75
18 - 19	*	*	*	32.7	32	33.7	*	32.35	33.7	32.35
19 - 20	*	*	*	32	32.5	35	*	32.25	35	32.25
20 - 21	*	*	*	32.5	35	31	*	33.75	31	33.75
21 - 22	*	*	*	34	33	32.7	*	33.5	32.7	33.5
22 - 23	*	*	*	33	31	31.5	*	32	31.5	32
23 - 24	*	*	*	31	26	28.8	*	28.5	28.8	28.5

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	0	32	0	*	*	*	31	32	31	32
1 - 2	36	0	43	*	*	*	35	39.5	35	39.5
2 - 3	0	23	30	*	*	*	28	26.5	28	26.5
3 - 4	0	30	0	*	*	*	26	30	26	30
4 - 5	29	30	33	*	*	*	28	30.67	28	30.67
5 - 6	0	37	31	*	*	*	0	34	0	34
6 - 7	35	34	34	*	*	*	26	34.33	26	34.33
7 - 8	31	35.3	35	*	*	*	31	33.77	31	33.77
8 - 9	31	32.6	34	*	*	*	32	32.53	32	32.53
9 - 10	34	35	34.5	*	*	*	33	34.5	33	34.5
10 - 11	32	33	34	*	*	*	34	33	34	33
11 - 12	33	32.3	*	*	*	*	33.5	32.65	33.5	32.65
12 - 13	32	32	*	*	*	*	33.7	32	33.7	32
13 - 14	34.3	33	*	*	*	*	33	33.65	33	33.65
14 - 15	34	34	*	*	*	*	31.5	34	31.5	34
15 - 16	32	32.2	*	*	*	*	33.5	32.1	33.5	32.1
16 - 17	34.7	33	*	*	*	*	33	33.85	33	33.85
17 - 18	33	34	*	*	*	*	34	33.5	34	33.5
18 - 19	32	34	*	*	*	*	32	33	32	33
19 - 20	33.5	34	*	*	*	*	32.5	33.75	32.5	33.75
20 - 21	33	32.5	*	*	*	*	34	32.75	34	32.75
21 - 22	31	33	*	*	*	*	32	32	32	32
22 - 23	28	34	*	*	*	*	34	31	34	31
23 - 24	37	30	*	*	*	*	24	33.5	24	33.5

Summary of Violators

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	15	27.8	0	0.0
01:00:00	10	29.5	0	0.0
02:00:00	8	26.1	0	0.0
03:00:00	4	31.0	0	0.0
04:00:00	9	29.0	0	0.0
05:00:00	7	30.7	0	0.0
06:00:00	40	32.2	0	0.0
07:00:00	50	30.6	1	52.0
08:00:00	112	30.2	1	46.0
09:00:00	124	30.2	0	0.0
10:00:00	106	30.6	0	0.0
11:00:00	119	29.6	0	0.0
12:00:00	154	29.4	0	0.0
13:00:00	118	29.5	0	0.0
14:00:00	125	29.5	0	0.0
15:00:00	157	28.5	0	0.0
16:00:00	149	29.6	0	0.0
17:00:00	133	30.2	0	0.0
18:00:00	137	29.4	0	0.0
19:00:00	115	29.8	0	0.0
20:00:00	86	30.6	0	0.0
21:00:00	81	28.8	0	0.0
22:00:00	45	27.9	0	0.0
23:00:00	28	26.4	0	0.0

Incoming Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	11:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	27	27	17 to 27	100.0	*	*	0	1	0	
8/29/2024	12:00	0	0	1	4	8	6	0	0	0	0	0	0	0	0	0	0	0	0	19	31.9	35.5	26 to 36	84.2	*	*	3	16	0	
8/29/2024	13:00	0	0	2	3	8	3	0	0	0	0	0	0	0	0	0	0	0	0	16	30.8	35.5	27 to 37	81.3	*	*	0	15	1	
8/29/2024	14:00	0	0	1	11	8	0	0	0	0	0	0	0	0	0	0	0	0	0	20	28.1	30.7	23 to 33	100.0	*	*	0	19	1	
8/29/2024	15:00	0	0	1	11	9	1	0	0	0	0	0	0	0	0	0	0	0	0	22	29	33	25 to 35	95.5	*	*	0	22	0	
8/29/2024	16:00	1	0	3	17	6	3	2	0	0	0	0	0	0	0	0	0	0	0	32	29	34	24 to 34	75.0	*	*	0	28	4	
8/29/2024	17:00	0	0	2	7	8	4	1	0	0	0	0	0	0	0	0	0	0	0	22	31.1	35	26 to 36	81.8	*	*	0	21	1	
8/29/2024	18:00	0	0	0	5	12	1	0	0	0	0	0	0	0	0	0	0	0	0	18	30.8	32.7	24 to 34	94.4	*	*	0	17	1	
8/29/2024	19:00	0	0	4	8	5	1	1	0	0	0	0	0	0	0	0	0	0	0	19	28.6	32	23 to 33	78.9	*	*	0	18	1	
8/29/2024	20:00	0	0	1	4	7	1	0	0	0	0	0	0	0	0	0	0	0	0	13	30	32.5	23 to 33	92.3	*	*	0	13	0	
8/29/2024	21:00	0	0	1	9	4	2	0	0	0	0	0	0	0	0	0	0	0	0	16	29.1	34	26 to 36	87.5	*	*	0	14	2	
8/29/2024	22:00	0	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	27	33	23 to 33	100.0	*	*	0	3	0	
8/29/2024	23:00	0	0	3	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	6	26.7	31	18 to 28	66.7	*	*	0	6	0	
24 Hr Summary		1	0	21	81	77	23	4	0	0	0	0	0	0	0	0	0	0	0	207	29.6	34	24 to 34	81.2	*	*	3	193	11	

Incoming Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/30/2024	00:00	0	1	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	4	27.8	30	22 to 32	75.0	*	*	0	4	0
8/30/2024	01:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	31	31	21 to 31	100.0	*	*	0	0	1
8/30/2024	02:00	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26.3	28	18 to 28	100.0	*	*	0	2	1
8/30/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0
8/30/2024	04:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28	31	21 to 31	100.0	*	*	0	2	0
8/30/2024	05:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30	32	22 to 32	100.0	*	*	0	2	0
8/30/2024	06:00	0	0	0	2	3	1	1	1	0	0	0	0	0	0	0	0	0	0	8	33.8	43	25 to 35	75.0	*	*	0	8	0
8/30/2024	07:00	0	0	0	8	4	1	0	0	1	0	0	0	0	0	0	0	0	0	14	30.6	32	22 to 32	85.7	1	7.1	0	12	2
8/30/2024	08:00	0	0	0	9	9	1	0	1	0	0	0	0	0	0	0	0	0	0	20	31.1	33	24 to 34	90.0	1	5.0	0	20	0
8/30/2024	09:00	0	0	1	12	11	2	0	0	0	0	0	0	0	0	0	0	0	0	26	29.3	32	25 to 35	92.3	*	*	0	25	1
8/30/2024	10:00	0	0	3	9	10	6	0	0	0	0	0	0	0	0	0	0	0	0	28	30.2	34.7	26 to 36	85.7	*	*	0	26	2
8/30/2024	11:00	0	0	0	12	4	4	0	0	0	0	0	0	0	0	0	0	0	0	20	30.1	35.5	26 to 36	85.0	*	*	0	17	3
8/30/2024	12:00	0	0	2	10	8	2	0	0	0	0	0	0	0	0	0	0	0	0	22	29	30.8	21 to 31	86.4	*	*	0	20	2
8/30/2024	13:00	0	0	2	4	9	2	1	0	0	0	0	0	0	0	0	0	0	0	18	31.1	33	23 to 33	83.3	*	*	0	17	1
8/30/2024	14:00	0	0	0	4	4	5	0	0	0	0	0	0	0	0	0	0	0	0	13	33	37.5	29 to 39	92.3	*	*	0	13	0
8/30/2024	15:00	0	0	4	12	4	1	1	0	0	0	0	0	0	0	0	0	0	0	22	28.1	31	22 to 32	90.9	*	*	0	20	2
8/30/2024	16:00	0	0	6	13	13	4	0	0	0	0	0	0	0	0	0	0	0	0	36	29.3	33.5	27 to 37	80.6	*	*	0	32	4
8/30/2024	17:00	0	0	4	7	12	4	1	0	0	0	0	0	0	0	0	0	0	0	28	30.6	34.5	24 to 34	75.0	*	*	0	24	4
8/30/2024	18:00	0	0	1	15	12	2	0	0	0	0	0	0	0	0	0	0	0	0	30	29.4	32	25 to 35	93.3	*	*	0	30	0
8/30/2024	19:00	0	0	2	15	6	2	0	0	0	0	0	0	0	0	0	0	0	0	25	28.8	32.5	24 to 34	88.0	*	*	0	25	0
8/30/2024	20:00	0	0	1	2	7	7	0	0	0	0	0	0	0	0	0	0	0	0	17	32.2	35	27 to 37	88.2	*	*	0	17	0
8/30/2024	21:00	0	0	2	1	9	1	0	0	0	0	0	0	0	0	0	0	0	0	13	30.5	33	24 to 34	84.6	*	*	0	12	1
8/30/2024	22:00	0	0	3	6	3	0	0	0	0	0	0	0	0	0	0	0	0	0	12	26.7	31	23 to 33	83.3	*	*	0	12	0
8/30/2024	23:00	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	23.3	26	16 to 26	100.0	*	*	0	3	0
24 Hr Summary		0	2	31	149	134	45	4	2	1	0	0	0	0	0	0	0	0	0	368	29.9	34	25 to 35	81.0	2	0.5	0	344	24

Incoming Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/31/2024	00:00	0	0	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	5	26.4	27	21 to 31	100.0	*	*	0	5	0	
8/31/2024	01:00	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	18.5	19	9 to 19	100.0	*	*	0	2	0	
8/31/2024	02:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0	
8/31/2024	03:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	40	40	30 to 40	100.0	*	*	0	1	0	
8/31/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/31/2024	05:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	34	34	24 to 34	100.0	*	*	0	1	0	
8/31/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/31/2024	07:00	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	27.5	28	19 to 29	100.0	*	*	0	3	1	
8/31/2024	08:00	0	0	0	2	3	1	1	0	0	0	0	0	0	0	0	0	0	0	7	32.1	36	26 to 36	85.7	*	*	0	6	1	
8/31/2024	09:00	0	0	4	8	8	1	0	0	0	0	0	0	0	0	0	0	0	0	21	28.9	32	24 to 34	95.2	*	*	0	19	2	
8/31/2024	10:00	0	0	0	5	10	2	1	0	0	0	0	0	0	0	0	0	0	0	18	31.3	34	25 to 35	88.9	*	*	0	17	1	
8/31/2024	11:00	0	0	2	15	6	0	0	0	0	0	0	0	0	0	0	0	0	0	23	28.5	31.5	22 to 32	91.3	*	*	0	22	1	
8/31/2024	12:00	0	1	3	9	16	0	0	0	0	0	0	0	0	0	0	0	0	0	29	29	32.3	24 to 34	93.1	*	*	0	29	0	
8/31/2024	13:00	0	0	3	6	11	2	0	0	0	0	0	0	0	0	0	0	0	0	22	29.4	33	23 to 33	86.4	*	*	0	21	1	
8/31/2024	14:00	0	0	2	8	12	0	0	0	0	0	0	0	0	0	0	0	0	0	22	29.3	32	24 to 34	95.5	*	*	0	20	2	
8/31/2024	15:00	0	2	5	14	8	2	0	0	0	0	0	0	0	0	0	0	0	0	31	27.7	30.8	22 to 32	83.9	*	*	0	28	3	
8/31/2024	16:00	0	0	0	5	11	2	0	0	0	0	0	0	0	0	0	0	0	0	18	31.2	33.7	24 to 34	88.9	*	*	0	17	1	
8/31/2024	17:00	0	0	1	7	10	1	0	0	0	0	0	0	0	0	0	0	0	0	19	29.9	33.3	24 to 34	94.7	*	*	0	18	1	
8/31/2024	18:00	0	0	3	7	11	2	0	0	0	0	0	0	0	0	0	0	0	0	23	29.6	33.7	24 to 34	78.3	*	*	0	22	1	
8/31/2024	19:00	0	0	0	3	7	2	1	0	0	0	0	0	0	0	0	0	0	0	13	31.9	35	25 to 35	84.6	*	*	0	13	0	
8/31/2024	20:00	0	0	1	2	6	0	0	0	0	0	0	0	0	0	0	0	0	0	9	29	31	24 to 34	100.0	*	*	0	8	1	
8/31/2024	21:00	0	1	1	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	9	28.4	32.7	23 to 33	77.8	*	*	0	9	0	
8/31/2024	22:00	0	0	4	2	5	1	0	0	0	0	0	0	0	0	0	0	0	0	12	28	31.5	22 to 32	83.3	*	*	0	12	0	
8/31/2024	23:00	0	1	2	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	11	26.3	28.8	22 to 32	90.9	*	*	0	11	0	
24 Hr Summary		0	7	33	109	132	16	4	0	0	0	0	0	0	0	0	0	0	0	301	29.2	33	24 to 34	85.4	*	*	0	285	16	

Incoming Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/1/2024	00:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27	31	21 to 31	100.0	*	*	0	2	0	
9/1/2024	01:00	0	0	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	30.3	35	25 to 35	100.0	*	*	0	3	0	
9/1/2024	02:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0	
9/1/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	1	0	
9/1/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0	
9/1/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/1/2024	06:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	0	1	
9/1/2024	07:00	0	0	1	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	4	29.3	31	22 to 32	100.0	*	*	0	4	0	
9/1/2024	08:00	0	0	1	3	5	1	0	0	0	0	0	0	0	0	0	0	0	0	10	29.7	32	24 to 34	80.0	*	*	0	10	0	
9/1/2024	09:00	0	0	3	3	4	1	0	0	0	0	0	0	0	0	0	0	0	0	11	28.7	33	21 to 31	72.7	*	*	0	10	1	
9/1/2024	10:00	0	0	3	3	11	2	1	0	0	0	0	0	0	0	0	0	0	0	20	31.5	34	24 to 34	80.0	*	*	0	20	0	
9/1/2024	11:00	0	0	3	11	13	2	0	0	0	0	0	0	0	0	0	0	0	0	29	29.6	33.5	26 to 36	89.7	*	*	0	28	1	
9/1/2024	12:00	1	1	2	7	13	2	1	0	0	0	0	0	0	0	0	0	0	0	27	28.9	33.7	25 to 35	77.8	*	*	0	25	2	
9/1/2024	13:00	0	0	4	8	8	3	0	0	0	0	0	0	0	0	0	0	0	0	23	28.5	33	25 to 35	78.3	*	*	0	22	1	
9/1/2024	14:00	0	0	1	13	7	2	0	0	0	0	0	0	0	0	0	0	0	0	23	29.3	31.5	22 to 32	91.3	*	*	0	22	1	
9/1/2024	15:00	0	2	3	5	6	2	0	0	0	0	0	0	0	0	0	0	0	0	18	28.1	33.5	23 to 33	66.7	*	*	0	18	0	
9/1/2024	16:00	0	0	5	5	9	2	0	0	0	0	0	0	0	0	0	0	0	0	21	28.5	33	24 to 34	76.2	*	*	0	20	1	
9/1/2024	17:00	0	0	2	11	11	4	0	0	0	0	0	0	0	0	0	0	0	0	28	29.9	34	25 to 35	89.3	*	*	0	27	1	
9/1/2024	18:00	0	0	5	9	10	1	0	0	0	0	0	0	0	0	0	0	0	0	25	29	32	23 to 33	84.0	*	*	0	24	1	
9/1/2024	19:00	0	0	1	8	6	2	0	0	0	0	0	0	0	0	0	0	0	0	17	29.5	32.5	23 to 33	88.2	*	*	0	17	0	
9/1/2024	20:00	0	0	0	4	7	2	0	0	0	0	0	0	0	0	0	0	0	0	13	31.5	34	25 to 35	92.3	*	*	0	13	0	
9/1/2024	21:00	0	1	5	5	7	1	0	0	0	0	0	0	0	0	0	0	0	0	19	27.8	32	22 to 32	78.9	*	*	0	19	0	
9/1/2024	22:00	0	0	1	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	6	29.5	34	24 to 34	83.3	*	*	0	6	0	
9/1/2024	23:00	0	1	2	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	4	24.3	24	14 to 24	75.0	*	*	0	4	0	
24 Hr Summary		1	5	43	103	123	30	2	0	0	0	0	0	0	0	0	0	0	0	307	29.2	34	24 to 34	78.8	*	*	0	297	10	

Incoming Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/2/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/2/2024	01:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30.5	36	15 to 25	50.0	*	*	0	2	0
9/2/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/2/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/2/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0
9/2/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/2/2024	06:00	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	33.5	35	27 to 37	100.0	*	*	0	4	0
9/2/2024	07:00	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	31	31	21 to 31	75.0	*	*	0	4	0
9/2/2024	08:00	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	30.3	31	21 to 31	100.0	*	*	0	3	0
9/2/2024	09:00	0	0	0	3	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	8	30.6	34	25 to 35	100.0	*	*	0	7	1
9/2/2024	10:00	0	0	0	5	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	9	29.8	32	25 to 35	100.0	*	*	0	9	0
9/2/2024	11:00	0	0	4	4	8	2	1	0	0	0	0	0	0	0	0	0	0	0	0	19	30.1	33	23 to 33	84.2	*	*	0	19	0
9/2/2024	12:00	0	0	4	16	10	1	0	0	0	0	0	0	0	0	0	0	0	0	0	31	28.5	32	24 to 34	87.1	*	*	0	29	2
9/2/2024	13:00	1	0	4	9	5	5	0	0	0	0	0	0	0	0	0	0	0	0	0	24	28.6	34.3	22 to 32	70.8	*	*	0	21	3
9/2/2024	14:00	0	1	2	8	8	3	0	0	0	0	0	0	0	0	0	0	0	0	0	22	29.1	34	24 to 34	77.3	*	*	0	21	1
9/2/2024	15:00	0	0	4	15	15	1	0	0	0	0	0	0	0	0	0	0	0	0	0	35	29	32	24 to 34	88.6	*	*	0	31	4
9/2/2024	16:00	0	0	0	9	6	4	1	0	0	0	0	0	0	0	0	0	0	0	0	20	31.5	34.7	25 to 35	90.0	*	*	0	20	0
9/2/2024	17:00	0	0	1	4	8	1	0	0	0	0	0	0	0	0	0	0	0	0	0	14	30.4	33	24 to 34	92.9	*	*	0	13	1
9/2/2024	18:00	1	0	3	1	6	1	0	0	0	0	0	0	0	0	0	0	0	0	0	12	27.7	32	21 to 31	66.7	*	*	0	12	0
9/2/2024	19:00	0	1	1	6	9	2	0	0	0	0	0	0	0	0	0	0	0	0	0	19	29.7	33.5	24 to 34	84.2	*	*	0	18	1
9/2/2024	20:00	0	0	0	6	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	30.5	33	24 to 34	93.8	*	*	0	15	1
9/2/2024	21:00	0	0	4	8	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	16	27.5	31	22 to 32	93.8	*	*	0	16	0
9/2/2024	22:00	0	0	1	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	26.8	28	18 to 28	83.3	*	*	0	6	0
9/2/2024	23:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	34	37	27 to 37	100.0	*	*	0	2	0
24 Hr Summary		2	2	29	101	102	29	2	0	0	0	0	0	0	0	0	0	0	0	0	267	29.4	34	24 to 34	82.0	*	*	0	253	14

Incoming Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/3/2024	00:00	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	30	32	24 to 34	100.0	*	*	0	4	0	
9/3/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	02:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	23	23	13 to 23	100.0	*	*	0	1	0	
9/3/2024	03:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	30	30	20 to 30	100.0	*	*	0	1	0	
9/3/2024	04:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	29	30	20 to 30	100.0	*	*	0	2	0	
9/3/2024	05:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	32	37	27 to 37	100.0	*	*	0	1	1	
9/3/2024	06:00	0	0	1	2	8	0	2	0	0	0	0	0	0	0	0	0	0	0	13	32.2	34	23 to 33	76.9	*	*	0	13	0	
9/3/2024	07:00	0	0	0	5	5	3	0	0	0	0	0	0	0	0	0	0	0	0	13	31.1	35.3	26 to 36	100.0	*	*	0	13	0	
9/3/2024	08:00	0	0	2	12	14	3	0	0	0	0	0	0	0	0	0	0	0	0	31	29.6	32.6	25 to 35	90.3	*	*	0	29	2	
9/3/2024	09:00	0	0	0	11	17	6	0	0	0	0	0	0	0	0	0	0	0	0	34	31.3	35	27 to 37	94.1	*	*	0	32	2	
9/3/2024	10:00	0	0	1	5	13	2	1	0	0	0	0	0	0	0	0	0	0	0	22	31	33	23 to 33	86.4	*	*	0	22	0	
9/3/2024	11:00	0	0	2	11	12	2	0	0	0	0	0	0	0	0	0	0	0	0	27	29.7	32.3	23 to 33	92.6	*	*	0	23	4	
9/3/2024	12:00	0	0	0	13	11	2	0	0	0	0	0	0	0	0	0	0	0	0	26	29.9	32	25 to 35	96.2	*	*	0	24	2	
9/3/2024	13:00	0	0	2	6	6	0	1	0	0	0	0	0	0	0	0	0	0	0	15	29.5	33	24 to 34	86.7	*	*	0	13	2	
9/3/2024	14:00	0	1	2	11	7	4	0	0	0	0	0	0	0	0	0	0	0	0	25	29.3	34	25 to 35	80.0	*	*	0	23	2	
9/3/2024	15:00	0	0	3	12	13	1	0	0	0	0	0	0	0	0	0	0	0	0	29	29.1	32.2	23 to 33	93.1	*	*	0	27	2	
9/3/2024	16:00	0	1	3	7	10	1	0	0	0	0	0	0	0	0	0	0	0	0	22	28.8	33	24 to 34	81.8	*	*	0	17	5	
9/3/2024	17:00	0	0	1	10	8	3	0	0	0	0	0	0	0	0	0	0	0	0	22	29.5	34	25 to 35	86.4	*	*	0	21	1	
9/3/2024	18:00	0	0	2	12	11	4	0	0	0	0	0	0	0	0	0	0	0	0	29	29.6	34	25 to 35	82.8	*	*	0	29	0	
9/3/2024	19:00	0	0	0	8	11	2	1	0	0	0	0	0	0	0	0	0	0	0	22	30.9	34	25 to 35	90.9	*	*	0	21	1	
9/3/2024	20:00	0	0	2	9	5	2	0	0	0	0	0	0	0	0	0	0	0	0	18	29.6	32.5	23 to 33	83.3	*	*	0	16	2	
9/3/2024	21:00	0	0	0	2	6	0	0	0	0	0	0	0	0	0	0	0	0	0	8	31	33	24 to 34	100.0	*	*	0	8	0	
9/3/2024	22:00	0	0	1	1	3	1	0	0	0	0	0	0	0	0	0	0	0	0	6	30.2	34	24 to 34	83.3	*	*	0	6	0	
9/3/2024	23:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28	30	20 to 30	100.0	*	*	0	2	0	
24 Hr Summary		0	2	23	142	165	37	5	0	0	0	0	0	0	0	0	0	0	0	374	30	33	25 to 35	85.0	*	*	0	348	26	

Incoming Histogram

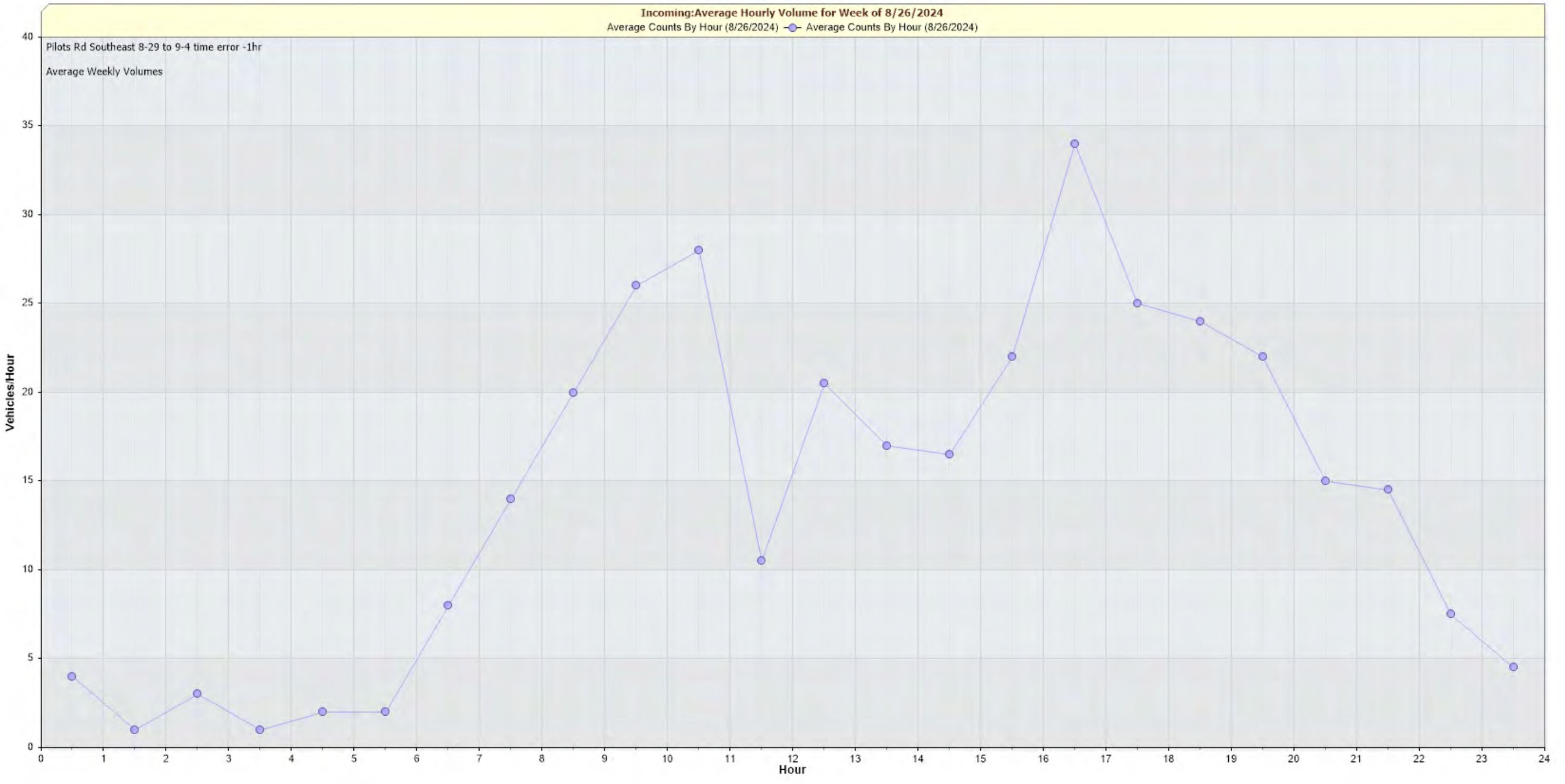
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-11-00-AM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	01:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	37.5	43	22 to 32	50.0	*	*	0	2	0
9/4/2024	02:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25	30	20 to 30	100.0	*	*	0	2	0
9/4/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	04:00	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	30	33	23 to 33	100.0	*	*	0	3	0
9/4/2024	05:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28.5	31	21 to 31	100.0	*	*	0	2	0
9/4/2024	06:00	0	0	1	5	6	1	1	0	0	0	0	0	0	0	0	0	0	0	0	14	31.3	34	27 to 37	85.7	*	*	0	14	0
9/4/2024	07:00	0	0	0	4	3	4	0	0	0	0	0	0	0	0	0	0	0	0	0	11	31.5	35	27 to 37	90.9	*	*	0	11	0
9/4/2024	08:00	0	0	2	22	11	5	1	0	0	0	0	0	0	0	0	0	0	0	0	41	29.9	34	25 to 35	87.8	*	*	0	39	2
9/4/2024	09:00	0	0	0	7	12	5	0	0	0	0	0	0	0	0	0	0	0	0	0	24	31	34.5	25 to 35	87.5	*	*	0	23	1
9/4/2024	10:00	0	0	1	6	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	9	28.8	34	24 to 34	88.9	*	*	0	8	1
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	5	47	37	16	3	0	0	0	0	0	0	0	0	0	0	0	0	108	30.4	35	25 to 35	84.3	*	*	0	104	4

Incoming: Average Hourly Volume for Week of 8/26/2024
Average Counts By Hour (8/26/2024)

Pilots Rd Southeast 8-29 to 9-4 time error -1hr
Average Weekly Volumes



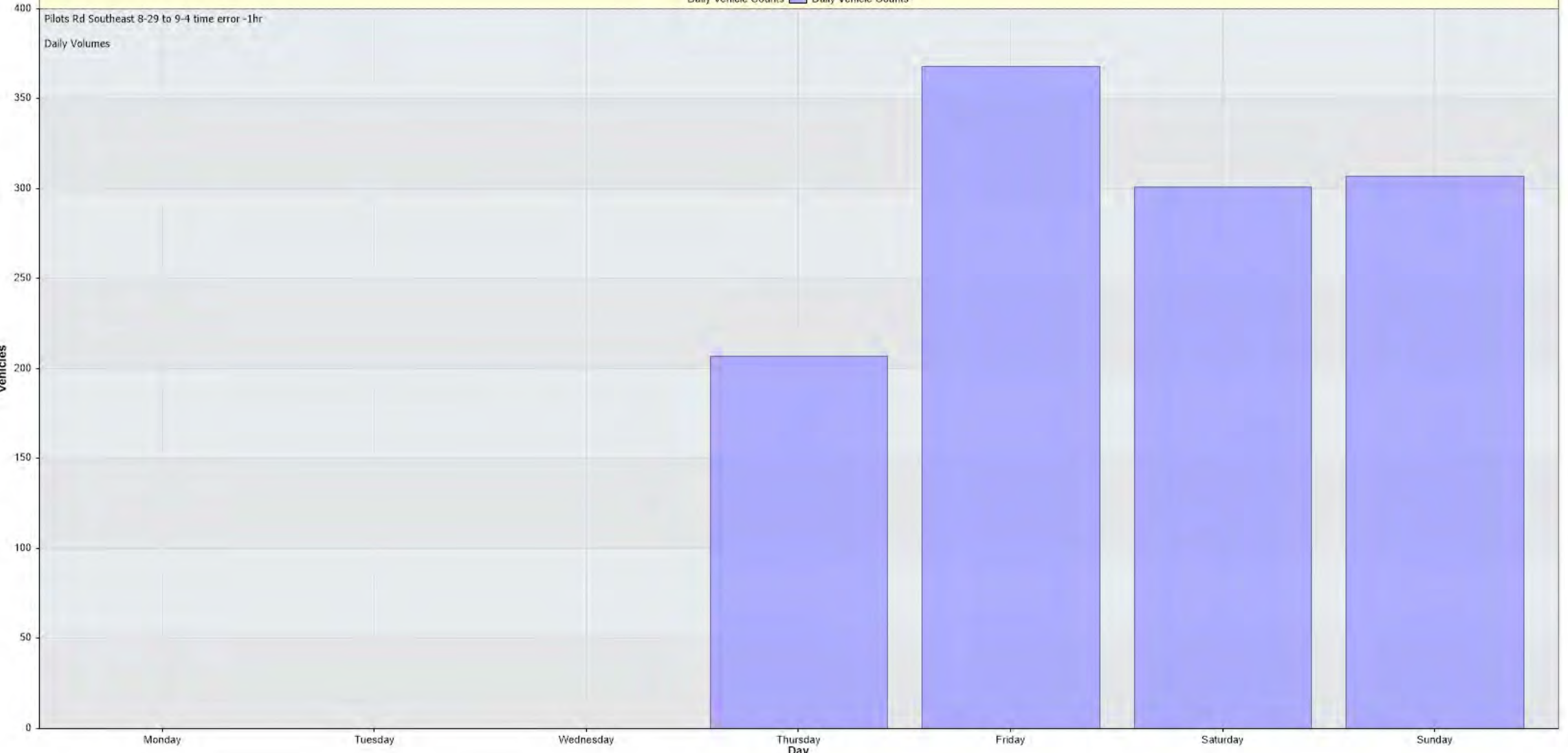
Incoming:Average Hourly WEEKDAY Speeds for Week of 8/26/2024
Average Hourly WEEKDAY Speeds By Hour (8/26/2024) — Average Hourly WEEKDAY Speeds By Hour (8/26/2024)

Pilots Rd Southeast 8-29 to 9-4 time error -1hr
Average Weekly Speeds



Incoming:Daily Volume for Week of 8/26/2024

Daily Vehicle Counts



Pilots Rd Southeast 8-29 to 9-4 time error -1hr

Daily Volumes

Vehicles

Day

For Project: Pilots Rd Southeast 8-29 to 9-4 time error -1hr
 Project Notes:
 Location/Name: Outgoing
 Report Generated: 10/3/2024 3:30:59 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 8/29/2024 12:00:00 PM through 9/4/2024 10:59:59 AM
 85th Percentile Speed: 33 MPH
 85th Percentile Vehicles: 1935
 Max Speed: 51 MPH on 8/30/2024 10:04:02 PM
 Total Vehicles: 2276
 AADT: 381

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	324	325
AM Peak 11:00 AM	15	16
PM Peak 6:00 PM	41	36

Speed

Speed Limit: 45
 85th Percentile Speed: 33
 50th Percentile Speed: 29
 10 MPH Pace Interval: 23.0 MPH to 33.0 MPH
 Average Speed: 28.73

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	1	0	3	3	0	0
% over limit	0.0	0.2	0.0	0.8	0.7	0.0	0.0
Avg Speeder	0.0	50.0	0.0	47.7	47.7	0.0	0.0
Avg Speed	28.5	29.0	29.1	28.7	29.0	28.3	28.6

Class Counts

	Number	%
VEH_SM	1	0
VEH_MED	2176	95.6
VEH_LG	99	4.3
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/29/2024 1:00:00 PM	30.0	15	18	36	0.0	0.0%	27.9
8/29/2024 2:00:00 PM	31.0	20	23	33	0.0	0.0%	28.0
8/29/2024 3:00:00 PM	30.0	20	24	32	0.0	0.0%	27.1
8/29/2024 4:00:00 PM	30.0	23	27	34	0.0	0.0%	26.7
8/29/2024 5:00:00 PM	33.0	34	40	38	0.0	0.0%	28.6
8/29/2024 6:00:00 PM	33.0	37	43	35	0.0	0.0%	28.6
8/29/2024 7:00:00 PM	33.0	37	43	37	0.0	0.0%	29.2
8/29/2024 8:00:00 PM	34.0	41	48	44	0.0	0.0%	29.1
8/29/2024 9:00:00 PM	39.0	25	29	48	48.0	3.4%	32.4
8/29/2024 10:00:00 PM	33.0	26	30	47	47.0	3.3%	28.8
8/29/2024 11:00:00 PM	32.0	19	22	48	48.0	4.5%	29.0
8/30/2024 12:00:00 AM	30.0	8	10	33	0.0	0.0%	26.7
8/30/2024 1:00:00 AM	35.0	5	6	37	0.0	0.0%	29.8
8/30/2024 2:00:00 AM	26.0	2	2	26	0.0	0.0%	24.5
8/30/2024 3:00:00 AM	23.0	1	1	23	0.0	0.0%	23.0
8/30/2024 4:00:00 AM	28.0	2	2	28	0.0	0.0%	20.5
8/30/2024 5:00:00 AM	**No Data**						
8/30/2024 6:00:00 AM	30.0	2	2	30	0.0	0.0%	26.5
8/30/2024 7:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
8/30/2024 8:00:00 AM	34.0	3	4	39	0.0	0.0%	32.0
8/30/2024 9:00:00 AM	33.0	14	16	35	0.0	0.0%	28.9
8/30/2024 10:00:00 AM	30.0	8	9	35	0.0	0.0%	28.7
8/30/2024 11:00:00 AM	31.0	18	21	35	0.0	0.0%	27.9
8/30/2024 12:00:00 PM	31.0	11	13	36	0.0	0.0%	27.9
8/30/2024 1:00:00 PM	35.0	20	23	37	0.0	0.0%	28.6
8/30/2024 2:00:00 PM	30.0	20	24	35	0.0	0.0%	27.4
8/30/2024 3:00:00 PM	33.0	28	33	41	0.0	0.0%	29.5
8/30/2024 4:00:00 PM	32.0	29	34	40	0.0	0.0%	29.2
8/30/2024 5:00:00 PM	34.0	37	43	45	0.0	0.0%	29.1
8/30/2024 6:00:00 PM	33.0	42	49	38	0.0	0.0%	28.8
8/30/2024 7:00:00 PM	33.0	38	45	46	46.0	2.2%	29.9
8/30/2024 8:00:00 PM	34.0	26	30	36	0.0	0.0%	28.8
8/30/2024 9:00:00 PM	35.0	33	39	46	46.0	2.6%	31.0
8/30/2024 10:00:00 PM	32.0	23	27	35	0.0	0.0%	28.6
8/30/2024 11:00:00 PM	33.0	15	18	51	51.0	5.6%	28.6
8/31/2024 12:00:00 AM	29.0	11	13	34	0.0	0.0%	26.9
8/31/2024 1:00:00 AM	27.0	3	3	27	0.0	0.0%	24.3
8/31/2024 2:00:00 AM	28.0	2	2	28	0.0	0.0%	25.5
8/31/2024 3:00:00 AM	22.0	1	1	22	0.0	0.0%	22.0
8/31/2024 4:00:00 AM	28.0	3	4	32	0.0	0.0%	26.0
8/31/2024 5:00:00 AM	**No Data**						
8/31/2024 6:00:00 AM	**No Data**						
8/31/2024 9:00:00 AM	3	4	35	0.0	0.0%	28.3
8/31/2024 10:00:00 AM	32.0	13	15	33	0.0	0.0%	28.2
8/31/2024 11:00:00 AM	31.0	8	9	35	0.0	0.0%	28.4
8/31/2024 12:00:00 PM	30.0	14	17	33	0.0	0.0%	26.9

Outgoing Summary
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

8/31/2024 1:00:00 PM	31.0	13	15	33	0.0	0.0%	27.9
8/31/2024 2:00:00 PM	32.0	20	23	36	0.0	0.0%	28.7
8/31/2024 3:00:00 PM	32.0	21	25	40	0.0	0.0%	28.7
8/31/2024 4:00:00 PM	31.0	27	32	34	0.0	0.0%	27.6
8/31/2024 5:00:00 PM	33.0	18	21	38	0.0	0.0%	28.7
8/31/2024 6:00:00 PM	32.0	20	24	36	0.0	0.0%	28.9
8/31/2024 7:00:00 PM	32.0	20	23	35	0.0	0.0%	27.9
8/31/2024 8:00:00 PM	35.0	25	29	39	0.0	0.0%	29.5
8/31/2024 9:00:00 PM	33.0	25	29	36	0.0	0.0%	29.3
8/31/2024 10:00:00 PM	32.0	20	23	34	0.0	0.0%	28.8
8/31/2024 11:00:00 PM	31.0	14	17	32	0.0	0.0%	27.2
9/1/2024 12:00:00 AM	32.0	14	16	36	0.0	0.0%	27.6
9/1/2024 1:00:00 AM	29.0	3	3	29	0.0	0.0%	27.0
9/1/2024 2:00:00 AM	33.0	2	2	33	0.0	0.0%	28.5
9/1/2024 3:00:00 AM	30.0	3	3	30	0.0	0.0%	26.7
9/1/2024 4:00:00 AM	27.0	1	1	27	0.0	0.0%	27.0
9/1/2024 5:00:00 AM	28.0	3	3	28	0.0	0.0%	26.7
9/1/2024 6:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
9/1/2024 7:00:00 AM	32.0	2	2	32	0.0	0.0%	31.5
9/1/2024 8:00:00 AM	36.0	1	1	36	0.0	0.0%	36.0
9/1/2024 9:00:00 AM	27.0	3	4	28	0.0	0.0%	26.3
9/1/2024 10:00:00 AM	33.0	8	9	37	0.0	0.0%	28.3
9/1/2024 11:00:00 AM	29.0	6	7	40	0.0	0.0%	28.1
9/1/2024 12:00:00 PM	30.0	15	18	30	0.0	0.0%	26.7
9/1/2024 1:00:00 PM	34.0	17	20	38	0.0	0.0%	30.6
9/1/2024 2:00:00 PM	33.0	18	21	39	0.0	0.0%	29.7
9/1/2024 3:00:00 PM	32.0	14	17	34	0.0	0.0%	29.1
9/1/2024 4:00:00 PM	33.0	21	25	40	0.0	0.0%	29.8
9/1/2024 5:00:00 PM	30.0	16	19	35	0.0	0.0%	27.1
9/1/2024 6:00:00 PM	31.0	24	28	37	0.0	0.0%	28.3
9/1/2024 7:00:00 PM	30.0	23	27	34	0.0	0.0%	27.8
9/1/2024 8:00:00 PM	34.0	21	25	39	0.0	0.0%	29.6
9/1/2024 9:00:00 PM	32.0	18	21	38	0.0	0.0%	29.0
9/1/2024 10:00:00 PM	32.0	31	36	35	0.0	0.0%	29.0
9/1/2024 11:00:00 PM	33.0	11	13	35	0.0	0.0%	28.5
9/2/2024 12:00:00 AM	30.0	14	17	34	0.0	0.0%	27.6
9/2/2024 1:00:00 AM	32.0	5	6	40	0.0	0.0%	30.5
9/2/2024 2:00:00 AM	32.0	3	4	37	0.0	0.0%	29.0
9/2/2024 3:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
9/2/2024 4:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
9/2/2024 5:00:00 AM	31.0	1	1	31	0.0	0.0%	31.0
9/2/2024 6:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
9/2/2024 7:00:00 AM	**No Data**						
9/2/2024 8:00:00 AM	24.0	1	1	24	0.0	0.0%	24.0
9/2/2024 9:00:00 AM	29.0	2	2	29	0.0	0.0%	27.0
9/2/2024 10:00:00 AM	32.0	3	3	32	0.0	0.0%	30.0
9/2/2024 11:00:00 AM	34.0	5	6	34	0.0	0.0%	30.5
9/2/2024 12:00:00 PM	30.0	13	15	32	0.0	0.0%	26.4

Outgoing Summary
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

9/2/2024 1:00:00 PM	32.0	17	20	38	0.0	0.0%	28.1
9/2/2024 2:00:00 PM	32.0	21	25	35	0.0	0.0%	28.0
9/2/2024 3:00:00 PM	31.0	23	27	33	0.0	0.0%	26.4
9/2/2024 4:00:00 PM	32.0	27	32	35	0.0	0.0%	27.7
9/2/2024 5:00:00 PM	32.0	26	30	42	0.0	0.0%	28.4
9/2/2024 6:00:00 PM	33.0	23	27	40	0.0	0.0%	29.1
9/2/2024 7:00:00 PM	31.0	22	26	38	0.0	0.0%	28.7
9/2/2024 8:00:00 PM	32.0	25	29	41	0.0	0.0%	28.6
9/2/2024 9:00:00 PM	33.0	20	24	35	0.0	0.0%	30.3
9/2/2024 10:00:00 PM	35.0	14	16	37	0.0	0.0%	30.9
9/2/2024 11:00:00 PM	32.0	16	19	36	0.0	0.0%	28.2
9/3/2024 12:00:00 AM	36.0	10	12	42	0.0	0.0%	30.6
9/3/2024 1:00:00 AM	32.0	4	5	34	0.0	0.0%	29.0
9/3/2024 2:00:00 AM	30.0	3	4	31	0.0	0.0%	27.8
9/3/2024 3:00:00 AM	36.0	3	3	36	0.0	0.0%	27.3
9/3/2024 4:00:00 AM	27.0	1	1	27	0.0	0.0%	27.0
9/3/2024 5:00:00 AM	31.0	2	2	31	0.0	0.0%	27.5
9/3/2024 6:00:00 AM	**No Data**						
9/3/2024 7:00:00 AM	32.0	1	1	32	0.0	0.0%	32.0
9/3/2024 8:00:00 AM	33.0	3	3	33	0.0	0.0%	31.7
9/3/2024 9:00:00 AM	34.0	13	15	39	0.0	0.0%	29.9
9/3/2024 10:00:00 AM	33.0	10	12	43	0.0	0.0%	31.2
9/3/2024 11:00:00 AM	32.0	15	18	36	0.0	0.0%	29.2
9/3/2024 12:00:00 PM	31.0	16	19	33	0.0	0.0%	28.3
9/3/2024 1:00:00 PM	31.0	16	19	33	0.0	0.0%	29.4
9/3/2024 2:00:00 PM	32.0	21	25	43	0.0	0.0%	30.1
9/3/2024 3:00:00 PM	32.0	24	28	35	0.0	0.0%	28.3
9/3/2024 4:00:00 PM	32.0	20	24	38	0.0	0.0%	28.3
9/3/2024 5:00:00 PM	32.0	38	45	35	0.0	0.0%	27.7
9/3/2024 6:00:00 PM	31.0	31	37	41	0.0	0.0%	28.5
9/3/2024 7:00:00 PM	33.0	44	52	38	0.0	0.0%	29.8
9/3/2024 8:00:00 PM	32.0	35	41	36	0.0	0.0%	28.8
9/3/2024 9:00:00 PM	32.0	25	29	50	50.0	3.4%	29.3
9/3/2024 10:00:00 PM	34.0	18	21	37	0.0	0.0%	29.5
9/3/2024 11:00:00 PM	32.0	15	18	35	0.0	0.0%	30.0
9/4/2024 12:00:00 AM	30.0	9	11	32	0.0	0.0%	27.5
9/4/2024 1:00:00 AM	36.0	6	7	38	0.0	0.0%	30.3
9/4/2024 2:00:00 AM	29.0	3	4	34	0.0	0.0%	28.8
9/4/2024 3:00:00 AM	**No Data**						
9/4/2024 4:00:00 AM	32.0	2	2	32	0.0	0.0%	30.0
9/4/2024 5:00:00 AM	**No Data**						
9/4/2024 6:00:00 AM	**No Data**						
9/4/2024 7:00:00 AM	29.0	2	2	29	0.0	0.0%	24.0
9/4/2024 8:00:00 AM	35.0	3	4	35	0.0	0.0%	32.8
9/4/2024 9:00:00 AM	33.0	11	13	35	0.0	0.0%	28.5
9/4/2024 10:00:00 AM	33.0	10	12	34	0.0	0.0%	29.8
9/4/2024 11:00:00 AM	27.0	3	4	30	0.0	0.0%	26.0

Outgoing Summary
Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/30/2024 12:00:00 AM	33.0	303	357	48	47.7	0.8%	28.7
8/31/2024 12:00:00 AM	33.0	387	455	51	47.7	0.7%	29.0
9/1/2024 12:00:00 AM	32.0	282	332	40	0.0	0.0%	28.3
9/2/2024 12:00:00 AM	32.0	275	323	40	0.0	0.0%	28.6
9/3/2024 12:00:00 AM	32.0	279	328	42	0.0	0.0%	28.5
9/4/2024 12:00:00 AM	33.0	368	433	50	50.0	0.2%	29.0
9/4/2024 10:59:59 AM	34.0	41	48	38	0.0	0.0%	29.1

Outgoing Weekly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	6	3	3	6	3	35
1 - 2	*	*	*	*	2	2	2	2	2	26
2 - 3	*	*	*	*	1	1	3	1	2	23
3 - 4	*	*	*	*	2	4	1	2	2.5	28
4 - 5	*	*	*	*	0	0	3	0	1.5	0
5 - 6	*	*	*	*	2	0	1	2	0.5	30
6 - 7	*	*	*	*	1	0	2	1	1	28
7 - 8	*	*	*	*	4	0	1	4	0.5	34
8 - 9	*	*	*	*	16	4	4	16	4	32.7
9 - 10	*	*	*	*	9	15	9	9	12	32
10 - 11	*	*	*	*	21	9	7	21	8	31
11 - 12	*	*	*	*	13	17	18	13	17.5	30.8
12 - 13	*	*	*	18	23	15	20	20.5	17.5	32.15
13 - 14	*	*	*	23	24	23	21	23.5	22	30.35
14 - 15	*	*	*	24	33	25	17	28.5	21	31.4
15 - 16	*	*	*	27	34	32	25	30.5	28.5	30.85
16 - 17	*	*	*	40	43	21	19	41.5	20	33
17 - 18	*	*	*	43	49	24	28	46	26	32.55
18 - 19	*	*	*	43	45	23	27	44	25	32.9
19 - 20	*	*	*	48	30	29	25	39	27	33.4
20 - 21	*	*	*	29	39	29	21	34	25	36.6
21 - 22	*	*	*	30	27	23	36	28.5	29.5	32.25
22 - 23	*	*	*	22	18	17	13	20	15	32.5
23 - 24	*	*	*	10	13	16	17	11.5	16.5	29.25
Totals	0	0	0	357	455	332	323			
% of Total	0%	0%	0%	24.34%	31.02%	22.63%	22.02%			

Outgoing Weekly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	6	5	7	*	*	*	*	6	0	33.33
1 - 2	4	4	4	*	*	*	*	4	0	30.33
2 - 3	1	3	0	*	*	*	*	1.33	0	31
3 - 4	1	1	2	*	*	*	*	1.33	0	28
4 - 5	1	2	0	*	*	*	*	1	0	31
5 - 6	1	0	0	*	*	*	*	0.33	0	28
6 - 7	0	1	2	*	*	*	*	1	0	30.5
7 - 8	1	3	4	*	*	*	*	2.67	0	30.5
8 - 9	2	15	13	*	*	*	*	10	0	31.83
9 - 10	3	12	12	*	*	*	*	9	0	32.67
10 - 11	6	18	4	*	*	*	*	9.33	0	30.77
11 - 12	15	19	*	*	*	*	*	17	0	30.5
12 - 13	20	19	*	*	*	*	*	19.5	0	31.5
13 - 14	25	25	*	*	*	*	*	25	0	32
14 - 15	27	28	*	*	*	*	*	27.5	0	31.5
15 - 16	32	24	*	*	*	*	*	28	0	31.55
16 - 17	30	45	*	*	*	*	*	37.5	0	32
17 - 18	27	37	*	*	*	*	*	32	0	32
18 - 19	26	52	*	*	*	*	*	39	0	31.75
19 - 20	29	41	*	*	*	*	*	35	0	31.85
20 - 21	24	29	*	*	*	*	*	26.5	0	32.25
21 - 22	16	21	*	*	*	*	*	18.5	0	34.25
22 - 23	19	18	*	*	*	*	*	18.5	0	31.85
23 - 24	12	11	*	*	*	*	*	11.5	0	33
Totals	328	433	48	0	0	0	0			
% of Total	40.54%	53.52%	5.93%	0%	0%	0%	0%			

Outgoing Monthly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	6	3	*	6	3	35
1 - 2	*	*	*	*	2	2	*	2	2	26
2 - 3	*	*	*	*	1	1	*	1	1	23
3 - 4	*	*	*	*	2	4	*	2	4	28
4 - 5	*	*	*	*	0	0	*	0	0	0
5 - 6	*	*	*	*	2	0	*	2	0	30
6 - 7	*	*	*	*	1	0	*	1	0	28
7 - 8	*	*	*	*	4	0	*	4	0	34
8 - 9	*	*	*	*	16	4	*	16	4	32.7
9 - 10	*	*	*	*	9	15	*	9	15	32
10 - 11	*	*	*	*	21	9	*	21	9	31
11 - 12	*	*	*	*	13	17	*	13	17	30.8
12 - 13	*	*	*	18	23	15	*	20.5	15	32.15
13 - 14	*	*	*	23	24	23	*	23.5	23	30.35
14 - 15	*	*	*	24	33	25	*	28.5	25	31.4
15 - 16	*	*	*	27	34	32	*	30.5	32	30.85
16 - 17	*	*	*	40	43	21	*	41.5	21	33
17 - 18	*	*	*	43	49	24	*	46	24	32.55
18 - 19	*	*	*	43	45	23	*	44	23	32.9
19 - 20	*	*	*	48	30	29	*	39	29	33.4
20 - 21	*	*	*	29	39	29	*	34	29	36.6
21 - 22	*	*	*	30	27	23	*	28.5	23	32.25
22 - 23	*	*	*	22	18	17	*	20	17	32.5
23 - 24	*	*	*	10	13	16	*	11.5	16	29.25
Totals	0	0	0	357	455	332	0			
% of Total	0%	0%	0%	31.21%	39.77%	29.02%	0%			

Outgoing Monthly Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	6	5	7	*	*	*	3	6	3	33.33
1 - 2	4	4	4	*	*	*	2	4	2	30.33
2 - 3	1	3	0	*	*	*	3	1.33	3	31
3 - 4	1	1	2	*	*	*	1	1.33	1	28
4 - 5	1	2	0	*	*	*	3	1	3	31
5 - 6	1	0	0	*	*	*	1	0.33	1	28
6 - 7	0	1	2	*	*	*	2	1	2	30.5
7 - 8	1	3	4	*	*	*	1	2.67	1	30.5
8 - 9	2	15	13	*	*	*	4	10	4	31.83
9 - 10	3	12	12	*	*	*	9	9	9	32.67
10 - 11	6	18	4	*	*	*	7	9.33	7	30.77
11 - 12	15	19	*	*	*	*	18	17	18	30.5
12 - 13	20	19	*	*	*	*	20	19.5	20	31.5
13 - 14	25	25	*	*	*	*	21	25	21	32
14 - 15	27	28	*	*	*	*	17	27.5	17	31.5
15 - 16	32	24	*	*	*	*	25	28	25	31.55
16 - 17	30	45	*	*	*	*	19	37.5	19	32
17 - 18	27	37	*	*	*	*	28	32	28	32
18 - 19	26	52	*	*	*	*	27	39	27	31.75
19 - 20	29	41	*	*	*	*	25	35	25	31.85
20 - 21	24	29	*	*	*	*	21	26.5	21	32.25
21 - 22	16	21	*	*	*	*	36	18.5	36	34.25
22 - 23	19	18	*	*	*	*	13	18.5	13	31.85
23 - 24	12	11	*	*	*	*	17	11.5	17	33
Totals	328	433	48	0	0	0	323			
% of Total	28.98%	38.25%	4.24%	0%	0%	0%	28.53%			

Outgoing Weekly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	29.83	24.33	27	29.83	25.67	35
1 - 2	*	*	*	*	24.5	25.5	28.5	24.5	27	26
2 - 3	*	*	*	*	23	22	26.67	23	25.5	23
3 - 4	*	*	*	*	20.5	26	27	20.5	26.2	28
4 - 5	*	*	*	*	0	0	26.67	0	26.67	0
5 - 6	*	*	*	*	26.5	0	25	26.5	25	30
6 - 7	*	*	*	*	28	0	31.5	28	31.5	28
7 - 8	*	*	*	*	32	0	36	32	36	34
8 - 9	*	*	*	*	28.94	28.25	26.25	28.94	27.25	32.7
9 - 10	*	*	*	*	28.67	28.2	28.33	28.67	28.25	32
10 - 11	*	*	*	*	27.9	28.44	28.14	27.9	28.31	31
11 - 12	*	*	*	*	27.92	26.94	26.72	27.92	26.83	30.8
12 - 13	*	*	*	27.89	28.57	27.93	30.65	28.27	29.49	32.15
13 - 14	*	*	*	28.04	27.38	28.7	29.71	27.7	29.18	30.35
14 - 15	*	*	*	27.12	29.55	28.72	29.06	28.53	28.86	31.4
15 - 16	*	*	*	26.67	29.21	27.56	29.8	28.08	28.54	30.85
16 - 17	*	*	*	28.62	29.12	28.67	27.05	28.88	27.9	33
17 - 18	*	*	*	28.6	28.82	28.92	28.32	28.72	28.6	32.55
18 - 19	*	*	*	29.19	29.89	27.91	27.81	29.55	27.86	32.9
19 - 20	*	*	*	29.15	28.83	29.48	29.6	29.03	29.54	33.4
20 - 21	*	*	*	32.45	31.03	29.34	29	31.63	29.2	36.6
21 - 22	*	*	*	28.77	28.56	28.83	29	28.67	28.93	32.25
22 - 23	*	*	*	28.95	28.61	27.18	28.46	28.8	27.73	32.5
23 - 24	*	*	*	26.7	26.92	27.62	27.59	26.83	27.61	29.25
Totals	0	0	0	28.7	28.9	28.3	28.6			
% of Total	0%	0%	0%	25.07%	25.24%	24.72%	24.98%			

Outgoing Weekly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	30.5	29	30.29	*	*	*	*	30	*	33.33
1 - 2	29	27.75	28.75	*	*	*	*	28.5	*	30.33
2 - 3	26	27.33	0	*	*	*	*	27	*	31
3 - 4	25	27	30	*	*	*	*	28	*	28
4 - 5	31	27.5	0	*	*	*	*	28.67	*	31
5 - 6	28	0	0	*	*	*	*	28	*	28
6 - 7	0	32	24	*	*	*	*	26.67	*	30.5
7 - 8	24	31.67	32.75	*	*	*	*	31.25	*	30.5
8 - 9	27	29.87	28.46	*	*	*	*	29.07	*	31.83
9 - 10	30	31.17	29.83	*	*	*	*	30.44	*	32.67
10 - 11	30.5	29.22	26	*	*	*	*	29.04	*	30.77
11 - 12	26.4	28.32	*	*	*	*	*	27.47	*	30.5
12 - 13	28.15	29.42	*	*	*	*	*	28.77	*	31.5
13 - 14	27.96	30.08	*	*	*	*	*	29.02	*	32
14 - 15	26.37	28.32	*	*	*	*	*	27.36	*	31.5
15 - 16	27.69	28.33	*	*	*	*	*	27.96	*	31.55
16 - 17	28.37	27.73	*	*	*	*	*	27.99	*	32
17 - 18	29.07	28.49	*	*	*	*	*	28.73	*	32
18 - 19	28.65	29.85	*	*	*	*	*	29.45	*	31.75
19 - 20	28.62	28.78	*	*	*	*	*	28.71	*	31.85
20 - 21	30.29	29.34	*	*	*	*	*	29.77	*	32.25
21 - 22	30.94	29.48	*	*	*	*	*	30.11	*	34.25
22 - 23	28.16	30	*	*	*	*	*	29.05	*	31.85
23 - 24	30.58	27.45	*	*	*	*	*	29.09	*	33
Totals	28.5	29	29.1	0	0	0	0			
% of Total	32.91%	33.49%	33.6%	0%	0%	0%	0%			

Outgoing Monthly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	29.83	24.33	*	29.83	24.33	35
1 - 2	*	*	*	*	24.5	25.5	*	24.5	25.5	26
2 - 3	*	*	*	*	23	22	*	23	22	23
3 - 4	*	*	*	*	20.5	26	*	20.5	26	28
4 - 5	*	*	*	*	0	0	*	*	*	0
5 - 6	*	*	*	*	26.5	0	*	26.5	*	30
6 - 7	*	*	*	*	28	0	*	28	*	28
7 - 8	*	*	*	*	32	0	*	32	*	34
8 - 9	*	*	*	*	28.94	28.25	*	28.94	28.25	32.7
9 - 10	*	*	*	*	28.67	28.2	*	28.67	28.2	32
10 - 11	*	*	*	*	27.9	28.44	*	27.9	28.44	31
11 - 12	*	*	*	*	27.92	26.94	*	27.92	26.94	30.8
12 - 13	*	*	*	27.89	28.57	27.93	*	28.27	27.93	32.15
13 - 14	*	*	*	28.04	27.38	28.7	*	27.7	28.7	30.35
14 - 15	*	*	*	27.12	29.55	28.72	*	28.53	28.72	31.4
15 - 16	*	*	*	26.67	29.21	27.56	*	28.08	27.56	30.85
16 - 17	*	*	*	28.62	29.12	28.67	*	28.88	28.67	33
17 - 18	*	*	*	28.6	28.82	28.92	*	28.72	28.92	32.55
18 - 19	*	*	*	29.19	29.89	27.91	*	29.55	27.91	32.9
19 - 20	*	*	*	29.15	28.83	29.48	*	29.03	29.48	33.4
20 - 21	*	*	*	32.45	31.03	29.34	*	31.63	29.34	36.6
21 - 22	*	*	*	28.77	28.56	28.83	*	28.67	28.83	32.25
22 - 23	*	*	*	28.95	28.61	27.18	*	28.8	27.18	32.5
23 - 24	*	*	*	26.7	26.92	27.62	*	26.83	27.62	29.25
Totals	0	0	0	28.7	28.9	28.3	0			
% of Total	0%	0%	0%	33.41%	33.64%	32.95%	0%			

Outgoing Monthly Speeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	30.5	29	30.29	*	*	*	27	30	27	33.33
1 - 2	29	27.75	28.75	*	*	*	28.5	28.5	28.5	30.33
2 - 3	26	27.33	0	*	*	*	26.67	27	26.67	31
3 - 4	25	27	30	*	*	*	27	28	27	28
4 - 5	31	27.5	0	*	*	*	26.67	28.67	26.67	31
5 - 6	28	0	0	*	*	*	25	28	25	28
6 - 7	0	32	24	*	*	*	31.5	26.67	31.5	30.5
7 - 8	24	31.67	32.75	*	*	*	36	31.25	36	30.5
8 - 9	27	29.87	28.46	*	*	*	26.25	29.07	26.25	31.83
9 - 10	30	31.17	29.83	*	*	*	28.33	30.44	28.33	32.67
10 - 11	30.5	29.22	26	*	*	*	28.14	29.04	28.14	30.77
11 - 12	26.4	28.32	*	*	*	*	26.72	27.47	26.72	30.5
12 - 13	28.15	29.42	*	*	*	*	30.65	28.77	30.65	31.5
13 - 14	27.96	30.08	*	*	*	*	29.71	29.02	29.71	32
14 - 15	26.37	28.32	*	*	*	*	29.06	27.36	29.06	31.5
15 - 16	27.69	28.33	*	*	*	*	29.8	27.96	29.8	31.55
16 - 17	28.37	27.73	*	*	*	*	27.05	27.99	27.05	32
17 - 18	29.07	28.49	*	*	*	*	28.32	28.73	28.32	32
18 - 19	28.65	29.85	*	*	*	*	27.81	29.45	27.81	31.75
19 - 20	28.62	28.78	*	*	*	*	29.6	28.71	29.6	31.85
20 - 21	30.29	29.34	*	*	*	*	29	29.77	29	32.25
21 - 22	30.94	29.48	*	*	*	*	29	30.11	29	34.25
22 - 23	28.16	30	*	*	*	*	28.46	29.05	28.46	31.85
23 - 24	30.58	27.45	*	*	*	*	27.59	29.09	27.59	33
Totals	28.5	29	29.1	0	0	0	28.6			
% of Total	24.74%	25.17%	25.26%	0%	0%	0%	24.83%			

Outgoing Weekly EightyFifthSpeeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	35	27	29	35	28	35
1 - 2	*	*	*	*	26	28	33	26	30.5	26
2 - 3	*	*	*	*	23	22	30	23	26	23
3 - 4	*	*	*	*	28	28	27	28	27.5	28
4 - 5	*	*	*	*	0	0	28	0	28	0
5 - 6	*	*	*	*	30	0	25	30	25	30
6 - 7	*	*	*	*	28	0	32	28	32	28
7 - 8	*	*	*	*	34	0	36	34	36	34
8 - 9	*	*	*	*	32.7	33	27	32.7	30	32.7
9 - 10	*	*	*	*	32	31.5	33	32	32.25	32
10 - 11	*	*	*	*	31	31	29	31	30	31
11 - 12	*	*	*	*	30.8	30	29.2	30.8	29.6	30.8
12 - 13	*	*	*	30	34.3	31	34	32.15	32.5	32.15
13 - 14	*	*	*	30.7	30	32	33	30.35	32.5	30.35
14 - 15	*	*	*	29.8	33	31.7	32	31.4	31.85	31.4
15 - 16	*	*	*	29.7	32	30.7	32.5	30.85	31.6	30.85
16 - 17	*	*	*	32.5	33.5	33	29.7	33	31.35	33
17 - 18	*	*	*	32.3	32.8	32	31	32.55	31.5	32.55
18 - 19	*	*	*	32.8	33	31.7	30	32.9	30.85	32.9
19 - 20	*	*	*	33.5	33.3	35	34	33.4	34.5	33.4
20 - 21	*	*	*	38.7	34.5	32.5	32	36.6	32.25	36.6
21 - 22	*	*	*	33	31.5	31.6	32	32.25	31.8	32.25
22 - 23	*	*	*	32	33	30.7	33	32.5	31.85	32.5
23 - 24	*	*	*	29.8	28.7	32	30	29.25	31	29.25
Totals	0	0	0	384.8	720.1	614.4	741.4			
% of Total	0%	0%	0%	15.64%	29.26%	24.97%	30.13%			

Outgoing Weekly EightyFifthSpeeds

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	32	32	36	*	*	*	*	33.33	0	33.33
1 - 2	32	30	29	*	*	*	*	30.33	0	30.33
2 - 3	26	36	0	*	*	*	*	31	0	31
3 - 4	25	27	32	*	*	*	*	28	0	28
4 - 5	31	31	0	*	*	*	*	31	0	31
5 - 6	28	0	0	*	*	*	*	28	0	28
6 - 7	0	32	29	*	*	*	*	30.5	0	30.5
7 - 8	24	33	34.5	*	*	*	*	30.5	0	30.5
8 - 9	29	33.5	33	*	*	*	*	31.83	0	31.83
9 - 10	32	33	33	*	*	*	*	32.67	0	32.67
10 - 11	33.5	31.8	27	*	*	*	*	30.77	0	30.77
11 - 12	30	31	*	*	*	*	*	30.5	0	30.5
12 - 13	32	31	*	*	*	*	*	31.5	0	31.5
13 - 14	32	32	*	*	*	*	*	32	0	32
14 - 15	31	32	*	*	*	*	*	31.5	0	31.5
15 - 16	31.5	31.6	*	*	*	*	*	31.55	0	31.55
16 - 17	32	32	*	*	*	*	*	32	0	32
17 - 18	33	31	*	*	*	*	*	32	0	32
18 - 19	30.8	32.7	*	*	*	*	*	31.75	0	31.75
19 - 20	31.7	32	*	*	*	*	*	31.85	0	31.85
20 - 21	33	31.5	*	*	*	*	*	32.25	0	32.25
21 - 22	35	33.5	*	*	*	*	*	34.25	0	34.25
22 - 23	31.7	32	*	*	*	*	*	31.85	0	31.85
23 - 24	36	30	*	*	*	*	*	33	0	33
Totals	712.2	731.6	253.5	0	0	0	0			
% of Total	41.96%	43.1%	14.94%	0%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	35	27	*	35	27	35
1 - 2	*	*	*	*	26	28	*	26	28	26
2 - 3	*	*	*	*	23	22	*	23	22	23
3 - 4	*	*	*	*	28	28	*	28	28	28
4 - 5	*	*	*	*	0	0	*	0	0	0
5 - 6	*	*	*	*	30	0	*	30	0	30
6 - 7	*	*	*	*	28	0	*	28	0	28
7 - 8	*	*	*	*	34	0	*	34	0	34
8 - 9	*	*	*	*	32.7	33	*	32.7	33	32.7
9 - 10	*	*	*	*	32	31.5	*	32	31.5	32
10 - 11	*	*	*	*	31	31	*	31	31	31
11 - 12	*	*	*	*	30.8	30	*	30.8	30	30.8
12 - 13	*	*	*	30	34.3	31	*	32.15	31	32.15
13 - 14	*	*	*	30.7	30	32	*	30.35	32	30.35
14 - 15	*	*	*	29.8	33	31.7	*	31.4	31.7	31.4
15 - 16	*	*	*	29.7	32	30.7	*	30.85	30.7	30.85
16 - 17	*	*	*	32.5	33.5	33	*	33	33	33
17 - 18	*	*	*	32.3	32.8	32	*	32.55	32	32.55
18 - 19	*	*	*	32.8	33	31.7	*	32.9	31.7	32.9
19 - 20	*	*	*	33.5	33.3	35	*	33.4	35	33.4
20 - 21	*	*	*	38.7	34.5	32.5	*	36.6	32.5	36.6
21 - 22	*	*	*	33	31.5	31.6	*	32.25	31.6	32.25
22 - 23	*	*	*	32	33	30.7	*	32.5	30.7	32.5
23 - 24	*	*	*	29.8	28.7	32	*	29.25	32	29.25

Outgoing Monthly EightyFifthSpeeds
 Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	32	32	36	*	*	*	29	33.33	29	33.33
1 - 2	32	30	29	*	*	*	33	30.33	33	30.33
2 - 3	26	36	0	*	*	*	30	31	30	31
3 - 4	25	27	32	*	*	*	27	28	27	28
4 - 5	31	31	0	*	*	*	28	31	28	31
5 - 6	28	0	0	*	*	*	25	28	25	28
6 - 7	0	32	29	*	*	*	32	30.5	32	30.5
7 - 8	24	33	34.5	*	*	*	36	30.5	36	30.5
8 - 9	29	33.5	33	*	*	*	27	31.83	27	31.83
9 - 10	32	33	33	*	*	*	33	32.67	33	32.67
10 - 11	33.5	31.8	27	*	*	*	29	30.77	29	30.77
11 - 12	30	31	*	*	*	*	29.2	30.5	29.2	30.5
12 - 13	32	31	*	*	*	*	34	31.5	34	31.5
13 - 14	32	32	*	*	*	*	33	32	33	32
14 - 15	31	32	*	*	*	*	32	31.5	32	31.5
15 - 16	31.5	31.6	*	*	*	*	32.5	31.55	32.5	31.55
16 - 17	32	32	*	*	*	*	29.7	32	29.7	32
17 - 18	33	31	*	*	*	*	31	32	31	32
18 - 19	30.8	32.7	*	*	*	*	30	31.75	30	31.75
19 - 20	31.7	32	*	*	*	*	34	31.85	34	31.85
20 - 21	33	31.5	*	*	*	*	32	32.25	32	32.25
21 - 22	35	33.5	*	*	*	*	32	34.25	32	34.25
22 - 23	31.7	32	*	*	*	*	33	31.85	33	31.85
23 - 24	36	30	*	*	*	*	30	33	30	33

Summary of Violators

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	30	29.1	0	0.0
01:00:00	18	27.7	0	0.0
02:00:00	9	25.9	0	0.0
03:00:00	11	25.8	0	0.0
04:00:00	6	27.7	0	0.0
05:00:00	4	26.5	0	0.0
06:00:00	6	28.5	0	0.0
07:00:00	13	31.8	0	0.0
08:00:00	54	28.8	0	0.0
09:00:00	60	29.3	0	0.0
10:00:00	65	28.5	0	0.0
11:00:00	82	27.3	0	0.0
12:00:00	115	28.8	0	0.0
13:00:00	141	28.6	0	0.0
14:00:00	154	28.2	0	0.0
15:00:00	174	28.2	0	0.0
16:00:00	198	28.3	0	0.0
17:00:00	208	28.7	0	0.0
18:00:00	216	29.1	1	46.0
19:00:00	202	29.1	0	0.0
20:00:00	171	30.3	3	48.0
21:00:00	153	29.1	1	47.0
22:00:00	107	28.6	2	49.5
23:00:00	79	27.8	0	0.0

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	0	0	2	11	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	18	27.9	30	21 to 31	88.9	*	*	1	17	0
8/29/2024	13:00	0	0	2	16	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	23	28	30.7	22 to 32	95.7	*	*	0	21	2
8/29/2024	14:00	0	0	5	12	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	24	27.1	29.8	22 to 32	95.8	*	*	0	23	1
8/29/2024	15:00	0	1	5	15	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	27	26.7	29.7	21 to 31	88.9	*	*	0	25	2
8/29/2024	16:00	0	1	5	17	15	2	0	0	0	0	0	0	0	0	0	0	0	0	0	40	28.6	32.5	23 to 33	82.5	*	*	0	36	4
8/29/2024	17:00	0	1	3	22	16	1	0	0	0	0	0	0	0	0	0	0	0	0	0	43	28.6	32.3	24 to 34	93.0	*	*	0	41	2
8/29/2024	18:00	0	1	3	19	15	5	0	0	0	0	0	0	0	0	0	0	0	0	0	43	29.2	32.8	25 to 35	83.7	*	*	0	42	1
8/29/2024	19:00	0	0	9	18	16	4	1	0	0	0	0	0	0	0	0	0	0	0	0	48	29.1	33.5	26 to 36	79.2	*	*	0	46	2
8/29/2024	20:00	0	0	0	12	8	6	2	1	0	0	0	0	0	0	0	0	0	0	0	29	32.4	38.7	26 to 36	79.3	1	3.4	0	29	0
8/29/2024	21:00	0	0	4	17	6	2	0	1	0	0	0	0	0	0	0	0	0	0	0	30	28.8	33	22 to 32	80.0	1	3.3	0	27	3
8/29/2024	22:00	0	1	3	8	8	1	0	1	0	0	0	0	0	0	0	0	0	0	0	22	29	32	24 to 34	77.3	1	4.5	0	21	1
8/29/2024	23:00	0	1	2	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	26.7	29.8	20 to 30	80.0	*	*	0	10	0
24 Hr Summary		0	6	43	169	110	23	3	3	0	0	0	0	0	0	0	0	0	0	357	28.7	33	23 to 33	81.2	3	0.8	1	338	18	

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/30/2024	00:00	0	0	1	2	1	2	0	0	0	0	0	0	0	0	0	0	0	0	6	29.8	35	22 to 32	66.7	*	*	0	6	0	
8/30/2024	01:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24.5	26	16 to 26	100.0	*	*	0	2	0	
8/30/2024	02:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	23	23	13 to 23	100.0	*	*	0	1	0	
8/30/2024	03:00	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	20.5	28	3 to 13	50.0	*	*	0	2	0	
8/30/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/30/2024	05:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	26.5	30	20 to 30	100.0	*	*	0	2	0	
8/30/2024	06:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0	
8/30/2024	07:00	0	0	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	4	32	34	24 to 34	75.0	*	*	0	3	1	
8/30/2024	08:00	0	0	4	5	6	1	0	0	0	0	0	0	0	0	0	0	0	0	16	28.9	32.7	23 to 33	93.8	*	*	0	14	2	
8/30/2024	09:00	0	0	1	5	2	1	0	0	0	0	0	0	0	0	0	0	0	0	9	28.7	32	22 to 32	88.9	*	*	0	9	0	
8/30/2024	10:00	0	0	4	11	5	1	0	0	0	0	0	0	0	0	0	0	0	0	21	27.9	31	22 to 32	95.2	*	*	0	20	1	
8/30/2024	11:00	0	0	3	5	4	1	0	0	0	0	0	0	0	0	0	0	0	0	13	27.9	30.8	21 to 31	92.3	*	*	0	12	1	
8/30/2024	12:00	0	1	3	10	5	4	0	0	0	0	0	0	0	0	0	0	0	0	23	28.6	34.3	21 to 31	78.3	*	*	0	22	1	
8/30/2024	13:00	0	1	5	10	7	1	0	0	0	0	0	0	0	0	0	0	0	0	24	27.4	30	20 to 30	79.2	*	*	0	22	2	
8/30/2024	14:00	0	0	1	17	13	1	1	0	0	0	0	0	0	0	0	0	0	0	33	29.5	33	24 to 34	90.9	*	*	0	31	2	
8/30/2024	15:00	0	0	6	11	15	1	1	0	0	0	0	0	0	0	0	0	0	0	34	29.2	32	23 to 33	91.2	*	*	0	32	2	
8/30/2024	16:00	0	1	5	22	10	4	0	1	0	0	0	0	0	0	0	0	0	0	43	29.1	33.5	25 to 35	79.1	*	*	0	39	4	
8/30/2024	17:00	0	1	6	18	21	3	0	0	0	0	0	0	0	0	0	0	0	0	49	28.8	32.8	23 to 33	83.7	*	*	0	48	1	
8/30/2024	18:00	0	0	5	15	19	5	0	1	0	0	0	0	0	0	0	0	0	0	45	29.9	33	23 to 33	80.0	1	2.2	0	43	2	
8/30/2024	19:00	0	1	0	17	10	2	0	0	0	0	0	0	0	0	0	0	0	0	30	28.8	33.3	25 to 35	93.3	*	*	0	29	1	
8/30/2024	20:00	0	0	3	9	20	4	2	1	0	0	0	0	0	0	0	0	0	0	39	31	34.5	26 to 36	82.1	1	2.6	0	39	0	
8/30/2024	21:00	0	0	3	13	10	1	0	0	0	0	0	0	0	0	0	0	0	0	27	28.6	31.5	23 to 33	92.6	*	*	0	27	0	
8/30/2024	22:00	0	0	5	7	5	0	0	0	1	0	0	0	0	0	0	0	0	0	18	28.6	33	24 to 34	83.3	1	5.6	0	18	0	
8/30/2024	23:00	0	0	3	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	13	26.9	28.7	19 to 29	92.3	*	*	0	13	0	
24 Hr Summary		1	5	61	191	156	33	4	3	1	0	0	0	0	0	0	0	0	0	455	29	33	23 to 33	82.2	3	0.7	0	435	20	

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/31/2024	00:00	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	24.3	27	17 to 27	100.0	*	*	0	3	0	
8/31/2024	01:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25.5	28	18 to 28	100.0	*	*	0	2	0	
8/31/2024	02:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	22	22	12 to 22	100.0	*	*	0	1	0	
8/31/2024	03:00	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26	28	22 to 32	75.0	*	*	0	4	0	
8/31/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/31/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/31/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/31/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
8/31/2024	08:00	0	0	2	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	4	28.3	33	13 to 23	50.0	*	*	0	3	1	
8/31/2024	09:00	0	0	3	5	7	0	0	0	0	0	0	0	0	0	0	0	0	0	15	28.2	31.5	22 to 32	93.3	*	*	0	14	1	
8/31/2024	10:00	0	0	1	4	3	1	0	0	0	0	0	0	0	0	0	0	0	0	9	28.4	31	21 to 31	88.9	*	*	0	9	0	
8/31/2024	11:00	0	1	2	10	4	0	0	0	0	0	0	0	0	0	0	0	0	0	17	26.9	30	23 to 33	88.2	*	*	0	17	0	
8/31/2024	12:00	0	0	1	9	5	0	0	0	0	0	0	0	0	0	0	0	0	0	15	27.9	31	23 to 33	100.0	*	*	0	15	0	
8/31/2024	13:00	0	0	2	11	8	2	0	0	0	0	0	0	0	0	0	0	0	0	23	28.7	32	22 to 32	87.0	*	*	0	22	1	
8/31/2024	14:00	0	0	4	14	5	1	1	0	0	0	0	0	0	0	0	0	0	0	25	28.7	31.7	22 to 32	88.0	*	*	0	24	1	
8/31/2024	15:00	0	0	7	15	10	0	0	0	0	0	0	0	0	0	0	0	0	0	32	27.6	30.7	22 to 32	90.6	*	*	0	30	2	
8/31/2024	16:00	0	1	3	10	5	2	0	0	0	0	0	0	0	0	0	0	0	0	21	28.7	33	23 to 33	81.0	*	*	0	21	0	
8/31/2024	17:00	0	0	3	11	9	1	0	0	0	0	0	0	0	0	0	0	0	0	24	28.9	32	23 to 33	91.7	*	*	0	24	0	
8/31/2024	18:00	0	1	4	10	7	1	0	0	0	0	0	0	0	0	0	0	0	0	23	27.9	31.7	22 to 32	87.0	*	*	0	22	1	
8/31/2024	19:00	0	0	2	18	4	5	0	0	0	0	0	0	0	0	0	0	0	0	29	29.5	35	23 to 33	82.8	*	*	0	28	1	
8/31/2024	20:00	0	0	1	14	12	2	0	0	0	0	0	0	0	0	0	0	0	0	29	29.3	32.5	25 to 35	93.1	*	*	0	28	1	
8/31/2024	21:00	0	0	1	11	11	0	0	0	0	0	0	0	0	0	0	0	0	0	23	28.8	31.6	24 to 34	95.7	*	*	0	22	1	
8/31/2024	22:00	0	2	2	7	6	0	0	0	0	0	0	0	0	0	0	0	0	0	17	27.2	30.7	22 to 32	88.2	*	*	0	17	0	
8/31/2024	23:00	0	0	5	6	4	1	0	0	0	0	0	0	0	0	0	0	0	0	16	27.6	32	22 to 32	87.5	*	*	0	16	0	
24 Hr Summary		0	6	46	160	102	17	1	0	0	0	0	0	0	0	0	0	0	0	332	28.3	32	23 to 33	87.3	*	*	0	322	10	

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/1/2024	00:00	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	27	29	19 to 29	100.0	*	*	0	3	0
9/1/2024	01:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28.5	33	23 to 33	100.0	*	*	0	2	0
9/1/2024	02:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26.7	30	20 to 30	100.0	*	*	0	2	1
9/1/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	27	27	17 to 27	100.0	*	*	0	1	0
9/1/2024	04:00	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26.7	28	18 to 28	100.0	*	*	0	3	0
9/1/2024	05:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0
9/1/2024	06:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	31.5	32	22 to 32	100.0	*	*	0	2	0
9/1/2024	07:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	36	36	26 to 36	100.0	*	*	0	1	0
9/1/2024	08:00	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26.3	27	18 to 28	100.0	*	*	0	3	1
9/1/2024	09:00	0	0	2	3	3	1	0	0	0	0	0	0	0	0	0	0	0	0	9	28.3	33	20 to 30	66.7	*	*	0	9	0
9/1/2024	10:00	0	0	2	4	0	0	1	0	0	0	0	0	0	0	0	0	0	0	7	28.1	29	19 to 29	85.7	*	*	0	7	0
9/1/2024	11:00	0	1	1	12	4	0	0	0	0	0	0	0	0	0	0	0	0	0	18	26.7	29.2	20 to 30	94.4	*	*	0	18	0
9/1/2024	12:00	0	0	1	7	9	3	0	0	0	0	0	0	0	0	0	0	0	0	20	30.6	34	26 to 36	90.0	*	*	0	20	0
9/1/2024	13:00	0	0	2	9	7	3	0	0	0	0	0	0	0	0	0	0	0	0	21	29.7	33	23 to 33	81.0	*	*	0	20	1
9/1/2024	14:00	0	1	0	7	9	0	0	0	0	0	0	0	0	0	0	0	0	0	17	29.1	32	24 to 34	94.1	*	*	0	16	1
9/1/2024	15:00	0	0	2	9	12	1	1	0	0	0	0	0	0	0	0	0	0	0	25	29.8	32.5	23 to 33	88.0	*	*	0	24	1
9/1/2024	16:00	0	1	4	9	4	1	0	0	0	0	0	0	0	0	0	0	0	0	19	27.1	29.7	21 to 31	89.5	*	*	0	19	0
9/1/2024	17:00	0	0	4	15	8	1	0	0	0	0	0	0	0	0	0	0	0	0	28	28.3	31	23 to 33	89.3	*	*	0	27	1
9/1/2024	18:00	0	0	3	15	9	0	0	0	0	0	0	0	0	0	0	0	0	0	27	27.8	30	22 to 32	96.3	*	*	0	26	1
9/1/2024	19:00	0	0	2	11	8	4	0	0	0	0	0	0	0	0	0	0	0	0	25	29.6	34	25 to 35	84.0	*	*	0	24	1
9/1/2024	20:00	0	0	2	12	5	2	0	0	0	0	0	0	0	0	0	0	0	0	21	29	32	22 to 32	85.7	*	*	0	20	1
9/1/2024	21:00	0	0	6	14	15	1	0	0	0	0	0	0	0	0	0	0	0	0	36	29	32	24 to 34	88.9	*	*	0	35	1
9/1/2024	22:00	0	1	2	3	5	2	0	0	0	0	0	0	0	0	0	0	0	0	13	28.5	33	25 to 35	76.9	*	*	0	13	0
9/1/2024	23:00	0	1	2	9	5	0	0	0	0	0	0	0	0	0	0	0	0	0	17	27.6	30	21 to 31	88.2	*	*	0	17	0
24 Hr Summary		0	5	39	150	107	20	2	0	0	0	0	0	0	0	0	0	0	0	323	28.6	32	24 to 34	85.1	*	*	0	313	10

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/2/2024	00:00	0	0	1	2	2	0	1	0	0	0	0	0	0	0	0	0	0	0	6	30.5	32	22 to 32	83.3	*	*	0	6	0	
9/2/2024	01:00	0	0	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	4	29	32	16 to 26	50.0	*	*	0	4	0	
9/2/2024	02:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	1	0	
9/2/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0	
9/2/2024	04:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	31	31	21 to 31	100.0	*	*	0	1	0	
9/2/2024	05:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0	
9/2/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/2/2024	07:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	24	24	14 to 24	100.0	*	*	0	1	0	
9/2/2024	08:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27	29	19 to 29	100.0	*	*	0	2	0	
9/2/2024	09:00	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	30	32	22 to 32	100.0	*	*	0	3	0	
9/2/2024	10:00	0	0	0	2	4	0	0	0	0	0	0	0	0	0	0	0	0	0	6	30.5	33.5	24 to 34	100.0	*	*	0	6	0	
9/2/2024	11:00	0	1	2	9	3	0	0	0	0	0	0	0	0	0	0	0	0	0	15	26.4	30	22 to 32	86.7	*	*	0	15	0	
9/2/2024	12:00	1	1	3	4	10	1	0	0	0	0	0	0	0	0	0	0	0	0	20	28.1	32	23 to 33	80.0	*	*	0	18	2	
9/2/2024	13:00	0	0	4	14	5	2	0	0	0	0	0	0	0	0	0	0	0	0	25	28	32	25 to 35	84.0	*	*	0	24	1	
9/2/2024	14:00	0	2	6	12	7	0	0	0	0	0	0	0	0	0	0	0	0	0	27	26.4	31	23 to 33	85.2	*	*	0	26	1	
9/2/2024	15:00	0	3	5	11	11	2	0	0	0	0	0	0	0	0	0	0	0	0	32	27.7	31.5	22 to 32	78.1	*	*	0	29	3	
9/2/2024	16:00	0	0	6	13	9	1	1	0	0	0	0	0	0	0	0	0	0	0	30	28.4	32	23 to 33	90.0	*	*	0	29	1	
9/2/2024	17:00	0	0	3	11	11	1	1	0	0	0	0	0	0	0	0	0	0	0	27	29.1	33	24 to 34	88.9	*	*	0	26	1	
9/2/2024	18:00	0	0	4	10	11	1	0	0	0	0	0	0	0	0	0	0	0	0	26	28.7	30.8	23 to 33	92.3	*	*	0	25	1	
9/2/2024	19:00	0	0	4	15	8	1	1	0	0	0	0	0	0	0	0	0	0	0	29	28.6	31.7	22 to 32	89.7	*	*	0	28	1	
9/2/2024	20:00	0	0	2	6	14	2	0	0	0	0	0	0	0	0	0	0	0	0	24	30.3	33	24 to 34	91.7	*	*	0	23	1	
9/2/2024	21:00	0	0	1	4	8	3	0	0	0	0	0	0	0	0	0	0	0	0	16	30.9	35	27 to 37	87.5	*	*	0	15	1	
9/2/2024	22:00	0	1	1	10	5	2	0	0	0	0	0	0	0	0	0	0	0	0	19	28.2	31.7	22 to 32	84.2	*	*	0	18	1	
9/2/2024	23:00	0	1	0	5	3	1	2	0	0	0	0	0	0	0	0	0	0	0	12	30.6	36	21 to 31	66.7	*	*	0	11	1	
24 Hr Summary		1	9	44	135	115	18	6	0	0	0	0	0	0	0	0	0	0	0	328	28.5	32	23 to 33	82.6	*	*	0	313	15	

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/3/2024	00:00	0	0	1	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	5	29	32	24 to 34	80.0	*	*	0	4	1	
9/3/2024	01:00	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	27.8	30	21 to 31	100.0	*	*	0	4	0	
9/3/2024	02:00	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	27.3	36	15 to 25	66.7	*	*	0	3	0	
9/3/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	27	27	17 to 27	100.0	*	*	0	1	0	
9/3/2024	04:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27.5	31	21 to 31	100.0	*	*	0	2	0	
9/3/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	06:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	32	32	22 to 32	100.0	*	*	0	1	0	
9/3/2024	07:00	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3	31.7	33	23 to 33	100.0	*	*	0	3	0	
9/3/2024	08:00	0	0	0	10	4	1	0	0	0	0	0	0	0	0	0	0	0	0	15	29.9	33.5	24 to 34	93.3	*	*	0	13	2	
9/3/2024	09:00	0	0	1	4	5	0	2	0	0	0	0	0	0	0	0	0	0	0	12	31.2	33	23 to 33	83.3	*	*	0	11	1	
9/3/2024	10:00	0	0	3	4	9	2	0	0	0	0	0	0	0	0	0	0	0	0	18	29.2	31.8	22 to 32	88.9	*	*	0	18	0	
9/3/2024	11:00	0	0	3	10	6	0	0	0	0	0	0	0	0	0	0	0	0	0	19	28.3	31	23 to 33	100.0	*	*	0	18	1	
9/3/2024	12:00	0	0	0	9	10	0	0	0	0	0	0	0	0	0	0	0	0	0	19	29.4	31	23 to 33	100.0	*	*	0	19	0	
9/3/2024	13:00	0	0	3	7	13	1	1	0	0	0	0	0	0	0	0	0	0	0	25	30.1	32	22 to 32	84.0	*	*	0	23	2	
9/3/2024	14:00	0	0	5	15	7	1	0	0	0	0	0	0	0	0	0	0	0	0	28	28.3	32	23 to 33	89.3	*	*	0	27	1	
9/3/2024	15:00	0	0	5	7	11	1	0	0	0	0	0	0	0	0	0	0	0	0	24	28.3	31.6	23 to 33	87.5	*	*	0	21	3	
9/3/2024	16:00	0	0	12	18	14	1	0	0	0	0	0	0	0	0	0	0	0	0	45	27.7	32	23 to 33	82.2	*	*	0	42	3	
9/3/2024	17:00	0	1	3	19	12	1	1	0	0	0	0	0	0	0	0	0	0	0	37	28.5	31	24 to 34	91.9	*	*	0	37	0	
9/3/2024	18:00	0	0	3	18	27	4	0	0	0	0	0	0	0	0	0	0	0	0	52	29.8	32.7	24 to 34	88.5	*	*	0	50	2	
9/3/2024	19:00	0	0	6	17	15	3	0	0	0	0	0	0	0	0	0	0	0	0	41	28.8	32	23 to 33	87.8	*	*	0	38	3	
9/3/2024	20:00	0	0	2	17	9	0	0	0	1	0	0	0	0	0	0	0	0	0	29	29.3	31.5	24 to 34	96.6	1	3.4	0	29	0	
9/3/2024	21:00	0	0	0	14	5	2	0	0	0	0	0	0	0	0	0	0	0	0	21	29.5	33.5	25 to 35	95.2	*	*	0	20	1	
9/3/2024	22:00	0	0	2	5	8	3	0	0	0	0	0	0	0	0	0	0	0	0	18	30	32	25 to 35	88.9	*	*	0	17	1	
9/3/2024	23:00	0	1	1	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	11	27.5	30	22 to 32	90.9	*	*	0	9	2	
24 Hr Summary		0	2	53	183	169	21	4	0	1	0	0	0	0	0	0	0	0	0	433	29	33	24 to 34	86.8	1	0.2	0	410	23	

Outgoing Histogram

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

from Thu-Aug-29-2024-12-00-PM to Wed-Sep-04-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	0	0	0	5	0	2	0	0	0	0	0	0	0	0	0	0	0	0	7	30.3	36	26 to 36	85.7	*	*	0	6	1	
9/4/2024	01:00	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28.8	29	24 to 34	100.0	*	*	0	4	0	
9/4/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	03:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30	32	22 to 32	100.0	*	*	0	2	0	
9/4/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	06:00	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24	29	19 to 29	100.0	*	*	0	2	0	
9/4/2024	07:00	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	4	32.8	34.5	25 to 35	100.0	*	*	0	4	0	
9/4/2024	08:00	0	0	1	7	4	1	0	0	0	0	0	0	0	0	0	0	0	0	13	28.5	33	25 to 35	92.3	*	*	0	11	2	
9/4/2024	09:00	0	0	1	6	5	0	0	0	0	0	0	0	0	0	0	0	0	0	12	29.8	33	24 to 34	100.0	*	*	0	12	0	
9/4/2024	10:00	0	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26	27	20 to 30	100.0	*	*	0	4	0	
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	1	4	24	14	5	0	0	0	0	0	0	0	0	0	0	0	0	48	29.1	34	25 to 35	85.4	*	*	0	45	3	

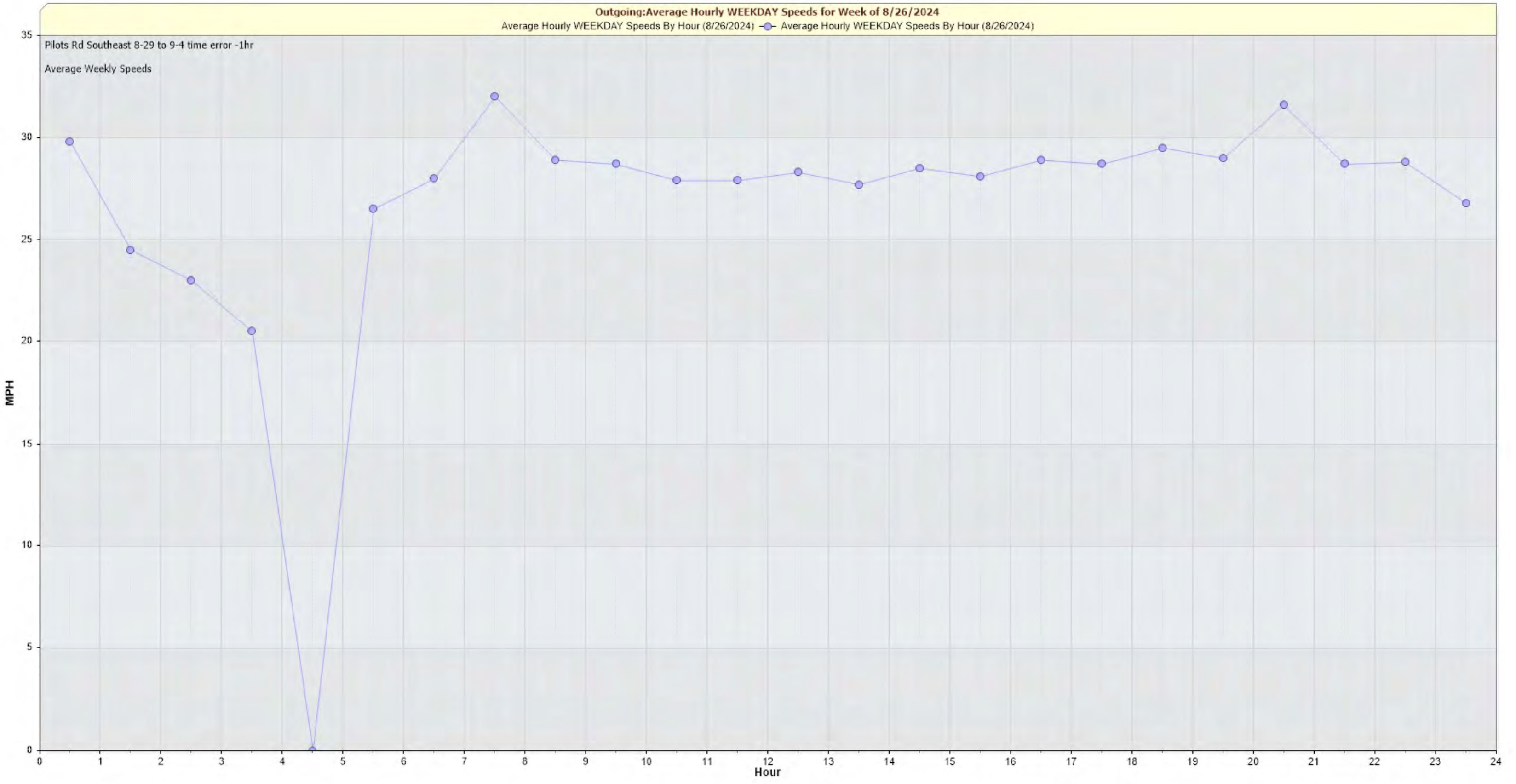
Outgoing: Average Hourly Volume for Week of 8/26/2024
Average Counts By Hour (8/26/2024)

Pilots Rd Southeast 8-29 to 9-4 time error -1hr
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 8/26/2024
Average Hourly WEEKDAY Speeds By Hour (8/26/2024) — Average Hourly WEEKDAY Speeds By Hour (8/26/2024)

Pilots Rd Southeast 8-29 to 9-4 time error -1hr
Average Weekly Speeds



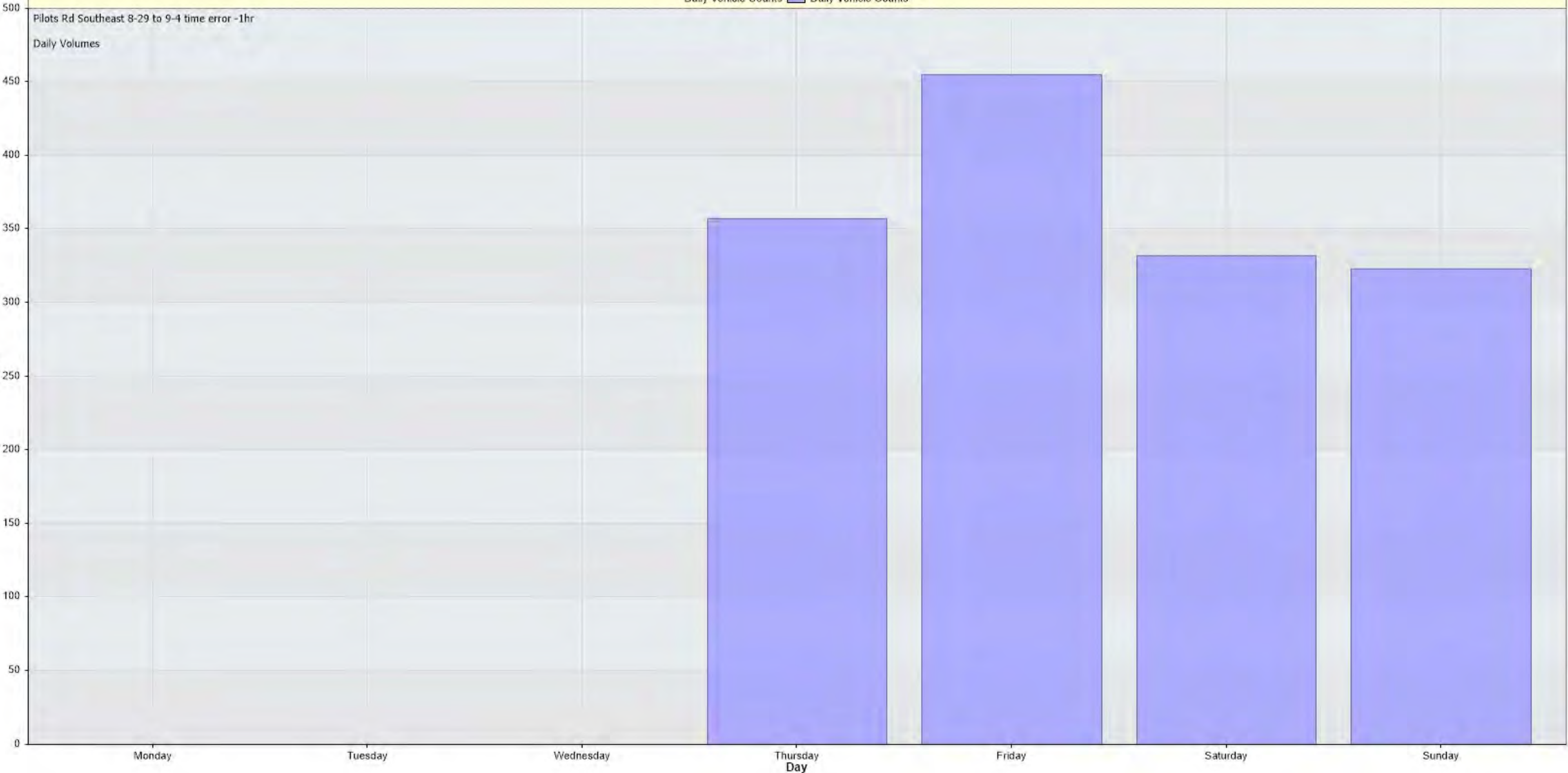
Outgoing:Daily Volume for Week of 8/26/2024

Daily Vehicle Counts

Pilots Rd Southeast 8-29 to 9-4 time error -1hr

Daily Volumes

Vehicles



For Project: Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs
 Project Notes:
 Location/Name: Incoming
 Report Generated: 10/3/2024 3:33:12 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 8/29/2024 1:00:00 PM through 9/3/2024 1:59:59 PM
 85th Percentile Speed: 44 MPH
 85th Percentile Vehicles: 5182
 Max Speed: 98 MPH on 8/29/2024 3:06:29 PM
 Total Vehicles: 6096
 AADT: 1209

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	992	1016
AM Peak 11:00 AM	70	64
PM Peak 8:00 PM	111	103

Speed

Speed Limit: 45
 85th Percentile Speed: 44
 50th Percentile Speed: 39
 10 MPH Pace Interval: 34.0 MPH to 44.0 MPH
 Average Speed: 38.54

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	73	34	N/A	59	123	64	83
% over limit	7.1	7.3	N/A	6.1	8.2	6.0	7.8
Avg Speeder	48.2	47.9	N/A	48.8	48.3	48.2	48.1
Avg Speed	38.5	38.5	N/A	38.6	38.5	38.6	38.6

Class Counts

	Number	%
VEH_SM	14	0.2
VEH_MED	5758	94.5
VEH_LG	324	5.3
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Incoming Summary
Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/29/2024 2:00:00 PM	43.0	24	28	46	46.0	3.6%	39.0
8/29/2024 3:00:00 PM	42.0	79	93	51	48.3	4.3%	37.2
8/29/2024 4:00:00 PM	43.0	93	109	98	64.0	2.8%	37.8
8/29/2024 5:00:00 PM	43.0	87	102	51	48.0	3.9%	38.7
8/29/2024 6:00:00 PM	43.0	87	102	49	47.6	4.9%	37.5
8/29/2024 7:00:00 PM	41.0	84	99	49	47.4	5.1%	37.7
8/29/2024 8:00:00 PM	43.0	93	109	52	48.4	7.3%	39.5
8/29/2024 9:00:00 PM	45.0	88	103	51	47.9	12.6%	40.0
8/29/2024 10:00:00 PM	44.0	76	89	54	49.3	9.0%	39.9
8/29/2024 11:00:00 PM	44.0	59	69	49	47.6	7.2%	38.8
8/30/2024 12:00:00 AM	41.0	58	68	47	47.0	4.4%	38.4
8/30/2024 1:00:00 AM	44.0	39	46	58	49.2	13.0%	39.2
8/30/2024 2:00:00 AM	43.0	17	20	48	48.0	5.0%	38.5
8/30/2024 3:00:00 AM	47.0	8	9	47	47.0	22.2%	38.3
8/30/2024 4:00:00 AM	40.0	3	4	42	0.0	0.0%	40.0
8/30/2024 5:00:00 AM	31.0	1	1	31	0.0	0.0%	31.0
8/30/2024 6:00:00 AM	52.0	2	2	52	51.0	100.0%	51.0
8/30/2024 7:00:00 AM	38.0	1	1	38	0.0	0.0%	38.0
8/30/2024 8:00:00 AM	40.0	4	5	40	0.0	0.0%	39.2
8/30/2024 9:00:00 AM	44.0	12	14	47	47.0	7.1%	40.7
8/30/2024 10:00:00 AM	44.0	36	42	52	48.8	9.5%	39.0
8/30/2024 11:00:00 AM	45.0	71	84	57	49.8	15.5%	40.4
8/30/2024 12:00:00 PM	44.0	71	83	49	46.7	13.1%	38.7
8/30/2024 1:00:00 PM	44.0	87	102	62	50.4	7.8%	38.1
8/30/2024 2:00:00 PM	43.0	77	91	51	49.0	2.2%	37.9
8/30/2024 3:00:00 PM	42.0	85	100	51	47.8	4.0%	38.1
8/30/2024 4:00:00 PM	44.0	82	96	50	47.0	10.4%	38.6
8/30/2024 5:00:00 PM	43.0	82	97	49	47.6	5.2%	37.9
8/30/2024 6:00:00 PM	43.0	101	119	50	47.7	5.0%	37.1
8/30/2024 7:00:00 PM	42.0	118	139	52	48.4	5.0%	37.5
8/30/2024 8:00:00 PM	43.0	100	118	58	49.8	6.8%	38.7
8/30/2024 9:00:00 PM	45.0	123	145	55	47.9	10.3%	39.1
8/30/2024 10:00:00 PM	42.0	74	87	50	47.0	5.7%	37.0
8/30/2024 11:00:00 PM	44.0	48	56	52	48.6	8.9%	39.5
8/31/2024 12:00:00 AM	47.0	39	46	50	47.8	17.4%	40.6
8/31/2024 1:00:00 AM	42.0	42	50	45	0.0	0.0%	37.6
8/31/2024 2:00:00 AM	44.0	19	22	45	0.0	0.0%	39.0
8/31/2024 3:00:00 AM	44.0	8	9	47	47.0	11.1%	40.7
8/31/2024 4:00:00 AM	36.0	1	1	36	0.0	0.0%	36.0
8/31/2024 5:00:00 AM	29.0	1	1	29	0.0	0.0%	29.0
8/31/2024 6:00:00 AM	42.0	3	3	42	0.0	0.0%	36.7
8/31/2024 7:00:00 AM	37.0	1	1	37	0.0	0.0%	37.0
8/31/2024 8:00:00 AM	43.0	2	2	43	0.0	0.0%	42.0
8/31/2024 9:00:00 AM	51.0	2	2	51	51.0	50.0%	43.0
8/31/2024 10:00:00 AM	43.0	13	15	50	48.5	13.3%	38.9
8/31/2024 11:00:00 AM	41.0	20	24	53	53.0	4.2%	36.8
8/31/2024 12:00:00 PM	43.0	43	51	48	47.0	5.9%	38.3

Incoming Summary
 Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

8/31/2024 1:00:00 PM	43.0	43	51	54	50.3	7.8%	39.1
8/31/2024 2:00:00 PM	44.0	59	69	50	48.0	4.3%	39.4
8/31/2024 3:00:00 PM	43.0	86	101	48	46.9	9.9%	37.9
8/31/2024 4:00:00 PM	43.0	75	88	48	46.8	4.5%	38.2
8/31/2024 5:00:00 PM	43.0	69	81	49	48.0	2.5%	38.6
8/31/2024 6:00:00 PM	43.0	72	85	51	48.8	4.7%	39.3
8/31/2024 7:00:00 PM	44.0	68	80	64	50.1	11.3%	39.5
8/31/2024 8:00:00 PM	43.0	74	87	53	48.4	5.7%	38.4
8/31/2024 9:00:00 PM	44.0	60	71	48	46.7	4.2%	39.1
8/31/2024 10:00:00 PM	43.0	54	63	54	48.2	7.9%	38.4
8/31/2024 11:00:00 PM	42.0	44	52	47	47.0	1.9%	37.9
9/1/2024 12:00:00 AM	44.0	43	51	48	47.0	11.8%	38.8
9/1/2024 1:00:00 AM	43.0	26	31	47	47.0	3.2%	38.6
9/1/2024 2:00:00 AM	40.0	31	37	52	48.0	10.8%	37.6
9/1/2024 3:00:00 AM	40.0	5	6	40	0.0	0.0%	30.3
9/1/2024 4:00:00 AM	41.0	4	5	43	0.0	0.0%	38.6
9/1/2024 5:00:00 AM	37.0	6	7	40	0.0	0.0%	33.3
9/1/2024 6:00:00 AM	49.0	3	4	49	49.0	50.0%	42.0
9/1/2024 7:00:00 AM	43.0	3	4	47	47.0	25.0%	41.3
9/1/2024 8:00:00 AM	41.0	2	2	41	0.0	0.0%	38.0
9/1/2024 9:00:00 AM	53.0	3	3	53	53.0	33.3%	41.0
9/1/2024 10:00:00 AM	43.0	15	18	46	46.0	5.6%	34.1
9/1/2024 11:00:00 AM	44.0	13	15	51	50.0	13.3%	39.6
9/1/2024 12:00:00 PM	44.0	51	60	47	46.3	6.7%	39.8
9/1/2024 1:00:00 PM	42.0	54	63	51	49.0	3.2%	38.6
9/1/2024 2:00:00 PM	44.0	54	63	48	47.5	6.3%	38.4
9/1/2024 3:00:00 PM	44.0	51	60	49	47.4	15.0%	39.3
9/1/2024 4:00:00 PM	44.0	65	76	50	48.1	11.8%	38.9
9/1/2024 5:00:00 PM	44.0	61	72	52	49.0	5.6%	40.0
9/1/2024 6:00:00 PM	43.0	77	91	56	49.0	9.9%	39.6
9/1/2024 7:00:00 PM	43.0	92	108	53	48.3	7.4%	38.7
9/1/2024 8:00:00 PM	42.0	78	92	55	48.8	6.5%	37.8
9/1/2024 9:00:00 PM	44.0	79	93	49	47.0	6.5%	38.8
9/1/2024 10:00:00 PM	43.0	52	61	50	47.5	6.6%	37.6
9/1/2024 11:00:00 PM	44.0	49	58	51	48.8	6.9%	38.5
9/2/2024 12:00:00 AM	43.0	31	37	46	46.0	5.4%	37.5
9/2/2024 1:00:00 AM	42.0	32	38	54	50.0	7.9%	38.0
9/2/2024 2:00:00 AM	45.0	14	17	47	46.7	17.6%	40.9
9/2/2024 3:00:00 AM	48.0	5	6	48	48.0	33.3%	41.3
9/2/2024 4:00:00 AM	45.0	3	3	45	0.0	0.0%	41.7
9/2/2024 5:00:00 AM	41.0	6	7	41	0.0	0.0%	31.7
9/2/2024 6:00:00 AM	38.0	5	6	47	47.0	16.7%	36.3
9/2/2024 7:00:00 AM	32.0	1	1	32	0.0	0.0%	32.0
9/2/2024 8:00:00 AM	41.0	2	2	41	0.0	0.0%	31.0
9/2/2024 9:00:00 AM	42.0	2	2	42	0.0	0.0%	38.0
9/2/2024 10:00:00 AM	37.0	6	7	46	46.0	14.3%	34.7
9/2/2024 11:00:00 AM	38.0	11	13	40	0.0	0.0%	34.6
9/2/2024 12:00:00 PM	42.0	34	40	54	50.0	7.5%	38.8

Incoming Summary
 Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

9/2/2024 1:00:00 PM	44.0	48	56	48	46.6	8.9%	39.4
9/2/2024 2:00:00 PM	42.0	69	81	49	47.0	4.9%	38.1
9/2/2024 3:00:00 PM	42.0	63	74	63	56.0	2.7%	38.1
9/2/2024 4:00:00 PM	44.0	60	70	49	47.0	5.7%	38.4
9/2/2024 5:00:00 PM	42.0	67	79	47	46.5	2.5%	37.5
9/2/2024 6:00:00 PM	44.0	67	79	55	49.1	8.9%	39.3
9/2/2024 7:00:00 PM	43.0	78	92	59	49.4	8.7%	38.2
9/2/2024 8:00:00 PM	44.0	59	69	50	47.0	7.2%	39.6
9/2/2024 9:00:00 PM	45.0	73	86	51	48.6	8.1%	39.9
9/2/2024 10:00:00 PM	45.0	74	87	50	47.2	11.5%	38.9
9/2/2024 11:00:00 PM	43.0	48	56	59	49.0	8.9%	38.1
9/3/2024 12:00:00 AM	43.0	46	54	47	47.0	1.9%	38.1
9/3/2024 1:00:00 AM	42.0	27	32	47	47.0	3.1%	37.9
9/3/2024 2:00:00 AM	39.0	9	11	41	0.0	0.0%	34.9
9/3/2024 3:00:00 AM	55.0	3	3	55	55.0	33.3%	44.0
9/3/2024 4:00:00 AM	46.0	3	3	46	46.0	33.3%	38.3
9/3/2024 5:00:00 AM	37.0	3	3	37	0.0	0.0%	33.3
9/3/2024 6:00:00 AM	39.0	2	2	39	0.0	0.0%	38.5
9/3/2024 7:00:00 AM	39.0	3	3	39	0.0	0.0%	31.3
9/3/2024 8:00:00 AM	32.0	4	5	40	0.0	0.0%	29.6
9/3/2024 9:00:00 AM	45.0	25	29	49	48.5	6.9%	39.6
9/3/2024 10:00:00 AM	43.0	45	53	47	46.5	3.8%	36.6
9/3/2024 11:00:00 AM	45.0	87	102	51	47.9	10.8%	40.0
9/3/2024 12:00:00 PM	44.0	72	85	53	48.6	9.3%	39.2
9/3/2024 1:00:00 PM	44.0	85	100	49	47.0	6.0%	37.9
9/3/2024 2:00:00 PM	42.0	31	36	46	46.0	5.6%	39.0

Incoming Summary

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/30/2024 12:00:00 AM	43.0	825	971	98	48.8	6.1%	38.6
8/31/2024 12:00:00 AM	44.0	1281	1507	62	48.3	8.2%	38.5
9/1/2024 12:00:00 AM	43.0	901	1060	64	48.2	6.0%	38.6
9/2/2024 12:00:00 AM	44.0	906	1066	56	48.1	7.8%	38.6
9/3/2024 12:00:00 AM	44.0	871	1025	63	48.2	7.1%	38.5
9/3/2024 1:59:59 PM	44.0	397	467	55	47.9	7.3%	38.5

Incoming Weekly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	46	50	31	46	40.5	44
1 - 2	*	*	*	*	20	22	37	20	29.5	43
2 - 3	*	*	*	*	9	9	6	9	7.5	46.5
3 - 4	*	*	*	*	4	1	5	4	3	40
4 - 5	*	*	*	*	1	1	7	1	4	31
5 - 6	*	*	*	*	2	3	4	2	3.5	52
6 - 7	*	*	*	*	1	1	4	1	2.5	38
7 - 8	*	*	*	*	5	2	2	5	2	39.7
8 - 9	*	*	*	*	14	2	3	14	2.5	44
9 - 10	*	*	*	*	42	15	18	42	16.5	43.5
10 - 11	*	*	*	*	84	24	15	84	19.5	45
11 - 12	*	*	*	*	83	51	60	83	55.5	44.5
12 - 13	*	*	*	*	102	51	63	102	57	43.6
13 - 14	*	*	*	28	91	69	63	59.5	66	42.7
14 - 15	*	*	*	93	100	101	60	96.5	80.5	41.95
15 - 16	*	*	*	109	96	88	76	102.5	82	43.3
16 - 17	*	*	*	102	97	81	72	99.5	76.5	42.7
17 - 18	*	*	*	102	119	85	91	110.5	88	42.6
18 - 19	*	*	*	99	139	80	108	119	94	41.25
19 - 20	*	*	*	109	118	87	92	113.5	89.5	42.6
20 - 21	*	*	*	103	145	71	93	124	82	44.4
21 - 22	*	*	*	89	87	63	61	88	62	42.3
22 - 23	*	*	*	69	56	52	58	62.5	55	43.4
23 - 24	*	*	*	68	46	51	37	57	44	43.6
Totals	0	0	0	971	1507	1060	1066			
% of Total	0%	0%	0%	21.09%	32.73%	23.02%	23.15%			

Incoming Weekly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	38	32	*	*	*	*	*	35	0	42
1 - 2	17	11	*	*	*	*	*	14	0	42
2 - 3	6	3	*	*	*	*	*	4.5	0	51.25
3 - 4	3	3	*	*	*	*	*	3	0	45.5
4 - 5	7	3	*	*	*	*	*	5	0	38.75
5 - 6	6	2	*	*	*	*	*	4	0	38.5
6 - 7	1	3	*	*	*	*	*	2	0	35.5
7 - 8	2	5	*	*	*	*	*	3.5	0	36.5
8 - 9	2	29	*	*	*	*	*	15.5	0	43.15
9 - 10	7	53	*	*	*	*	*	30	0	39.75
10 - 11	13	102	*	*	*	*	*	57.5	0	41.1
11 - 12	40	85	*	*	*	*	*	62.5	0	42.75
12 - 13	56	100	*	*	*	*	*	78	0	43.6
13 - 14	81	36	*	*	*	*	*	58.5	0	41.75
14 - 15	74	*	*	*	*	*	*	74	0	42
15 - 16	70	*	*	*	*	*	*	70	0	43.2
16 - 17	79	*	*	*	*	*	*	79	0	42
17 - 18	79	*	*	*	*	*	*	79	0	43.8
18 - 19	92	*	*	*	*	*	*	92	0	42.8
19 - 20	69	*	*	*	*	*	*	69	0	44
20 - 21	86	*	*	*	*	*	*	86	0	44.1
21 - 22	87	*	*	*	*	*	*	87	0	44.4
22 - 23	56	*	*	*	*	*	*	56	0	42.7
23 - 24	54	*	*	*	*	*	*	54	0	42.5
Totals	1025	467	0	0	0	0	0			
% of Total	68.7%	31.3%	0%	0%	0%	0%	0%			

Incoming Monthly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	46	50	*	46	50	44
1 - 2	*	*	*	*	20	22	*	20	22	43
2 - 3	*	*	*	*	9	9	*	9	9	46.5
3 - 4	*	*	*	*	4	1	*	4	1	40
4 - 5	*	*	*	*	1	1	*	1	1	31
5 - 6	*	*	*	*	2	3	*	2	3	52
6 - 7	*	*	*	*	1	1	*	1	1	38
7 - 8	*	*	*	*	5	2	*	5	2	39.7
8 - 9	*	*	*	*	14	2	*	14	2	44
9 - 10	*	*	*	*	42	15	*	42	15	43.5
10 - 11	*	*	*	*	84	24	*	84	24	45
11 - 12	*	*	*	*	83	51	*	83	51	44.5
12 - 13	*	*	*	*	102	51	*	102	51	43.6
13 - 14	*	*	*	28	91	69	*	59.5	69	42.7
14 - 15	*	*	*	93	100	101	*	96.5	101	41.95
15 - 16	*	*	*	109	96	88	*	102.5	88	43.3
16 - 17	*	*	*	102	97	81	*	99.5	81	42.7
17 - 18	*	*	*	102	119	85	*	110.5	85	42.6
18 - 19	*	*	*	99	139	80	*	119	80	41.25
19 - 20	*	*	*	109	118	87	*	113.5	87	42.6
20 - 21	*	*	*	103	145	71	*	124	71	44.4
21 - 22	*	*	*	89	87	63	*	88	63	42.3
22 - 23	*	*	*	69	56	52	*	62.5	52	43.4
23 - 24	*	*	*	68	46	51	*	57	51	43.6
Totals	0	0	0	971	1507	1060	0			
% of Total	0%	0%	0%	27.44%	42.59%	29.96%	0%			

Incoming Monthly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	38	32	*	*	*	*	31	35	31	42
1 - 2	17	11	*	*	*	*	37	14	37	42
2 - 3	6	3	*	*	*	*	6	4.5	6	51.25
3 - 4	3	3	*	*	*	*	5	3	5	45.5
4 - 5	7	3	*	*	*	*	7	5	7	38.75
5 - 6	6	2	*	*	*	*	4	4	4	38.5
6 - 7	1	3	*	*	*	*	4	2	4	35.5
7 - 8	2	5	*	*	*	*	2	3.5	2	36.5
8 - 9	2	29	*	*	*	*	3	15.5	3	43.15
9 - 10	7	53	*	*	*	*	18	30	18	39.75
10 - 11	13	102	*	*	*	*	15	57.5	15	41.1
11 - 12	40	85	*	*	*	*	60	62.5	60	42.75
12 - 13	56	100	*	*	*	*	63	78	63	43.6
13 - 14	81	36	*	*	*	*	63	58.5	63	41.75
14 - 15	74	*	*	*	*	*	60	74	60	42
15 - 16	70	*	*	*	*	*	76	70	76	43.2
16 - 17	79	*	*	*	*	*	72	79	72	42
17 - 18	79	*	*	*	*	*	91	79	91	43.8
18 - 19	92	*	*	*	*	*	108	92	108	42.8
19 - 20	69	*	*	*	*	*	92	69	92	44
20 - 21	86	*	*	*	*	*	93	86	93	44.1
21 - 22	87	*	*	*	*	*	61	87	61	44.4
22 - 23	56	*	*	*	*	*	58	56	58	42.7
23 - 24	54	*	*	*	*	*	37	54	37	42.5
Totals	1025	467	0	0	0	0	1066			
% of Total	40.07%	18.26%	0%	0%	0%	0%	41.67%			

Incoming Weekly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	39.17	37.64	38.65	39.17	38.02	44
1 - 2	*	*	*	*	38.55	39	37.57	38.55	38.1	43
2 - 3	*	*	*	*	38.33	40.67	30.33	38.33	36.53	46.5
3 - 4	*	*	*	*	40	36	38.6	40	38.17	40
4 - 5	*	*	*	*	31	29	33.29	31	32.75	31
5 - 6	*	*	*	*	51	36.67	42	51	39.71	52
6 - 7	*	*	*	*	38	37	41.25	38	40.4	38
7 - 8	*	*	*	*	39.2	42	38	39.2	40	39.7
8 - 9	*	*	*	*	40.71	43	41	40.71	41.8	44
9 - 10	*	*	*	*	39.05	38.87	34.06	39.05	36.24	43.5
10 - 11	*	*	*	*	40.42	36.83	39.6	40.42	37.9	45
11 - 12	*	*	*	*	38.75	38.25	39.77	38.75	39.07	44.5
12 - 13	*	*	*	*	38.13	39.1	38.63	38.13	38.84	43.6
13 - 14	*	*	*	39.11	37.95	39.41	38.38	38.22	38.92	42.7
14 - 15	*	*	*	37.2	38.14	37.87	39.27	37.69	38.39	41.95
15 - 16	*	*	*	37.78	38.58	38.17	38.88	38.16	38.5	43.3
16 - 17	*	*	*	38.7	37.92	38.58	39.96	38.32	39.23	42.7
17 - 18	*	*	*	37.51	37.07	39.28	39.62	37.27	39.45	42.6
18 - 19	*	*	*	37.71	37.51	39.53	38.71	37.59	39.06	41.25
19 - 20	*	*	*	39.49	38.72	38.41	37.75	39.09	38.07	42.6
20 - 21	*	*	*	40.03	39.09	39.06	38.84	39.48	38.93	44.4
21 - 22	*	*	*	39.91	37	38.44	37.62	38.47	38.04	42.3
22 - 23	*	*	*	38.8	39.48	37.94	38.53	39.1	38.25	43.4
23 - 24	*	*	*	38.38	40.61	38.84	37.51	39.28	38.28	43.6
Totals	0	0	0	38.6	38.5	38.6	38.6			
% of Total	0%	0%	0%	25.02%	24.95%	25.02%	25.02%			

Incoming Weekly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	38.03	37.94	*	*	*	*	*	37.99	*	42
1 - 2	40.88	34.91	*	*	*	*	*	38.54	*	42
2 - 3	41.33	44	*	*	*	*	*	42.22	*	51.25
3 - 4	41.67	38.33	*	*	*	*	*	40	*	45.5
4 - 5	31.71	33.33	*	*	*	*	*	32.2	*	38.75
5 - 6	36.33	38.5	*	*	*	*	*	36.87	*	38.5
6 - 7	32	31.33	*	*	*	*	*	31.5	*	35.5
7 - 8	31	29.6	*	*	*	*	*	30	*	36.5
8 - 9	38	39.62	*	*	*	*	*	39.52	*	43.15
9 - 10	34.71	36.62	*	*	*	*	*	36.4	*	39.75
10 - 11	34.62	40.03	*	*	*	*	*	39.42	*	41.1
11 - 12	38.78	39.2	*	*	*	*	*	39.06	*	42.75
12 - 13	39.39	37.94	*	*	*	*	*	38.46	*	43.6
13 - 14	38.12	39	*	*	*	*	*	38.39	*	41.75
14 - 15	38.11	*	*	*	*	*	*	38.11	*	42
15 - 16	38.39	*	*	*	*	*	*	38.39	*	43.2
16 - 17	37.53	*	*	*	*	*	*	37.53	*	42
17 - 18	39.32	*	*	*	*	*	*	39.32	*	43.8
18 - 19	38.2	*	*	*	*	*	*	38.2	*	42.8
19 - 20	39.64	*	*	*	*	*	*	39.64	*	44
20 - 21	39.88	*	*	*	*	*	*	39.88	*	44.1
21 - 22	38.93	*	*	*	*	*	*	38.93	*	44.4
22 - 23	38.12	*	*	*	*	*	*	38.12	*	42.7
23 - 24	38.15	*	*	*	*	*	*	38.15	*	42.5
Totals	38.5	38.5	0	0	0	0	0			
% of Total	50%	50%	0%	0%	0%	0%	0%			

Incoming Monthly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	39.17	37.64	*	39.17	37.64	44
1 - 2	*	*	*	*	38.55	39	*	38.55	39	43
2 - 3	*	*	*	*	38.33	40.67	*	38.33	40.67	46.5
3 - 4	*	*	*	*	40	36	*	40	36	40
4 - 5	*	*	*	*	31	29	*	31	29	31
5 - 6	*	*	*	*	51	36.67	*	51	36.67	52
6 - 7	*	*	*	*	38	37	*	38	37	38
7 - 8	*	*	*	*	39.2	42	*	39.2	42	39.7
8 - 9	*	*	*	*	40.71	43	*	40.71	43	44
9 - 10	*	*	*	*	39.05	38.87	*	39.05	38.87	43.5
10 - 11	*	*	*	*	40.42	36.83	*	40.42	36.83	45
11 - 12	*	*	*	*	38.75	38.25	*	38.75	38.25	44.5
12 - 13	*	*	*	*	38.13	39.1	*	38.13	39.1	43.6
13 - 14	*	*	*	39.11	37.95	39.41	*	38.22	39.41	42.7
14 - 15	*	*	*	37.2	38.14	37.87	*	37.69	37.87	41.95
15 - 16	*	*	*	37.78	38.58	38.17	*	38.16	38.17	43.3
16 - 17	*	*	*	38.7	37.92	38.58	*	38.32	38.58	42.7
17 - 18	*	*	*	37.51	37.07	39.28	*	37.27	39.28	42.6
18 - 19	*	*	*	37.71	37.51	39.53	*	37.59	39.53	41.25
19 - 20	*	*	*	39.49	38.72	38.41	*	39.09	38.41	42.6
20 - 21	*	*	*	40.03	39.09	39.06	*	39.48	39.06	44.4
21 - 22	*	*	*	39.91	37	38.44	*	38.47	38.44	42.3
22 - 23	*	*	*	38.8	39.48	37.94	*	39.1	37.94	43.4
23 - 24	*	*	*	38.38	40.61	38.84	*	39.28	38.84	43.6
Totals	0	0	0	38.6	38.5	38.6	0			
% of Total	0%	0%	0%	33.36%	33.28%	33.36%	0%			

Incoming Monthly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	38.03	37.94	*	*	*	*	38.65	37.99	38.65	42
1 - 2	40.88	34.91	*	*	*	*	37.57	38.54	37.57	42
2 - 3	41.33	44	*	*	*	*	30.33	42.22	30.33	51.25
3 - 4	41.67	38.33	*	*	*	*	38.6	40	38.6	45.5
4 - 5	31.71	33.33	*	*	*	*	33.29	32.2	33.29	38.75
5 - 6	36.33	38.5	*	*	*	*	42	36.87	42	38.5
6 - 7	32	31.33	*	*	*	*	41.25	31.5	41.25	35.5
7 - 8	31	29.6	*	*	*	*	38	30	38	36.5
8 - 9	38	39.62	*	*	*	*	41	39.52	41	43.15
9 - 10	34.71	36.62	*	*	*	*	34.06	36.4	34.06	39.75
10 - 11	34.62	40.03	*	*	*	*	39.6	39.42	39.6	41.1
11 - 12	38.78	39.2	*	*	*	*	39.77	39.06	39.77	42.75
12 - 13	39.39	37.94	*	*	*	*	38.63	38.46	38.63	43.6
13 - 14	38.12	39	*	*	*	*	38.38	38.39	38.38	41.75
14 - 15	38.11	*	*	*	*	*	39.27	38.11	39.27	42
15 - 16	38.39	*	*	*	*	*	38.88	38.39	38.88	43.2
16 - 17	37.53	*	*	*	*	*	39.96	37.53	39.96	42
17 - 18	39.32	*	*	*	*	*	39.62	39.32	39.62	43.8
18 - 19	38.2	*	*	*	*	*	38.71	38.2	38.71	42.8
19 - 20	39.64	*	*	*	*	*	37.75	39.64	37.75	44
20 - 21	39.88	*	*	*	*	*	38.84	39.88	38.84	44.1
21 - 22	38.93	*	*	*	*	*	37.62	38.93	37.62	44.4
22 - 23	38.12	*	*	*	*	*	38.53	38.12	38.53	42.7
23 - 24	38.15	*	*	*	*	*	37.51	38.15	37.51	42.5
Totals	38.5	38.5	0	0	0	0	38.6			
% of Total	33.3%	33.3%	0%	0%	0%	0%	33.39%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	44	41.3	42.3	44	41.8	44
1 - 2	*	*	*	*	43	44	40	43	42	43
2 - 3	*	*	*	*	46.5	44	39.5	46.5	41.75	46.5
3 - 4	*	*	*	*	40	36	41	40	38.5	40
4 - 5	*	*	*	*	31	29	37	31	33	31
5 - 6	*	*	*	*	52	42	48.5	52	45.25	52
6 - 7	*	*	*	*	38	37	43	38	40	38
7 - 8	*	*	*	*	39.7	43	41	39.7	42	39.7
8 - 9	*	*	*	*	44	51	53	44	52	44
9 - 10	*	*	*	*	43.5	43	42.5	43.5	42.75	43.5
10 - 11	*	*	*	*	45	41	44	45	42.5	45
11 - 12	*	*	*	*	44.5	43	43.5	44.5	43.25	44.5
12 - 13	*	*	*	*	43.6	42.7	41.8	43.6	42.25	43.6
13 - 14	*	*	*	43	42.4	43.7	43.2	42.7	43.45	42.7
14 - 15	*	*	*	41.9	42	43	44	41.95	43.5	41.95
15 - 16	*	*	*	42.8	43.8	42.8	43.8	43.3	43.3	43.3
16 - 17	*	*	*	42.7	42.7	42.5	43.2	42.7	42.85	42.7
17 - 18	*	*	*	42.7	42.5	43	43	42.6	43	42.6
18 - 19	*	*	*	40.9	41.6	43.5	43	41.25	43.25	41.25
19 - 20	*	*	*	42.7	42.5	42.3	41.6	42.6	41.95	42.6
20 - 21	*	*	*	44.7	44.1	43.3	43.3	44.4	43.3	44.4
21 - 22	*	*	*	43.3	41.3	43	42.6	42.3	42.8	42.3
22 - 23	*	*	*	43.5	43.3	41.2	43.7	43.4	42.45	43.4
23 - 24	*	*	*	41	46.2	43.7	42.3	43.6	43	43.6
Totals	0	0	0	469.2	1027.2	1009	1030.8			
% of Total	0%	0%	0%	13.27%	29.05%	28.53%	29.15%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	42	42	*	*	*	*	*	42	0	42
1 - 2	45	39	*	*	*	*	*	42	0	42
2 - 3	47.5	55	*	*	*	*	*	51.25	0	51.25
3 - 4	45	46	*	*	*	*	*	45.5	0	45.5
4 - 5	40.5	37	*	*	*	*	*	38.75	0	38.75
5 - 6	38	39	*	*	*	*	*	38.5	0	38.5
6 - 7	32	39	*	*	*	*	*	35.5	0	35.5
7 - 8	41	32	*	*	*	*	*	36.5	0	36.5
8 - 9	42	44.3	*	*	*	*	*	43.15	0	43.15
9 - 10	37	42.5	*	*	*	*	*	39.75	0	39.75
10 - 11	38	44.2	*	*	*	*	*	41.1	0	41.1
11 - 12	42	43.5	*	*	*	*	*	42.75	0	42.75
12 - 13	44	43.2	*	*	*	*	*	43.6	0	43.6
13 - 14	41.5	42	*	*	*	*	*	41.75	0	41.75
14 - 15	42	*	*	*	*	*	*	42	0	42
15 - 16	43.2	*	*	*	*	*	*	43.2	0	43.2
16 - 17	42	*	*	*	*	*	*	42	0	42
17 - 18	43.8	*	*	*	*	*	*	43.8	0	43.8
18 - 19	42.8	*	*	*	*	*	*	42.8	0	42.8
19 - 20	44	*	*	*	*	*	*	44	0	44
20 - 21	44.1	*	*	*	*	*	*	44.1	0	44.1
21 - 22	44.4	*	*	*	*	*	*	44.4	0	44.4
22 - 23	42.7	*	*	*	*	*	*	42.7	0	42.7
23 - 24	42.5	*	*	*	*	*	*	42.5	0	42.5
Totals	1007	588.7	0	0	0	0	0			
% of Total	63.11%	36.89%	0%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	44	41.3	*	44	41.3	44
1 - 2	*	*	*	*	43	44	*	43	44	43
2 - 3	*	*	*	*	46.5	44	*	46.5	44	46.5
3 - 4	*	*	*	*	40	36	*	40	36	40
4 - 5	*	*	*	*	31	29	*	31	29	31
5 - 6	*	*	*	*	52	42	*	52	42	52
6 - 7	*	*	*	*	38	37	*	38	37	38
7 - 8	*	*	*	*	39.7	43	*	39.7	43	39.7
8 - 9	*	*	*	*	44	51	*	44	51	44
9 - 10	*	*	*	*	43.5	43	*	43.5	43	43.5
10 - 11	*	*	*	*	45	41	*	45	41	45
11 - 12	*	*	*	*	44.5	43	*	44.5	43	44.5
12 - 13	*	*	*	*	43.6	42.7	*	43.6	42.7	43.6
13 - 14	*	*	*	43	42.4	43.7	*	42.7	43.7	42.7
14 - 15	*	*	*	41.9	42	43	*	41.95	43	41.95
15 - 16	*	*	*	42.8	43.8	42.8	*	43.3	42.8	43.3
16 - 17	*	*	*	42.7	42.7	42.5	*	42.7	42.5	42.7
17 - 18	*	*	*	42.7	42.5	43	*	42.6	43	42.6
18 - 19	*	*	*	40.9	41.6	43.5	*	41.25	43.5	41.25
19 - 20	*	*	*	42.7	42.5	42.3	*	42.6	42.3	42.6
20 - 21	*	*	*	44.7	44.1	43.3	*	44.4	43.3	44.4
21 - 22	*	*	*	43.3	41.3	43	*	42.3	43	42.3
22 - 23	*	*	*	43.5	43.3	41.2	*	43.4	41.2	43.4
23 - 24	*	*	*	41	46.2	43.7	*	43.6	43.7	43.6

Incoming Monthly EightyFifthSpeeds

Birchwood Loop northwest norh of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	42	42	*	*	*	*	42.3	42	42.3	42
1 - 2	45	39	*	*	*	*	40	42	40	42
2 - 3	47.5	55	*	*	*	*	39.5	51.25	39.5	51.25
3 - 4	45	46	*	*	*	*	41	45.5	41	45.5
4 - 5	40.5	37	*	*	*	*	37	38.75	37	38.75
5 - 6	38	39	*	*	*	*	48.5	38.5	48.5	38.5
6 - 7	32	39	*	*	*	*	43	35.5	43	35.5
7 - 8	41	32	*	*	*	*	41	36.5	41	36.5
8 - 9	42	44.3	*	*	*	*	53	43.15	53	43.15
9 - 10	37	42.5	*	*	*	*	42.5	39.75	42.5	39.75
10 - 11	38	44.2	*	*	*	*	44	41.1	44	41.1
11 - 12	42	43.5	*	*	*	*	43.5	42.75	43.5	42.75
12 - 13	44	43.2	*	*	*	*	41.8	43.6	41.8	43.6
13 - 14	41.5	42	*	*	*	*	43.2	41.75	43.2	41.75
14 - 15	42	*	*	*	*	*	44	42	44	42
15 - 16	43.2	*	*	*	*	*	43.8	43.2	43.8	43.2
16 - 17	42	*	*	*	*	*	43.2	42	43.2	42
17 - 18	43.8	*	*	*	*	*	43	43.8	43	43.8
18 - 19	42.8	*	*	*	*	*	43	42.8	43	42.8
19 - 20	44	*	*	*	*	*	41.6	44	41.6	44
20 - 21	44.1	*	*	*	*	*	43.3	44.1	43.3	44.1
21 - 22	44.4	*	*	*	*	*	42.6	44.4	42.6	44.4
22 - 23	42.7	*	*	*	*	*	43.7	42.7	43.7	42.7
23 - 24	42.5	*	*	*	*	*	42.3	42.5	42.3	42.5

Summary of Violators

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM
time error -3hrs

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	197	38.3	11	49.0
01:00:00	107	38.3	8	47.5
02:00:00	33	38.6	6	48.7
03:00:00	16	39.3	1	46.0
04:00:00	19	32.4	0	0.0
05:00:00	17	39.7	5	49.4
06:00:00	10	36.6	1	47.0
07:00:00	16	35.4	0	0.0
08:00:00	50	40.1	5	49.6
09:00:00	135	37.2	10	47.7
10:00:00	238	39.5	27	49.2
11:00:00	319	39.0	29	47.6
12:00:00	372	38.5	25	48.7
13:00:00	368	38.5	16	47.4
14:00:00	428	38.0	29	48.0
15:00:00	439	38.3	30	49.0
16:00:00	431	38.5	17	47.9
17:00:00	476	38.4	31	48.5
18:00:00	518	38.2	37	48.9
19:00:00	475	38.8	32	48.6
20:00:00	498	39.4	44	47.8
21:00:00	387	38.4	32	47.9
22:00:00	291	38.6	20	48.4
23:00:00	256	38.7	20	47.2

Incoming Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	13:00	0	1	0	0	2	12	11	2	0	0	0	0	0	0	0	0	0	0	0	28	39.1	43	36 to 46	89.3	1	3.6	1	24	3
8/29/2024	14:00	2	0	1	3	15	42	24	5	1	0	0	0	0	0	0	0	0	0	93	37.2	41.9	34 to 44	76.3	4	4.3	1	80	12	
8/29/2024	15:00	0	0	3	7	22	34	35	7	0	0	0	0	0	0	0	0	0	1	109	37.8	42.8	35 to 45	67.9	3	2.8	0	97	12	
8/29/2024	16:00	0	0	0	1	17	42	33	8	1	0	0	0	0	0	0	0	0	0	102	38.7	42.7	33 to 43	81.4	4	3.9	0	90	12	
8/29/2024	17:00	0	2	0	6	16	39	32	7	0	0	0	0	0	0	0	0	0	0	102	37.5	42.7	34 to 44	74.5	5	4.9	0	94	8	
8/29/2024	18:00	0	1	1	2	16	47	24	8	0	0	0	0	0	0	0	0	0	0	99	37.7	40.9	31 to 41	80.8	5	5.1	0	89	10	
8/29/2024	19:00	0	0	0	2	7	47	42	8	3	0	0	0	0	0	0	0	0	0	109	39.5	42.7	34 to 44	84.4	8	7.3	0	97	12	
8/29/2024	20:00	0	1	1	0	13	29	40	16	3	0	0	0	0	0	0	0	0	0	103	40	44.7	35 to 45	72.8	13	12.6	1	97	5	
8/29/2024	21:00	0	0	0	1	8	34	37	5	4	0	0	0	0	0	0	0	0	0	89	39.9	43.3	34 to 44	82.0	8	9.0	0	82	7	
8/29/2024	22:00	0	0	0	3	8	31	19	8	0	0	0	0	0	0	0	0	0	0	69	38.8	43.5	35 to 45	76.8	5	7.2	0	68	1	
8/29/2024	23:00	0	0	0	2	4	38	20	4	0	0	0	0	0	0	0	0	0	0	68	38.4	41	34 to 44	86.8	3	4.4	0	66	2	
24 Hr Summary		2	5	6	27	128	395	317	78	12	0	0	0	0	0	0	0	0	1	971	38.6	43	34 to 44	77.1	59	6.1	3	884	84	

Incoming Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/30/2024	00:00	0	0	1	1	8	12	17	6	0	1	0	0	0	0	0	0	0	0	46	39.2	44	34 to 44	69.6	6	13.0	0	46	0
8/30/2024	01:00	0	0	0	1	0	12	6	1	0	0	0	0	0	0	0	0	0	0	20	38.5	43	34 to 44	90.0	1	5.0	0	20	0
8/30/2024	02:00	0	0	0	2	0	3	2	2	0	0	0	0	0	0	0	0	0	0	9	38.3	46.5	37 to 47	66.7	2	22.2	0	9	0
8/30/2024	03:00	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4	40	40	32 to 42	100.0	*	*	0	4	0
8/30/2024	04:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	31	31	21 to 31	100.0	*	*	0	1	0
8/30/2024	05:00	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	2	51	52	42 to 52	100.0	2	100.0	0	2	0
8/30/2024	06:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	38	38	28 to 38	100.0	*	*	0	1	0
8/30/2024	07:00	0	0	0	0	0	2	3	0	0	0	0	0	0	0	0	0	0	0	5	39.2	39.7	30 to 40	100.0	*	*	0	5	0
8/30/2024	08:00	0	0	0	0	1	4	7	2	0	0	0	0	0	0	0	0	0	0	14	40.7	44	35 to 45	85.7	1	7.1	0	14	0
8/30/2024	09:00	0	0	0	1	7	16	13	4	1	0	0	0	0	0	0	0	0	0	42	39	43.5	32 to 42	78.6	4	9.5	0	41	1
8/30/2024	10:00	0	0	0	0	11	28	26	13	5	1	0	0	0	0	0	0	0	0	84	40.4	45	35 to 45	71.4	13	15.5	0	83	1
8/30/2024	11:00	0	0	1	2	14	28	25	13	0	0	0	0	0	0	0	0	0	0	83	38.7	44.5	34 to 44	68.7	11	13.3	0	76	7
8/30/2024	12:00	1	0	4	6	7	47	24	9	3	0	1	0	0	0	0	0	0	0	102	38.1	43.6	35 to 45	74.5	8	7.8	0	89	13
8/30/2024	13:00	0	0	2	3	16	31	33	5	1	0	0	0	0	0	0	0	0	0	91	37.9	42.4	33 to 43	78.0	2	2.2	1	81	9
8/30/2024	14:00	0	0	0	2	20	42	30	5	1	0	0	0	0	0	0	0	0	0	100	38.1	42	32 to 42	81.0	4	4.0	0	90	10
8/30/2024	15:00	2	0	1	2	11	39	29	11	1	0	0	0	0	0	0	0	0	0	96	38.6	43.8	34 to 44	76.0	10	10.4	4	83	9
8/30/2024	16:00	0	0	2	4	15	38	32	6	0	0	0	0	0	0	0	0	0	0	97	37.9	42.7	34 to 44	78.4	5	5.2	0	86	11
8/30/2024	17:00	0	4	3	6	13	53	33	5	2	0	0	0	0	0	0	0	0	0	119	37.1	42.5	34 to 44	76.5	6	5.0	1	109	9
8/30/2024	18:00	1	0	1	4	19	76	30	6	2	0	0	0	0	0	0	0	0	0	139	37.5	41.6	33 to 43	78.4	7	5.0	0	125	14
8/30/2024	19:00	0	0	2	1	13	56	35	9	1	1	0	0	0	0	0	0	0	0	118	38.7	42.5	34 to 44	79.7	8	6.8	1	107	10
8/30/2024	20:00	0	0	1	3	19	59	40	20	2	1	0	0	0	0	0	0	0	0	145	39.1	44.1	34 to 44	73.8	15	10.3	1	132	12
8/30/2024	21:00	1	0	1	2	15	46	17	4	1	0	0	0	0	0	0	0	0	0	87	37	41.3	32 to 42	83.9	5	5.7	0	83	4
8/30/2024	22:00	0	0	0	1	6	22	21	4	2	0	0	0	0	0	0	0	0	0	56	39.5	43.3	34 to 44	80.4	5	8.9	0	56	0
8/30/2024	23:00	0	0	0	0	5	14	19	7	1	0	0	0	0	0	0	0	0	0	46	40.6	46.2	34 to 44	78.3	8	17.4	0	44	2
24 Hr Summary		5	4	19	41	201	631	444	132	25	4	1	0	0	0	0	0	0	0	1507	38.5	44	34 to 44	76.0	123	8.2	8	1387	112

Incoming Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/31/2024	00:00	0	0	0	1	9	25	13	2	0	0	0	0	0	0	0	0	0	0	50	37.6	41.3	32 to 42	82.0	*	*	0	50	0
8/31/2024	01:00	0	0	0	0	5	8	6	3	0	0	0	0	0	0	0	0	0	0	22	39	44	35 to 45	77.3	*	*	0	22	0
8/31/2024	02:00	0	0	0	0	0	3	5	1	0	0	0	0	0	0	0	0	0	0	9	40.7	44	34 to 44	88.9	1	11.1	0	8	1
8/31/2024	03:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	36	36	26 to 36	100.0	*	*	0	1	0
8/31/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	29	29	19 to 29	100.0	*	*	0	1	0
8/31/2024	05:00	0	0	0	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	3	36.7	42	32 to 42	100.0	*	*	0	3	0
8/31/2024	06:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	37	37	27 to 37	100.0	*	*	0	1	0
8/31/2024	07:00	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	42	43	33 to 43	100.0	*	*	0	2	0
8/31/2024	08:00	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	2	43	51	25 to 35	50.0	1	50.0	0	1	1
8/31/2024	09:00	0	0	0	0	3	5	5	1	1	0	0	0	0	0	0	0	0	0	15	38.9	43	33 to 43	80.0	2	13.3	0	14	1
8/31/2024	10:00	0	1	0	2	3	9	8	0	1	0	0	0	0	0	0	0	0	0	24	36.8	41	33 to 43	70.8	1	4.2	0	24	0
8/31/2024	11:00	0	0	0	1	8	26	11	5	0	0	0	0	0	0	0	0	0	0	51	38.3	43	33 to 43	78.4	3	5.9	0	49	2
8/31/2024	12:00	0	0	0	1	6	20	20	2	2	0	0	0	0	0	0	0	0	0	51	39.1	42.7	32 to 42	78.4	4	7.8	0	49	2
8/31/2024	13:00	0	0	0	0	10	23	27	8	1	0	0	0	0	0	0	0	0	0	69	39.4	43.7	35 to 45	81.2	3	4.3	0	67	2
8/31/2024	14:00	0	2	1	2	18	36	30	12	0	0	0	0	0	0	0	0	0	0	101	37.9	43	33 to 43	70.3	10	9.9	0	97	4
8/31/2024	15:00	0	0	0	3	11	44	23	7	0	0	0	0	0	0	0	0	0	0	88	38.2	42.8	34 to 44	80.7	4	4.5	0	85	3
8/31/2024	16:00	0	0	0	1	17	25	34	4	0	0	0	0	0	0	0	0	0	0	81	38.6	42.5	32 to 42	81.5	2	2.5	0	79	2
8/31/2024	17:00	0	0	1	1	9	34	32	6	2	0	0	0	0	0	0	0	0	0	85	39.3	43	34 to 44	83.5	4	4.7	0	82	3
8/31/2024	18:00	0	0	1	1	13	23	32	8	0	0	2	0	0	0	0	0	0	0	80	39.5	43.5	32 to 42	73.8	9	11.3	0	78	2
8/31/2024	19:00	0	0	0	2	11	40	27	6	1	0	0	0	0	0	0	0	0	0	87	38.4	42.3	34 to 44	81.6	5	5.7	0	84	3
8/31/2024	20:00	0	0	0	0	10	29	25	7	0	0	0	0	0	0	0	0	0	0	71	39.1	43.3	34 to 44	83.1	3	4.2	1	67	3
8/31/2024	21:00	0	0	2	2	5	29	17	7	1	0	0	0	0	0	0	0	0	0	63	38.4	43	33 to 43	77.8	5	7.9	0	61	2
8/31/2024	22:00	0	0	0	0	10	24	16	2	0	0	0	0	0	0	0	0	0	0	52	37.9	41.2	32 to 42	86.5	1	1.9	0	52	0
8/31/2024	23:00	0	0	0	2	8	16	18	7	0	0	0	0	0	0	0	0	0	0	51	38.8	43.7	33 to 43	72.5	6	11.8	0	49	2
24 Hr Summary		0	3	5	20	158	422	352	88	10	0	2	0	0	0	0	0	0	0	1060	38.6	43	34 to 44	77.9	64	6.0	1	1026	33

Incoming Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/1/2024	00:00	0	0	0	0	7	11	10	3	0	0	0	0	0	0	0	0	0	0	31	38.6	42.3	33 to 43	83.9	1	3.2	0	31	0
9/1/2024	01:00	0	0	0	3	6	15	9	3	1	0	0	0	0	0	0	0	0	0	37	37.6	40	30 to 40	75.7	4	10.8	0	35	2
9/1/2024	02:00	1	0	0	2	0	1	2	0	0	0	0	0	0	0	0	0	0	0	6	30.3	39.5	25 to 35	50.0	*	*	0	6	0
9/1/2024	03:00	0	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5	38.6	41	33 to 43	100.0	*	*	0	5	0
9/1/2024	04:00	0	0	0	2	2	2	1	0	0	0	0	0	0	0	0	0	0	0	7	33.3	37	27 to 37	85.7	*	*	0	7	0
9/1/2024	05:00	0	0	0	0	1	1	0	2	0	0	0	0	0	0	0	0	0	0	4	42	48.5	26 to 36	50.0	2	50.0	0	4	0
9/1/2024	06:00	0	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	4	41.3	43	33 to 43	75.0	1	25.0	0	4	0
9/1/2024	07:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	38	41	31 to 41	100.0	*	*	0	2	0
9/1/2024	08:00	0	0	0	0	1	1	0	0	1	0	0	0	0	0	0	0	0	0	3	41	53	26 to 36	66.7	1	33.3	0	3	0
9/1/2024	09:00	0	1	0	2	9	2	3	1	0	0	0	0	0	0	0	0	0	0	18	34.1	42.5	24 to 34	61.1	1	5.6	0	18	0
9/1/2024	10:00	0	0	0	0	3	4	6	1	1	0	0	0	0	0	0	0	0	0	15	39.6	44	34 to 44	66.7	2	13.3	0	14	1
9/1/2024	11:00	0	0	0	1	7	14	32	6	0	0	0	0	0	0	0	0	0	0	60	39.8	43.5	36 to 46	81.7	4	6.7	0	59	1
9/1/2024	12:00	0	0	0	1	7	26	26	2	1	0	0	0	0	0	0	0	0	0	63	38.6	41.8	34 to 44	85.7	2	3.2	0	63	0
9/1/2024	13:00	0	0	1	1	10	26	20	5	0	0	0	0	0	0	0	0	0	0	63	38.4	43.2	34 to 44	77.8	4	6.3	0	62	1
9/1/2024	14:00	0	0	1	0	7	26	17	9	0	0	0	0	0	0	0	0	0	0	60	39.3	44	34 to 44	75.0	9	15.0	0	59	1
9/1/2024	15:00	1	0	0	2	12	24	27	8	2	0	0	0	0	0	0	0	0	0	76	38.9	43.8	33 to 43	72.4	9	11.8	0	74	2
9/1/2024	16:00	1	0	0	1	2	25	36	5	2	0	0	0	0	0	0	0	0	0	72	40	43.2	35 to 45	88.9	4	5.6	0	70	2
9/1/2024	17:00	0	0	0	0	8	41	31	8	2	1	0	0	0	0	0	0	0	0	91	39.6	43	34 to 44	81.3	9	9.9	0	88	3
9/1/2024	18:00	0	0	0	5	16	40	35	10	2	0	0	0	0	0	0	0	0	0	108	38.7	43	33 to 43	73.1	8	7.4	0	101	7
9/1/2024	19:00	0	0	0	5	18	33	29	5	1	1	0	0	0	0	0	0	0	0	92	37.8	41.6	32 to 42	77.2	6	6.5	0	92	0
9/1/2024	20:00	1	0	0	3	9	34	37	9	0	0	0	0	0	0	0	0	0	0	93	38.8	43.3	35 to 45	79.6	6	6.5	0	88	5
9/1/2024	21:00	0	1	1	2	11	25	15	5	1	0	0	0	0	0	0	0	0	0	61	37.6	42.6	33 to 43	73.8	4	6.6	0	59	2
9/1/2024	22:00	0	0	0	1	9	29	11	6	2	0	0	0	0	0	0	0	0	0	58	38.5	43.7	34 to 44	75.9	4	6.9	0	57	1
9/1/2024	23:00	1	0	1	1	4	13	14	3	0	0	0	0	0	0	0	0	0	0	37	37.5	42.3	34 to 44	75.7	2	5.4	0	36	1
24 Hr Summary		5	2	4	32	150	397	366	92	16	2	0	0	0	0	0	0	0	0	1066	38.6	44	34 to 44	75.2	83	7.8	0	1037	29

Incoming Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/2/2024	00:00	0	0	0	1	8	15	10	3	1	0	0	0	0	0	0	0	0	0	38	38	42	31 to 41	78.9	3	7.9	0	37	1
9/2/2024	01:00	0	0	0	0	0	8	4	5	0	0	0	0	0	0	0	0	0	0	17	40.9	45	36 to 46	88.2	3	17.6	0	17	0
9/2/2024	02:00	0	0	0	0	0	3	1	2	0	0	0	0	0	0	0	0	0	0	6	41.3	47.5	31 to 41	66.7	2	33.3	0	6	0
9/2/2024	03:00	0	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	3	41.7	45	35 to 45	100.0	*	*	0	3	0
9/2/2024	04:00	0	0	2	1	1	1	2	0	0	0	0	0	0	0	0	0	0	0	7	31.7	40.5	31 to 41	57.1	*	*	0	7	0
9/2/2024	05:00	0	0	0	0	3	2	0	1	0	0	0	0	0	0	0	0	0	0	6	36.3	38	28 to 38	83.3	1	16.7	0	5	1
9/2/2024	06:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	32	32	22 to 32	100.0	*	*	0	1	0
9/2/2024	07:00	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	2	31	41	11 to 21	50.0	*	*	0	2	0
9/2/2024	08:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	38	42	32 to 42	100.0	*	*	0	2	0
9/2/2024	09:00	0	0	0	2	2	2	0	1	0	0	0	0	0	0	0	0	0	0	7	34.7	37	27 to 37	85.7	1	14.3	0	7	0
9/2/2024	10:00	0	0	0	0	5	6	2	0	0	0	0	0	0	0	0	0	0	0	13	34.6	38	30 to 40	100.0	*	*	0	11	2
9/2/2024	11:00	0	0	1	0	5	17	12	3	2	0	0	0	0	0	0	0	0	0	40	38.8	42	32 to 42	77.5	3	7.5	0	38	2
9/2/2024	12:00	0	0	0	0	7	22	19	8	0	0	0	0	0	0	0	0	0	0	56	39.4	44	35 to 45	78.6	5	8.9	0	55	1
9/2/2024	13:00	0	0	0	0	16	38	23	4	0	0	0	0	0	0	0	0	0	0	81	38.1	41.5	31 to 41	84.0	4	4.9	0	75	6
9/2/2024	14:00	0	1	0	2	8	36	24	2	0	0	1	0	0	0	0	0	0	0	74	38.1	42	34 to 44	82.4	2	2.7	0	70	4
9/2/2024	15:00	0	0	0	3	9	31	22	5	0	0	0	0	0	0	0	0	0	0	70	38.4	43.2	34 to 44	78.6	4	5.7	0	67	3
9/2/2024	16:00	1	0	0	5	17	26	25	5	0	0	0	0	0	0	0	0	0	0	79	37.5	42	33 to 43	77.2	2	2.5	2	71	6
9/2/2024	17:00	0	0	0	0	9	35	24	8	2	1	0	0	0	0	0	0	0	0	79	39.3	43.8	35 to 45	79.7	7	8.9	0	76	3
9/2/2024	18:00	0	1	1	2	18	33	28	6	2	1	0	0	0	0	0	0	0	0	92	38.2	42.8	34 to 44	75.0	8	8.7	0	90	2
9/2/2024	19:00	1	0	0	0	7	29	22	9	1	0	0	0	0	0	0	0	0	0	69	39.6	44	36 to 46	84.1	5	7.2	0	68	1
9/2/2024	20:00	0	0	1	1	8	29	33	11	3	0	0	0	0	0	0	0	0	0	86	39.9	44.1	35 to 45	80.2	7	8.1	0	83	3
9/2/2024	21:00	0	0	0	2	14	33	23	14	1	0	0	0	0	0	0	0	0	0	87	38.9	44.4	32 to 42	72.4	10	11.5	0	86	1
9/2/2024	22:00	0	0	0	1	14	20	15	5	0	1	0	0	0	0	0	0	0	0	56	38.1	42.7	32 to 42	75.0	5	8.9	0	54	2
9/2/2024	23:00	0	0	0	1	10	22	17	4	0	0	0	0	0	0	0	0	0	0	54	38.1	42.5	35 to 45	77.8	1	1.9	0	52	2
24 Hr Summary		2	2	6	21	163	409	309	97	12	3	1	0	0	0	0	0	0	0	1025	38.5	44	34 to 44	74.8	73	7.1	2	983	40

Incoming Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

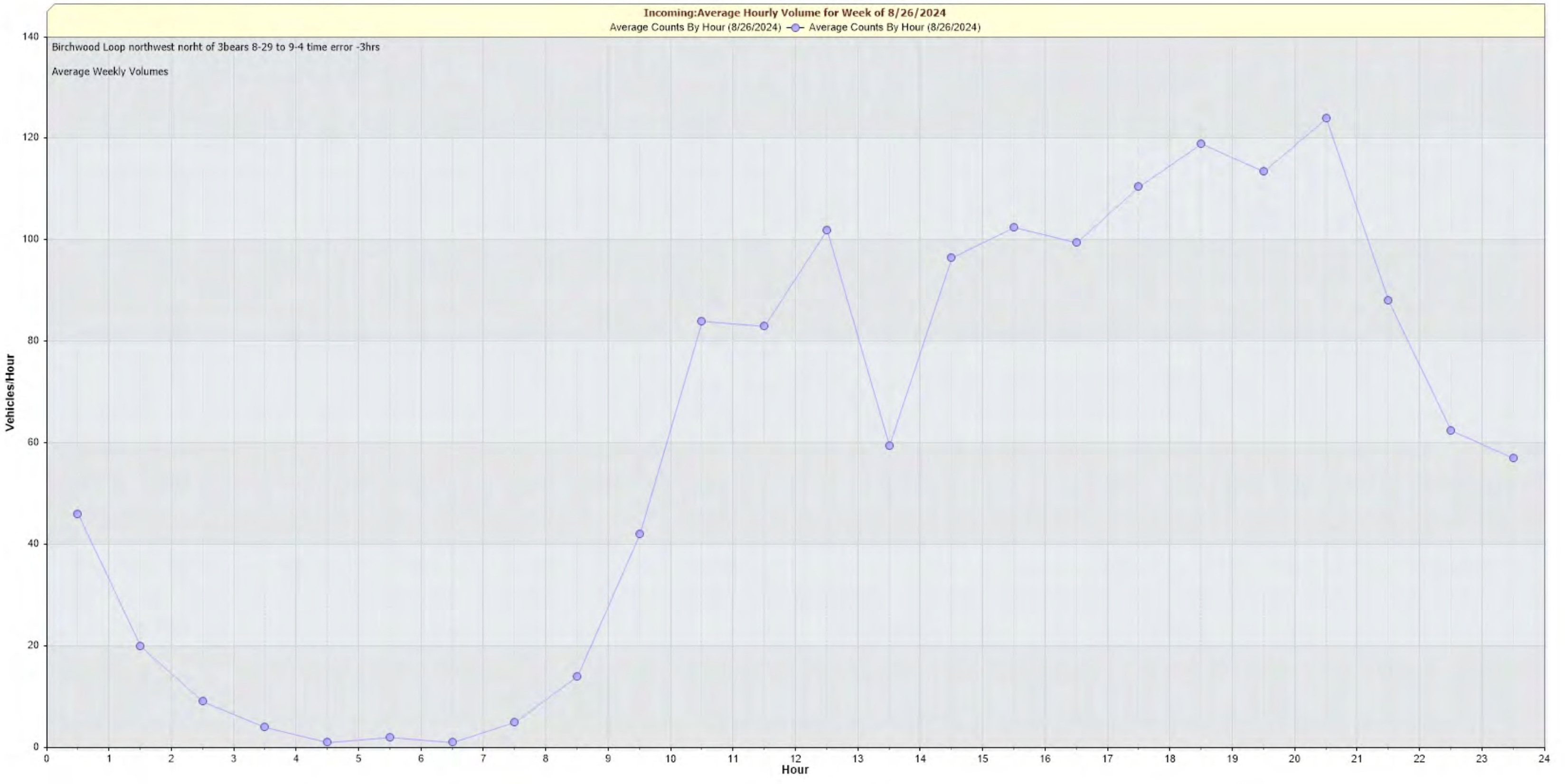
Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/3/2024	00:00	1	0	0	2	2	12	12	3	0	0	0	0	0	0	0	0	0	0	32	37.9	42	35 to 45	81.3	1	3.1	0	31	1	
9/3/2024	01:00	0	0	0	3	2	4	2	0	0	0	0	0	0	0	0	0	0	0	11	34.9	39	29 to 39	72.7	*	*	0	10	1	
9/3/2024	02:00	0	0	0	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	3	44	55	29 to 39	66.7	1	33.3	0	3	0	
9/3/2024	03:00	0	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	3	38.3	46	27 to 37	66.7	1	33.3	0	3	0	
9/3/2024	04:00	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	3	33.3	37	27 to 37	100.0	*	*	0	3	0	
9/3/2024	05:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	38.5	39	29 to 39	100.0	*	*	0	2	0	
9/3/2024	06:00	0	0	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	3	31.3	39	29 to 39	66.7	*	*	0	3	0	
9/3/2024	07:00	0	0	2	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	5	29.6	32	22 to 32	80.0	*	*	0	5	0	
9/3/2024	08:00	0	0	0	0	4	10	10	5	0	0	0	0	0	0	0	0	0	0	29	39.6	44.3	35 to 45	79.3	2	6.9	0	29	0	
9/3/2024	09:00	2	0	0	0	17	16	14	4	0	0	0	0	0	0	0	0	0	0	53	36.6	42.5	31 to 41	67.9	2	3.8	0	50	3	
9/3/2024	10:00	0	0	0	0	12	33	41	13	3	0	0	0	0	0	0	0	0	0	102	40	44.2	35 to 45	77.5	11	10.8	0	99	3	
9/3/2024	11:00	0	1	0	3	9	28	33	8	3	0	0	0	0	0	0	0	0	0	85	39.2	43.5	34 to 44	75.3	8	9.4	0	81	4	
9/3/2024	12:00	0	1	1	6	14	40	28	10	0	0	0	0	0	0	0	0	0	0	100	37.9	43.2	34 to 44	73.0	6	6.0	0	90	10	
9/3/2024	13:00	0	0	0	0	5	15	13	3	0	0	0	0	0	0	0	0	0	0	36	39	42	32 to 42	86.1	2	5.6	0	32	4	
9/3/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		3	2	4	14	70	166	154	47	6	1	0	0	0	0	0	0	0	0	467	38.5	44	35 to 45	72.8	34	7.3	0	441	26	

Incoming: Average Hourly Volume for Week of 8/26/2024

Average Counts By Hour (8/26/2024) ● Average Counts By Hour (8/26/2024)

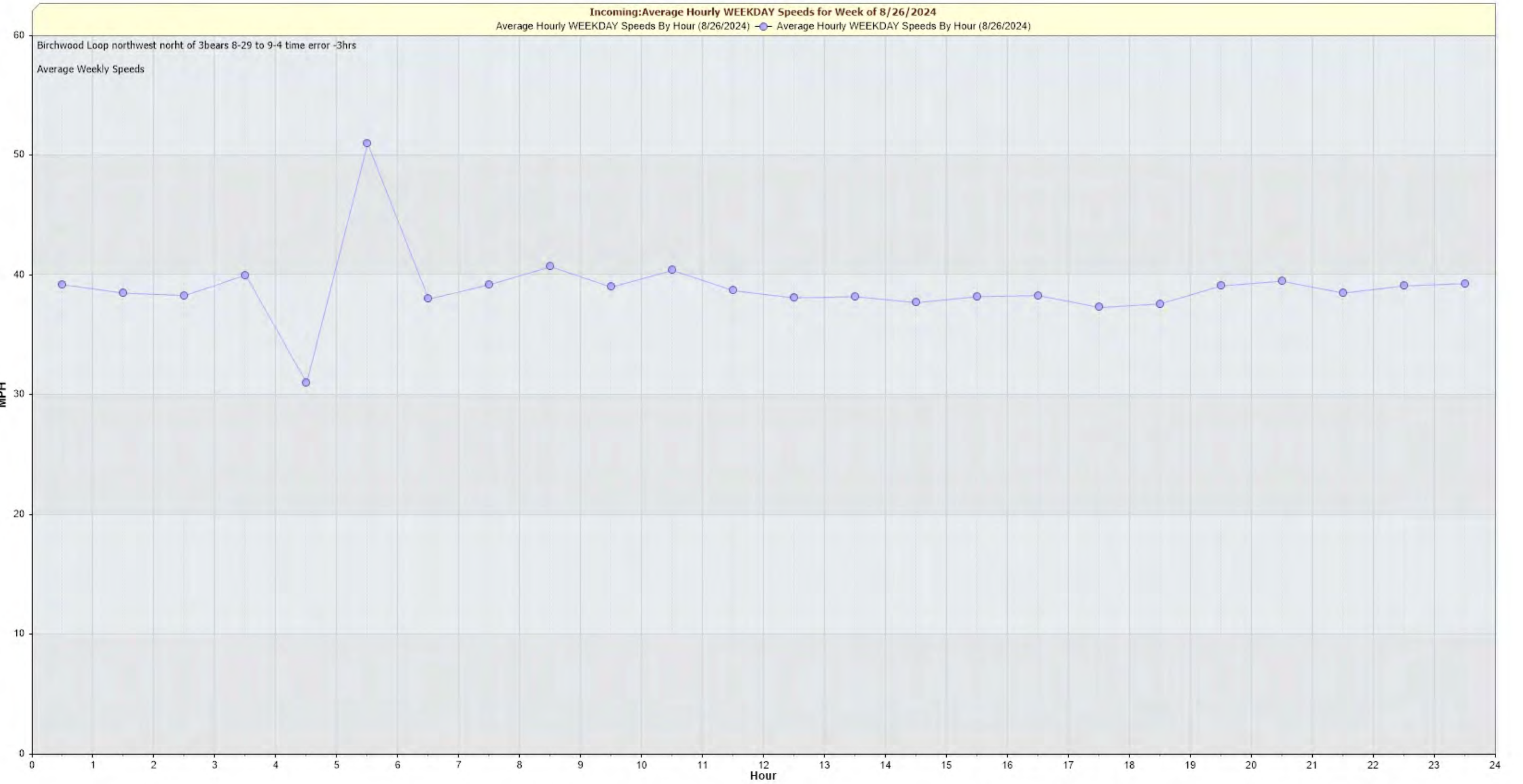
Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

Average Weekly Volumes



Incoming:Average Hourly WEEKDAY Speeds for Week of 8/26/2024
Average Hourly WEEKDAY Speeds By Hour (8/26/2024) — Average Hourly WEEKDAY Speeds By Hour (8/26/2024)

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs
Average Weekly Speeds

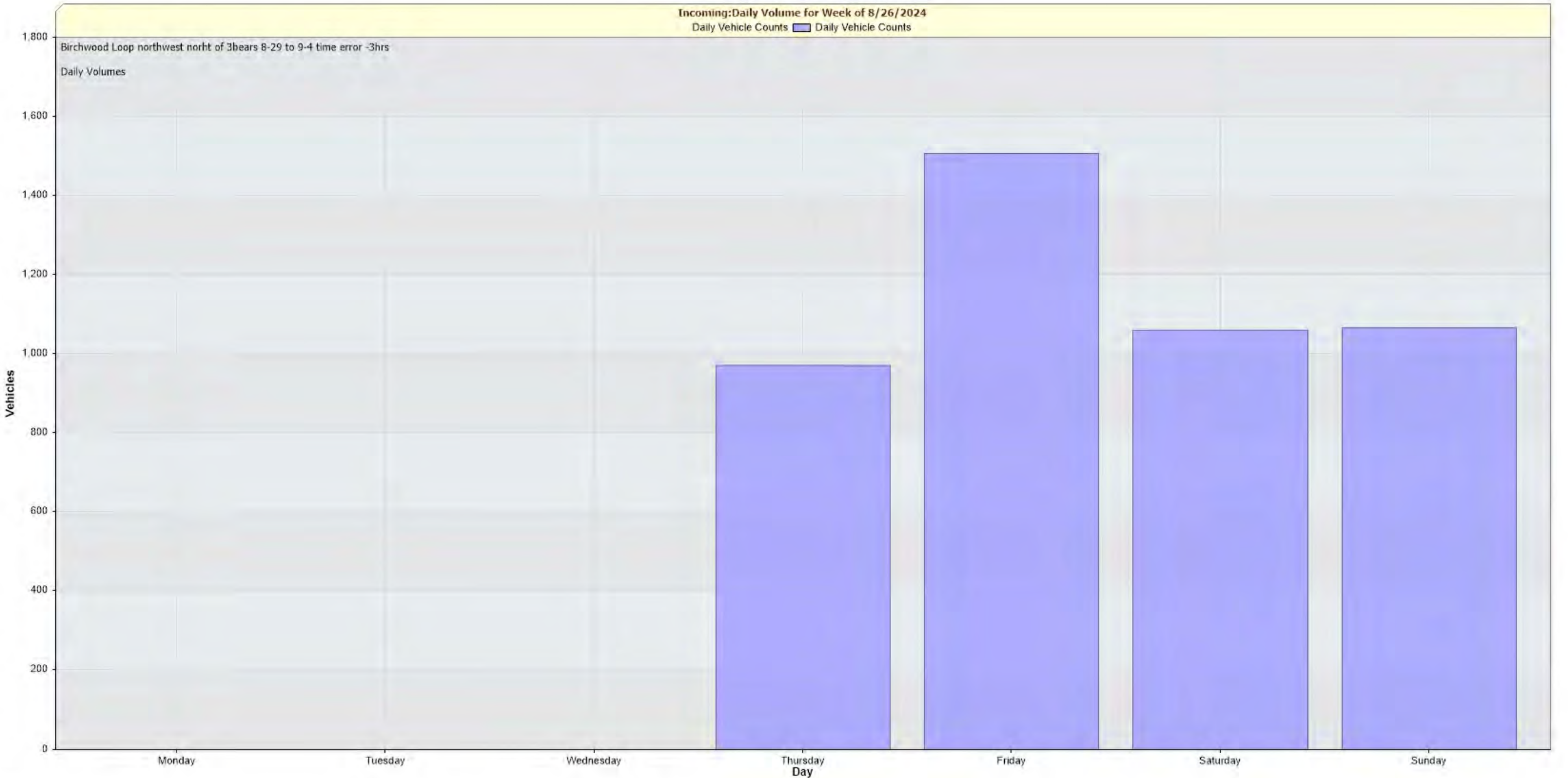


Incoming: Daily Volume for Week of 8/26/2024

Daily Vehicle Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

Daily Volumes



For Project: Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs
 Project Notes:
 Location/Name: Outgoing
 Report Generated: 10/3/2024 3:33:12 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 8/29/2024 1:00:00 PM through 9/3/2024 1:59:59 PM
 85th Percentile Speed: 44 MPH
 85th Percentile Vehicles: 5199
 Max Speed: 72 MPH on 8/30/2024 2:20:39 AM
 Total Vehicles: 6117
 AADT: 1213

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	993	1019
AM Peak 11:00 AM	62	50
PM Peak 7:00 PM	119	103

Speed

Speed Limit: 45
 85th Percentile Speed: 44
 50th Percentile Speed: 39
 10 MPH Pace Interval: 35.0 MPH to 45.0 MPH
 Average Speed: 39.12

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	87	26	N/A	90	146	80	98
% over limit	8.2	7.6	N/A	8.6	9.6	7.4	9.2
Avg Speeder	48.5	47.5	N/A	47.7	48.1	48.5	48.3
Avg Speed	38.9	38.5	N/A	39.2	39.4	39.0	39.1

Class Counts

	Number	%
VEH_SM	12	0.2
VEH_MED	5805	94.9
VEH_LG	300	4.9
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Outgoing Summary
Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/29/2024 2:00:00 PM	44.0	20	23	46	46.0	4.3%	39.6
8/29/2024 3:00:00 PM	41.0	74	87	48	47.0	3.4%	37.3
8/29/2024 4:00:00 PM	43.0	89	105	50	47.3	3.8%	38.2
8/29/2024 5:00:00 PM	44.0	97	114	50	46.9	11.4%	39.5
8/29/2024 6:00:00 PM	44.0	78	92	48	47.2	5.4%	39.0
8/29/2024 7:00:00 PM	44.0	84	99	50	47.4	8.1%	38.9
8/29/2024 8:00:00 PM	44.0	117	138	58	48.6	10.1%	39.6
8/29/2024 9:00:00 PM	45.0	111	131	51	47.6	13.0%	40.0
8/29/2024 10:00:00 PM	43.0	88	104	53	48.3	6.7%	39.8
8/29/2024 11:00:00 PM	45.0	68	80	53	47.9	15.0%	40.1
8/30/2024 12:00:00 AM	44.0	58	68	53	47.5	8.8%	39.2
8/30/2024 1:00:00 AM	44.0	42	49	48	46.7	6.1%	39.8
8/30/2024 2:00:00 AM	45.0	26	30	47	46.3	13.3%	39.7
8/30/2024 3:00:00 AM	45.0	11	13	72	59.5	15.4%	41.5
8/30/2024 4:00:00 AM	43.0	6	7	48	48.0	14.3%	39.3
8/30/2024 5:00:00 AM	38.0	2	2	38	0.0	0.0%	35.0
8/30/2024 6:00:00 AM	50.0	1	1	50	50.0	100.0%	50.0
8/30/2024 7:00:00 AM	39.0	1	1	39	0.0	0.0%	39.0
8/30/2024 8:00:00 AM	39.0	2	2	39	0.0	0.0%	38.0
8/30/2024 9:00:00 AM	44.0	13	15	46	46.0	6.7%	39.4
8/30/2024 10:00:00 AM	43.0	30	35	46	46.0	2.9%	38.0
8/30/2024 11:00:00 AM	46.0	34	40	68	49.1	20.0%	41.5
8/30/2024 12:00:00 PM	43.0	63	74	49	47.7	4.1%	39.0
8/30/2024 1:00:00 PM	44.0	83	98	50	48.0	9.2%	39.3
8/30/2024 2:00:00 PM	43.0	62	73	50	49.3	4.1%	38.7
8/30/2024 3:00:00 PM	43.0	76	89	51	49.4	5.6%	38.4
8/30/2024 4:00:00 PM	43.0	82	97	51	47.5	8.2%	39.1
8/30/2024 5:00:00 PM	43.0	95	112	49	47.4	6.2%	38.9
8/30/2024 6:00:00 PM	44.0	101	119	50	47.2	10.1%	39.6
8/30/2024 7:00:00 PM	45.0	124	146	51	47.8	14.4%	39.7
8/30/2024 8:00:00 PM	45.0	121	142	53	47.4	9.9%	39.5
8/30/2024 9:00:00 PM	45.0	122	143	52	48.4	12.6%	40.2
8/30/2024 10:00:00 PM	45.0	76	90	56	47.8	14.4%	39.8
8/30/2024 11:00:00 PM	42.0	59	69	46	46.0	2.9%	38.8
8/31/2024 12:00:00 AM	45.0	64	75	56	49.7	13.3%	39.8
8/31/2024 1:00:00 AM	44.0	45	53	59	48.8	11.3%	38.3
8/31/2024 2:00:00 AM	43.0	22	26	50	48.5	7.7%	38.9
8/31/2024 3:00:00 AM	42.0	14	17	49	48.5	11.8%	39.6
8/31/2024 4:00:00 AM	42.0	9	11	62	62.0	9.1%	39.1
8/31/2024 5:00:00 AM	48.0	2	2	48	48.0	50.0%	45.0
8/31/2024 6:00:00 AM	42.0	3	4	44	0.0	0.0%	41.3
8/31/2024 7:00:00 AM	38.0	2	2	38	0.0	0.0%	37.0
8/31/2024 8:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
8/31/2024 9:00:00 AM	40.0	4	5	44	0.0	0.0%	38.8
8/31/2024 10:00:00 AM	41.0	10	12	45	0.0	0.0%	38.9
8/31/2024 11:00:00 AM	44.0	19	22	53	53.0	9.1%	40.1
8/31/2024 12:00:00 PM	44.0	29	34	46	46.0	5.9%	38.2

Outgoing Summary
 Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

8/31/2024 1:00:00 PM	43.0	37	43	46	46.0	7.0%	38.1
8/31/2024 2:00:00 PM	44.0	52	61	50	48.0	8.2%	39.8
8/31/2024 3:00:00 PM	44.0	76	89	59	48.8	6.7%	39.5
8/31/2024 4:00:00 PM	44.0	71	84	51	47.8	7.1%	39.8
8/31/2024 5:00:00 PM	43.0	77	91	52	47.6	5.5%	38.1
8/31/2024 6:00:00 PM	44.0	68	80	50	47.9	10.0%	39.2
8/31/2024 7:00:00 PM	43.0	69	81	49	47.8	4.9%	37.9
8/31/2024 8:00:00 PM	42.0	72	85	52	48.0	3.5%	38.5
8/31/2024 9:00:00 PM	43.0	69	81	49	46.7	7.4%	39.2
8/31/2024 10:00:00 PM	44.0	53	62	54	47.7	9.7%	39.7
8/31/2024 11:00:00 PM	43.0	60	71	59	51.5	8.5%	39.7
9/1/2024 12:00:00 AM	44.0	54	64	52	48.8	9.4%	38.8
9/1/2024 1:00:00 AM	43.0	34	40	48	47.0	7.5%	38.6
9/1/2024 2:00:00 AM	43.0	25	29	50	48.5	6.9%	38.1
9/1/2024 3:00:00 AM	41.0	8	10	43	0.0	0.0%	36.7
9/1/2024 4:00:00 AM	44.0	8	10	53	53.0	10.0%	39.3
9/1/2024 5:00:00 AM	42.0	5	6	43	0.0	0.0%	40.2
9/1/2024 6:00:00 AM	43.0	4	5	46	46.0	20.0%	38.4
9/1/2024 7:00:00 AM	45.0	5	6	56	56.0	16.7%	43.7
9/1/2024 8:00:00 AM	42.0	3	3	42	0.0	0.0%	33.7
9/1/2024 9:00:00 AM	42.0	4	5	47	47.0	20.0%	37.4
9/1/2024 10:00:00 AM	46.0	22	26	61	53.4	19.2%	41.0
9/1/2024 11:00:00 AM	46.0	22	26	51	48.2	19.2%	40.6
9/1/2024 12:00:00 PM	44.0	27	32	47	46.3	9.4%	39.7
9/1/2024 1:00:00 PM	44.0	31	36	50	48.0	11.1%	38.3
9/1/2024 2:00:00 PM	44.0	47	55	47	46.7	5.5%	38.7
9/1/2024 3:00:00 PM	44.0	63	74	50	47.2	12.2%	39.8
9/1/2024 4:00:00 PM	45.0	83	98	55	48.8	8.1%	39.2
9/1/2024 5:00:00 PM	43.0	103	121	53	47.5	8.3%	38.7
9/1/2024 6:00:00 PM	44.0	73	86	47	46.3	4.7%	38.8
9/1/2024 7:00:00 PM	45.0	63	74	50	47.6	13.5%	39.5
9/1/2024 8:00:00 PM	43.0	62	73	56	49.1	9.6%	39.1
9/1/2024 9:00:00 PM	45.0	60	70	54	50.5	8.6%	39.5
9/1/2024 10:00:00 PM	44.0	65	76	50	49.0	3.9%	38.9
9/1/2024 11:00:00 PM	45.0	51	60	50	47.1	13.3%	39.5
9/2/2024 12:00:00 AM	45.0	37	43	49	47.5	9.3%	38.4
9/2/2024 1:00:00 AM	44.0	23	27	52	49.0	14.8%	39.4
9/2/2024 2:00:00 AM	41.0	20	24	44	0.0	0.0%	38.3
9/2/2024 3:00:00 AM	46.0	11	13	60	51.0	23.1%	41.5
9/2/2024 4:00:00 AM	45.0	7	8	48	48.0	12.5%	38.1
9/2/2024 5:00:00 AM	40.0	3	3	40	0.0	0.0%	38.0
9/2/2024 6:00:00 AM	36.0	5	6	39	0.0	0.0%	33.5
9/2/2024 7:00:00 AM	38.0	3	3	38	0.0	0.0%	36.0
9/2/2024 8:00:00 AM	37.0	2	2	37	0.0	0.0%	36.5
9/2/2024 9:00:00 AM	**No Data**						
9/2/2024 10:00:00 AM	40.0	9	11	45	0.0	0.0%	38.1
9/2/2024 11:00:00 AM	39.0	10	12	45	0.0	0.0%	35.7
9/2/2024 12:00:00 PM	42.0	33	39	45	0.0	0.0%	37.8

Outgoing Summary
Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

9/2/2024 1:00:00 PM	44.0	47	55	50	48.1	12.7%	38.6
9/2/2024 2:00:00 PM	41.0	58	68	48	47.0	2.9%	38.0
9/2/2024 3:00:00 PM	44.0	75	88	57	49.0	4.5%	39.2
9/2/2024 4:00:00 PM	44.0	58	68	58	49.8	7.4%	39.6
9/2/2024 5:00:00 PM	43.0	64	75	49	47.0	4.0%	38.2
9/2/2024 6:00:00 PM	42.0	80	94	51	47.9	7.4%	38.4
9/2/2024 7:00:00 PM	44.0	78	92	69	50.1	8.7%	39.9
9/2/2024 8:00:00 PM	44.0	66	78	58	47.8	12.8%	39.6
9/2/2024 9:00:00 PM	46.0	65	77	51	48.4	16.9%	40.8
9/2/2024 10:00:00 PM	42.0	82	96	59	49.2	6.3%	38.5
9/2/2024 11:00:00 PM	44.0	56	66	48	46.7	10.6%	38.9
9/3/2024 12:00:00 AM	45.0	52	61	56	48.9	11.5%	39.1
9/3/2024 1:00:00 AM	43.0	14	16	44	0.0	0.0%	38.4
9/3/2024 2:00:00 AM	42.0	16	19	46	46.0	5.3%	35.2
9/3/2024 3:00:00 AM	45.0	6	7	46	46.0	14.3%	38.6
9/3/2024 4:00:00 AM	43.0	6	7	49	49.0	14.3%	40.7
9/3/2024 5:00:00 AM	43.0	3	4	44	0.0	0.0%	39.3
9/3/2024 6:00:00 AM	40.0	2	2	40	0.0	0.0%	39.0
9/3/2024 7:00:00 AM	37.0	2	2	37	0.0	0.0%	35.5
9/3/2024 8:00:00 AM	54.0	3	3	54	54.0	33.3%	43.0
9/3/2024 9:00:00 AM	48.0	14	16	49	48.3	18.8%	42.3
9/3/2024 10:00:00 AM	43.0	23	27	46	46.0	7.4%	38.6
9/3/2024 11:00:00 AM	43.0	37	43	49	47.2	11.6%	38.1
9/3/2024 12:00:00 PM	45.0	63	74	52	47.3	9.5%	39.4
9/3/2024 1:00:00 PM	42.0	66	78	49	47.3	5.1%	37.7
9/3/2024 2:00:00 PM	41.0	38	45	46	46.0	2.2%	38.3

Outgoing Summary

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/30/2024 12:00:00 AM	44.0	885	1041	58	47.7	8.6%	39.2
8/31/2024 12:00:00 AM	44.0	1294	1522	72	48.1	9.6%	39.4
9/1/2024 12:00:00 AM	44.0	919	1081	62	48.5	7.4%	39.0
9/2/2024 12:00:00 AM	44.0	904	1064	61	48.3	9.2%	39.1
9/3/2024 12:00:00 AM	44.0	906	1066	69	48.5	8.2%	38.9
9/3/2024 1:59:59 PM	44.0	292	343	54	47.5	7.6%	38.5

Outgoing Weekly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	49	53	40	49	46.5	43.3
1 - 2	*	*	*	*	30	26	29	30	27.5	45
2 - 3	*	*	*	*	13	17	10	13	13.5	45
3 - 4	*	*	*	*	7	11	10	7	10.5	43
4 - 5	*	*	*	*	2	2	6	2	4	38
5 - 6	*	*	*	*	1	4	5	1	4.5	50
6 - 7	*	*	*	*	1	2	6	1	4	39
7 - 8	*	*	*	*	2	1	3	2	2	39
8 - 9	*	*	*	*	15	5	5	15	5	44
9 - 10	*	*	*	*	35	12	26	35	19	42.5
10 - 11	*	*	*	*	40	22	26	40	24	45.4
11 - 12	*	*	*	*	74	34	32	74	33	42.5
12 - 13	*	*	*	*	98	43	36	98	39.5	44
13 - 14	*	*	*	23	73	61	55	48	58	42.95
14 - 15	*	*	*	87	89	89	74	88	81.5	41.55
15 - 16	*	*	*	105	97	84	98	101	91	42.8
16 - 17	*	*	*	114	112	91	121	113	106	43.15
17 - 18	*	*	*	92	119	80	86	105.5	83	43.55
18 - 19	*	*	*	99	146	81	74	122.5	77.5	43.95
19 - 20	*	*	*	138	142	85	73	140	79	44.2
20 - 21	*	*	*	131	143	81	70	137	75.5	44.6
21 - 22	*	*	*	104	90	62	76	97	69	43.85
22 - 23	*	*	*	80	69	71	60	74.5	65.5	43.5
23 - 24	*	*	*	68	75	64	43	71.5	53.5	44
Totals	0	0	0	1041	1522	1081	1064			
% of Total	0%	0%	0%	22.11%	32.33%	22.96%	22.6%			

Outgoing Weekly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	27	16	*	*	*	*	*	21.5	0	43.5
1 - 2	24	19	*	*	*	*	*	21.5	0	41
2 - 3	13	7	*	*	*	*	*	10	0	45.5
3 - 4	8	7	*	*	*	*	*	7.5	0	44
4 - 5	3	4	*	*	*	*	*	3.5	0	41.5
5 - 6	6	2	*	*	*	*	*	4	0	38
6 - 7	3	2	*	*	*	*	*	2.5	0	37.5
7 - 8	2	3	*	*	*	*	*	2.5	0	45.5
8 - 9	0	16	*	*	*	*	*	8	0	47.5
9 - 10	11	27	*	*	*	*	*	19	0	41.4
10 - 11	12	43	*	*	*	*	*	27.5	0	41
11 - 12	39	74	*	*	*	*	*	56.5	0	42.95
12 - 13	55	78	*	*	*	*	*	66.5	0	42.9
13 - 14	68	45	*	*	*	*	*	56.5	0	40.95
14 - 15	88	*	*	*	*	*	*	88	0	43.5
15 - 16	68	*	*	*	*	*	*	68	0	44
16 - 17	75	*	*	*	*	*	*	75	0	42.6
17 - 18	94	*	*	*	*	*	*	94	0	42
18 - 19	92	*	*	*	*	*	*	92	0	43.5
19 - 20	78	*	*	*	*	*	*	78	0	44
20 - 21	77	*	*	*	*	*	*	77	0	45.3
21 - 22	96	*	*	*	*	*	*	96	0	41.9
22 - 23	66	*	*	*	*	*	*	66	0	44
23 - 24	61	*	*	*	*	*	*	61	0	44.5
Totals	1066	343	0	0	0	0	0			
% of Total	75.66%	24.34%	0%	0%	0%	0%	0%			

Outgoing Monthly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	49	53	*	49	53	43.3
1 - 2	*	*	*	*	30	26	*	30	26	45
2 - 3	*	*	*	*	13	17	*	13	17	45
3 - 4	*	*	*	*	7	11	*	7	11	43
4 - 5	*	*	*	*	2	2	*	2	2	38
5 - 6	*	*	*	*	1	4	*	1	4	50
6 - 7	*	*	*	*	1	2	*	1	2	39
7 - 8	*	*	*	*	2	1	*	2	1	39
8 - 9	*	*	*	*	15	5	*	15	5	44
9 - 10	*	*	*	*	35	12	*	35	12	42.5
10 - 11	*	*	*	*	40	22	*	40	22	45.4
11 - 12	*	*	*	*	74	34	*	74	34	42.5
12 - 13	*	*	*	*	98	43	*	98	43	44
13 - 14	*	*	*	23	73	61	*	48	61	42.95
14 - 15	*	*	*	87	89	89	*	88	89	41.55
15 - 16	*	*	*	105	97	84	*	101	84	42.8
16 - 17	*	*	*	114	112	91	*	113	91	43.15
17 - 18	*	*	*	92	119	80	*	105.5	80	43.55
18 - 19	*	*	*	99	146	81	*	122.5	81	43.95
19 - 20	*	*	*	138	142	85	*	140	85	44.2
20 - 21	*	*	*	131	143	81	*	137	81	44.6
21 - 22	*	*	*	104	90	62	*	97	62	43.85
22 - 23	*	*	*	80	69	71	*	74.5	71	43.5
23 - 24	*	*	*	68	75	64	*	71.5	64	44
Totals	0	0	0	1041	1522	1081	0			
% of Total	0%	0%	0%	28.57%	41.77%	29.67%	0%			

Outgoing Monthly Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	27	16	*	*	*	*	40	21.5	40	43.5
1 - 2	24	19	*	*	*	*	29	21.5	29	41
2 - 3	13	7	*	*	*	*	10	10	10	45.5
3 - 4	8	7	*	*	*	*	10	7.5	10	44
4 - 5	3	4	*	*	*	*	6	3.5	6	41.5
5 - 6	6	2	*	*	*	*	5	4	5	38
6 - 7	3	2	*	*	*	*	6	2.5	6	37.5
7 - 8	2	3	*	*	*	*	3	2.5	3	45.5
8 - 9	0	16	*	*	*	*	5	8	5	47.5
9 - 10	11	27	*	*	*	*	26	19	26	41.4
10 - 11	12	43	*	*	*	*	26	27.5	26	41
11 - 12	39	74	*	*	*	*	32	56.5	32	42.95
12 - 13	55	78	*	*	*	*	36	66.5	36	42.9
13 - 14	68	45	*	*	*	*	55	56.5	55	40.95
14 - 15	88	*	*	*	*	*	74	88	74	43.5
15 - 16	68	*	*	*	*	*	98	68	98	44
16 - 17	75	*	*	*	*	*	121	75	121	42.6
17 - 18	94	*	*	*	*	*	86	94	86	42
18 - 19	92	*	*	*	*	*	74	92	74	43.5
19 - 20	78	*	*	*	*	*	73	78	73	44
20 - 21	77	*	*	*	*	*	70	77	70	45.3
21 - 22	96	*	*	*	*	*	76	96	76	41.9
22 - 23	66	*	*	*	*	*	60	66	60	44
23 - 24	61	*	*	*	*	*	43	61	43	44.5
Totals	1066	343	0	0	0	0	1064			
% of Total	43.11%	13.87%	0%	0%	0%	0%	43.02%			

Outgoing Weekly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	39.8	38.3	38.62	39.8	38.44	43.3
1 - 2	*	*	*	*	39.67	38.88	38.1	39.67	38.47	45
2 - 3	*	*	*	*	41.46	39.59	36.7	41.46	38.52	45
3 - 4	*	*	*	*	39.29	39.09	39.3	39.29	39.19	43
4 - 5	*	*	*	*	35	45	40.17	35	41.38	38
5 - 6	*	*	*	*	50	41.25	38.4	50	39.67	50
6 - 7	*	*	*	*	39	37	43.67	39	42	39
7 - 8	*	*	*	*	38	35	33.67	38	34	39
8 - 9	*	*	*	*	39.4	38.8	37.4	39.4	38.1	44
9 - 10	*	*	*	*	38	38.92	40.96	38	40.32	42.5
10 - 11	*	*	*	*	41.45	40.14	40.58	41.45	40.38	45.4
11 - 12	*	*	*	*	39.04	38.21	39.66	39.04	38.91	42.5
12 - 13	*	*	*	*	39.35	38.07	38.25	39.35	38.15	44
13 - 14	*	*	*	39.78	38.68	39.75	38.71	38.95	39.26	42.95
14 - 15	*	*	*	37.28	38.45	39.49	39.77	37.87	39.62	41.55
15 - 16	*	*	*	38.18	39.09	39.79	39.2	38.62	39.47	42.8
16 - 17	*	*	*	39.5	38.88	38.05	38.74	39.19	38.44	43.15
17 - 18	*	*	*	38.97	39.56	39.16	38.76	39.3	38.95	43.55
18 - 19	*	*	*	38.88	39.66	37.89	39.51	39.35	38.66	43.95
19 - 20	*	*	*	39.6	39.54	38.51	39.05	39.57	38.76	44.2
20 - 21	*	*	*	40.02	40.15	39.16	39.5	40.09	39.32	44.6
21 - 22	*	*	*	39.75	39.77	39.68	38.89	39.76	39.25	43.85
22 - 23	*	*	*	40.06	38.81	39.72	39.47	39.48	39.6	43.5
23 - 24	*	*	*	39.21	39.76	38.83	38.4	39.5	38.65	44
Totals	0	0	0	39.2	39.4	39	39.1			
% of Total	0%	0%	0%	25.02%	25.14%	24.89%	24.95%			

Outgoing Weekly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	39.41	38.38	*	*	*	*	*	39.02	*	43.5
1 - 2	38.25	35.21	*	*	*	*	*	36.91	*	41
2 - 3	41.54	38.57	*	*	*	*	*	40.5	*	45.5
3 - 4	38.12	40.71	*	*	*	*	*	39.33	*	44
4 - 5	38	39.25	*	*	*	*	*	38.71	*	41.5
5 - 6	33.5	39	*	*	*	*	*	34.88	*	38
6 - 7	36	35.5	*	*	*	*	*	35.8	*	37.5
7 - 8	36.5	43	*	*	*	*	*	40.4	*	45.5
8 - 9	0	42.31	*	*	*	*	*	42.31	*	47.5
9 - 10	38.09	38.63	*	*	*	*	*	38.47	*	41.4
10 - 11	35.67	38.09	*	*	*	*	*	37.56	*	41
11 - 12	37.82	39.42	*	*	*	*	*	38.87	*	42.95
12 - 13	38.62	37.68	*	*	*	*	*	38.07	*	42.9
13 - 14	37.99	38.27	*	*	*	*	*	38.1	*	40.95
14 - 15	39.24	*	*	*	*	*	*	39.24	*	43.5
15 - 16	39.63	*	*	*	*	*	*	39.63	*	44
16 - 17	38.17	*	*	*	*	*	*	38.17	*	42.6
17 - 18	38.39	*	*	*	*	*	*	38.39	*	42
18 - 19	39.88	*	*	*	*	*	*	39.88	*	43.5
19 - 20	39.6	*	*	*	*	*	*	39.6	*	44
20 - 21	40.77	*	*	*	*	*	*	40.77	*	45.3
21 - 22	38.47	*	*	*	*	*	*	38.47	*	41.9
22 - 23	38.94	*	*	*	*	*	*	38.94	*	44
23 - 24	39.07	*	*	*	*	*	*	39.07	*	44.5
Totals	38.9	38.5	0	0	0	0	0			
% of Total	50.26%	49.74%	0%	0%	0%	0%	0%			

Outgoing Monthly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	39.8	38.3	*	39.8	38.3	43.3
1 - 2	*	*	*	*	39.67	38.88	*	39.67	38.88	45
2 - 3	*	*	*	*	41.46	39.59	*	41.46	39.59	45
3 - 4	*	*	*	*	39.29	39.09	*	39.29	39.09	43
4 - 5	*	*	*	*	35	45	*	35	45	38
5 - 6	*	*	*	*	50	41.25	*	50	41.25	50
6 - 7	*	*	*	*	39	37	*	39	37	39
7 - 8	*	*	*	*	38	35	*	38	35	39
8 - 9	*	*	*	*	39.4	38.8	*	39.4	38.8	44
9 - 10	*	*	*	*	38	38.92	*	38	38.92	42.5
10 - 11	*	*	*	*	41.45	40.14	*	41.45	40.14	45.4
11 - 12	*	*	*	*	39.04	38.21	*	39.04	38.21	42.5
12 - 13	*	*	*	*	39.35	38.07	*	39.35	38.07	44
13 - 14	*	*	*	39.78	38.68	39.75	*	38.95	39.75	42.95
14 - 15	*	*	*	37.28	38.45	39.49	*	37.87	39.49	41.55
15 - 16	*	*	*	38.18	39.09	39.79	*	38.62	39.79	42.8
16 - 17	*	*	*	39.5	38.88	38.05	*	39.19	38.05	43.15
17 - 18	*	*	*	38.97	39.56	39.16	*	39.3	39.16	43.55
18 - 19	*	*	*	38.88	39.66	37.89	*	39.35	37.89	43.95
19 - 20	*	*	*	39.6	39.54	38.51	*	39.57	38.51	44.2
20 - 21	*	*	*	40.02	40.15	39.16	*	40.09	39.16	44.6
21 - 22	*	*	*	39.75	39.77	39.68	*	39.76	39.68	43.85
22 - 23	*	*	*	40.06	38.81	39.72	*	39.48	39.72	43.5
23 - 24	*	*	*	39.21	39.76	38.83	*	39.5	38.83	44
Totals	0	0	0	39.2	39.4	39	0			
% of Total	0%	0%	0%	33.33%	33.5%	33.16%	0%			

Outgoing Monthly Speeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Sep 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	39.41	38.38	*	*	*	*	38.62	39.02	38.62	43.5
1 - 2	38.25	35.21	*	*	*	*	38.1	36.91	38.1	41
2 - 3	41.54	38.57	*	*	*	*	36.7	40.5	36.7	45.5
3 - 4	38.12	40.71	*	*	*	*	39.3	39.33	39.3	44
4 - 5	38	39.25	*	*	*	*	40.17	38.71	40.17	41.5
5 - 6	33.5	39	*	*	*	*	38.4	34.88	38.4	38
6 - 7	36	35.5	*	*	*	*	43.67	35.8	43.67	37.5
7 - 8	36.5	43	*	*	*	*	33.67	40.4	33.67	45.5
8 - 9	0	42.31	*	*	*	*	37.4	42.31	37.4	47.5
9 - 10	38.09	38.63	*	*	*	*	40.96	38.47	40.96	41.4
10 - 11	35.67	38.09	*	*	*	*	40.58	37.56	40.58	41
11 - 12	37.82	39.42	*	*	*	*	39.66	38.87	39.66	42.95
12 - 13	38.62	37.68	*	*	*	*	38.25	38.07	38.25	42.9
13 - 14	37.99	38.27	*	*	*	*	38.71	38.1	38.71	40.95
14 - 15	39.24	*	*	*	*	*	39.77	39.24	39.77	43.5
15 - 16	39.63	*	*	*	*	*	39.2	39.63	39.2	44
16 - 17	38.17	*	*	*	*	*	38.74	38.17	38.74	42.6
17 - 18	38.39	*	*	*	*	*	38.76	38.39	38.76	42
18 - 19	39.88	*	*	*	*	*	39.51	39.88	39.51	43.5
19 - 20	39.6	*	*	*	*	*	39.05	39.6	39.05	44
20 - 21	40.77	*	*	*	*	*	39.5	40.77	39.5	45.3
21 - 22	38.47	*	*	*	*	*	38.89	38.47	38.89	41.9
22 - 23	38.94	*	*	*	*	*	39.47	38.94	39.47	44
23 - 24	39.07	*	*	*	*	*	38.4	39.07	38.4	44.5
Totals	38.9	38.5	0	0	0	0	39.1			
% of Total	33.39%	33.05%	0%	0%	0%	0%	33.56%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	43.3	44	43	43.3	43.5	43.3
1 - 2	*	*	*	*	45	42.5	43	45	42.75	45
2 - 3	*	*	*	*	45	42	41	45	41.5	45
3 - 4	*	*	*	*	43	41.5	44	43	42.75	43
4 - 5	*	*	*	*	38	48	42	38	45	38
5 - 6	*	*	*	*	50	42	43	50	42.5	50
6 - 7	*	*	*	*	39	38	45	39	41.5	39
7 - 8	*	*	*	*	39	35	42	39	38.5	39
8 - 9	*	*	*	*	44	40	42	44	41	44
9 - 10	*	*	*	*	42.5	41	46	42.5	43.5	42.5
10 - 11	*	*	*	*	45.4	43.5	46	45.4	44.75	45.4
11 - 12	*	*	*	*	42.5	43.3	44	42.5	43.65	42.5
12 - 13	*	*	*	*	44	43	44	44	43.5	44
13 - 14	*	*	*	43.5	42.4	43.5	43.2	42.95	43.35	42.95
14 - 15	*	*	*	40.9	42.2	43.1	44	41.55	43.55	41.55
15 - 16	*	*	*	43	42.6	43.4	44.2	42.8	43.8	42.8
16 - 17	*	*	*	43.7	42.6	42.6	43	43.15	42.8	43.15
17 - 18	*	*	*	43.3	43.8	44	43.5	43.55	43.75	43.55
18 - 19	*	*	*	43.2	44.7	43	44.8	43.95	43.9	43.95
19 - 20	*	*	*	43.9	44.5	41.8	43	44.2	42.4	44.2
20 - 21	*	*	*	44.7	44.5	42.5	44.3	44.6	43.4	44.6
21 - 22	*	*	*	43	44.7	43.8	43.2	43.85	43.5	43.85
22 - 23	*	*	*	45	42	43	44.8	43.5	43.9	43.5
23 - 24	*	*	*	43.3	44.7	43.7	44.3	44	44	44
Totals	0	0	0	477.5	1039.4	1018.2	1047.3			
% of Total	0%	0%	0%	13.33%	29.01%	28.42%	29.23%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	44	43	*	*	*	*	*	43.5	0	43.5
1 - 2	40.5	41.5	*	*	*	*	*	41	0	41
2 - 3	46	45	*	*	*	*	*	45.5	0	45.5
3 - 4	45	43	*	*	*	*	*	44	0	44
4 - 5	40	43	*	*	*	*	*	41.5	0	41.5
5 - 6	36	40	*	*	*	*	*	38	0	38
6 - 7	38	37	*	*	*	*	*	37.5	0	37.5
7 - 8	37	54	*	*	*	*	*	45.5	0	45.5
8 - 9	0	47.5	*	*	*	*	*	47.5	0	47.5
9 - 10	40	42.8	*	*	*	*	*	41.4	0	41.4
10 - 11	39	43	*	*	*	*	*	41	0	41
11 - 12	41.7	44.2	*	*	*	*	*	42.95	0	42.95
12 - 13	44	41.8	*	*	*	*	*	42.9	0	42.9
13 - 14	40.9	41	*	*	*	*	*	40.95	0	40.95
14 - 15	43.5	*	*	*	*	*	*	43.5	0	43.5
15 - 16	44	*	*	*	*	*	*	44	0	44
16 - 17	42.6	*	*	*	*	*	*	42.6	0	42.6
17 - 18	42	*	*	*	*	*	*	42	0	42
18 - 19	43.5	*	*	*	*	*	*	43.5	0	43.5
19 - 20	44	*	*	*	*	*	*	44	0	44
20 - 21	45.3	*	*	*	*	*	*	45.3	0	45.3
21 - 22	41.9	*	*	*	*	*	*	41.9	0	41.9
22 - 23	44	*	*	*	*	*	*	44	0	44
23 - 24	44.5	*	*	*	*	*	*	44.5	0	44.5
Totals	967.4	606.8	0	0	0	0	0			
% of Total	61.45%	38.55%	0%	0%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	43.3	44	*	43.3	44	43.3
1 - 2	*	*	*	*	45	42.5	*	45	42.5	45
2 - 3	*	*	*	*	45	42	*	45	42	45
3 - 4	*	*	*	*	43	41.5	*	43	41.5	43
4 - 5	*	*	*	*	38	48	*	38	48	38
5 - 6	*	*	*	*	50	42	*	50	42	50
6 - 7	*	*	*	*	39	38	*	39	38	39
7 - 8	*	*	*	*	39	35	*	39	35	39
8 - 9	*	*	*	*	44	40	*	44	40	44
9 - 10	*	*	*	*	42.5	41	*	42.5	41	42.5
10 - 11	*	*	*	*	45.4	43.5	*	45.4	43.5	45.4
11 - 12	*	*	*	*	42.5	43.3	*	42.5	43.3	42.5
12 - 13	*	*	*	*	44	43	*	44	43	44
13 - 14	*	*	*	43.5	42.4	43.5	*	42.95	43.5	42.95
14 - 15	*	*	*	40.9	42.2	43.1	*	41.55	43.1	41.55
15 - 16	*	*	*	43	42.6	43.4	*	42.8	43.4	42.8
16 - 17	*	*	*	43.7	42.6	42.6	*	43.15	42.6	43.15
17 - 18	*	*	*	43.3	43.8	44	*	43.55	44	43.55
18 - 19	*	*	*	43.2	44.7	43	*	43.95	43	43.95
19 - 20	*	*	*	43.9	44.5	41.8	*	44.2	41.8	44.2
20 - 21	*	*	*	44.7	44.5	42.5	*	44.6	42.5	44.6
21 - 22	*	*	*	43	44.7	43.8	*	43.85	43.8	43.85
22 - 23	*	*	*	45	42	43	*	43.5	43	43.5
23 - 24	*	*	*	43.3	44.7	43.7	*	44	43.7	44

Outgoing Monthly EightyFifthSpeeds

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	44	43	*	*	*	*	43	43.5	43	43.5
1 - 2	40.5	41.5	*	*	*	*	43	41	43	41
2 - 3	46	45	*	*	*	*	41	45.5	41	45.5
3 - 4	45	43	*	*	*	*	44	44	44	44
4 - 5	40	43	*	*	*	*	42	41.5	42	41.5
5 - 6	36	40	*	*	*	*	43	38	43	38
6 - 7	38	37	*	*	*	*	45	37.5	45	37.5
7 - 8	37	54	*	*	*	*	42	45.5	42	45.5
8 - 9	0	47.5	*	*	*	*	42	47.5	42	47.5
9 - 10	40	42.8	*	*	*	*	46	41.4	46	41.4
10 - 11	39	43	*	*	*	*	46	41	46	41
11 - 12	41.7	44.2	*	*	*	*	44	42.95	44	42.95
12 - 13	44	41.8	*	*	*	*	44	42.9	44	42.9
13 - 14	40.9	41	*	*	*	*	43.2	40.95	43.2	40.95
14 - 15	43.5	*	*	*	*	*	44	43.5	44	43.5
15 - 16	44	*	*	*	*	*	44.2	44	44.2	44
16 - 17	42.6	*	*	*	*	*	43	42.6	43	42.6
17 - 18	42	*	*	*	*	*	43.5	42	43.5	42
18 - 19	43.5	*	*	*	*	*	44.8	43.5	44.8	43.5
19 - 20	44	*	*	*	*	*	43	44	43	44
20 - 21	45.3	*	*	*	*	*	44.3	45.3	44.3	45.3
21 - 22	41.9	*	*	*	*	*	43.2	41.9	43.2	41.9
22 - 23	44	*	*	*	*	*	44.8	44	44.8	44
23 - 24	44.5	*	*	*	*	*	44.3	44.5	44.3	44.5

Summary of Violators

Birchwood Loop northwest north of 3bears 8-29 to 9-4 from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM
time error -3hrs

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	185	38.9	16	48.1
01:00:00	128	38.2	9	47.2
02:00:00	60	39.8	8	51.9
03:00:00	43	39.3	5	52.0
04:00:00	17	39.5	1	48.0
05:00:00	18	38.1	2	48.0
06:00:00	14	39.6	1	56.0
07:00:00	11	37.6	1	54.0
08:00:00	41	40.2	5	47.6
09:00:00	111	39.0	8	50.6
10:00:00	143	39.6	20	48.8
11:00:00	253	38.9	15	47.0
12:00:00	310	38.5	27	47.7
13:00:00	325	38.8	15	47.6
14:00:00	427	38.8	27	48.2
15:00:00	452	39.1	31	48.2
16:00:00	513	38.7	38	47.3
17:00:00	471	39.0	36	47.4
18:00:00	492	39.2	51	48.1
19:00:00	516	39.3	48	48.1
20:00:00	502	40.0	60	48.2
21:00:00	428	39.3	35	48.2
22:00:00	346	39.4	35	48.0
23:00:00	311	39.1	33	48.7

Outgoing Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	13:00	0	0	0	0	3	8	10	2	0	0	0	0	0	0	0	0	0	0	23	39.8	43.5	34 to 44	87.0	1	4.3	0	20	3	
8/29/2024	14:00	0	0	1	5	16	34	27	4	0	0	0	0	0	0	0	0	0	0	87	37.3	40.9	31 to 41	74.7	3	3.4	1	76	10	
8/29/2024	15:00	0	0	1	4	19	38	35	7	1	0	0	0	0	0	0	0	0	0	105	38.2	43	34 to 44	73.3	4	3.8	1	92	12	
8/29/2024	16:00	0	0	0	6	9	36	48	14	1	0	0	0	0	0	0	0	0	0	114	39.5	43.7	34 to 44	78.1	13	11.4	0	107	7	
8/29/2024	17:00	0	0	0	1	9	41	34	7	0	0	0	0	0	0	0	0	0	0	92	39	43.3	34 to 44	87.0	5	5.4	1	87	4	
8/29/2024	18:00	0	0	0	1	16	38	32	11	1	0	0	0	0	0	0	0	0	0	99	38.9	43.2	35 to 45	74.7	8	8.1	0	97	2	
8/29/2024	19:00	0	0	0	3	19	44	52	16	3	1	0	0	0	0	0	0	0	0	138	39.6	43.9	36 to 46	75.4	14	10.1	0	129	9	
8/29/2024	20:00	0	0	0	3	10	48	42	24	4	0	0	0	0	0	0	0	0	0	131	40	44.7	35 to 45	77.1	17	13.0	0	125	6	
8/29/2024	21:00	0	0	0	3	11	31	48	9	2	0	0	0	0	0	0	0	0	0	104	39.8	43	33 to 43	80.8	7	6.7	0	99	5	
8/29/2024	22:00	0	0	0	2	5	30	26	15	2	0	0	0	0	0	0	0	0	0	80	40.1	45	35 to 45	76.3	12	15.0	0	78	2	
8/29/2024	23:00	0	1	0	1	5	29	24	7	1	0	0	0	0	0	0	0	0	0	68	39.2	43.3	34 to 44	80.9	6	8.8	0	66	2	
24 Hr Summary		0	1	2	29	122	377	378	116	15	1	0	0	0	0	0	0	0	0	1041	39.2	44	35 to 45	76.6	90	8.6	3	976	62	

Outgoing Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/30/2024	00:00	0	0	0	1	2	18	23	5	0	0	0	0	0	0	0	0	0	0	49	39.8	43.3	35 to 45	87.8	3	6.1	0	46	3
8/30/2024	01:00	0	0	0	1	2	13	7	7	0	0	0	0	0	0	0	0	0	0	30	39.7	45	36 to 46	83.3	4	13.3	0	30	0
8/30/2024	02:00	0	0	0	1	1	5	3	2	0	0	0	0	1	0	0	0	0	0	13	41.5	45	34 to 44	69.2	2	15.4	0	13	0
8/30/2024	03:00	0	0	0	0	2	2	2	1	0	0	0	0	0	0	0	0	0	0	7	39.3	43	33 to 43	85.7	1	14.3	0	7	0
8/30/2024	04:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	35	38	28 to 38	100.0	*	*	0	2	0
8/30/2024	05:00	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	50	50	40 to 50	100.0	1	100.0	0	1	0
8/30/2024	06:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	39	39	29 to 39	100.0	*	*	0	1	0
8/30/2024	07:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	38	39	29 to 39	100.0	*	*	0	2	0
8/30/2024	08:00	0	0	0	0	4	3	6	2	0	0	0	0	0	0	0	0	0	0	15	39.4	44	34 to 44	80.0	1	6.7	0	13	2
8/30/2024	09:00	0	1	1	2	2	11	15	3	0	0	0	0	0	0	0	0	0	0	35	38	42.5	35 to 45	80.0	1	2.9	0	32	3
8/30/2024	10:00	0	0	0	2	3	6	19	9	0	0	0	1	0	0	0	0	0	0	40	41.5	45.4	36 to 46	77.5	8	20.0	0	39	1
8/30/2024	11:00	0	0	0	1	12	26	29	6	0	0	0	0	0	0	0	0	0	0	74	39	42.5	34 to 44	83.8	3	4.1	0	66	8
8/30/2024	12:00	0	0	0	0	13	41	29	13	2	0	0	0	0	0	0	0	0	0	98	39.3	44	35 to 45	77.6	9	9.2	0	87	11
8/30/2024	13:00	0	0	0	2	11	24	31	3	2	0	0	0	0	0	0	0	0	0	73	38.7	42.4	35 to 45	78.1	3	4.1	0	65	8
8/30/2024	14:00	0	0	1	1	13	33	33	6	2	0	0	0	0	0	0	0	0	0	89	38.4	42.2	35 to 45	77.5	5	5.6	0	78	11
8/30/2024	15:00	0	0	0	3	11	33	38	11	1	0	0	0	0	0	0	0	0	0	97	39.1	42.6	33 to 43	78.4	8	8.2	0	91	6
8/30/2024	16:00	0	0	0	1	17	38	47	9	0	0	0	0	0	0	0	0	0	0	112	38.9	42.6	34 to 44	80.4	7	6.3	1	99	12
8/30/2024	17:00	0	1	0	1	14	34	53	15	1	0	0	0	0	0	0	0	0	0	119	39.6	43.8	34 to 44	76.5	12	10.1	1	114	4
8/30/2024	18:00	0	0	0	1	20	57	44	20	4	0	0	0	0	0	0	0	0	0	146	39.7	44.7	34 to 44	75.3	21	14.4	2	138	6
8/30/2024	19:00	0	0	1	2	18	51	43	26	1	0	0	0	0	0	0	0	0	0	142	39.5	44.5	35 to 45	75.4	14	9.9	1	131	10
8/30/2024	20:00	0	0	0	2	14	43	60	19	5	0	0	0	0	0	0	0	0	0	143	40.2	44.5	35 to 45	76.2	18	12.6	0	137	6
8/30/2024	21:00	0	0	0	2	8	35	29	14	1	1	0	0	0	0	0	0	0	0	90	39.8	44.7	36 to 46	75.6	13	14.4	0	89	1
8/30/2024	22:00	0	0	0	0	10	30	20	9	0	0	0	0	0	0	0	0	0	0	69	38.8	42	33 to 43	87.0	2	2.9	0	68	1
8/30/2024	23:00	0	0	0	2	9	29	22	8	4	1	0	0	0	0	0	0	0	0	75	39.8	44.7	34 to 44	74.7	10	13.3	0	73	2
24 Hr Summary		0	2	3	25	187	536	553	188	24	2	0	1	1	0	0	0	0	0	1522	39.4	44	35 to 45	76.1	146	9.6	5	1422	95

Outgoing Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/31/2024	00:00	0	1	2	1	11	13	17	7	0	1	0	0	0	0	0	0	0	0	53	38.3	44	32 to 42	66.0	6	11.3	0	51	2
8/31/2024	01:00	0	0	0	0	6	5	12	2	1	0	0	0	0	0	0	0	0	0	26	38.9	42.5	32 to 42	73.1	2	7.7	0	25	1
8/31/2024	02:00	0	0	0	0	1	9	4	3	0	0	0	0	0	0	0	0	0	0	17	39.6	42	32 to 42	82.4	2	11.8	0	17	0
8/31/2024	03:00	0	0	0	1	2	3	4	0	0	0	1	0	0	0	0	0	0	0	11	39.1	41.5	32 to 42	81.8	1	9.1	0	11	0
8/31/2024	04:00	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	45	48	38 to 48	100.0	1	50.0	0	2	0
8/31/2024	05:00	0	0	0	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	4	41.3	42	34 to 44	100.0	*	*	0	4	0
8/31/2024	06:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	37	38	28 to 38	100.0	*	*	0	2	0
8/31/2024	07:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	1	0
8/31/2024	08:00	0	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5	38.8	40	34 to 44	100.0	*	*	0	5	0
8/31/2024	09:00	0	0	0	0	1	5	5	1	0	0	0	0	0	0	0	0	0	0	12	38.9	41	32 to 42	91.7	*	*	0	11	1
8/31/2024	10:00	0	0	0	0	3	9	8	0	2	0	0	0	0	0	0	0	0	0	22	40.1	43.5	34 to 44	77.3	2	9.1	0	22	0
8/31/2024	11:00	0	0	0	2	6	10	13	3	0	0	0	0	0	0	0	0	0	0	34	38.2	43.3	34 to 44	73.5	2	5.9	0	33	1
8/31/2024	12:00	0	0	0	3	6	17	13	4	0	0	0	0	0	0	0	0	0	0	43	38.1	43	36 to 46	76.7	3	7.0	0	42	1
8/31/2024	13:00	0	0	0	1	9	15	28	7	1	0	0	0	0	0	0	0	0	0	61	39.8	43.5	33 to 43	78.7	5	8.2	0	60	1
8/31/2024	14:00	0	0	0	1	13	27	41	6	0	1	0	0	0	0	0	0	0	0	89	39.5	43.1	34 to 44	83.1	6	6.7	0	87	2
8/31/2024	15:00	0	0	0	1	11	29	35	7	1	0	0	0	0	0	0	0	0	0	84	39.8	43.4	34 to 44	84.5	6	7.1	0	81	3
8/31/2024	16:00	0	0	1	5	16	31	29	8	1	0	0	0	0	0	0	0	0	0	91	38.1	42.6	33 to 43	70.3	5	5.5	0	89	2
8/31/2024	17:00	0	0	0	3	9	33	23	11	1	0	0	0	0	0	0	0	0	0	80	39.2	44	34 to 44	75.0	8	10.0	0	78	2
8/31/2024	18:00	0	0	0	3	14	35	21	8	0	0	0	0	0	0	0	0	0	0	81	37.9	43	34 to 44	74.1	4	4.9	0	78	3
8/31/2024	19:00	0	0	0	3	10	36	30	5	1	0	0	0	0	0	0	0	0	0	85	38.5	41.8	35 to 45	81.2	3	3.5	1	77	7
8/31/2024	20:00	0	0	0	1	10	28	34	8	0	0	0	0	0	0	0	0	0	0	81	39.2	42.5	36 to 46	81.5	6	7.4	0	77	4
8/31/2024	21:00	0	0	0	1	8	22	23	7	1	0	0	0	0	0	0	0	0	0	62	39.7	43.8	34 to 44	77.4	6	9.7	0	59	3
8/31/2024	22:00	0	0	0	2	9	24	26	6	3	1	0	0	0	0	0	0	0	0	71	39.7	43	33 to 43	76.1	6	8.5	0	71	0
8/31/2024	23:00	0	0	0	2	14	20	19	6	3	0	0	0	0	0	0	0	0	0	64	38.8	43.7	32 to 42	71.9	6	9.4	0	62	2
24 Hr Summary		0	1	3	30	159	378	391	100	15	3	1	0	0	0	0	0	0	0	1081	39	44	34 to 44	75.9	80	7.4	1	1045	35

Outgoing Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/1/2024	00:00	0	0	0	0	9	11	17	3	0	0	0	0	0	0	0	0	0	0	40	38.6	43	32 to 42	77.5	3	7.5	0	39	1
9/1/2024	01:00	0	0	0	1	5	12	7	3	1	0	0	0	0	0	0	0	0	0	29	38.1	43	31 to 41	79.3	2	6.9	1	26	2
9/1/2024	02:00	0	0	0	0	4	3	3	0	0	0	0	0	0	0	0	0	0	0	10	36.7	41	32 to 42	90.0	*	*	0	10	0
9/1/2024	03:00	0	0	0	1	0	5	2	1	1	0	0	0	0	0	0	0	0	0	10	39.3	44	35 to 45	80.0	1	10.0	0	10	0
9/1/2024	04:00	0	0	0	0	0	1	5	0	0	0	0	0	0	0	0	0	0	0	6	40.2	42	33 to 43	100.0	*	*	0	6	0
9/1/2024	05:00	0	0	0	0	1	2	1	1	0	0	0	0	0	0	0	0	0	0	5	38.4	43	36 to 46	80.0	1	20.0	0	5	0
9/1/2024	06:00	0	0	0	0	0	2	2	1	0	1	0	0	0	0	0	0	0	0	6	43.7	45	35 to 45	83.3	1	16.7	0	6	0
9/1/2024	07:00	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	3	33.7	42	22 to 32	66.7	*	*	0	3	0
9/1/2024	08:00	0	0	0	1	1	1	1	1	0	0	0	0	0	0	0	0	0	0	5	37.4	42	27 to 37	60.0	1	20.0	0	5	0
9/1/2024	09:00	0	0	1	0	1	11	8	2	1	0	2	0	0	0	0	0	0	0	26	41	46	34 to 44	76.9	5	19.2	0	25	1
9/1/2024	10:00	0	0	0	0	3	7	10	5	1	0	0	0	0	0	0	0	0	0	26	40.6	46	33 to 43	73.1	5	19.2	0	26	0
9/1/2024	11:00	0	0	1	0	3	11	12	5	0	0	0	0	0	0	0	0	0	0	32	39.7	44	36 to 46	81.3	3	9.4	0	31	1
9/1/2024	12:00	0	0	1	2	5	13	10	3	2	0	0	0	0	0	0	0	0	0	36	38.3	44	32 to 42	72.2	4	11.1	0	35	1
9/1/2024	13:00	0	0	0	1	9	20	21	4	0	0	0	0	0	0	0	0	0	0	55	38.7	43.2	34 to 44	78.2	3	5.5	0	53	2
9/1/2024	14:00	0	0	0	1	8	27	27	9	2	0	0	0	0	0	0	0	0	0	74	39.8	44	36 to 46	82.4	9	12.2	0	73	1
9/1/2024	15:00	0	0	1	1	16	37	26	14	2	1	0	0	0	0	0	0	0	0	98	39.2	44.2	35 to 45	73.5	8	8.2	0	95	3
9/1/2024	16:00	0	0	1	3	15	50	38	13	1	0	0	0	0	0	0	0	0	0	121	38.7	43	34 to 44	76.9	10	8.3	0	115	6
9/1/2024	17:00	0	0	0	4	13	25	34	10	0	0	0	0	0	0	0	0	0	0	86	38.8	43.5	34 to 44	76.7	4	4.7	0	86	0
9/1/2024	18:00	0	0	0	0	14	25	19	14	2	0	0	0	0	0	0	0	0	0	74	39.5	44.8	35 to 45	67.6	10	13.5	0	69	5
9/1/2024	19:00	0	0	0	3	12	20	30	5	2	1	0	0	0	0	0	0	0	0	73	39.1	43	34 to 44	75.3	7	9.6	0	71	2
9/1/2024	20:00	0	0	0	1	12	26	19	9	3	0	0	0	0	0	0	0	0	0	70	39.5	44.3	35 to 45	72.9	6	8.6	0	66	4
9/1/2024	21:00	0	0	0	1	8	35	25	6	1	0	0	0	0	0	0	0	0	0	76	38.9	43.2	35 to 45	84.2	3	3.9	0	75	1
9/1/2024	22:00	0	0	1	0	8	22	17	11	1	0	0	0	0	0	0	0	0	0	60	39.5	44.8	32 to 42	71.7	8	13.3	0	59	1
9/1/2024	23:00	0	0	1	0	7	18	10	7	0	0	0	0	0	0	0	0	0	0	43	38.4	44.3	32 to 42	81.4	4	9.3	0	41	2
24 Hr Summary		0	0	7	21	155	384	345	127	20	3	2	0	0	0	0	0	0	0	1064	39.1	44	34 to 44	73.6	98	9.2	1	1030	33

Outgoing Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/2/2024	00:00	0	0	0	1	4	10	8	3	1	0	0	0	0	0	0	0	0	0	27	39.4	44	33 to 43	74.1	4	14.8	0	27	0	
9/2/2024	01:00	0	0	0	1	1	13	9	0	0	0	0	0	0	0	0	0	0	0	24	38.3	40.5	34 to 44	95.8	*	*	0	23	1	
9/2/2024	02:00	0	0	0	0	3	2	5	2	0	0	1	0	0	0	0	0	0	0	13	41.5	46	33 to 43	69.2	3	23.1	0	13	0	
9/2/2024	03:00	0	0	1	0	1	2	1	3	0	0	0	0	0	0	0	0	0	0	8	38.1	45	35 to 45	62.5	1	12.5	0	8	0	
9/2/2024	04:00	0	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	3	38	40	30 to 40	100.0	*	*	0	3	0	
9/2/2024	05:00	0	0	0	1	3	2	0	0	0	0	0	0	0	0	0	0	0	0	6	33.5	36	29 to 39	100.0	*	*	0	6	0	
9/2/2024	06:00	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3	36	38	28 to 38	100.0	*	*	0	3	0	
9/2/2024	07:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	36.5	37	27 to 37	100.0	*	*	0	2	0	
9/2/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/2/2024	09:00	0	0	0	0	1	6	3	1	0	0	0	0	0	0	0	0	0	0	11	38.1	40	31 to 41	90.9	*	*	0	11	0	
9/2/2024	10:00	0	1	0	1	1	7	1	1	0	0	0	0	0	0	0	0	0	0	12	35.7	39	29 to 39	75.0	*	*	0	11	1	
9/2/2024	11:00	0	0	0	1	7	14	16	1	0	0	0	0	0	0	0	0	0	0	39	37.8	41.7	32 to 42	79.5	*	*	0	38	1	
9/2/2024	12:00	0	0	0	3	8	22	14	7	1	0	0	0	0	0	0	0	0	0	55	38.6	44	33 to 43	74.5	7	12.7	0	55	0	
9/2/2024	13:00	0	0	0	2	9	29	25	3	0	0	0	0	0	0	0	0	0	0	68	38	40.9	31 to 41	83.8	2	2.9	0	66	2	
9/2/2024	14:00	0	0	0	3	8	37	29	10	0	1	0	0	0	0	0	0	0	0	88	39.2	43.5	35 to 45	83.0	4	4.5	0	83	5	
9/2/2024	15:00	0	0	0	1	8	27	22	8	1	1	0	0	0	0	0	0	0	0	68	39.6	44	35 to 45	79.4	5	7.4	0	67	1	
9/2/2024	16:00	0	0	0	3	14	27	25	6	0	0	0	0	0	0	0	0	0	0	75	38.2	42.6	33 to 43	77.3	3	4.0	0	65	10	
9/2/2024	17:00	0	0	0	1	17	38	29	8	1	0	0	0	0	0	0	0	0	0	94	38.4	42	32 to 42	77.7	7	7.4	0	91	3	
9/2/2024	18:00	0	0	0	2	8	33	37	10	1	0	0	1	0	0	0	0	0	0	92	39.9	43.5	34 to 44	81.5	8	8.7	0	90	2	
9/2/2024	19:00	0	0	1	0	11	24	30	10	1	1	0	0	0	0	0	0	0	0	78	39.6	44	36 to 46	79.5	10	12.8	0	73	5	
9/2/2024	20:00	0	0	1	2	5	17	32	16	4	0	0	0	0	0	0	0	0	0	77	40.8	45.3	35 to 45	72.7	13	16.9	0	74	3	
9/2/2024	21:00	0	0	0	2	16	39	30	7	1	1	0	0	0	0	0	0	0	0	96	38.5	41.9	32 to 42	78.1	6	6.3	0	90	6	
9/2/2024	22:00	0	2	0	1	8	20	25	10	0	0	0	0	0	0	0	0	0	0	66	38.9	44	34 to 44	72.7	7	10.6	1	64	1	
9/2/2024	23:00	0	0	0	4	8	20	18	9	1	1	0	0	0	0	0	0	0	0	61	39.1	44.5	36 to 46	70.5	7	11.5	0	60	1	
24 Hr Summary		0	3	3	29	142	395	360	115	12	5	1	1	0	0	0	0	0	0	1066	38.9	44	35 to 45	75.2	87	8.2	1	1023	42	

Outgoing Histogram

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

from Thu-Aug-29-2024-01-00-PM to Tue-Sep-03-2024-01-59-PM

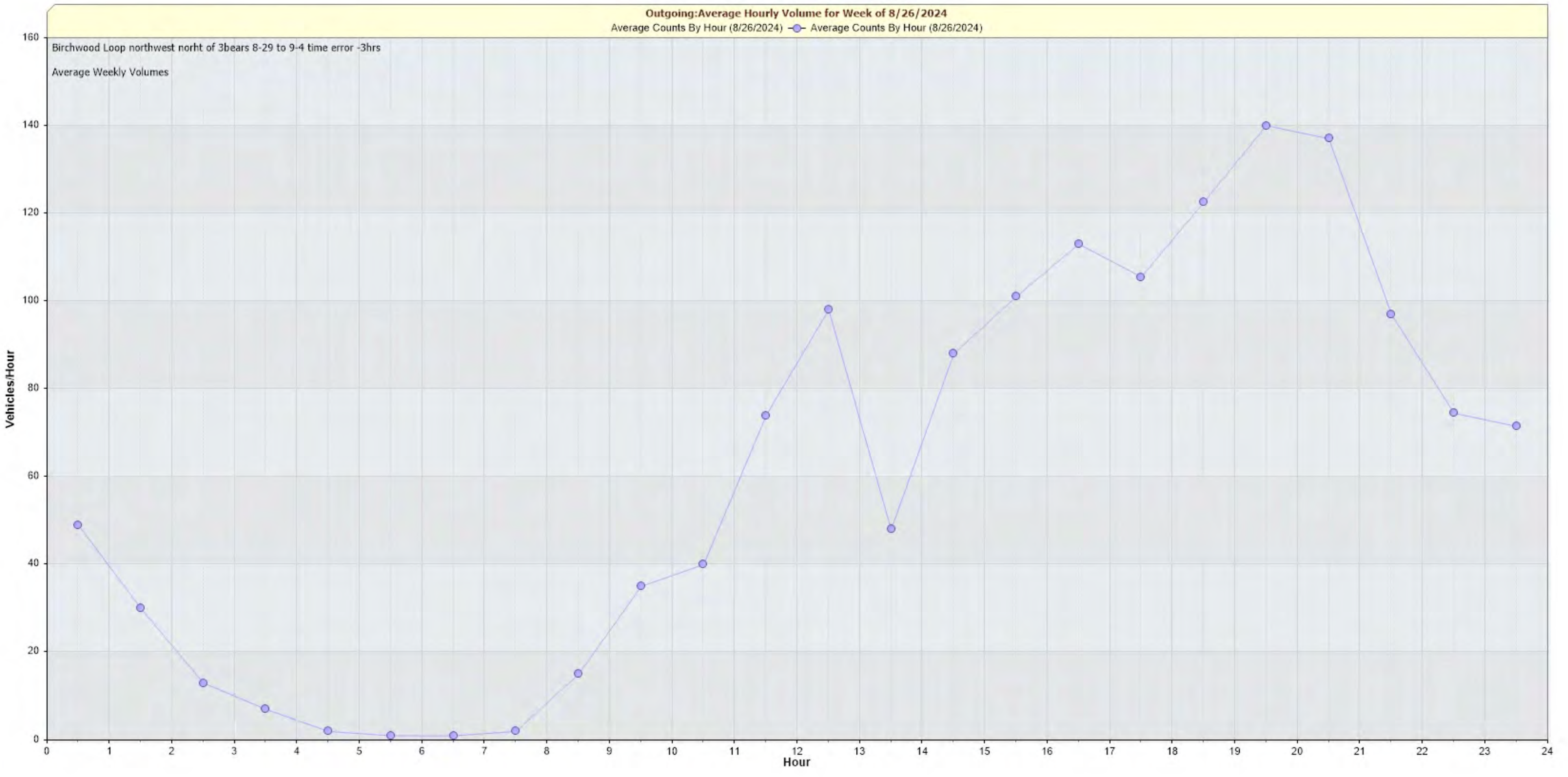
Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/3/2024	00:00	0	0	0	0	1	10	5	0	0	0	0	0	0	0	0	0	0	0	16	38.4	43	34 to 44	93.8	*	*	0	16	0	
9/3/2024	01:00	0	0	2	3	3	5	4	2	0	0	0	0	0	0	0	0	0	0	19	35.2	41.5	29 to 39	57.9	1	5.3	0	18	1	
9/3/2024	02:00	0	0	1	0	1	0	3	2	0	0	0	0	0	0	0	0	0	0	7	38.6	45	36 to 46	71.4	1	14.3	0	6	1	
9/3/2024	03:00	0	0	0	0	1	1	4	1	0	0	0	0	0	0	0	0	0	0	7	40.7	43	33 to 43	71.4	1	14.3	0	7	0	
9/3/2024	04:00	0	0	0	1	0	0	3	0	0	0	0	0	0	0	0	0	0	0	4	39.3	43	34 to 44	75.0	*	*	0	4	0	
9/3/2024	05:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	39	40	30 to 40	100.0	*	*	0	2	0	
9/3/2024	06:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	35.5	37	27 to 37	100.0	*	*	0	2	0	
9/3/2024	07:00	0	0	0	0	0	2	0	0	1	0	0	0	0	0	0	0	0	0	3	43	54	28 to 38	66.7	1	33.3	0	2	1	
9/3/2024	08:00	0	0	0	0	2	2	7	5	0	0	0	0	0	0	0	0	0	0	16	42.3	47.5	39 to 49	87.5	3	18.8	0	16	0	
9/3/2024	09:00	0	0	0	1	2	13	8	3	0	0	0	0	0	0	0	0	0	0	27	38.6	42.8	33 to 43	85.2	2	7.4	0	25	2	
9/3/2024	10:00	0	0	0	2	8	17	10	6	0	0	0	0	0	0	0	0	0	0	43	38.1	43	32 to 42	67.4	5	11.6	0	40	3	
9/3/2024	11:00	0	0	0	3	10	20	29	11	1	0	0	0	0	0	0	0	0	0	74	39.4	44.2	36 to 46	74.3	7	9.5	0	66	8	
9/3/2024	12:00	0	0	1	3	13	30	25	6	0	0	0	0	0	0	0	0	0	0	78	37.7	41.8	33 to 43	75.6	4	5.1	0	68	10	
9/3/2024	13:00	0	0	0	1	6	18	18	2	0	0	0	0	0	0	0	0	0	0	45	38.3	41	34 to 44	88.9	1	2.2	1	37	7	
9/3/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/3/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	4	14	48	120	117	38	2	0	0	0	0	0	0	0	0	0	343	38.5	44	35 to 45	73.2	26	7.6	1	309	33	

Outgoing: Average Hourly Volume for Week of 8/26/2024

Average Counts By Hour (8/26/2024) — Average Counts By Hour (8/26/2024)

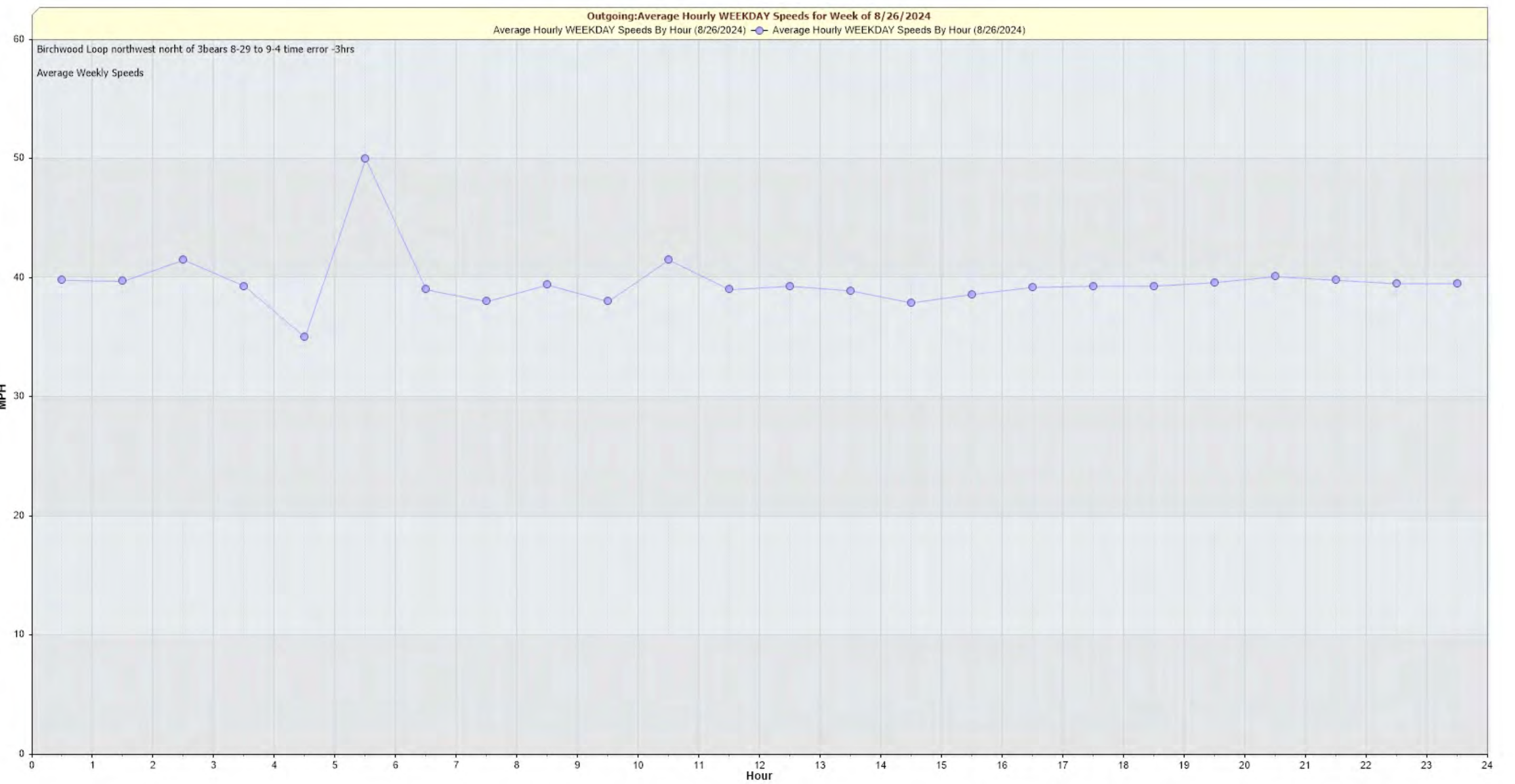
Birchwood Loop northwest north of 3bears 8-29 to 9-4 time error -3hrs

Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 8/26/2024
Average Hourly WEEKDAY Speeds By Hour (8/26/2024) — Average Hourly WEEKDAY Speeds By Hour (8/26/2024)

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs
Average Weekly Speeds

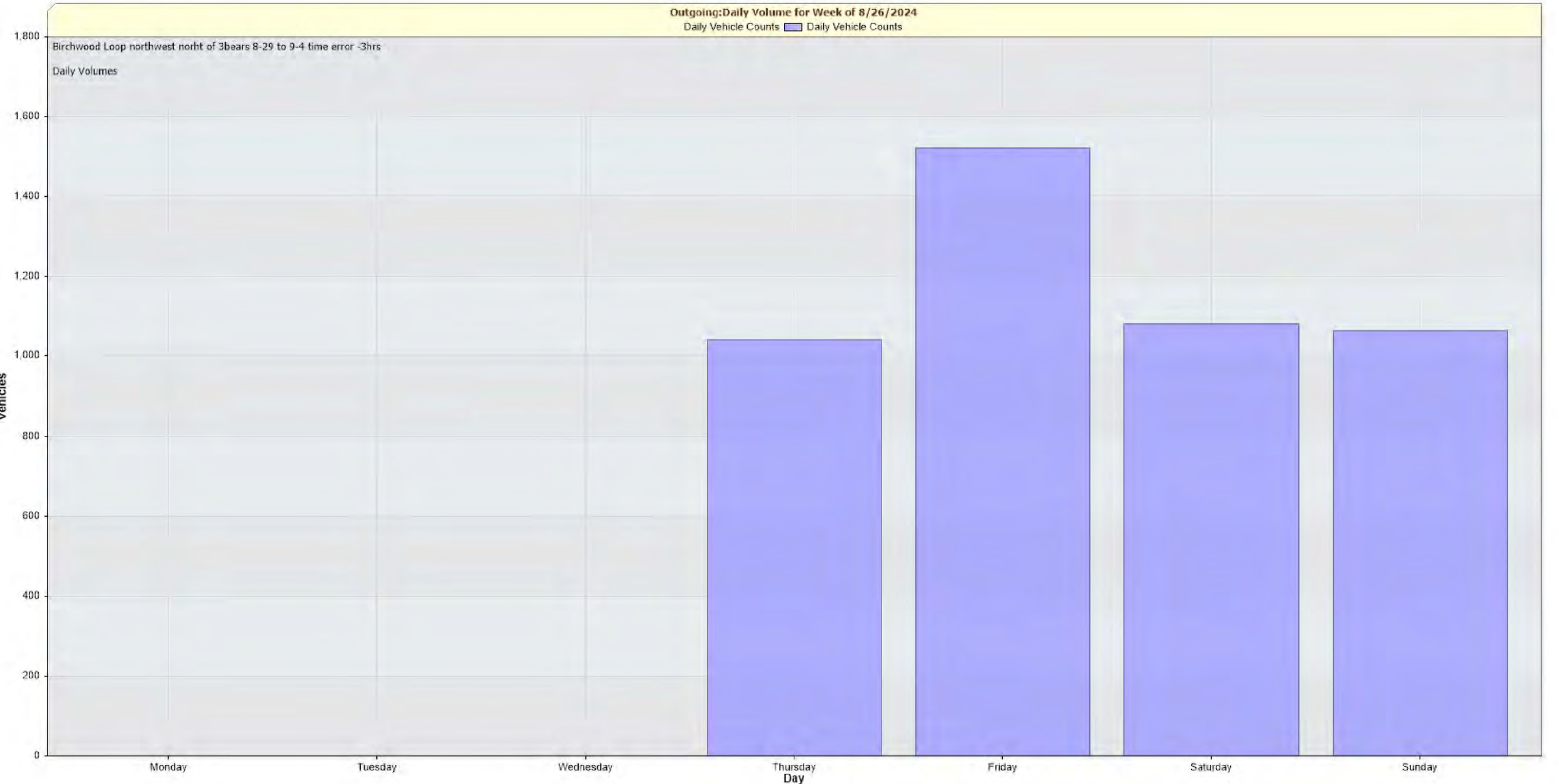


Outgoing: Daily Volume for Week of 8/26/2024

Daily Vehicle Counts

Birchwood Loop northwest norht of 3bears 8-29 to 9-4 time error -3hrs

Daily Volumes



For Project: N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:38:47 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 8/29/2024 2:00:00 PM through 9/4/2024 1:59:59 PM

85th Percentile Speed 37 MPH

85th Percentile Vehicles 10926

Max Speed 62 MPH on 9/1/2024 10:45:40 AM

Total Vehicles 12854

AADT: 2142

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1791	1836
AM Peak 11:00 AM	115	100
PM Peak 9:00 PM	212	190

Speed

Speed Limit: 45

85th Percentile Speed: 37

50th Percentile Speed: 31

10 MPH Pace Interval: 29.0 MPH to 39.0 MPH

Average Speed: 29.17

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	3	10	3	10	9	4	9
% over limit	0.2	0.4	0.5	0.6	0.4	0.2	0.5
Avg Speeder	47.7	47.8	49.0	46.5	48.1	48.0	49.9
Avg Speed	28.3	29.7	28.0	29.8	29.7	29.0	28.7

Class Counts

	Number	%
VEH_SM	1	0
VEH_MED	12090	94.1
VEH_LG	763	5.9
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Incoming Summary
 N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/29/2024 3:00:00 PM	37.0	15	18	40	0.0	0.0%	28.1
8/29/2024 4:00:00 PM	36.0	116	137	46	46.0	0.7%	28.4
8/29/2024 5:00:00 PM	38.0	134	158	46	46.0	0.6%	30.1
8/29/2024 6:00:00 PM	37.0	129	152	46	46.0	0.7%	30.6
8/29/2024 7:00:00 PM	36.0	156	183	45	0.0	0.0%	29.5
8/29/2024 8:00:00 PM	37.0	144	169	44	0.0	0.0%	29.8
8/29/2024 9:00:00 PM	37.0	184	216	46	46.0	0.5%	29.8
8/29/2024 10:00:00 PM	37.0	176	207	49	47.5	1.0%	29.8
8/29/2024 11:00:00 PM	38.0	156	183	44	0.0	0.0%	29.5
8/30/2024 12:00:00 AM	39.0	116	136	48	46.5	2.9%	30.5
8/30/2024 1:00:00 AM	36.0	88	103	42	0.0	0.0%	28.0
8/30/2024 2:00:00 AM	37.0	54	64	40	0.0	0.0%	28.5
8/30/2024 3:00:00 AM	36.0	37	44	45	0.0	0.0%	27.9
8/30/2024 4:00:00 AM	34.0	20	24	41	0.0	0.0%	27.0
8/30/2024 5:00:00 AM	36.0	8	10	49	49.0	10.0%	29.3
8/30/2024 6:00:00 AM	19.0	2	2	19	0.0	0.0%	18.5
8/30/2024 7:00:00 AM	27.0	3	4	28	0.0	0.0%	23.3
8/30/2024 8:00:00 AM	26.0	4	5	34	0.0	0.0%	23.8
8/30/2024 9:00:00 AM	34.0	8	9	37	0.0	0.0%	23.1
8/30/2024 10:00:00 AM	37.0	16	19	41	0.0	0.0%	26.0
8/30/2024 11:00:00 AM	36.0	41	48	44	0.0	0.0%	27.4
8/30/2024 12:00:00 PM	37.0	118	139	45	0.0	0.0%	28.5
8/30/2024 1:00:00 PM	37.0	103	121	42	0.0	0.0%	29.4
8/30/2024 2:00:00 PM	39.0	109	128	46	46.0	0.8%	30.2
8/30/2024 3:00:00 PM	37.0	109	128	45	0.0	0.0%	30.0
8/30/2024 4:00:00 PM	38.0	130	153	46	46.0	0.7%	31.0
8/30/2024 5:00:00 PM	37.0	130	153	43	0.0	0.0%	29.3
8/30/2024 6:00:00 PM	36.0	179	211	45	0.0	0.0%	29.9
8/30/2024 7:00:00 PM	37.0	162	190	41	0.0	0.0%	30.4
8/30/2024 8:00:00 PM	37.0	190	223	44	0.0	0.0%	30.1
8/30/2024 9:00:00 PM	37.0	205	241	47	47.0	0.4%	30.1
8/30/2024 10:00:00 PM	38.0	206	242	53	49.3	1.7%	31.3
8/30/2024 11:00:00 PM	37.0	139	163	48	48.0	0.6%	29.3
8/31/2024 12:00:00 AM	37.0	105	123	44	0.0	0.0%	28.9
8/31/2024 1:00:00 AM	36.0	99	116	45	0.0	0.0%	28.0
8/31/2024 2:00:00 AM	35.0	69	81	43	0.0	0.0%	27.0
8/31/2024 3:00:00 AM	36.0	37	43	41	0.0	0.0%	27.3
8/31/2024 4:00:00 AM	36.0	20	24	45	0.0	0.0%	28.0
8/31/2024 5:00:00 AM	38.0	11	13	39	0.0	0.0%	29.5
8/31/2024 6:00:00 AM	33.0	4	5	35	0.0	0.0%	27.0
8/31/2024 7:00:00 AM	36.0	8	9	40	0.0	0.0%	27.0
8/31/2024 8:00:00 AM	32.0	5	6	33	0.0	0.0%	26.3
8/31/2024 9:00:00 AM	28.0	5	6	32	0.0	0.0%	24.3
8/31/2024 10:00:00 AM	32.0	5	6	37	0.0	0.0%	30.5
8/31/2024 11:00:00 AM	35.0	16	19	39	0.0	0.0%	29.9
8/31/2024 12:00:00 PM	36.0	32	38	42	0.0	0.0%	28.7
8/31/2024 1:00:00 PM	36.0	48	57	49	49.0	1.8%	29.1

Incoming Summary
 N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

8/31/2024 2:00:00 PM	36.0	82	96	42	0.0	0.0%	29.2
8/31/2024 3:00:00 PM	37.0	96	113	47	47.0	0.9%	29.4
8/31/2024 4:00:00 PM	38.0	121	142	45	0.0	0.0%	30.1
8/31/2024 5:00:00 PM	37.0	128	151	44	0.0	0.0%	29.4
8/31/2024 6:00:00 PM	37.0	135	159	48	48.0	0.6%	30.0
8/31/2024 7:00:00 PM	37.0	133	156	44	0.0	0.0%	29.4
8/31/2024 8:00:00 PM	37.0	117	138	43	0.0	0.0%	29.2
8/31/2024 9:00:00 PM	36.0	131	154	44	0.0	0.0%	28.2
8/31/2024 10:00:00 PM	37.0	125	147	45	0.0	0.0%	28.6
8/31/2024 11:00:00 PM	37.0	111	131	45	0.0	0.0%	29.9
9/1/2024 12:00:00 AM	36.0	109	128	48	48.0	0.8%	28.3
9/1/2024 1:00:00 AM	37.0	78	92	49	48.0	2.2%	28.5
9/1/2024 2:00:00 AM	36.0	48	56	43	0.0	0.0%	28.3
9/1/2024 3:00:00 AM	35.0	31	37	45	0.0	0.0%	28.1
9/1/2024 4:00:00 AM	35.0	17	20	41	0.0	0.0%	26.3
9/1/2024 5:00:00 AM	34.0	8	10	36	0.0	0.0%	27.7
9/1/2024 6:00:00 AM	34.0	8	9	37	0.0	0.0%	25.2
9/1/2024 7:00:00 AM	33.0	3	3	33	0.0	0.0%	28.3
9/1/2024 8:00:00 AM	38.0	9	11	41	0.0	0.0%	32.5
9/1/2024 9:00:00 AM	33.0	8	10	36	0.0	0.0%	26.3
9/1/2024 10:00:00 AM	33.0	11	13	42	0.0	0.0%	25.8
9/1/2024 11:00:00 AM	41.0	17	20	62	58.0	10.0%	34.4
9/1/2024 12:00:00 PM	37.0	46	54	48	48.0	1.9%	28.8
9/1/2024 1:00:00 PM	38.0	79	93	44	0.0	0.0%	28.6
9/1/2024 2:00:00 PM	36.0	130	153	49	49.0	0.7%	28.4
9/1/2024 3:00:00 PM	36.0	121	142	44	0.0	0.0%	28.6
9/1/2024 4:00:00 PM	36.0	127	149	42	0.0	0.0%	28.9
9/1/2024 5:00:00 PM	37.0	119	140	47	47.0	0.7%	28.7
9/1/2024 6:00:00 PM	37.0	150	176	44	0.0	0.0%	30.0
9/1/2024 7:00:00 PM	37.0	113	133	45	0.0	0.0%	29.9
9/1/2024 8:00:00 PM	38.0	103	121	46	46.0	0.8%	29.8
9/1/2024 9:00:00 PM	37.0	122	144	47	47.0	0.7%	28.7
9/1/2024 10:00:00 PM	36.0	122	143	44	0.0	0.0%	27.8
9/1/2024 11:00:00 PM	36.0	109	128	42	0.0	0.0%	27.7
9/2/2024 12:00:00 AM	35.0	85	100	45	0.0	0.0%	27.1
9/2/2024 1:00:00 AM	36.0	81	95	44	0.0	0.0%	27.1
9/2/2024 2:00:00 AM	36.0	37	44	45	0.0	0.0%	25.7
9/2/2024 3:00:00 AM	31.0	37	44	36	0.0	0.0%	23.3
9/2/2024 4:00:00 AM	36.0	24	28	40	0.0	0.0%	27.3
9/2/2024 5:00:00 AM	32.0	9	11	45	0.0	0.0%	26.1
9/2/2024 6:00:00 AM	38.0	2	2	38	0.0	0.0%	35.0
9/2/2024 7:00:00 AM	35.0	3	3	35	0.0	0.0%	28.7
9/2/2024 8:00:00 AM	28.0	3	4	29	0.0	0.0%	27.0
9/2/2024 9:00:00 AM	28.0	4	5	32	0.0	0.0%	23.4
9/2/2024 10:00:00 AM	34.0	6	7	35	0.0	0.0%	23.0
9/2/2024 11:00:00 AM	34.0	20	24	42	0.0	0.0%	28.2
9/2/2024 12:00:00 PM	39.0	17	20	42	0.0	0.0%	29.4
9/2/2024 1:00:00 PM	38.0	38	45	43	0.0	0.0%	30.4

Incoming Summary
 N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

9/2/2024 2:00:00 PM	37.0	60	70	45	0.0	0.0%	28.4
9/2/2024 3:00:00 PM	36.0	96	113	44	0.0	0.0%	29.0
9/2/2024 4:00:00 PM	36.0	108	127	44	0.0	0.0%	29.6
9/2/2024 5:00:00 PM	38.0	118	139	47	47.0	0.7%	31.4
9/2/2024 6:00:00 PM	35.0	133	157	43	0.0	0.0%	28.0
9/2/2024 7:00:00 PM	37.0	134	158	48	48.0	0.6%	28.6
9/2/2024 8:00:00 PM	36.0	144	169	44	0.0	0.0%	28.6
9/2/2024 9:00:00 PM	36.0	133	157	43	0.0	0.0%	28.0
9/2/2024 10:00:00 PM	37.0	118	139	48	48.0	0.7%	27.3
9/2/2024 11:00:00 PM	37.0	114	134	45	0.0	0.0%	27.4
9/3/2024 12:00:00 AM	36.0	88	103	45	0.0	0.0%	27.7
9/3/2024 1:00:00 AM	36.0	68	80	47	47.0	1.3%	27.6
9/3/2024 2:00:00 AM	34.0	31	37	45	0.0	0.0%	27.6
9/3/2024 3:00:00 AM	33.0	30	35	40	0.0	0.0%	26.3
9/3/2024 4:00:00 AM	36.0	14	16	38	0.0	0.0%	27.1
9/3/2024 5:00:00 AM	38.0	8	10	40	0.0	0.0%	30.9
9/3/2024 6:00:00 AM	29.0	3	4	30	0.0	0.0%	26.5
9/3/2024 7:00:00 AM	33.0	5	6	35	0.0	0.0%	27.7
9/3/2024 8:00:00 AM	30.0	5	6	35	0.0	0.0%	24.3
9/3/2024 9:00:00 AM	32.0	8	10	36	0.0	0.0%	27.4
9/3/2024 10:00:00 AM	36.0	25	29	43	0.0	0.0%	26.5
9/3/2024 11:00:00 AM	37.0	43	51	46	46.0	2.0%	30.1
9/3/2024 12:00:00 PM	37.0	130	153	45	0.0	0.0%	28.6
9/3/2024 1:00:00 PM	37.0	99	117	46	46.0	0.9%	29.6
9/3/2024 2:00:00 PM	36.0	118	139	43	0.0	0.0%	29.4
9/3/2024 3:00:00 PM	36.0	110	130	44	0.0	0.0%	28.8
9/3/2024 4:00:00 PM	38.0	92	108	49	49.0	0.9%	31.2
9/3/2024 5:00:00 PM	37.0	130	153	43	0.0	0.0%	29.4
9/3/2024 6:00:00 PM	37.0	138	162	45	0.0	0.0%	30.3
9/3/2024 7:00:00 PM	37.0	167	197	45	0.0	0.0%	30.2
9/3/2024 8:00:00 PM	37.0	177	208	48	47.0	1.0%	31.0
9/3/2024 9:00:00 PM	37.0	194	228	46	46.0	0.4%	30.1
9/3/2024 10:00:00 PM	38.0	222	261	55	50.5	0.8%	30.7
9/3/2024 11:00:00 PM	37.0	157	185	49	49.0	0.5%	31.1
9/4/2024 12:00:00 AM	35.0	110	130	40	0.0	0.0%	28.3
9/4/2024 1:00:00 AM	37.0	98	115	44	0.0	0.0%	29.8
9/4/2024 2:00:00 AM	33.0	44	52	50	50.0	1.9%	27.8
9/4/2024 3:00:00 AM	38.0	25	29	50	50.0	3.4%	30.8
9/4/2024 4:00:00 AM	37.0	14	16	41	0.0	0.0%	29.7
9/4/2024 5:00:00 AM	36.0	6	7	41	0.0	0.0%	28.3
9/4/2024 6:00:00 AM	16.0	1	1	16	0.0	0.0%	16.0
9/4/2024 7:00:00 AM	30.0	3	3	30	0.0	0.0%	28.0
9/4/2024 8:00:00 AM	32.0	2	2	32	0.0	0.0%	24.0
9/4/2024 9:00:00 AM	35.0	9	11	36	0.0	0.0%	26.4
9/4/2024 10:00:00 AM	34.0	22	26	42	0.0	0.0%	24.8
9/4/2024 11:00:00 AM	36.0	40	47	47	47.0	2.1%	27.0
9/4/2024 12:00:00 PM	35.0	127	149	43	0.0	0.0%	27.6
9/4/2024 1:00:00 PM	35.0	94	111	45	0.0	0.0%	27.4

Incoming Summary

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

9/4/2024 2:00:00 PM	35.0	26	31	42	0.0	0.0%	27.1
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Incoming Summary

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/30/2024 12:00:00 AM	37.0	1325	1559	49	46.5	0.6%	29.8
8/31/2024 12:00:00 AM	37.0	2165	2547	53	48.1	0.4%	29.7
9/1/2024 12:00:00 AM	37.0	1647	1938	49	48.0	0.2%	29.0
9/2/2024 12:00:00 AM	37.0	1663	1957	62	49.9	0.5%	28.7
9/3/2024 12:00:00 AM	37.0	1528	1798	48	47.7	0.2%	28.3
9/4/2024 12:00:00 AM	37.0	2087	2455	55	47.8	0.4%	29.7
9/4/2024 1:59:59 PM	36.0	510	600	50	49.0	0.5%	28.0

Incoming Weekly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	103	116	92	103	104	35.8
1 - 2	*	*	*	*	64	81	56	64	68.5	36.7
2 - 3	*	*	*	*	44	43	37	44	40	35.8
3 - 4	*	*	*	*	24	24	20	24	22	34
4 - 5	*	*	*	*	10	13	10	10	11.5	36
5 - 6	*	*	*	*	2	5	9	2	7	19
6 - 7	*	*	*	*	4	9	3	4	6	27
7 - 8	*	*	*	*	5	6	11	5	8.5	26
8 - 9	*	*	*	*	9	6	10	9	8	34
9 - 10	*	*	*	*	19	6	13	19	9.5	36.5
10 - 11	*	*	*	*	48	19	20	48	19.5	36
11 - 12	*	*	*	*	139	38	54	139	46	36.2
12 - 13	*	*	*	*	121	57	93	121	75	36.8
13 - 14	*	*	*	*	128	96	153	128	124.5	38.2
14 - 15	*	*	*	18	128	113	142	73	127.5	36.6
15 - 16	*	*	*	137	153	142	149	145	145.5	37
16 - 17	*	*	*	158	153	151	140	155.5	145.5	36.85
17 - 18	*	*	*	152	211	159	176	181.5	167.5	36.5
18 - 19	*	*	*	183	190	156	133	186.5	144.5	36.1
19 - 20	*	*	*	169	223	138	121	196	129.5	36.8
20 - 21	*	*	*	216	241	154	144	228.5	149	36.8
21 - 22	*	*	*	207	242	147	143	224.5	145	37.2
22 - 23	*	*	*	183	163	131	128	173	129.5	37
23 - 24	*	*	*	136	123	128	100	129.5	114	37.9
Totals	0	0	0	1559	2547	1938	1957			
% of Total	0%	0%	0%	19.49%	31.83%	24.22%	24.46%			

Incoming Weekly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	95	80	115	*	*	*	*	96.67	0	36.23
1 - 2	44	37	52	*	*	*	*	44.33	0	34.17
2 - 3	44	35	29	*	*	*	*	36	0	33.5
3 - 4	28	16	16	*	*	*	*	20	0	36.17
4 - 5	11	10	7	*	*	*	*	9.33	0	35.33
5 - 6	2	4	1	*	*	*	*	2.33	0	27.67
6 - 7	3	6	3	*	*	*	*	4	0	32.67
7 - 8	4	6	2	*	*	*	*	4	0	30
8 - 9	5	10	11	*	*	*	*	8.67	0	31.67
9 - 10	7	29	26	*	*	*	*	20.67	0	34.67
10 - 11	24	51	47	*	*	*	*	40.67	0	35.5
11 - 12	20	153	149	*	*	*	*	107.33	0	36.8
12 - 13	45	117	111	*	*	*	*	91	0	36.57
13 - 14	70	139	31	*	*	*	*	80	0	35.73
14 - 15	113	130	*	*	*	*	*	121.5	0	35.75
15 - 16	127	108	*	*	*	*	*	117.5	0	36.85
16 - 17	139	153	*	*	*	*	*	146	0	36.75
17 - 18	157	162	*	*	*	*	*	159.5	0	35.9
18 - 19	158	197	*	*	*	*	*	177.5	0	36.2
19 - 20	169	208	*	*	*	*	*	188.5	0	36.4
20 - 21	157	228	*	*	*	*	*	192.5	0	35.95
21 - 22	139	261	*	*	*	*	*	200	0	36.85
22 - 23	134	185	*	*	*	*	*	159.5	0	36.6
23 - 24	103	130	*	*	*	*	*	116.5	0	35.45
Totals	1798	2455	600	0	0	0	0			
% of Total	37.05%	50.59%	12.36%	0%	0%	0%	0%			

Incoming Monthly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	103	116	*	103	116	35.8
1 - 2	*	*	*	*	64	81	*	64	81	36.7
2 - 3	*	*	*	*	44	43	*	44	43	35.8
3 - 4	*	*	*	*	24	24	*	24	24	34
4 - 5	*	*	*	*	10	13	*	10	13	36
5 - 6	*	*	*	*	2	5	*	2	5	19
6 - 7	*	*	*	*	4	9	*	4	9	27
7 - 8	*	*	*	*	5	6	*	5	6	26
8 - 9	*	*	*	*	9	6	*	9	6	34
9 - 10	*	*	*	*	19	6	*	19	6	36.5
10 - 11	*	*	*	*	48	19	*	48	19	36
11 - 12	*	*	*	*	139	38	*	139	38	36.2
12 - 13	*	*	*	*	121	57	*	121	57	36.8
13 - 14	*	*	*	*	128	96	*	128	96	38.2
14 - 15	*	*	*	18	128	113	*	73	113	36.6
15 - 16	*	*	*	137	153	142	*	145	142	37
16 - 17	*	*	*	158	153	151	*	155.5	151	36.85
17 - 18	*	*	*	152	211	159	*	181.5	159	36.5
18 - 19	*	*	*	183	190	156	*	186.5	156	36.1
19 - 20	*	*	*	169	223	138	*	196	138	36.8
20 - 21	*	*	*	216	241	154	*	228.5	154	36.8
21 - 22	*	*	*	207	242	147	*	224.5	147	37.2
22 - 23	*	*	*	183	163	131	*	173	131	37
23 - 24	*	*	*	136	123	128	*	129.5	128	37.9
Totals	0	0	0	1559	2547	1938	0			
% of Total	0%	0%	0%	25.79%	42.14%	32.06%	0%			

Incoming Monthly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Sep 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	95	80	115	*	*	*	92	96.67	92	36.23
1 - 2	44	37	52	*	*	*	56	44.33	56	34.17
2 - 3	44	35	29	*	*	*	37	36	37	33.5
3 - 4	28	16	16	*	*	*	20	20	20	36.17
4 - 5	11	10	7	*	*	*	10	9.33	10	35.33
5 - 6	2	4	1	*	*	*	9	2.33	9	27.67
6 - 7	3	6	3	*	*	*	3	4	3	32.67
7 - 8	4	6	2	*	*	*	11	4	11	30
8 - 9	5	10	11	*	*	*	10	8.67	10	31.67
9 - 10	7	29	26	*	*	*	13	20.67	13	34.67
10 - 11	24	51	47	*	*	*	20	40.67	20	35.5
11 - 12	20	153	149	*	*	*	54	107.33	54	36.8
12 - 13	45	117	111	*	*	*	93	91	93	36.57
13 - 14	70	139	31	*	*	*	153	80	153	35.73
14 - 15	113	130	*	*	*	*	142	121.5	142	35.75
15 - 16	127	108	*	*	*	*	149	117.5	149	36.85
16 - 17	139	153	*	*	*	*	140	146	140	36.75
17 - 18	157	162	*	*	*	*	176	159.5	176	35.9
18 - 19	158	197	*	*	*	*	133	177.5	133	36.2
19 - 20	169	208	*	*	*	*	121	188.5	121	36.4
20 - 21	157	228	*	*	*	*	144	192.5	144	35.95
21 - 22	139	261	*	*	*	*	143	200	143	36.85
22 - 23	134	185	*	*	*	*	128	159.5	128	36.6
23 - 24	103	130	*	*	*	*	100	116.5	100	35.45
Totals	1798	2455	600	0	0	0	1957			
% of Total	26.4%	36.05%	8.81%	0%	0%	0%	28.74%			

Incoming Weekly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	28	28.04	28.48	28	28.24	35.8
1 - 2	*	*	*	*	28.47	27.05	28.32	28.47	27.57	36.7
2 - 3	*	*	*	*	27.91	27.26	28.14	27.91	27.66	35.8
3 - 4	*	*	*	*	27	28	26.3	27	27.23	34
4 - 5	*	*	*	*	29.3	29.54	27.7	29.3	28.74	36
5 - 6	*	*	*	*	18.5	27	25.22	18.5	25.86	19
6 - 7	*	*	*	*	23.25	27	28.33	23.25	27.33	27
7 - 8	*	*	*	*	23.8	26.33	32.45	23.8	30.29	26
8 - 9	*	*	*	*	23.11	24.33	26.3	23.11	25.56	34
9 - 10	*	*	*	*	26	30.5	25.85	26	27.32	36.5
10 - 11	*	*	*	*	27.42	29.89	34.4	27.42	32.21	36
11 - 12	*	*	*	*	28.49	28.71	28.76	28.49	28.74	36.2
12 - 13	*	*	*	*	29.4	29.05	28.65	29.4	28.8	36.8
13 - 14	*	*	*	*	30.21	29.19	28.39	30.21	28.7	38.2
14 - 15	*	*	*	28.17	30.01	29.4	28.56	29.78	28.93	36.6
15 - 16	*	*	*	28.42	31.03	30.11	28.91	29.8	29.49	37
16 - 17	*	*	*	30.15	29.28	29.4	28.74	29.72	29.08	36.85
17 - 18	*	*	*	30.61	29.92	30.01	29.99	30.21	30	36.5
18 - 19	*	*	*	29.55	30.42	29.36	29.89	29.99	29.61	36.1
19 - 20	*	*	*	29.75	30.06	29.21	29.82	29.93	29.49	36.8
20 - 21	*	*	*	29.81	30.05	28.21	28.65	29.94	28.42	36.8
21 - 22	*	*	*	29.83	31.31	28.59	27.84	30.63	28.22	37.2
22 - 23	*	*	*	29.51	29.33	29.92	27.74	29.42	28.85	37
23 - 24	*	*	*	30.49	28.92	28.34	27.1	29.74	27.8	37.9
Totals	0	0	0	29.8	29.7	29	28.7			
% of Total	0%	0%	0%	25.43%	25.34%	24.74%	24.49%			

Incoming Weekly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	27.13	27.62	29.83	*	*	*	*	28.33	*	36.23
1 - 2	25.66	27.62	27.81	*	*	*	*	27.05	*	34.17
2 - 3	23.25	26.34	30.76	*	*	*	*	26.27	*	33.5
3 - 4	27.29	27.06	29.69	*	*	*	*	27.87	*	36.17
4 - 5	26.09	30.9	28.29	*	*	*	*	28.36	*	35.33
5 - 6	35	26.5	16	*	*	*	*	27.43	*	27.67
6 - 7	28.67	27.67	28	*	*	*	*	28	*	32.67
7 - 8	27	24.33	24	*	*	*	*	25.17	*	30
8 - 9	23.4	27.4	26.36	*	*	*	*	26.19	*	31.67
9 - 10	23	26.52	24.81	*	*	*	*	25.4	*	34.67
10 - 11	28.21	30.1	26.98	*	*	*	*	28.52	*	35.5
11 - 12	29.4	28.63	27.6	*	*	*	*	28.2	*	36.8
12 - 13	30.36	29.6	27.39	*	*	*	*	28.82	*	36.57
13 - 14	28.36	29.39	27.06	*	*	*	*	28.79	*	35.73
14 - 15	29.01	28.81	*	*	*	*	*	28.9	*	35.75
15 - 16	29.56	31.19	*	*	*	*	*	30.31	*	36.85
16 - 17	31.38	29.44	*	*	*	*	*	30.36	*	36.75
17 - 18	28.03	30.3	*	*	*	*	*	29.18	*	35.9
18 - 19	28.56	30.19	*	*	*	*	*	29.46	*	36.2
19 - 20	28.6	30.96	*	*	*	*	*	29.9	*	36.4
20 - 21	28	30.12	*	*	*	*	*	29.26	*	35.95
21 - 22	27.32	30.65	*	*	*	*	*	29.49	*	36.85
22 - 23	27.44	31.08	*	*	*	*	*	29.55	*	36.6
23 - 24	27.73	28.27	*	*	*	*	*	28.03	*	35.45
Totals	28.3	29.8	28	0	0	0	0			
% of Total	32.87%	34.61%	32.52%	0%	0%	0%	0%			

Incoming Monthly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	28	28.04	*	28	28.04	35.8
1 - 2	*	*	*	*	28.47	27.05	*	28.47	27.05	36.7
2 - 3	*	*	*	*	27.91	27.26	*	27.91	27.26	35.8
3 - 4	*	*	*	*	27	28	*	27	28	34
4 - 5	*	*	*	*	29.3	29.54	*	29.3	29.54	36
5 - 6	*	*	*	*	18.5	27	*	18.5	27	19
6 - 7	*	*	*	*	23.25	27	*	23.25	27	27
7 - 8	*	*	*	*	23.8	26.33	*	23.8	26.33	26
8 - 9	*	*	*	*	23.11	24.33	*	23.11	24.33	34
9 - 10	*	*	*	*	26	30.5	*	26	30.5	36.5
10 - 11	*	*	*	*	27.42	29.89	*	27.42	29.89	36
11 - 12	*	*	*	*	28.49	28.71	*	28.49	28.71	36.2
12 - 13	*	*	*	*	29.4	29.05	*	29.4	29.05	36.8
13 - 14	*	*	*	*	30.21	29.19	*	30.21	29.19	38.2
14 - 15	*	*	*	28.17	30.01	29.4	*	29.78	29.4	36.6
15 - 16	*	*	*	28.42	31.03	30.11	*	29.8	30.11	37
16 - 17	*	*	*	30.15	29.28	29.4	*	29.72	29.4	36.85
17 - 18	*	*	*	30.61	29.92	30.01	*	30.21	30.01	36.5
18 - 19	*	*	*	29.55	30.42	29.36	*	29.99	29.36	36.1
19 - 20	*	*	*	29.75	30.06	29.21	*	29.93	29.21	36.8
20 - 21	*	*	*	29.81	30.05	28.21	*	29.94	28.21	36.8
21 - 22	*	*	*	29.83	31.31	28.59	*	30.63	28.59	37.2
22 - 23	*	*	*	29.51	29.33	29.92	*	29.42	29.92	37
23 - 24	*	*	*	30.49	28.92	28.34	*	29.74	28.34	37.9
Totals	0	0	0	29.8	29.7	29	0			
% of Total	0%	0%	0%	33.67%	33.56%	32.77%	0%			

Incoming Monthly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	27.13	27.62	29.83	*	*	*	28.48	28.33	28.48	36.23
1 - 2	25.66	27.62	27.81	*	*	*	28.32	27.05	28.32	34.17
2 - 3	23.25	26.34	30.76	*	*	*	28.14	26.27	28.14	33.5
3 - 4	27.29	27.06	29.69	*	*	*	26.3	27.87	26.3	36.17
4 - 5	26.09	30.9	28.29	*	*	*	27.7	28.36	27.7	35.33
5 - 6	35	26.5	16	*	*	*	25.22	27.43	25.22	27.67
6 - 7	28.67	27.67	28	*	*	*	28.33	28	28.33	32.67
7 - 8	27	24.33	24	*	*	*	32.45	25.17	32.45	30
8 - 9	23.4	27.4	26.36	*	*	*	26.3	26.19	26.3	31.67
9 - 10	23	26.52	24.81	*	*	*	25.85	25.4	25.85	34.67
10 - 11	28.21	30.1	26.98	*	*	*	34.4	28.52	34.4	35.5
11 - 12	29.4	28.63	27.6	*	*	*	28.76	28.2	28.76	36.8
12 - 13	30.36	29.6	27.39	*	*	*	28.65	28.82	28.65	36.57
13 - 14	28.36	29.39	27.06	*	*	*	28.39	28.79	28.39	35.73
14 - 15	29.01	28.81	*	*	*	*	28.56	28.9	28.56	35.75
15 - 16	29.56	31.19	*	*	*	*	28.91	30.31	28.91	36.85
16 - 17	31.38	29.44	*	*	*	*	28.74	30.36	28.74	36.75
17 - 18	28.03	30.3	*	*	*	*	29.99	29.18	29.99	35.9
18 - 19	28.56	30.19	*	*	*	*	29.89	29.46	29.89	36.2
19 - 20	28.6	30.96	*	*	*	*	29.82	29.9	29.82	36.4
20 - 21	28	30.12	*	*	*	*	28.65	29.26	28.65	35.95
21 - 22	27.32	30.65	*	*	*	*	27.84	29.49	27.84	36.85
22 - 23	27.44	31.08	*	*	*	*	27.74	29.55	27.74	36.6
23 - 24	27.73	28.27	*	*	*	*	27.1	28.03	27.1	35.45
Totals	28.3	29.8	28	0	0	0	28.7			
% of Total	24.65%	25.96%	24.39%	0%	0%	0%	25%			

Incoming Weekly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	35.8	36	37	35.8	36.5	35.8
1 - 2	*	*	*	*	36.7	35	36	36.7	35.5	36.7
2 - 3	*	*	*	*	35.8	35.7	35	35.8	35.35	35.8
3 - 4	*	*	*	*	34	36	35	34	35.5	34
4 - 5	*	*	*	*	36	37.5	34	36	35.75	36
5 - 6	*	*	*	*	19	33	34	19	33.5	19
6 - 7	*	*	*	*	27	36	33	27	34.5	27
7 - 8	*	*	*	*	26	32	38	26	35	26
8 - 9	*	*	*	*	34	28	33	34	30.5	34
9 - 10	*	*	*	*	36.5	32	33	36.5	32.5	36.5
10 - 11	*	*	*	*	36	35	41	36	38	36
11 - 12	*	*	*	*	36.2	35.7	36.7	36.2	36.2	36.2
12 - 13	*	*	*	*	36.8	36	37.3	36.8	36.65	36.8
13 - 14	*	*	*	*	38.2	36	35.3	38.2	35.65	38.2
14 - 15	*	*	*	36.5	36.7	36.7	36	36.6	36.35	36.6
15 - 16	*	*	*	36	38	37.4	36	37	36.7	37
16 - 17	*	*	*	37.2	36.5	36.8	36.4	36.85	36.6	36.85
17 - 18	*	*	*	37	36	36.8	36.6	36.5	36.7	36.5
18 - 19	*	*	*	35.8	36.4	36.4	37	36.1	36.7	36.1
19 - 20	*	*	*	37	36.6	36.3	37.5	36.8	36.9	36.8
20 - 21	*	*	*	36.7	36.9	35.8	36.7	36.8	36.25	36.8
21 - 22	*	*	*	36.8	37.6	36	35.9	37.2	35.95	37.2
22 - 23	*	*	*	37.2	36.8	37	35.3	37	36.15	37
23 - 24	*	*	*	39	36.8	35.8	35	37.9	35.4	37.9
Totals	0	0	0	369.2	836.3	848.9	860.7			
% of Total	0%	0%	0%	12.67%	28.69%	29.12%	29.53%			

Incoming Weekly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	36	36	36.7	*	*	*	*	36.23	0	36.23
1 - 2	35.5	34	33	*	*	*	*	34.17	0	34.17
2 - 3	30	33	37.5	*	*	*	*	33.5	0	33.5
3 - 4	36	36	36.5	*	*	*	*	36.17	0	36.17
4 - 5	32	38	36	*	*	*	*	35.33	0	35.33
5 - 6	38	29	16	*	*	*	*	27.67	0	27.67
6 - 7	35	33	30	*	*	*	*	32.67	0	32.67
7 - 8	28	30	32	*	*	*	*	30	0	30
8 - 9	28	32	35	*	*	*	*	31.67	0	31.67
9 - 10	34	36	34	*	*	*	*	34.67	0	34.67
10 - 11	34	37	35.5	*	*	*	*	35.5	0	35.5
11 - 12	39	36.7	34.7	*	*	*	*	36.8	0	36.8
12 - 13	37.7	37	35	*	*	*	*	36.57	0	36.57
13 - 14	36.6	35.9	34.7	*	*	*	*	35.73	0	35.73
14 - 15	35.9	35.6	*	*	*	*	*	35.75	0	35.75
15 - 16	36	37.7	*	*	*	*	*	36.85	0	36.85
16 - 17	37.2	36.3	*	*	*	*	*	36.75	0	36.75
17 - 18	34.9	36.9	*	*	*	*	*	35.9	0	35.9
18 - 19	36.2	36.2	*	*	*	*	*	36.2	0	36.2
19 - 20	35.9	36.9	*	*	*	*	*	36.4	0	36.4
20 - 21	35.6	36.3	*	*	*	*	*	35.95	0	35.95
21 - 22	36.4	37.3	*	*	*	*	*	36.85	0	36.85
22 - 23	36.2	37	*	*	*	*	*	36.6	0	36.6
23 - 24	36	34.9	*	*	*	*	*	35.45	0	35.45
Totals	840.1	848.7	466.6	0	0	0	0			
% of Total	38.98%	39.38%	21.65%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	35.8	36	*	35.8	36	35.8
1 - 2	*	*	*	*	36.7	35	*	36.7	35	36.7
2 - 3	*	*	*	*	35.8	35.7	*	35.8	35.7	35.8
3 - 4	*	*	*	*	34	36	*	34	36	34
4 - 5	*	*	*	*	36	37.5	*	36	37.5	36
5 - 6	*	*	*	*	19	33	*	19	33	19
6 - 7	*	*	*	*	27	36	*	27	36	27
7 - 8	*	*	*	*	26	32	*	26	32	26
8 - 9	*	*	*	*	34	28	*	34	28	34
9 - 10	*	*	*	*	36.5	32	*	36.5	32	36.5
10 - 11	*	*	*	*	36	35	*	36	35	36
11 - 12	*	*	*	*	36.2	35.7	*	36.2	35.7	36.2
12 - 13	*	*	*	*	36.8	36	*	36.8	36	36.8
13 - 14	*	*	*	*	38.2	36	*	38.2	36	38.2
14 - 15	*	*	*	36.5	36.7	36.7	*	36.6	36.7	36.6
15 - 16	*	*	*	36	38	37.4	*	37	37.4	37
16 - 17	*	*	*	37.2	36.5	36.8	*	36.85	36.8	36.85
17 - 18	*	*	*	37	36	36.8	*	36.5	36.8	36.5
18 - 19	*	*	*	35.8	36.4	36.4	*	36.1	36.4	36.1
19 - 20	*	*	*	37	36.6	36.3	*	36.8	36.3	36.8
20 - 21	*	*	*	36.7	36.9	35.8	*	36.8	35.8	36.8
21 - 22	*	*	*	36.8	37.6	36	*	37.2	36	37.2
22 - 23	*	*	*	37.2	36.8	37	*	37	37	37
23 - 24	*	*	*	39	36.8	35.8	*	37.9	35.8	37.9

Incoming Monthly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	36	36	36.7	*	*	*	37	36.23	37	36.23
1 - 2	35.5	34	33	*	*	*	36	34.17	36	34.17
2 - 3	30	33	37.5	*	*	*	35	33.5	35	33.5
3 - 4	36	36	36.5	*	*	*	35	36.17	35	36.17
4 - 5	32	38	36	*	*	*	34	35.33	34	35.33
5 - 6	38	29	16	*	*	*	34	27.67	34	27.67
6 - 7	35	33	30	*	*	*	33	32.67	33	32.67
7 - 8	28	30	32	*	*	*	38	30	38	30
8 - 9	28	32	35	*	*	*	33	31.67	33	31.67
9 - 10	34	36	34	*	*	*	33	34.67	33	34.67
10 - 11	34	37	35.5	*	*	*	41	35.5	41	35.5
11 - 12	39	36.7	34.7	*	*	*	36.7	36.8	36.7	36.8
12 - 13	37.7	37	35	*	*	*	37.3	36.57	37.3	36.57
13 - 14	36.6	35.9	34.7	*	*	*	35.3	35.73	35.3	35.73
14 - 15	35.9	35.6	*	*	*	*	36	35.75	36	35.75
15 - 16	36	37.7	*	*	*	*	36	36.85	36	36.85
16 - 17	37.2	36.3	*	*	*	*	36.4	36.75	36.4	36.75
17 - 18	34.9	36.9	*	*	*	*	36.6	35.9	36.6	35.9
18 - 19	36.2	36.2	*	*	*	*	37	36.2	37	36.2
19 - 20	35.9	36.9	*	*	*	*	37.5	36.4	37.5	36.4
20 - 21	35.6	36.3	*	*	*	*	36.7	35.95	36.7	35.95
21 - 22	36.4	37.3	*	*	*	*	35.9	36.85	35.9	36.85
22 - 23	36.2	37	*	*	*	*	35.3	36.6	35.3	36.6
23 - 24	36	34.9	*	*	*	*	35	35.45	35	35.45

Summary of Violators

N Birchwood southeast pilots to on ramp 8-29 to 9-4 from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM
 time error -4hrs

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	601	28.2	3	47.7
01:00:00	334	27.5	1	50.0
02:00:00	232	27.1	1	50.0
03:00:00	128	27.5	0	0.0
04:00:00	61	28.7	1	49.0
05:00:00	23	25.7	0	0.0
06:00:00	28	27.0	0	0.0
07:00:00	34	27.5	0	0.0
08:00:00	51	25.5	0	0.0
09:00:00	100	25.9	0	0.0
10:00:00	209	29.0	4	52.3
11:00:00	553	28.4	1	48.0
12:00:00	544	28.9	2	47.5
13:00:00	617	29.0	2	47.5
14:00:00	644	29.1	1	47.0
15:00:00	816	29.8	3	47.0
16:00:00	894	29.7	3	46.7
17:00:00	1017	29.8	2	47.0
18:00:00	1017	29.7	1	48.0
19:00:00	1028	29.8	3	46.7
20:00:00	1140	29.3	4	46.5
21:00:00	1139	29.6	9	49.0
22:00:00	924	29.3	2	48.5
23:00:00	720	28.6	5	46.8

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	14:00	0	4	4	1	3	5	1	0	0	0	0	0	0	0	0	0	0	0	18	28.2	36.5	15 to 25	50.0	*	*	0	18	0	
8/29/2024	15:00	2	24	30	9	32	31	8	1	0	0	0	0	0	0	0	0	0	0	137	28.4	36	29 to 39	49.6	1	0.7	1	124	12	
8/29/2024	16:00	4	19	19	16	45	46	6	3	0	0	0	0	0	0	0	0	0	0	158	30.1	37.2	29 to 39	59.5	1	0.6	0	145	13	
8/29/2024	17:00	0	16	24	16	43	44	8	1	0	0	0	0	0	0	0	0	0	0	152	30.6	37	29 to 39	60.5	1	0.7	0	139	13	
8/29/2024	18:00	1	19	31	22	62	41	6	1	0	0	0	0	0	0	0	0	0	0	183	29.5	35.8	28 to 38	62.3	*	*	0	172	11	
8/29/2024	19:00	1	25	17	28	51	38	9	0	0	0	0	0	0	0	0	0	0	0	169	29.8	37	27 to 37	58.6	*	*	0	157	12	
8/29/2024	20:00	1	31	32	18	71	50	12	1	0	0	0	0	0	0	0	0	0	0	216	29.8	36.7	28 to 38	59.3	1	0.5	0	206	10	
8/29/2024	21:00	2	25	33	25	58	46	16	2	0	0	0	0	0	0	0	0	0	0	207	29.8	36.8	29 to 39	56.0	2	1.0	0	196	11	
8/29/2024	22:00	1	27	35	12	43	55	10	0	0	0	0	0	0	0	0	0	0	0	183	29.5	37.2	30 to 40	54.6	*	*	0	173	10	
8/29/2024	23:00	0	20	20	13	34	29	16	4	0	0	0	0	0	0	0	0	0	0	136	30.5	39	31 to 41	53.7	4	2.9	0	130	6	
24 Hr Summary		12	210	245	160	442	385	92	13	0	0	0	0	0	0	0	0	0	0	1559	29.8	37	29 to 39	56.7	10	0.6	1	1460	98	

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/30/2024	00:00	1	19	20	10	25	23	5	0	0	0	0	0	0	0	0	0	0	0	103	28	35.8	28 to 38	51.5	*	*	0	100	3
8/30/2024	01:00	0	8	16	11	11	17	1	0	0	0	0	0	0	0	0	0	0	0	64	28.5	36.7	19 to 29	51.6	*	*	0	59	5
8/30/2024	02:00	1	9	8	4	8	12	1	1	0	0	0	0	0	0	0	0	0	0	44	27.9	35.8	27 to 37	47.7	*	*	0	44	0
8/30/2024	03:00	0	5	6	3	6	3	1	0	0	0	0	0	0	0	0	0	0	0	24	27	34	15 to 25	50.0	*	*	0	23	1
8/30/2024	04:00	0	3	1	1	1	3	0	1	0	0	0	0	0	0	0	0	0	0	10	29.3	36	28 to 38	50.0	1	10.0	0	10	0
8/30/2024	05:00	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	18.5	19	9 to 19	100.0	*	*	0	2	0
8/30/2024	06:00	1	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	23.3	27	18 to 28	75.0	*	*	0	4	0
8/30/2024	07:00	0	1	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	5	23.8	26	16 to 26	80.0	*	*	0	5	0
8/30/2024	08:00	0	3	4	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	9	23.1	34	13 to 23	77.8	*	*	0	9	0
8/30/2024	09:00	0	6	5	2	1	4	1	0	0	0	0	0	0	0	0	0	0	0	19	26	36.5	14 to 24	57.9	*	*	0	19	0
8/30/2024	10:00	1	7	15	2	11	9	3	0	0	0	0	0	0	0	0	0	0	0	48	27.4	36	14 to 24	47.9	*	*	0	44	4
8/30/2024	11:00	0	19	36	15	30	29	8	2	0	0	0	0	0	0	0	0	0	0	139	28.5	36.2	27 to 37	48.2	*	*	0	130	9
8/30/2024	12:00	1	18	21	10	29	34	8	0	0	0	0	0	0	0	0	0	0	0	121	29.4	36.8	30 to 40	55.4	*	*	0	112	9
8/30/2024	13:00	1	15	24	10	30	32	15	1	0	0	0	0	0	0	0	0	0	0	128	30.2	38.2	31 to 41	53.9	1	0.8	0	118	10
8/30/2024	14:00	3	12	22	12	38	33	6	2	0	0	0	0	0	0	0	0	0	0	128	30	36.7	28 to 38	57.8	*	*	0	115	13
8/30/2024	15:00	0	17	19	16	40	44	16	1	0	0	0	0	0	0	0	0	0	0	153	31	38	30 to 40	59.5	1	0.7	0	142	11
8/30/2024	16:00	1	23	27	11	44	39	8	0	0	0	0	0	0	0	0	0	0	0	153	29.3	36.5	29 to 39	57.5	*	*	0	143	10
8/30/2024	17:00	1	27	27	27	61	59	8	1	0	0	0	0	0	0	0	0	0	0	211	29.9	36	29 to 39	60.7	*	*	0	192	19
8/30/2024	18:00	0	16	29	23	65	50	7	0	0	0	0	0	0	0	0	0	0	0	190	30.4	36.4	27 to 37	66.3	*	*	0	180	10
8/30/2024	19:00	0	18	45	22	75	46	17	0	0	0	0	0	0	0	0	0	0	0	223	30.1	36.6	28 to 38	56.5	*	*	0	209	14
8/30/2024	20:00	1	33	38	19	70	61	17	2	0	0	0	0	0	0	0	0	0	0	241	30.1	36.9	29 to 39	56.4	1	0.4	0	225	16
8/30/2024	21:00	0	27	30	15	74	69	21	5	1	0	0	0	0	0	0	0	0	0	242	31.3	37.6	30 to 40	63.2	4	1.7	0	228	14
8/30/2024	22:00	2	25	29	12	41	46	6	2	0	0	0	0	0	0	0	0	0	0	163	29.3	36.8	29 to 39	55.8	1	0.6	0	157	6
8/30/2024	23:00	0	21	24	9	34	27	8	0	0	0	0	0	0	0	0	0	0	0	123	28.9	36.8	30 to 40	52.0	*	*	0	114	9
24 Hr Summary		14	334	448	238	696	641	157	18	1	0	0	0	0	0	0	0	0	0	2547	29.7	37	29 to 39	55.0	9	0.4	0	2384	163

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/31/2024	00:00	0	22	25	10	35	14	9	1	0	0	0	0	0	0	0	0	0	0	116	28	36	30 to 40	46.6	*	*	0	110	6
8/31/2024	01:00	1	15	21	10	20	8	6	0	0	0	0	0	0	0	0	0	0	0	81	27	35	16 to 26	49.4	*	*	0	77	4
8/31/2024	02:00	0	6	13	4	11	8	1	0	0	0	0	0	0	0	0	0	0	0	43	27.3	35.7	18 to 28	48.8	*	*	0	42	1
8/31/2024	03:00	0	6	4	3	5	3	2	1	0	0	0	0	0	0	0	0	0	0	24	28	36	17 to 27	50.0	*	*	0	24	0
8/31/2024	04:00	1	1	2	1	3	5	0	0	0	0	0	0	0	0	0	0	0	0	13	29.5	37.5	28 to 38	61.5	*	*	0	12	1
8/31/2024	05:00	0	2	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	5	27	33	25 to 35	60.0	*	*	0	5	0
8/31/2024	06:00	0	3	1	0	3	1	1	0	0	0	0	0	0	0	0	0	0	0	9	27	36	30 to 40	55.6	*	*	0	9	0
8/31/2024	07:00	0	2	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	6	26.3	32	23 to 33	66.7	*	*	0	6	0
8/31/2024	08:00	0	2	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	6	24.3	28	18 to 28	83.3	*	*	0	5	1
8/31/2024	09:00	0	0	1	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	6	30.5	32	27 to 37	83.3	*	*	0	5	1
8/31/2024	10:00	0	1	4	3	7	4	0	0	0	0	0	0	0	0	0	0	0	0	19	29.9	35	29 to 39	73.7	*	*	0	16	3
8/31/2024	11:00	1	7	6	2	9	12	1	0	0	0	0	0	0	0	0	0	0	0	38	28.7	35.7	28 to 38	57.9	*	*	0	34	4
8/31/2024	12:00	0	10	11	2	22	6	4	2	0	0	0	0	0	0	0	0	0	0	57	29.1	36	30 to 40	54.4	1	1.8	0	54	3
8/31/2024	13:00	0	11	21	6	30	22	6	0	0	0	0	0	0	0	0	0	0	0	96	29.2	36	30 to 40	58.3	*	*	0	92	4
8/31/2024	14:00	1	17	20	8	35	25	6	1	0	0	0	0	0	0	0	0	0	0	113	29.4	36.7	29 to 39	55.8	1	0.9	0	106	7
8/31/2024	15:00	0	19	25	10	39	38	10	1	0	0	0	0	0	0	0	0	0	0	142	30.1	37.4	30 to 40	58.5	*	*	0	132	10
8/31/2024	16:00	0	20	30	11	43	38	9	0	0	0	0	0	0	0	0	0	0	0	151	29.4	36.8	30 to 40	57.6	*	*	0	144	7
8/31/2024	17:00	0	22	22	21	43	39	11	1	0	0	0	0	0	0	0	0	0	0	159	30	36.8	27 to 37	57.2	1	0.6	0	153	6
8/31/2024	18:00	0	24	26	14	49	33	10	0	0	0	0	0	0	0	0	0	0	0	156	29.4	36.4	30 to 40	55.8	*	*	0	148	8
8/31/2024	19:00	2	20	19	21	36	36	4	0	0	0	0	0	0	0	0	0	0	0	138	29.2	36.3	29 to 39	55.8	*	*	0	132	6
8/31/2024	20:00	0	28	34	18	28	37	9	0	0	0	0	0	0	0	0	0	0	0	154	28.2	35.8	27 to 37	46.8	*	*	0	142	12
8/31/2024	21:00	0	26	31	13	38	31	7	1	0	0	0	0	0	0	0	0	0	0	147	28.6	36	30 to 40	48.3	*	*	0	138	9
8/31/2024	22:00	0	17	23	9	38	30	13	1	0	0	0	0	0	0	0	0	0	0	131	29.9	37	30 to 40	58.8	*	*	0	125	6
8/31/2024	23:00	2	20	26	9	36	29	5	1	0	0	0	0	0	0	0	0	0	0	128	28.3	35.8	30 to 40	53.1	1	0.8	0	123	5
24 Hr Summary		8	301	366	178	540	421	114	10	0	0	0	0	0	0	0	0	0	0	1938	29	37	30 to 40	52.6	4	0.2	0	1834	104

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/1/2024	00:00	0	20	20	6	20	16	7	3	0	0	0	0	0	0	0	0	0	0	92	28.5	37	17 to 27	44.6	2	2.2	0	88	4
9/1/2024	01:00	0	10	12	7	12	10	5	0	0	0	0	0	0	0	0	0	0	0	56	28.3	36	28 to 38	46.4	*	*	0	54	2
9/1/2024	02:00	0	8	6	6	8	6	2	1	0	0	0	0	0	0	0	0	0	0	37	28.1	35	27 to 37	48.6	*	*	0	36	1
9/1/2024	03:00	0	4	5	4	3	3	1	0	0	0	0	0	0	0	0	0	0	0	20	26.3	35	17 to 27	60.0	*	*	0	20	0
9/1/2024	04:00	0	2	2	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	10	27.7	34	26 to 36	60.0	*	*	0	10	0
9/1/2024	05:00	0	4	0	3	1	1	0	0	0	0	0	0	0	0	0	0	0	0	9	25.2	34	15 to 25	55.6	*	*	0	8	1
9/1/2024	06:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	28.3	33	23 to 33	100.0	*	*	0	3	0
9/1/2024	07:00	0	0	0	4	2	4	1	0	0	0	0	0	0	0	0	0	0	0	11	32.5	38	25 to 35	63.6	*	*	0	11	0
9/1/2024	08:00	0	2	2	3	1	2	0	0	0	0	0	0	0	0	0	0	0	0	10	26.3	33	16 to 26	60.0	*	*	0	10	0
9/1/2024	09:00	0	5	2	1	3	0	2	0	0	0	0	0	0	0	0	0	0	0	13	25.8	33	16 to 26	61.5	*	*	0	13	0
9/1/2024	10:00	0	0	5	1	4	5	3	0	1	0	1	0	0	0	0	0	0	0	20	34.4	41	31 to 41	55.0	2	10.0	0	19	1
9/1/2024	11:00	0	6	13	7	13	11	3	1	0	0	0	0	0	0	0	0	0	0	54	28.8	36.7	20 to 30	46.3	1	1.9	0	52	2
9/1/2024	12:00	0	15	20	9	27	15	7	0	0	0	0	0	0	0	0	0	0	0	93	28.6	37.3	29 to 39	47.3	*	*	0	88	5
9/1/2024	13:00	0	23	26	28	49	22	4	1	0	0	0	0	0	0	0	0	0	0	153	28.4	35.3	27 to 37	57.5	1	0.7	0	148	5
9/1/2024	14:00	1	27	19	21	41	26	7	0	0	0	0	0	0	0	0	0	0	0	142	28.6	36	28 to 38	57.0	*	*	0	137	5
9/1/2024	15:00	0	24	27	11	46	34	7	0	0	0	0	0	0	0	0	0	0	0	149	28.9	36	30 to 40	56.4	*	*	0	144	5
9/1/2024	16:00	1	24	32	6	34	34	8	1	0	0	0	0	0	0	0	0	0	0	140	28.7	36.4	30 to 40	50.7	1	0.7	0	134	6
9/1/2024	17:00	1	21	28	13	52	50	11	0	0	0	0	0	0	0	0	0	0	0	176	30	36.6	30 to 40	61.4	*	*	0	170	6
9/1/2024	18:00	1	17	20	15	28	41	10	1	0	0	0	0	0	0	0	0	0	0	133	29.9	37	30 to 40	55.6	*	*	0	127	6
9/1/2024	19:00	1	21	16	7	37	24	14	1	0	0	0	0	0	0	0	0	0	0	121	29.8	37.5	31 to 41	55.4	1	0.8	0	116	5
9/1/2024	20:00	0	23	34	9	40	31	6	1	0	0	0	0	0	0	0	0	0	0	144	28.7	36.7	29 to 39	52.1	1	0.7	0	137	7
9/1/2024	21:00	1	22	36	15	33	29	7	0	0	0	0	0	0	0	0	0	0	0	143	27.8	35.9	27 to 37	47.6	*	*	0	136	7
9/1/2024	22:00	0	22	35	8	34	26	3	0	0	0	0	0	0	0	0	0	0	0	128	27.7	35.3	29 to 39	48.4	*	*	0	123	5
9/1/2024	23:00	0	18	29	13	21	14	4	1	0	0	0	0	0	0	0	0	0	0	100	27.1	35	18 to 28	52.0	*	*	0	96	4
24 Hr Summary		6	318	390	198	514	406	112	11	1	0	1	0	0	0	0	0	0	0	1957	28.7	37	28 to 38	50.1	9	0.5	0	1880	77

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/2/2024	00:00	2	19	23	8	26	12	5	0	0	0	0	0	0	0	0	0	0	0	95	27.1	36	14 to 24	46.3	*	*	0	91	4
9/2/2024	01:00	1	11	14	5	4	6	2	1	0	0	0	0	0	0	0	0	0	0	44	25.7	35.5	14 to 24	59.1	*	*	0	44	0
9/2/2024	02:00	0	17	12	4	8	3	0	0	0	0	0	0	0	0	0	0	0	0	44	23.3	30	15 to 25	68.2	*	*	0	43	1
9/2/2024	03:00	0	3	9	5	6	3	2	0	0	0	0	0	0	0	0	0	0	0	28	27.3	36	22 to 32	57.1	*	*	0	28	0
9/2/2024	04:00	0	1	6	1	1	0	1	1	0	0	0	0	0	0	0	0	0	0	11	26.1	32	15 to 25	72.7	*	*	0	10	1
9/2/2024	05:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	35	38	28 to 38	100.0	*	*	0	2	0
9/2/2024	06:00	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	28.7	35	17 to 27	66.7	*	*	0	3	0
9/2/2024	07:00	0	0	0	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	27	28	19 to 29	100.0	*	*	0	4	0
9/2/2024	08:00	0	2	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	5	23.4	28	18 to 28	80.0	*	*	0	5	0
9/2/2024	09:00	0	4	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	7	23	34	10 to 20	71.4	*	*	0	6	1
9/2/2024	10:00	0	5	3	4	8	3	1	0	0	0	0	0	0	0	0	0	0	0	24	28.2	34	27 to 37	62.5	*	*	0	24	0
9/2/2024	11:00	0	4	3	2	4	4	3	0	0	0	0	0	0	0	0	0	0	0	20	29.4	39	30 to 40	50.0	*	*	0	18	2
9/2/2024	12:00	1	7	5	2	12	13	5	0	0	0	0	0	0	0	0	0	0	0	45	30.4	37.7	30 to 40	57.8	*	*	0	43	2
9/2/2024	13:00	0	12	17	4	15	18	3	1	0	0	0	0	0	0	0	0	0	0	70	28.4	36.6	28 to 38	50.0	*	*	0	65	5
9/2/2024	14:00	0	13	28	4	39	22	7	0	0	0	0	0	0	0	0	0	0	0	113	29	35.9	30 to 40	57.5	*	*	0	107	6
9/2/2024	15:00	2	16	23	11	31	35	9	0	0	0	0	0	0	0	0	0	0	0	127	29.6	36	28 to 38	56.7	*	*	0	118	9
9/2/2024	16:00	0	13	12	15	47	42	9	1	0	0	0	0	0	0	0	0	0	0	139	31.4	37.2	30 to 40	69.1	1	0.7	0	131	8
9/2/2024	17:00	0	22	39	17	40	33	6	0	0	0	0	0	0	0	0	0	0	0	157	28	34.9	28 to 38	51.0	*	*	0	148	9
9/2/2024	18:00	1	26	31	14	45	34	6	1	0	0	0	0	0	0	0	0	0	0	158	28.6	36.2	29 to 39	52.5	1	0.6	0	150	8
9/2/2024	19:00	0	28	34	18	42	40	7	0	0	0	0	0	0	0	0	0	0	0	169	28.6	35.9	29 to 39	52.1	*	*	0	164	5
9/2/2024	20:00	0	29	31	20	43	27	7	0	0	0	0	0	0	0	0	0	0	0	157	28	35.6	30 to 40	47.1	*	*	0	145	12
9/2/2024	21:00	1	33	33	10	28	23	9	2	0	0	0	0	0	0	0	0	0	0	139	27.3	36.4	15 to 25	48.9	1	0.7	0	133	6
9/2/2024	22:00	1	27	35	9	26	28	7	1	0	0	0	0	0	0	0	0	0	0	134	27.4	36.2	18 to 28	50.0	*	*	0	126	8
9/2/2024	23:00	0	18	26	12	23	18	5	1	0	0	0	0	0	0	0	0	0	0	103	27.7	36	17 to 27	44.7	*	*	0	100	3
24 Hr Summary		9	310	387	171	451	367	94	9	0	0	0	0	0	0	0	0	0	0	1798	28.3	37	30 to 40	48.1	3	0.2	0	1708	90

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/3/2024	00:00	1	15	20	6	20	12	4	2	0	0	0	0	0	0	0	0	0	0	80	27.6	36	17 to 27	51.3	1	1.3	0	76	4
9/3/2024	01:00	0	4	11	5	11	4	1	1	0	0	0	0	0	0	0	0	0	0	37	27.6	34	18 to 28	48.6	*	*	0	36	1
9/3/2024	02:00	0	6	8	11	5	4	1	0	0	0	0	0	0	0	0	0	0	0	35	26.3	33	18 to 28	57.1	*	*	0	33	2
9/3/2024	03:00	0	4	3	1	4	4	0	0	0	0	0	0	0	0	0	0	0	0	16	27.1	36	28 to 38	56.3	*	*	0	15	1
9/3/2024	04:00	0	1	1	2	3	2	1	0	0	0	0	0	0	0	0	0	0	0	10	30.9	38	23 to 33	60.0	*	*	0	10	0
9/3/2024	05:00	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26.5	29	19 to 29	75.0	*	*	0	4	0
9/3/2024	06:00	0	0	2	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	6	27.7	33	25 to 35	66.7	*	*	0	6	0
9/3/2024	07:00	0	3	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	6	24.3	30	16 to 26	66.7	*	*	0	6	0
9/3/2024	08:00	1	1	2	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	10	27.4	32	26 to 36	60.0	*	*	0	8	2
9/3/2024	09:00	0	5	10	5	2	4	3	0	0	0	0	0	0	0	0	0	0	0	29	26.5	36	17 to 27	58.6	*	*	0	28	1
9/3/2024	10:00	0	5	12	1	16	12	4	1	0	0	0	0	0	0	0	0	0	0	51	30.1	37	30 to 40	60.8	1	2.0	0	47	4
9/3/2024	11:00	1	15	43	14	38	36	5	1	0	0	0	0	0	0	0	0	0	0	153	28.6	36.7	30 to 40	51.0	*	*	0	145	8
9/3/2024	12:00	1	16	18	13	28	34	6	1	0	0	0	0	0	0	0	0	0	0	117	29.6	37	29 to 39	55.6	1	0.9	0	103	14
9/3/2024	13:00	1	11	29	18	37	39	4	0	0	0	0	0	0	0	0	0	0	0	139	29.4	35.9	28 to 38	58.3	*	*	0	127	12
9/3/2024	14:00	1	15	30	13	40	23	8	0	0	0	0	0	0	0	0	0	0	0	130	28.8	35.6	28 to 38	52.3	*	*	0	114	16
9/3/2024	15:00	1	7	16	13	29	31	10	1	0	0	0	0	0	0	0	0	0	0	108	31.2	37.7	30 to 40	61.1	1	0.9	0	98	10
9/3/2024	16:00	0	18	31	15	45	37	7	0	0	0	0	0	0	0	0	0	0	0	153	29.4	36.3	28 to 38	56.2	*	*	0	139	14
9/3/2024	17:00	0	11	31	18	48	44	9	1	0	0	0	0	0	0	0	0	0	0	162	30.3	36.9	28 to 38	59.3	*	*	0	153	9
9/3/2024	18:00	0	21	26	32	57	51	9	1	0	0	0	0	0	0	0	0	0	0	197	30.2	36.2	28 to 38	62.9	*	*	0	182	15
9/3/2024	19:00	0	19	25	22	68	60	12	2	0	0	0	0	0	0	0	0	0	0	208	31	36.9	29 to 39	65.9	2	1.0	0	191	17
9/3/2024	20:00	2	25	33	23	74	56	14	1	0	0	0	0	0	0	0	0	0	0	228	30.1	36.3	27 to 37	59.6	1	0.4	0	206	22
9/3/2024	21:00	0	20	48	29	80	62	19	2	0	1	0	0	0	0	0	0	0	0	261	30.7	37.3	29 to 39	57.9	2	0.8	0	247	14
9/3/2024	22:00	0	18	21	22	50	60	13	1	0	0	0	0	0	0	0	0	0	0	185	31.1	37	28 to 38	65.4	1	0.5	0	169	16
9/3/2024	23:00	0	18	28	17	40	25	2	0	0	0	0	0	0	0	0	0	0	0	130	28.3	34.9	29 to 39	53.8	*	*	0	121	9
24 Hr Summary		9	259	448	284	703	604	132	15	0	1	0	0	0	0	0	0	0	0	2455	29.7	37	29 to 39	56.3	10	0.4	0	2264	191

Incoming Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG		
9/4/2024	00:00	2	12	17	13	36	30	5	0	0	0	0	0	0	0	0	0	0	0	115	29.8	36.7	29 to 39	60.0	*	*	0	108	7		
9/4/2024	01:00	0	10	10	7	18	5	1	0	1	0	0	0	0	0	0	0	0	0	52	27.8	33	23 to 33	53.8	1	1.9	0	50	2		
9/4/2024	02:00	0	2	5	6	6	7	2	0	1	0	0	0	0	0	0	0	0	0	29	30.8	37.5	28 to 38	58.6	1	3.4	0	26	3		
9/4/2024	03:00	0	1	4	1	5	4	1	0	0	0	0	0	0	0	0	0	0	0	16	29.7	36.5	27 to 37	56.3	*	*	0	16	0		
9/4/2024	04:00	0	2	0	2	1	1	1	0	0	0	0	0	0	0	0	0	0	0	7	28.3	36	16 to 26	57.1	*	*	0	6	1		
9/4/2024	05:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	16	16	6 to 16	100.0	*	*	0	1	0		
9/4/2024	06:00	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	28	30	20 to 30	100.0	*	*	0	2	1		
9/4/2024	07:00	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24	32	6 to 16	50.0	*	*	0	2	0		
9/4/2024	08:00	0	3	2	0	3	3	0	0	0	0	0	0	0	0	0	0	0	0	11	26.4	35	26 to 36	54.5	*	*	0	11	0		
9/4/2024	09:00	0	5	11	5	1	3	1	0	0	0	0	0	0	0	0	0	0	0	26	24.8	34	17 to 27	76.9	*	*	0	26	0		
9/4/2024	10:00	1	7	16	3	11	6	1	2	0	0	0	0	0	0	0	0	0	0	47	27	35.5	14 to 24	48.9	1	2.1	0	45	2		
9/4/2024	11:00	0	27	34	13	49	22	4	0	0	0	0	0	0	0	0	0	0	0	149	27.6	34.7	29 to 39	50.3	*	*	0	139	10		
9/4/2024	12:00	0	19	29	18	24	14	6	1	0	0	0	0	0	0	0	0	0	0	111	27.4	35	17 to 27	48.6	*	*	0	99	12		
9/4/2024	13:00	0	5	10	3	6	5	2	0	0	0	0	0	0	0	0	0	0	0	31	27.1	34.7	13 to 23	48.4	*	*	0	29	2		
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/4/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		3	95	138	73	162	100	24	3	2	0	0	0	0	0	0	0	0	0	600	28	36	27 to 37	47.3	3	0.5	0	560	40		

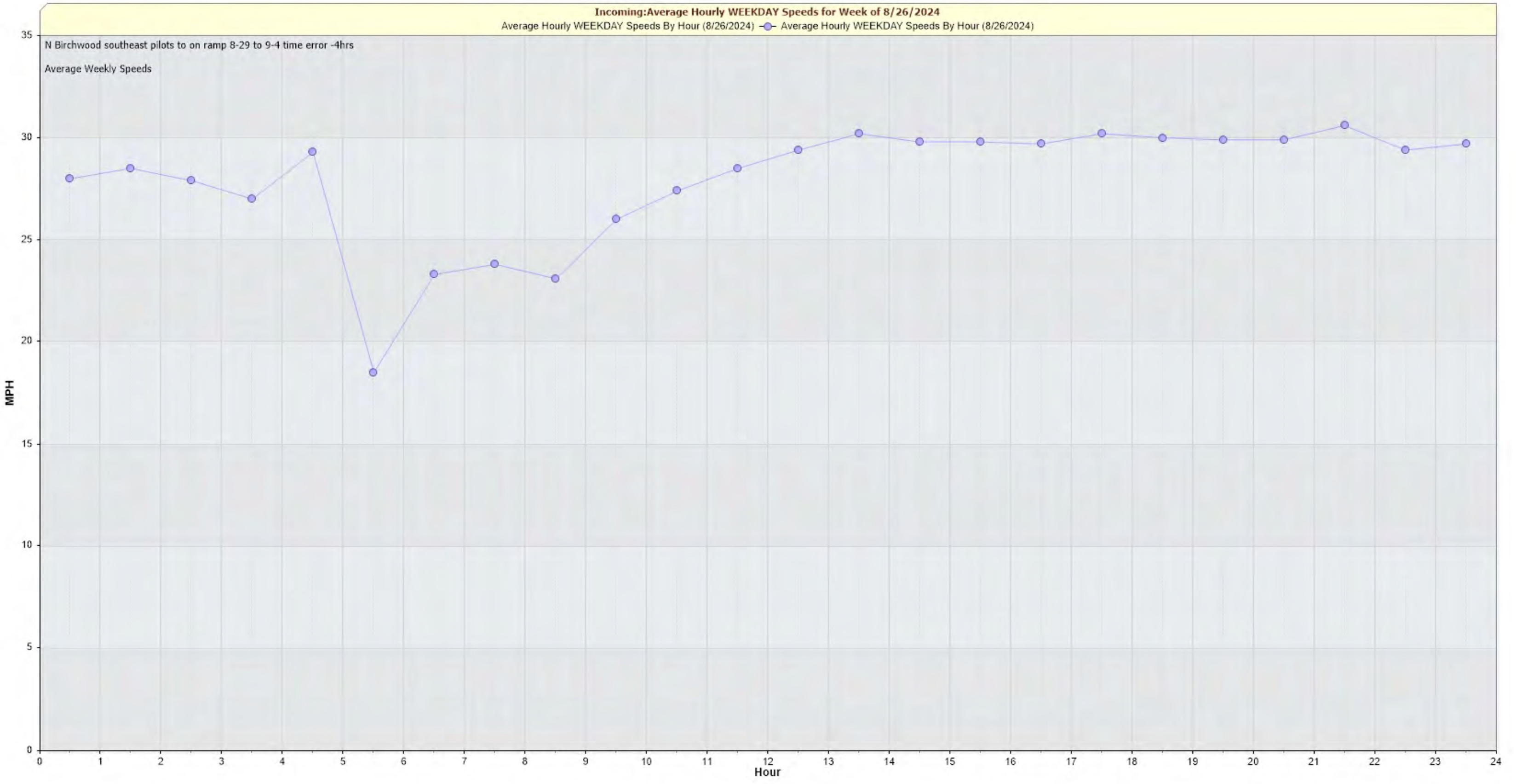
Incoming: Average Hourly Volume for Week of 8/26/2024
Average Counts By Hour (8/26/2024) — Average Counts By Hour (8/26/2024)

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs
Average Weekly Volumes



Incoming: Average Hourly WEEKDAY Speeds for Week of 8/26/2024
Average Hourly WEEKDAY Speeds By Hour (8/26/2024) — Average Hourly WEEKDAY Speeds By Hour (8/26/2024)

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs
Average Weekly Speeds

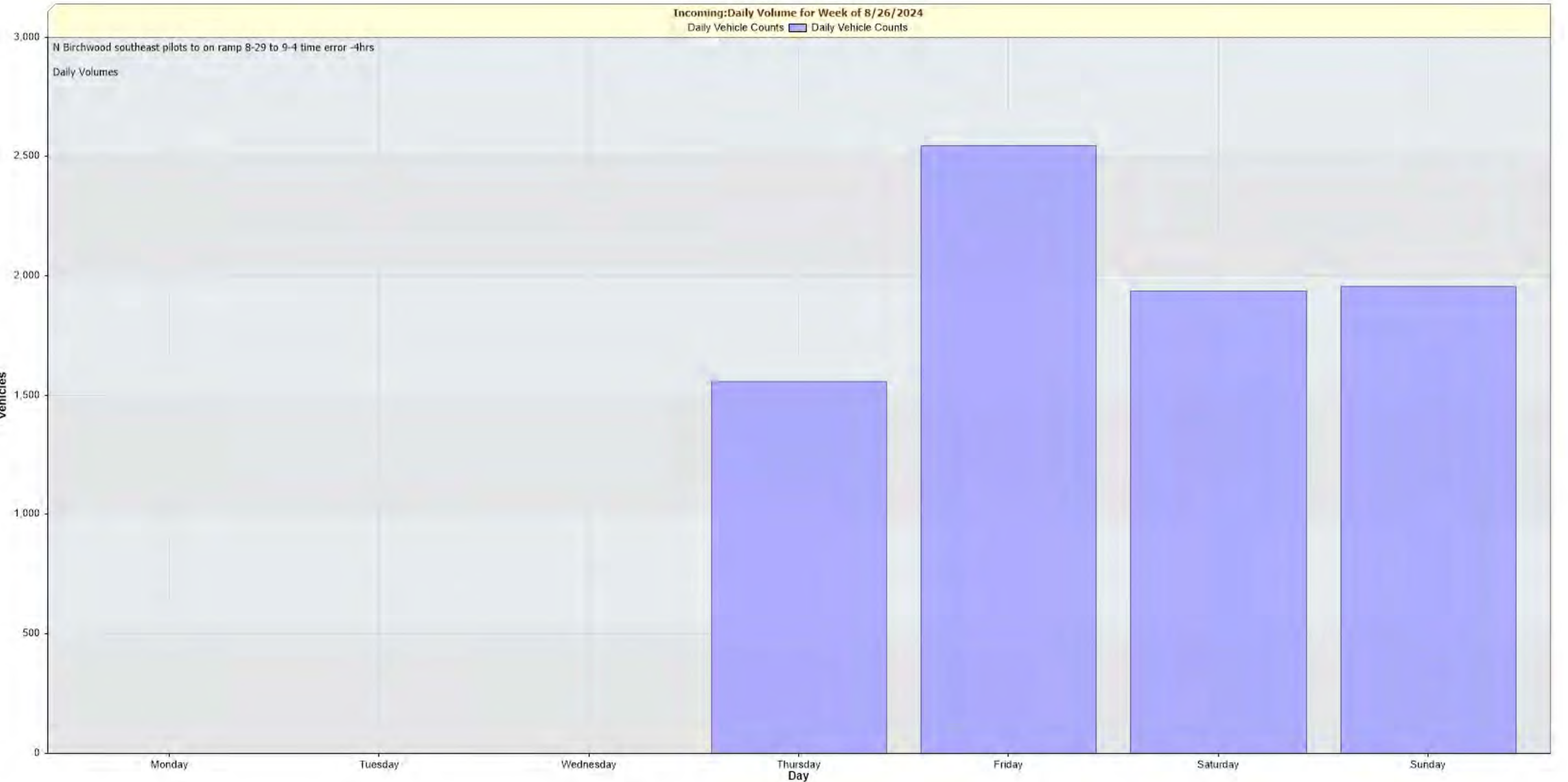


Incoming: Daily Volume for Week of 8/26/2024

Daily Vehicle Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

Daily Volumes



For Project: N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:38:47 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 8/29/2024 2:00:00 PM through 9/4/2024 1:59:59 PM

85th Percentile Speed 37 MPH

85th Percentile Vehicles 15024

Max Speed 70 MPH on 9/3/2024 1:43:06 PM

Total Vehicles 17675

AADT: 2945

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	2460	2525
AM Peak 11:00 AM	177	132
PM Peak 7:00 PM	285	259

Speed

Speed Limit: 45

85th Percentile Speed: 37

50th Percentile Speed: 31

10 MPH Pace Interval: 26.0 MPH to 36.0 MPH

Average Speed: 31.29

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	30	31	12	29	37	33	33
% over limit	1.2	0.9	1.3	1.4	1.1	1.3	1.2
Avg Speeder	48.0	49.5	48.1	48.1	47.7	48.6	47.5
Avg Speed	31.5	31.2	30.2	31.7	31.3	31.4	31.1

Class Counts

	Number	%
VEH_SM	83	0.5
VEH_MED	16868	95.4
VEH_LG	724	4.1
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Outgoing Summary
 N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/29/2024 3:00:00 PM	40.0	23	27	43	0.0	0.0%	33.3
8/29/2024 4:00:00 PM	38.0	151	178	52	49.7	1.7%	32.0
8/29/2024 5:00:00 PM	37.0	190	224	46	46.0	0.9%	31.2
8/29/2024 6:00:00 PM	39.0	161	189	50	47.2	2.6%	32.1
8/29/2024 7:00:00 PM	36.0	230	271	53	50.7	1.1%	29.9
8/29/2024 8:00:00 PM	36.0	240	282	49	46.8	1.4%	31.2
8/29/2024 9:00:00 PM	39.0	212	250	52	48.4	2.0%	32.3
8/29/2024 10:00:00 PM	39.0	221	260	52	48.0	1.5%	33.0
8/29/2024 11:00:00 PM	38.0	191	225	50	48.0	0.9%	31.9
8/30/2024 12:00:00 AM	38.0	156	184	48	48.0	0.5%	32.3
8/30/2024 1:00:00 AM	37.0	131	154	46	46.0	0.6%	30.8
8/30/2024 2:00:00 AM	38.0	66	78	45	0.0	0.0%	31.7
8/30/2024 3:00:00 AM	38.0	39	46	46	46.0	2.2%	29.7
8/30/2024 4:00:00 AM	35.0	18	21	44	0.0	0.0%	29.2
8/30/2024 5:00:00 AM	33.0	6	7	43	0.0	0.0%	31.0
8/30/2024 6:00:00 AM	32.0	8	9	37	0.0	0.0%	25.3
8/30/2024 7:00:00 AM	34.0	8	9	34	0.0	0.0%	25.1
8/30/2024 8:00:00 AM	31.0	7	8	33	0.0	0.0%	27.9
8/30/2024 9:00:00 AM	33.0	8	9	35	0.0	0.0%	27.2
8/30/2024 10:00:00 AM	36.0	25	29	38	0.0	0.0%	29.5
8/30/2024 11:00:00 AM	40.0	71	83	46	46.0	1.2%	31.5
8/30/2024 12:00:00 PM	35.0	183	215	48	47.5	0.9%	29.3
8/30/2024 1:00:00 PM	38.0	169	199	45	0.0	0.0%	31.1
8/30/2024 2:00:00 PM	38.0	151	178	46	46.0	2.2%	31.6
8/30/2024 3:00:00 PM	38.0	154	181	53	48.8	2.2%	31.0
8/30/2024 4:00:00 PM	38.0	188	221	46	46.0	0.5%	31.9
8/30/2024 5:00:00 PM	38.0	185	218	49	48.7	1.4%	31.3
8/30/2024 6:00:00 PM	37.0	206	242	51	51.0	0.4%	30.9
8/30/2024 7:00:00 PM	36.0	249	293	48	47.3	1.0%	31.3
8/30/2024 8:00:00 PM	38.0	267	314	56	51.7	1.0%	31.8
8/30/2024 9:00:00 PM	38.0	241	284	49	47.5	0.7%	32.2
8/30/2024 10:00:00 PM	38.0	273	321	48	46.8	1.6%	32.6
8/30/2024 11:00:00 PM	36.0	212	249	48	47.0	0.8%	30.9
8/31/2024 12:00:00 AM	37.0	129	152	48	47.5	2.6%	31.7
8/31/2024 1:00:00 AM	37.0	102	120	45	0.0	0.0%	31.7
8/31/2024 2:00:00 AM	37.0	85	100	48	47.0	2.0%	30.3
8/31/2024 3:00:00 AM	37.0	43	51	46	46.0	3.9%	29.0
8/31/2024 4:00:00 AM	37.0	22	26	47	47.0	7.7%	29.3
8/31/2024 5:00:00 AM	30.0	8	10	39	0.0	0.0%	27.8
8/31/2024 6:00:00 AM	28.0	9	11	37	0.0	0.0%	25.0
8/31/2024 7:00:00 AM	25.0	4	5	37	0.0	0.0%	25.8
8/31/2024 8:00:00 AM	33.0	3	4	36	0.0	0.0%	30.8
8/31/2024 9:00:00 AM	34.0	6	7	44	0.0	0.0%	31.3
8/31/2024 10:00:00 AM	29.0	6	7	37	0.0	0.0%	25.4
8/31/2024 11:00:00 AM	36.0	22	26	50	50.0	3.8%	30.5
8/31/2024 12:00:00 PM	36.0	38	45	45	0.0	0.0%	29.8
8/31/2024 1:00:00 PM	38.0	81	95	50	50.0	1.1%	31.2

Outgoing Summary
 N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

8/31/2024 2:00:00 PM	36.0	120	141	49	49.0	0.7%	31.0
8/31/2024 3:00:00 PM	38.0	156	183	49	47.5	1.1%	31.7
8/31/2024 4:00:00 PM	38.0	186	219	51	48.0	1.4%	31.8
8/31/2024 5:00:00 PM	37.0	175	206	52	48.3	1.5%	31.4
8/31/2024 6:00:00 PM	38.0	167	197	55	52.3	1.5%	31.4
8/31/2024 7:00:00 PM	37.0	207	244	42	0.0	0.0%	31.0
8/31/2024 8:00:00 PM	39.0	166	195	50	47.8	2.1%	32.2
8/31/2024 9:00:00 PM	38.0	180	212	51	49.0	1.4%	31.9
8/31/2024 10:00:00 PM	38.0	171	201	53	49.3	2.0%	31.9
8/31/2024 11:00:00 PM	38.0	145	171	51	49.5	1.2%	32.3
9/1/2024 12:00:00 AM	37.0	121	142	45	0.0	0.0%	31.4
9/1/2024 1:00:00 AM	38.0	106	125	48	47.3	2.4%	31.8
9/1/2024 2:00:00 AM	38.0	54	63	45	0.0	0.0%	31.2
9/1/2024 3:00:00 AM	35.0	58	68	45	0.0	0.0%	28.4
9/1/2024 4:00:00 AM	37.0	20	24	42	0.0	0.0%	27.4
9/1/2024 5:00:00 AM	32.0	11	13	45	0.0	0.0%	29.3
9/1/2024 6:00:00 AM	30.0	10	12	42	0.0	0.0%	25.8
9/1/2024 7:00:00 AM	37.0	5	6	38	0.0	0.0%	27.3
9/1/2024 8:00:00 AM	43.0	8	9	45	0.0	0.0%	35.3
9/1/2024 9:00:00 AM	27.0	3	4	32	0.0	0.0%	28.0
9/1/2024 10:00:00 AM	34.0	6	7	35	0.0	0.0%	28.3
9/1/2024 11:00:00 AM	36.0	20	23	42	0.0	0.0%	30.3
9/1/2024 12:00:00 PM	35.0	31	37	42	0.0	0.0%	29.6
9/1/2024 1:00:00 PM	40.0	80	94	47	47.0	1.1%	32.4
9/1/2024 2:00:00 PM	38.0	102	120	48	47.0	1.7%	31.9
9/1/2024 3:00:00 PM	36.0	207	244	49	49.0	0.4%	29.7
9/1/2024 4:00:00 PM	36.0	154	181	46	46.0	2.2%	30.6
9/1/2024 5:00:00 PM	36.0	269	317	47	47.0	0.6%	30.2
9/1/2024 6:00:00 PM	37.0	191	225	48	48.0	0.4%	31.7
9/1/2024 7:00:00 PM	39.0	172	202	51	47.5	2.0%	32.7
9/1/2024 8:00:00 PM	37.0	187	220	53	49.0	2.3%	31.5
9/1/2024 9:00:00 PM	37.0	205	241	51	49.7	1.2%	31.6
9/1/2024 10:00:00 PM	37.0	173	203	47	47.0	0.5%	31.7
9/1/2024 11:00:00 PM	37.0	156	184	48	47.0	1.1%	31.4
9/2/2024 12:00:00 AM	38.0	115	135	47	46.3	3.0%	31.8
9/2/2024 1:00:00 AM	36.0	118	139	45	0.0	0.0%	29.5
9/2/2024 2:00:00 AM	36.0	62	73	50	49.0	2.7%	30.4
9/2/2024 3:00:00 AM	38.0	33	39	49	49.0	2.6%	29.5
9/2/2024 4:00:00 AM	39.0	20	24	46	46.0	4.2%	31.0
9/2/2024 5:00:00 AM	35.0	7	8	37	0.0	0.0%	26.6
9/2/2024 6:00:00 AM	32.0	9	11	47	46.5	18.2%	29.7
9/2/2024 7:00:00 AM	36.0	5	6	37	0.0	0.0%	30.2
9/2/2024 8:00:00 AM	25.0	2	2	25	0.0	0.0%	24.0
9/2/2024 9:00:00 AM	31.0	3	4	33	0.0	0.0%	28.0
9/2/2024 10:00:00 AM	29.0	8	10	42	0.0	0.0%	25.8
9/2/2024 11:00:00 AM	36.0	19	22	40	0.0	0.0%	29.1
9/2/2024 12:00:00 PM	37.0	31	36	44	0.0	0.0%	30.2
9/2/2024 1:00:00 PM	39.0	53	62	43	0.0	0.0%	32.1

Outgoing Summary
 N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

9/2/2024 2:00:00 PM	39.0	85	100	47	46.3	3.0%	32.5
9/2/2024 3:00:00 PM	37.0	143	168	46	46.0	0.6%	31.7
9/2/2024 4:00:00 PM	37.0	145	171	47	47.0	0.6%	31.7
9/2/2024 5:00:00 PM	37.0	158	186	45	0.0	0.0%	31.8
9/2/2024 6:00:00 PM	38.0	163	192	46	46.0	0.5%	31.9
9/2/2024 7:00:00 PM	36.0	200	235	54	54.0	0.4%	31.0
9/2/2024 8:00:00 PM	39.0	209	246	54	51.0	1.2%	32.3
9/2/2024 9:00:00 PM	38.0	180	212	52	49.5	0.9%	31.8
9/2/2024 10:00:00 PM	39.0	163	192	48	46.8	2.1%	31.8
9/2/2024 11:00:00 PM	40.0	146	172	50	48.3	4.1%	32.5
9/3/2024 12:00:00 AM	37.0	140	165	46	46.0	0.6%	31.7
9/3/2024 1:00:00 AM	36.0	99	117	50	49.0	2.6%	31.2
9/3/2024 2:00:00 AM	36.0	58	68	45	0.0	0.0%	29.4
9/3/2024 3:00:00 AM	34.0	20	23	38	0.0	0.0%	28.8
9/3/2024 4:00:00 AM	33.0	12	14	66	66.0	7.1%	29.5
9/3/2024 5:00:00 AM	30.0	4	5	31	0.0	0.0%	28.0
9/3/2024 6:00:00 AM	37.0	3	4	43	0.0	0.0%	33.0
9/3/2024 7:00:00 AM	35.0	8	9	42	0.0	0.0%	29.2
9/3/2024 8:00:00 AM	33.0	4	5	37	0.0	0.0%	28.6
9/3/2024 9:00:00 AM	32.0	9	11	35	0.0	0.0%	26.8
9/3/2024 10:00:00 AM	34.0	50	59	41	0.0	0.0%	28.0
9/3/2024 11:00:00 AM	35.0	75	88	46	46.0	1.1%	29.7
9/3/2024 12:00:00 PM	36.0	198	233	45	0.0	0.0%	29.5
9/3/2024 1:00:00 PM	36.0	162	191	48	48.0	0.5%	30.8
9/3/2024 2:00:00 PM	37.0	161	189	70	70.0	0.5%	30.8
9/3/2024 3:00:00 PM	37.0	150	177	46	46.0	0.6%	31.0
9/3/2024 4:00:00 PM	38.0	169	199	46	46.0	1.0%	31.3
9/3/2024 5:00:00 PM	37.0	146	172	47	47.0	0.6%	31.2
9/3/2024 6:00:00 PM	37.0	158	186	43	0.0	0.0%	30.7
9/3/2024 7:00:00 PM	36.0	222	261	46	46.0	0.4%	30.3
9/3/2024 8:00:00 PM	37.0	255	300	47	46.7	1.0%	31.9
9/3/2024 9:00:00 PM	38.0	254	299	49	47.5	0.7%	32.0
9/3/2024 10:00:00 PM	38.0	244	287	48	47.3	1.0%	32.6
9/3/2024 11:00:00 PM	39.0	192	226	52	48.8	3.5%	32.8
9/4/2024 12:00:00 AM	38.0	156	184	56	53.7	1.6%	32.3
9/4/2024 1:00:00 AM	39.0	115	135	55	49.7	2.2%	32.1
9/4/2024 2:00:00 AM	39.0	82	96	49	48.0	2.1%	33.1
9/4/2024 3:00:00 AM	39.0	24	28	48	48.0	3.6%	31.6
9/4/2024 4:00:00 AM	36.0	10	12	46	46.0	16.7%	34.2
9/4/2024 5:00:00 AM	30.0	4	5	32	0.0	0.0%	28.0
9/4/2024 6:00:00 AM	33.0	6	7	36	0.0	0.0%	28.7
9/4/2024 7:00:00 AM	35.0	3	3	35	0.0	0.0%	27.7
9/4/2024 8:00:00 AM	34.0	6	7	37	0.0	0.0%	27.4
9/4/2024 9:00:00 AM	29.0	10	12	48	48.0	8.3%	27.7
9/4/2024 10:00:00 AM	34.0	36	42	42	0.0	0.0%	28.3
9/4/2024 11:00:00 AM	39.0	82	96	49	48.0	3.1%	30.8
9/4/2024 12:00:00 PM	34.0	192	226	44	0.0	0.0%	28.9
9/4/2024 1:00:00 PM	35.0	167	197	44	0.0	0.0%	29.6

Outgoing Summary

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

9/4/2024 2:00:00 PM	35.0	36	42	44	0.0	0.0%	28.3
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Outgoing Summary

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
8/30/2024 12:00:00 AM	38.0	1776	2090	53	48.1	1.4%	31.7
8/31/2024 12:00:00 AM	37.0	2992	3520	56	47.7	1.1%	31.3
9/1/2024 12:00:00 AM	38.0	2225	2618	55	48.6	1.3%	31.4
9/2/2024 12:00:00 AM	37.0	2343	2757	53	47.5	1.2%	31.1
9/3/2024 12:00:00 AM	38.0	2104	2475	54	48.0	1.2%	31.5
9/4/2024 12:00:00 AM	37.0	2811	3307	70	49.5	0.9%	31.2
9/4/2024 1:59:59 PM	37.0	772	908	55	48.1	1.3%	30.2

Outgoing Weekly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	154	120	125	154	122.5	36.7
1 - 2	*	*	*	*	78	100	63	78	81.5	38
2 - 3	*	*	*	*	46	51	68	46	59.5	37.3
3 - 4	*	*	*	*	21	26	24	21	25	35
4 - 5	*	*	*	*	7	10	13	7	11.5	33
5 - 6	*	*	*	*	9	11	12	9	11.5	32
6 - 7	*	*	*	*	9	5	6	9	5.5	33.5
7 - 8	*	*	*	*	8	4	9	8	6.5	31
8 - 9	*	*	*	*	9	7	4	9	5.5	33
9 - 10	*	*	*	*	29	7	7	29	7	36
10 - 11	*	*	*	*	83	26	23	83	24.5	39.3
11 - 12	*	*	*	*	215	45	37	215	41	34.5
12 - 13	*	*	*	*	199	95	94	199	94.5	37.3
13 - 14	*	*	*	*	178	141	120	178	130.5	37.2
14 - 15	*	*	*	27	181	183	244	104	213.5	39
15 - 16	*	*	*	178	221	219	181	199.5	200	37.55
16 - 17	*	*	*	224	218	206	317	221	261.5	36.95
17 - 18	*	*	*	189	242	197	225	215.5	211	37.65
18 - 19	*	*	*	271	293	244	202	282	223	35.75
19 - 20	*	*	*	282	314	195	220	298	207.5	36.45
20 - 21	*	*	*	250	284	212	241	267	226.5	37.75
21 - 22	*	*	*	260	321	201	203	290.5	202	37.9
22 - 23	*	*	*	225	249	171	184	237	177.5	36.85
23 - 24	*	*	*	184	152	142	135	168	138.5	37.1
Totals	0	0	0	2090	3520	2618	2757			
% of Total	0%	0%	0%	19.03%	32.04%	23.83%	25.1%			

Outgoing Weekly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	139	117	135	*	*	*	*	130.33	0	36.5
1 - 2	73	68	96	*	*	*	*	79	0	36.67
2 - 3	39	23	28	*	*	*	*	30	0	36.83
3 - 4	24	14	12	*	*	*	*	16.67	0	36
4 - 5	8	5	5	*	*	*	*	6	0	31.67
5 - 6	11	4	7	*	*	*	*	7.33	0	34
6 - 7	6	9	3	*	*	*	*	6	0	35.33
7 - 8	2	5	7	*	*	*	*	4.67	0	30.67
8 - 9	4	11	12	*	*	*	*	9	0	30.67
9 - 10	10	59	42	*	*	*	*	37	0	32.23
10 - 11	22	88	96	*	*	*	*	68.67	0	36.47
11 - 12	36	233	226	*	*	*	*	165	0	35.17
12 - 13	62	191	197	*	*	*	*	150	0	36.4
13 - 14	100	189	42	*	*	*	*	110.33	0	36.4
14 - 15	168	177	*	*	*	*	*	172.5	0	36.9
15 - 16	171	199	*	*	*	*	*	185	0	37.25
16 - 17	186	172	*	*	*	*	*	179	0	36.5
17 - 18	192	186	*	*	*	*	*	189	0	36.85
18 - 19	235	261	*	*	*	*	*	248	0	35.75
19 - 20	246	300	*	*	*	*	*	273	0	37.55
20 - 21	212	299	*	*	*	*	*	255.5	0	37.45
21 - 22	192	287	*	*	*	*	*	239.5	0	37.95
22 - 23	172	226	*	*	*	*	*	199	0	38.9
23 - 24	165	184	*	*	*	*	*	174.5	0	37.2
Totals	2475	3307	908	0	0	0	0			
% of Total	37%	49.43%	13.57%	0%	0%	0%	0%			

Outgoing Monthly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Hour	Aug 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	*	*	154	120	*	154	120	36.7
1 - 2	*	*	*	*	78	100	*	78	100	38
2 - 3	*	*	*	*	46	51	*	46	51	37.3
3 - 4	*	*	*	*	21	26	*	21	26	35
4 - 5	*	*	*	*	7	10	*	7	10	33
5 - 6	*	*	*	*	9	11	*	9	11	32
6 - 7	*	*	*	*	9	5	*	9	5	33.5
7 - 8	*	*	*	*	8	4	*	8	4	31
8 - 9	*	*	*	*	9	7	*	9	7	33
9 - 10	*	*	*	*	29	7	*	29	7	36
10 - 11	*	*	*	*	83	26	*	83	26	39.3
11 - 12	*	*	*	*	215	45	*	215	45	34.5
12 - 13	*	*	*	*	199	95	*	199	95	37.3
13 - 14	*	*	*	*	178	141	*	178	141	37.2
14 - 15	*	*	*	27	181	183	*	104	183	39
15 - 16	*	*	*	178	221	219	*	199.5	219	37.55
16 - 17	*	*	*	224	218	206	*	221	206	36.95
17 - 18	*	*	*	189	242	197	*	215.5	197	37.65
18 - 19	*	*	*	271	293	244	*	282	244	35.75
19 - 20	*	*	*	282	314	195	*	298	195	36.45
20 - 21	*	*	*	250	284	212	*	267	212	37.75
21 - 22	*	*	*	260	321	201	*	290.5	201	37.9
22 - 23	*	*	*	225	249	171	*	237	171	36.85
23 - 24	*	*	*	184	152	142	*	168	142	37.1
Totals	0	0	0	2090	3520	2618	0			
% of Total	0%	0%	0%	25.4%	42.78%	31.82%	0%			

Outgoing Monthly Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Sep 2024									
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour								Day Avg	Avg	Avg Speed
0 - 1	139	117	135	*	*	*	125	130.33	125	36.5
1 - 2	73	68	96	*	*	*	63	79	63	36.67
2 - 3	39	23	28	*	*	*	68	30	68	36.83
3 - 4	24	14	12	*	*	*	24	16.67	24	36
4 - 5	8	5	5	*	*	*	13	6	13	31.67
5 - 6	11	4	7	*	*	*	12	7.33	12	34
6 - 7	6	9	3	*	*	*	6	6	6	35.33
7 - 8	2	5	7	*	*	*	9	4.67	9	30.67
8 - 9	4	11	12	*	*	*	4	9	4	30.67
9 - 10	10	59	42	*	*	*	7	37	7	32.23
10 - 11	22	88	96	*	*	*	23	68.67	23	36.47
11 - 12	36	233	226	*	*	*	37	165	37	35.17
12 - 13	62	191	197	*	*	*	94	150	94	36.4
13 - 14	100	189	42	*	*	*	120	110.33	120	36.4
14 - 15	168	177	*	*	*	*	244	172.5	244	36.9
15 - 16	171	199	*	*	*	*	181	185	181	37.25
16 - 17	186	172	*	*	*	*	317	179	317	36.5
17 - 18	192	186	*	*	*	*	225	189	225	36.85
18 - 19	235	261	*	*	*	*	202	248	202	35.75
19 - 20	246	300	*	*	*	*	220	273	220	37.55
20 - 21	212	299	*	*	*	*	241	255.5	241	37.45
21 - 22	192	287	*	*	*	*	203	239.5	203	37.95
22 - 23	172	226	*	*	*	*	184	199	184	38.9
23 - 24	165	184	*	*	*	*	135	174.5	135	37.2
Totals	2475	3307	908	0	0	0	2757			
% of Total	26.2%	35.01%	9.61%	0%	0%	0%	29.18%			

Outgoing Weekly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	30.83	31.73	31.82	30.83	31.77	36.7
1 - 2	*	*	*	*	31.68	30.27	31.19	31.68	30.63	38
2 - 3	*	*	*	*	29.67	29	28.44	29.67	28.68	37.3
3 - 4	*	*	*	*	29.24	29.27	27.42	29.24	28.38	35
4 - 5	*	*	*	*	31	27.8	29.31	31	28.65	33
5 - 6	*	*	*	*	25.33	25	25.75	25.33	25.39	32
6 - 7	*	*	*	*	25.11	25.8	27.33	25.11	26.64	33.5
7 - 8	*	*	*	*	27.88	30.75	35.33	27.88	33.92	31
8 - 9	*	*	*	*	27.22	31.29	28	27.22	30.09	33
9 - 10	*	*	*	*	29.48	25.43	28.29	29.48	26.86	36
10 - 11	*	*	*	*	31.52	30.54	30.35	31.52	30.45	39.3
11 - 12	*	*	*	*	29.31	29.76	29.65	29.31	29.71	34.5
12 - 13	*	*	*	*	31.09	31.23	32.43	31.09	31.83	37.3
13 - 14	*	*	*	*	31.61	30.96	31.88	31.61	31.38	37.2
14 - 15	*	*	*	33.04	30.99	31.73	29.74	31.26	30.59	39
15 - 16	*	*	*	32.01	31.89	31.78	30.58	31.94	31.24	37.55
16 - 17	*	*	*	31.16	31.34	31.39	30.22	31.25	30.68	36.95
17 - 18	*	*	*	32.13	30.86	31.41	31.7	31.42	31.56	37.65
18 - 19	*	*	*	29.93	31.29	31	32.69	30.63	31.76	35.75
19 - 20	*	*	*	31.23	31.84	32.24	31.53	31.55	31.87	36.45
20 - 21	*	*	*	32.32	32.24	31.92	31.6	32.28	31.75	37.75
21 - 22	*	*	*	32.99	32.63	31.87	31.75	32.79	31.81	37.9
22 - 23	*	*	*	31.88	30.9	32.26	31.38	31.36	31.8	36.85
23 - 24	*	*	*	32.31	31.66	31.39	31.83	32.01	31.61	37.1
Totals	0	0	0	31.7	31.3	31.4	31.1			
% of Total	0%	0%	0%	25.26%	24.94%	25.02%	24.78%			

Outgoing Weekly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	29.47	31.21	32.07	*	*	*	*	30.89	*	36.5
1 - 2	30.38	29.43	33.09	*	*	*	*	31.21	*	36.67
2 - 3	29.46	28.83	31.61	*	*	*	*	29.97	*	36.83
3 - 4	31.04	29.5	34.17	*	*	*	*	31.36	*	36
4 - 5	26.62	28	28	*	*	*	*	27.39	*	31.67
5 - 6	29.73	33	28.71	*	*	*	*	30	*	34
6 - 7	30.17	29.22	27.67	*	*	*	*	29.28	*	35.33
7 - 8	24	28.6	27.43	*	*	*	*	27.36	*	30.67
8 - 9	28	26.82	27.67	*	*	*	*	27.37	*	30.67
9 - 10	25.8	28.03	28.31	*	*	*	*	27.94	*	32.23
10 - 11	29.09	29.66	30.79	*	*	*	*	30.13	*	36.47
11 - 12	30.17	29.55	28.93	*	*	*	*	29.31	*	35.17
12 - 13	32.11	30.79	29.65	*	*	*	*	30.47	*	36.4
13 - 14	32.47	30.83	28.31	*	*	*	*	31	*	36.4
14 - 15	31.73	31.01	*	*	*	*	*	31.36	*	36.9
15 - 16	31.73	31.31	*	*	*	*	*	31.5	*	37.25
16 - 17	31.76	31.2	*	*	*	*	*	31.49	*	36.5
17 - 18	31.94	30.67	*	*	*	*	*	31.31	*	36.85
18 - 19	30.99	30.29	*	*	*	*	*	30.62	*	35.75
19 - 20	32.26	31.85	*	*	*	*	*	32.03	*	37.55
20 - 21	31.8	32.05	*	*	*	*	*	31.95	*	37.45
21 - 22	31.82	32.55	*	*	*	*	*	32.26	*	37.95
22 - 23	32.53	32.84	*	*	*	*	*	32.71	*	38.9
23 - 24	31.7	32.3	*	*	*	*	*	32.02	*	37.2
Totals	31.5	31.2	30.2	0	0	0	0			
% of Total	33.91%	33.58%	32.51%	0%	0%	0%	0%			

Outgoing Monthly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Aug 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	*	*	*	*	30.83	31.73	*	30.83	31.73	36.7
1 - 2	*	*	*	*	31.68	30.27	*	31.68	30.27	38
2 - 3	*	*	*	*	29.67	29	*	29.67	29	37.3
3 - 4	*	*	*	*	29.24	29.27	*	29.24	29.27	35
4 - 5	*	*	*	*	31	27.8	*	31	27.8	33
5 - 6	*	*	*	*	25.33	25	*	25.33	25	32
6 - 7	*	*	*	*	25.11	25.8	*	25.11	25.8	33.5
7 - 8	*	*	*	*	27.88	30.75	*	27.88	30.75	31
8 - 9	*	*	*	*	27.22	31.29	*	27.22	31.29	33
9 - 10	*	*	*	*	29.48	25.43	*	29.48	25.43	36
10 - 11	*	*	*	*	31.52	30.54	*	31.52	30.54	39.3
11 - 12	*	*	*	*	29.31	29.76	*	29.31	29.76	34.5
12 - 13	*	*	*	*	31.09	31.23	*	31.09	31.23	37.3
13 - 14	*	*	*	*	31.61	30.96	*	31.61	30.96	37.2
14 - 15	*	*	*	33.04	30.99	31.73	*	31.26	31.73	39
15 - 16	*	*	*	32.01	31.89	31.78	*	31.94	31.78	37.55
16 - 17	*	*	*	31.16	31.34	31.39	*	31.25	31.39	36.95
17 - 18	*	*	*	32.13	30.86	31.41	*	31.42	31.41	37.65
18 - 19	*	*	*	29.93	31.29	31	*	30.63	31	35.75
19 - 20	*	*	*	31.23	31.84	32.24	*	31.55	32.24	36.45
20 - 21	*	*	*	32.32	32.24	31.92	*	32.28	31.92	37.75
21 - 22	*	*	*	32.99	32.63	31.87	*	32.79	31.87	37.9
22 - 23	*	*	*	31.88	30.9	32.26	*	31.36	32.26	36.85
23 - 24	*	*	*	32.31	31.66	31.39	*	32.01	31.39	37.1
Totals	0	0	0	31.7	31.3	31.4	0			
% of Total	0%	0%	0%	33.58%	33.16%	33.26%	0%			

Outgoing Monthly Speeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	29.47	31.21	32.07	*	*	*	31.82	30.89	31.82	36.5
1 - 2	30.38	29.43	33.09	*	*	*	31.19	31.21	31.19	36.67
2 - 3	29.46	28.83	31.61	*	*	*	28.44	29.97	28.44	36.83
3 - 4	31.04	29.5	34.17	*	*	*	27.42	31.36	27.42	36
4 - 5	26.62	28	28	*	*	*	29.31	27.39	29.31	31.67
5 - 6	29.73	33	28.71	*	*	*	25.75	30	25.75	34
6 - 7	30.17	29.22	27.67	*	*	*	27.33	29.28	27.33	35.33
7 - 8	24	28.6	27.43	*	*	*	35.33	27.36	35.33	30.67
8 - 9	28	26.82	27.67	*	*	*	28	27.37	28	30.67
9 - 10	25.8	28.03	28.31	*	*	*	28.29	27.94	28.29	32.23
10 - 11	29.09	29.66	30.79	*	*	*	30.35	30.13	30.35	36.47
11 - 12	30.17	29.55	28.93	*	*	*	29.65	29.31	29.65	35.17
12 - 13	32.11	30.79	29.65	*	*	*	32.43	30.47	32.43	36.4
13 - 14	32.47	30.83	28.31	*	*	*	31.88	31	31.88	36.4
14 - 15	31.73	31.01	*	*	*	*	29.74	31.36	29.74	36.9
15 - 16	31.73	31.31	*	*	*	*	30.58	31.5	30.58	37.25
16 - 17	31.76	31.2	*	*	*	*	30.22	31.49	30.22	36.5
17 - 18	31.94	30.67	*	*	*	*	31.7	31.31	31.7	36.85
18 - 19	30.99	30.29	*	*	*	*	32.69	30.62	32.69	35.75
19 - 20	32.26	31.85	*	*	*	*	31.53	32.03	31.53	37.55
20 - 21	31.8	32.05	*	*	*	*	31.6	31.95	31.6	37.45
21 - 22	31.82	32.55	*	*	*	*	31.75	32.26	31.75	37.95
22 - 23	32.53	32.84	*	*	*	*	31.38	32.71	31.38	38.9
23 - 24	31.7	32.3	*	*	*	*	31.83	32.02	31.83	37.2
Totals	31.5	31.2	30.2	0	0	0	31.1			
% of Total	25.4%	25.16%	24.35%	0%	0%	0%	25.08%			

Outgoing Weekly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	8/26/2024	to	9/1/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	8/26/2024	8/27/2024	8/28/2024	8/29/2024	8/30/2024	8/31/2024	9/1/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	36.7	37	37.8	36.7	37.4	36.7
1 - 2	*	*	*	*	38	36.4	37.7	38	37.05	38
2 - 3	*	*	*	*	37.3	36.3	34.2	37.3	35.25	37.3
3 - 4	*	*	*	*	35	36.5	37	35	36.75	35
4 - 5	*	*	*	*	33	30	32	33	31	33
5 - 6	*	*	*	*	32	28	30	32	29	32
6 - 7	*	*	*	*	33.5	25	37	33.5	31	33.5
7 - 8	*	*	*	*	31	33	43	31	38	31
8 - 9	*	*	*	*	33	34	27	33	30.5	33
9 - 10	*	*	*	*	36	29	34	36	31.5	36
10 - 11	*	*	*	*	39.3	36	36	39.3	36	39.3
11 - 12	*	*	*	*	34.5	35.5	35	34.5	35.25	34.5
12 - 13	*	*	*	*	37.3	38	39.5	37.3	38.75	37.3
13 - 14	*	*	*	*	37.2	35.9	37.7	37.2	36.8	37.2
14 - 15	*	*	*	40	38	38	35.5	39	36.75	39
15 - 16	*	*	*	37.9	37.2	37.7	35.8	37.55	36.75	37.55
16 - 17	*	*	*	36.8	37.1	36.9	35.8	36.95	36.35	36.95
17 - 18	*	*	*	39	36.3	37.6	36.9	37.65	37.25	37.65
18 - 19	*	*	*	35.9	35.6	36.2	39	35.75	37.6	35.75
19 - 20	*	*	*	35.8	37.1	38.2	36.2	36.45	37.2	36.45
20 - 21	*	*	*	38.4	37.1	37.1	36.7	37.75	36.9	37.75
21 - 22	*	*	*	38.6	37.2	37.7	36.9	37.9	37.3	37.9
22 - 23	*	*	*	38	35.7	37.2	36.5	36.85	36.85	36.85
23 - 24	*	*	*	37.6	36.6	37	38	37.1	37.5	37.1
Totals	0	0	0	378	861.7	844.2	865.2			
% of Total	0%	0%	0%	12.82%	29.22%	28.63%	29.34%			

Outgoing Weekly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	35.3	35.7	38.5	*	*	*	*	36.5	0	36.5
1 - 2	35.6	35.7	38.7	*	*	*	*	36.67	0	36.67
2 - 3	37.5	34	39	*	*	*	*	36.83	0	36.83
3 - 4	39	33	36	*	*	*	*	36	0	36
4 - 5	35	30	30	*	*	*	*	31.67	0	31.67
5 - 6	32	37	33	*	*	*	*	34	0	34
6 - 7	36	35	35	*	*	*	*	35.33	0	35.33
7 - 8	25	33	34	*	*	*	*	30.67	0	30.67
8 - 9	31	32	29	*	*	*	*	30.67	0	30.67
9 - 10	29	33.7	34	*	*	*	*	32.23	0	32.23
10 - 11	36	34.8	38.6	*	*	*	*	36.47	0	36.47
11 - 12	36.5	35.2	33.8	*	*	*	*	35.17	0	35.17
12 - 13	39	35.8	34.4	*	*	*	*	36.4	0	36.4
13 - 14	38.6	36.3	34.3	*	*	*	*	36.4	0	36.4
14 - 15	36.8	37	*	*	*	*	*	36.9	0	36.9
15 - 16	36.9	37.6	*	*	*	*	*	37.25	0	37.25
16 - 17	36.3	36.7	*	*	*	*	*	36.5	0	36.5
17 - 18	37.2	36.5	*	*	*	*	*	36.85	0	36.85
18 - 19	35.8	35.7	*	*	*	*	*	35.75	0	35.75
19 - 20	38.4	36.7	*	*	*	*	*	37.55	0	37.55
20 - 21	37.7	37.2	*	*	*	*	*	37.45	0	37.45
21 - 22	38.2	37.7	*	*	*	*	*	37.95	0	37.95
22 - 23	39.6	38.2	*	*	*	*	*	38.9	0	38.9
23 - 24	36.9	37.5	*	*	*	*	*	37.2	0	37.2
Totals	859.3	852	488.3	0	0	0	0			
% of Total	39.07%	38.73%	22.2%	0%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Aug 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	36.7	37	*	36.7	37	36.7
1 - 2	*	*	*	*	38	36.4	*	38	36.4	38
2 - 3	*	*	*	*	37.3	36.3	*	37.3	36.3	37.3
3 - 4	*	*	*	*	35	36.5	*	35	36.5	35
4 - 5	*	*	*	*	33	30	*	33	30	33
5 - 6	*	*	*	*	32	28	*	32	28	32
6 - 7	*	*	*	*	33.5	25	*	33.5	25	33.5
7 - 8	*	*	*	*	31	33	*	31	33	31
8 - 9	*	*	*	*	33	34	*	33	34	33
9 - 10	*	*	*	*	36	29	*	36	29	36
10 - 11	*	*	*	*	39.3	36	*	39.3	36	39.3
11 - 12	*	*	*	*	34.5	35.5	*	34.5	35.5	34.5
12 - 13	*	*	*	*	37.3	38	*	37.3	38	37.3
13 - 14	*	*	*	*	37.2	35.9	*	37.2	35.9	37.2
14 - 15	*	*	*	40	38	38	*	39	38	39
15 - 16	*	*	*	37.9	37.2	37.7	*	37.55	37.7	37.55
16 - 17	*	*	*	36.8	37.1	36.9	*	36.95	36.9	36.95
17 - 18	*	*	*	39	36.3	37.6	*	37.65	37.6	37.65
18 - 19	*	*	*	35.9	35.6	36.2	*	35.75	36.2	35.75
19 - 20	*	*	*	35.8	37.1	38.2	*	36.45	38.2	36.45
20 - 21	*	*	*	38.4	37.1	37.1	*	37.75	37.1	37.75
21 - 22	*	*	*	38.6	37.2	37.7	*	37.9	37.7	37.9
22 - 23	*	*	*	38	35.7	37.2	*	36.85	37.2	36.85
23 - 24	*	*	*	37.6	36.6	37	*	37.1	37	37.1

Outgoing Monthly EightyFifthSpeeds

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

	Sep 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	35.3	35.7	38.5	*	*	*	37.8	36.5	37.8	36.5
1 - 2	35.6	35.7	38.7	*	*	*	37.7	36.67	37.7	36.67
2 - 3	37.5	34	39	*	*	*	34.2	36.83	34.2	36.83
3 - 4	39	33	36	*	*	*	37	36	37	36
4 - 5	35	30	30	*	*	*	32	31.67	32	31.67
5 - 6	32	37	33	*	*	*	30	34	30	34
6 - 7	36	35	35	*	*	*	37	35.33	37	35.33
7 - 8	25	33	34	*	*	*	43	30.67	43	30.67
8 - 9	31	32	29	*	*	*	27	30.67	27	30.67
9 - 10	29	33.7	34	*	*	*	34	32.23	34	32.23
10 - 11	36	34.8	38.6	*	*	*	36	36.47	36	36.47
11 - 12	36.5	35.2	33.8	*	*	*	35	35.17	35	35.17
12 - 13	39	35.8	34.4	*	*	*	39.5	36.4	39.5	36.4
13 - 14	38.6	36.3	34.3	*	*	*	37.7	36.4	37.7	36.4
14 - 15	36.8	37	*	*	*	*	35.5	36.9	35.5	36.9
15 - 16	36.9	37.6	*	*	*	*	35.8	37.25	35.8	37.25
16 - 17	36.3	36.7	*	*	*	*	35.8	36.5	35.8	36.5
17 - 18	37.2	36.5	*	*	*	*	36.9	36.85	36.9	36.85
18 - 19	35.8	35.7	*	*	*	*	39	35.75	39	35.75
19 - 20	38.4	36.7	*	*	*	*	36.2	37.55	36.2	37.55
20 - 21	37.7	37.2	*	*	*	*	36.7	37.45	36.7	37.45
21 - 22	38.2	37.7	*	*	*	*	36.9	37.95	36.9	37.95
22 - 23	39.6	38.2	*	*	*	*	36.5	38.9	36.5	38.9
23 - 24	36.9	37.5	*	*	*	*	38	37.2	38	37.2

Summary of Violators

N Birchwood southeast pilots to on ramp 8-29 to 9-4 from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM
 time error -4hrs

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	790	31.2	10	48.4
01:00:00	478	31.1	6	48.0
02:00:00	255	29.3	5	47.0
03:00:00	121	29.8	6	49.7
04:00:00	48	28.5	0	0.0
05:00:00	54	27.3	2	46.5
06:00:00	38	27.5	0	0.0
07:00:00	35	29.9	0	0.0
08:00:00	47	28.0	1	48.0
09:00:00	154	28.1	0	0.0
10:00:00	338	30.5	6	47.7
11:00:00	792	29.4	2	47.5
12:00:00	838	30.9	3	48.3
13:00:00	770	31.3	11	48.7
14:00:00	980	31.0	9	47.9
15:00:00	1169	31.6	14	47.3
16:00:00	1323	31.1	11	47.6
17:00:00	1231	31.4	11	48.9
18:00:00	1506	31.0	12	48.7
19:00:00	1557	31.8	22	48.7
20:00:00	1498	32.0	17	48.6
21:00:00	1464	32.3	21	47.6
22:00:00	1227	31.9	23	48.3
23:00:00	962	31.9	13	48.5

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
8/29/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
8/29/2024	14:00	0	0	3	7	3	9	5	0	0	0	0	0	0	0	0	0	0	0	27	33	40	27 to 37	59.3	*	*	1	22	4	
8/29/2024	15:00	0	4	18	38	54	41	18	3	2	0	0	0	0	0	0	0	0	0	178	32	37.9	26 to 36	60.7	3	1.7	1	162	15	
8/29/2024	16:00	0	2	30	52	79	38	19	4	0	0	0	0	0	0	0	0	0	0	224	31.2	36.8	25 to 35	65.2	2	0.9	1	206	17	
8/29/2024	17:00	0	2	19	49	58	33	22	5	1	0	0	0	0	0	0	0	0	0	189	32.1	39	27 to 37	65.6	5	2.6	1	178	10	
8/29/2024	18:00	0	1	58	83	68	40	18	1	2	0	0	0	0	0	0	0	0	0	271	29.9	35.9	23 to 33	66.8	3	1.1	0	257	14	
8/29/2024	19:00	0	6	22	75	107	56	10	6	0	0	0	0	0	0	0	0	0	0	282	31.2	35.8	26 to 36	72.3	4	1.4	1	269	12	
8/29/2024	20:00	1	3	28	50	74	61	28	4	1	0	0	0	0	0	0	0	0	0	250	32.3	38.4	27 to 37	62.0	5	2.0	0	238	12	
8/29/2024	21:00	0	0	18	61	82	63	29	6	1	0	0	0	0	0	0	0	0	0	260	33	38.6	27 to 37	69.6	4	1.5	1	247	12	
8/29/2024	22:00	0	4	20	59	69	49	21	2	1	0	0	0	0	0	0	0	0	0	225	31.9	38	26 to 36	64.0	2	0.9	2	215	8	
8/29/2024	23:00	0	2	12	50	50	50	18	2	0	0	0	0	0	0	0	0	0	0	184	32.3	37.6	27 to 37	64.7	1	0.5	2	175	7	
24 Hr Summary		1	24	228	524	644	440	188	33	8	0	0	0	0	0	0	0	0	0	2090	31.7	38	27 to 37	63.9	29	1.4	10	1969	111	

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/30/2024	00:00	0	1	23	37	55	25	11	2	0	0	0	0	0	0	0	0	0	0	154	30.8	36.7	24 to 34	62.3	1	0.6	1	149	4
8/30/2024	01:00	0	1	13	14	28	11	10	1	0	0	0	0	0	0	0	0	0	0	78	31.7	38	27 to 37	62.8	*	*	0	77	1
8/30/2024	02:00	0	1	14	8	11	7	3	2	0	0	0	0	0	0	0	0	0	0	46	29.7	37.3	20 to 30	54.3	1	2.2	0	46	0
8/30/2024	03:00	0	2	4	6	5	2	2	0	0	0	0	0	0	0	0	0	0	0	21	29.2	35	24 to 34	66.7	*	*	0	21	0
8/30/2024	04:00	0	0	0	4	2	0	1	0	0	0	0	0	0	0	0	0	0	0	7	31	33	23 to 33	85.7	*	*	0	7	0
8/30/2024	05:00	1	0	4	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	9	25.3	32	22 to 32	66.7	*	*	0	9	0
8/30/2024	06:00	1	0	4	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	9	25.1	33.5	15 to 25	55.6	*	*	0	9	0
8/30/2024	07:00	0	0	2	3	3	0	0	0	0	0	0	0	0	0	0	0	0	0	8	27.9	31	21 to 31	87.5	*	*	0	8	0
8/30/2024	08:00	0	1	2	3	2	1	0	0	0	0	0	0	0	0	0	0	0	0	9	27.2	33	23 to 33	66.7	*	*	0	9	0
8/30/2024	09:00	0	1	3	11	8	6	0	0	0	0	0	0	0	0	0	0	0	0	29	29.5	36	23 to 33	69.0	*	*	0	29	0
8/30/2024	10:00	0	4	12	19	17	18	10	3	0	0	0	0	0	0	0	0	0	0	83	31.5	39.3	26 to 36	56.6	1	1.2	1	79	3
8/30/2024	11:00	0	2	41	80	58	23	8	3	0	0	0	0	0	0	0	0	0	0	215	29.3	34.5	23 to 33	72.1	2	0.9	0	213	2
8/30/2024	12:00	0	5	17	57	70	27	21	2	0	0	0	0	0	0	0	0	0	0	199	31.1	37.3	24 to 34	66.8	*	*	2	184	13
8/30/2024	13:00	0	3	15	56	47	40	10	7	0	0	0	0	0	0	0	0	0	0	178	31.6	37.2	27 to 37	65.2	4	2.2	2	161	15
8/30/2024	14:00	0	7	29	43	47	37	12	5	1	0	0	0	0	0	0	0	0	0	181	31	38	25 to 35	55.8	4	2.2	1	163	17
8/30/2024	15:00	0	0	25	45	81	49	19	2	0	0	0	0	0	0	0	0	0	0	221	31.9	37.2	27 to 37	67.0	1	0.5	1	203	17
8/30/2024	16:00	0	3	27	57	66	45	15	5	0	0	0	0	0	0	0	0	0	0	218	31.3	37.1	26 to 36	62.4	3	1.4	2	202	14
8/30/2024	17:00	0	5	28	66	77	48	16	1	1	0	0	0	0	0	0	0	0	0	242	30.9	36.3	26 to 36	65.7	1	0.4	2	223	17
8/30/2024	18:00	0	3	27	82	100	63	14	4	0	0	0	0	0	0	0	0	0	0	293	31.3	35.6	26 to 36	75.1	3	1.0	3	281	9
8/30/2024	19:00	0	3	26	84	97	74	26	2	1	1	0	0	0	0	0	0	0	0	314	31.8	37.1	27 to 37	66.2	3	1.0	3	292	19
8/30/2024	20:00	0	2	26	58	100	75	19	4	0	0	0	0	0	0	0	0	0	0	284	32.2	37.1	28 to 38	71.8	2	0.7	1	268	15
8/30/2024	21:00	0	0	20	77	108	87	21	8	0	0	0	0	0	0	0	0	0	0	321	32.6	37.2	27 to 37	71.3	5	1.6	3	302	16
8/30/2024	22:00	0	3	22	81	83	46	11	3	0	0	0	0	0	0	0	0	0	0	249	30.9	35.7	26 to 36	71.9	2	0.8	1	239	9
8/30/2024	23:00	0	1	21	32	52	36	6	4	0	0	0	0	0	0	0	0	0	0	152	31.7	36.6	27 to 37	67.1	4	2.6	0	149	3
24 Hr Summary		2	48	405	925	1122	721	235	58	3	1	0	0	0	0	0	0	0	0	3520	31.3	37	27 to 37	65.1	37	1.1	23	3323	174

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
8/31/2024	00:00	0	1	16	21	46	27	6	3	0	0	0	0	0	0	0	0	0	0	120	31.7	37	29 to 39	68.3	*	*	0	117	3
8/31/2024	01:00	0	2	14	39	20	16	6	3	0	0	0	0	0	0	0	0	0	0	100	30.3	36.4	23 to 33	62.0	2	2.0	0	99	1
8/31/2024	02:00	0	4	10	17	9	6	3	2	0	0	0	0	0	0	0	0	0	0	51	29	36.3	21 to 31	56.9	2	3.9	0	51	0
8/31/2024	03:00	1	3	4	6	5	5	0	2	0	0	0	0	0	0	0	0	0	0	26	29.3	36.5	25 to 35	50.0	2	7.7	0	25	1
8/31/2024	04:00	0	1	1	4	3	1	0	0	0	0	0	0	0	0	0	0	0	0	10	27.8	30	20 to 30	70.0	*	*	0	10	0
8/31/2024	05:00	0	1	6	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	11	25	28	18 to 28	81.8	*	*	0	11	0
8/31/2024	06:00	0	0	3	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	5	25.8	25	15 to 25	80.0	*	*	0	5	0
8/31/2024	07:00	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	4	30.8	33	23 to 33	75.0	*	*	0	4	0
8/31/2024	08:00	0	0	1	2	3	0	1	0	0	0	0	0	0	0	0	0	0	0	7	31.3	34	24 to 34	71.4	*	*	0	7	0
8/31/2024	09:00	0	1	2	3	0	1	0	0	0	0	0	0	0	0	0	0	0	0	7	25.4	29	17 to 27	71.4	*	*	0	7	0
8/31/2024	10:00	0	1	7	3	7	6	1	0	1	0	0	0	0	0	0	0	0	0	26	30.5	36	27 to 37	57.7	1	3.8	0	21	5
8/31/2024	11:00	0	2	6	19	8	6	2	2	0	0	0	0	0	0	0	0	0	0	45	29.8	35.5	22 to 32	68.9	*	*	1	42	2
8/31/2024	12:00	0	0	16	24	27	16	7	4	1	0	0	0	0	0	0	0	0	0	95	31.2	38	27 to 37	58.9	1	1.1	1	89	5
8/31/2024	13:00	0	2	12	50	41	30	4	2	0	0	0	0	0	0	0	0	0	0	141	31	35.9	26 to 36	73.8	1	0.7	1	136	4
8/31/2024	14:00	0	2	15	60	52	32	18	4	0	0	0	0	0	0	0	0	0	0	183	31.7	38	26 to 36	65.6	2	1.1	0	175	8
8/31/2024	15:00	0	6	20	56	65	45	23	3	1	0	0	0	0	0	0	0	0	0	219	31.8	37.7	27 to 37	63.0	3	1.4	1	204	14
8/31/2024	16:00	0	0	29	53	63	40	18	2	1	0	0	0	0	0	0	0	0	0	206	31.4	36.9	23 to 33	64.1	3	1.5	0	196	10
8/31/2024	17:00	0	0	29	53	61	31	20	1	1	1	0	0	0	0	0	0	0	0	197	31.4	37.6	24 to 34	64.0	3	1.5	2	187	8
8/31/2024	18:00	0	2	24	74	82	45	17	0	0	0	0	0	0	0	0	0	0	0	244	31	36.2	25 to 35	68.4	*	*	2	233	9
8/31/2024	19:00	0	2	11	59	62	36	17	7	1	0	0	0	0	0	0	0	0	0	195	32.2	38.2	26 to 36	64.6	4	2.1	1	188	6
8/31/2024	20:00	0	0	16	59	73	45	14	4	1	0	0	0	0	0	0	0	0	0	212	31.9	37.1	25 to 35	69.3	3	1.4	0	206	6
8/31/2024	21:00	0	1	17	61	60	41	16	4	1	0	0	0	0	0	0	0	0	0	201	31.9	37.7	27 to 37	65.7	4	2.0	0	193	8
8/31/2024	22:00	0	1	15	37	63	40	13	1	1	0	0	0	0	0	0	0	0	0	171	32.3	37.2	27 to 37	71.3	2	1.2	1	166	4
8/31/2024	23:00	0	1	14	37	47	35	7	1	0	0	0	0	0	0	0	0	0	0	142	31.4	37	27 to 37	64.8	*	*	0	140	2
24 Hr Summary		1	33	289	740	800	507	193	45	9	1	0	0	0	0	0	0	0	0	2618	31.4	38	27 to 37	63.6	33	1.3	10	2512	96

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/1/2024	00:00	0	0	13	29	50	22	7	4	0	0	0	0	0	0	0	0	0	0	125	31.8	37.8	24 to 34	69.6	3	2.4	0	123	2
9/1/2024	01:00	0	1	13	10	17	16	5	1	0	0	0	0	0	0	0	0	0	0	63	31.2	37.7	30 to 40	54.0	*	*	0	63	0
9/1/2024	02:00	0	8	12	18	19	8	2	1	0	0	0	0	0	0	0	0	0	0	68	28.4	34.2	22 to 32	61.8	*	*	0	65	3
9/1/2024	03:00	0	1	10	6	2	4	1	0	0	0	0	0	0	0	0	0	0	0	24	27.4	37	18 to 28	66.7	*	*	0	24	0
9/1/2024	04:00	0	0	3	4	4	0	1	1	0	0	0	0	0	0	0	0	0	0	13	29.3	32	22 to 32	76.9	*	*	0	13	0
9/1/2024	05:00	0	3	2	4	2	0	1	0	0	0	0	0	0	0	0	0	0	0	12	25.8	30	17 to 27	75.0	*	*	0	12	0
9/1/2024	06:00	0	2	1	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	6	27.3	37	11 to 21	50.0	*	*	0	6	0
9/1/2024	07:00	0	0	0	2	2	2	2	1	0	0	0	0	0	0	0	0	0	0	9	35.3	43	25 to 35	55.6	*	*	0	9	0
9/1/2024	08:00	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28	27	22 to 32	100.0	*	*	0	4	0
9/1/2024	09:00	0	0	3	0	3	1	0	0	0	0	0	0	0	0	0	0	0	0	7	28.3	34	24 to 34	57.1	*	*	0	6	1
9/1/2024	10:00	0	0	5	5	8	3	2	0	0	0	0	0	0	0	0	0	0	0	23	30.3	36	23 to 33	69.6	*	*	0	23	0
9/1/2024	11:00	0	1	8	10	11	4	3	0	0	0	0	0	0	0	0	0	0	0	37	29.6	35	23 to 33	64.9	*	*	0	35	2
9/1/2024	12:00	0	7	6	16	34	16	11	4	0	0	0	0	0	0	0	0	0	0	94	32.4	39.5	28 to 38	64.9	1	1.1	0	92	2
9/1/2024	13:00	0	2	15	25	34	30	11	3	0	0	0	0	0	0	0	0	0	0	120	31.9	37.7	27 to 37	60.8	2	1.7	0	119	1
9/1/2024	14:00	0	8	42	71	73	41	7	2	0	0	0	0	0	0	0	0	0	0	244	29.7	35.5	24 to 34	63.5	1	0.4	1	238	5
9/1/2024	15:00	0	2	24	49	68	28	6	4	0	0	0	0	0	0	0	0	0	0	181	30.6	35.8	24 to 34	70.2	4	2.2	0	177	4
9/1/2024	16:00	0	3	52	99	92	54	14	3	0	0	0	0	0	0	0	0	0	0	317	30.2	35.8	24 to 34	66.6	2	0.6	1	307	9
9/1/2024	17:00	0	3	15	59	81	52	14	1	0	0	0	0	0	0	0	0	0	0	225	31.7	36.9	26 to 36	69.3	1	0.4	0	220	5
9/1/2024	18:00	0	3	17	43	63	46	23	6	1	0	0	0	0	0	0	0	0	0	202	32.7	39	27 to 37	59.4	4	2.0	0	201	1
9/1/2024	19:00	0	3	16	64	76	42	13	4	2	0	0	0	0	0	0	0	0	0	220	31.5	36.2	25 to 35	70.9	5	2.3	0	214	6
9/1/2024	20:00	0	4	17	67	88	46	15	2	2	0	0	0	0	0	0	0	0	0	241	31.6	36.7	26 to 36	70.1	3	1.2	0	236	5
9/1/2024	21:00	0	0	22	56	62	45	16	2	0	0	0	0	0	0	0	0	0	0	203	31.7	36.9	27 to 37	70.4	1	0.5	0	201	2
9/1/2024	22:00	0	0	25	41	61	45	7	5	0	0	0	0	0	0	0	0	0	0	184	31.4	36.5	27 to 37	66.3	2	1.1	0	180	4
9/1/2024	23:00	0	3	16	34	35	29	14	4	0	0	0	0	0	0	0	0	0	0	135	31.8	38	25 to 35	58.5	4	3.0	0	130	5
24 Hr Summary		0	54	337	715	887	536	175	48	5	0	0	0	0	0	0	0	0	0	2757	31.1	37	26 to 36	63.7	33	1.2	2	2698	57

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/2/2024	00:00	0	6	23	41	43	18	6	2	0	0	0	0	0	0	0	0	0	0	139	29.5	35.3	24 to 34	65.5	*	*	0	139	0
9/2/2024	01:00	0	2	12	22	17	17	1	1	1	0	0	0	0	0	0	0	0	0	73	30.4	35.6	26 to 36	67.1	2	2.7	2	68	3
9/2/2024	02:00	0	5	6	10	5	10	2	1	0	0	0	0	0	0	0	0	0	0	39	29.5	37.5	26 to 36	48.7	1	2.6	0	38	1
9/2/2024	03:00	0	4	2	4	5	5	3	1	0	0	0	0	0	0	0	0	0	0	24	31	39	26 to 36	45.8	1	4.2	0	24	0
9/2/2024	04:00	0	2	1	1	2	2	0	0	0	0	0	0	0	0	0	0	0	0	8	26.6	35	15 to 25	50.0	*	*	0	8	0
9/2/2024	05:00	0	1	1	6	1	0	0	2	0	0	0	0	0	0	0	0	0	0	11	29.7	32	18 to 28	72.7	2	18.2	0	11	0
9/2/2024	06:00	0	0	2	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	6	30.2	36	27 to 37	66.7	*	*	0	6	0
9/2/2024	07:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24	25	15 to 25	100.0	*	*	0	2	0
9/2/2024	08:00	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28	31	23 to 33	100.0	*	*	0	4	0
9/2/2024	09:00	0	1	5	2	1	0	1	0	0	0	0	0	0	0	0	0	0	0	10	25.8	29	19 to 29	80.0	*	*	0	10	0
9/2/2024	10:00	0	1	2	9	6	3	1	0	0	0	0	0	0	0	0	0	0	0	22	29.1	36	22 to 32	72.7	*	*	0	20	2
9/2/2024	11:00	0	0	5	13	10	6	2	0	0	0	0	0	0	0	0	0	0	0	36	30.2	36.5	24 to 34	69.4	*	*	0	34	2
9/2/2024	12:00	0	1	5	15	20	12	9	0	0	0	0	0	0	0	0	0	0	0	62	32.1	39	27 to 37	66.1	*	*	0	60	2
9/2/2024	13:00	0	2	7	20	39	19	9	4	0	0	0	0	0	0	0	0	0	0	100	32.5	38.6	25 to 35	66.0	3	3.0	0	95	5
9/2/2024	14:00	0	0	19	46	50	37	15	1	0	0	0	0	0	0	0	0	0	0	168	31.7	36.8	26 to 36	67.9	1	0.6	0	160	8
9/2/2024	15:00	0	1	18	40	58	39	12	3	0	0	0	0	0	0	0	0	0	0	171	31.7	36.9	27 to 37	66.1	1	0.6	1	163	7
9/2/2024	16:00	0	1	16	43	73	40	11	2	0	0	0	0	0	0	0	0	0	0	186	31.8	36.3	27 to 37	73.1	*	*	0	181	5
9/2/2024	17:00	0	2	19	41	61	57	11	1	0	0	0	0	0	0	0	0	0	0	192	31.9	37.2	27 to 37	67.7	1	0.5	1	181	10
9/2/2024	18:00	0	2	20	70	87	47	8	0	1	0	0	0	0	0	0	0	0	0	235	31	35.8	26 to 36	71.1	1	0.4	0	227	8
9/2/2024	19:00	0	1	22	61	81	47	27	5	2	0	0	0	0	0	0	0	0	0	246	32.3	38.4	27 to 37	62.6	3	1.2	0	243	3
9/2/2024	20:00	0	3	20	60	59	44	23	2	1	0	0	0	0	0	0	0	0	0	212	31.8	37.7	26 to 36	63.7	2	0.9	0	208	4
9/2/2024	21:00	0	6	17	47	61	37	18	6	0	0	0	0	0	0	0	0	0	0	192	31.8	38.2	25 to 35	60.9	4	2.1	1	186	5
9/2/2024	22:00	0	4	16	39	50	33	23	5	2	0	0	0	0	0	0	0	0	0	172	32.5	39.6	27 to 37	61.6	7	4.1	1	165	6
9/2/2024	23:00	0	2	19	31	57	46	9	1	0	0	0	0	0	0	0	0	0	0	165	31.7	36.9	29 to 39	67.9	1	0.6	0	159	6
24 Hr Summary		0	47	260	623	789	521	191	36	7	0	0	0	0	0	0	0	0	0	2475	31.5	38	27 to 37	64.1	29	1.2	6	2392	77

Outgoing Histogram

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/3/2024	00:00	0	2	13	28	44	22	5	1	2	0	0	0	0	0	0	0	0	0	117	31.2	35.7	26 to 36	71.8	3	2.6	1	115	1
9/3/2024	01:00	1	4	16	11	20	10	5	1	0	0	0	0	0	0	0	0	0	0	68	29.4	35.7	27 to 37	55.9	*	*	0	65	3
9/3/2024	02:00	0	1	6	4	9	3	0	0	0	0	0	0	0	0	0	0	0	0	23	28.8	34	23 to 33	65.2	*	*	0	22	1
9/3/2024	03:00	0	2	2	6	2	0	1	0	0	0	0	1	0	0	0	0	0	0	14	29.5	33	18 to 28	64.3	1	7.1	0	13	1
9/3/2024	04:00	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	5	28	30	21 to 31	100.0	*	*	0	5	0
9/3/2024	05:00	0	0	1	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	4	33	37	27 to 37	50.0	*	*	0	3	1
9/3/2024	06:00	1	0	1	2	3	1	1	0	0	0	0	0	0	0	0	0	0	0	9	29.2	35	22 to 32	66.7	*	*	0	9	0
9/3/2024	07:00	0	1	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	5	28.6	33	18 to 28	60.0	*	*	0	5	0
9/3/2024	08:00	0	2	0	6	1	2	0	0	0	0	0	0	0	0	0	0	0	0	11	26.8	32	25 to 35	81.8	*	*	0	10	1
9/3/2024	09:00	0	3	21	11	16	7	1	0	0	0	0	0	0	0	0	0	0	0	59	28	33.7	23 to 33	62.7	*	*	0	58	1
9/3/2024	10:00	0	2	16	27	27	10	5	1	0	0	0	0	0	0	0	0	0	0	88	29.7	34.8	23 to 33	68.2	1	1.1	0	84	4
9/3/2024	11:00	0	6	48	63	77	30	8	1	0	0	0	0	0	0	0	0	0	0	233	29.5	35.2	24 to 34	67.8	*	*	4	225	4
9/3/2024	12:00	0	1	22	57	65	27	17	2	0	0	0	0	0	0	0	0	0	0	191	30.8	35.8	25 to 35	69.6	1	0.5	0	182	9
9/3/2024	13:00	0	6	29	43	62	31	17	0	0	0	0	0	1	0	0	0	0	0	189	30.8	36.3	27 to 37	63.5	1	0.5	4	169	16
9/3/2024	14:00	0	3	27	49	44	35	16	3	0	0	0	0	0	0	0	0	0	0	177	31	37	26 to 36	59.3	1	0.6	1	164	12
9/3/2024	15:00	0	3	29	51	55	36	20	5	0	0	0	0	0	0	0	0	0	0	199	31.3	37.6	23 to 33	58.8	2	1.0	3	179	17
9/3/2024	16:00	0	5	13	52	55	28	17	2	0	0	0	0	0	0	0	0	0	0	172	31.2	36.7	26 to 36	67.4	1	0.6	2	159	11
9/3/2024	17:00	0	2	25	56	60	28	15	0	0	0	0	0	0	0	0	0	0	0	186	30.7	36.5	24 to 34	68.8	*	*	0	176	10
9/3/2024	18:00	0	4	35	79	80	52	10	1	0	0	0	0	0	0	0	0	0	0	261	30.3	35.7	27 to 37	67.0	1	0.4	2	246	13
9/3/2024	19:00	1	2	20	73	110	73	17	4	0	0	0	0	0	0	0	0	0	0	300	31.9	36.7	26 to 36	71.3	3	1.0	0	279	21
9/3/2024	20:00	0	3	24	65	115	67	21	4	0	0	0	0	0	0	0	0	0	0	299	32	37.2	26 to 36	69.2	2	0.7	3	279	17
9/3/2024	21:00	0	2	26	64	86	82	21	6	0	0	0	0	0	0	0	0	0	0	287	32.6	37.7	27 to 37	67.9	3	1.0	1	269	17
9/3/2024	22:00	0	0	15	58	72	50	22	6	3	0	0	0	0	0	0	0	0	0	226	32.8	38.2	26 to 36	67.7	8	3.5	0	216	10
9/3/2024	23:00	0	2	14	44	67	37	12	6	0	2	0	0	0	0	0	0	0	0	184	32.3	37.5	28 to 38	69.6	3	1.6	2	177	5
24 Hr Summary		3	56	403	854	1074	633	232	43	5	2	0	1	1	0	0	0	0	0	3307	31.2	37	26 to 36	64.9	31	0.9	23	3109	175

Outgoing Histogram

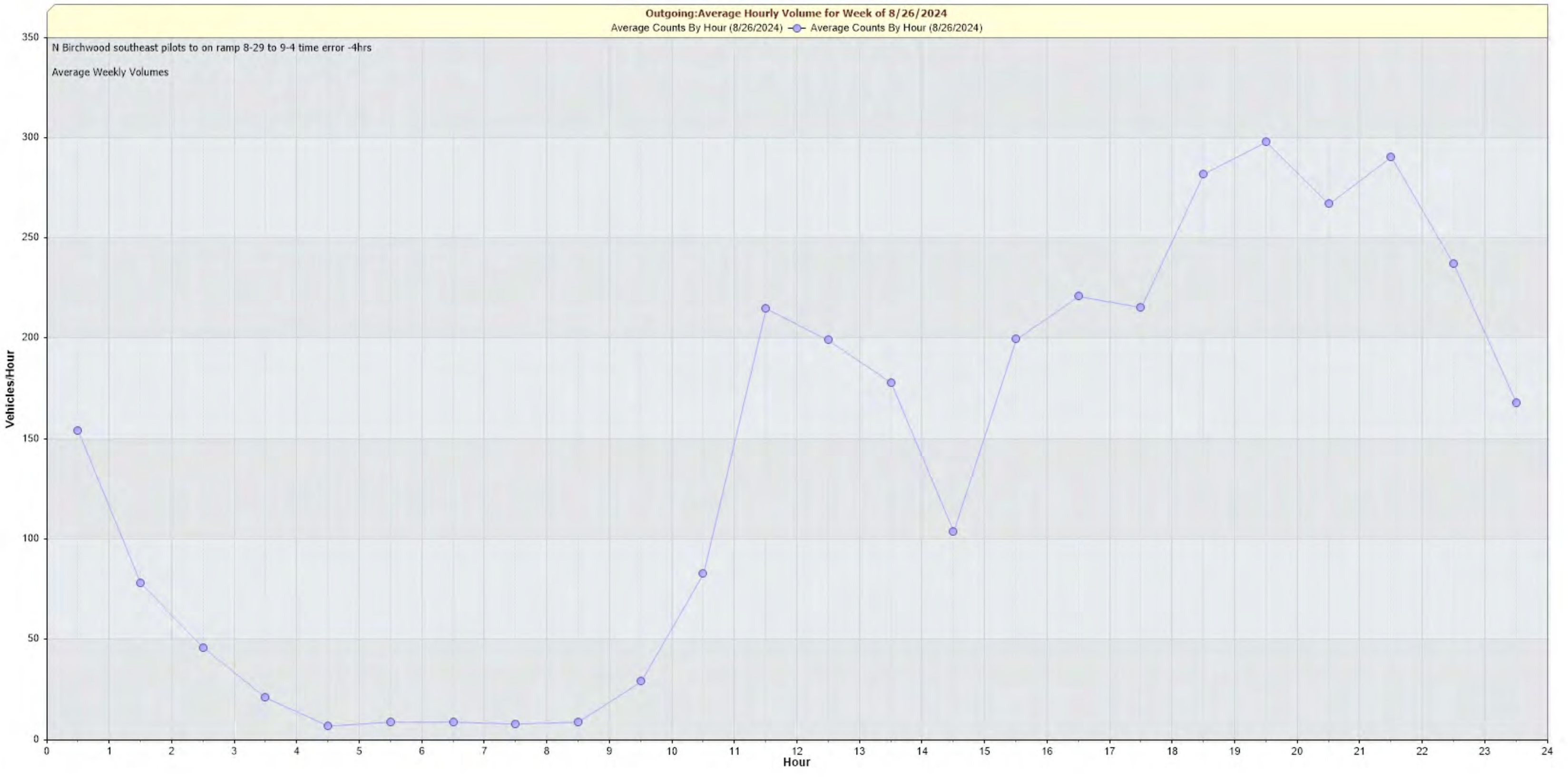
N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

from Thu-Aug-29-2024-02-00-PM to Wed-Sep-04-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG		
9/4/2024	00:00	0	2	16	33	33	34	11	5	0	1	0	0	0	0	0	0	0	0	135	32.1	38.5	26 to 36	60.7	3	2.2	1	127	7		
9/4/2024	01:00	0	1	6	19	29	28	11	2	0	0	0	0	0	0	0	0	0	0	96	33.1	38.7	29 to 39	65.6	2	2.1	0	95	1		
9/4/2024	02:00	0	0	7	4	7	6	2	2	0	0	0	0	0	0	0	0	0	0	28	31.6	39	23 to 33	53.6	1	3.6	0	27	1		
9/4/2024	03:00	0	0	0	3	5	2	0	2	0	0	0	0	0	0	0	0	0	0	12	34.2	36	26 to 36	83.3	2	16.7	0	12	0		
9/4/2024	04:00	0	0	1	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	5	28	30	22 to 32	100.0	*	*	0	3	2		
9/4/2024	05:00	0	0	2	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	7	28.7	33	23 to 33	85.7	*	*	0	7	0		
9/4/2024	06:00	1	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	27.7	35	25 to 35	66.7	*	*	0	3	0		
9/4/2024	07:00	0	0	3	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	7	27.4	34	16 to 26	71.4	*	*	0	7	0		
9/4/2024	08:00	0	1	3	6	1	0	0	1	0	0	0	0	0	0	0	0	0	0	12	27.7	29	22 to 32	83.3	1	8.3	0	12	0		
9/4/2024	09:00	0	0	14	12	10	3	3	0	0	0	0	0	0	0	0	0	0	0	42	28.3	34	20 to 30	69.0	*	*	0	42	0		
9/4/2024	10:00	0	3	17	23	25	16	8	4	0	0	0	0	0	0	0	0	0	0	96	30.8	38.6	24 to 34	54.2	3	3.1	1	93	2		
9/4/2024	11:00	1	4	45	84	60	19	13	0	0	0	0	0	0	0	0	0	0	0	226	28.9	33.8	24 to 34	72.6	*	*	2	217	7		
9/4/2024	12:00	0	3	28	68	65	26	7	0	0	0	0	0	0	0	0	0	0	0	197	29.6	34.4	23 to 33	73.1	*	*	4	181	12		
9/4/2024	13:00	1	2	7	17	8	6	1	0	0	0	0	0	0	0	0	0	0	0	42	28.3	34.3	23 to 33	69.0	*	*	1	39	2		
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		3	16	149	275	249	143	56	16	0	1	0	0	0	0	0	0	0	0	908	30.2	37	23 to 33	63.4	12	1.3	9	865	34		

Outgoing: Average Hourly Volume for Week of 8/26/2024
Average Counts By Hour (8/26/2024) ● Average Counts By Hour (8/26/2024)

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 8/26/2024
Average Hourly WEEKDAY Speeds By Hour (8/26/2024) — Average Hourly WEEKDAY Speeds By Hour (8/26/2024)

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs
Average Weekly Speeds

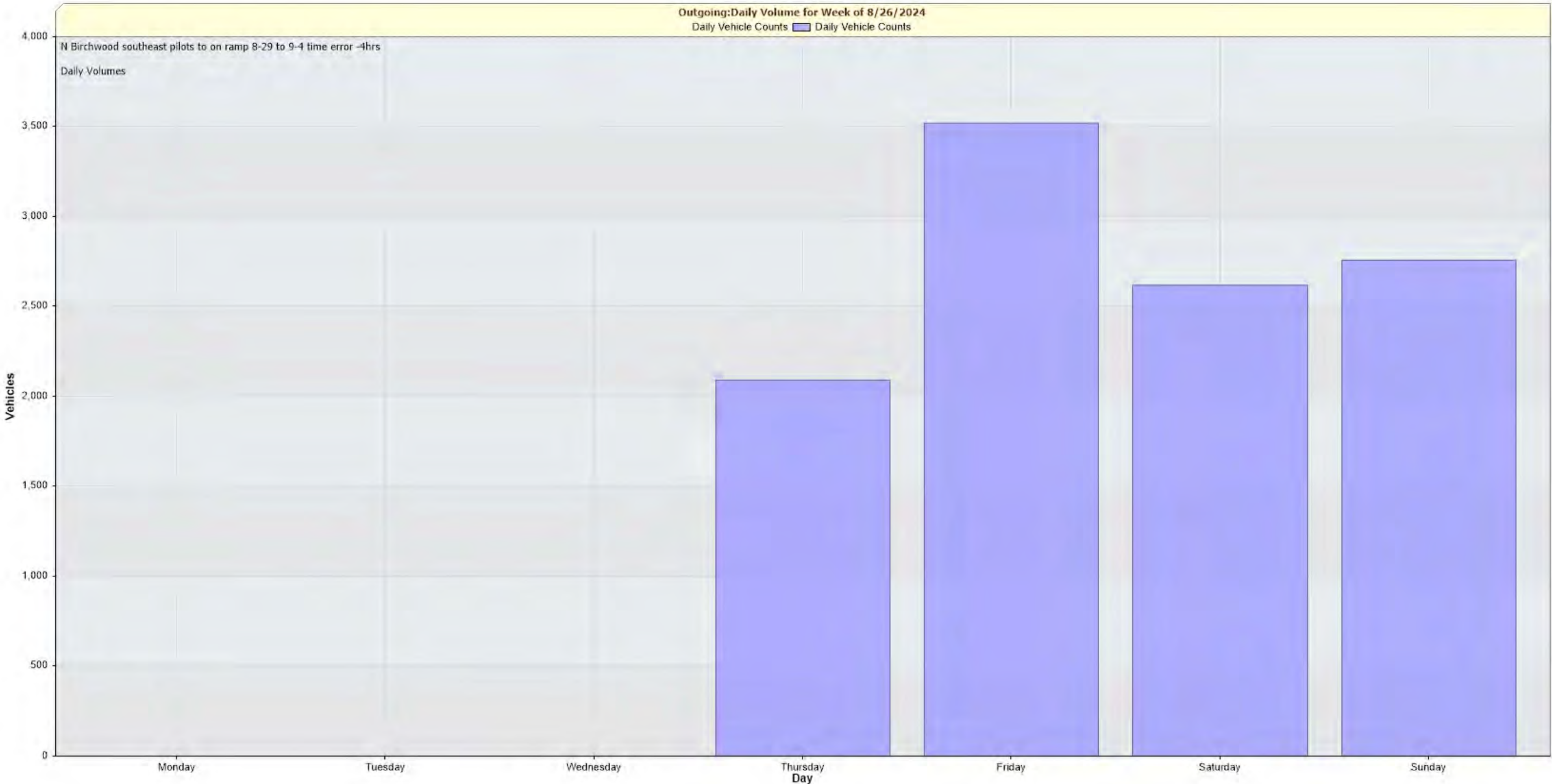


Outgoing: Daily Volume for Week of 8/26/2024

Daily Vehicle Counts

N Birchwood southeast pilots to on ramp 8-29 to 9-4 time error -4hrs

Daily Volumes



For Project: Birchwood Loop SE on overpass 9-4 to 9-10 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:40:57 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/4/2024 6:00:00 PM through 9/10/2024 3:59:59 PM

85th Percentile Speed 37 MPH

85th Percentile Vehicles 9285

Max Speed 70 MPH on 9/6/2024 11:59:11 PM

Total Vehicles 10924

AADT: 1846

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1545	1560
AM Peak 8:00 AM	146	111
PM Peak 5:00 PM	201	169

Speed

Speed Limit: 45

85th Percentile Speed: 37

50th Percentile Speed: 32

10 MPH Pace Interval: 27.0 MPH to 37.0 MPH

Average Speed: 31.29

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	1	3	1	7	7	19	11
% over limit	0.1	0.3	0.2	0.4	0.3	1.1	0.8
Avg Speeder	49.0	46.7	49.0	47.0	51.3	47.8	47.3
Avg Speed	31.1	31.1	31.5	31.0	31.0	31.7	31.7

Class Counts

	Number	%
VEH_SM	3	0
VEH_MED	10380	95
VEH_LG	541	5
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Incoming Summary
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/4/2024 7:00:00 PM	37.0	148	174	44	0.0	0.0%	31.0
9/4/2024 8:00:00 PM	37.0	139	163	41	0.0	0.0%	31.6
9/4/2024 9:00:00 PM	38.0	95	112	45	0.0	0.0%	31.8
9/4/2024 10:00:00 PM	38.0	78	92	45	0.0	0.0%	32.5
9/4/2024 11:00:00 PM	35.0	30	35	42	0.0	0.0%	29.5
9/5/2024 12:00:00 AM	40.0	17	20	49	49.0	5.0%	32.9
9/5/2024 1:00:00 AM	37.0	10	12	44	0.0	0.0%	31.8
9/5/2024 2:00:00 AM	31.0	6	7	31	0.0	0.0%	25.4
9/5/2024 3:00:00 AM	32.0	2	2	32	0.0	0.0%	30.0
9/5/2024 4:00:00 AM	46.0	1	1	46	46.0	100.0%	46.0
9/5/2024 5:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
9/5/2024 6:00:00 AM	33.0	10	12	37	0.0	0.0%	27.1
9/5/2024 7:00:00 AM	32.0	24	28	33	0.0	0.0%	27.8
9/5/2024 8:00:00 AM	35.0	57	67	42	0.0	0.0%	29.8
9/5/2024 9:00:00 AM	34.0	137	161	42	0.0	0.0%	29.0
9/5/2024 10:00:00 AM	34.0	94	110	42	0.0	0.0%	28.8
9/5/2024 11:00:00 AM	38.0	99	117	49	47.0	2.6%	31.8
9/5/2024 12:00:00 PM	37.0	72	85	44	0.0	0.0%	31.5
9/5/2024 1:00:00 PM	37.0	75	88	42	0.0	0.0%	31.2
9/5/2024 2:00:00 PM	37.0	88	104	41	0.0	0.0%	31.2
9/5/2024 3:00:00 PM	37.0	93	109	42	0.0	0.0%	31.8
9/5/2024 4:00:00 PM	34.0	130	153	48	48.0	0.7%	29.0
9/5/2024 5:00:00 PM	36.0	144	169	44	0.0	0.0%	31.2
9/5/2024 6:00:00 PM	37.0	178	210	45	0.0	0.0%	31.9
9/5/2024 7:00:00 PM	38.0	159	187	45	0.0	0.0%	32.8
9/5/2024 8:00:00 PM	37.0	118	139	47	47.0	1.4%	32.0
9/5/2024 9:00:00 PM	37.0	68	80	45	0.0	0.0%	32.5
9/5/2024 10:00:00 PM	36.0	52	61	41	0.0	0.0%	31.1
9/5/2024 11:00:00 PM	36.0	30	35	40	0.0	0.0%	30.6
9/6/2024 12:00:00 AM	38.0	19	22	44	0.0	0.0%	31.5
9/6/2024 1:00:00 AM	40.0	13	15	43	0.0	0.0%	31.6
9/6/2024 2:00:00 AM	33.0	5	6	43	0.0	0.0%	30.5
9/6/2024 3:00:00 AM	32.0	7	8	33	0.0	0.0%	28.8
9/6/2024 4:00:00 AM	33.0	2	2	33	0.0	0.0%	31.5
9/6/2024 5:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
9/6/2024 6:00:00 AM	32.0	4	5	47	47.0	20.0%	31.2
9/6/2024 7:00:00 AM	33.0	17	20	38	0.0	0.0%	27.1
9/6/2024 8:00:00 AM	33.0	62	73	44	0.0	0.0%	28.0
9/6/2024 9:00:00 AM	33.0	112	132	40	0.0	0.0%	28.4
9/6/2024 10:00:00 AM	35.0	99	116	44	0.0	0.0%	29.7
9/6/2024 11:00:00 AM	36.0	62	73	41	0.0	0.0%	30.9
9/6/2024 12:00:00 PM	37.0	100	118	48	48.0	0.8%	30.5
9/6/2024 1:00:00 PM	37.0	112	132	42	0.0	0.0%	31.5
9/6/2024 2:00:00 PM	36.0	109	128	42	0.0	0.0%	30.7
9/6/2024 3:00:00 PM	37.0	177	208	44	0.0	0.0%	31.8
9/6/2024 4:00:00 PM	35.0	157	185	40	0.0	0.0%	30.2
9/6/2024 5:00:00 PM	36.0	164	193	43	0.0	0.0%	31.4

Incoming Summary
Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

9/6/2024 6:00:00 PM	37.0	175	206	43	0.0	0.0%	32.5
9/6/2024 7:00:00 PM	36.0	169	199	41	0.0	0.0%	32.2
9/6/2024 8:00:00 PM	37.0	145	171	45	0.0	0.0%	32.2
9/6/2024 9:00:00 PM	38.0	93	109	47	47.0	2.8%	30.8
9/6/2024 10:00:00 PM	36.0	47	55	43	0.0	0.0%	31.7
9/6/2024 11:00:00 PM	35.0	30	35	42	0.0	0.0%	31.0
9/7/2024 12:00:00 AM	37.0	30	35	70	61.5	5.7%	31.3
9/7/2024 1:00:00 AM	32.0	12	14	39	0.0	0.0%	29.4
9/7/2024 2:00:00 AM	35.0	8	10	42	0.0	0.0%	31.6
9/7/2024 3:00:00 AM	34.0	11	13	48	48.0	7.7%	30.2
9/7/2024 4:00:00 AM	30.0	3	3	30	0.0	0.0%	28.7
9/7/2024 5:00:00 AM	29.0	2	2	29	0.0	0.0%	28.5
9/7/2024 6:00:00 AM	38.0	6	7	44	0.0	0.0%	33.1
9/7/2024 7:00:00 AM	36.0	12	14	43	0.0	0.0%	29.6
9/7/2024 8:00:00 AM	38.0	31	36	44	0.0	0.0%	31.2
9/7/2024 9:00:00 AM	38.0	37	43	44	0.0	0.0%	32.1
9/7/2024 10:00:00 AM	37.0	43	51	47	47.0	3.9%	32.3
9/7/2024 11:00:00 AM	35.0	84	99	44	0.0	0.0%	30.2
9/7/2024 12:00:00 PM	37.0	110	129	50	48.0	1.6%	31.3
9/7/2024 1:00:00 PM	36.0	110	129	45	0.0	0.0%	31.7
9/7/2024 2:00:00 PM	37.0	132	155	47	46.5	1.3%	31.9
9/7/2024 3:00:00 PM	37.0	125	147	43	0.0	0.0%	31.2
9/7/2024 4:00:00 PM	37.0	116	136	46	46.0	0.7%	31.4
9/7/2024 5:00:00 PM	37.0	119	140	42	0.0	0.0%	31.5
9/7/2024 6:00:00 PM	38.0	121	142	57	57.0	0.7%	32.5
9/7/2024 7:00:00 PM	39.0	132	155	48	47.0	1.3%	33.0
9/7/2024 8:00:00 PM	37.0	94	111	46	46.0	0.9%	32.3
9/7/2024 9:00:00 PM	37.0	94	111	49	49.0	0.9%	31.4
9/7/2024 10:00:00 PM	38.0	64	75	48	48.0	6.7%	33.0
9/7/2024 11:00:00 PM	39.0	36	42	46	46.0	2.4%	31.5
9/8/2024 12:00:00 AM	36.0	37	43	45	0.0	0.0%	32.4
9/8/2024 1:00:00 AM	37.0	15	18	46	46.0	5.6%	31.4
9/8/2024 2:00:00 AM	34.0	8	9	36	0.0	0.0%	30.9
9/8/2024 3:00:00 AM	31.0	3	3	31	0.0	0.0%	29.3
9/8/2024 4:00:00 AM	31.0	3	3	31	0.0	0.0%	26.0
9/8/2024 5:00:00 AM	42.0	2	2	42	0.0	0.0%	32.5
9/8/2024 6:00:00 AM	38.0	2	2	38	0.0	0.0%	31.5
9/8/2024 7:00:00 AM	31.0	4	5	31	0.0	0.0%	27.6
9/8/2024 8:00:00 AM	32.0	16	19	39	0.0	0.0%	29.2
9/8/2024 9:00:00 AM	37.0	34	40	44	0.0	0.0%	30.6
9/8/2024 10:00:00 AM	36.0	55	65	44	0.0	0.0%	31.6
9/8/2024 11:00:00 AM	36.0	90	106	42	0.0	0.0%	30.9
9/8/2024 12:00:00 PM	37.0	89	105	49	48.0	1.9%	31.8
9/8/2024 1:00:00 PM	36.0	99	117	45	0.0	0.0%	31.8
9/8/2024 2:00:00 PM	38.0	84	99	44	0.0	0.0%	32.8
9/8/2024 3:00:00 PM	38.0	93	109	47	47.0	0.9%	32.9
9/8/2024 4:00:00 PM	36.0	99	116	46	46.0	0.9%	31.6
9/8/2024 5:00:00 PM	37.0	84	99	43	0.0	0.0%	31.2

Incoming Summary
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

9/8/2024 6:00:00 PM	38.0	87	102	42	0.0	0.0%	31.9
9/8/2024 7:00:00 PM	39.0	82	96	51	49.0	3.1%	32.8
9/8/2024 8:00:00 PM	38.0	69	81	46	46.0	3.7%	32.2
9/8/2024 9:00:00 PM	35.0	65	77	43	0.0	0.0%	31.9
9/8/2024 10:00:00 PM	36.0	50	59	45	0.0	0.0%	30.2
9/8/2024 11:00:00 PM	36.0	30	35	42	0.0	0.0%	30.8
9/9/2024 12:00:00 AM	35.0	18	21	44	0.0	0.0%	32.9
9/9/2024 1:00:00 AM	37.0	15	18	39	0.0	0.0%	30.3
9/9/2024 2:00:00 AM	32.0	4	5	32	0.0	0.0%	30.0
9/9/2024 3:00:00 AM	42.0	12	14	44	0.0	0.0%	32.1
9/9/2024 4:00:00 AM	30.0	7	8	36	0.0	0.0%	26.8
9/9/2024 5:00:00 AM	35.0	3	4	39	0.0	0.0%	33.0
9/9/2024 6:00:00 AM	37.0	8	9	45	0.0	0.0%	29.6
9/9/2024 7:00:00 AM	31.0	20	24	34	0.0	0.0%	25.4
9/9/2024 8:00:00 AM	34.0	48	57	40	0.0	0.0%	29.9
9/9/2024 9:00:00 AM	34.0	110	130	44	0.0	0.0%	29.5
9/9/2024 10:00:00 AM	34.0	95	112	45	0.0	0.0%	29.8
9/9/2024 11:00:00 AM	36.0	99	116	43	0.0	0.0%	30.0
9/9/2024 12:00:00 PM	36.0	84	99	42	0.0	0.0%	31.4
9/9/2024 1:00:00 PM	37.0	88	104	45	0.0	0.0%	31.5
9/9/2024 2:00:00 PM	38.0	94	111	49	49.0	0.9%	32.2
9/9/2024 3:00:00 PM	39.0	85	100	43	0.0	0.0%	31.7
9/9/2024 4:00:00 PM	35.0	107	126	41	0.0	0.0%	30.4
9/9/2024 5:00:00 PM	36.0	106	125	43	0.0	0.0%	31.5
9/9/2024 6:00:00 PM	36.0	161	189	44	0.0	0.0%	31.9
9/9/2024 7:00:00 PM	38.0	161	189	45	0.0	0.0%	32.7
9/9/2024 8:00:00 PM	37.0	113	133	42	0.0	0.0%	32.4
9/9/2024 9:00:00 PM	37.0	70	82	43	0.0	0.0%	31.6
9/9/2024 10:00:00 PM	37.0	65	77	42	0.0	0.0%	31.2
9/9/2024 11:00:00 PM	38.0	30	35	40	0.0	0.0%	29.9
9/10/2024 12:00:00 AM	35.0	16	19	38	0.0	0.0%	28.2
9/10/2024 1:00:00 AM	37.0	13	15	44	0.0	0.0%	31.9
9/10/2024 2:00:00 AM	45.0	7	8	46	46.0	12.5%	33.6
9/10/2024 3:00:00 AM	31.0	3	3	31	0.0	0.0%	31.0
9/10/2024 4:00:00 AM	28.0	2	2	28	0.0	0.0%	26.5
9/10/2024 5:00:00 AM	30.0	2	2	30	0.0	0.0%	25.5
9/10/2024 6:00:00 AM	35.0	8	10	36	0.0	0.0%	31.3
9/10/2024 7:00:00 AM	35.0	13	15	40	0.0	0.0%	31.1
9/10/2024 8:00:00 AM	35.0	56	66	42	0.0	0.0%	29.7
9/10/2024 9:00:00 AM	34.0	139	163	43	0.0	0.0%	28.9
9/10/2024 10:00:00 AM	36.0	132	155	43	0.0	0.0%	30.1
9/10/2024 11:00:00 AM	35.0	104	122	40	0.0	0.0%	30.3
9/10/2024 12:00:00 PM	37.0	90	106	44	0.0	0.0%	32.3
9/10/2024 1:00:00 PM	37.0	97	114	44	0.0	0.0%	32.0
9/10/2024 2:00:00 PM	38.0	111	131	48	48.0	0.8%	33.4
9/10/2024 3:00:00 PM	39.0	125	147	46	46.0	0.7%	32.4
9/10/2024 4:00:00 PM	40.0	3	3	40	0.0	0.0%	33.0

Incoming Summary
Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/5/2024 12:00:00 AM	37.0	507	596	49	49.0	0.2%	31.5
9/6/2024 12:00:00 AM	36.0	1666	1960	49	47.0	0.4%	31.0
9/7/2024 12:00:00 AM	36.0	1891	2225	70	51.3	0.3%	31.0
9/8/2024 12:00:00 AM	37.0	1536	1807	57	47.8	1.1%	31.7
9/9/2024 12:00:00 AM	37.0	1180	1388	51	47.3	0.8%	31.7
9/10/2024 12:00:00 AM	37.0	1603	1886	49	49.0	0.1%	31.1
9/10/2024 3:59:59 PM	36.0	903	1062	48	46.7	0.3%	31.1

Incoming Weekly Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	12	15	14	18	13.5	16	38.5
1 - 2	*	*	*	7	6	10	9	6.5	9.5	31.75
2 - 3	*	*	*	2	8	13	3	5	8	32
3 - 4	*	*	*	1	2	3	3	1.5	3	39.5
4 - 5	*	*	*	1	1	2	2	1	2	31.5
5 - 6	*	*	*	12	5	7	2	8.5	4.5	32.5
6 - 7	*	*	*	28	20	14	5	24	9.5	32.35
7 - 8	*	*	*	67	73	36	19	70	27.5	34
8 - 9	*	*	*	161	132	43	40	146.5	41.5	33.15
9 - 10	*	*	*	110	116	51	65	113	58	34.2
10 - 11	*	*	*	117	73	99	106	95	102.5	36.65
11 - 12	*	*	*	85	118	129	105	101.5	117	36.3
12 - 13	*	*	*	88	132	129	117	110	123	36.8
13 - 14	*	*	*	104	128	155	99	116	127	35.9
14 - 15	*	*	*	109	208	147	109	158.5	128	36.65
15 - 16	*	*	*	153	185	136	116	169	126	33.9
16 - 17	*	*	*	169	193	140	99	181	119.5	35.6
17 - 18	*	*	*	210	206	142	102	208	122	36.5
18 - 19	*	*	174	187	199	155	96	186.67	125.5	36.47
19 - 20	*	*	163	139	171	111	81	157.67	96	36.7
20 - 21	*	*	112	80	109	111	77	100.33	94	37.4
21 - 22	*	*	92	61	55	75	59	69.33	67	36.3
22 - 23	*	*	35	35	35	42	35	35	38.5	34.9
23 - 24	*	*	20	22	35	43	21	25.67	32	38.33
Totals	0	0	596	1960	2225	1807	1388			
% of Total	0%	0%	7.47%	24.57%	27.9%	22.66%	17.4%			

Incoming Weekly Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	18	15	*	*	*	*	*	16.5	0	37
1 - 2	5	8	*	*	*	*	*	6.5	0	38.25
2 - 3	14	3	*	*	*	*	*	8.5	0	36.5
3 - 4	8	2	*	*	*	*	*	5	0	29
4 - 5	4	2	*	*	*	*	*	3	0	32.5
5 - 6	9	10	*	*	*	*	*	9.5	0	36
6 - 7	24	15	*	*	*	*	*	19.5	0	32.75
7 - 8	57	66	*	*	*	*	*	61.5	0	34.5
8 - 9	130	163	*	*	*	*	*	146.5	0	33.55
9 - 10	112	155	*	*	*	*	*	133.5	0	34.5
10 - 11	116	122	*	*	*	*	*	119	0	35.2
11 - 12	99	106	*	*	*	*	*	102.5	0	36.05
12 - 13	104	114	*	*	*	*	*	109	0	36.7
13 - 14	111	131	*	*	*	*	*	121	0	37.3
14 - 15	100	147	*	*	*	*	*	123.5	0	38.25
15 - 16	126	3	*	*	*	*	*	64.5	0	37.5
16 - 17	125	*	*	*	*	*	*	125	0	36
17 - 18	189	*	*	*	*	*	*	189	0	35.5
18 - 19	189	*	*	*	*	*	*	189	0	37.3
19 - 20	133	*	*	*	*	*	*	133	0	36.4
20 - 21	82	*	*	*	*	*	*	82	0	36.7
21 - 22	77	*	*	*	*	*	*	77	0	36.6
22 - 23	35	*	*	*	*	*	*	35	0	37.3
23 - 24	19	*	*	*	*	*	*	19	0	35
Totals	1886	1062	0	0	0	0	0			
% of Total	63.98%	36.02%	0%	0%	0%	0%	0%			

Incoming Monthly Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	18	15	*	12	15	14	18	15	16	37.75
1 - 2	5	8	*	7	6	10	9	6.5	9.5	35
2 - 3	14	3	*	2	8	13	3	6.75	8	34.25
3 - 4	8	2	*	1	2	3	3	3.25	3	34.25
4 - 5	4	2	*	1	1	2	2	2	2	32
5 - 6	9	10	*	12	5	7	2	9	4.5	34.25
6 - 7	24	15	*	28	20	14	5	21.75	9.5	32.55
7 - 8	57	66	*	67	73	36	19	65.75	27.5	34.25
8 - 9	130	163	*	161	132	43	40	146.5	41.5	33.35
9 - 10	112	155	*	110	116	51	65	123.25	58	34.35
10 - 11	116	122	*	117	73	99	106	107	102.5	35.92
11 - 12	99	106	*	85	118	129	105	102	117	36.17
12 - 13	104	114	*	88	132	129	117	109.5	123	36.75
13 - 14	111	131	*	104	128	155	99	118.5	127	36.6
14 - 15	100	147	*	109	208	147	109	141	128	37.45
15 - 16	126	3	*	153	185	136	116	116.75	126	35.7
16 - 17	125	*	*	169	193	140	99	162.33	119.5	35.73
17 - 18	189	*	*	210	206	142	102	201.67	122	36.17
18 - 19	189	*	174	187	199	155	96	187.25	125.5	36.67
19 - 20	133	*	163	139	171	111	81	151.5	96	36.62
20 - 21	82	*	112	80	109	111	77	95.75	94	37.22
21 - 22	77	*	92	61	55	75	59	71.25	67	36.38
22 - 23	35	*	35	35	35	42	35	35	38.5	35.5
23 - 24	19	*	20	22	35	43	21	24	32	37.5
Totals	1886	1062	596	1960	2225	1807	1388			
% of Total	17.26%	9.72%	5.46%	17.94%	20.37%	16.54%	12.71%			

Incoming Weekly Speeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	31.75	31.6	29.43	31.44	31.67	30.56	38.5
1 - 2	*	*	*	25.43	30.5	31.6	30.89	27.77	31.26	31.75
2 - 3	*	*	*	30	28.75	30.23	29.33	29	30.06	32
3 - 4	*	*	*	46	31.5	28.67	26	36.33	27.33	39.5
4 - 5	*	*	*	35	28	28.5	32.5	31.5	30.5	31.5
5 - 6	*	*	*	27.08	31.2	33.14	31.5	28.29	32.78	32.5
6 - 7	*	*	*	27.82	27.1	29.64	27.6	27.52	29.11	32.35
7 - 8	*	*	*	29.81	27.97	31.22	29.16	28.85	30.51	34
8 - 9	*	*	*	29.05	28.42	32.09	30.65	28.77	31.4	33.15
9 - 10	*	*	*	28.83	29.73	32.27	31.57	29.29	31.88	34.2
10 - 11	*	*	*	31.82	30.9	30.17	30.92	31.47	30.56	36.65
11 - 12	*	*	*	31.53	30.5	31.34	31.8	30.93	31.55	36.3
12 - 13	*	*	*	31.22	31.52	31.74	31.78	31.4	31.76	36.8
13 - 14	*	*	*	31.17	30.73	31.86	32.8	30.93	32.23	35.9
14 - 15	*	*	*	31.77	31.79	31.22	32.94	31.78	31.95	36.65
15 - 16	*	*	*	28.98	30.22	31.35	31.65	29.66	31.49	33.9
16 - 17	*	*	*	31.18	31.43	31.49	31.21	31.31	31.37	35.6
17 - 18	*	*	*	31.86	32.46	32.48	31.9	32.16	32.24	36.5
18 - 19	*	*	30.95	32.84	32.22	32.97	32.83	32.03	32.92	36.47
19 - 20	*	*	31.64	31.99	32.18	32.31	32.17	31.94	32.25	36.7
20 - 21	*	*	31.82	32.5	30.75	31.37	31.94	31.61	31.6	37.4
21 - 22	*	*	32.48	31.15	31.73	33	30.17	31.89	31.75	36.3
22 - 23	*	*	29.51	30.57	30.97	31.48	30.83	30.35	31.18	34.9
23 - 24	*	*	32.9	31.55	31.29	32.37	32.86	31.78	32.53	38.33
Totals	0	0	31.5	31	31	31.8	31.7			
% of Total	0%	0%	20.06%	19.75%	19.75%	20.25%	20.19%			

Incoming Weekly Speeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	30.33	31.87	*	*	*	*	*	31.03	*	37
1 - 2	30	33.62	*	*	*	*	*	32.23	*	38.25
2 - 3	32.14	31	*	*	*	*	*	31.94	*	36.5
3 - 4	26.75	26.5	*	*	*	*	*	26.7	*	29
4 - 5	33	25.5	*	*	*	*	*	30.5	*	32.5
5 - 6	29.56	31.3	*	*	*	*	*	30.47	*	36
6 - 7	25.42	31.07	*	*	*	*	*	27.59	*	32.75
7 - 8	29.95	29.73	*	*	*	*	*	29.83	*	34.5
8 - 9	29.54	28.88	*	*	*	*	*	29.17	*	33.55
9 - 10	29.75	30.15	*	*	*	*	*	29.98	*	34.5
10 - 11	29.97	30.28	*	*	*	*	*	30.13	*	35.2
11 - 12	31.35	32.33	*	*	*	*	*	31.86	*	36.05
12 - 13	31.5	32.04	*	*	*	*	*	31.78	*	36.7
13 - 14	32.23	33.43	*	*	*	*	*	32.88	*	37.3
14 - 15	31.68	32.38	*	*	*	*	*	32.1	*	38.25
15 - 16	30.37	33	*	*	*	*	*	30.43	*	37.5
16 - 17	31.53	*	*	*	*	*	*	31.53	*	36
17 - 18	31.9	*	*	*	*	*	*	31.9	*	35.5
18 - 19	32.69	*	*	*	*	*	*	32.69	*	37.3
19 - 20	32.39	*	*	*	*	*	*	32.39	*	36.4
20 - 21	31.62	*	*	*	*	*	*	31.62	*	36.7
21 - 22	31.22	*	*	*	*	*	*	31.22	*	36.6
22 - 23	29.89	*	*	*	*	*	*	29.89	*	37.3
23 - 24	28.21	*	*	*	*	*	*	28.21	*	35
Totals	31.1	31.1	0	0	0	0	0			
% of Total	50%	50%	0%	0%	0%	0%	0%			

Incoming Monthly Speeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	30.33	31.87	*	31.75	31.6	29.43	31.44	31.32	30.56	37.75
1 - 2	30	33.62	*	25.43	30.5	31.6	30.89	30	31.26	35
2 - 3	32.14	31	*	30	28.75	30.23	29.33	30.85	30.06	34.25
3 - 4	26.75	26.5	*	46	31.5	28.67	26	28.92	27.33	34.25
4 - 5	33	25.5	*	35	28	28.5	32.5	30.75	30.5	32
5 - 6	29.56	31.3	*	27.08	31.2	33.14	31.5	29.44	32.78	34.25
6 - 7	25.42	31.07	*	27.82	27.1	29.64	27.6	27.55	29.11	32.55
7 - 8	29.95	29.73	*	29.81	27.97	31.22	29.16	29.31	30.51	34.25
8 - 9	29.54	28.88	*	29.05	28.42	32.09	30.65	28.97	31.4	33.35
9 - 10	29.75	30.15	*	28.83	29.73	32.27	31.57	29.67	31.88	34.35
10 - 11	29.97	30.28	*	31.82	30.9	30.17	30.92	30.72	30.56	35.92
11 - 12	31.35	32.33	*	31.53	30.5	31.34	31.8	31.4	31.55	36.17
12 - 13	31.5	32.04	*	31.22	31.52	31.74	31.78	31.59	31.76	36.75
13 - 14	32.23	33.43	*	31.17	30.73	31.86	32.8	31.93	32.23	36.6
14 - 15	31.68	32.38	*	31.77	31.79	31.22	32.94	31.92	31.95	37.45
15 - 16	30.37	33	*	28.98	30.22	31.35	31.65	29.87	31.49	35.7
16 - 17	31.53	*	*	31.18	31.43	31.49	31.21	31.37	31.37	35.73
17 - 18	31.9	*	*	31.86	32.46	32.48	31.9	32.08	32.24	36.17
18 - 19	32.69	*	30.95	32.84	32.22	32.97	32.83	32.2	32.92	36.67
19 - 20	32.39	*	31.64	31.99	32.18	32.31	32.17	32.04	32.25	36.62
20 - 21	31.62	*	31.82	32.5	30.75	31.37	31.94	31.62	31.6	37.22
21 - 22	31.22	*	32.48	31.15	31.73	33	30.17	31.71	31.75	36.38
22 - 23	29.89	*	29.51	30.57	30.97	31.48	30.83	30.24	31.18	35.5
23 - 24	28.21	*	32.9	31.55	31.29	32.37	32.86	31.07	32.53	37.5
Totals	31.1	31.1	31.5	31	31	31.8	31.7			
% of Total	14.19%	14.19%	14.37%	14.14%	14.14%	14.51%	14.46%			

Incoming Weekly EightyFifthSpeeds
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	37	40	32	37	38.5	34.5	38.5
1 - 2	*	*	*	30.5	33	35	34	31.75	34.5	31.75
2 - 3	*	*	*	32	32	34	31	32	32.5	32
3 - 4	*	*	*	46	33	30	31	39.5	30.5	39.5
4 - 5	*	*	*	35	28	29	42	31.5	35.5	31.5
5 - 6	*	*	*	33	32	38	38	32.5	38	32.5
6 - 7	*	*	*	31.7	33	36	30.5	32.35	33.25	32.35
7 - 8	*	*	*	35	33	37.7	32	34	34.85	34
8 - 9	*	*	*	33.4	32.9	38	36.3	33.15	37.15	33.15
9 - 10	*	*	*	34	34.4	37	35.3	34.2	36.15	34.2
10 - 11	*	*	*	37.6	35.7	34.2	36	36.65	35.1	36.65
11 - 12	*	*	*	36.5	36.1	36.3	36.5	36.3	36.4	36.3
12 - 13	*	*	*	37	36.6	35.8	36	36.8	35.9	36.8
13 - 14	*	*	*	36.3	35.5	36.8	37.6	35.9	37.2	35.9
14 - 15	*	*	*	36.8	36.5	36.1	37.5	36.65	36.8	36.65
15 - 16	*	*	*	33.6	34.2	36.3	36	33.9	36.15	33.9
16 - 17	*	*	*	35.7	35.5	37	37	35.6	37	35.6
17 - 18	*	*	*	36.1	36.9	37.2	37.2	36.5	37.2	36.5
18 - 19	*	*	36.2	37.2	36	38.2	38.4	36.47	38.3	36.47
19 - 20	*	*	36.8	36.5	36.8	37	38	36.7	37.5	36.7
20 - 21	*	*	38	37	37.2	36.5	35	37.4	35.75	37.4
21 - 22	*	*	37.4	35.9	35.6	37.7	36	36.3	36.85	36.3
22 - 23	*	*	34.5	35.7	34.5	38.5	36	34.9	37.25	34.9
23 - 24	*	*	40	38	37	36	35	38.33	35.5	38.33
Totals	0	0	222.9	857.5	835.4	860.3	859.3			
% of Total	0%	0%	6.13%	23.59%	22.98%	23.66%	23.64%			

Incoming Weekly EightyFifthSpeeds
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	37	37	*	*	*	*	*	37	0	37
1 - 2	31.5	45	*	*	*	*	*	38.25	0	38.25
2 - 3	42	31	*	*	*	*	*	36.5	0	36.5
3 - 4	30	28	*	*	*	*	*	29	0	29
4 - 5	35	30	*	*	*	*	*	32.5	0	32.5
5 - 6	37	35	*	*	*	*	*	36	0	36
6 - 7	30.5	35	*	*	*	*	*	32.75	0	32.75
7 - 8	34	35	*	*	*	*	*	34.5	0	34.5
8 - 9	33.6	33.5	*	*	*	*	*	33.55	0	33.55
9 - 10	33.8	35.2	*	*	*	*	*	34.5	0	34.5
10 - 11	35.4	35	*	*	*	*	*	35.2	0	35.2
11 - 12	35.8	36.3	*	*	*	*	*	36.05	0	36.05
12 - 13	36.6	36.8	*	*	*	*	*	36.7	0	36.7
13 - 14	37.3	37.3	*	*	*	*	*	37.3	0	37.3
14 - 15	38.4	38.1	*	*	*	*	*	38.25	0	38.25
15 - 16	35	40	*	*	*	*	*	37.5	0	37.5
16 - 17	36	*	*	*	*	*	*	36	0	36
17 - 18	35.5	*	*	*	*	*	*	35.5	0	35.5
18 - 19	37.3	*	*	*	*	*	*	37.3	0	37.3
19 - 20	36.4	*	*	*	*	*	*	36.4	0	36.4
20 - 21	36.7	*	*	*	*	*	*	36.7	0	36.7
21 - 22	36.6	*	*	*	*	*	*	36.6	0	36.6
22 - 23	37.3	*	*	*	*	*	*	37.3	0	37.3
23 - 24	35	*	*	*	*	*	*	35	0	35
Totals	853.7	568.2	0	0	0	0	0			
% of Total	60.04%	39.96%	0%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	37	37	*	37	40	32	37	37.75	34.5	37.75
1 - 2	31.5	45	*	30.5	33	35	34	35	34.5	35
2 - 3	42	31	*	32	32	34	31	34.25	32.5	34.25
3 - 4	30	28	*	46	33	30	31	34.25	30.5	34.25
4 - 5	35	30	*	35	28	29	42	32	35.5	32
5 - 6	37	35	*	33	32	38	38	34.25	38	34.25
6 - 7	30.5	35	*	31.7	33	36	30.5	32.55	33.25	32.55
7 - 8	34	35	*	35	33	37.7	32	34.25	34.85	34.25
8 - 9	33.6	33.5	*	33.4	32.9	38	36.3	33.35	37.15	33.35
9 - 10	33.8	35.2	*	34	34.4	37	35.3	34.35	36.15	34.35
10 - 11	35.4	35	*	37.6	35.7	34.2	36	35.92	35.1	35.92
11 - 12	35.8	36.3	*	36.5	36.1	36.3	36.5	36.17	36.4	36.17
12 - 13	36.6	36.8	*	37	36.6	35.8	36	36.75	35.9	36.75
13 - 14	37.3	37.3	*	36.3	35.5	36.8	37.6	36.6	37.2	36.6
14 - 15	38.4	38.1	*	36.8	36.5	36.1	37.5	37.45	36.8	37.45
15 - 16	35	40	*	33.6	34.2	36.3	36	35.7	36.15	35.7
16 - 17	36	*	*	35.7	35.5	37	37	35.73	37	35.73
17 - 18	35.5	*	*	36.1	36.9	37.2	37.2	36.17	37.2	36.17
18 - 19	37.3	*	36.2	37.2	36	38.2	38.4	36.67	38.3	36.67
19 - 20	36.4	*	36.8	36.5	36.8	37	38	36.62	37.5	36.62
20 - 21	36.7	*	38	37	37.2	36.5	35	37.22	35.75	37.22
21 - 22	36.6	*	37.4	35.9	35.6	37.7	36	36.38	36.85	36.38
22 - 23	37.3	*	34.5	35.7	34.5	38.5	36	35.5	37.25	35.5
23 - 24	35	*	40	38	37	36	35	37.5	35.5	37.5

Summary of Violators

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	92	31.1	1	46.0
01:00:00	45	30.5	1	46.0
02:00:00	43	30.6	1	48.0
03:00:00	19	28.4	1	46.0
04:00:00	12	30.7	0	0.0
05:00:00	45	30.1	1	47.0
06:00:00	106	27.8	0	0.0
07:00:00	318	29.5	0	0.0
08:00:00	669	29.3	0	0.0
09:00:00	609	30.1	2	47.0
10:00:00	633	30.7	3	47.0
11:00:00	642	31.5	5	48.0
12:00:00	684	31.7	0	0.0
13:00:00	728	32.0	4	47.5
14:00:00	820	31.9	2	46.5
15:00:00	719	30.4	3	46.7
16:00:00	726	31.4	0	0.0
17:00:00	849	32.1	1	57.0
18:00:00	1000	32.4	5	48.2
19:00:00	798	32.1	6	46.3
20:00:00	571	31.6	4	47.5
21:00:00	419	31.7	5	48.0
22:00:00	217	30.6	1	46.0
23:00:00	160	31.7	3	57.3

Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	6	7	7	33	63	47	11	0	0	0	0	0	0	0	0	0	0	0	174	30.9	36.2	27 to 37	72.4	*	*	2	166	6	
9/4/2024	19:00	3	2	6	35	70	35	12	0	0	0	0	0	0	0	0	0	0	0	163	31.6	36.8	28 to 38	76.1	*	*	0	157	6	
9/4/2024	20:00	4	0	4	25	48	18	12	1	0	0	0	0	0	0	0	0	0	0	112	31.8	38	26 to 36	70.5	*	*	0	110	2	
9/4/2024	21:00	1	1	0	21	41	22	5	1	0	0	0	0	0	0	0	0	0	0	92	32.5	37.4	26 to 36	73.9	*	*	0	91	1	
9/4/2024	22:00	2	1	2	12	12	4	2	0	0	0	0	0	0	0	0	0	0	0	35	29.5	34.5	24 to 34	74.3	*	*	0	35	0	
9/4/2024	23:00	0	0	0	7	7	2	2	2	0	0	0	0	0	0	0	0	0	0	20	32.9	40	23 to 33	70.0	1	5.0	0	19	1	
24 Hr Summary		16	11	19	133	241	128	44	4	0	0	0	0	0	0	0	0	0	0	596	31.5	37	26 to 36	72.0	1	0.2	2	578	16	

Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/5/2024	00:00	0	1	0	4	1	4	2	0	0	0	0	0	0	0	0	0	0	0	12	31.8	37	25 to 35	66.7	*	*	0	12	0
9/5/2024	01:00	0	2	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	7	25.4	30.5	21 to 31	71.4	*	*	0	7	0
9/5/2024	02:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30	32	22 to 32	100.0	*	*	0	2	0
9/5/2024	03:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	46	46	36 to 46	100.0	1	100.0	0	1	0
9/5/2024	04:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	1	0
9/5/2024	05:00	2	0	2	0	6	2	0	0	0	0	0	0	0	0	0	0	0	0	12	27.1	33	27 to 37	66.7	*	*	0	12	0
9/5/2024	06:00	2	0	2	8	16	0	0	0	0	0	0	0	0	0	0	0	0	0	28	27.8	31.7	23 to 33	85.7	*	*	0	27	1
9/5/2024	07:00	1	4	3	23	22	12	2	0	0	0	0	0	0	0	0	0	0	0	67	29.8	35	26 to 36	76.1	*	*	0	65	2
9/5/2024	08:00	5	3	14	50	68	19	2	0	0	0	0	0	0	0	0	0	0	0	161	29	33.4	25 to 35	78.9	*	*	0	158	3
9/5/2024	09:00	4	2	13	42	33	13	3	0	0	0	0	0	0	0	0	0	0	0	110	28.8	34	24 to 34	72.7	*	*	0	108	2
9/5/2024	10:00	0	2	7	36	42	19	8	3	0	0	0	0	0	0	0	0	0	0	117	31.8	37.6	24 to 34	68.4	3	2.6	0	108	9
9/5/2024	11:00	0	0	9	20	27	23	6	0	0	0	0	0	0	0	0	0	0	0	85	31.5	36.5	27 to 37	68.2	*	*	0	81	4
9/5/2024	12:00	1	2	3	23	36	18	5	0	0	0	0	0	0	0	0	0	0	0	88	31.2	37	25 to 35	70.5	*	*	0	79	9
9/5/2024	13:00	3	1	6	25	39	25	5	0	0	0	0	0	0	0	0	0	0	0	104	31.2	36.3	25 to 35	70.2	*	*	0	96	8
9/5/2024	14:00	1	3	2	25	45	27	6	0	0	0	0	0	0	0	0	0	0	0	109	31.8	36.8	28 to 38	75.2	*	*	0	103	6
9/5/2024	15:00	9	2	11	55	58	10	7	1	0	0	0	0	0	0	0	0	0	0	153	29	33.6	24 to 34	77.8	1	0.7	0	150	3
9/5/2024	16:00	5	4	5	40	74	32	9	0	0	0	0	0	0	0	0	0	0	0	169	31.2	35.7	26 to 36	77.5	*	*	0	158	11
9/5/2024	17:00	5	1	6	39	98	49	11	1	0	0	0	0	0	0	0	0	0	0	210	31.9	36.1	28 to 38	80.5	*	*	0	194	16
9/5/2024	18:00	0	1	5	39	79	43	19	1	0	0	0	0	0	0	0	0	0	0	187	32.8	37.2	27 to 37	78.6	*	*	0	180	7
9/5/2024	19:00	3	0	3	33	59	33	5	3	0	0	0	0	0	0	0	0	0	0	139	32	36.5	26 to 36	78.4	2	1.4	0	134	5
9/5/2024	20:00	1	1	2	16	32	25	2	1	0	0	0	0	0	0	0	0	0	0	80	32.5	37	27 to 37	77.5	*	*	0	77	3
9/5/2024	21:00	2	2	3	8	28	12	6	0	0	0	0	0	0	0	0	0	0	0	61	31.1	35.9	27 to 37	75.4	*	*	0	59	2
9/5/2024	22:00	1	0	2	9	16	6	1	0	0	0	0	0	0	0	0	0	0	0	35	30.6	35.7	26 to 36	80.0	*	*	0	34	1
9/5/2024	23:00	1	1	0	5	7	5	3	0	0	0	0	0	0	0	0	0	0	0	22	31.5	38	26 to 36	68.2	*	*	0	21	1
24 Hr Summary		46	32	98	503	790	378	102	11	0	0	0	0	0	0	0	0	0	0	1960	31	36	26 to 36	73.5	7	0.4	0	1867	93

Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/6/2024	00:00	0	0	1	8	1	2	3	0	0	0	0	0	0	0	0	0	0	0	15	31.6	40	18 to 28	60.0	*	*	0	14	1
9/6/2024	01:00	0	1	0	1	3	0	1	0	0	0	0	0	0	0	0	0	0	0	6	30.5	33	23 to 33	66.7	*	*	0	6	0
9/6/2024	02:00	0	0	1	2	5	0	0	0	0	0	0	0	0	0	0	0	0	0	8	28.8	32	22 to 32	87.5	*	*	0	8	0
9/6/2024	03:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	31.5	33	23 to 33	100.0	*	*	0	2	0
9/6/2024	04:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0
9/6/2024	05:00	0	0	1	2	1	0	0	1	0	0	0	0	0	0	0	0	0	0	5	31.2	32	22 to 32	80.0	1	20.0	0	5	0
9/6/2024	06:00	1	1	5	5	6	2	0	0	0	0	0	0	0	0	0	0	0	0	20	27.1	33	21 to 31	60.0	*	*	0	19	1
9/6/2024	07:00	5	3	9	25	21	5	5	0	0	0	0	0	0	0	0	0	0	0	73	28	33	23 to 33	74.0	*	*	0	70	3
9/6/2024	08:00	2	5	21	41	49	11	3	0	0	0	0	0	0	0	0	0	0	0	132	28.4	32.9	24 to 34	74.2	*	*	0	128	4
9/6/2024	09:00	2	1	9	48	35	18	3	0	0	0	0	0	0	0	0	0	0	0	116	29.7	34.4	25 to 35	80.2	*	*	0	110	6
9/6/2024	10:00	1	1	4	21	32	12	2	0	0	0	0	0	0	0	0	0	0	0	73	30.9	35.7	26 to 36	78.1	*	*	0	67	6
9/6/2024	11:00	3	5	8	28	46	22	4	2	0	0	0	0	0	0	0	0	0	0	118	30.5	36.1	27 to 37	68.6	1	0.8	0	108	10
9/6/2024	12:00	1	3	5	34	52	31	6	0	0	0	0	0	0	0	0	0	0	0	132	31.5	36.6	28 to 38	76.5	*	*	0	128	4
9/6/2024	13:00	3	1	5	33	56	28	2	0	0	0	0	0	0	0	0	0	0	0	128	30.7	35.5	27 to 37	81.3	*	*	0	117	11
9/6/2024	14:00	6	3	5	41	95	46	12	0	0	0	0	0	0	0	0	0	0	0	208	31.8	36.5	27 to 37	76.9	*	*	0	195	13
9/6/2024	15:00	3	5	10	52	83	29	3	0	0	0	0	0	0	0	0	0	0	0	185	30.2	34.2	26 to 36	83.8	*	*	0	174	11
9/6/2024	16:00	2	4	7	40	95	35	10	0	0	0	0	0	0	0	0	0	0	0	193	31.4	35.5	27 to 37	79.3	*	*	0	180	13
9/6/2024	17:00	1	2	6	35	97	48	17	0	0	0	0	0	0	0	0	0	0	0	206	32.5	36.9	27 to 37	76.2	*	*	0	189	17
9/6/2024	18:00	2	3	6	31	93	57	7	0	0	0	0	0	0	0	0	0	0	0	199	32.2	36	27 to 37	81.9	*	*	0	189	10
9/6/2024	19:00	2	2	6	33	76	36	14	2	0	0	0	0	0	0	0	0	0	0	171	32.2	36.8	26 to 36	76.6	*	*	0	164	7
9/6/2024	20:00	5	3	9	23	35	25	6	3	0	0	0	0	0	0	0	0	0	0	109	30.8	37.2	28 to 38	64.2	3	2.8	0	104	5
9/6/2024	21:00	0	1	1	14	26	10	3	0	0	0	0	0	0	0	0	0	0	0	55	31.7	35.6	26 to 36	83.6	*	*	0	54	1
9/6/2024	22:00	0	0	3	9	17	5	1	0	0	0	0	0	0	0	0	0	0	0	35	31	34.5	24 to 34	80.0	*	*	0	34	1
9/6/2024	23:00	1	1	3	11	12	4	1	0	1	0	0	0	1	0	0	0	0	0	35	31.3	37	24 to 34	68.6	2	5.7	0	35	0
24 Hr Summary		40	45	125	538	938	426	103	8	1	0	0	0	1	0	0	0	0	0	2225	31	36	26 to 36	74.6	7	0.3	0	2101	124

Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/7/2024	00:00	0	0	2	6	5	1	0	0	0	0	0	0	0	0	0	0	0	0	14	29.4	32	23 to 33	85.7	*	*	0	12	2
9/7/2024	01:00	0	0	1	3	3	2	1	0	0	0	0	0	0	0	0	0	0	0	10	31.6	35	28 to 38	80.0	*	*	0	10	0
9/7/2024	02:00	1	0	0	6	4	0	1	1	0	0	0	0	0	0	0	0	0	0	13	30.2	34	24 to 34	76.9	1	7.7	0	12	1
9/7/2024	03:00	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	28.7	30	20 to 30	100.0	*	*	0	3	0
9/7/2024	04:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28.5	29	19 to 29	100.0	*	*	0	2	0
9/7/2024	05:00	0	0	0	3	1	2	1	0	0	0	0	0	0	0	0	0	0	0	7	33.1	38	21 to 31	57.1	*	*	0	7	0
9/7/2024	06:00	1	0	2	3	5	2	1	0	0	0	0	0	0	0	0	0	0	0	14	29.6	36	24 to 34	64.3	*	*	0	13	1
9/7/2024	07:00	0	1	3	8	16	5	3	0	0	0	0	0	0	0	0	0	0	0	36	31.2	37.7	23 to 33	72.2	*	*	0	33	3
9/7/2024	08:00	1	2	1	6	19	8	6	0	0	0	0	0	0	0	0	0	0	0	43	32.1	38	27 to 37	69.8	*	*	0	42	1
9/7/2024	09:00	0	2	3	10	18	12	4	2	0	0	0	0	0	0	0	0	0	0	51	32.3	37	26 to 36	70.6	2	3.9	0	45	6
9/7/2024	10:00	3	2	6	26	46	13	3	0	0	0	0	0	0	0	0	0	0	0	99	30.2	34.2	26 to 36	79.8	*	*	0	97	2
9/7/2024	11:00	2	3	6	32	53	27	4	1	1	0	0	0	0	0	0	0	0	0	129	31.3	36.3	28 to 38	76.0	2	1.6	0	122	7
9/7/2024	12:00	1	0	7	28	64	23	5	1	0	0	0	0	0	0	0	0	0	0	129	31.7	35.8	27 to 37	82.2	*	*	0	124	5
9/7/2024	13:00	2	3	5	35	67	31	10	2	0	0	0	0	0	0	0	0	0	0	155	31.9	36.8	28 to 38	76.8	2	1.3	0	150	5
9/7/2024	14:00	6	2	6	26	64	34	9	0	0	0	0	0	0	0	0	0	0	0	147	31.2	36.1	27 to 37	75.5	*	*	0	138	9
9/7/2024	15:00	4	1	6	32	50	34	8	1	0	0	0	0	0	0	0	0	0	0	136	31.4	36.3	27 to 37	73.5	1	0.7	0	127	9
9/7/2024	16:00	7	1	5	25	55	39	8	0	0	0	0	0	0	0	0	0	0	0	140	31.5	37	27 to 37	72.9	*	*	0	135	5
9/7/2024	17:00	2	1	7	25	63	29	13	1	0	1	0	0	0	0	0	0	0	0	142	32.5	37.2	27 to 37	73.2	1	0.7	0	135	7
9/7/2024	18:00	3	1	2	25	66	39	17	2	0	0	0	0	0	0	0	0	0	0	155	33	38.2	27 to 37	76.1	2	1.3	0	151	4
9/7/2024	19:00	3	3	2	12	54	29	6	2	0	0	0	0	0	0	0	0	0	0	111	32.3	37	29 to 39	77.5	1	0.9	0	106	5
9/7/2024	20:00	2	2	8	25	39	23	11	1	0	0	0	0	0	0	0	0	0	0	111	31.4	36.5	26 to 36	71.2	1	0.9	0	107	4
9/7/2024	21:00	0	0	2	17	31	17	3	5	0	0	0	0	0	0	0	0	0	0	75	33	37.7	25 to 35	70.7	5	6.7	0	74	1
9/7/2024	22:00	2	0	4	7	15	9	4	1	0	0	0	0	0	0	0	0	0	0	42	31.5	38.5	25 to 35	64.3	1	2.4	0	40	2
9/7/2024	23:00	0	1	1	13	14	10	3	1	0	0	0	0	0	0	0	0	0	0	43	32.4	36	26 to 36	81.4	*	*	0	43	0
24 Hr Summary		40	25	79	376	754	389	121	21	1	1	0	0	0	0	0	0	0	0	1807	31.7	37	27 to 37	74.0	19	1.1	0	1728	79

Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/8/2024	00:00	0	0	4	2	7	2	1	2	0	0	0	0	0	0	0	0	0	0	18	31.4	37	22 to 32	72.2	1	5.6	0	18	0
9/8/2024	01:00	0	0	1	2	5	1	0	0	0	0	0	0	0	0	0	0	0	0	9	30.9	34	24 to 34	88.9	*	*	0	9	0
9/8/2024	02:00	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	29.3	31	21 to 31	100.0	*	*	0	3	0
9/8/2024	03:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26	31	21 to 31	100.0	*	*	0	3	0
9/8/2024	04:00	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	2	32.5	42	13 to 23	50.0	*	*	0	2	0
9/8/2024	05:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	31.5	38	15 to 25	50.0	*	*	0	2	0
9/8/2024	06:00	0	0	1	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	5	27.6	30.5	21 to 31	100.0	*	*	0	5	0
9/8/2024	07:00	1	0	1	7	8	2	0	0	0	0	0	0	0	0	0	0	0	0	19	29.2	32	24 to 34	78.9	*	*	0	17	2
9/8/2024	08:00	2	0	2	11	17	5	3	0	0	0	0	0	0	0	0	0	0	0	40	30.6	36.3	24 to 34	75.0	*	*	0	35	5
9/8/2024	09:00	0	0	4	16	32	10	3	0	0	0	0	0	0	0	0	0	0	0	65	31.6	35.3	27 to 37	83.1	*	*	0	62	3
9/8/2024	10:00	2	1	5	25	53	15	5	0	0	0	0	0	0	0	0	0	0	0	106	30.9	36	27 to 37	76.4	*	*	0	102	4
9/8/2024	11:00	1	1	5	29	39	25	2	3	0	0	0	0	0	0	0	0	0	0	105	31.8	36.5	27 to 37	76.2	2	1.9	0	102	3
9/8/2024	12:00	2	3	2	29	47	22	10	2	0	0	0	0	0	0	0	0	0	0	117	31.8	36	25 to 35	75.2	*	*	0	113	4
9/8/2024	13:00	1	1	2	21	37	28	9	0	0	0	0	0	0	0	0	0	0	0	99	32.8	37.6	28 to 38	76.8	*	*	0	93	6
9/8/2024	14:00	0	1	3	24	40	33	6	2	0	0	0	0	0	0	0	0	0	0	109	32.9	37.5	27 to 37	77.1	1	0.9	0	104	5
9/8/2024	15:00	1	2	1	36	47	21	7	1	0	0	0	0	0	0	0	0	0	0	116	31.6	36	26 to 36	80.2	1	0.9	0	112	4
9/8/2024	16:00	4	0	4	23	42	18	8	0	0	0	0	0	0	0	0	0	0	0	99	31.2	37	27 to 37	73.7	*	*	0	97	2
9/8/2024	17:00	3	0	3	24	38	26	8	0	0	0	0	0	0	0	0	0	0	0	102	31.9	37.2	28 to 38	74.5	*	*	0	100	2
9/8/2024	18:00	2	1	1	21	38	22	8	2	1	0	0	0	0	0	0	0	0	0	96	32.8	38.4	28 to 38	75.0	3	3.1	0	94	2
9/8/2024	19:00	2	1	5	16	35	13	5	4	0	0	0	0	0	0	0	0	0	0	81	32.2	38	26 to 36	70.4	3	3.7	0	77	4
9/8/2024	20:00	2	1	0	13	41	14	6	0	0	0	0	0	0	0	0	0	0	0	77	31.9	35	26 to 36	81.8	*	*	0	75	2
9/8/2024	21:00	4	0	1	21	19	11	2	1	0	0	0	0	0	0	0	0	0	0	59	30.2	36	26 to 36	74.6	*	*	0	56	3
9/8/2024	22:00	1	0	1	9	18	4	2	0	0	0	0	0	0	0	0	0	0	0	35	30.8	36	27 to 37	82.9	*	*	0	35	0
9/8/2024	23:00	0	0	0	4	13	3	1	0	0	0	0	0	0	0	0	0	0	0	21	32.9	35	25 to 35	85.7	*	*	0	20	1
24 Hr Summary		28	12	48	338	581	276	87	17	1	0	0	0	0	0	0	0	0	0	1388	31.7	37	27 to 37	74.6	11	0.8	0	1336	52

Incoming Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/9/2024	00:00	0	1	1	7	4	5	0	0	0	0	0	0	0	0	0	0	0	0	18	30.3	37	23 to 33	66.7	*	*	0	17	1
9/9/2024	01:00	0	0	0	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	5	30	31.5	22 to 32	100.0	*	*	0	5	0
9/9/2024	02:00	1	0	3	2	3	0	5	0	0	0	0	0	0	0	0	0	0	0	14	32.1	42	33 to 43	50.0	*	*	0	12	2
9/9/2024	03:00	1	0	1	4	1	1	0	0	0	0	0	0	0	0	0	0	0	0	8	26.8	30	20 to 30	75.0	*	*	0	8	0
9/9/2024	04:00	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	4	33	35	25 to 35	75.0	*	*	0	4	0
9/9/2024	05:00	0	1	2	2	2	1	0	1	0	0	0	0	0	0	0	0	0	0	9	29.6	37	19 to 29	55.6	*	*	0	8	1
9/9/2024	06:00	2	2	5	8	7	0	0	0	0	0	0	0	0	0	0	0	0	0	24	25.4	30.5	24 to 34	70.8	*	*	0	23	1
9/9/2024	07:00	1	0	3	24	20	8	1	0	0	0	0	0	0	0	0	0	0	0	57	29.9	34	25 to 35	87.7	*	*	0	56	1
9/9/2024	08:00	4	4	4	46	56	11	5	0	0	0	0	0	0	0	0	0	0	0	130	29.5	33.6	25 to 35	80.0	*	*	0	124	6
9/9/2024	09:00	3	2	7	35	49	12	3	1	0	0	0	0	0	0	0	0	0	0	112	29.8	33.8	25 to 35	79.5	*	*	0	106	6
9/9/2024	10:00	3	2	16	31	40	18	6	0	0	0	0	0	0	0	0	0	0	0	116	30	35.4	24 to 34	68.1	*	*	0	109	7
9/9/2024	11:00	2	2	5	15	50	22	3	0	0	0	0	0	0	0	0	0	0	0	99	31.4	35.8	28 to 38	78.8	*	*	0	91	8
9/9/2024	12:00	4	1	3	27	34	26	7	2	0	0	0	0	0	0	0	0	0	0	104	31.5	36.6	26 to 36	72.1	*	*	0	98	6
9/9/2024	13:00	4	0	1	21	41	34	9	1	0	0	0	0	0	0	0	0	0	0	111	32.2	37.3	28 to 38	73.9	1	0.9	0	103	8
9/9/2024	14:00	4	1	8	16	38	22	11	0	0	0	0	0	0	0	0	0	0	0	100	31.7	38.4	29 to 39	65.0	*	*	0	92	8
9/9/2024	15:00	2	1	10	41	46	23	3	0	0	0	0	0	0	0	0	0	0	0	126	30.4	35	25 to 35	74.6	*	*	0	121	5
9/9/2024	16:00	3	0	4	30	53	29	6	0	0	0	0	0	0	0	0	0	0	0	125	31.5	36	25 to 35	76.0	*	*	0	117	8
9/9/2024	17:00	2	1	5	40	90	38	13	0	0	0	0	0	0	0	0	0	0	0	189	31.9	35.5	26 to 36	82.0	*	*	0	179	10
9/9/2024	18:00	3	4	4	23	81	61	12	1	0	0	0	0	0	0	0	0	0	0	189	32.7	37.3	29 to 39	82.0	*	*	0	184	5
9/9/2024	19:00	0	2	1	25	68	27	10	0	0	0	0	0	0	0	0	0	0	0	133	32.4	36.4	27 to 37	81.2	*	*	0	125	8
9/9/2024	20:00	3	1	1	18	30	23	6	0	0	0	0	0	0	0	0	0	0	0	82	31.6	36.7	27 to 37	75.6	*	*	0	79	3
9/9/2024	21:00	2	2	4	17	29	18	5	0	0	0	0	0	0	0	0	0	0	0	77	31.2	36.6	28 to 38	74.0	*	*	0	76	1
9/9/2024	22:00	1	1	5	10	9	7	2	0	0	0	0	0	0	0	0	0	0	0	35	29.9	37.3	28 to 38	62.9	*	*	0	32	3
9/9/2024	23:00	2	0	2	5	6	4	0	0	0	0	0	0	0	0	0	0	0	0	19	28.2	35	23 to 33	68.4	*	*	0	17	2
24 Hr Summary		47	28	95	449	762	392	107	6	0	0	0	0	0	0	0	0	0	0	1886	31.1	37	26 to 36	73.0	1	0.1	0	1786	100

Incoming Histogram

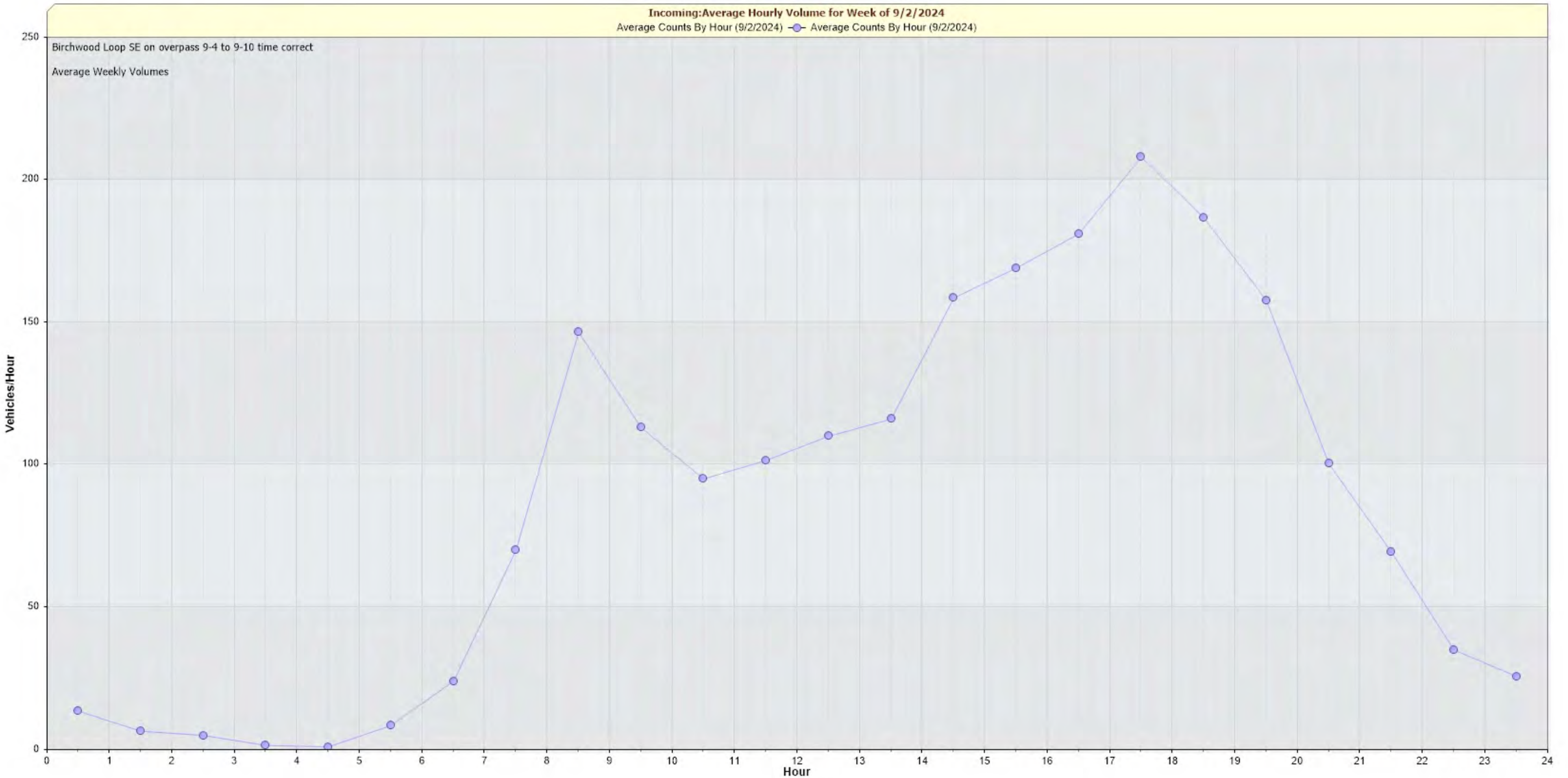
Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	1	0	1	3	5	3	2	0	0	0	0	0	0	0	0	0	0	0	15	31.9	37	27 to 37	73.3	*	*	0	14	1	
9/10/2024	01:00	0	0	1	3	0	2	0	2	0	0	0	0	0	0	0	0	0	0	8	33.6	45	18 to 28	50.0	1	12.5	0	8	0	
9/10/2024	02:00	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3	31	31	21 to 31	100.0	*	*	0	3	0	
9/10/2024	03:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	26.5	28	18 to 28	100.0	*	*	0	2	0	
9/10/2024	04:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25.5	30	20 to 30	100.0	*	*	0	2	0	
9/10/2024	05:00	0	0	1	2	4	3	0	0	0	0	0	0	0	0	0	0	0	0	10	31.3	35	26 to 36	90.0	*	*	0	7	3	
9/10/2024	06:00	0	0	1	6	5	1	2	0	0	0	0	0	0	0	0	0	0	0	15	31.1	35	22 to 32	80.0	*	*	0	15	0	
9/10/2024	07:00	1	1	6	23	23	8	4	0	0	0	0	0	0	0	0	0	0	0	66	29.7	35	24 to 34	74.2	*	*	0	66	0	
9/10/2024	08:00	5	2	20	53	64	16	3	0	0	0	0	0	0	0	0	0	0	0	163	28.9	33.5	25 to 35	76.7	*	*	1	150	12	
9/10/2024	09:00	6	1	11	48	60	23	6	0	0	0	0	0	0	0	0	0	0	0	155	30.1	35.2	25 to 35	72.9	*	*	0	144	11	
9/10/2024	10:00	6	0	10	26	57	21	2	0	0	0	0	0	0	0	0	0	0	0	122	30.3	35	28 to 38	74.6	*	*	0	111	11	
9/10/2024	11:00	0	1	3	23	43	29	7	0	0	0	0	0	0	0	0	0	0	0	106	32.3	36.3	27 to 37	80.2	*	*	0	94	12	
9/10/2024	12:00	0	2	5	27	44	26	10	0	0	0	0	0	0	0	0	0	0	0	114	32	36.8	26 to 36	73.7	*	*	0	106	8	
9/10/2024	13:00	1	0	4	22	48	42	13	1	0	0	0	0	0	0	0	0	0	0	131	33.4	37.3	27 to 37	76.3	1	0.8	0	124	7	
9/10/2024	14:00	4	3	4	29	45	46	14	2	0	0	0	0	0	0	0	0	0	0	147	32.4	38.1	27 to 37	70.1	1	0.7	0	135	12	
9/10/2024	15:00	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	3	33	40	21 to 31	66.7	*	*	0	3	0	
9/10/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		24	10	68	268	403	220	64	5	0	0	0	0	0	0	0	0	0	0	1062	31.1	36	26 to 36	72.8	3	0.3	1	984	77	

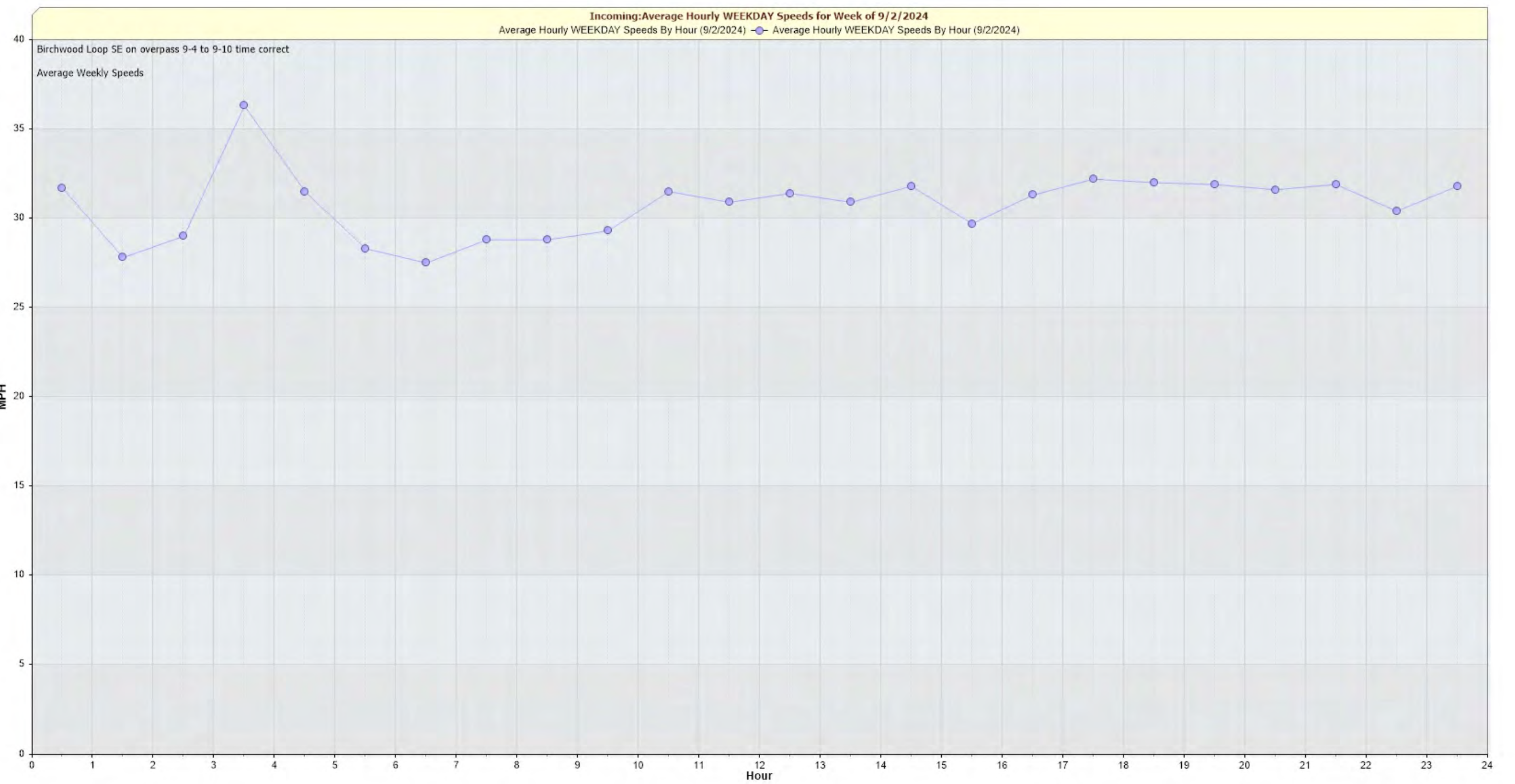
Incoming: Average Hourly Volume for Week of 9/2/2024
Average Counts By Hour (9/2/2024) — Average Counts By Hour (9/2/2024)

Birchwood Loop SE on overpass 9-4 to 9-10 time correct
Average Weekly Volumes



Incoming: Average Hourly WEEKDAY Speeds for Week of 9/2/2024
Average Hourly WEEKDAY Speeds By Hour (9/2/2024)

Birchwood Loop SE on overpass 9-4 to 9-10 time correct
Average Weekly Speeds



Incoming:Daily Volume for Week of 9/2/2024

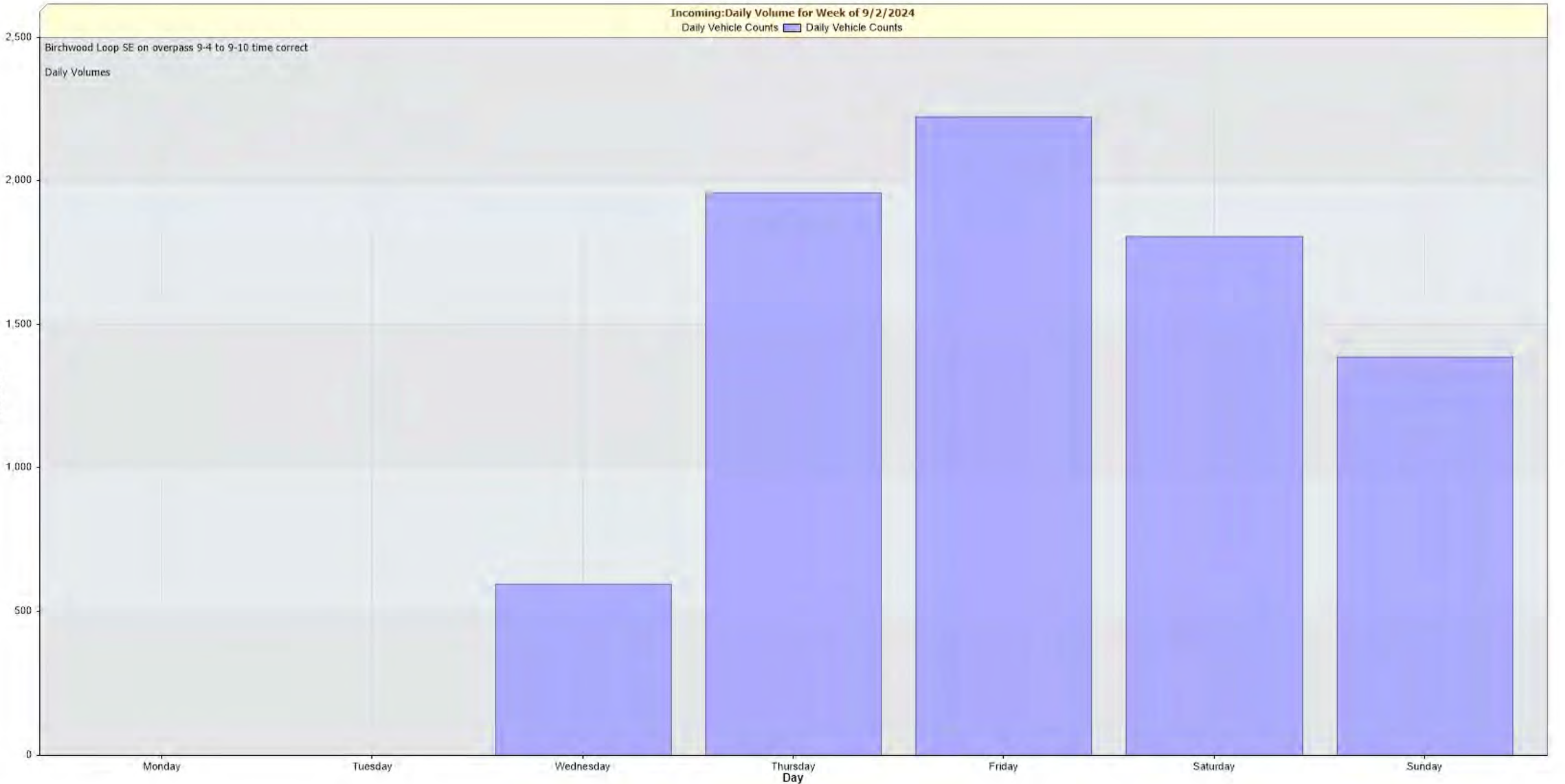
Daily Vehicle Counts

Daily Vehicle Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

Daily Volumes

Vehicles



Monday

Tuesday

Wednesday

Thursday
Day

Friday

Saturday

Sunday

For Project: Birchwood Loop SE on overpass 9-4 to 9-10 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:40:57 PM

Speed Intervals: 1 MPH

Time Intervals: Instant

Traffic Report From: 9/4/2024 6:00:00 PM through 9/10/2024 3:59:59 PM

85th Percentile Speed: 37 MPH

85th Percentile Vehicles: 11006

Max Speed: 63 MPH on 9/5/2024 2:16:14 PM

Total Vehicles: 12948

AADT: 2188

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1816	1849
AM Peak 10:00 AM	135	130
PM Peak 5:00 PM	237	211

Speed

Speed Limit: 45

85th Percentile Speed: 37

50th Percentile Speed: 31

10 MPH Pace Interval: 26.0 MPH to 36.0 MPH

Average Speed: 31.57

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	25	8	8	29	24	32	22
% over limit	1.1	0.7	1.1	1.2	0.9	1.5	1.3
Avg Speeder	49.3	48.6	47.1	48.1	47.5	48.8	47.6
Avg Speed	31.5	31.3	32.2	31.1	31.5	31.8	32.0

Class Counts

	Number	%
VEH_SM	102	0.8
VEH_MED	11992	92.6
VEH_LG	854	6.6
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Outgoing Summary
Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/4/2024 7:00:00 PM	37.0	147	173	50	48.0	1.2%	32.2
9/4/2024 8:00:00 PM	38.0	197	232	45	0.0	0.0%	32.2
9/4/2024 9:00:00 PM	38.0	96	113	44	0.0	0.0%	32.8
9/4/2024 10:00:00 PM	37.0	100	118	49	48.0	1.7%	31.6
9/4/2024 11:00:00 PM	39.0	49	58	47	46.3	5.2%	31.9
9/5/2024 12:00:00 AM	41.0	20	24	46	46.0	4.2%	32.3
9/5/2024 1:00:00 AM	44.0	13	15	51	49.5	13.3%	33.6
9/5/2024 2:00:00 AM	41.0	5	6	44	0.0	0.0%	31.2
9/5/2024 3:00:00 AM	31.0	3	4	44	0.0	0.0%	33.3
9/5/2024 4:00:00 AM	30.0	3	3	30	0.0	0.0%	28.7
9/5/2024 5:00:00 AM	29.0	3	4	32	0.0	0.0%	26.0
9/5/2024 6:00:00 AM	25.0	3	4	28	0.0	0.0%	25.5
9/5/2024 7:00:00 AM	33.0	13	15	43	0.0	0.0%	29.4
9/5/2024 8:00:00 AM	38.0	59	69	47	47.0	4.3%	31.1
9/5/2024 9:00:00 AM	36.0	110	129	44	0.0	0.0%	30.1
9/5/2024 10:00:00 AM	38.0	98	115	46	46.0	0.9%	31.4
9/5/2024 11:00:00 AM	37.0	128	151	46	46.0	1.3%	30.0
9/5/2024 12:00:00 PM	37.0	127	149	47	47.0	0.7%	31.2
9/5/2024 1:00:00 PM	37.0	135	159	49	47.8	3.8%	31.1
9/5/2024 2:00:00 PM	37.0	122	143	55	55.0	0.7%	31.6
9/5/2024 3:00:00 PM	36.0	143	168	63	63.0	0.6%	30.6
9/5/2024 4:00:00 PM	34.0	173	204	48	48.0	0.5%	29.5
9/5/2024 5:00:00 PM	36.0	198	233	47	47.0	0.4%	30.8
9/5/2024 6:00:00 PM	37.0	209	246	46	46.0	1.2%	31.9
9/5/2024 7:00:00 PM	36.0	201	236	49	48.0	0.8%	31.9
9/5/2024 8:00:00 PM	37.0	145	171	50	48.0	1.2%	32.2
9/5/2024 9:00:00 PM	36.0	88	103	48	46.7	2.9%	31.7
9/5/2024 10:00:00 PM	37.0	81	95	45	0.0	0.0%	32.5
9/5/2024 11:00:00 PM	35.0	53	62	44	0.0	0.0%	30.5
9/6/2024 12:00:00 AM	36.0	15	18	44	0.0	0.0%	32.8
9/6/2024 1:00:00 AM	36.0	10	12	44	0.0	0.0%	30.1
9/6/2024 2:00:00 AM	39.0	4	5	43	0.0	0.0%	33.0
9/6/2024 3:00:00 AM	30.0	2	2	30	0.0	0.0%	24.0
9/6/2024 4:00:00 AM	44.0	1	1	44	0.0	0.0%	44.0
9/6/2024 5:00:00 AM	33.0	5	6	34	0.0	0.0%	30.7
9/6/2024 6:00:00 AM	34.0	4	5	38	0.0	0.0%	30.8
9/6/2024 7:00:00 AM	32.0	14	17	34	0.0	0.0%	29.2
9/6/2024 8:00:00 AM	36.0	49	58	46	46.0	1.7%	31.3
9/6/2024 9:00:00 AM	34.0	102	120	45	0.0	0.0%	30.0
9/6/2024 10:00:00 AM	37.0	93	109	49	48.0	1.8%	31.2
9/6/2024 11:00:00 AM	36.0	119	140	43	0.0	0.0%	30.0
9/6/2024 12:00:00 PM	37.0	94	110	46	46.0	0.9%	30.4
9/6/2024 1:00:00 PM	38.0	137	161	47	46.3	1.9%	31.4
9/6/2024 2:00:00 PM	38.0	131	154	49	48.0	1.3%	31.9
9/6/2024 3:00:00 PM	37.0	168	198	42	0.0	0.0%	31.5
9/6/2024 4:00:00 PM	36.0	161	189	49	47.0	2.6%	31.4
9/6/2024 5:00:00 PM	37.0	197	232	49	48.0	0.9%	32.2

Outgoing Summary
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

9/6/2024 6:00:00 PM	37.0	209	246	52	52.0	0.4%	31.9
9/6/2024 7:00:00 PM	37.0	207	244	51	49.0	0.8%	32.5
9/6/2024 8:00:00 PM	37.0	171	201	46	46.0	0.5%	31.3
9/6/2024 9:00:00 PM	37.0	110	130	49	49.0	0.8%	32.3
9/6/2024 10:00:00 PM	37.0	76	89	46	46.0	2.2%	31.1
9/6/2024 11:00:00 PM	35.0	56	66	48	48.0	1.5%	31.2
9/7/2024 12:00:00 AM	36.0	27	32	45	0.0	0.0%	30.6
9/7/2024 1:00:00 AM	34.0	15	18	41	0.0	0.0%	27.5
9/7/2024 2:00:00 AM	36.0	8	10	44	0.0	0.0%	31.8
9/7/2024 3:00:00 AM	32.0	8	9	38	0.0	0.0%	28.1
9/7/2024 4:00:00 AM	37.0	3	3	37	0.0	0.0%	30.0
9/7/2024 5:00:00 AM	39.0	2	2	39	0.0	0.0%	32.0
9/7/2024 6:00:00 AM	42.0	6	7	43	0.0	0.0%	37.6
9/7/2024 7:00:00 AM	33.0	12	14	44	0.0	0.0%	30.4
9/7/2024 8:00:00 AM	36.0	22	26	45	0.0	0.0%	30.7
9/7/2024 9:00:00 AM	39.0	26	31	47	47.0	3.2%	33.3
9/7/2024 10:00:00 AM	38.0	48	57	47	47.0	1.8%	32.4
9/7/2024 11:00:00 AM	36.0	76	90	62	62.0	1.1%	31.8
9/7/2024 12:00:00 PM	38.0	114	134	46	46.0	0.7%	32.1
9/7/2024 1:00:00 PM	38.0	139	164	50	48.7	1.8%	32.3
9/7/2024 2:00:00 PM	36.0	171	201	58	52.3	1.5%	30.8
9/7/2024 3:00:00 PM	37.0	144	170	48	46.7	1.8%	31.9
9/7/2024 4:00:00 PM	38.0	139	164	48	48.0	0.6%	32.7
9/7/2024 5:00:00 PM	38.0	147	173	46	46.0	1.2%	31.9
9/7/2024 6:00:00 PM	37.0	160	188	50	48.0	2.7%	31.6
9/7/2024 7:00:00 PM	38.0	171	201	44	0.0	0.0%	32.0
9/7/2024 8:00:00 PM	37.0	137	161	46	46.0	0.6%	32.1
9/7/2024 9:00:00 PM	36.0	108	127	49	48.0	1.6%	31.7
9/7/2024 10:00:00 PM	37.0	79	93	57	49.4	5.4%	31.9
9/7/2024 11:00:00 PM	38.0	39	46	47	47.0	2.2%	31.4
9/8/2024 12:00:00 AM	40.0	30	35	51	50.5	5.7%	31.7
9/8/2024 1:00:00 AM	34.0	19	22	43	0.0	0.0%	29.5
9/8/2024 2:00:00 AM	34.0	9	11	49	49.0	9.1%	32.8
9/8/2024 3:00:00 AM	33.0	8	9	36	0.0	0.0%	27.1
9/8/2024 4:00:00 AM	30.0	2	2	30	0.0	0.0%	28.0
9/8/2024 5:00:00 AM	30.0	3	4	33	0.0	0.0%	28.5
9/8/2024 6:00:00 AM	42.0	2	2	42	0.0	0.0%	37.0
9/8/2024 7:00:00 AM	34.0	3	4	43	0.0	0.0%	32.3
9/8/2024 8:00:00 AM	34.0	9	11	36	0.0	0.0%	30.6
9/8/2024 9:00:00 AM	35.0	23	27	51	51.0	3.7%	30.0
9/8/2024 10:00:00 AM	38.0	39	46	44	0.0	0.0%	32.1
9/8/2024 11:00:00 AM	38.0	68	80	47	47.0	2.5%	31.9
9/8/2024 12:00:00 PM	37.0	120	141	51	48.7	2.1%	32.0
9/8/2024 1:00:00 PM	40.0	106	125	46	46.0	1.6%	32.9
9/8/2024 2:00:00 PM	37.0	173	203	48	47.5	1.0%	31.9
9/8/2024 3:00:00 PM	37.0	124	146	49	49.0	0.7%	31.6
9/8/2024 4:00:00 PM	38.0	116	136	47	47.0	0.7%	32.0
9/8/2024 5:00:00 PM	37.0	104	122	46	46.0	0.8%	31.8

Outgoing Summary
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

9/8/2024 6:00:00 PM	39.0	133	156	47	46.3	1.9%	33.0
9/8/2024 7:00:00 PM	37.0	132	155	47	47.0	0.6%	32.1
9/8/2024 8:00:00 PM	37.0	94	110	48	47.5	1.8%	32.1
9/8/2024 9:00:00 PM	39.0	78	92	51	48.5	2.2%	33.1
9/8/2024 10:00:00 PM	37.0	69	81	45	0.0	0.0%	31.3
9/8/2024 11:00:00 PM	36.0	31	37	42	0.0	0.0%	31.7
9/9/2024 12:00:00 AM	37.0	15	18	41	0.0	0.0%	32.1
9/9/2024 1:00:00 AM	30.0	8	10	36	0.0	0.0%	27.6
9/9/2024 2:00:00 AM	31.0	6	7	32	0.0	0.0%	28.9
9/9/2024 3:00:00 AM	39.0	3	3	39	0.0	0.0%	31.0
9/9/2024 4:00:00 AM	33.0	3	4	35	0.0	0.0%	32.0
9/9/2024 5:00:00 AM	38.0	6	7	50	50.0	14.3%	28.0
9/9/2024 6:00:00 AM	30.0	3	4	30	0.0	0.0%	28.3
9/9/2024 7:00:00 AM	42.0	11	13	47	47.0	7.7%	33.2
9/9/2024 8:00:00 AM	34.0	41	48	38	0.0	0.0%	29.8
9/9/2024 9:00:00 AM	38.0	111	131	52	50.0	1.5%	32.1
9/9/2024 10:00:00 AM	38.0	110	130	51	49.3	2.3%	31.6
9/9/2024 11:00:00 AM	38.0	100	118	45	0.0	0.0%	31.8
9/9/2024 12:00:00 PM	36.0	110	129	43	0.0	0.0%	29.7
9/9/2024 1:00:00 PM	38.0	97	114	47	46.5	1.8%	32.8
9/9/2024 2:00:00 PM	38.0	121	142	47	46.5	1.4%	32.5
9/9/2024 3:00:00 PM	38.0	115	135	49	47.7	2.2%	31.9
9/9/2024 4:00:00 PM	36.0	165	194	45	0.0	0.0%	30.3
9/9/2024 5:00:00 PM	36.0	187	220	58	58.0	0.5%	30.9
9/9/2024 6:00:00 PM	36.0	188	221	44	0.0	0.0%	31.8
9/9/2024 7:00:00 PM	36.0	163	192	55	52.0	1.0%	31.8
9/9/2024 8:00:00 PM	38.0	162	191	49	47.8	2.6%	31.9
9/9/2024 9:00:00 PM	38.0	93	109	50	50.0	0.9%	32.6
9/9/2024 10:00:00 PM	37.0	65	76	45	0.0	0.0%	32.0
9/9/2024 11:00:00 PM	35.0	43	51	43	0.0	0.0%	29.9
9/10/2024 12:00:00 AM	39.0	16	19	55	54.0	10.5%	32.1
9/10/2024 1:00:00 AM	36.0	10	12	42	0.0	0.0%	29.4
9/10/2024 2:00:00 AM	35.0	3	3	35	0.0	0.0%	29.3
9/10/2024 3:00:00 AM	30.0	4	5	34	0.0	0.0%	26.0
9/10/2024 4:00:00 AM	32.0	5	6	39	0.0	0.0%	29.7
9/10/2024 5:00:00 AM	30.0	7	8	36	0.0	0.0%	25.8
9/10/2024 6:00:00 AM	39.0	3	3	39	0.0	0.0%	30.3
9/10/2024 7:00:00 AM	33.0	16	19	44	0.0	0.0%	28.7
9/10/2024 8:00:00 AM	38.0	50	59	48	48.0	1.7%	31.8
9/10/2024 9:00:00 AM	37.0	102	120	44	0.0	0.0%	31.7
9/10/2024 10:00:00 AM	37.0	123	145	53	49.5	1.4%	31.3
9/10/2024 11:00:00 AM	37.0	112	132	53	49.5	1.5%	30.8
9/10/2024 12:00:00 PM	37.0	101	119	43	0.0	0.0%	30.8
9/10/2024 1:00:00 PM	37.0	120	141	48	48.0	0.7%	31.3
9/10/2024 2:00:00 PM	38.0	116	137	46	46.0	0.7%	32.5
9/10/2024 3:00:00 PM	37.0	134	158	49	49.0	0.6%	31.4
9/10/2024 4:00:00 PM	29.0	2	2	29	0.0	0.0%	27.0

Outgoing Summary
Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/5/2024 12:00:00 AM	38.0	610	718	50	47.1	1.1%	32.2
9/6/2024 12:00:00 AM	37.0	2127	2502	63	48.1	1.2%	31.1
9/7/2024 12:00:00 AM	37.0	2148	2527	52	47.5	0.9%	31.5
9/8/2024 12:00:00 AM	38.0	1805	2124	62	48.8	1.5%	31.8
9/9/2024 12:00:00 AM	38.0	1479	1740	51	47.6	1.3%	32.0
9/10/2024 12:00:00 AM	37.0	1928	2268	58	49.3	1.1%	31.5
9/10/2024 3:59:59 PM	37.0	909	1069	53	48.6	0.7%	31.3

Outgoing Weekly Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	15	12	18	22	13.5	20	40
1 - 2	*	*	*	6	5	10	11	5.5	10.5	40
2 - 3	*	*	*	4	2	9	9	3	9	30.5
3 - 4	*	*	*	3	1	3	2	2	2.5	37
4 - 5	*	*	*	4	6	2	4	5	3	31
5 - 6	*	*	*	4	5	7	2	4.5	4.5	29.5
6 - 7	*	*	*	15	17	14	4	16	9	32.25
7 - 8	*	*	*	69	58	26	11	63.5	18.5	36.65
8 - 9	*	*	*	129	120	31	27	124.5	29	34.8
9 - 10	*	*	*	115	109	57	46	112	51.5	36.9
10 - 11	*	*	*	151	140	90	80	145.5	85	35.7
11 - 12	*	*	*	149	110	134	141	129.5	137.5	36.25
12 - 13	*	*	*	159	161	164	125	160	144.5	37.25
13 - 14	*	*	*	143	154	201	203	148.5	202	37.05
14 - 15	*	*	*	168	198	170	146	183	158	36
15 - 16	*	*	*	204	189	164	136	196.5	150	34.8
16 - 17	*	*	*	233	232	173	122	232.5	147.5	35.75
17 - 18	*	*	*	246	246	188	156	246	172	36.6
18 - 19	*	*	173	236	244	201	155	217.67	178	36.3
19 - 20	*	*	232	171	201	161	110	201.33	135.5	36.7
20 - 21	*	*	113	103	130	127	92	115.33	109.5	36.77
21 - 22	*	*	118	95	89	93	81	100.67	87	36.57
22 - 23	*	*	58	62	66	46	37	62	41.5	36.17
23 - 24	*	*	24	18	32	35	18	24.67	26.5	37.33
Totals	0	0	718	2502	2527	2124	1740			
% of Total	0%	0%	7.47%	26.03%	26.29%	22.1%	18.1%			

Outgoing Weekly Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	10	12	*	*	*	*	*	11	0	33
1 - 2	7	3	*	*	*	*	*	5	0	33
2 - 3	3	5	*	*	*	*	*	4	0	34.5
3 - 4	4	6	*	*	*	*	*	5	0	32.5
4 - 5	7	8	*	*	*	*	*	7.5	0	34
5 - 6	4	3	*	*	*	*	*	3.5	0	34.25
6 - 7	13	19	*	*	*	*	*	16	0	37.5
7 - 8	48	59	*	*	*	*	*	53.5	0	35.65
8 - 9	131	120	*	*	*	*	*	125.5	0	37.05
9 - 10	130	145	*	*	*	*	*	137.5	0	37.05
10 - 11	118	132	*	*	*	*	*	125	0	36.95
11 - 12	129	119	*	*	*	*	*	124	0	35.75
12 - 13	114	141	*	*	*	*	*	127.5	0	37.5
13 - 14	142	137	*	*	*	*	*	139.5	0	37.45
14 - 15	135	158	*	*	*	*	*	146.5	0	37
15 - 16	194	2	*	*	*	*	*	98	0	32.2
16 - 17	220	*	*	*	*	*	*	220	0	35.5
17 - 18	221	*	*	*	*	*	*	221	0	35.9
18 - 19	192	*	*	*	*	*	*	192	0	35.7
19 - 20	191	*	*	*	*	*	*	191	0	37.1
20 - 21	109	*	*	*	*	*	*	109	0	37.7
21 - 22	76	*	*	*	*	*	*	76	0	37
22 - 23	51	*	*	*	*	*	*	51	0	35
23 - 24	19	*	*	*	*	*	*	19	0	38.5
Totals	2268	1069	0	0	0	0	0			
% of Total	67.97%	32.03%	0%	0%	0%	0%	0%			

Outgoing Monthly Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	10	12	*	15	12	18	22	12.25	20	36.5
1 - 2	7	3	*	6	5	10	11	5.25	10.5	36.5
2 - 3	3	5	*	4	2	9	9	3.5	9	32.5
3 - 4	4	6	*	3	1	3	2	3.5	2.5	34.75
4 - 5	7	8	*	4	6	2	4	6.25	3	32.5
5 - 6	4	3	*	4	5	7	2	4	4.5	31.88
6 - 7	13	19	*	15	17	14	4	16	9	34.88
7 - 8	48	59	*	69	58	26	11	58.5	18.5	36.15
8 - 9	131	120	*	129	120	31	27	125	29	35.92
9 - 10	130	145	*	115	109	57	46	124.75	51.5	36.98
10 - 11	118	132	*	151	140	90	80	135.25	85	36.33
11 - 12	129	119	*	149	110	134	141	126.75	137.5	36
12 - 13	114	141	*	159	161	164	125	143.75	144.5	37.38
13 - 14	142	137	*	143	154	201	203	144	202	37.25
14 - 15	135	158	*	168	198	170	146	164.75	158	36.5
15 - 16	194	2	*	204	189	164	136	147.25	150	33.5
16 - 17	220	*	*	233	232	173	122	228.33	147.5	35.67
17 - 18	221	*	*	246	246	188	156	237.67	172	36.37
18 - 19	192	*	173	236	244	201	155	211.25	178	36.15
19 - 20	191	*	232	171	201	161	110	198.75	135.5	36.8
20 - 21	109	*	113	103	130	127	92	113.75	109.5	37
21 - 22	76	*	118	95	89	93	81	94.5	87	36.68
22 - 23	51	*	58	62	66	46	37	59.25	41.5	35.88
23 - 24	19	*	24	18	32	35	18	23.25	26.5	37.62
Totals	2268	1069	718	2502	2527	2124	1740			
% of Total	17.52%	8.26%	5.55%	19.32%	19.52%	16.4%	13.44%			

Outgoing Weekly Speeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	33.6	30.08	27.5	29.5	32.04	28.6	40
1 - 2	*	*	*	31.17	33	31.8	32.82	32	32.33	40
2 - 3	*	*	*	33.25	24	28.11	27.11	30.17	27.61	30.5
3 - 4	*	*	*	28.67	44	30	28	32.5	29.2	37
4 - 5	*	*	*	26	30.67	32	28.5	28.8	29.67	31
5 - 6	*	*	*	25.5	30.8	37.57	37	28.44	37.44	29.5
6 - 7	*	*	*	29.4	29.18	30.36	32.25	29.28	30.78	32.25
7 - 8	*	*	*	31.07	31.28	30.69	30.64	31.17	30.68	36.65
8 - 9	*	*	*	30.14	30.02	33.32	30.04	30.08	31.79	34.8
9 - 10	*	*	*	31.4	31.17	32.37	32.07	31.29	32.23	36.9
10 - 11	*	*	*	30.03	30.04	31.8	31.9	30.03	31.85	35.7
11 - 12	*	*	*	31.17	30.43	32.1	31.99	30.86	32.04	36.25
12 - 13	*	*	*	31.11	31.38	32.33	32.9	31.24	32.57	37.25
13 - 14	*	*	*	31.59	31.88	30.78	31.9	31.74	31.34	37.05
14 - 15	*	*	*	30.62	31.52	31.89	31.64	31.11	31.78	36
15 - 16	*	*	*	29.52	31.41	32.72	31.99	30.43	32.39	34.8
16 - 17	*	*	*	30.75	32.22	31.87	31.81	31.49	31.84	35.75
17 - 18	*	*	*	31.88	31.92	31.63	32.99	31.9	32.25	36.6
18 - 19	*	*	32.22	31.88	32.51	31.98	32.12	32.21	32.04	36.3
19 - 20	*	*	32.24	32.19	31.3	32.06	32.13	31.91	32.08	36.7
20 - 21	*	*	32.76	31.71	32.28	31.7	33.14	32.27	32.31	36.77
21 - 22	*	*	31.58	32.55	31.12	31.92	31.3	31.75	31.63	36.57
22 - 23	*	*	31.88	30.48	31.15	31.37	31.68	31.16	31.51	36.17
23 - 24	*	*	32.33	32.83	30.59	31.71	32.06	31.7	31.83	37.33
Totals	0	0	32.2	31.1	31.5	31.8	32			
% of Total	0%	0%	20.3%	19.61%	19.86%	20.05%	20.18%			

Outgoing Weekly Speeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	27.6	29.42	*	*	*	*	*	28.59	*	33
1 - 2	28.86	29.33	*	*	*	*	*	29	*	33
2 - 3	31	26	*	*	*	*	*	27.88	*	34.5
3 - 4	32	29.67	*	*	*	*	*	30.6	*	32.5
4 - 5	28	25.75	*	*	*	*	*	26.8	*	34
5 - 6	28.25	30.33	*	*	*	*	*	29.14	*	34.25
6 - 7	33.15	28.74	*	*	*	*	*	30.53	*	37.5
7 - 8	29.77	31.78	*	*	*	*	*	30.88	*	35.65
8 - 9	32.05	31.67	*	*	*	*	*	31.87	*	37.05
9 - 10	31.64	31.3	*	*	*	*	*	31.46	*	37.05
10 - 11	31.77	30.77	*	*	*	*	*	31.24	*	36.95
11 - 12	29.65	30.84	*	*	*	*	*	30.22	*	35.75
12 - 13	32.76	31.26	*	*	*	*	*	31.93	*	37.5
13 - 14	32.46	32.53	*	*	*	*	*	32.5	*	37.45
14 - 15	31.93	31.39	*	*	*	*	*	31.64	*	37
15 - 16	30.32	27	*	*	*	*	*	30.29	*	32.2
16 - 17	30.93	*	*	*	*	*	*	30.93	*	35.5
17 - 18	31.76	*	*	*	*	*	*	31.76	*	35.9
18 - 19	31.81	*	*	*	*	*	*	31.81	*	35.7
19 - 20	31.86	*	*	*	*	*	*	31.86	*	37.1
20 - 21	32.63	*	*	*	*	*	*	32.63	*	37.7
21 - 22	31.97	*	*	*	*	*	*	31.97	*	37
22 - 23	29.9	*	*	*	*	*	*	29.9	*	35
23 - 24	32.05	*	*	*	*	*	*	32.05	*	38.5
Totals	31.5	31.3	0	0	0	0	0			
% of Total	50.16%	49.84%	0%	0%	0%	0%	0%			

Outgoing Monthly Speeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	27.6	29.42	*	33.6	30.08	27.5	29.5	30.49	28.6	36.5
1 - 2	28.86	29.33	*	31.17	33	31.8	32.82	30.57	32.33	36.5
2 - 3	31	26	*	33.25	24	28.11	27.11	28.86	27.61	32.5
3 - 4	32	29.67	*	28.67	44	30	28	31.14	29.2	34.75
4 - 5	28	25.75	*	26	30.67	32	28.5	27.6	29.67	32.5
5 - 6	28.25	30.33	*	25.5	30.8	37.57	37	28.75	37.44	31.88
6 - 7	33.15	28.74	*	29.4	29.18	30.36	32.25	29.91	30.78	34.88
7 - 8	29.77	31.78	*	31.07	31.28	30.69	30.64	31.03	30.68	36.15
8 - 9	32.05	31.67	*	30.14	30.02	33.32	30.04	30.98	31.79	35.92
9 - 10	31.64	31.3	*	31.4	31.17	32.37	32.07	31.38	32.23	36.98
10 - 11	31.77	30.77	*	30.03	30.04	31.8	31.9	30.59	31.85	36.33
11 - 12	29.65	30.84	*	31.17	30.43	32.1	31.99	30.55	32.04	36
12 - 13	32.76	31.26	*	31.11	31.38	32.33	32.9	31.55	32.57	37.38
13 - 14	32.46	32.53	*	31.59	31.88	30.78	31.9	32.11	31.34	37.25
14 - 15	31.93	31.39	*	30.62	31.52	31.89	31.64	31.34	31.78	36.5
15 - 16	30.32	27	*	29.52	31.41	32.72	31.99	30.38	32.39	33.5
16 - 17	30.93	*	*	30.75	32.22	31.87	31.81	31.31	31.84	35.67
17 - 18	31.76	*	*	31.88	31.92	31.63	32.99	31.86	32.25	36.37
18 - 19	31.81	*	32.22	31.88	32.51	31.98	32.12	32.12	32.04	36.15
19 - 20	31.86	*	32.24	32.19	31.3	32.06	32.13	31.9	32.08	36.8
20 - 21	32.63	*	32.76	31.71	32.28	31.7	33.14	32.36	32.31	37
21 - 22	31.97	*	31.58	32.55	31.12	31.92	31.3	31.8	31.63	36.68
22 - 23	29.9	*	31.88	30.48	31.15	31.37	31.68	30.89	31.51	35.88
23 - 24	32.05	*	32.33	32.83	30.59	31.71	32.06	31.77	31.83	37.62
Totals	31.5	31.3	32.2	31.1	31.5	31.8	32			
% of Total	14.23%	14.14%	14.54%	14.05%	14.23%	14.36%	14.45%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	44	36	34	34	40	34	40
1 - 2	*	*	*	41	39	36	34	40	35	40
2 - 3	*	*	*	31	30	32	33	30.5	32.5	30.5
3 - 4	*	*	*	30	44	37	30	37	33.5	37
4 - 5	*	*	*	29	33	39	30	31	34.5	31
5 - 6	*	*	*	25	34	42	42	29.5	42	29.5
6 - 7	*	*	*	32.5	32	33	34	32.25	33.5	32.25
7 - 8	*	*	*	38	35.3	35.7	34	36.65	34.85	36.65
8 - 9	*	*	*	35.7	33.9	38.7	35	34.8	36.85	34.8
9 - 10	*	*	*	37.2	36.6	37.8	38	36.9	37.9	36.9
10 - 11	*	*	*	36.3	35.1	36	38	35.7	37	35.7
11 - 12	*	*	*	36.2	36.3	37.6	37	36.25	37.3	36.25
12 - 13	*	*	*	36.9	37.6	37.2	39.3	37.25	38.25	37.25
13 - 14	*	*	*	36.7	37.4	35.6	36.3	37.05	35.95	37.05
14 - 15	*	*	*	35.7	36.3	37	36.3	36	36.65	36
15 - 16	*	*	*	34	35.6	37.3	37.6	34.8	37.45	34.8
16 - 17	*	*	*	35.1	36.4	37.4	36.3	35.75	36.85	35.75
17 - 18	*	*	*	36.4	36.8	36.8	38.2	36.6	37.5	36.6
18 - 19	*	*	36.8	35.4	36.7	37.2	36.8	36.3	37	36.3
19 - 20	*	*	37.1	36.5	36.5	36.4	36.3	36.7	36.35	36.7
20 - 21	*	*	37.6	35.7	37	35.7	39	36.77	37.35	36.77
21 - 22	*	*	36.4	36.9	36.4	37	37	36.57	37	36.57
22 - 23	*	*	38.5	35	35	38	36	36.17	37	36.17
23 - 24	*	*	40.3	36	35.7	40	37	37.33	38.5	37.33
Totals	0	0	226.7	846.2	862.6	884.4	865.1			
% of Total	0%	0%	6.15%	22.96%	23.41%	24%	23.48%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	30	36	*	*	*	*	*	33	0	33
1 - 2	31	35	*	*	*	*	*	33	0	33
2 - 3	39	30	*	*	*	*	*	34.5	0	34.5
3 - 4	33	32	*	*	*	*	*	32.5	0	32.5
4 - 5	38	30	*	*	*	*	*	34	0	34
5 - 6	29.5	39	*	*	*	*	*	34.25	0	34.25
6 - 7	42	33	*	*	*	*	*	37.5	0	37.5
7 - 8	33.3	38	*	*	*	*	*	35.65	0	35.65
8 - 9	37.2	36.9	*	*	*	*	*	37.05	0	37.05
9 - 10	37.4	36.7	*	*	*	*	*	37.05	0	37.05
10 - 11	37.5	36.4	*	*	*	*	*	36.95	0	36.95
11 - 12	35.2	36.3	*	*	*	*	*	35.75	0	35.75
12 - 13	38	37	*	*	*	*	*	37.5	0	37.5
13 - 14	37.8	37.1	*	*	*	*	*	37.45	0	37.45
14 - 15	37.5	36.5	*	*	*	*	*	37	0	37
15 - 16	35.4	29	*	*	*	*	*	32.2	0	32.2
16 - 17	35.5	*	*	*	*	*	*	35.5	0	35.5
17 - 18	35.9	*	*	*	*	*	*	35.9	0	35.9
18 - 19	35.7	*	*	*	*	*	*	35.7	0	35.7
19 - 20	37.1	*	*	*	*	*	*	37.1	0	37.1
20 - 21	37.7	*	*	*	*	*	*	37.7	0	37.7
21 - 22	37	*	*	*	*	*	*	37	0	37
22 - 23	35	*	*	*	*	*	*	35	0	35
23 - 24	38.5	*	*	*	*	*	*	38.5	0	38.5
Totals	864.2	558.9	0	0	0	0	0			
% of Total	60.73%	39.27%	0%	0%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	30	36	*	44	36	34	34	36.5	34	36.5
1 - 2	31	35	*	41	39	36	34	36.5	35	36.5
2 - 3	39	30	*	31	30	32	33	32.5	32.5	32.5
3 - 4	33	32	*	30	44	37	30	34.75	33.5	34.75
4 - 5	38	30	*	29	33	39	30	32.5	34.5	32.5
5 - 6	29.5	39	*	25	34	42	42	31.88	42	31.88
6 - 7	42	33	*	32.5	32	33	34	34.88	33.5	34.88
7 - 8	33.3	38	*	38	35.3	35.7	34	36.15	34.85	36.15
8 - 9	37.2	36.9	*	35.7	33.9	38.7	35	35.92	36.85	35.92
9 - 10	37.4	36.7	*	37.2	36.6	37.8	38	36.98	37.9	36.98
10 - 11	37.5	36.4	*	36.3	35.1	36	38	36.33	37	36.33
11 - 12	35.2	36.3	*	36.2	36.3	37.6	37	36	37.3	36
12 - 13	38	37	*	36.9	37.6	37.2	39.3	37.38	38.25	37.38
13 - 14	37.8	37.1	*	36.7	37.4	35.6	36.3	37.25	35.95	37.25
14 - 15	37.5	36.5	*	35.7	36.3	37	36.3	36.5	36.65	36.5
15 - 16	35.4	29	*	34	35.6	37.3	37.6	33.5	37.45	33.5
16 - 17	35.5	*	*	35.1	36.4	37.4	36.3	35.67	36.85	35.67
17 - 18	35.9	*	*	36.4	36.8	36.8	38.2	36.37	37.5	36.37
18 - 19	35.7	*	36.8	35.4	36.7	37.2	36.8	36.15	37	36.15
19 - 20	37.1	*	37.1	36.5	36.5	36.4	36.3	36.8	36.35	36.8
20 - 21	37.7	*	37.6	35.7	37	35.7	39	37	37.35	37
21 - 22	37	*	36.4	36.9	36.4	37	37	36.68	37	36.68
22 - 23	35	*	38.5	35	35	38	36	35.88	37	35.88
23 - 24	38.5	*	40.3	36	35.7	40	37	37.62	38.5	37.62

Summary of Violators

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	89	29.6	2	49.5
01:00:00	42	31.5	1	49.0
02:00:00	32	28.2	0	0.0
03:00:00	19	30.6	0	0.0
04:00:00	31	28.0	1	50.0
05:00:00	25	31.9	0	0.0
06:00:00	82	30.1	1	47.0
07:00:00	271	31.0	5	47.0
08:00:00	558	31.1	4	49.5
09:00:00	602	31.5	9	48.4
10:00:00	711	30.9	7	49.6
11:00:00	782	31.1	6	47.5
12:00:00	864	31.9	17	47.4
13:00:00	980	31.8	11	49.3
14:00:00	975	31.5	9	49.3
15:00:00	889	31.1	8	47.3
16:00:00	980	31.5	7	48.4
17:00:00	1057	32.0	12	47.4
18:00:00	1201	32.1	9	49.0
19:00:00	1066	31.9	11	47.5
20:00:00	674	32.3	9	48.0
21:00:00	552	31.7	9	48.3
22:00:00	320	31.0	5	46.8
23:00:00	146	31.8	5	51.0

Outgoing Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	0	1	7	38	78	35	11	2	1	0	0	0	0	0	0	0	0	0	173	32.2	36.8	27 to 37	75.1	2	1.2	1	168	4	
9/4/2024	19:00	0	2	12	50	102	43	21	2	0	0	0	0	0	0	0	0	0	0	232	32.2	37.1	27 to 37	71.6	*	*	2	221	9	
9/4/2024	20:00	0	0	4	25	45	27	12	0	0	0	0	0	0	0	0	0	0	0	113	32.8	37.6	27 to 37	73.5	*	*	0	109	4	
9/4/2024	21:00	0	0	10	33	45	21	7	2	0	0	0	0	0	0	0	0	0	0	118	31.6	36.4	27 to 37	74.6	2	1.7	1	114	3	
9/4/2024	22:00	0	0	5	18	21	6	5	3	0	0	0	0	0	0	0	0	0	0	58	31.9	38.5	24 to 34	72.4	3	5.2	0	57	1	
9/4/2024	23:00	0	0	4	7	4	4	4	1	0	0	0	0	0	0	0	0	0	0	24	32.3	40.3	23 to 33	58.3	1	4.2	0	23	1	
24 Hr Summary		0	3	42	171	295	136	60	10	1	0	0	0	0	0	0	0	0	0	718	32.2	38	27 to 37	72.4	8	1.1	4	692	22	

Outgoing Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/5/2024	00:00	0	0	2	1	7	2	1	1	1	0	0	0	0	0	0	0	0	0	15	33.6	44	22 to 32	60.0	2	13.3	0	14	1
9/5/2024	01:00	0	0	2	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	6	31.2	41	20 to 30	50.0	*	*	0	6	0
9/5/2024	02:00	0	0	0	1	2	0	1	0	0	0	0	0	0	0	0	0	0	0	4	33.3	31	21 to 31	75.0	*	*	0	4	0
9/5/2024	03:00	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	28.7	30	20 to 30	100.0	*	*	0	3	0
9/5/2024	04:00	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	26	29	22 to 32	75.0	*	*	0	4	0
9/5/2024	05:00	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	25.5	25	18 to 28	100.0	*	*	0	4	0
9/5/2024	06:00	0	0	2	6	6	0	1	0	0	0	0	0	0	0	0	0	0	0	15	29.4	32.5	23 to 33	86.7	*	*	0	12	3
9/5/2024	07:00	1	1	9	15	27	7	4	5	0	0	0	0	0	0	0	0	0	0	69	31.1	38	23 to 33	66.7	3	4.3	1	61	7
9/5/2024	08:00	0	1	15	46	42	20	5	0	0	0	0	0	0	0	0	0	0	0	129	30.1	35.7	24 to 34	72.1	*	*	0	119	10
9/5/2024	09:00	0	1	7	37	38	26	4	2	0	0	0	0	0	0	0	0	0	0	115	31.4	37.2	25 to 35	68.7	1	0.9	3	103	9
9/5/2024	10:00	0	4	22	51	39	26	5	4	0	0	0	0	0	0	0	0	0	0	151	30	36.3	24 to 34	66.9	2	1.3	1	133	17
9/5/2024	11:00	0	0	11	50	46	31	10	1	0	0	0	0	0	0	0	0	0	0	149	31.2	36.2	25 to 35	70.5	1	0.7	1	136	12
9/5/2024	12:00	0	3	25	43	46	27	6	9	0	0	0	0	0	0	0	0	0	0	159	31.1	36.9	23 to 33	63.5	6	3.8	2	130	27
9/5/2024	13:00	0	4	9	37	47	37	6	2	0	1	0	0	0	0	0	0	0	0	143	31.6	36.7	25 to 35	67.1	1	0.7	1	126	16
9/5/2024	14:00	1	4	19	46	63	27	5	2	0	0	1	0	0	0	0	0	0	0	168	30.6	35.7	24 to 34	70.2	1	0.6	3	140	25
9/5/2024	15:00	0	3	22	86	62	26	4	1	0	0	0	0	0	0	0	0	0	0	204	29.5	34	25 to 35	77.9	1	0.5	4	183	17
9/5/2024	16:00	0	4	22	70	88	36	10	3	0	0	0	0	0	0	0	0	0	0	233	30.8	35.1	26 to 36	73.8	1	0.4	1	213	19
9/5/2024	17:00	0	0	16	68	91	50	15	6	0	0	0	0	0	0	0	0	0	0	246	31.9	36.4	25 to 35	72.8	3	1.2	4	224	18
9/5/2024	18:00	1	0	9	56	111	48	9	2	0	0	0	0	0	0	0	0	0	0	236	31.9	35.4	26 to 36	81.8	2	0.8	2	221	13
9/5/2024	19:00	0	0	5	50	63	35	15	2	1	0	0	0	0	0	0	0	0	0	171	32.2	36.5	26 to 36	73.1	2	1.2	1	166	4
9/5/2024	20:00	0	0	5	25	52	14	4	3	0	0	0	0	0	0	0	0	0	0	103	31.7	35.7	27 to 37	80.6	3	2.9	0	101	2
9/5/2024	21:00	0	0	5	20	34	30	4	2	0	0	0	0	0	0	0	0	0	0	95	32.5	36.9	28 to 38	77.9	*	*	0	91	4
9/5/2024	22:00	0	0	9	16	23	10	4	0	0	0	0	0	0	0	0	0	0	0	62	30.5	35	25 to 35	71.0	*	*	0	59	3
9/5/2024	23:00	0	0	0	3	9	3	3	0	0	0	0	0	0	0	0	0	0	0	18	32.8	36	26 to 36	77.8	*	*	0	18	0
24 Hr Summary		3	26	217	733	901	455	118	45	2	1	1	0	0	0	0	0	0	0	2502	31.1	37	25 to 35	71.0	29	1.2	24	2271	207

Outgoing Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/6/2024	00:00	0	0	1	6	2	2	1	0	0	0	0	0	0	0	0	0	0	0	12	30.1	36	22 to 32	75.0	*	*	0	12	0
9/6/2024	01:00	0	0	0	2	1	1	1	0	0	0	0	0	0	0	0	0	0	0	5	33	39	21 to 31	60.0	*	*	0	5	0
9/6/2024	02:00	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24	30	8 to 18	50.0	*	*	0	2	0
9/6/2024	03:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	44	44	34 to 44	100.0	*	*	0	1	0
9/6/2024	04:00	0	0	1	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	6	30.7	33	23 to 33	83.3	*	*	0	6	0
9/6/2024	05:00	0	0	1	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	5	30.8	34	28 to 38	80.0	*	*	0	5	0
9/6/2024	06:00	0	0	3	4	10	0	0	0	0	0	0	0	0	0	0	0	0	0	17	29.2	32	22 to 32	82.4	*	*	0	16	1
9/6/2024	07:00	0	0	5	19	20	10	3	1	0	0	0	0	0	0	0	0	0	0	58	31.3	35.3	25 to 35	74.1	1	1.7	0	55	3
9/6/2024	08:00	0	1	7	49	46	14	2	1	0	0	0	0	0	0	0	0	0	0	120	30	33.9	24 to 34	81.7	*	*	0	114	6
9/6/2024	09:00	0	1	13	35	29	22	7	2	0	0	0	0	0	0	0	0	0	0	109	31.2	36.6	27 to 37	68.8	2	1.8	0	100	9
9/6/2024	10:00	0	4	22	37	48	20	9	0	0	0	0	0	0	0	0	0	0	0	140	30	35.1	26 to 36	68.6	*	*	1	124	15
9/6/2024	11:00	0	3	7	44	36	13	5	2	0	0	0	0	0	0	0	0	0	0	110	30.4	36.3	23 to 33	75.5	1	0.9	1	100	9
9/6/2024	12:00	0	7	16	35	57	29	12	5	0	0	0	0	0	0	0	0	0	0	161	31.4	37.6	26 to 36	63.4	3	1.9	1	142	18
9/6/2024	13:00	0	2	7	47	49	35	11	3	0	0	0	0	0	0	0	0	0	0	154	31.9	37.4	25 to 35	69.5	2	1.3	1	140	13
9/6/2024	14:00	0	0	16	53	76	40	13	0	0	0	0	0	0	0	0	0	0	0	198	31.5	36.3	26 to 36	72.2	*	*	4	182	12
9/6/2024	15:00	0	1	12	55	78	30	8	5	0	0	0	0	0	0	0	0	0	0	189	31.4	35.6	26 to 36	76.7	5	2.6	0	177	12
9/6/2024	16:00	0	2	10	50	99	55	12	4	0	0	0	0	0	0	0	0	0	0	232	32.2	36.4	27 to 37	79.3	2	0.9	3	206	23
9/6/2024	17:00	0	2	11	63	104	44	20	1	1	0	0	0	0	0	0	0	0	0	246	31.9	36.8	26 to 36	74.8	1	0.4	3	226	17
9/6/2024	18:00	0	1	11	47	109	58	15	2	1	0	0	0	0	0	0	0	0	0	244	32.5	36.7	27 to 37	76.6	2	0.8	0	229	15
9/6/2024	19:00	0	1	12	67	70	38	12	1	0	0	0	0	0	0	0	0	0	0	201	31.3	36.5	27 to 37	75.1	1	0.5	3	186	12
9/6/2024	20:00	0	0	6	36	48	24	12	4	0	0	0	0	0	0	0	0	0	0	130	32.3	37	27 to 37	73.1	1	0.8	2	122	6
9/6/2024	21:00	0	0	9	23	37	15	3	2	0	0	0	0	0	0	0	0	0	0	89	31.1	36.4	23 to 33	73.0	2	2.2	2	83	4
9/6/2024	22:00	0	1	4	24	21	11	3	2	0	0	0	0	0	0	0	0	0	0	66	31.2	35	26 to 36	78.8	1	1.5	0	65	1
9/6/2024	23:00	0	0	4	13	6	7	1	1	0	0	0	0	0	0	0	0	0	0	32	30.6	35.7	23 to 33	68.8	*	*	0	29	3
24 Hr Summary		0	27	178	711	953	469	151	36	2	0	0	0	0	0	0	0	0	0	2527	31.5	37	26 to 36	72.3	24	0.9	21	2327	179

Outgoing Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/7/2024	00:00	0	2	4	6	3	2	1	0	0	0	0	0	0	0	0	0	0	0	18	27.5	34	21 to 31	66.7	*	*	0	16	2
9/7/2024	01:00	0	0	1	3	3	1	2	0	0	0	0	0	0	0	0	0	0	0	10	31.8	36	21 to 31	70.0	*	*	0	10	0
9/7/2024	02:00	0	1	1	3	3	1	0	0	0	0	0	0	0	0	0	0	0	0	9	28.1	32	22 to 32	77.8	*	*	0	9	0
9/7/2024	03:00	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	30	37	21 to 31	66.7	*	*	0	3	0
9/7/2024	04:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	32	39	15 to 25	50.0	*	*	0	2	0
9/7/2024	05:00	0	0	1	0	1	1	4	0	0	0	0	0	0	0	0	0	0	0	7	37.6	42	33 to 43	85.7	*	*	1	6	0
9/7/2024	06:00	0	0	1	6	6	0	1	0	0	0	0	0	0	0	0	0	0	0	14	30.4	33	24 to 34	85.7	*	*	0	14	0
9/7/2024	07:00	0	0	5	7	8	3	2	1	0	0	0	0	0	0	0	0	0	0	26	30.7	35.7	23 to 33	61.5	*	*	0	20	6
9/7/2024	08:00	0	0	2	4	14	7	3	1	0	0	0	0	0	0	0	0	0	0	31	33.3	38.7	29 to 39	74.2	1	3.2	0	29	2
9/7/2024	09:00	0	0	6	19	8	16	5	3	0	0	0	0	0	0	0	0	0	0	57	32.4	37.8	28 to 38	59.6	1	1.8	0	53	4
9/7/2024	10:00	0	2	2	27	38	14	6	0	0	0	1	0	0	0	0	0	0	0	90	31.8	36	25 to 35	76.7	1	1.1	2	84	4
9/7/2024	11:00	0	0	11	37	43	29	12	2	0	0	0	0	0	0	0	0	0	0	134	32.1	37.6	26 to 36	70.1	1	0.7	1	123	10
9/7/2024	12:00	0	1	10	41	59	36	13	3	1	0	0	0	0	0	0	0	0	0	164	32.3	37.2	26 to 36	73.2	3	1.8	1	155	8
9/7/2024	13:00	0	1	20	67	70	33	7	1	1	1	0	0	0	0	0	0	0	0	201	30.8	35.6	26 to 36	72.1	3	1.5	2	189	10
9/7/2024	14:00	0	0	7	54	57	40	9	3	0	0	0	0	0	0	0	0	0	0	170	31.9	37	26 to 36	72.9	3	1.8	2	160	8
9/7/2024	15:00	0	2	5	41	60	38	14	4	0	0	0	0	0	0	0	0	0	0	164	32.7	37.3	27 to 37	76.2	1	0.6	0	154	10
9/7/2024	16:00	0	2	13	44	65	35	12	2	0	0	0	0	0	0	0	0	0	0	173	31.9	37.4	28 to 38	69.4	2	1.2	4	160	9
9/7/2024	17:00	1	3	15	51	65	34	14	4	1	0	0	0	0	0	0	0	0	0	188	31.6	36.8	27 to 37	70.7	5	2.7	3	176	9
9/7/2024	18:00	1	3	10	56	66	49	16	0	0	0	0	0	0	0	0	0	0	0	201	32	37.2	28 to 38	71.1	*	*	2	192	7
9/7/2024	19:00	1	1	9	37	59	43	10	1	0	0	0	0	0	0	0	0	0	0	161	32.1	36.4	27 to 37	77.6	1	0.6	1	152	8
9/7/2024	20:00	0	0	6	37	51	25	6	2	0	0	0	0	0	0	0	0	0	0	127	31.7	35.7	26 to 36	80.3	2	1.6	0	126	1
9/7/2024	21:00	0	0	5	29	36	13	5	4	0	1	0	0	0	0	0	0	0	0	93	31.9	37	25 to 35	76.3	5	5.4	0	90	3
9/7/2024	22:00	0	1	7	11	12	9	4	2	0	0	0	0	0	0	0	0	0	0	46	31.4	38	26 to 36	60.9	1	2.2	0	45	1
9/7/2024	23:00	0	2	3	10	9	5	4	0	2	0	0	0	0	0	0	0	0	0	35	31.7	40	25 to 35	62.9	2	5.7	0	35	0
24 Hr Summary		3	21	145	591	737	436	150	33	5	2	1	0	0	0	0	0	0	0	2124	31.8	38	26 to 36	70.1	32	1.5	19	2003	102

Outgoing Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/8/2024	00:00	0	0	3	10	6	2	1	0	0	0	0	0	0	0	0	0	0	0	22	29.5	34	22 to 32	81.8	*	*	0	21	1
9/8/2024	01:00	0	0	0	4	5	0	1	1	0	0	0	0	0	0	0	0	0	0	11	32.8	34	24 to 34	81.8	1	9.1	0	11	0
9/8/2024	02:00	0	1	2	2	3	1	0	0	0	0	0	0	0	0	0	0	0	0	9	27.1	33	23 to 33	77.8	*	*	0	9	0
9/8/2024	03:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28	30	20 to 30	100.0	*	*	0	2	0
9/8/2024	04:00	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28.5	30	23 to 33	100.0	*	*	0	4	0
9/8/2024	05:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	37	42	32 to 42	100.0	*	*	0	2	0
9/8/2024	06:00	0	0	0	2	1	0	1	0	0	0	0	0	0	0	0	0	0	0	4	32.3	34	24 to 34	75.0	*	*	0	4	0
9/8/2024	07:00	0	0	1	4	4	2	0	0	0	0	0	0	0	0	0	0	0	0	11	30.6	34	26 to 36	90.9	*	*	0	11	0
9/8/2024	08:00	0	2	3	10	5	5	0	1	1	0	0	0	0	0	0	0	0	0	27	30	35	25 to 35	66.7	1	3.7	0	24	3
9/8/2024	09:00	0	0	3	12	19	7	5	0	0	0	0	0	0	0	0	0	0	0	46	32.1	38	26 to 36	73.9	*	*	0	46	0
9/8/2024	10:00	0	1	7	28	15	18	6	5	0	0	0	0	0	0	0	0	0	0	80	31.9	38	22 to 32	58.8	2	2.5	1	76	3
9/8/2024	11:00	0	0	11	39	50	27	8	5	1	0	0	0	0	0	0	0	0	0	141	32	37	26 to 36	70.2	3	2.1	0	136	5
9/8/2024	12:00	0	1	6	24	52	21	16	5	0	0	0	0	0	0	0	0	0	0	125	32.9	39.3	26 to 36	65.6	2	1.6	0	121	4
9/8/2024	13:00	0	1	8	63	78	37	14	2	0	0	0	0	0	0	0	0	0	0	203	31.9	36.3	26 to 36	77.8	2	1.0	1	193	9
9/8/2024	14:00	0	0	10	45	50	32	8	1	0	0	0	0	0	0	0	0	0	0	146	31.6	36.3	27 to 37	76.0	1	0.7	0	140	6
9/8/2024	15:00	0	3	10	33	46	30	12	2	0	0	0	0	0	0	0	0	0	0	136	32	37.6	28 to 38	69.1	1	0.7	0	131	5
9/8/2024	16:00	0	0	9	33	44	29	6	1	0	0	0	0	0	0	0	0	0	0	122	31.8	36.3	27 to 37	74.6	1	0.8	0	120	2
9/8/2024	17:00	1	1	3	35	63	33	15	5	0	0	0	0	0	0	0	0	0	0	156	33	38.2	27 to 37	75.0	3	1.9	1	151	4
9/8/2024	18:00	0	0	9	45	60	26	13	2	0	0	0	0	0	0	0	0	0	0	155	32.1	36.8	26 to 36	73.5	1	0.6	1	151	3
9/8/2024	19:00	0	0	4	29	48	19	8	2	0	0	0	0	0	0	0	0	0	0	110	32.1	36.3	26 to 36	78.2	2	1.8	0	109	1
9/8/2024	20:00	0	0	7	18	31	22	12	1	1	0	0	0	0	0	0	0	0	0	92	33.1	39	27 to 37	66.3	2	2.2	0	89	3
9/8/2024	21:00	0	0	9	23	23	23	2	1	0	0	0	0	0	0	0	0	0	0	81	31.3	37	28 to 38	66.7	*	*	0	79	2
9/8/2024	22:00	0	0	5	8	14	8	2	0	0	0	0	0	0	0	0	0	0	0	37	31.7	36	27 to 37	73.0	*	*	0	36	1
9/8/2024	23:00	0	0	1	5	6	3	3	0	0	0	0	0	0	0	0	0	0	0	18	32.1	37	25 to 35	66.7	*	*	0	17	1
24 Hr Summary		1	10	111	475	627	345	134	34	3	0	0	0	0	0	0	0	0	0	1740	32	38	26 to 36	70.9	22	1.3	4	1683	53

Outgoing Histogram

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/9/2024	00:00	0	1	2	4	2	1	0	0	0	0	0	0	0	0	0	0	0	0	10	27.6	30	23 to 33	80.0	*	*	0	9	1
9/9/2024	01:00	0	0	1	1	5	0	0	0	0	0	0	0	0	0	0	0	0	0	7	28.9	31	21 to 31	85.7	*	*	1	5	1
9/9/2024	02:00	0	0	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	31	39	18 to 28	66.7	*	*	0	3	0
9/9/2024	03:00	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	4	32	33	25 to 35	100.0	*	*	0	4	0
9/9/2024	04:00	1	1	2	0	1	1	0	0	1	0	0	0	0	0	0	0	0	0	7	28	38	13 to 23	42.9	1	14.3	0	7	0
9/9/2024	05:00	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28.3	29.5	20 to 30	100.0	*	*	0	4	0
9/9/2024	06:00	0	0	1	3	4	2	2	1	0	0	0	0	0	0	0	0	0	0	13	33.2	42	25 to 35	69.2	1	7.7	0	12	1
9/9/2024	07:00	0	0	6	14	23	5	0	0	0	0	0	0	0	0	0	0	0	0	48	29.8	33.3	23 to 33	79.2	*	*	0	45	3
9/9/2024	08:00	0	0	6	39	54	20	9	2	1	0	0	0	0	0	0	0	0	0	131	32.1	37.2	24 to 34	73.3	2	1.5	0	125	6
9/9/2024	09:00	0	1	11	39	44	22	9	2	2	0	0	0	0	0	0	0	0	0	130	31.6	37.4	24 to 34	70.0	3	2.3	3	112	15
9/9/2024	10:00	0	2	5	38	37	24	11	1	0	0	0	0	0	0	0	0	0	0	118	31.8	37.5	26 to 36	67.8	*	*	2	103	13
9/9/2024	11:00	0	0	25	43	36	19	6	0	0	0	0	0	0	0	0	0	0	0	129	29.7	35.2	23 to 33	71.3	*	*	3	111	15
9/9/2024	12:00	0	0	7	28	38	29	9	3	0	0	0	0	0	0	0	0	0	0	114	32.8	38	28 to 38	67.5	2	1.8	1	104	9
9/9/2024	13:00	0	0	13	28	49	35	15	2	0	0	0	0	0	0	0	0	0	0	142	32.5	37.8	26 to 36	67.6	2	1.4	0	131	11
9/9/2024	14:00	0	1	10	41	38	32	10	3	0	0	0	0	0	0	0	0	0	0	135	31.9	37.5	26 to 36	70.4	3	2.2	0	125	10
9/9/2024	15:00	0	1	18	71	66	33	4	1	0	0	0	0	0	0	0	0	0	0	194	30.3	35.4	23 to 33	75.8	*	*	1	180	13
9/9/2024	16:00	2	3	17	69	73	44	11	0	0	1	0	0	0	0	0	0	0	0	220	30.9	35.5	26 to 36	74.5	1	0.5	2	205	13
9/9/2024	17:00	0	0	9	53	104	46	9	0	0	0	0	0	0	0	0	0	0	0	221	31.8	35.9	27 to 37	81.4	*	*	0	209	12
9/9/2024	18:00	0	3	8	50	77	42	9	2	0	1	0	0	0	0	0	0	0	0	192	31.8	35.7	26 to 36	79.7	2	1.0	0	180	12
9/9/2024	19:00	0	3	8	56	74	32	13	5	0	0	0	0	0	0	0	0	0	0	191	31.9	37.1	25 to 35	72.3	5	2.6	4	177	10
9/9/2024	20:00	0	0	6	25	43	23	10	1	1	0	0	0	0	0	0	0	0	0	109	32.6	37.7	27 to 37	74.3	1	0.9	0	104	5
9/9/2024	21:00	0	0	4	23	29	15	4	1	0	0	0	0	0	0	0	0	0	0	76	32	37	27 to 37	76.3	*	*	0	75	1
9/9/2024	22:00	0	2	5	19	15	6	4	0	0	0	0	0	0	0	0	0	0	0	51	29.9	35	22 to 32	72.5	*	*	0	47	4
9/9/2024	23:00	0	1	5	3	2	6	0	0	1	1	0	0	0	0	0	0	0	0	19	32.1	38.5	16 to 26	47.4	2	10.5	0	18	1
24 Hr Summary		3	19	169	652	818	439	135	24	6	3	0	0	0	0	0	0	0	0	2268	31.5	37	26 to 36	71.6	25	1.1	17	2095	156

Outgoing Histogram

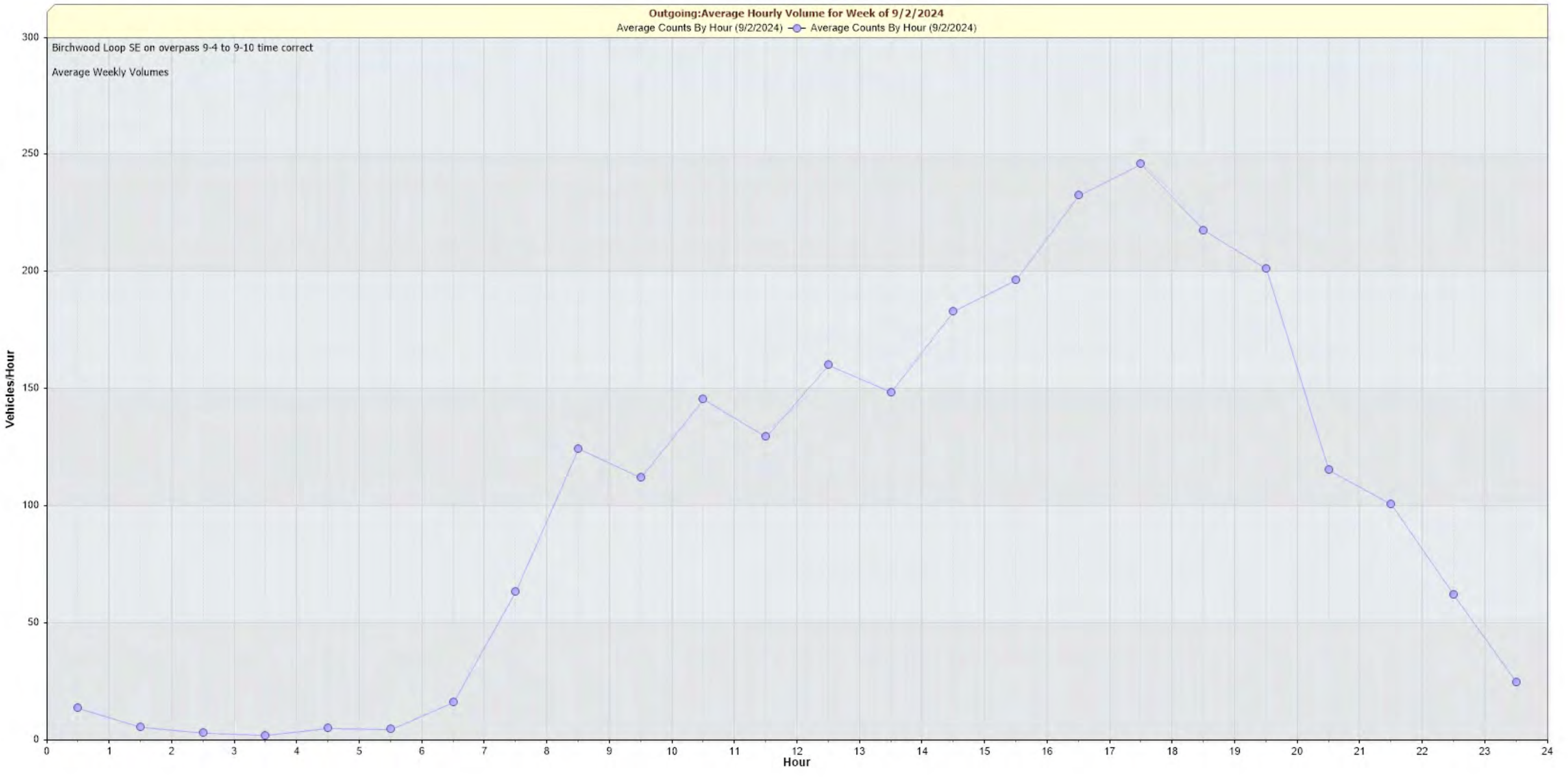
Birchwood Loop SE on overpass 9-4 to 9-10 time correct

from Wed-Sep-04-2024-06-00-PM to Tue-Sep-10-2024-03-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG		
9/10/2024	00:00	0	0	3	5	0	3	1	0	0	0	0	0	0	0	0	0	0	0	12	29.4	36	18 to 28	66.7	*	*	0	11	1		
9/10/2024	01:00	0	0	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	29.3	35	25 to 35	100.0	*	*	0	3	0		
9/10/2024	02:00	0	1	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	5	26	30	20 to 30	60.0	*	*	0	5	0		
9/10/2024	03:00	0	0	1	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	6	29.7	32	22 to 32	66.7	*	*	0	6	0		
9/10/2024	04:00	0	1	2	2	2	1	0	0	0	0	0	0	0	0	0	0	0	0	8	25.8	30	20 to 30	75.0	*	*	0	8	0		
9/10/2024	05:00	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	3	30.3	39	19 to 29	66.7	*	*	0	3	0		
9/10/2024	06:00	0	2	3	7	4	1	2	0	0	0	0	0	0	0	0	0	0	0	19	28.7	33	23 to 33	73.7	*	*	0	16	3		
9/10/2024	07:00	0	1	4	17	21	11	4	1	0	0	0	0	0	0	0	0	0	0	59	31.8	38	24 to 34	67.8	1	1.7	0	55	4		
9/10/2024	08:00	0	0	4	41	45	20	10	0	0	0	0	0	0	0	0	0	0	0	120	31.7	36.9	25 to 35	74.2	*	*	1	111	8		
9/10/2024	09:00	1	2	12	42	48	30	8	1	1	0	0	0	0	0	0	0	0	0	145	31.3	36.7	27 to 37	73.1	2	1.4	1	125	19		
9/10/2024	10:00	0	0	16	49	32	26	7	1	1	0	0	0	0	0	0	0	0	0	132	30.8	36.4	24 to 34	68.2	2	1.5	3	109	20		
9/10/2024	11:00	0	2	15	31	35	30	6	0	0	0	0	0	0	0	0	0	0	0	119	30.8	36.3	27 to 37	66.4	*	*	1	101	17		
9/10/2024	12:00	1	5	13	34	45	31	11	1	0	0	0	0	0	0	0	0	0	0	141	31.3	37	27 to 37	67.4	1	0.7	4	113	24		
9/10/2024	13:00	0	0	10	27	53	36	9	2	0	0	0	0	0	0	0	0	0	0	137	32.5	37.1	28 to 38	73.0	1	0.7	2	119	16		
9/10/2024	14:00	0	1	13	49	47	36	10	2	0	0	0	0	0	0	0	0	0	0	158	31.4	36.5	26 to 36	69.6	1	0.6	1	134	23		
9/10/2024	15:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27	29	19 to 29	100.0	*	*	0	2	0		
9/10/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/10/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/10/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/10/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/10/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/10/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/10/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		2	15	98	312	336	228	68	8	2	0	0	0	0	0	0	0	0	0	1069	31.3	37	27 to 37	67.1	8	0.7	13	921	135		

Outgoing:Average Hourly Volume for Week of 9/2/2024
Average Counts By Hour (9/2/2024) ● Average Counts By Hour (9/2/2024)

Birchwood Loop SE on overpass 9-4 to 9-10 time correct
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/2/2024
Average Hourly WEEKDAY Speeds By Hour (9/2/2024) — Average Hourly WEEKDAY Speeds By Hour (9/2/2024)

Birchwood Loop SE on overpass 9-4 to 9-10 time correct
Average Weekly Speeds

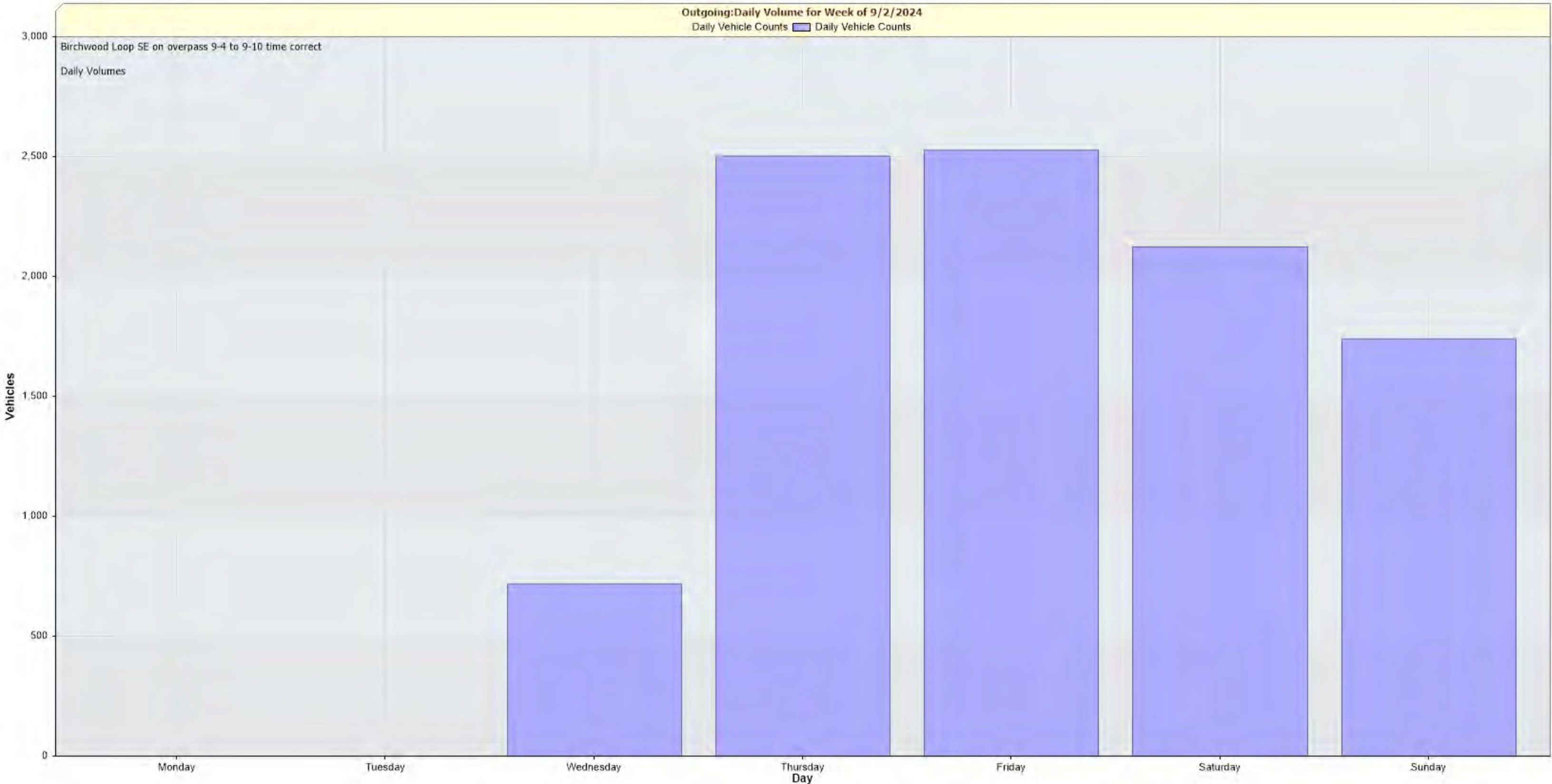


Outgoing: Daily Volume for Week of 9/2/2024

Daily Vehicle Counts

Birchwood Loop SE on overpass 9-4 to 9-10 time correct

Daily Volumes



For Project: Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:43:14 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/4/2024 7:00:00 PM through 9/10/2024 10:59:59 AM

85th Percentile Speed 25 MPH

85th Percentile Vehicles 8

Max Speed 59 MPH on 9/6/2024 7:56:05 PM

Total Vehicles 10

AADT: 1

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	2	2
AM Peak 9:00 AM	2	2
PM Peak 7:00 PM	1	1

Speed

Speed Limit: 65

85th Percentile Speed: 25

50th Percentile Speed: 21

10 MPH Pace Interval: 15.0 MPH to 25.0 MPH

Average Speed: 24.5

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	0	0	0	N/A	N/A
% over limit	0.0	0.0	0.0	0.0	0.0	N/A	N/A
Avg Speeder	0.0	0.0	0.0	0.0	0.0	N/A	N/A
Avg Speed	22.0	21.0	15.5	22.0	31.8	N/A	N/A

Class Counts

	Number	%
VEH_SM	3	30
VEH_MED	7	70
VEH_LG	0	0
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Incoming Summary
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/4/2024 8:00:00 PM	17.0	2	2	17	0.0	0.0%	15.5
9/4/2024 9:00:00 PM	**No Data**						
9/4/2024 10:00:00 PM	**No Data**						
						
9/5/2024 11:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
9/5/2024 12:00:00 PM	19.0	1	1	19	0.0	0.0%	19.0
9/5/2024 1:00:00 PM	**No Data**						
9/5/2024 2:00:00 PM	**No Data**						
9/6/2024 9:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
9/6/2024 10:00:00 AM	23.0	2	2	23	0.0	0.0%	21.5
9/6/2024 11:00:00 AM	**No Data**						
9/6/2024 12:00:00 PM	**No Data**						
9/6/2024 8:00:00 PM	59.0	1	1	59	0.0	0.0%	59.0
9/6/2024 9:00:00 PM	**No Data**						
9/6/2024 10:00:00 PM	**No Data**						
9/9/2024 11:00:00 AM	22.0	1	1	22	0.0	0.0%	22.0
9/9/2024 12:00:00 PM	**No Data**						
9/9/2024 1:00:00 PM	**No Data**						
9/10/2024 11:00:00 AM	21.0	1	1	21	0.0	0.0%	21.0

Incoming Summary
Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/5/2024 12:00:00 AM	17.0	2	2	17	0.0	0.0%	15.5
9/6/2024 12:00:00 AM	25.0	2	2	25	0.0	0.0%	22.0
9/7/2024 12:00:00 AM	25.0	3	4	59	0.0	0.0%	31.8
9/8/2024 12:00:00 AM	**No Data**						
9/9/2024 12:00:00 AM	**No Data**						
9/10/2024 12:00:00 AM	22.0	1	1	22	0.0	0.0%	22.0
9/10/2024 10:59:59 AM	21.0	1	1	21	0.0	0.0%	21.0

Incoming Weekly Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	0	0	0	0	0	0	0
1 - 2	*	*	*	0	0	0	0	0	0	0
2 - 3	*	*	*	0	0	0	0	0	0	0
3 - 4	*	*	*	0	0	0	0	0	0	0
4 - 5	*	*	*	0	0	0	0	0	0	0
5 - 6	*	*	*	0	0	0	0	0	0	0
6 - 7	*	*	*	0	0	0	0	0	0	0
7 - 8	*	*	*	0	0	0	0	0	0	0
8 - 9	*	*	*	0	1	0	0	0.5	0	25
9 - 10	*	*	*	0	2	0	0	1	0	23
10 - 11	*	*	*	1	0	0	0	0.5	0	25
11 - 12	*	*	*	1	0	0	0	0.5	0	19
12 - 13	*	*	*	0	0	0	0	0	0	0
13 - 14	*	*	*	0	0	0	0	0	0	0
14 - 15	*	*	*	0	0	0	0	0	0	0
15 - 16	*	*	*	0	0	0	0	0	0	0
16 - 17	*	*	*	0	0	0	0	0	0	0
17 - 18	*	*	*	0	0	0	0	0	0	0
18 - 19	*	*	*	0	0	0	0	0	0	0
19 - 20	*	*	2	0	1	0	0	1	0	38
20 - 21	*	*	0	0	0	0	0	0	0	0
21 - 22	*	*	0	0	0	0	0	0	0	0
22 - 23	*	*	0	0	0	0	0	0	0	0
23 - 24	*	*	0	0	0	0	0	0	0	0
Totals	0	0	2	2	4	0	0			
% of Total	0%	0%	25%	25%	50%	0%	0%			

Incoming Weekly Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	0	0	*	*	*	*	*	0	0	0
1 - 2	0	0	*	*	*	*	*	0	0	0
2 - 3	0	0	*	*	*	*	*	0	0	0
3 - 4	0	0	*	*	*	*	*	0	0	0
4 - 5	0	0	*	*	*	*	*	0	0	0
5 - 6	0	0	*	*	*	*	*	0	0	0
6 - 7	0	0	*	*	*	*	*	0	0	0
7 - 8	0	0	*	*	*	*	*	0	0	0
8 - 9	0	0	*	*	*	*	*	0	0	0
9 - 10	0	0	*	*	*	*	*	0	0	0
10 - 11	1	1	*	*	*	*	*	1	0	21.5
11 - 12	0	*	*	*	*	*	*	0	0	0
12 - 13	0	*	*	*	*	*	*	0	0	0
13 - 14	0	*	*	*	*	*	*	0	0	0
14 - 15	0	*	*	*	*	*	*	0	0	0
15 - 16	0	*	*	*	*	*	*	0	0	0
16 - 17	0	*	*	*	*	*	*	0	0	0
17 - 18	0	*	*	*	*	*	*	0	0	0
18 - 19	0	*	*	*	*	*	*	0	0	0
19 - 20	0	*	*	*	*	*	*	0	0	0
20 - 21	0	*	*	*	*	*	*	0	0	0
21 - 22	0	*	*	*	*	*	*	0	0	0
22 - 23	0	*	*	*	*	*	*	0	0	0
23 - 24	0	*	*	*	*	*	*	0	0	0
Totals	1	1	0	0	0	0	0			
% of Total	50%	50%	0%	0%	0%	0%	0%			

Incoming Monthly Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	0	0	*	0	0	0	0	0	0	0
1 - 2	0	0	*	0	0	0	0	0	0	0
2 - 3	0	0	*	0	0	0	0	0	0	0
3 - 4	0	0	*	0	0	0	0	0	0	0
4 - 5	0	0	*	0	0	0	0	0	0	0
5 - 6	0	0	*	0	0	0	0	0	0	0
6 - 7	0	0	*	0	0	0	0	0	0	0
7 - 8	0	0	*	0	0	0	0	0	0	0
8 - 9	0	0	*	0	1	0	0	0.25	0	25
9 - 10	0	0	*	0	2	0	0	0.5	0	23
10 - 11	1	1	*	1	0	0	0	0.75	0	22.67
11 - 12	0	*	*	1	0	0	0	0.33	0	19
12 - 13	0	*	*	0	0	0	0	0	0	0
13 - 14	0	*	*	0	0	0	0	0	0	0
14 - 15	0	*	*	0	0	0	0	0	0	0
15 - 16	0	*	*	0	0	0	0	0	0	0
16 - 17	0	*	*	0	0	0	0	0	0	0
17 - 18	0	*	*	0	0	0	0	0	0	0
18 - 19	0	*	*	0	0	0	0	0	0	0
19 - 20	0	*	2	0	1	0	0	0.75	0	38
20 - 21	0	*	0	0	0	0	0	0	0	0
21 - 22	0	*	0	0	0	0	0	0	0	0
22 - 23	0	*	0	0	0	0	0	0	0	0
23 - 24	0	*	0	0	0	0	0	0	0	0
Totals	1	1	2	2	4	0	0			
% of Total	10%	10%	20%	20%	40%	0%	0%			

Incoming Weekly Speeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	0	0	0	0	0	0	0
1 - 2	*	*	*	0	0	0	0	0	0	0
2 - 3	*	*	*	0	0	0	0	0	0	0
3 - 4	*	*	*	0	0	0	0	0	0	0
4 - 5	*	*	*	0	0	0	0	0	0	0
5 - 6	*	*	*	0	0	0	0	0	0	0
6 - 7	*	*	*	0	0	0	0	0	0	0
7 - 8	*	*	*	0	0	0	0	0	0	0
8 - 9	*	*	*	0	25	0	0	25	0	25
9 - 10	*	*	*	0	21.5	0	0	21.5	0	23
10 - 11	*	*	*	25	0	0	0	25	0	25
11 - 12	*	*	*	19	0	0	0	19	0	19
12 - 13	*	*	*	0	0	0	0	0	0	0
13 - 14	*	*	*	0	0	0	0	0	0	0
14 - 15	*	*	*	0	0	0	0	0	0	0
15 - 16	*	*	*	0	0	0	0	0	0	0
16 - 17	*	*	*	0	0	0	0	0	0	0
17 - 18	*	*	*	0	0	0	0	0	0	0
18 - 19	*	*	*	0	0	0	0	0	0	0
19 - 20	*	*	15.5	0	59	0	0	30	0	38
20 - 21	*	*	0	0	0	0	0	0	0	0
21 - 22	*	*	0	0	0	0	0	0	0	0
22 - 23	*	*	0	0	0	0	0	0	0	0
23 - 24	*	*	0	0	0	0	0	0	0	0
Totals	0	0	15.5	22	31.8	0	0			
% of Total	0%	0%	22.37%	31.75%	45.89%	0%	0%			

Incoming Weekly Speeds
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	0	0	*	*	*	*	*	*	*	0
1 - 2	0	0	*	*	*	*	*	*	*	0
2 - 3	0	0	*	*	*	*	*	*	*	0
3 - 4	0	0	*	*	*	*	*	*	*	0
4 - 5	0	0	*	*	*	*	*	*	*	0
5 - 6	0	0	*	*	*	*	*	*	*	0
6 - 7	0	0	*	*	*	*	*	*	*	0
7 - 8	0	0	*	*	*	*	*	*	*	0
8 - 9	0	0	*	*	*	*	*	*	*	0
9 - 10	0	0	*	*	*	*	*	*	*	0
10 - 11	22	21	*	*	*	*	*	21.5	*	21.5
11 - 12	0	*	*	*	*	*	*	*	*	0
12 - 13	0	*	*	*	*	*	*	*	*	0
13 - 14	0	*	*	*	*	*	*	*	*	0
14 - 15	0	*	*	*	*	*	*	*	*	0
15 - 16	0	*	*	*	*	*	*	*	*	0
16 - 17	0	*	*	*	*	*	*	*	*	0
17 - 18	0	*	*	*	*	*	*	*	*	0
18 - 19	0	*	*	*	*	*	*	*	*	0
19 - 20	0	*	*	*	*	*	*	*	*	0
20 - 21	0	*	*	*	*	*	*	*	*	0
21 - 22	0	*	*	*	*	*	*	*	*	0
22 - 23	0	*	*	*	*	*	*	*	*	0
23 - 24	0	*	*	*	*	*	*	*	*	0
Totals	22	21	0	0	0	0	0			
% of Total	51.16%	48.84%	0%	0%	0%	0%	0%			

Incoming Monthly Speeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	0	0	*	0	0	0	0	0	0	0
1 - 2	0	0	*	0	0	0	0	0	0	0
2 - 3	0	0	*	0	0	0	0	0	0	0
3 - 4	0	0	*	0	0	0	0	0	0	0
4 - 5	0	0	*	0	0	0	0	0	0	0
5 - 6	0	0	*	0	0	0	0	0	0	0
6 - 7	0	0	*	0	0	0	0	0	0	0
7 - 8	0	0	*	0	0	0	0	0	0	0
8 - 9	0	0	*	0	25	0	0	25	0	25
9 - 10	0	0	*	0	21.5	0	0	21.5	0	23
10 - 11	22	21	*	25	0	0	0	22.67	0	22.67
11 - 12	0	*	*	19	0	0	0	19	0	19
12 - 13	0	*	*	0	0	0	0	0	0	0
13 - 14	0	*	*	0	0	0	0	0	0	0
14 - 15	0	*	*	0	0	0	0	0	0	0
15 - 16	0	*	*	0	0	0	0	0	0	0
16 - 17	0	*	*	0	0	0	0	0	0	0
17 - 18	0	*	*	0	0	0	0	0	0	0
18 - 19	0	*	*	0	0	0	0	0	0	0
19 - 20	0	*	15.5	0	59	0	0	30	0	38
20 - 21	0	*	0	0	0	0	0	0	0	0
21 - 22	0	*	0	0	0	0	0	0	0	0
22 - 23	0	*	0	0	0	0	0	0	0	0
23 - 24	0	*	0	0	0	0	0	0	0	0
Totals	22	21	15.5	22	31.8	0	0			
% of Total	19.59%	18.7%	13.8%	19.59%	28.32%	0%	0%			

Incoming Weekly EightyFifthSpeeds
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	0	0	0	0	0	0	0
1 - 2	*	*	*	0	0	0	0	0	0	0
2 - 3	*	*	*	0	0	0	0	0	0	0
3 - 4	*	*	*	0	0	0	0	0	0	0
4 - 5	*	*	*	0	0	0	0	0	0	0
5 - 6	*	*	*	0	0	0	0	0	0	0
6 - 7	*	*	*	0	0	0	0	0	0	0
7 - 8	*	*	*	0	0	0	0	0	0	0
8 - 9	*	*	*	0	25	0	0	25	0	25
9 - 10	*	*	*	0	23	0	0	23	0	23
10 - 11	*	*	*	25	0	0	0	25	0	25
11 - 12	*	*	*	19	0	0	0	19	0	19
12 - 13	*	*	*	0	0	0	0	0	0	0
13 - 14	*	*	*	0	0	0	0	0	0	0
14 - 15	*	*	*	0	0	0	0	0	0	0
15 - 16	*	*	*	0	0	0	0	0	0	0
16 - 17	*	*	*	0	0	0	0	0	0	0
17 - 18	*	*	*	0	0	0	0	0	0	0
18 - 19	*	*	*	0	0	0	0	0	0	0
19 - 20	*	*	17	0	59	0	0	38	0	38
20 - 21	*	*	0	0	0	0	0	0	0	0
21 - 22	*	*	0	0	0	0	0	0	0	0
22 - 23	*	*	0	0	0	0	0	0	0	0
23 - 24	*	*	0	0	0	0	0	0	0	0
Totals	0	0	17	44	107	0	0			
% of Total	0%	0%	10.12%	26.19%	63.69%	0%	0%			

Incoming Weekly EightyFifthSpeeds
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	0	0	*	*	*	*	*	0	0	0
1 - 2	0	0	*	*	*	*	*	0	0	0
2 - 3	0	0	*	*	*	*	*	0	0	0
3 - 4	0	0	*	*	*	*	*	0	0	0
4 - 5	0	0	*	*	*	*	*	0	0	0
5 - 6	0	0	*	*	*	*	*	0	0	0
6 - 7	0	0	*	*	*	*	*	0	0	0
7 - 8	0	0	*	*	*	*	*	0	0	0
8 - 9	0	0	*	*	*	*	*	0	0	0
9 - 10	0	0	*	*	*	*	*	0	0	0
10 - 11	22	21	*	*	*	*	*	21.5	0	21.5
11 - 12	0	*	*	*	*	*	*	0	0	0
12 - 13	0	*	*	*	*	*	*	0	0	0
13 - 14	0	*	*	*	*	*	*	0	0	0
14 - 15	0	*	*	*	*	*	*	0	0	0
15 - 16	0	*	*	*	*	*	*	0	0	0
16 - 17	0	*	*	*	*	*	*	0	0	0
17 - 18	0	*	*	*	*	*	*	0	0	0
18 - 19	0	*	*	*	*	*	*	0	0	0
19 - 20	0	*	*	*	*	*	*	0	0	0
20 - 21	0	*	*	*	*	*	*	0	0	0
21 - 22	0	*	*	*	*	*	*	0	0	0
22 - 23	0	*	*	*	*	*	*	0	0	0
23 - 24	0	*	*	*	*	*	*	0	0	0
Totals	22	21	0	0	0	0	0			
% of Total	51.16%	48.84%	0%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

	Sep 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	0	0	*	0	0	0	0	0	0	0
1 - 2	0	0	*	0	0	0	0	0	0	0
2 - 3	0	0	*	0	0	0	0	0	0	0
3 - 4	0	0	*	0	0	0	0	0	0	0
4 - 5	0	0	*	0	0	0	0	0	0	0
5 - 6	0	0	*	0	0	0	0	0	0	0
6 - 7	0	0	*	0	0	0	0	0	0	0
7 - 8	0	0	*	0	0	0	0	0	0	0
8 - 9	0	0	*	0	25	0	0	25	0	25
9 - 10	0	0	*	0	23	0	0	23	0	23
10 - 11	22	21	*	25	0	0	0	22.67	0	22.67
11 - 12	0	*	*	19	0	0	0	19	0	19
12 - 13	0	*	*	0	0	0	0	0	0	0
13 - 14	0	*	*	0	0	0	0	0	0	0
14 - 15	0	*	*	0	0	0	0	0	0	0
15 - 16	0	*	*	0	0	0	0	0	0	0
16 - 17	0	*	*	0	0	0	0	0	0	0
17 - 18	0	*	*	0	0	0	0	0	0	0
18 - 19	0	*	*	0	0	0	0	0	0	0
19 - 20	0	*	17	0	59	0	0	38	0	38
20 - 21	0	*	0	0	0	0	0	0	0	0
21 - 22	0	*	0	0	0	0	0	0	0	0
22 - 23	0	*	0	0	0	0	0	0	0	0
23 - 24	0	*	0	0	0	0	0	0	0	0

Summary of Violators

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	0	N/A	N/A	N/A
01:00:00	0	N/A	N/A	N/A
02:00:00	0	N/A	N/A	N/A
03:00:00	0	N/A	N/A	N/A
04:00:00	0	N/A	N/A	N/A
05:00:00	0	N/A	N/A	N/A
06:00:00	0	N/A	N/A	N/A
07:00:00	0	N/A	N/A	N/A
08:00:00	1	25.0	0	0.0
09:00:00	2	21.5	0	0.0
10:00:00	3	22.7	0	0.0
11:00:00	1	19.0	0	0.0
12:00:00	0	N/A	N/A	N/A
13:00:00	0	N/A	N/A	N/A
14:00:00	0	N/A	N/A	N/A
15:00:00	0	N/A	N/A	N/A
16:00:00	0	N/A	N/A	N/A
17:00:00	0	N/A	N/A	N/A
18:00:00	0	N/A	N/A	N/A
19:00:00	3	30.0	0	0.0
20:00:00	0	N/A	N/A	N/A
21:00:00	0	N/A	N/A	N/A
22:00:00	0	N/A	N/A	N/A
23:00:00	0	N/A	N/A	N/A

Incoming Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	19:00	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	15.5	17	7 to 17	100.0	*	*	1	1	0
9/4/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	15.5	17	7 to 17	100.0	*	*	1	1	0

Incoming Histogram
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/5/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	10:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0
9/5/2024	11:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	19	19	9 to 19	100.0	*	*	1	0	0
9/5/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/5/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	22	25	15 to 25	100.0	*	*	1	1	0

Incoming Histogram
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/6/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/6/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/6/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/6/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/6/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/6/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/6/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/6/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/6/2024	08:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0
9/6/2024	09:00	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	21.5	23	13 to 23	100.0	*	*	1	1	0
9/6/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	19:00	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	59	59	49 to 59	100.0	*	*	0	1	0
9/6/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	2	1	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	4	31.8	25	15 to 25	75.0	*	*	1	3	0

Incoming Histogram
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/7/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/7/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	*	*	*	*	0	0	0

Incoming Histogram
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/8/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/8/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	*	*	*	*	0	0	0

Incoming Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/9/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	10:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	22	22	12 to 22	100.0	*	*	0	1	0
9/9/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/9/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	22	22	12 to 22	100.0	*	*	0	1	0

Incoming Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-10-59-AM

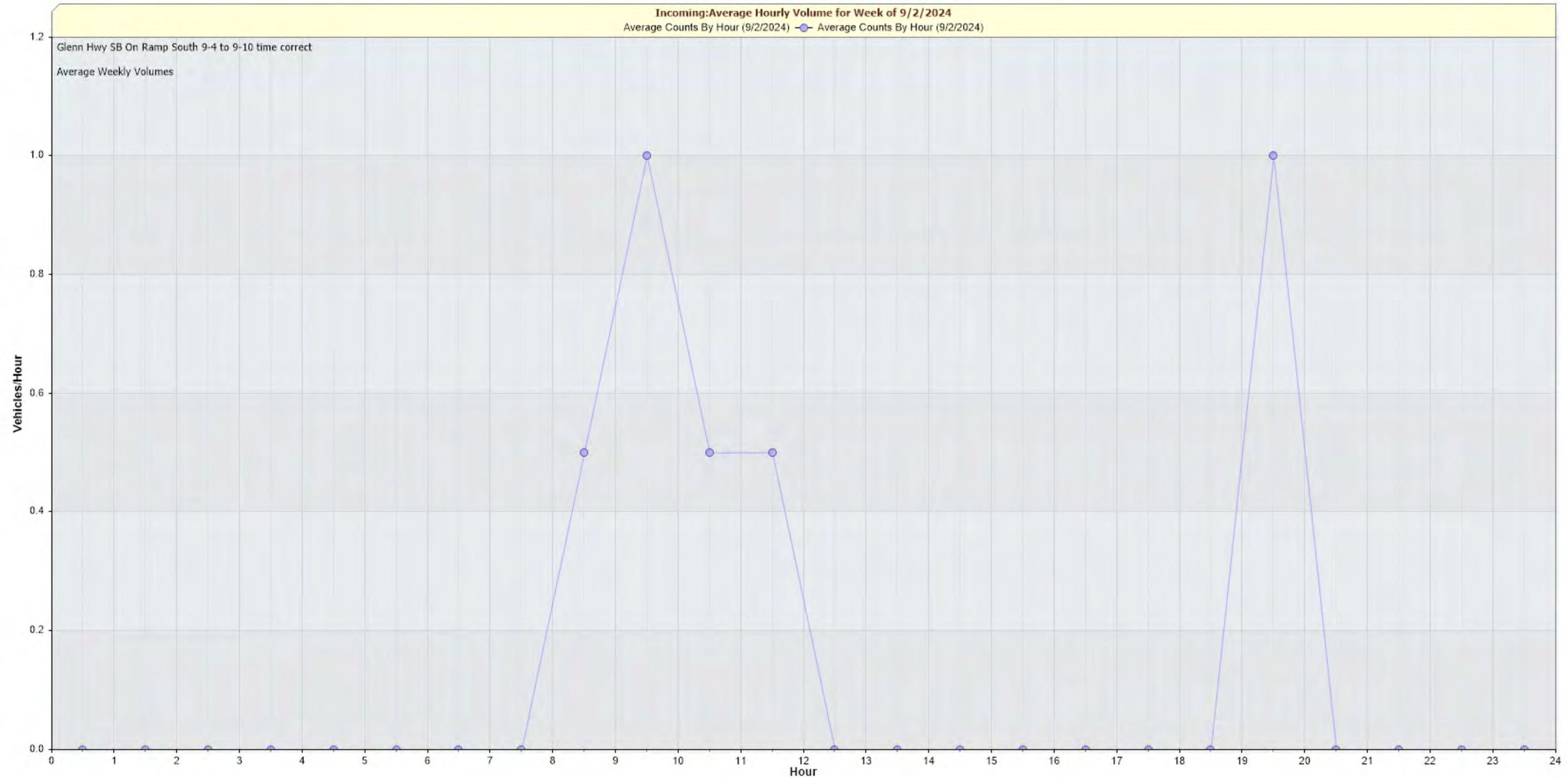
Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	10:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	21	21	11 to 21	100.0	*	*	0	1	0
9/10/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	21	21	11 to 21	100.0	*	*	0	1	0

Incoming:Average Hourly Volume for Week of 9/2/2024

Average Counts By Hour (9/2/2024) ● Average Counts By Hour (9/2/2024)

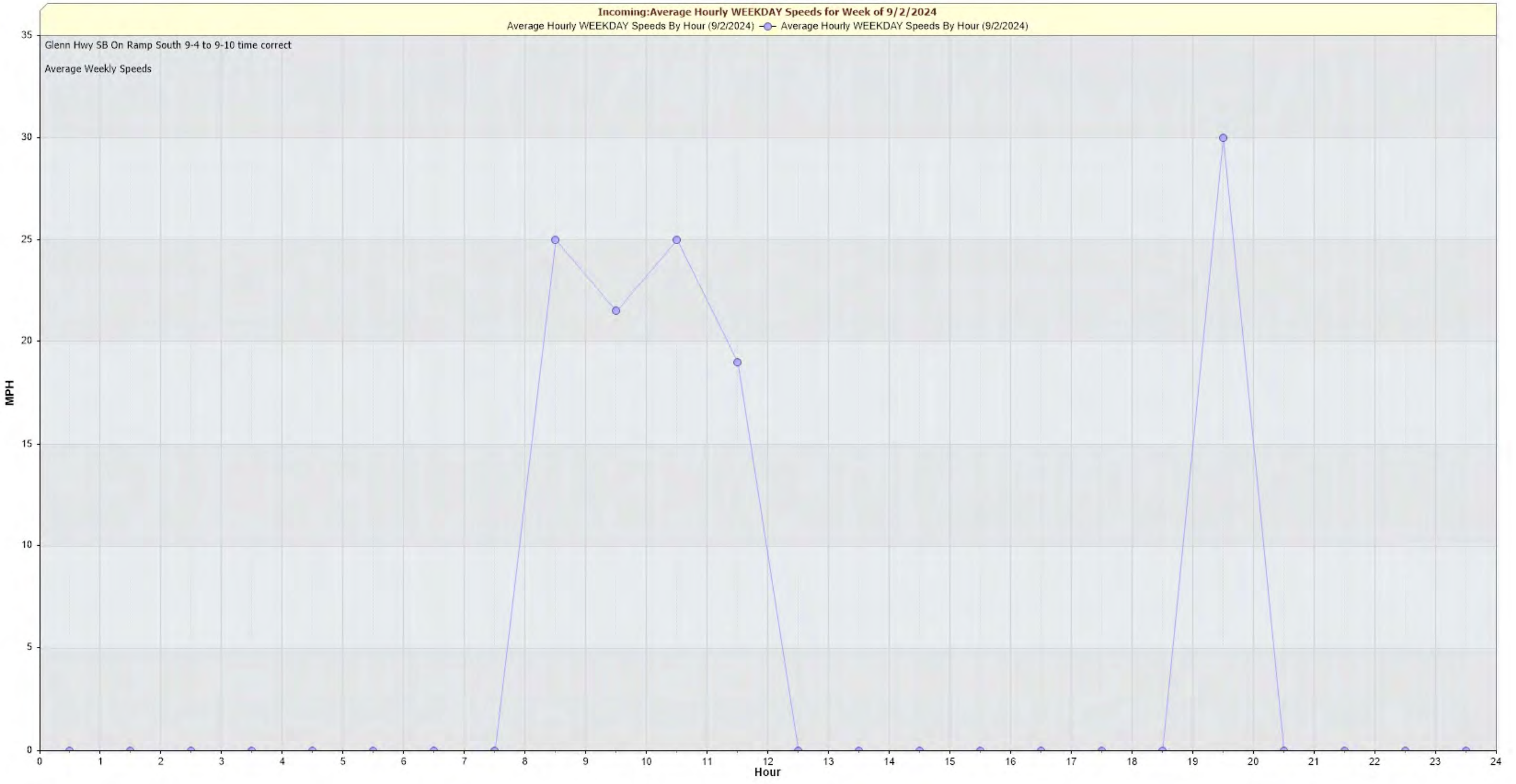
Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

Average Weekly Volumes



Incoming: Average Hourly WEEKDAY Speeds for Week of 9/2/2024
Average Hourly WEEKDAY Speeds By Hour (9/2/2024) — Average Hourly WEEKDAY Speeds By Hour (9/2/2024)

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct
Average Weekly Speeds

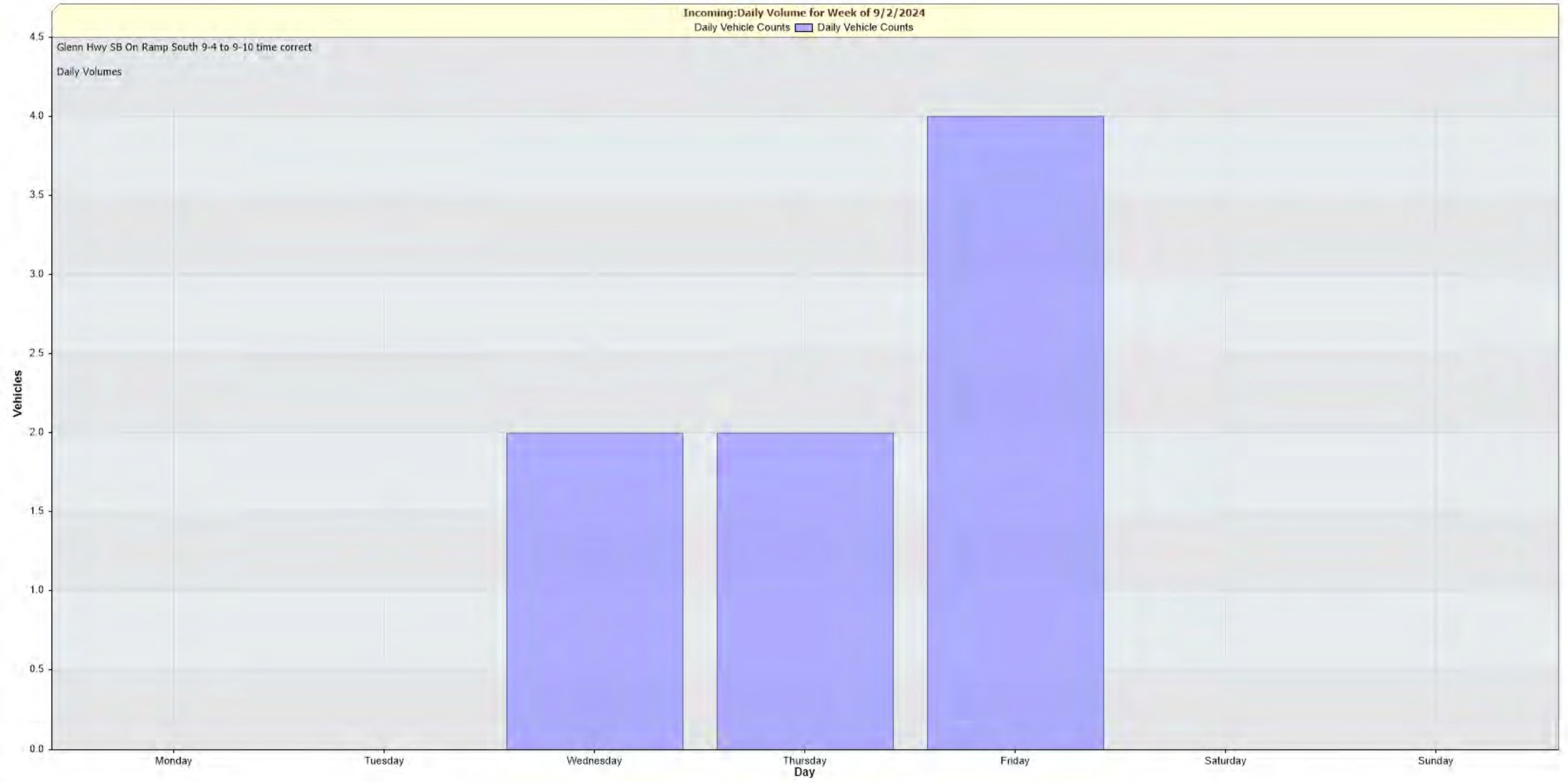


Incoming:Daily Volume for Week of 9/2/2024

Daily Vehicle Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

Daily Volumes



For Project: Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:43:14 PM

Speed Intervals: 1 MPH

Time Intervals: Instant

Traffic Report From: 9/4/2024 7:00:00 PM through 9/10/2024 1:59:59 PM

85th Percentile Speed: 37 MPH

85th Percentile Vehicles: 7862

Max Speed: 50 MPH on 9/6/2024 4:04:36 PM

Total Vehicles: 9249

AADT: 1596

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1322	1321
AM Peak 7:00 AM	215	152
PM Peak 2:00 PM	149	130

Speed

Speed Limit: 65

85th Percentile Speed: 37

50th Percentile Speed: 33

10 MPH Pace Interval: 28.0 MPH to 38.0 MPH

Average Speed: 33.01

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	0	0	0	0	0
% over limit	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speeder	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speed	32.7	33.0	32.8	33.2	33.1	33.2	32.9

Class Counts

	Number	%
VEH_SM	51	0.6
VEH_MED	9198	99.4
VEH_LG	0	0
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Outgoing Summary
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/4/2024 8:00:00 PM	37.0	60	70	48	0.0	0.0%	33.4
9/4/2024 9:00:00 PM	36.0	62	73	41	0.0	0.0%	32.9
9/4/2024 10:00:00 PM	36.0	27	32	41	0.0	0.0%	31.9
9/4/2024 11:00:00 PM	36.0	11	13	38	0.0	0.0%	32.3
9/5/2024 12:00:00 AM	36.0	10	12	38	0.0	0.0%	32.2
9/5/2024 1:00:00 AM	36.0	3	4	37	0.0	0.0%	35.3
9/5/2024 2:00:00 AM	34.0	3	4	35	0.0	0.0%	31.5
9/5/2024 3:00:00 AM	**No Data**						
9/5/2024 4:00:00 AM	38.0	5	6	46	0.0	0.0%	34.0
9/5/2024 5:00:00 AM	37.0	11	13	43	0.0	0.0%	32.9
9/5/2024 6:00:00 AM	38.0	51	60	49	0.0	0.0%	34.1
9/5/2024 7:00:00 AM	38.0	86	101	46	0.0	0.0%	34.1
9/5/2024 8:00:00 AM	37.0	199	234	45	0.0	0.0%	33.6
9/5/2024 9:00:00 AM	36.0	144	170	44	0.0	0.0%	33.3
9/5/2024 10:00:00 AM	37.0	90	106	43	0.0	0.0%	32.8
9/5/2024 11:00:00 AM	37.0	68	80	43	0.0	0.0%	32.8
9/5/2024 12:00:00 PM	37.0	89	105	42	0.0	0.0%	33.0
9/5/2024 1:00:00 PM	36.0	68	80	44	0.0	0.0%	33.0
9/5/2024 2:00:00 PM	37.0	70	82	43	0.0	0.0%	32.9
9/5/2024 3:00:00 PM	37.0	124	146	44	0.0	0.0%	32.8
9/5/2024 4:00:00 PM	37.0	103	121	44	0.0	0.0%	33.1
9/5/2024 5:00:00 PM	37.0	97	114	44	0.0	0.0%	33.3
9/5/2024 6:00:00 PM	37.0	88	104	43	0.0	0.0%	33.6
9/5/2024 7:00:00 PM	37.0	56	66	42	0.0	0.0%	32.3
9/5/2024 8:00:00 PM	37.0	61	72	45	0.0	0.0%	33.1
9/5/2024 9:00:00 PM	37.0	48	56	42	0.0	0.0%	33.4
9/5/2024 10:00:00 PM	36.0	36	42	39	0.0	0.0%	31.8
9/5/2024 11:00:00 PM	33.0	15	18	38	0.0	0.0%	31.2
9/6/2024 12:00:00 AM	36.0	3	4	37	0.0	0.0%	34.0
9/6/2024 1:00:00 AM	36.0	8	9	39	0.0	0.0%	32.0
9/6/2024 2:00:00 AM	30.0	4	5	35	0.0	0.0%	29.2
9/6/2024 3:00:00 AM	37.0	2	2	37	0.0	0.0%	36.0
9/6/2024 4:00:00 AM	35.0	5	6	36	0.0	0.0%	32.7
9/6/2024 5:00:00 AM	37.0	11	13	41	0.0	0.0%	33.2
9/6/2024 6:00:00 AM	37.0	31	37	43	0.0	0.0%	33.1
9/6/2024 7:00:00 AM	37.0	89	105	44	0.0	0.0%	33.3
9/6/2024 8:00:00 AM	37.0	190	224	42	0.0	0.0%	33.4
9/6/2024 9:00:00 AM	36.0	126	148	44	0.0	0.0%	32.3
9/6/2024 10:00:00 AM	36.0	82	96	44	0.0	0.0%	32.2
9/6/2024 11:00:00 AM	37.0	69	81	44	0.0	0.0%	32.8
9/6/2024 12:00:00 PM	37.0	83	98	45	0.0	0.0%	33.1
9/6/2024 1:00:00 PM	37.0	86	101	46	0.0	0.0%	32.6
9/6/2024 2:00:00 PM	37.0	110	130	46	0.0	0.0%	33.0
9/6/2024 3:00:00 PM	38.0	134	158	45	0.0	0.0%	33.8
9/6/2024 4:00:00 PM	37.0	109	128	44	0.0	0.0%	33.1
9/6/2024 5:00:00 PM	37.0	94	110	50	0.0	0.0%	33.6
9/6/2024 6:00:00 PM	37.0	109	128	48	0.0	0.0%	33.7

Outgoing Summary
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

9/6/2024 7:00:00 PM	37.0	85	100	49	0.0	0.0%	32.9
9/6/2024 8:00:00 PM	37.0	63	74	47	0.0	0.0%	33.7
9/6/2024 9:00:00 PM	37.0	52	61	43	0.0	0.0%	33.8
9/6/2024 10:00:00 PM	36.0	38	45	39	0.0	0.0%	32.2
9/6/2024 11:00:00 PM	34.0	27	32	41	0.0	0.0%	30.0
9/7/2024 12:00:00 AM	37.0	11	13	39	0.0	0.0%	30.5
9/7/2024 1:00:00 AM	34.0	7	8	47	0.0	0.0%	31.5
9/7/2024 2:00:00 AM	33.0	8	9	36	0.0	0.0%	27.3
9/7/2024 3:00:00 AM	33.0	5	6	33	0.0	0.0%	29.2
9/7/2024 4:00:00 AM	32.0	3	4	34	0.0	0.0%	28.5
9/7/2024 5:00:00 AM	32.0	4	5	37	0.0	0.0%	32.0
9/7/2024 6:00:00 AM	36.0	13	15	38	0.0	0.0%	31.1
9/7/2024 7:00:00 AM	34.0	21	25	37	0.0	0.0%	30.7
9/7/2024 8:00:00 AM	37.0	29	34	43	0.0	0.0%	33.3
9/7/2024 9:00:00 AM	38.0	47	55	44	0.0	0.0%	33.4
9/7/2024 10:00:00 AM	36.0	74	87	40	0.0	0.0%	31.9
9/7/2024 11:00:00 AM	38.0	95	112	44	0.0	0.0%	33.5
9/7/2024 12:00:00 PM	37.0	91	107	45	0.0	0.0%	33.3
9/7/2024 1:00:00 PM	37.0	98	115	42	0.0	0.0%	33.2
9/7/2024 2:00:00 PM	36.0	88	104	44	0.0	0.0%	32.4
9/7/2024 3:00:00 PM	37.0	97	114	44	0.0	0.0%	33.5
9/7/2024 4:00:00 PM	37.0	84	99	44	0.0	0.0%	34.0
9/7/2024 5:00:00 PM	37.0	82	96	45	0.0	0.0%	33.5
9/7/2024 6:00:00 PM	37.0	87	102	44	0.0	0.0%	34.1
9/7/2024 7:00:00 PM	39.0	65	77	46	0.0	0.0%	34.2
9/7/2024 8:00:00 PM	37.0	60	71	43	0.0	0.0%	33.6
9/7/2024 9:00:00 PM	37.0	51	60	45	0.0	0.0%	33.3
9/7/2024 10:00:00 PM	38.0	35	41	47	0.0	0.0%	34.1
9/7/2024 11:00:00 PM	36.0	20	24	42	0.0	0.0%	32.2
9/8/2024 12:00:00 AM	33.0	8	9	33	0.0	0.0%	29.6
9/8/2024 1:00:00 AM	36.0	10	12	41	0.0	0.0%	32.7
9/8/2024 2:00:00 AM	34.0	4	5	36	0.0	0.0%	33.4
9/8/2024 3:00:00 AM	36.0	2	2	36	0.0	0.0%	34.5
9/8/2024 4:00:00 AM	35.0	4	5	36	0.0	0.0%	31.8
9/8/2024 5:00:00 AM	36.0	2	2	36	0.0	0.0%	34.0
9/8/2024 6:00:00 AM	33.0	7	8	36	0.0	0.0%	30.4
9/8/2024 7:00:00 AM	36.0	14	16	37	0.0	0.0%	31.8
9/8/2024 8:00:00 AM	39.0	20	23	43	0.0	0.0%	34.4
9/8/2024 9:00:00 AM	37.0	57	67	42	0.0	0.0%	32.9
9/8/2024 10:00:00 AM	36.0	54	64	42	0.0	0.0%	32.9
9/8/2024 11:00:00 AM	37.0	81	95	49	0.0	0.0%	33.0
9/8/2024 12:00:00 PM	36.0	75	88	40	0.0	0.0%	32.7
9/8/2024 1:00:00 PM	36.0	144	169	42	0.0	0.0%	32.7
9/8/2024 2:00:00 PM	36.0	78	92	46	0.0	0.0%	32.5
9/8/2024 3:00:00 PM	37.0	80	94	44	0.0	0.0%	33.0
9/8/2024 4:00:00 PM	36.0	74	87	44	0.0	0.0%	32.6
9/8/2024 5:00:00 PM	37.0	86	101	42	0.0	0.0%	33.5
9/8/2024 6:00:00 PM	37.0	83	98	42	0.0	0.0%	33.2

Outgoing Summary
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

9/8/2024 7:00:00 PM	37.0	65	76	46	0.0	0.0%	33.2
9/8/2024 8:00:00 PM	36.0	38	45	43	0.0	0.0%	33.3
9/8/2024 9:00:00 PM	37.0	48	56	41	0.0	0.0%	33.9
9/8/2024 10:00:00 PM	35.0	24	28	49	0.0	0.0%	31.8
9/8/2024 11:00:00 PM	36.0	14	16	37	0.0	0.0%	32.4
9/9/2024 12:00:00 AM	36.0	7	8	38	0.0	0.0%	31.8
9/9/2024 1:00:00 AM	39.0	5	6	42	0.0	0.0%	36.0
9/9/2024 2:00:00 AM	34.0	4	5	36	0.0	0.0%	31.6
9/9/2024 3:00:00 AM	34.0	8	10	45	0.0	0.0%	32.2
9/9/2024 4:00:00 AM	34.0	7	8	35	0.0	0.0%	32.5
9/9/2024 5:00:00 AM	37.0	8	10	39	0.0	0.0%	32.8
9/9/2024 6:00:00 AM	38.0	37	43	43	0.0	0.0%	33.1
9/9/2024 7:00:00 AM	38.0	86	101	45	0.0	0.0%	33.3
9/9/2024 8:00:00 AM	37.0	138	162	46	0.0	0.0%	33.3
9/9/2024 9:00:00 AM	36.0	131	154	43	0.0	0.0%	32.2
9/9/2024 10:00:00 AM	36.0	110	130	39	0.0	0.0%	32.4
9/9/2024 11:00:00 AM	35.0	79	93	42	0.0	0.0%	31.5
9/9/2024 12:00:00 PM	36.0	75	88	40	0.0	0.0%	31.8
9/9/2024 1:00:00 PM	36.0	72	85	48	0.0	0.0%	32.5
9/9/2024 2:00:00 PM	36.0	85	100	45	0.0	0.0%	32.6
9/9/2024 3:00:00 PM	36.0	122	144	45	0.0	0.0%	32.7
9/9/2024 4:00:00 PM	36.0	104	122	43	0.0	0.0%	32.9
9/9/2024 5:00:00 PM	36.0	67	79	42	0.0	0.0%	32.2
9/9/2024 6:00:00 PM	37.0	72	85	43	0.0	0.0%	33.7
9/9/2024 7:00:00 PM	37.0	60	70	42	0.0	0.0%	33.7
9/9/2024 8:00:00 PM	36.0	41	48	44	0.0	0.0%	32.8
9/9/2024 9:00:00 PM	36.0	42	49	41	0.0	0.0%	32.8
9/9/2024 10:00:00 PM	36.0	20	24	40	0.0	0.0%	33.0
9/9/2024 11:00:00 PM	34.0	10	12	36	0.0	0.0%	31.7
9/10/2024 12:00:00 AM	34.0	7	8	36	0.0	0.0%	31.3
9/10/2024 1:00:00 AM	29.0	2	2	29	0.0	0.0%	28.5
9/10/2024 2:00:00 AM	46.0	3	3	46	0.0	0.0%	37.7
9/10/2024 3:00:00 AM	32.0	1	1	32	0.0	0.0%	32.0
9/10/2024 4:00:00 AM	35.0	5	6	38	0.0	0.0%	33.5
9/10/2024 5:00:00 AM	35.0	8	10	44	0.0	0.0%	32.1
9/10/2024 6:00:00 AM	37.0	38	45	43	0.0	0.0%	33.4
9/10/2024 7:00:00 AM	37.0	96	113	41	0.0	0.0%	33.2
9/10/2024 8:00:00 AM	37.0	204	240	46	0.0	0.0%	33.7
9/10/2024 9:00:00 AM	37.0	163	192	46	0.0	0.0%	33.2
9/10/2024 10:00:00 AM	36.0	90	106	48	0.0	0.0%	32.9
9/10/2024 11:00:00 AM	37.0	79	93	43	0.0	0.0%	32.5
9/10/2024 12:00:00 PM	37.0	76	90	40	0.0	0.0%	32.8
9/10/2024 1:00:00 PM	37.0	76	90	43	0.0	0.0%	32.8
9/10/2024 2:00:00 PM	36.0	80	94	43	0.0	0.0%	31.9

Outgoing Summary
Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/5/2024 12:00:00 AM	36.0	170	200	48	0.0	0.0%	32.8
9/6/2024 12:00:00 AM	37.0	1520	1788	49	0.0	0.0%	33.2
9/7/2024 12:00:00 AM	37.0	1618	1904	50	0.0	0.0%	33.1
9/8/2024 12:00:00 AM	37.0	1172	1379	47	0.0	0.0%	33.2
9/9/2024 12:00:00 AM	36.0	1068	1257	49	0.0	0.0%	32.9
9/10/2024 12:00:00 AM	36.0	1391	1636	48	0.0	0.0%	32.7
9/10/2024 1:59:59 PM	37.0	922	1085	48	0.0	0.0%	33.0

Outgoing Weekly Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	4	9	8	12	6.5	10	36
1 - 2	*	*	*	4	5	9	5	4.5	7	32
2 - 3	*	*	*	0	2	6	2	1	4	37
3 - 4	*	*	*	6	6	4	5	6	4.5	36.5
4 - 5	*	*	*	13	13	5	2	13	3.5	37
5 - 6	*	*	*	60	37	15	8	48.5	11.5	37.05
6 - 7	*	*	*	101	105	25	16	103	20.5	37.15
7 - 8	*	*	*	234	224	34	23	229	28.5	36.6
8 - 9	*	*	*	170	148	55	67	159	61	35.9
9 - 10	*	*	*	106	96	87	64	101	75.5	36.05
10 - 11	*	*	*	80	81	112	95	80.5	103.5	36.55
11 - 12	*	*	*	105	98	107	88	101.5	97.5	36.5
12 - 13	*	*	*	80	101	115	169	90.5	142	36.1
13 - 14	*	*	*	82	130	104	92	106	98	36.4
14 - 15	*	*	*	146	158	114	94	152	104	36.75
15 - 16	*	*	*	121	128	99	87	124.5	93	36.4
16 - 17	*	*	*	114	110	96	101	112	98.5	36.6
17 - 18	*	*	*	104	128	102	98	116	100	36.65
18 - 19	*	*	*	66	100	77	76	83	76.5	36.45
19 - 20	*	*	70	72	74	71	45	72	58	36.73
20 - 21	*	*	73	56	61	60	56	63.33	58	36.4
21 - 22	*	*	32	42	45	41	28	39.67	34.5	35.57
22 - 23	*	*	13	18	32	24	16	21	20	34.23
23 - 24	*	*	12	4	13	9	8	9.67	8.5	36.17
Totals	0	0	200	1788	1904	1379	1257			
% of Total	0%	0%	3.06%	27.39%	29.17%	21.12%	19.26%			

Outgoing Weekly Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	6	2	*	*	*	*	*	4	0	34
1 - 2	5	3	*	*	*	*	*	4	0	40
2 - 3	10	1	*	*	*	*	*	5.5	0	33
3 - 4	8	6	*	*	*	*	*	7	0	34.5
4 - 5	10	10	*	*	*	*	*	10	0	36
5 - 6	43	45	*	*	*	*	*	44	0	36.85
6 - 7	101	113	*	*	*	*	*	107	0	36.9
7 - 8	162	240	*	*	*	*	*	201	0	36.6
8 - 9	154	192	*	*	*	*	*	173	0	36.15
9 - 10	130	106	*	*	*	*	*	118	0	35.95
10 - 11	93	93	*	*	*	*	*	93	0	35.55
11 - 12	88	90	*	*	*	*	*	89	0	36.05
12 - 13	85	90	*	*	*	*	*	87.5	0	36.35
13 - 14	100	94	*	*	*	*	*	97	0	35.2
14 - 15	144	*	*	*	*	*	*	144	0	35.8
15 - 16	122	*	*	*	*	*	*	122	0	35.7
16 - 17	79	*	*	*	*	*	*	79	0	35.4
17 - 18	85	*	*	*	*	*	*	85	0	37
18 - 19	70	*	*	*	*	*	*	70	0	36.7
19 - 20	48	*	*	*	*	*	*	48	0	36
20 - 21	49	*	*	*	*	*	*	49	0	35.9
21 - 22	24	*	*	*	*	*	*	24	0	36
22 - 23	12	*	*	*	*	*	*	12	0	33.5
23 - 24	8	*	*	*	*	*	*	8	0	34
Totals	1636	1085	0	0	0	0	0			
% of Total	60.12%	39.88%	0%	0%	0%	0%	0%			

Outgoing Monthly Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	6	2	*	4	9	8	12	5.25	10	35
1 - 2	5	3	*	4	5	9	5	4.25	7	36
2 - 3	10	1	*	0	2	6	2	3.25	4	34.33
3 - 4	8	6	*	6	6	4	5	6.5	4.5	35.5
4 - 5	10	10	*	13	13	5	2	11.5	3.5	36.5
5 - 6	43	45	*	60	37	15	8	46.25	11.5	36.95
6 - 7	101	113	*	101	105	25	16	105	20.5	37.03
7 - 8	162	240	*	234	224	34	23	215	28.5	36.6
8 - 9	154	192	*	170	148	55	67	166	61	36.02
9 - 10	130	106	*	106	96	87	64	109.5	75.5	36
10 - 11	93	93	*	80	81	112	95	86.75	103.5	36.05
11 - 12	88	90	*	105	98	107	88	95.25	97.5	36.28
12 - 13	85	90	*	80	101	115	169	89	142	36.22
13 - 14	100	94	*	82	130	104	92	101.5	98	35.8
14 - 15	144	*	*	146	158	114	94	149.33	104	36.43
15 - 16	122	*	*	121	128	99	87	123.67	93	36.17
16 - 17	79	*	*	114	110	96	101	101	98.5	36.2
17 - 18	85	*	*	104	128	102	98	105.67	100	36.77
18 - 19	70	*	*	66	100	77	76	78.67	76.5	36.53
19 - 20	48	*	70	72	74	71	45	66	58	36.55
20 - 21	49	*	73	56	61	60	56	59.75	58	36.28
21 - 22	24	*	32	42	45	41	28	35.75	34.5	35.67
22 - 23	12	*	13	18	32	24	16	18.75	20	34.05
23 - 24	8	*	12	4	13	9	8	9.25	8.5	35.62
Totals	1636	1085	200	1788	1904	1379	1257			
% of Total	17.69%	11.73%	2.16%	19.33%	20.59%	14.91%	13.59%			

Outgoing Weekly Speeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	35.25	32	31.5	32.67	33	32.2	36
1 - 2	*	*	*	31.5	29.2	27.33	33.4	30.22	29.5	32
2 - 3	*	*	*	0	36	29.17	34.5	36	30.5	37
3 - 4	*	*	*	34	32.67	28.5	31.8	33.33	30.33	36.5
4 - 5	*	*	*	32.92	33.15	32	34	33.04	32.57	37
5 - 6	*	*	*	34.08	33.14	31.07	30.38	33.72	30.83	37.05
6 - 7	*	*	*	34.08	33.28	30.72	31.81	33.67	31.15	37.15
7 - 8	*	*	*	33.63	33.38	33.32	34.39	33.51	33.75	36.6
8 - 9	*	*	*	33.28	32.26	33.38	32.85	32.81	33.09	35.9
9 - 10	*	*	*	32.79	32.21	31.89	32.92	32.51	32.32	36.05
10 - 11	*	*	*	32.76	32.81	33.46	33	32.79	33.25	36.55
11 - 12	*	*	*	32.97	33.11	33.34	32.73	33.04	33.06	36.5
12 - 13	*	*	*	33.03	32.63	33.23	32.69	32.81	32.9	36.1
13 - 14	*	*	*	32.94	33.04	32.37	32.53	33	32.44	36.4
14 - 15	*	*	*	32.78	33.77	33.5	33.01	33.29	33.28	36.75
15 - 16	*	*	*	33.1	33.11	34.04	32.64	33.1	33.39	36.4
16 - 17	*	*	*	33.26	33.65	33.52	33.5	33.45	33.51	36.6
17 - 18	*	*	*	33.58	33.69	34.09	33.15	33.64	33.63	36.65
18 - 19	*	*	*	32.35	32.91	34.23	33.16	32.69	33.7	36.45
19 - 20	*	*	33.4	33.07	33.72	33.59	33.29	33.4	33.47	36.73
20 - 21	*	*	32.88	33.41	33.82	33.27	33.86	33.34	33.55	36.4
21 - 22	*	*	31.94	31.79	32.18	34.12	31.82	31.97	33.19	35.57
22 - 23	*	*	32.31	31.22	30.03	32.21	32.38	30.84	32.27	34.23
23 - 24	*	*	32.17	34	30.54	29.56	31.75	31.69	30.59	36.17
Totals	0	0	32.8	33.2	33.1	33.2	32.9			
% of Total	0%	0%	19.85%	20.1%	20.04%	20.1%	19.92%			

Outgoing Weekly Speeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	36	28.5	*	*	*	*	*	34.12	*	34
1 - 2	31.6	37.67	*	*	*	*	*	33.88	*	40
2 - 3	32.2	32	*	*	*	*	*	32.18	*	33
3 - 4	32.5	33.5	*	*	*	*	*	32.93	*	34.5
4 - 5	32.8	32.1	*	*	*	*	*	32.45	*	36
5 - 6	33.07	33.38	*	*	*	*	*	33.23	*	36.85
6 - 7	33.29	33.24	*	*	*	*	*	33.26	*	36.9
7 - 8	33.27	33.7	*	*	*	*	*	33.52	*	36.6
8 - 9	32.2	33.17	*	*	*	*	*	32.74	*	36.15
9 - 10	32.4	32.87	*	*	*	*	*	32.61	*	35.95
10 - 11	31.53	32.47	*	*	*	*	*	32	*	35.55
11 - 12	31.84	32.8	*	*	*	*	*	32.33	*	36.05
12 - 13	32.47	32.76	*	*	*	*	*	32.62	*	36.35
13 - 14	32.57	31.94	*	*	*	*	*	32.26	*	35.2
14 - 15	32.69	*	*	*	*	*	*	32.69	*	35.8
15 - 16	32.87	*	*	*	*	*	*	32.87	*	35.7
16 - 17	32.15	*	*	*	*	*	*	32.15	*	35.4
17 - 18	33.67	*	*	*	*	*	*	33.67	*	37
18 - 19	33.67	*	*	*	*	*	*	33.67	*	36.7
19 - 20	32.77	*	*	*	*	*	*	32.77	*	36
20 - 21	32.78	*	*	*	*	*	*	32.78	*	35.9
21 - 22	32.96	*	*	*	*	*	*	32.96	*	36
22 - 23	31.67	*	*	*	*	*	*	31.67	*	33.5
23 - 24	31.25	*	*	*	*	*	*	31.25	*	34
Totals	32.7	33	0	0	0	0	0			
% of Total	49.77%	50.23%	0%	0%	0%	0%	0%			

Outgoing Monthly Speeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	36	28.5	*	35.25	32	31.5	32.67	33.43	32.2	35
1 - 2	31.6	37.67	*	31.5	29.2	27.33	33.4	31.94	29.5	36
2 - 3	32.2	32	*	0	36	29.17	34.5	32.77	30.5	34.33
3 - 4	32.5	33.5	*	34	32.67	28.5	31.8	33.12	30.33	35.5
4 - 5	32.8	32.1	*	32.92	33.15	32	34	32.78	32.57	36.5
5 - 6	33.07	33.38	*	34.08	33.14	31.07	30.38	33.49	30.83	36.95
6 - 7	33.29	33.24	*	34.08	33.28	30.72	31.81	33.46	31.15	37.03
7 - 8	33.27	33.7	*	33.63	33.38	33.32	34.39	33.52	33.75	36.6
8 - 9	32.2	33.17	*	33.28	32.26	33.38	32.85	32.77	33.09	36.02
9 - 10	32.4	32.87	*	32.79	32.21	31.89	32.92	32.57	32.32	36
10 - 11	31.53	32.47	*	32.76	32.81	33.46	33	32.37	33.25	36.05
11 - 12	31.84	32.8	*	32.97	33.11	33.34	32.73	32.71	33.06	36.28
12 - 13	32.47	32.76	*	33.03	32.63	33.23	32.69	32.71	32.9	36.22
13 - 14	32.57	31.94	*	32.94	33.04	32.37	32.53	32.65	32.44	35.8
14 - 15	32.69	*	*	32.78	33.77	33.5	33.01	33.1	33.28	36.43
15 - 16	32.87	*	*	33.1	33.11	34.04	32.64	33.03	33.39	36.17
16 - 17	32.15	*	*	33.26	33.65	33.52	33.5	33.11	33.51	36.2
17 - 18	33.67	*	*	33.58	33.69	34.09	33.15	33.65	33.63	36.77
18 - 19	33.67	*	*	32.35	32.91	34.23	33.16	32.98	33.7	36.53
19 - 20	32.77	*	33.4	33.07	33.72	33.59	33.29	33.28	33.47	36.55
20 - 21	32.78	*	32.88	33.41	33.82	33.27	33.86	33.22	33.55	36.28
21 - 22	32.96	*	31.94	31.79	32.18	34.12	31.82	32.14	33.19	35.67
22 - 23	31.67	*	32.31	31.22	30.03	32.21	32.38	30.97	32.27	34.05
23 - 24	31.25	*	32.17	34	30.54	29.56	31.75	31.59	30.59	35.62
Totals	32.7	33	32.8	33.2	33.1	33.2	32.9			
% of Total	14.16%	14.29%	14.21%	14.38%	14.34%	14.38%	14.25%			

Outgoing Weekly EightyFifthSpeeds
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	36	36	34	35.5	36	34.75	36
1 - 2	*	*	*	34	30	33	34	32	33.5	32
2 - 3	*	*	*	0	37	32.5	36	37	34.25	37
3 - 4	*	*	*	38	35	32	35	36.5	33.5	36.5
4 - 5	*	*	*	37	37	32	36	37	34	37
5 - 6	*	*	*	37.3	36.8	36	33	37.05	34.5	37.05
6 - 7	*	*	*	37.6	36.7	34	36	37.15	35	37.15
7 - 8	*	*	*	36.7	36.5	36.6	39	36.6	37.8	36.6
8 - 9	*	*	*	36	35.8	37.4	36.3	35.9	36.85	35.9
9 - 10	*	*	*	36.1	36	35.4	35.7	36.05	35.55	36.05
10 - 11	*	*	*	36.5	36.6	37.1	36.4	36.55	36.75	36.55
11 - 12	*	*	*	36.3	36.7	36.6	35.5	36.5	36.05	36.5
12 - 13	*	*	*	36	36.2	36.1	35.8	36.1	35.95	36.1
13 - 14	*	*	*	36.5	36.3	35.9	35.4	36.4	35.65	36.4
14 - 15	*	*	*	36.1	37.4	37	36.8	36.75	36.9	36.75
15 - 16	*	*	*	36.7	36.1	36.6	35.5	36.4	36.05	36.4
16 - 17	*	*	*	36.2	37	36.5	36.4	36.6	36.45	36.6
17 - 18	*	*	*	36.6	36.7	36.9	36.3	36.65	36.6	36.65
18 - 19	*	*	*	36.6	36.3	38.2	36.6	36.45	37.4	36.45
19 - 20	*	*	36.5	37	36.7	36.6	36	36.73	36.3	36.73
20 - 21	*	*	35.9	36.5	36.8	36.2	36.3	36.4	36.25	36.4
21 - 22	*	*	35.3	36	35.4	38	35	35.57	36.5	35.57
22 - 23	*	*	35.7	33	34	36	36	34.23	36	34.23
23 - 24	*	*	35.5	36	37	32.5	36	36.17	34.25	36.17
Totals	0	0	178.9	834.7	866	853.1	860.5			
% of Total	0%	0%	4.98%	23.23%	24.1%	23.74%	23.95%			

Outgoing Weekly EightyFifthSpeeds

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	39	29	*	*	*	*	*	34	0	34
1 - 2	34	46	*	*	*	*	*	40	0	40
2 - 3	34	32	*	*	*	*	*	33	0	33
3 - 4	34	35	*	*	*	*	*	34.5	0	34.5
4 - 5	37	35	*	*	*	*	*	36	0	36
5 - 6	37.2	36.5	*	*	*	*	*	36.85	0	36.85
6 - 7	37.2	36.6	*	*	*	*	*	36.9	0	36.9
7 - 8	36.4	36.8	*	*	*	*	*	36.6	0	36.6
8 - 9	35.5	36.8	*	*	*	*	*	36.15	0	36.15
9 - 10	35.9	36	*	*	*	*	*	35.95	0	35.95
10 - 11	35	36.1	*	*	*	*	*	35.55	0	35.55
11 - 12	35.4	36.7	*	*	*	*	*	36.05	0	36.05
12 - 13	35.7	37	*	*	*	*	*	36.35	0	36.35
13 - 14	35.3	35.1	*	*	*	*	*	35.2	0	35.2
14 - 15	35.8	*	*	*	*	*	*	35.8	0	35.8
15 - 16	35.7	*	*	*	*	*	*	35.7	0	35.7
16 - 17	35.4	*	*	*	*	*	*	35.4	0	35.4
17 - 18	37	*	*	*	*	*	*	37	0	37
18 - 19	36.7	*	*	*	*	*	*	36.7	0	36.7
19 - 20	36	*	*	*	*	*	*	36	0	36
20 - 21	35.9	*	*	*	*	*	*	35.9	0	35.9
21 - 22	36	*	*	*	*	*	*	36	0	36
22 - 23	33.5	*	*	*	*	*	*	33.5	0	33.5
23 - 24	34	*	*	*	*	*	*	34	0	34
Totals	857.6	504.6	0	0	0	0	0			
% of Total	62.96%	37.04%	0%	0%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	39	29	*	36	36	34	35.5	35	34.75	35
1 - 2	34	46	*	34	30	33	34	36	33.5	36
2 - 3	34	32	*	0	37	32.5	36	34.33	34.25	34.33
3 - 4	34	35	*	38	35	32	35	35.5	33.5	35.5
4 - 5	37	35	*	37	37	32	36	36.5	34	36.5
5 - 6	37.2	36.5	*	37.3	36.8	36	33	36.95	34.5	36.95
6 - 7	37.2	36.6	*	37.6	36.7	34	36	37.03	35	37.03
7 - 8	36.4	36.8	*	36.7	36.5	36.6	39	36.6	37.8	36.6
8 - 9	35.5	36.8	*	36	35.8	37.4	36.3	36.02	36.85	36.02
9 - 10	35.9	36	*	36.1	36	35.4	35.7	36	35.55	36
10 - 11	35	36.1	*	36.5	36.6	37.1	36.4	36.05	36.75	36.05
11 - 12	35.4	36.7	*	36.3	36.7	36.6	35.5	36.28	36.05	36.28
12 - 13	35.7	37	*	36	36.2	36.1	35.8	36.22	35.95	36.22
13 - 14	35.3	35.1	*	36.5	36.3	35.9	35.4	35.8	35.65	35.8
14 - 15	35.8	*	*	36.1	37.4	37	36.8	36.43	36.9	36.43
15 - 16	35.7	*	*	36.7	36.1	36.6	35.5	36.17	36.05	36.17
16 - 17	35.4	*	*	36.2	37	36.5	36.4	36.2	36.45	36.2
17 - 18	37	*	*	36.6	36.7	36.9	36.3	36.77	36.6	36.77
18 - 19	36.7	*	*	36.6	36.3	38.2	36.6	36.53	37.4	36.53
19 - 20	36	*	36.5	37	36.7	36.6	36	36.55	36.3	36.55
20 - 21	35.9	*	35.9	36.5	36.8	36.2	36.3	36.28	36.25	36.28
21 - 22	36	*	35.3	36	35.4	38	35	35.67	36.5	35.67
22 - 23	33.5	*	35.7	33	34	36	36	34.05	36	34.05
23 - 24	34	*	35.5	36	37	32.5	36	35.62	34.25	35.62

Summary of Violators

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	41	32.8	0	0.0
01:00:00	31	30.8	0	0.0
02:00:00	21	31.9	0	0.0
03:00:00	35	32.4	0	0.0
04:00:00	53	32.8	0	0.0
05:00:00	208	33.2	0	0.0
06:00:00	461	33.3	0	0.0
07:00:00	917	33.5	0	0.0
08:00:00	786	32.8	0	0.0
09:00:00	589	32.5	0	0.0
10:00:00	554	32.7	0	0.0
11:00:00	576	32.8	0	0.0
12:00:00	640	32.8	0	0.0
13:00:00	602	32.6	0	0.0
14:00:00	656	33.2	0	0.0
15:00:00	557	33.1	0	0.0
16:00:00	500	33.3	0	0.0
17:00:00	517	33.6	0	0.0
18:00:00	389	33.3	0	0.0
19:00:00	380	33.3	0	0.0
20:00:00	355	33.3	0	0.0
21:00:00	212	32.5	0	0.0
22:00:00	115	31.4	0	0.0
23:00:00	54	31.3	0	0.0

Outgoing Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	19:00	0	0	1	7	36	22	3	1	0	0	0	0	0	0	0	0	0	0	70	33.4	36.5	29 to 39	88.6	*	*	0	70	0	
9/4/2024	20:00	0	0	0	17	34	19	3	0	0	0	0	0	0	0	0	0	0	0	73	32.9	35.9	28 to 38	87.7	*	*	0	73	0	
9/4/2024	21:00	0	0	2	6	15	8	1	0	0	0	0	0	0	0	0	0	0	0	32	31.9	35.3	27 to 37	87.5	*	*	0	32	0	
9/4/2024	22:00	0	0	1	2	5	5	0	0	0	0	0	0	0	0	0	0	0	0	13	32.3	35.7	28 to 38	92.3	*	*	0	13	0	
9/4/2024	23:00	0	0	1	1	5	5	0	0	0	0	0	0	0	0	0	0	0	0	12	32.2	35.5	26 to 36	83.3	*	*	0	12	0	
24 Hr Summary		0	0	5	33	95	59	7	1	0	0	0	0	0	0	0	0	0	0	200	32.8	36	28 to 38	86.0	*	*	0	200	0	

Outgoing Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/5/2024	00:00	0	0	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	4	35.3	36	27 to 37	100.0	*	*	0	4	0	
9/5/2024	01:00	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	4	31.5	34	25 to 35	100.0	*	*	0	4	0	
9/5/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/5/2024	03:00	1	0	0	0	1	3	0	1	0	0	0	0	0	0	0	0	0	0	6	34	38	28 to 38	66.7	*	*	0	6	0	
9/5/2024	04:00	0	0	1	1	7	2	2	0	0	0	0	0	0	0	0	0	0	0	13	32.9	37	27 to 37	76.9	*	*	0	13	0	
9/5/2024	05:00	0	0	1	6	28	20	4	1	0	0	0	0	0	0	0	0	0	0	60	34.1	37.3	28 to 38	83.3	*	*	0	60	0	
9/5/2024	06:00	0	0	1	13	43	36	7	1	0	0	0	0	0	0	0	0	0	0	101	34.1	37.6	28 to 38	86.1	*	*	1	100	0	
9/5/2024	07:00	0	0	0	25	122	77	9	1	0	0	0	0	0	0	0	0	0	0	234	33.6	36.7	29 to 39	89.7	*	*	0	234	0	
9/5/2024	08:00	0	1	1	21	84	55	8	0	0	0	0	0	0	0	0	0	0	0	170	33.3	36	28 to 38	87.6	*	*	2	168	0	
9/5/2024	09:00	0	0	2	22	44	32	6	0	0	0	0	0	0	0	0	0	0	0	106	32.8	36.1	27 to 37	84.9	*	*	0	106	0	
9/5/2024	10:00	0	0	1	16	40	19	4	0	0	0	0	0	0	0	0	0	0	0	80	32.8	36.5	28 to 38	83.8	*	*	0	80	0	
9/5/2024	11:00	0	1	1	14	54	32	3	0	0	0	0	0	0	0	0	0	0	0	105	33	36.3	29 to 39	90.5	*	*	1	104	0	
9/5/2024	12:00	0	0	2	8	44	24	2	0	0	0	0	0	0	0	0	0	0	0	80	33	36	28 to 38	92.5	*	*	0	80	0	
9/5/2024	13:00	0	0	1	16	33	29	3	0	0	0	0	0	0	0	0	0	0	0	82	32.9	36.5	28 to 38	84.1	*	*	0	82	0	
9/5/2024	14:00	0	0	4	26	68	42	6	0	0	0	0	0	0	0	0	0	0	0	146	32.8	36.1	28 to 38	86.3	*	*	2	144	0	
9/5/2024	15:00	0	1	4	12	61	36	7	0	0	0	0	0	0	0	0	0	0	0	121	33.1	36.7	29 to 39	84.3	*	*	2	119	0	
9/5/2024	16:00	0	0	1	15	53	39	6	0	0	0	0	0	0	0	0	0	0	0	114	33.3	36.2	28 to 38	88.6	*	*	0	114	0	
9/5/2024	17:00	0	0	1	10	56	31	6	0	0	0	0	0	0	0	0	0	0	0	104	33.6	36.6	29 to 39	89.4	*	*	0	104	0	
9/5/2024	18:00	0	2	2	10	28	19	5	0	0	0	0	0	0	0	0	0	0	0	66	32.3	36.6	27 to 37	77.3	*	*	1	65	0	
9/5/2024	19:00	0	0	2	12	31	20	6	1	0	0	0	0	0	0	0	0	0	0	72	33.1	37	27 to 37	77.8	*	*	0	72	0	
9/5/2024	20:00	0	0	1	9	27	16	3	0	0	0	0	0	0	0	0	0	0	0	56	33.4	36.5	28 to 38	89.3	*	*	1	55	0	
9/5/2024	21:00	0	0	2	11	17	12	0	0	0	0	0	0	0	0	0	0	0	0	42	31.8	36	27 to 37	85.7	*	*	0	42	0	
9/5/2024	22:00	0	0	0	5	11	2	0	0	0	0	0	0	0	0	0	0	0	0	18	31.2	33	25 to 35	94.4	*	*	0	18	0	
9/5/2024	23:00	0	0	0	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	4	34	36	27 to 37	100.0	*	*	0	4	0	
24 Hr Summary		1	5	28	254	856	552	87	5	0	0	0	0	0	0	0	0	0	0	1788	33.2	37	28 to 38	85.9	*	*	10	1778	0	

Outgoing Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/6/2024	00:00	0	0	0	2	4	3	0	0	0	0	0	0	0	0	0	0	0	0	9	32	36	29 to 39	88.9	*	*	0	9	0
9/6/2024	01:00	0	0	1	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	5	29.2	30	20 to 30	80.0	*	*	0	5	0
9/6/2024	02:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	36	37	27 to 37	100.0	*	*	0	2	0
9/6/2024	03:00	0	0	0	1	3	2	0	0	0	0	0	0	0	0	0	0	0	0	6	32.7	35	26 to 36	100.0	*	*	1	5	0
9/6/2024	04:00	0	0	1	1	5	5	1	0	0	0	0	0	0	0	0	0	0	0	13	33.2	37	28 to 38	84.6	*	*	0	13	0
9/6/2024	05:00	0	0	0	6	17	12	2	0	0	0	0	0	0	0	0	0	0	0	37	33.1	36.8	28 to 38	81.1	*	*	0	37	0
9/6/2024	06:00	0	0	0	17	51	31	6	0	0	0	0	0	0	0	0	0	0	0	105	33.3	36.7	28 to 38	87.6	*	*	1	104	0
9/6/2024	07:00	0	0	3	23	118	72	8	0	0	0	0	0	0	0	0	0	0	0	224	33.4	36.5	28 to 38	87.9	*	*	3	221	0
9/6/2024	08:00	0	2	3	27	79	30	7	0	0	0	0	0	0	0	0	0	0	0	148	32.3	35.8	28 to 38	83.8	*	*	2	146	0
9/6/2024	09:00	0	1	3	18	50	20	4	0	0	0	0	0	0	0	0	0	0	0	96	32.2	36	28 to 38	86.5	*	*	3	93	0
9/6/2024	10:00	0	0	2	14	40	20	5	0	0	0	0	0	0	0	0	0	0	0	81	32.8	36.6	27 to 37	81.5	*	*	1	80	0
9/6/2024	11:00	0	0	2	14	45	29	7	1	0	0	0	0	0	0	0	0	0	0	98	33.1	36.7	28 to 38	82.7	*	*	0	98	0
9/6/2024	12:00	0	0	4	19	41	34	2	1	0	0	0	0	0	0	0	0	0	0	101	32.6	36.2	28 to 38	85.1	*	*	1	100	0
9/6/2024	13:00	0	1	0	19	67	36	6	1	0	0	0	0	0	0	0	0	0	0	130	33	36.3	27 to 37	87.7	*	*	0	130	0
9/6/2024	14:00	0	0	4	19	64	59	11	1	0	0	0	0	0	0	0	0	0	0	158	33.8	37.4	28 to 38	85.4	*	*	1	157	0
9/6/2024	15:00	0	0	1	19	57	46	5	0	0	0	0	0	0	0	0	0	0	0	128	33.1	36.1	28 to 38	86.7	*	*	0	128	0
9/6/2024	16:00	0	0	1	9	64	27	7	0	2	0	0	0	0	0	0	0	0	0	110	33.6	37	30 to 40	86.4	*	*	1	109	0
9/6/2024	17:00	0	0	2	14	59	46	6	1	0	0	0	0	0	0	0	0	0	0	128	33.7	36.7	29 to 39	87.5	*	*	2	126	0
9/6/2024	18:00	0	0	3	16	47	31	2	1	0	0	0	0	0	0	0	0	0	0	100	32.9	36.3	28 to 38	86.0	*	*	0	100	0
9/6/2024	19:00	0	0	0	8	39	22	4	1	0	0	0	0	0	0	0	0	0	0	74	33.7	36.7	30 to 40	85.1	*	*	0	74	0
9/6/2024	20:00	0	0	1	6	27	23	4	0	0	0	0	0	0	0	0	0	0	0	61	33.8	36.8	28 to 38	90.2	*	*	0	61	0
9/6/2024	21:00	0	0	1	10	19	15	0	0	0	0	0	0	0	0	0	0	0	0	45	32.2	35.4	26 to 36	91.1	*	*	0	45	0
9/6/2024	22:00	0	0	5	9	13	4	1	0	0	0	0	0	0	0	0	0	0	0	32	30	34	26 to 36	81.3	*	*	0	32	0
9/6/2024	23:00	0	0	1	7	1	4	0	0	0	0	0	0	0	0	0	0	0	0	13	30.5	37	22 to 32	69.2	*	*	0	13	0
24 Hr Summary		0	4	38	280	911	574	88	7	2	0	0	0	0	0	0	0	0	0	1904	33.1	37	28 to 38	85.1	*	*	16	1888	0

Outgoing Histogram
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/7/2024	00:00	0	0	0	4	3	0	0	1	0	0	0	0	0	0	0	0	0	0	8	31.5	34	24 to 34	87.5	*	*	0	8	0
9/7/2024	01:00	0	0	3	4	1	1	0	0	0	0	0	0	0	0	0	0	0	0	9	27.3	33	23 to 33	88.9	*	*	0	9	0
9/7/2024	02:00	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	6	29.2	32.5	23 to 33	100.0	*	*	0	6	0
9/7/2024	03:00	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	28.5	32	24 to 34	100.0	*	*	0	4	0
9/7/2024	04:00	0	0	0	1	3	1	0	0	0	0	0	0	0	0	0	0	0	0	5	32	32	27 to 37	100.0	*	*	0	5	0
9/7/2024	05:00	0	0	1	4	7	3	0	0	0	0	0	0	0	0	0	0	0	0	15	31.1	36	28 to 38	86.7	*	*	0	15	0
9/7/2024	06:00	0	0	3	7	11	4	0	0	0	0	0	0	0	0	0	0	0	0	25	30.7	34	27 to 37	88.0	*	*	0	25	0
9/7/2024	07:00	0	0	0	5	15	13	1	0	0	0	0	0	0	0	0	0	0	0	34	33.3	36.6	27 to 37	91.2	*	*	0	34	0
9/7/2024	08:00	0	0	1	8	26	16	4	0	0	0	0	0	0	0	0	0	0	0	55	33.4	37.4	28 to 38	83.6	*	*	1	54	0
9/7/2024	09:00	0	0	1	19	47	19	1	0	0	0	0	0	0	0	0	0	0	0	87	31.9	35.4	27 to 37	90.8	*	*	0	87	0
9/7/2024	10:00	0	0	1	17	50	36	8	0	0	0	0	0	0	0	0	0	0	0	112	33.5	37.1	28 to 38	83.9	*	*	0	112	0
9/7/2024	11:00	0	0	1	15	52	31	7	1	0	0	0	0	0	0	0	0	0	0	107	33.3	36.6	27 to 37	86.0	*	*	0	107	0
9/7/2024	12:00	0	0	0	13	65	34	3	0	0	0	0	0	0	0	0	0	0	0	115	33.2	36.1	28 to 38	93.0	*	*	0	115	0
9/7/2024	13:00	0	0	5	19	51	28	1	0	0	0	0	0	0	0	0	0	0	0	104	32.4	35.9	28 to 38	81.7	*	*	1	103	0
9/7/2024	14:00	0	0	2	12	56	37	7	0	0	0	0	0	0	0	0	0	0	0	114	33.5	37	29 to 39	85.1	*	*	1	113	0
9/7/2024	15:00	0	0	0	4	56	33	6	0	0	0	0	0	0	0	0	0	0	0	99	34	36.6	29 to 39	92.9	*	*	0	99	0
9/7/2024	16:00	0	0	0	10	53	31	1	1	0	0	0	0	0	0	0	0	0	0	96	33.5	36.5	29 to 39	92.7	*	*	1	95	0
9/7/2024	17:00	0	1	1	9	45	37	9	0	0	0	0	0	0	0	0	0	0	0	102	34.1	36.9	29 to 39	84.3	*	*	1	101	0
9/7/2024	18:00	0	0	1	7	32	30	6	1	0	0	0	0	0	0	0	0	0	0	77	34.2	38.2	29 to 39	81.8	*	*	0	77	0
9/7/2024	19:00	0	0	0	6	38	25	2	0	0	0	0	0	0	0	0	0	0	0	71	33.6	36.6	29 to 39	94.4	*	*	0	71	0
9/7/2024	20:00	1	0	1	6	31	19	1	1	0	0	0	0	0	0	0	0	0	0	60	33.3	36.2	27 to 37	85.0	*	*	2	58	0
9/7/2024	21:00	0	0	0	5	18	14	2	2	0	0	0	0	0	0	0	0	0	0	41	34.1	38	27 to 37	82.9	*	*	0	41	0
9/7/2024	22:00	0	0	1	6	12	3	2	0	0	0	0	0	0	0	0	0	0	0	24	32.2	36	27 to 37	83.3	*	*	0	24	0
9/7/2024	23:00	0	0	0	5	4	0	0	0	0	0	0	0	0	0	0	0	0	0	9	29.6	32.5	23 to 33	100.0	*	*	0	9	0
24 Hr Summary		1	1	25	188	681	415	61	7	0	0	0	0	0	0	0	0	0	0	1379	33.2	37	28 to 38	84.8	*	*	7	1372	0

Outgoing Histogram

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/8/2024	00:00	0	0	0	1	8	2	1	0	0	0	0	0	0	0	0	0	0	0	12	32.7	35.5	26 to 36	91.7	*	*	0	12	0
9/8/2024	01:00	0	0	0	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	5	33.4	34	26 to 36	100.0	*	*	0	5	0
9/8/2024	02:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34.5	36	26 to 36	100.0	*	*	0	2	0
9/8/2024	03:00	0	0	0	2	1	2	0	0	0	0	0	0	0	0	0	0	0	0	5	31.8	35	26 to 36	100.0	*	*	0	5	0
9/8/2024	04:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	36	26 to 36	100.0	*	*	0	2	0
9/8/2024	05:00	0	0	0	4	3	1	0	0	0	0	0	0	0	0	0	0	0	0	8	30.4	33	26 to 36	100.0	*	*	0	8	0
9/8/2024	06:00	0	0	0	5	7	4	0	0	0	0	0	0	0	0	0	0	0	0	16	31.8	36	27 to 37	93.8	*	*	0	16	0
9/8/2024	07:00	0	0	0	3	8	9	3	0	0	0	0	0	0	0	0	0	0	0	23	34.4	39	28 to 38	78.3	*	*	0	23	0
9/8/2024	08:00	0	0	1	14	31	19	2	0	0	0	0	0	0	0	0	0	0	0	67	32.9	36.3	27 to 37	91.0	*	*	0	67	0
9/8/2024	09:00	0	0	0	12	30	19	3	0	0	0	0	0	0	0	0	0	0	0	64	32.9	35.7	28 to 38	89.1	*	*	0	64	0
9/8/2024	10:00	0	0	1	18	47	21	7	1	0	0	0	0	0	0	0	0	0	0	95	33	36.4	27 to 37	85.3	*	*	0	95	0
9/8/2024	11:00	0	0	2	13	41	31	1	0	0	0	0	0	0	0	0	0	0	0	88	32.7	35.5	27 to 37	89.8	*	*	1	87	0
9/8/2024	12:00	0	0	1	28	89	47	4	0	0	0	0	0	0	0	0	0	0	0	169	32.7	35.8	26 to 36	85.8	*	*	1	168	0
9/8/2024	13:00	0	0	1	21	44	23	2	1	0	0	0	0	0	0	0	0	0	0	92	32.5	35.4	27 to 37	89.1	*	*	0	92	0
9/8/2024	14:00	0	0	0	21	42	25	6	0	0	0	0	0	0	0	0	0	0	0	94	33	36.8	28 to 38	86.2	*	*	0	94	0
9/8/2024	15:00	0	0	1	13	47	25	1	0	0	0	0	0	0	0	0	0	0	0	87	32.6	35.5	28 to 38	90.8	*	*	0	87	0
9/8/2024	16:00	0	0	1	16	43	37	4	0	0	0	0	0	0	0	0	0	0	0	101	33.5	36.4	27 to 37	88.1	*	*	0	101	0
9/8/2024	17:00	0	0	0	16	44	33	5	0	0	0	0	0	0	0	0	0	0	0	98	33.2	36.3	28 to 38	85.7	*	*	0	98	0
9/8/2024	18:00	0	0	3	12	28	29	3	1	0	0	0	0	0	0	0	0	0	0	76	33.2	36.6	28 to 38	84.2	*	*	1	75	0
9/8/2024	19:00	0	0	0	7	22	13	3	0	0	0	0	0	0	0	0	0	0	0	45	33.3	36	27 to 37	84.4	*	*	0	45	0
9/8/2024	20:00	0	0	0	3	32	19	2	0	0	0	0	0	0	0	0	0	0	0	56	33.9	36.3	29 to 39	96.4	*	*	1	55	0
9/8/2024	21:00	0	0	1	7	13	6	0	1	0	0	0	0	0	0	0	0	0	0	28	31.8	35	26 to 36	85.7	*	*	0	28	0
9/8/2024	22:00	0	0	1	2	8	5	0	0	0	0	0	0	0	0	0	0	0	0	16	32.4	36	27 to 37	93.8	*	*	0	16	0
9/8/2024	23:00	0	0	1	3	1	3	0	0	0	0	0	0	0	0	0	0	0	0	8	31.8	36	28 to 38	87.5	*	*	0	8	0
24 Hr Summary		0	0	14	221	595	376	47	4	0	0	0	0	0	0	0	0	0	0	1257	32.9	36	28 to 38	86.9	*	*	4	1253	0

Outgoing Histogram
 Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/9/2024	00:00	0	0	0	0	2	3	1	0	0	0	0	0	0	0	0	0	0	0	6	36	39	32 to 42	100.0	*	*	0	6	0
9/9/2024	01:00	0	0	0	1	3	1	0	0	0	0	0	0	0	0	0	0	0	0	5	31.6	34	26 to 36	100.0	*	*	0	5	0
9/9/2024	02:00	0	0	0	4	4	0	1	1	0	0	0	0	0	0	0	0	0	0	10	32.2	34	24 to 34	80.0	*	*	0	10	0
9/9/2024	03:00	0	0	0	1	6	1	0	0	0	0	0	0	0	0	0	0	0	0	8	32.5	34	25 to 35	100.0	*	*	0	8	0
9/9/2024	04:00	0	0	1	1	5	3	0	0	0	0	0	0	0	0	0	0	0	0	10	32.8	37	29 to 39	90.0	*	*	0	10	0
9/9/2024	05:00	0	0	1	6	22	11	3	0	0	0	0	0	0	0	0	0	0	0	43	33.1	37.2	28 to 38	88.4	*	*	1	42	0
9/9/2024	06:00	0	0	3	14	43	36	4	1	0	0	0	0	0	0	0	0	0	0	101	33.3	37.2	29 to 39	83.2	*	*	0	101	0
9/9/2024	07:00	0	0	4	16	77	62	2	1	0	0	0	0	0	0	0	0	0	0	162	33.3	36.4	28 to 38	89.5	*	*	0	162	0
9/9/2024	08:00	0	0	3	30	78	36	7	0	0	0	0	0	0	0	0	0	0	0	154	32.2	35.5	26 to 36	83.1	*	*	0	154	0
9/9/2024	09:00	0	0	2	29	56	43	0	0	0	0	0	0	0	0	0	0	0	0	130	32.4	35.9	28 to 38	90.0	*	*	1	129	0
9/9/2024	10:00	0	0	5	18	51	17	2	0	0	0	0	0	0	0	0	0	0	0	93	31.5	35	26 to 36	82.8	*	*	0	93	0
9/9/2024	11:00	0	0	2	18	46	21	1	0	0	0	0	0	0	0	0	0	0	0	88	31.8	35.4	27 to 37	89.8	*	*	0	88	0
9/9/2024	12:00	0	0	2	19	34	28	1	1	0	0	0	0	0	0	0	0	0	0	85	32.5	35.7	27 to 37	83.5	*	*	0	85	0
9/9/2024	13:00	0	0	2	14	54	27	2	1	0	0	0	0	0	0	0	0	0	0	100	32.6	35.3	26 to 36	87.0	*	*	0	100	0
9/9/2024	14:00	0	1	3	19	80	35	5	1	0	0	0	0	0	0	0	0	0	0	144	32.7	35.8	28 to 38	86.8	*	*	3	141	0
9/9/2024	15:00	0	0	1	19	62	38	2	0	0	0	0	0	0	0	0	0	0	0	122	32.9	35.7	28 to 38	90.2	*	*	2	120	0
9/9/2024	16:00	0	1	3	13	42	18	2	0	0	0	0	0	0	0	0	0	0	0	79	32.2	35.4	27 to 37	87.3	*	*	0	79	0
9/9/2024	17:00	0	0	1	10	40	30	4	0	0	0	0	0	0	0	0	0	0	0	85	33.7	37	28 to 38	90.6	*	*	0	85	0
9/9/2024	18:00	0	0	0	4	40	24	2	0	0	0	0	0	0	0	0	0	0	0	70	33.7	36.7	28 to 38	97.1	*	*	0	70	0
9/9/2024	19:00	0	0	2	3	31	9	3	0	0	0	0	0	0	0	0	0	0	0	48	32.8	36	29 to 39	85.4	*	*	1	47	0
9/9/2024	20:00	0	0	0	8	27	13	1	0	0	0	0	0	0	0	0	0	0	0	49	32.8	35.9	26 to 36	87.8	*	*	0	49	0
9/9/2024	21:00	0	0	0	5	11	7	1	0	0	0	0	0	0	0	0	0	0	0	24	33	36	27 to 37	83.3	*	*	0	24	0
9/9/2024	22:00	0	0	0	3	8	1	0	0	0	0	0	0	0	0	0	0	0	0	12	31.7	33.5	26 to 36	100.0	*	*	0	12	0
9/9/2024	23:00	0	0	0	2	5	1	0	0	0	0	0	0	0	0	0	0	0	0	8	31.3	34	24 to 34	87.5	*	*	0	8	0
24 Hr Summary		0	2	35	257	827	465	44	6	0	0	0	0	0	0	0	0	0	0	1636	32.7	36	28 to 38	85.9	*	*	8	1628	0

Outgoing Histogram

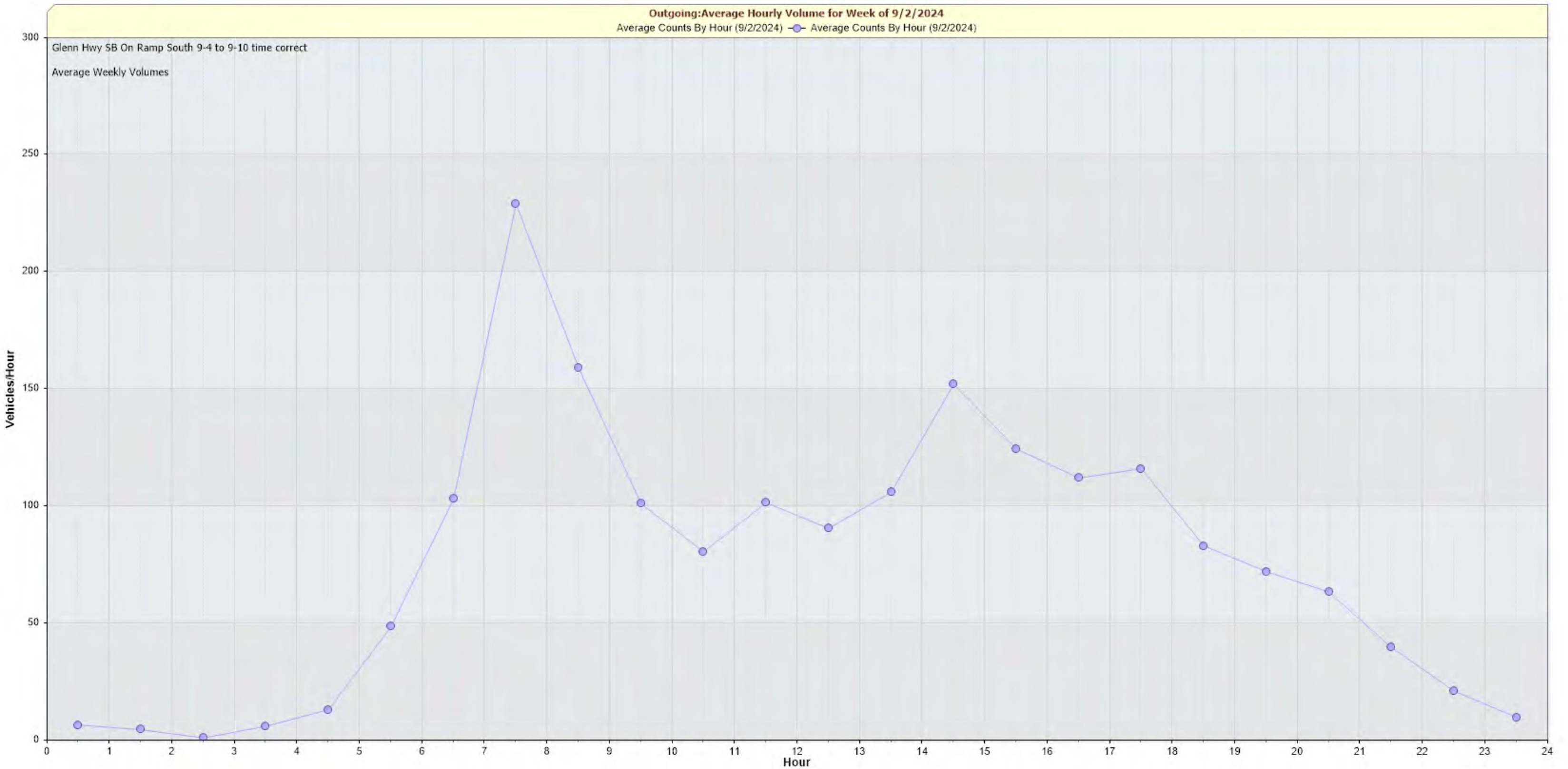
Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

from Wed-Sep-04-2024-07-00-PM to Tue-Sep-10-2024-01-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28.5	29	19 to 29	100.0	*	*	0	2	0	
9/10/2024	01:00	0	0	0	1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	3	37.7	46	36 to 46	66.7	*	*	0	3	0	
9/10/2024	02:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	32	32	22 to 32	100.0	*	*	0	1	0	
9/10/2024	03:00	0	0	0	0	4	2	0	0	0	0	0	0	0	0	0	0	0	0	6	33.5	35	28 to 38	100.0	*	*	0	6	0	
9/10/2024	04:00	0	0	1	1	5	2	1	0	0	0	0	0	0	0	0	0	0	0	10	32.1	35	27 to 37	80.0	*	*	0	10	0	
9/10/2024	05:00	0	0	0	8	19	15	3	0	0	0	0	0	0	0	0	0	0	0	45	33.4	36.5	27 to 37	82.2	*	*	0	45	0	
9/10/2024	06:00	0	0	1	20	46	42	4	0	0	0	0	0	0	0	0	0	0	0	113	33.2	36.6	27 to 37	83.2	*	*	1	112	0	
9/10/2024	07:00	0	0	3	22	122	81	11	1	0	0	0	0	0	0	0	0	0	0	240	33.7	36.8	29 to 39	88.8	*	*	2	238	0	
9/10/2024	08:00	0	0	4	32	84	64	7	1	0	0	0	0	0	0	0	0	0	0	192	33.2	36.8	28 to 38	83.3	*	*	0	192	0	
9/10/2024	09:00	0	0	3	12	56	33	1	1	0	0	0	0	0	0	0	0	0	0	106	32.9	36	27 to 37	88.7	*	*	2	104	0	
9/10/2024	10:00	0	0	3	19	43	26	2	0	0	0	0	0	0	0	0	0	0	0	93	32.5	36.1	27 to 37	86.0	*	*	0	93	0	
9/10/2024	11:00	0	0	1	18	41	27	3	0	0	0	0	0	0	0	0	0	0	0	90	32.8	36.7	28 to 38	82.2	*	*	0	90	0	
9/10/2024	12:00	0	0	2	16	46	22	4	0	0	0	0	0	0	0	0	0	0	0	90	32.8	37	28 to 38	82.2	*	*	1	89	0	
9/10/2024	13:00	0	0	3	17	55	16	3	0	0	0	0	0	0	0	0	0	0	0	94	31.9	35.1	27 to 37	90.4	*	*	0	94	0	
9/10/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	21	168	522	330	40	4	0	0	0	0	0	0	0	0	0	0	1085	33	37	28 to 38	84.1	*	*	6	1079	0	

Outgoing:Average Hourly Volume for Week of 9/2/2024
Average Counts By Hour (9/2/2024) —●— Average Counts By Hour (9/2/2024)

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/2/2024
Average Hourly WEEKDAY Speeds By Hour (9/2/2024) — Average Hourly WEEKDAY Speeds By Hour (9/2/2024)

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct
Average Weekly Speeds

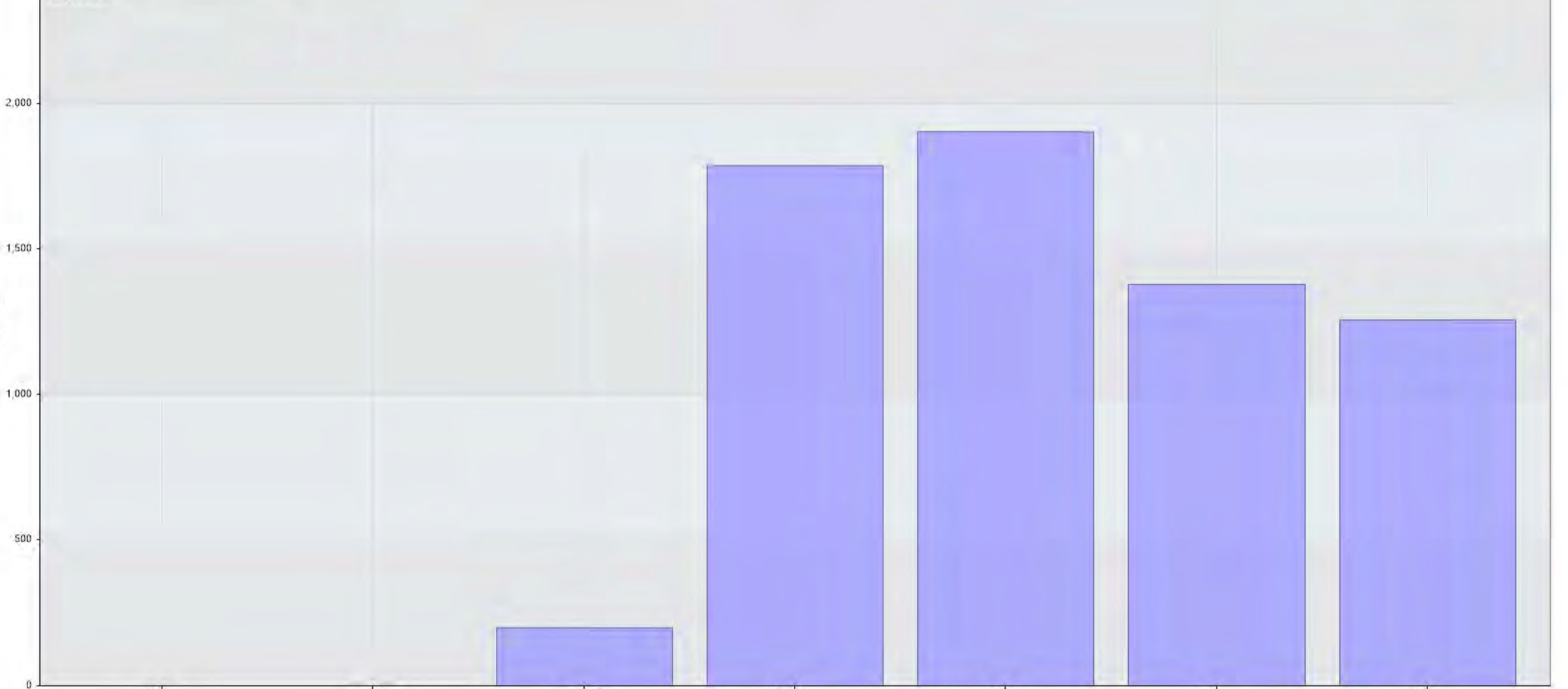


Outgoing: Daily Volume for Week of 9/2/2024

Daily Vehicle Counts

Glenn Hwy SB On Ramp South 9-4 to 9-10 time correct

Daily Volumes



Monday Tuesday Wednesday Thursday Day Friday Saturday Sunday

For Project: Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:44:56 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/4/2024 5:00:00 PM through 9/10/2024 2:59:59 PM

85th Percentile Speed 39 MPH

85th Percentile Vehicles 4834

Max Speed 54 MPH on 9/6/2024 11:00:51 AM

Total Vehicles 5687

AADT: 961

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	811	812
AM Peak 7:00 AM	81	66
PM Peak 4:00 PM	99	93

Speed

Speed Limit: 65

85th Percentile Speed: 39

50th Percentile Speed: 34

10 MPH Pace Interval: 29.0 MPH to 39.0 MPH

Average Speed: 33.76

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	0	0	0	0	0
% over limit	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speeder	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speed	32.8	33.4	34.2	34.3	34.1	34.1	32.7

Class Counts

	Number	%
VEH_SM	15	0.3
VEH_MED	5167	90.9
VEH_LG	505	8.9
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/4/2024 6:00:00 PM	39.0	60	71	44	0.0	0.0%	35.0
9/4/2024 7:00:00 PM	38.0	88	104	52	0.0	0.0%	34.3
9/4/2024 8:00:00 PM	41.0	40	47	48	0.0	0.0%	34.8
9/4/2024 9:00:00 PM	37.0	40	47	46	0.0	0.0%	32.5
9/4/2024 10:00:00 PM	40.0	17	20	45	0.0	0.0%	33.3
9/4/2024 11:00:00 PM	39.0	14	16	48	0.0	0.0%	34.9
9/5/2024 12:00:00 AM	39.0	3	3	39	0.0	0.0%	34.0
9/5/2024 1:00:00 AM	34.0	2	2	34	0.0	0.0%	30.5
9/5/2024 2:00:00 AM	43.0	3	3	43	0.0	0.0%	39.3
9/5/2024 3:00:00 AM	44.0	1	1	44	0.0	0.0%	44.0
9/5/2024 4:00:00 AM	34.0	3	4	41	0.0	0.0%	34.5
9/5/2024 5:00:00 AM	36.0	3	4	40	0.0	0.0%	34.5
9/5/2024 6:00:00 AM	41.0	13	15	42	0.0	0.0%	36.1
9/5/2024 7:00:00 AM	40.0	49	58	45	0.0	0.0%	35.1
9/5/2024 8:00:00 AM	39.0	82	97	45	0.0	0.0%	34.1
9/5/2024 9:00:00 AM	39.0	70	82	50	0.0	0.0%	34.6
9/5/2024 10:00:00 AM	39.0	81	95	46	0.0	0.0%	33.8
9/5/2024 11:00:00 AM	40.0	67	79	48	0.0	0.0%	34.8
9/5/2024 12:00:00 PM	40.0	73	86	46	0.0	0.0%	34.4
9/5/2024 1:00:00 PM	40.0	72	85	49	0.0	0.0%	34.3
9/5/2024 2:00:00 PM	39.0	74	87	48	0.0	0.0%	34.7
9/5/2024 3:00:00 PM	38.0	90	106	47	0.0	0.0%	33.0
9/5/2024 4:00:00 PM	42.0	76	89	48	0.0	0.0%	34.9
9/5/2024 5:00:00 PM	39.0	75	88	47	0.0	0.0%	34.2
9/5/2024 6:00:00 PM	39.0	66	78	47	0.0	0.0%	33.4
9/5/2024 7:00:00 PM	37.0	49	58	43	0.0	0.0%	32.6
9/5/2024 8:00:00 PM	39.0	44	52	48	0.0	0.0%	35.2
9/5/2024 9:00:00 PM	38.0	32	38	45	0.0	0.0%	34.1
9/5/2024 10:00:00 PM	38.0	19	22	51	0.0	0.0%	34.5
9/5/2024 11:00:00 PM	44.0	8	9	45	0.0	0.0%	36.6
9/6/2024 12:00:00 AM	36.0	4	5	37	0.0	0.0%	34.0
9/6/2024 1:00:00 AM	35.0	3	3	35	0.0	0.0%	32.0
9/6/2024 2:00:00 AM	35.0	2	2	35	0.0	0.0%	31.5
9/6/2024 3:00:00 AM	**No Data**						
9/6/2024 4:00:00 AM	40.0	6	7	41	0.0	0.0%	31.4
9/6/2024 5:00:00 AM	38.0	3	3	38	0.0	0.0%	32.7
9/6/2024 6:00:00 AM	39.0	15	18	45	0.0	0.0%	34.7
9/6/2024 7:00:00 AM	42.0	31	36	49	0.0	0.0%	36.0
9/6/2024 8:00:00 AM	38.0	67	79	48	0.0	0.0%	33.9
9/6/2024 9:00:00 AM	38.0	52	61	45	0.0	0.0%	33.9
9/6/2024 10:00:00 AM	37.0	75	88	44	0.0	0.0%	31.9
9/6/2024 11:00:00 AM	40.0	56	66	45	0.0	0.0%	34.6
9/6/2024 12:00:00 PM	40.0	77	91	54	0.0	0.0%	34.5
9/6/2024 1:00:00 PM	39.0	74	87	46	0.0	0.0%	33.5
9/6/2024 2:00:00 PM	41.0	67	79	53	0.0	0.0%	35.0
9/6/2024 3:00:00 PM	39.0	70	82	48	0.0	0.0%	34.1
9/6/2024 4:00:00 PM	40.0	66	78	49	0.0	0.0%	34.6

Incoming Summary
Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

9/6/2024 5:00:00 PM	38.0	107	126	47	0.0	0.0%	33.8
9/6/2024 6:00:00 PM	40.0	84	99	48	0.0	0.0%	34.5
9/6/2024 7:00:00 PM	40.0	71	84	46	0.0	0.0%	35.2
9/6/2024 8:00:00 PM	40.0	46	54	47	0.0	0.0%	36.1
9/6/2024 9:00:00 PM	36.0	41	48	41	0.0	0.0%	32.4
9/6/2024 10:00:00 PM	35.0	10	12	37	0.0	0.0%	32.1
9/6/2024 11:00:00 PM	28.0	5	6	31	0.0	0.0%	26.8
9/7/2024 12:00:00 AM	38.0	3	3	38	0.0	0.0%	35.0
9/7/2024 1:00:00 AM	36.0	3	3	36	0.0	0.0%	31.0
9/7/2024 2:00:00 AM	26.0	1	1	26	0.0	0.0%	26.0
9/7/2024 3:00:00 AM	**No Data**						
9/7/2024 4:00:00 AM	42.0	2	2	42	0.0	0.0%	35.0
9/7/2024 5:00:00 AM	26.0	2	2	26	0.0	0.0%	25.5
9/7/2024 6:00:00 AM	40.0	4	5	42	0.0	0.0%	37.4
9/7/2024 7:00:00 AM	42.0	9	11	44	0.0	0.0%	37.5
9/7/2024 8:00:00 AM	40.0	20	24	46	0.0	0.0%	36.2
9/7/2024 9:00:00 AM	38.0	35	41	44	0.0	0.0%	33.9
9/7/2024 10:00:00 AM	37.0	41	48	41	0.0	0.0%	32.4
9/7/2024 11:00:00 AM	38.0	59	69	42	0.0	0.0%	33.2
9/7/2024 12:00:00 PM	39.0	67	79	44	0.0	0.0%	34.4
9/7/2024 1:00:00 PM	38.0	81	95	44	0.0	0.0%	32.1
9/7/2024 2:00:00 PM	40.0	70	82	49	0.0	0.0%	34.7
9/7/2024 3:00:00 PM	39.0	66	78	47	0.0	0.0%	34.2
9/7/2024 4:00:00 PM	38.0	76	90	46	0.0	0.0%	34.3
9/7/2024 5:00:00 PM	41.0	80	94	52	0.0	0.0%	35.0
9/7/2024 6:00:00 PM	39.0	79	93	45	0.0	0.0%	34.1
9/7/2024 7:00:00 PM	41.0	60	71	48	0.0	0.0%	34.7
9/7/2024 8:00:00 PM	39.0	43	51	45	0.0	0.0%	34.4
9/7/2024 9:00:00 PM	39.0	39	46	49	0.0	0.0%	34.8
9/7/2024 10:00:00 PM	40.0	23	27	48	0.0	0.0%	33.6
9/7/2024 11:00:00 PM	40.0	19	22	51	0.0	0.0%	35.2
9/8/2024 12:00:00 AM	37.0	17	20	50	0.0	0.0%	33.8
9/8/2024 1:00:00 AM	35.0	2	2	35	0.0	0.0%	34.0
9/8/2024 2:00:00 AM	29.0	4	5	36	0.0	0.0%	27.8
9/8/2024 3:00:00 AM	**No Data**						
9/8/2024 4:00:00 AM	40.0	3	3	40	0.0	0.0%	31.7
9/8/2024 5:00:00 AM	30.0	2	2	30	0.0	0.0%	30.0
9/8/2024 6:00:00 AM	40.0	1	1	40	0.0	0.0%	40.0
9/8/2024 7:00:00 AM	35.0	3	3	35	0.0	0.0%	32.0
9/8/2024 8:00:00 AM	35.0	3	4	38	0.0	0.0%	32.0
9/8/2024 9:00:00 AM	33.0	12	14	40	0.0	0.0%	31.1
9/8/2024 10:00:00 AM	36.0	36	42	44	0.0	0.0%	32.1
9/8/2024 11:00:00 AM	36.0	23	27	43	0.0	0.0%	31.9
9/8/2024 12:00:00 PM	36.0	25	29	41	0.0	0.0%	31.5
9/8/2024 1:00:00 PM	35.0	20	23	41	0.0	0.0%	29.7
9/8/2024 2:00:00 PM	34.0	22	26	39	0.0	0.0%	29.2
9/8/2024 3:00:00 PM	36.0	37	43	44	0.0	0.0%	29.1
9/8/2024 4:00:00 PM	38.0	39	46	53	0.0	0.0%	33.0

Incoming Summary
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

9/8/2024 5:00:00 PM	39.0	64	75	47	0.0	0.0%	34.4
9/8/2024 6:00:00 PM	38.0	60	70	48	0.0	0.0%	33.3
9/8/2024 7:00:00 PM	39.0	46	54	45	0.0	0.0%	35.7
9/8/2024 8:00:00 PM	38.0	33	39	41	0.0	0.0%	33.1
9/8/2024 9:00:00 PM	38.0	37	44	46	0.0	0.0%	33.2
9/8/2024 10:00:00 PM	39.0	10	12	45	0.0	0.0%	35.3
9/8/2024 11:00:00 PM	35.0	6	7	43	0.0	0.0%	33.0
9/9/2024 12:00:00 AM	42.0	6	7	46	0.0	0.0%	35.7
9/9/2024 1:00:00 AM	41.0	1	1	41	0.0	0.0%	41.0
9/9/2024 2:00:00 AM	47.0	2	2	47	0.0	0.0%	43.5
9/9/2024 3:00:00 AM	47.0	1	1	47	0.0	0.0%	47.0
9/9/2024 4:00:00 AM	36.0	3	4	40	0.0	0.0%	33.0
9/9/2024 5:00:00 AM	34.0	3	4	42	0.0	0.0%	34.8
9/9/2024 6:00:00 AM	39.0	12	14	43	0.0	0.0%	33.8
9/9/2024 7:00:00 AM	39.0	34	40	48	0.0	0.0%	34.8
9/9/2024 8:00:00 AM	38.0	52	61	45	0.0	0.0%	32.3
9/9/2024 9:00:00 AM	38.0	29	34	43	0.0	0.0%	33.6
9/9/2024 10:00:00 AM	36.0	20	23	43	0.0	0.0%	33.0
9/9/2024 11:00:00 AM	36.0	43	51	39	0.0	0.0%	29.8
9/9/2024 12:00:00 PM	36.0	24	28	40	0.0	0.0%	30.8
9/9/2024 1:00:00 PM	38.0	18	21	40	0.0	0.0%	31.0
9/9/2024 2:00:00 PM	37.0	26	30	40	0.0	0.0%	32.1
9/9/2024 3:00:00 PM	36.0	58	68	48	0.0	0.0%	32.2
9/9/2024 4:00:00 PM	38.0	48	56	44	0.0	0.0%	31.6
9/9/2024 5:00:00 PM	38.0	71	83	46	0.0	0.0%	32.7
9/9/2024 6:00:00 PM	39.0	58	68	44	0.0	0.0%	33.8
9/9/2024 7:00:00 PM	39.0	55	65	49	0.0	0.0%	35.4
9/9/2024 8:00:00 PM	38.0	31	36	45	0.0	0.0%	35.0
9/9/2024 9:00:00 PM	37.0	25	29	41	0.0	0.0%	32.3
9/9/2024 10:00:00 PM	36.0	14	17	37	0.0	0.0%	30.5
9/9/2024 11:00:00 PM	41.0	11	13	46	0.0	0.0%	32.9
9/10/2024 12:00:00 AM	36.0	6	7	38	0.0	0.0%	33.0
9/10/2024 1:00:00 AM	23.0	1	1	23	0.0	0.0%	23.0
9/10/2024 2:00:00 AM	39.0	3	3	39	0.0	0.0%	37.3
9/10/2024 3:00:00 AM	39.0	5	6	45	0.0	0.0%	36.3
9/10/2024 4:00:00 AM	34.0	3	4	44	0.0	0.0%	32.5
9/10/2024 5:00:00 AM	39.0	6	7	45	0.0	0.0%	36.0
9/10/2024 6:00:00 AM	42.0	15	18	52	0.0	0.0%	35.4
9/10/2024 7:00:00 AM	42.0	38	45	47	0.0	0.0%	36.2
9/10/2024 8:00:00 AM	39.0	75	88	48	0.0	0.0%	34.1
9/10/2024 9:00:00 AM	39.0	54	63	43	0.0	0.0%	32.8
9/10/2024 10:00:00 AM	39.0	88	103	46	0.0	0.0%	33.3
9/10/2024 11:00:00 AM	38.0	51	60	44	0.0	0.0%	32.8
9/10/2024 12:00:00 PM	40.0	42	49	46	0.0	0.0%	32.4
9/10/2024 1:00:00 PM	38.0	28	33	41	0.0	0.0%	31.4
9/10/2024 2:00:00 PM	38.0	42	49	50	0.0	0.0%	32.4
9/10/2024 3:00:00 PM	39.0	6	7	42	0.0	0.0%	28.7

Incoming Summary
Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/5/2024 12:00:00 AM	39.0	262	308	52	0.0	0.0%	34.2
9/6/2024 12:00:00 AM	40.0	1057	1243	51	0.0	0.0%	34.3
9/7/2024 12:00:00 AM	40.0	1030	1212	54	0.0	0.0%	34.1
9/8/2024 12:00:00 AM	39.0	896	1054	52	0.0	0.0%	34.1
9/9/2024 12:00:00 AM	38.0	491	578	53	0.0	0.0%	32.7
9/10/2024 12:00:00 AM	38.0	643	756	49	0.0	0.0%	32.8
9/10/2024 2:59:59 PM	39.0	456	536	52	0.0	0.0%	33.4

Incoming Weekly Counts

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	2	3	3	2	2.5	2.5	34.5
1 - 2	*	*	*	3	2	1	5	2.5	3	39
2 - 3	*	*	*	1	0	0	0	0.5	0	44
3 - 4	*	*	*	4	7	2	3	5.5	2.5	37
4 - 5	*	*	*	4	3	2	2	3.5	2	37
5 - 6	*	*	*	15	18	5	1	16.5	3	39.75
6 - 7	*	*	*	58	36	11	3	47	7	40.55
7 - 8	*	*	*	97	79	24	4	88	14	38
8 - 9	*	*	*	82	61	41	14	71.5	27.5	38.45
9 - 10	*	*	*	95	88	48	42	91.5	45	37.6
10 - 11	*	*	*	79	66	69	27	72.5	48	39.6
11 - 12	*	*	*	86	91	79	29	88.5	54	40
12 - 13	*	*	*	85	87	95	23	86	59	39.25
13 - 14	*	*	*	87	79	82	26	83	54	39.8
14 - 15	*	*	*	106	82	78	43	94	60.5	38.45
15 - 16	*	*	*	89	78	90	46	83.5	68	40.45
16 - 17	*	*	*	88	126	94	75	107	84.5	38.1
17 - 18	*	*	71	78	99	93	70	82.67	81.5	38.87
18 - 19	*	*	104	58	84	71	54	82	62.5	38.07
19 - 20	*	*	47	52	54	51	39	51	45	40
20 - 21	*	*	47	38	48	46	44	44.33	45	37
21 - 22	*	*	20	22	12	27	12	18	19.5	37.27
22 - 23	*	*	16	9	6	22	7	10.33	14.5	37
23 - 24	*	*	3	5	3	20	7	3.67	13.5	37.67
Totals	0	0	308	1243	1212	1054	578			
% of Total	0%	0%	7.01%	28.28%	27.58%	23.98%	13.15%			

Incoming Weekly Counts

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	1	1	*	*	*	*	*	1	0	32
1 - 2	2	3	*	*	*	*	*	2.5	0	43
2 - 3	1	6	*	*	*	*	*	3.5	0	43
3 - 4	4	4	*	*	*	*	*	4	0	35
4 - 5	4	7	*	*	*	*	*	5.5	0	36.5
5 - 6	14	18	*	*	*	*	*	16	0	40.15
6 - 7	40	45	*	*	*	*	*	42.5	0	39.9
7 - 8	61	88	*	*	*	*	*	74.5	0	38.1
8 - 9	34	63	*	*	*	*	*	48.5	0	37.95
9 - 10	23	103	*	*	*	*	*	63	0	37.1
10 - 11	51	60	*	*	*	*	*	55.5	0	36.65
11 - 12	28	49	*	*	*	*	*	38.5	0	37.4
12 - 13	21	33	*	*	*	*	*	27	0	37.5
13 - 14	30	49	*	*	*	*	*	39.5	0	36.9
14 - 15	68	7	*	*	*	*	*	37.5	0	37.15
15 - 16	56	*	*	*	*	*	*	56	0	38
16 - 17	83	*	*	*	*	*	*	83	0	37.5
17 - 18	68	*	*	*	*	*	*	68	0	38.8
18 - 19	65	*	*	*	*	*	*	65	0	39
19 - 20	36	*	*	*	*	*	*	36	0	38
20 - 21	29	*	*	*	*	*	*	29	0	37
21 - 22	17	*	*	*	*	*	*	17	0	35.7
22 - 23	13	*	*	*	*	*	*	13	0	41
23 - 24	7	*	*	*	*	*	*	7	0	36
Totals	756	536	0	0	0	0	0			
% of Total	58.51%	41.49%	0%	0%	0%	0%	0%			

Incoming Monthly Counts

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	1	1	*	2	3	3	2	1.75	2.5	33.25
1 - 2	2	3	*	3	2	1	5	2.5	3	41
2 - 3	1	6	*	1	0	0	0	2	0	43.33
3 - 4	4	4	*	4	7	2	3	4.75	2.5	36
4 - 5	4	7	*	4	3	2	2	4.5	2	36.75
5 - 6	14	18	*	15	18	5	1	16.25	3	39.95
6 - 7	40	45	*	58	36	11	3	44.75	7	40.22
7 - 8	61	88	*	97	79	24	4	81.25	14	38.05
8 - 9	34	63	*	82	61	41	14	60	27.5	38.2
9 - 10	23	103	*	95	88	48	42	77.25	45	37.35
10 - 11	51	60	*	79	66	69	27	64	48	38.12
11 - 12	28	49	*	86	91	79	29	63.5	54	38.7
12 - 13	21	33	*	85	87	95	23	56.5	59	38.38
13 - 14	30	49	*	87	79	82	26	61.25	54	38.35
14 - 15	68	7	*	106	82	78	43	65.75	60.5	37.8
15 - 16	56	*	*	89	78	90	46	74.33	68	39.63
16 - 17	83	*	*	88	126	94	75	99	84.5	37.9
17 - 18	68	*	71	78	99	93	70	79	81.5	38.85
18 - 19	65	*	104	58	84	71	54	77.75	62.5	38.3
19 - 20	36	*	47	52	54	51	39	47.25	45	39.5
20 - 21	29	*	47	38	48	46	44	40.5	45	37
21 - 22	17	*	20	22	12	27	12	17.75	19.5	36.88
22 - 23	13	*	16	9	6	22	7	11	14.5	38
23 - 24	7	*	3	5	3	20	7	4.5	13.5	37.25
Totals	756	536	308	1243	1212	1054	578			
% of Total	13.29%	9.43%	5.42%	21.86%	21.31%	18.53%	10.16%			

Incoming Weekly Speeds

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	30.5	32	31	34	31.4	32.2	34.5
1 - 2	*	*	*	39.33	31.5	26	27.8	36.2	27.5	39
2 - 3	*	*	*	44	0	0	0	44	0	44
3 - 4	*	*	*	34.5	31.43	35	31.67	32.55	33	37
4 - 5	*	*	*	34.5	32.67	25.5	30	33.71	27.75	37
5 - 6	*	*	*	36.13	34.72	37.4	40	35.36	37.83	39.75
6 - 7	*	*	*	35.14	36	37.55	32	35.47	36.36	40.55
7 - 8	*	*	*	34.13	33.9	36.17	32	34.03	35.57	38
8 - 9	*	*	*	34.65	33.93	33.9	31.07	34.34	33.18	38.45
9 - 10	*	*	*	33.81	31.92	32.4	32.1	32.9	32.26	37.6
10 - 11	*	*	*	34.77	34.62	33.19	31.93	34.7	32.83	39.6
11 - 12	*	*	*	34.42	34.48	34.35	31.52	34.45	33.59	40
12 - 13	*	*	*	34.33	33.47	32.11	29.7	33.9	31.64	39.25
13 - 14	*	*	*	34.7	34.99	34.68	29.15	34.84	33.35	39.8
14 - 15	*	*	*	33.03	34.07	34.23	29.07	33.48	32.4	38.45
15 - 16	*	*	*	34.91	34.59	34.3	33.04	34.76	33.88	40.45
16 - 17	*	*	*	34.16	33.85	35.04	34.4	33.98	34.76	38.1
17 - 18	*	*	34.97	33.45	34.47	34.13	33.33	34.29	33.79	38.87
18 - 19	*	*	34.26	32.57	35.2	34.73	35.74	34.18	35.17	38.07
19 - 20	*	*	34.83	35.15	36.11	34.41	33.1	35.39	33.84	40
20 - 21	*	*	32.53	34.13	32.38	34.76	33.16	32.93	33.98	37
21 - 22	*	*	33.3	34.45	32.08	33.56	35.33	33.5	34.1	37.27
22 - 23	*	*	34.88	36.56	26.83	35.23	33	33.81	34.69	37
23 - 24	*	*	34	34	35	33.75	35.71	34.27	34.26	37.67
Totals	0	0	34.2	34.2	34.1	34.1	32.7			
% of Total	0%	0%	20.2%	20.2%	20.14%	20.14%	19.31%			

Incoming Weekly Speeds

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	41	23	*	*	*	*	*	32	*	32
1 - 2	43.5	37.33	*	*	*	*	*	39.8	*	43
2 - 3	47	36.33	*	*	*	*	*	37.86	*	43
3 - 4	33	32.5	*	*	*	*	*	32.75	*	35
4 - 5	34.75	36	*	*	*	*	*	35.55	*	36.5
5 - 6	33.79	35.39	*	*	*	*	*	34.69	*	40.15
6 - 7	34.8	36.24	*	*	*	*	*	35.56	*	39.9
7 - 8	32.33	34.14	*	*	*	*	*	33.4	*	38.1
8 - 9	33.65	32.78	*	*	*	*	*	33.08	*	37.95
9 - 10	33.04	33.33	*	*	*	*	*	33.28	*	37.1
10 - 11	29.78	32.8	*	*	*	*	*	31.41	*	36.65
11 - 12	30.79	32.37	*	*	*	*	*	31.79	*	37.4
12 - 13	30.95	31.36	*	*	*	*	*	31.2	*	37.5
13 - 14	32.1	32.45	*	*	*	*	*	32.32	*	36.9
14 - 15	32.24	28.71	*	*	*	*	*	31.91	*	37.15
15 - 16	31.55	*	*	*	*	*	*	31.55	*	38
16 - 17	32.65	*	*	*	*	*	*	32.65	*	37.5
17 - 18	33.79	*	*	*	*	*	*	33.79	*	38.8
18 - 19	35.43	*	*	*	*	*	*	35.43	*	39
19 - 20	34.97	*	*	*	*	*	*	34.97	*	38
20 - 21	32.34	*	*	*	*	*	*	32.34	*	37
21 - 22	30.47	*	*	*	*	*	*	30.47	*	35.7
22 - 23	32.92	*	*	*	*	*	*	32.92	*	41
23 - 24	33	*	*	*	*	*	*	33	*	36
Totals	32.8	33.4	0	0	0	0	0			
% of Total	49.55%	50.45%	0%	0%	0%	0%	0%			

Incoming Monthly Speeds

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	41	23	*	30.5	32	31	34	31.57	32.2	33.25
1 - 2	43.5	37.33	*	39.33	31.5	26	27.8	38	27.5	41
2 - 3	47	36.33	*	44	0	0	0	38.62	0	43.33
3 - 4	33	32.5	*	34.5	31.43	35	31.67	32.63	33	36
4 - 5	34.75	36	*	34.5	32.67	25.5	30	34.83	27.75	36.75
5 - 6	33.79	35.39	*	36.13	34.72	37.4	40	35.03	37.83	39.95
6 - 7	34.8	36.24	*	35.14	36	37.55	32	35.51	36.36	40.22
7 - 8	32.33	34.14	*	34.13	33.9	36.17	32	33.74	35.57	38.05
8 - 9	33.65	32.78	*	34.65	33.93	33.9	31.07	33.83	33.18	38.2
9 - 10	33.04	33.33	*	33.81	31.92	32.4	32.1	33.06	32.26	37.35
10 - 11	29.78	32.8	*	34.77	34.62	33.19	31.93	33.28	32.83	38.12
11 - 12	30.79	32.37	*	34.42	34.48	34.35	31.52	33.65	33.59	38.7
12 - 13	30.95	31.36	*	34.33	33.47	32.11	29.7	33.25	31.64	38.38
13 - 14	32.1	32.45	*	34.7	34.99	34.68	29.15	34.02	33.35	38.35
14 - 15	32.24	28.71	*	33.03	34.07	34.23	29.07	33.03	32.4	37.8
15 - 16	31.55	*	*	34.91	34.59	34.3	33.04	33.96	33.88	39.63
16 - 17	32.65	*	*	34.16	33.85	35.04	34.4	33.61	34.76	37.9
17 - 18	33.79	*	34.97	33.45	34.47	34.13	33.33	34.19	33.79	38.85
18 - 19	35.43	*	34.26	32.57	35.2	34.73	35.74	34.44	35.17	38.3
19 - 20	34.97	*	34.83	35.15	36.11	34.41	33.1	35.31	33.84	39.5
20 - 21	32.34	*	32.53	34.13	32.38	34.76	33.16	32.83	33.98	37
21 - 22	30.47	*	33.3	34.45	32.08	33.56	35.33	32.77	34.1	36.88
22 - 23	32.92	*	34.88	36.56	26.83	35.23	33	33.55	34.69	38
23 - 24	33	*	34	34	35	33.75	35.71	33.78	34.26	37.25
Totals	32.8	33.4	34.2	34.2	34.1	34.1	32.7			
% of Total	13.93%	14.18%	14.52%	14.52%	14.48%	14.48%	13.89%			

Incoming Weekly EightyFifthSpeeds

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	34	35	36	35	34.5	35.5	34.5
1 - 2	*	*	*	43	35	26	29	39	27.5	39
2 - 3	*	*	*	44	0	0	0	44	0	44
3 - 4	*	*	*	34	40	42	40	37	41	37
4 - 5	*	*	*	36	38	26	30	37	28	37
5 - 6	*	*	*	40.5	39	40	40	39.75	40	39.75
6 - 7	*	*	*	39.8	41.3	42	35	40.55	38.5	40.55
7 - 8	*	*	*	38.2	37.8	39.5	35	38	37.25	38
8 - 9	*	*	*	38.9	38	37.8	33	38.45	35.4	38.45
9 - 10	*	*	*	38.9	36.3	36.8	36	37.6	36.4	37.6
10 - 11	*	*	*	39.7	39.5	37.4	36	39.6	36.7	39.6
11 - 12	*	*	*	40	40	38.7	36	40	37.35	40
12 - 13	*	*	*	39.5	39	37.3	35	39.25	36.15	39.25
13 - 14	*	*	*	38.8	40.8	39.2	34	39.8	36.6	39.8
14 - 15	*	*	*	38	38.9	38.4	36	38.45	37.2	38.45
15 - 16	*	*	*	41.2	39.7	37.7	37.5	40.45	37.6	40.45
16 - 17	*	*	*	38.3	37.9	40.5	38.3	38.1	39.4	38.1
17 - 18	*	*	39	38.4	39.2	39	37.8	38.87	38.4	38.87
18 - 19	*	*	37.6	37	39.6	40.2	39	38.07	39.6	38.07
19 - 20	*	*	41	39	40	39	37.3	40	38.15	40
20 - 21	*	*	37	38	36	39	37.5	37	38.25	37
21 - 22	*	*	39.5	37.3	35	39.3	39	37.27	39.15	37.27
22 - 23	*	*	39	44	28	39.5	35	37	37.25	37
23 - 24	*	*	39	36	38	36.3	42	37.67	39.15	37.67
Totals	0	0	272.1	932.5	872	867.6	833.4			
% of Total	0%	0%	7.2%	24.68%	23.08%	22.97%	22.06%			

Incoming Weekly EightyFifthSpeeds
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	41	23	*	*	*	*	*	32	0	32
1 - 2	47	39	*	*	*	*	*	43	0	43
2 - 3	47	39	*	*	*	*	*	43	0	43
3 - 4	36	34	*	*	*	*	*	35	0	35
4 - 5	34	39	*	*	*	*	*	36.5	0	36.5
5 - 6	39	41.3	*	*	*	*	*	40.15	0	40.15
6 - 7	38.3	41.5	*	*	*	*	*	39.9	0	39.9
7 - 8	37.8	38.4	*	*	*	*	*	38.1	0	38.1
8 - 9	37.7	38.2	*	*	*	*	*	37.95	0	37.95
9 - 10	36	38.2	*	*	*	*	*	37.1	0	37.1
10 - 11	36	37.3	*	*	*	*	*	36.65	0	36.65
11 - 12	35.5	39.3	*	*	*	*	*	37.4	0	37.4
12 - 13	37.5	37.5	*	*	*	*	*	37.5	0	37.5
13 - 14	36.5	37.3	*	*	*	*	*	36.9	0	36.9
14 - 15	35.3	39	*	*	*	*	*	37.15	0	37.15
15 - 16	38	*	*	*	*	*	*	38	0	38
16 - 17	37.5	*	*	*	*	*	*	37.5	0	37.5
17 - 18	38.8	*	*	*	*	*	*	38.8	0	38.8
18 - 19	39	*	*	*	*	*	*	39	0	39
19 - 20	38	*	*	*	*	*	*	38	0	38
20 - 21	37	*	*	*	*	*	*	37	0	37
21 - 22	35.7	*	*	*	*	*	*	35.7	0	35.7
22 - 23	41	*	*	*	*	*	*	41	0	41
23 - 24	36	*	*	*	*	*	*	36	0	36
Totals	915.6	562	0	0	0	0	0			
% of Total	61.97%	38.03%	0%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	41	23	*	34	35	36	35	33.25	35.5	33.25
1 - 2	47	39	*	43	35	26	29	41	27.5	41
2 - 3	47	39	*	44	0	0	0	43.33	0	43.33
3 - 4	36	34	*	34	40	42	40	36	41	36
4 - 5	34	39	*	36	38	26	30	36.75	28	36.75
5 - 6	39	41.3	*	40.5	39	40	40	39.95	40	39.95
6 - 7	38.3	41.5	*	39.8	41.3	42	35	40.22	38.5	40.22
7 - 8	37.8	38.4	*	38.2	37.8	39.5	35	38.05	37.25	38.05
8 - 9	37.7	38.2	*	38.9	38	37.8	33	38.2	35.4	38.2
9 - 10	36	38.2	*	38.9	36.3	36.8	36	37.35	36.4	37.35
10 - 11	36	37.3	*	39.7	39.5	37.4	36	38.12	36.7	38.12
11 - 12	35.5	39.3	*	40	40	38.7	36	38.7	37.35	38.7
12 - 13	37.5	37.5	*	39.5	39	37.3	35	38.38	36.15	38.38
13 - 14	36.5	37.3	*	38.8	40.8	39.2	34	38.35	36.6	38.35
14 - 15	35.3	39	*	38	38.9	38.4	36	37.8	37.2	37.8
15 - 16	38	*	*	41.2	39.7	37.7	37.5	39.63	37.6	39.63
16 - 17	37.5	*	*	38.3	37.9	40.5	38.3	37.9	39.4	37.9
17 - 18	38.8	*	39	38.4	39.2	39	37.8	38.85	38.4	38.85
18 - 19	39	*	37.6	37	39.6	40.2	39	38.3	39.6	38.3
19 - 20	38	*	41	39	40	39	37.3	39.5	38.15	39.5
20 - 21	37	*	37	38	36	39	37.5	37	38.25	37
21 - 22	35.7	*	39.5	37.3	35	39.3	39	36.88	39.15	36.88
22 - 23	41	*	39	44	28	39.5	35	38	37.25	38
23 - 24	36	*	39	36	38	36.3	42	37.25	39.15	37.25

Summary of Violators

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	12	31.8	0	0.0
01:00:00	16	34.1	0	0.0
02:00:00	8	38.6	0	0.0
03:00:00	24	32.7	0	0.0
04:00:00	22	33.5	0	0.0
05:00:00	71	35.3	0	0.0
06:00:00	193	35.6	0	0.0
07:00:00	353	33.9	0	0.0
08:00:00	295	33.7	0	0.0
09:00:00	399	32.9	0	0.0
10:00:00	352	33.2	0	0.0
11:00:00	362	33.6	0	0.0
12:00:00	344	32.7	0	0.0
13:00:00	353	33.8	0	0.0
14:00:00	384	32.8	0	0.0
15:00:00	359	33.9	0	0.0
16:00:00	466	34.0	0	0.0
17:00:00	479	34.1	0	0.0
18:00:00	436	34.7	0	0.0
19:00:00	279	34.8	0	0.0
20:00:00	252	33.2	0	0.0
21:00:00	110	33.2	0	0.0
22:00:00	73	34.0	0	0.0
23:00:00	45	34.1	0	0.0

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/4/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/4/2024	17:00	0	0	1	5	28	26	11	0	0	0	0	0	0	0	0	0	0	0	71	35	39	30 to 40	83.1	*	*	0	70	1	
9/4/2024	18:00	2	0	1	10	38	42	6	4	1	0	0	0	0	0	0	0	0	0	104	34.3	37.6	28 to 38	79.8	*	*	1	97	6	
9/4/2024	19:00	0	0	4	3	19	11	7	3	0	0	0	0	0	0	0	0	0	0	47	34.8	41	33 to 43	70.2	*	*	0	46	1	
9/4/2024	20:00	0	2	2	10	16	12	4	1	0	0	0	0	0	0	0	0	0	0	47	32.5	37	26 to 36	72.3	*	*	0	46	1	
9/4/2024	21:00	0	0	1	3	10	2	3	1	0	0	0	0	0	0	0	0	0	0	20	33.3	39.5	27 to 37	75.0	*	*	0	20	0	
9/4/2024	22:00	0	0	0	1	7	6	1	1	0	0	0	0	0	0	0	0	0	0	16	34.9	39	29 to 39	81.3	*	*	0	16	0	
9/4/2024	23:00	0	0	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	0	3	34	39	25 to 35	66.7	*	*	0	3	0	
24 Hr Summary		2	2	9	33	118	101	32	10	1	0	0	0	0	0	0	0	0	0	308	34.2	39	30 to 40	75.3	*	*	1	298	9	

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/5/2024	00:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30.5	34	24 to 34	100.0	*	*	0	2	0
9/5/2024	01:00	0	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3	39.3	43	33 to 43	100.0	*	*	0	3	0
9/5/2024	02:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	44	44	34 to 44	100.0	*	*	0	1	0
9/5/2024	03:00	0	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	0	0	4	34.5	34	31 to 41	100.0	*	*	0	4	0
9/5/2024	04:00	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	4	34.5	36	26 to 36	75.0	*	*	0	4	0
9/5/2024	05:00	0	0	0	2	2	7	4	0	0	0	0	0	0	0	0	0	0	0	15	36.1	40.5	32 to 42	80.0	*	*	0	14	1
9/5/2024	06:00	0	1	1	4	18	22	10	2	0	0	0	0	0	0	0	0	0	0	58	35.1	39.8	30 to 40	75.9	*	*	0	52	6
9/5/2024	07:00	0	0	4	17	22	44	9	1	0	0	0	0	0	0	0	0	0	0	97	34.1	38.2	29 to 39	76.3	*	*	0	81	16
9/5/2024	08:00	1	0	0	11	28	31	8	2	1	0	0	0	0	0	0	0	0	0	82	34.6	38.9	29 to 39	74.4	*	*	0	69	13
9/5/2024	09:00	0	3	2	16	27	34	10	3	0	0	0	0	0	0	0	0	0	0	95	33.8	38.9	30 to 40	70.5	*	*	0	80	15
9/5/2024	10:00	0	2	4	6	21	32	12	2	0	0	0	0	0	0	0	0	0	0	79	34.8	39.7	31 to 41	74.7	*	*	0	64	15
9/5/2024	11:00	0	0	4	10	30	27	13	2	0	0	0	0	0	0	0	0	0	0	86	34.4	40	29 to 39	69.8	*	*	0	72	14
9/5/2024	12:00	2	0	4	10	26	29	6	8	0	0	0	0	0	0	0	0	0	0	85	34.3	39.5	29 to 39	71.8	*	*	0	68	17
9/5/2024	13:00	0	1	2	8	29	35	8	4	0	0	0	0	0	0	0	0	0	0	87	34.7	38.8	29 to 39	75.9	*	*	0	68	19
9/5/2024	14:00	1	2	5	15	41	28	12	2	0	0	0	0	0	0	0	0	0	0	106	33	38	31 to 41	69.8	*	*	1	87	18
9/5/2024	15:00	1	0	4	13	22	27	18	4	0	0	0	0	0	0	0	0	0	0	89	34.9	41.2	32 to 42	66.3	*	*	1	68	20
9/5/2024	16:00	0	1	3	7	32	34	9	2	0	0	0	0	0	0	0	0	0	0	88	34.2	38.3	29 to 39	80.7	*	*	0	80	8
9/5/2024	17:00	0	0	5	10	29	25	8	1	0	0	0	0	0	0	0	0	0	0	78	33.4	38.4	27 to 37	73.1	*	*	0	74	4
9/5/2024	18:00	0	1	0	13	24	15	5	0	0	0	0	0	0	0	0	0	0	0	58	32.6	37	26 to 36	79.3	*	*	0	56	2
9/5/2024	19:00	0	1	0	2	23	18	7	1	0	0	0	0	0	0	0	0	0	0	52	35.2	39	30 to 40	82.7	*	*	0	51	1
9/5/2024	20:00	1	0	1	5	11	15	4	1	0	0	0	0	0	0	0	0	0	0	38	34.1	38	29 to 39	76.3	*	*	0	38	0
9/5/2024	21:00	0	0	0	2	9	10	0	0	1	0	0	0	0	0	0	0	0	0	22	34.5	37.3	28 to 38	90.9	*	*	0	20	2
9/5/2024	22:00	0	0	0	2	1	3	2	1	0	0	0	0	0	0	0	0	0	0	9	36.6	44	26 to 36	66.7	*	*	0	8	1
9/5/2024	23:00	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	5	34	36	27 to 37	100.0	*	*	0	5	0
24 Hr Summary		6	12	39	155	402	441	150	36	2	0	0	0	0	0	0	0	0	0	1243	34.3	40	29 to 39	71.8	*	*	2	1069	172

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/6/2024	00:00	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	3	32	35	25 to 35	100.0	*	*	0	3	0	
9/6/2024	01:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	31.5	35	25 to 35	100.0	*	*	0	1	1	
9/6/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	03:00	0	0	1	2	2	0	2	0	0	0	0	0	0	0	0	0	0	0	7	31.4	40	21 to 31	71.4	*	*	0	7	0	
9/6/2024	04:00	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	32.7	38	28 to 38	100.0	*	*	0	3	0	
9/6/2024	05:00	0	0	0	0	11	4	2	1	0	0	0	0	0	0	0	0	0	0	18	34.7	39	30 to 40	88.9	*	*	0	18	0	
9/6/2024	06:00	0	0	1	2	10	14	8	1	0	0	0	0	0	0	0	0	0	0	36	36	41.3	30 to 40	72.2	*	*	0	34	2	
9/6/2024	07:00	0	0	3	12	29	26	7	2	0	0	0	0	0	0	0	0	0	0	79	33.9	37.8	29 to 39	75.9	*	*	0	75	4	
9/6/2024	08:00	0	0	0	13	23	17	4	4	0	0	0	0	0	0	0	0	0	0	61	33.9	38	27 to 37	77.0	*	*	0	49	12	
9/6/2024	09:00	1	0	6	21	30	22	8	0	0	0	0	0	0	0	0	0	0	0	88	31.9	36.3	26 to 36	75.0	*	*	1	66	21	
9/6/2024	10:00	0	0	2	6	27	19	11	1	0	0	0	0	0	0	0	0	0	0	66	34.6	39.5	30 to 40	75.8	*	*	1	58	7	
9/6/2024	11:00	1	3	2	7	32	28	12	5	1	0	0	0	0	0	0	0	0	0	91	34.5	40	30 to 40	70.3	*	*	1	74	16	
9/6/2024	12:00	0	2	3	14	32	23	11	2	0	0	0	0	0	0	0	0	0	0	87	33.5	39	29 to 39	71.3	*	*	0	80	7	
9/6/2024	13:00	0	1	2	7	28	25	13	1	2	0	0	0	0	0	0	0	0	0	79	35	40.8	28 to 38	70.9	*	*	0	75	4	
9/6/2024	14:00	1	1	4	9	28	28	9	2	0	0	0	0	0	0	0	0	0	0	82	34.1	38.9	29 to 39	74.4	*	*	0	73	9	
9/6/2024	15:00	3	0	2	8	24	25	12	4	0	0	0	0	0	0	0	0	0	0	78	34.6	39.7	30 to 40	70.5	*	*	1	71	6	
9/6/2024	16:00	1	2	2	17	40	49	13	2	0	0	0	0	0	0	0	0	0	0	126	33.8	37.9	28 to 38	77.0	*	*	0	113	13	
9/6/2024	17:00	0	1	5	11	33	33	12	4	0	0	0	0	0	0	0	0	0	0	99	34.5	39.2	29 to 39	72.7	*	*	0	90	9	
9/6/2024	18:00	2	0	0	6	27	32	14	3	0	0	0	0	0	0	0	0	0	0	84	35.2	39.6	31 to 41	79.8	*	*	0	83	1	
9/6/2024	19:00	0	1	0	1	19	21	10	2	0	0	0	0	0	0	0	0	0	0	54	36.1	40	30 to 40	81.5	*	*	0	52	2	
9/6/2024	20:00	1	0	0	8	24	12	3	0	0	0	0	0	0	0	0	0	0	0	48	32.4	36	26 to 36	81.3	*	*	0	47	1	
9/6/2024	21:00	0	0	1	1	6	4	0	0	0	0	0	0	0	0	0	0	0	0	12	32.1	35	27 to 37	91.7	*	*	0	12	0	
9/6/2024	22:00	0	0	1	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	6	26.8	28	21 to 31	100.0	*	*	0	6	0	
9/6/2024	23:00	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3	35	38	28 to 38	100.0	*	*	0	3	0	
24 Hr Summary		10	11	35	151	430	387	151	34	3	0	0	0	0	0	0	0	0	0	1212	34.1	40	30 to 40	71.9	*	*	4	1093	115	

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/7/2024	00:00	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	31	36	26 to 36	100.0	*	*	0	3	0	
9/7/2024	01:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	26	26	16 to 26	100.0	*	*	0	1	0	
9/7/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/7/2024	03:00	0	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	2	35	42	18 to 28	50.0	*	*	0	2	0	
9/7/2024	04:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25.5	26	16 to 26	100.0	*	*	0	2	0	
9/7/2024	05:00	0	0	0	0	1	2	2	0	0	0	0	0	0	0	0	0	0	0	5	37.4	40	30 to 40	80.0	*	*	0	5	0	
9/7/2024	06:00	0	0	0	0	4	3	4	0	0	0	0	0	0	0	0	0	0	0	11	37.5	42	32 to 42	81.8	*	*	0	7	4	
9/7/2024	07:00	0	0	0	0	11	8	4	1	0	0	0	0	0	0	0	0	0	0	24	36.2	39.5	30 to 40	87.5	*	*	0	22	2	
9/7/2024	08:00	0	0	2	4	15	15	5	0	0	0	0	0	0	0	0	0	0	0	41	33.9	37.8	30 to 40	82.9	*	*	0	40	1	
9/7/2024	09:00	0	2	2	10	14	18	2	0	0	0	0	0	0	0	0	0	0	0	48	32.4	36.8	28 to 38	79.2	*	*	0	47	1	
9/7/2024	10:00	0	0	6	8	22	29	4	0	0	0	0	0	0	0	0	0	0	0	69	33.2	37.4	28 to 38	76.8	*	*	0	67	2	
9/7/2024	11:00	1	1	3	9	21	34	10	0	0	0	0	0	0	0	0	0	0	0	79	34.4	38.7	29 to 39	73.4	*	*	0	75	4	
9/7/2024	12:00	2	3	6	15	34	28	7	0	0	0	0	0	0	0	0	0	0	0	95	32.1	37.3	28 to 38	72.6	*	*	0	88	7	
9/7/2024	13:00	1	1	1	5	30	31	11	2	0	0	0	0	0	0	0	0	0	0	82	34.7	39.2	30 to 40	79.3	*	*	1	78	3	
9/7/2024	14:00	0	2	0	9	31	27	7	2	0	0	0	0	0	0	0	0	0	0	78	34.2	38.4	30 to 40	76.9	*	*	0	76	2	
9/7/2024	15:00	0	1	2	9	34	32	11	1	0	0	0	0	0	0	0	0	0	0	90	34.3	37.7	28 to 38	78.9	*	*	0	86	4	
9/7/2024	16:00	1	2	1	12	26	35	11	4	2	0	0	0	0	0	0	0	0	0	94	35	40.5	29 to 39	72.3	*	*	0	92	2	
9/7/2024	17:00	0	0	6	11	26	36	12	2	0	0	0	0	0	0	0	0	0	0	93	34.1	39	30 to 40	75.3	*	*	0	88	5	
9/7/2024	18:00	0	0	4	4	24	25	12	2	0	0	0	0	0	0	0	0	0	0	71	34.7	40.2	31 to 41	74.6	*	*	0	67	4	
9/7/2024	19:00	0	0	1	7	18	17	7	1	0	0	0	0	0	0	0	0	0	0	51	34.4	39	29 to 39	74.5	*	*	0	51	0	
9/7/2024	20:00	0	0	3	4	12	20	6	1	0	0	0	0	0	0	0	0	0	0	46	34.8	39	29 to 39	73.9	*	*	0	46	0	
9/7/2024	21:00	0	1	2	3	7	9	3	2	0	0	0	0	0	0	0	0	0	0	27	33.6	39.3	30 to 40	70.4	*	*	1	26	0	
9/7/2024	22:00	0	0	1	2	6	9	3	0	1	0	0	0	0	0	0	0	0	0	22	35.2	39.5	30 to 40	77.3	*	*	0	21	1	
9/7/2024	23:00	0	0	0	4	8	7	0	0	1	0	0	0	0	0	0	0	0	0	20	33.8	36.3	27 to 37	95.0	*	*	0	20	0	
24 Hr Summary		5	13	40	121	345	386	122	18	4	0	0	0	0	0	0	0	0	0	1054	34.1	39	30 to 40	73.5	*	*	2	1010	42	

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/8/2024	00:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	35	25 to 35	100.0	*	*	0	2	0	
9/8/2024	01:00	0	0	2	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	5	27.8	29	19 to 29	80.0	*	*	0	5	0	
9/8/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/8/2024	03:00	0	0	0	2	0	0	1	0	0	0	0	0	0	0	0	0	0	0	3	31.7	40	19 to 29	66.7	*	*	0	3	0	
9/8/2024	04:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30	30	20 to 30	100.0	*	*	0	2	0	
9/8/2024	05:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	40	40	30 to 40	100.0	*	*	0	1	0	
9/8/2024	06:00	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	32	35	25 to 35	100.0	*	*	0	3	0	
9/8/2024	07:00	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	0	0	4	32	35	25 to 35	75.0	*	*	0	4	0	
9/8/2024	08:00	0	0	1	4	7	1	1	0	0	0	0	0	0	0	0	0	0	0	14	31.1	33	23 to 33	85.7	*	*	0	14	0	
9/8/2024	09:00	0	0	1	12	15	12	2	0	0	0	0	0	0	0	0	0	0	0	42	32.1	36	26 to 36	81.0	*	*	0	41	1	
9/8/2024	10:00	0	0	2	8	8	8	1	0	0	0	0	0	0	0	0	0	0	0	27	31.9	36	28 to 38	85.2	*	*	0	26	1	
9/8/2024	11:00	0	1	1	6	13	6	2	0	0	0	0	0	0	0	0	0	0	0	29	31.5	36	26 to 36	75.9	*	*	0	29	0	
9/8/2024	12:00	1	2	2	4	10	2	2	0	0	0	0	0	0	0	0	0	0	0	23	29.7	35	24 to 34	69.6	*	*	0	20	3	
9/8/2024	13:00	0	3	2	5	12	4	0	0	0	0	0	0	0	0	0	0	0	0	26	29.2	34	26 to 36	73.1	*	*	0	25	1	
9/8/2024	14:00	5	2	6	4	16	7	3	0	0	0	0	0	0	0	0	0	0	0	43	29.1	36	30 to 40	58.1	*	*	0	41	2	
9/8/2024	15:00	1	1	2	8	16	12	4	1	1	0	0	0	0	0	0	0	0	0	46	33	37.5	27 to 37	73.9	*	*	0	41	5	
9/8/2024	16:00	0	0	3	5	31	27	8	1	0	0	0	0	0	0	0	0	0	0	75	34.4	38.3	30 to 40	82.7	*	*	0	73	2	
9/8/2024	17:00	0	1	6	6	25	24	7	1	0	0	0	0	0	0	0	0	0	0	70	33.3	37.8	28 to 38	72.9	*	*	0	70	0	
9/8/2024	18:00	0	0	0	2	19	25	7	1	0	0	0	0	0	0	0	0	0	0	54	35.7	39	31 to 41	90.7	*	*	0	52	2	
9/8/2024	19:00	0	0	1	7	16	14	1	0	0	0	0	0	0	0	0	0	0	0	39	33.1	37.3	28 to 38	87.2	*	*	0	38	1	
9/8/2024	20:00	1	0	2	6	15	14	5	1	0	0	0	0	0	0	0	0	0	0	44	33.2	37.5	27 to 37	75.0	*	*	0	42	2	
9/8/2024	21:00	0	0	1	0	4	5	1	1	0	0	0	0	0	0	0	0	0	0	12	35.3	39	29 to 39	75.0	*	*	0	12	0	
9/8/2024	22:00	0	0	0	2	3	1	1	0	0	0	0	0	0	0	0	0	0	0	7	33	35	25 to 35	85.7	*	*	0	7	0	
9/8/2024	23:00	0	0	0	1	3	1	1	1	0	0	0	0	0	0	0	0	0	0	7	35.7	42	32 to 42	71.4	*	*	0	6	1	
24 Hr Summary		8	10	32	87	217	168	48	7	1	0	0	0	0	0	0	0	0	0	578	32.7	38	28 to 38	73.4	*	*	0	557	21	

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/9/2024	00:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	41	41	31 to 41	100.0	*	*	0	1	0
9/9/2024	01:00	0	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	2	43.5	47	37 to 47	100.0	*	*	0	2	0
9/9/2024	02:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	47	47	37 to 47	100.0	*	*	0	1	0
9/9/2024	03:00	0	0	0	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	4	33	36	26 to 36	75.0	*	*	0	4	0
9/9/2024	04:00	0	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	0	0	4	34.8	34	24 to 34	75.0	*	*	0	4	0
9/9/2024	05:00	0	0	0	4	3	5	2	0	0	0	0	0	0	0	0	0	0	0	14	33.8	39	26 to 36	78.6	*	*	0	14	0
9/9/2024	06:00	0	1	0	4	12	19	3	1	0	0	0	0	0	0	0	0	0	0	40	34.8	38.3	29 to 39	85.0	*	*	0	37	3
9/9/2024	07:00	2	0	2	10	26	18	2	1	0	0	0	0	0	0	0	0	0	0	61	32.3	37.8	27 to 37	72.1	*	*	0	58	3
9/9/2024	08:00	0	0	1	3	14	14	2	0	0	0	0	0	0	0	0	0	0	0	34	33.6	37.7	29 to 39	85.3	*	*	0	29	5
9/9/2024	09:00	0	0	0	3	14	4	2	0	0	0	0	0	0	0	0	0	0	0	23	33	36	26 to 36	87.0	*	*	0	21	2
9/9/2024	10:00	2	2	6	14	13	14	0	0	0	0	0	0	0	0	0	0	0	0	51	29.8	36	29 to 39	64.7	*	*	0	38	13
9/9/2024	11:00	0	1	3	7	10	6	1	0	0	0	0	0	0	0	0	0	0	0	28	30.8	35.5	26 to 36	75.0	*	*	0	22	6
9/9/2024	12:00	0	1	2	4	9	4	1	0	0	0	0	0	0	0	0	0	0	0	21	31	37.5	24 to 34	66.7	*	*	0	15	6
9/9/2024	13:00	0	1	2	4	15	7	1	0	0	0	0	0	0	0	0	0	0	0	30	32.1	36.5	27 to 37	76.7	*	*	0	26	4
9/9/2024	14:00	1	0	3	11	35	17	0	1	0	0	0	0	0	0	0	0	0	0	68	32.2	35.3	29 to 39	83.8	*	*	0	62	6
9/9/2024	15:00	0	4	2	15	16	13	6	0	0	0	0	0	0	0	0	0	0	0	56	31.6	38	28 to 38	73.2	*	*	0	51	5
9/9/2024	16:00	2	3	3	16	20	31	6	2	0	0	0	0	0	0	0	0	0	0	83	32.7	37.5	28 to 38	72.3	*	*	0	76	7
9/9/2024	17:00	0	0	3	8	28	20	9	0	0	0	0	0	0	0	0	0	0	0	68	33.8	38.8	29 to 39	73.5	*	*	0	63	5
9/9/2024	18:00	0	0	0	5	24	26	7	3	0	0	0	0	0	0	0	0	0	0	65	35.4	39	29 to 39	81.5	*	*	0	62	3
9/9/2024	19:00	0	0	0	4	13	14	3	2	0	0	0	0	0	0	0	0	0	0	36	35	38	28 to 38	83.3	*	*	0	36	0
9/9/2024	20:00	0	0	0	8	13	4	4	0	0	0	0	0	0	0	0	0	0	0	29	32.3	37	25 to 35	82.8	*	*	0	28	1
9/9/2024	21:00	1	0	1	3	7	5	0	0	0	0	0	0	0	0	0	0	0	0	17	30.5	35.7	27 to 37	82.4	*	*	0	16	1
9/9/2024	22:00	0	1	0	4	3	2	2	1	0	0	0	0	0	0	0	0	0	0	13	32.9	41	26 to 36	69.2	*	*	0	13	0
9/9/2024	23:00	0	0	0	1	3	3	0	0	0	0	0	0	0	0	0	0	0	0	7	33	36	26 to 36	85.7	*	*	0	4	3
24 Hr Summary		8	14	28	129	282	227	55	13	0	0	0	0	0	0	0	0	0	0	756	32.8	38	28 to 38	73.9	*	*	0	683	73

Incoming Histogram

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Wed-Sep-04-2024-05-00-PM to Tue-Sep-10-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	23	23	13 to 23	100.0	*	*	0	1	0	
9/10/2024	01:00	0	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	3	37.3	39	29 to 39	100.0	*	*	0	3	0	
9/10/2024	02:00	0	0	0	1	1	3	0	1	0	0	0	0	0	0	0	0	0	0	6	36.3	39	29 to 39	83.3	*	*	0	6	0	
9/10/2024	03:00	0	0	1	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	4	32.5	34	21 to 31	50.0	*	*	0	4	0	
9/10/2024	04:00	0	0	1	0	1	4	0	1	0	0	0	0	0	0	0	0	0	0	7	36	39	29 to 39	71.4	*	*	0	6	1	
9/10/2024	05:00	0	0	0	5	2	6	4	0	1	0	0	0	0	0	0	0	0	0	18	35.4	41.3	27 to 37	61.1	*	*	1	16	1	
9/10/2024	06:00	1	0	0	0	15	19	6	4	0	0	0	0	0	0	0	0	0	0	45	36.2	41.5	30 to 40	80.0	*	*	0	43	2	
9/10/2024	07:00	0	3	4	5	33	33	5	5	0	0	0	0	0	0	0	0	0	0	88	34.1	38.4	29 to 39	77.3	*	*	1	81	6	
9/10/2024	08:00	1	2	3	9	19	23	6	0	0	0	0	0	0	0	0	0	0	0	63	32.8	38.2	29 to 39	71.4	*	*	0	52	11	
9/10/2024	09:00	0	3	7	14	28	40	10	1	0	0	0	0	0	0	0	0	0	0	103	33.3	38.2	29 to 39	70.9	*	*	2	82	19	
9/10/2024	10:00	2	1	0	9	21	23	4	0	0	0	0	0	0	0	0	0	0	0	60	32.8	37.3	28 to 38	78.3	*	*	1	50	9	
9/10/2024	11:00	1	2	4	8	13	13	6	2	0	0	0	0	0	0	0	0	0	0	49	32.4	39.3	28 to 38	61.2	*	*	0	39	10	
9/10/2024	12:00	1	1	2	7	11	10	1	0	0	0	0	0	0	0	0	0	0	0	33	31.4	37.5	27 to 37	69.7	*	*	1	28	4	
9/10/2024	13:00	0	2	1	10	21	11	3	0	1	0	0	0	0	0	0	0	0	0	49	32.4	37.3	28 to 38	79.6	*	*	0	41	8	
9/10/2024	14:00	1	1	0	2	0	2	1	0	0	0	0	0	0	0	0	0	0	0	7	28.7	39	27 to 37	42.9	*	*	0	5	2	
9/10/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		7	15	24	70	167	190	47	14	2	0	0	0	0	0	0	0	0	0	536	33.4	39	29 to 39	70.5	*	*	6	457	73	

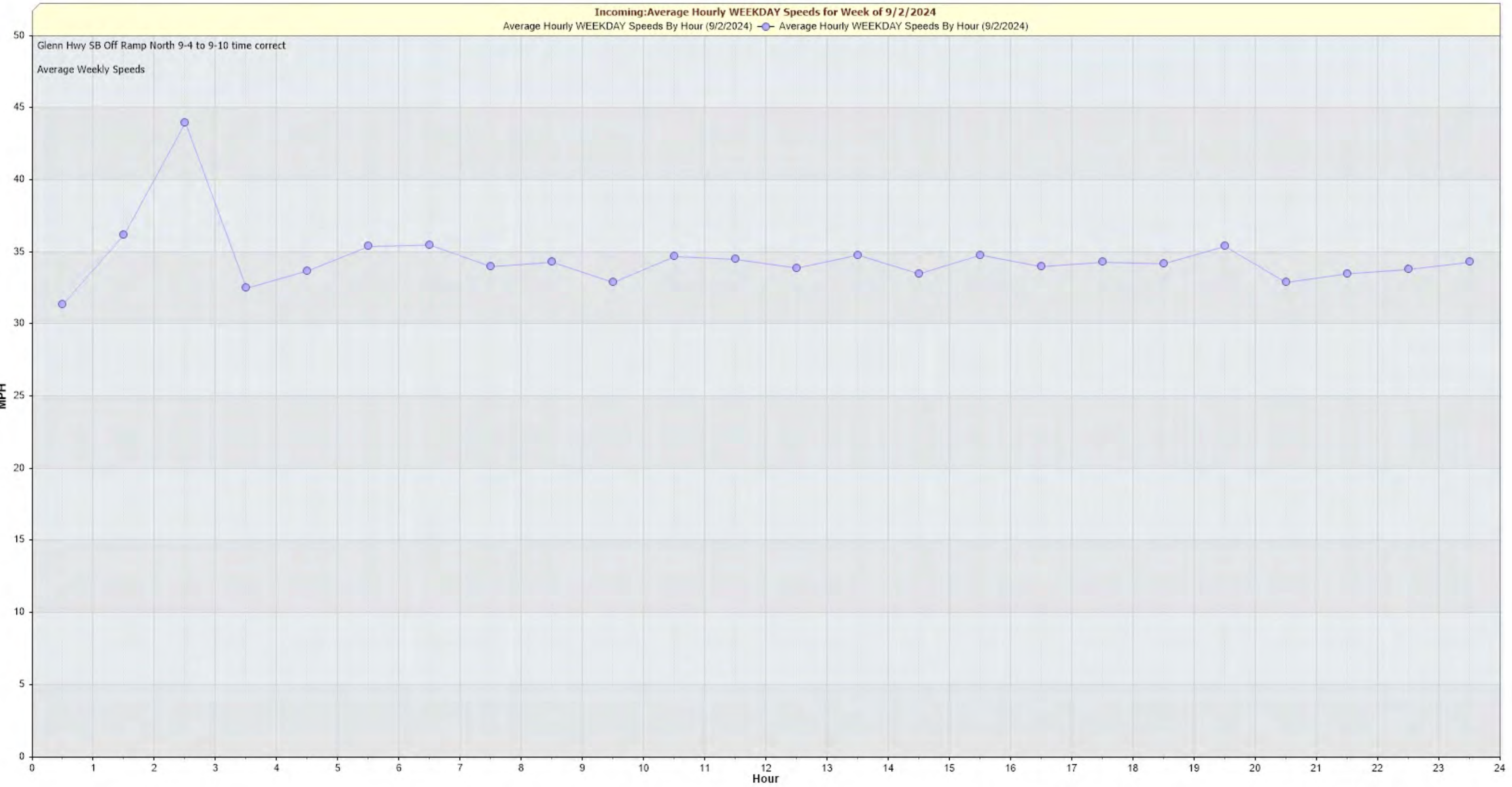
Incoming:Average Hourly Volume for Week of 9/2/2024
Average Counts By Hour (9/2/2024) ● Average Counts By Hour (9/2/2024)

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct
Average Weekly Volumes



Incoming: Average Hourly WEEKDAY Speeds for Week of 9/2/2024
Average Hourly WEEKDAY Speeds By Hour (9/2/2024) — Average Hourly WEEKDAY Speeds By Hour (9/2/2024)

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct
Average Weekly Speeds



Incoming:Daily Volume for Week of 9/2/2024

Daily Vehicle Counts

Day

Monday

Tuesday

Wednesday

Thursday

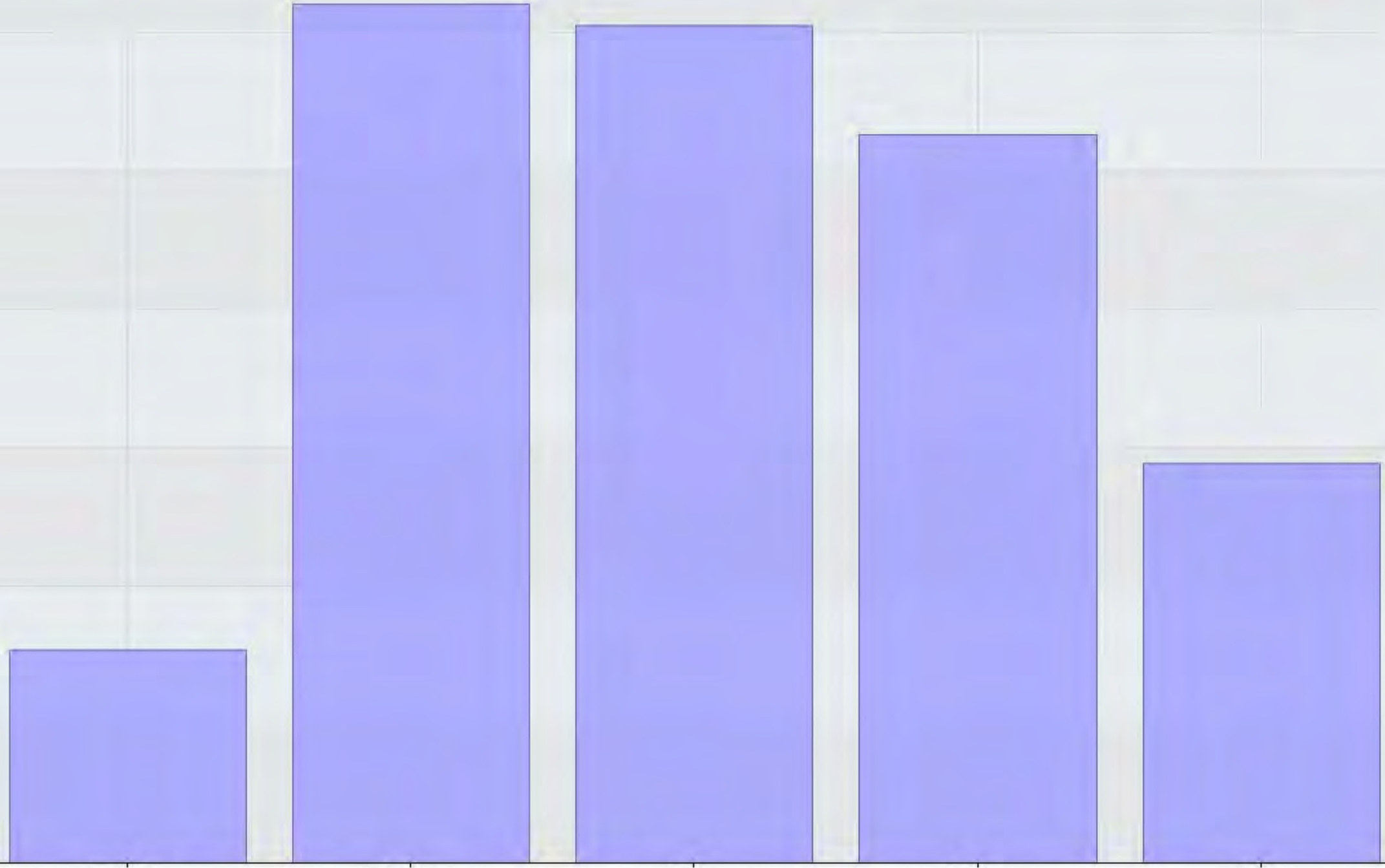
Friday

Saturday

Sunday

1,400
1,200
1,000
800
600
400
200
0

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct
Daily Volumes



For Project: Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:44:55 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/6/2024 5:00:00 PM through 9/6/2024 5:59:59 PM

85th Percentile Speed 42 MPH

85th Percentile Vehicles 1

Max Speed 42 MPH on 9/6/2024 5:21:22 PM

Total Vehicles 1

AADT: 24

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1	1
AM Peak 12:00 AM	0	0
PM Peak 5:00 PM	1	1

Speed

Speed Limit: 65

85th Percentile Speed: 42

50th Percentile Speed: 1

10 MPH Pace Interval: 32.0 MPH to 42.0 MPH

Average Speed: 42

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	N/A	N/A	N/A	N/A	0	N/A	N/A
% over limit	N/A	N/A	N/A	N/A	0.0	N/A	N/A
Avg Speeder	N/A	N/A	N/A	N/A	0.0	N/A	N/A
Avg Speed	N/A	N/A	N/A	N/A	42.0	N/A	N/A

Class Counts

	Number	%
VEH_SM	0	0
VEH_MED	1	100
VEH_LG	0	0
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Outgoing Weekly Counts

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	*	*	0	0	0
1 - 2	*	*	*	*	*	*	*	0	0	0
2 - 3	*	*	*	*	*	*	*	0	0	0
3 - 4	*	*	*	*	*	*	*	0	0	0
4 - 5	*	*	*	*	*	*	*	0	0	0
5 - 6	*	*	*	*	*	*	*	0	0	0
6 - 7	*	*	*	*	*	*	*	0	0	0
7 - 8	*	*	*	*	*	*	*	0	0	0
8 - 9	*	*	*	*	*	*	*	0	0	0
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	1	*	*	1	0	42
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	0	0	0	0	1	0	0			
% of Total	0%	0%	0%	0%	100%	0%	0%			

Outgoing Monthly Counts

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	*	*	0	0	0
1 - 2	*	*	*	*	*	*	*	0	0	0
2 - 3	*	*	*	*	*	*	*	0	0	0
3 - 4	*	*	*	*	*	*	*	0	0	0
4 - 5	*	*	*	*	*	*	*	0	0	0
5 - 6	*	*	*	*	*	*	*	0	0	0
6 - 7	*	*	*	*	*	*	*	0	0	0
7 - 8	*	*	*	*	*	*	*	0	0	0
8 - 9	*	*	*	*	*	*	*	0	0	0
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	1	*	*	1	0	42
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	0	0	0	0	1	0	0			
% of Total	0%	0%	0%	0%	100%	0%	0%			

Outgoing Weekly Speeds
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	*	*	*	*	0
1 - 2	*	*	*	*	*	*	*	*	*	0
2 - 3	*	*	*	*	*	*	*	*	*	0
3 - 4	*	*	*	*	*	*	*	*	*	0
4 - 5	*	*	*	*	*	*	*	*	*	0
5 - 6	*	*	*	*	*	*	*	*	*	0
6 - 7	*	*	*	*	*	*	*	*	*	0
7 - 8	*	*	*	*	*	*	*	*	*	0
8 - 9	*	*	*	*	*	*	*	*	*	0
9 - 10	*	*	*	*	*	*	*	*	*	0
10 - 11	*	*	*	*	*	*	*	*	*	0
11 - 12	*	*	*	*	*	*	*	*	*	0
12 - 13	*	*	*	*	*	*	*	*	*	0
13 - 14	*	*	*	*	*	*	*	*	*	0
14 - 15	*	*	*	*	*	*	*	*	*	0
15 - 16	*	*	*	*	*	*	*	*	*	0
16 - 17	*	*	*	*	*	*	*	*	*	0
17 - 18	*	*	*	*	42	*	*	42	*	42
18 - 19	*	*	*	*	*	*	*	*	*	0
19 - 20	*	*	*	*	*	*	*	*	*	0
20 - 21	*	*	*	*	*	*	*	*	*	0
21 - 22	*	*	*	*	*	*	*	*	*	0
22 - 23	*	*	*	*	*	*	*	*	*	0
23 - 24	*	*	*	*	*	*	*	*	*	0
Totals	0	0	0	0	42	0	0			
% of Total	0%	0%	0%	0%	100%	0%	0%			

Outgoing Monthly Speeds
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	*	*	*	*	0
1 - 2	*	*	*	*	*	*	*	*	*	0
2 - 3	*	*	*	*	*	*	*	*	*	0
3 - 4	*	*	*	*	*	*	*	*	*	0
4 - 5	*	*	*	*	*	*	*	*	*	0
5 - 6	*	*	*	*	*	*	*	*	*	0
6 - 7	*	*	*	*	*	*	*	*	*	0
7 - 8	*	*	*	*	*	*	*	*	*	0
8 - 9	*	*	*	*	*	*	*	*	*	0
9 - 10	*	*	*	*	*	*	*	*	*	0
10 - 11	*	*	*	*	*	*	*	*	*	0
11 - 12	*	*	*	*	*	*	*	*	*	0
12 - 13	*	*	*	*	*	*	*	*	*	0
13 - 14	*	*	*	*	*	*	*	*	*	0
14 - 15	*	*	*	*	*	*	*	*	*	0
15 - 16	*	*	*	*	*	*	*	*	*	0
16 - 17	*	*	*	*	*	*	*	*	*	0
17 - 18	*	*	*	*	42	*	*	42	*	42
18 - 19	*	*	*	*	*	*	*	*	*	0
19 - 20	*	*	*	*	*	*	*	*	*	0
20 - 21	*	*	*	*	*	*	*	*	*	0
21 - 22	*	*	*	*	*	*	*	*	*	0
22 - 23	*	*	*	*	*	*	*	*	*	0
23 - 24	*	*	*	*	*	*	*	*	*	0
Totals	0	0	0	0	42	0	0			
% of Total	0%	0%	0%	0%	100%	0%	0%			

Outgoing Weekly EightyFifthSpeeds
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

	9/2/2024	to	9/8/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/2/2024	9/3/2024	9/4/2024	9/5/2024	9/6/2024	9/7/2024	9/8/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	*	*	0	0	0
1 - 2	*	*	*	*	*	*	*	0	0	0
2 - 3	*	*	*	*	*	*	*	0	0	0
3 - 4	*	*	*	*	*	*	*	0	0	0
4 - 5	*	*	*	*	*	*	*	0	0	0
5 - 6	*	*	*	*	*	*	*	0	0	0
6 - 7	*	*	*	*	*	*	*	0	0	0
7 - 8	*	*	*	*	*	*	*	0	0	0
8 - 9	*	*	*	*	*	*	*	0	0	0
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	42	*	*	42	0	42
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	0	0	0	0	42	0	0			
% of Total	0%	0%	0%	0%	100%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	*	*	*	*	*	0	0	0
1 - 2	*	*	*	*	*	*	*	0	0	0
2 - 3	*	*	*	*	*	*	*	0	0	0
3 - 4	*	*	*	*	*	*	*	0	0	0
4 - 5	*	*	*	*	*	*	*	0	0	0
5 - 6	*	*	*	*	*	*	*	0	0	0
6 - 7	*	*	*	*	*	*	*	0	0	0
7 - 8	*	*	*	*	*	*	*	0	0	0
8 - 9	*	*	*	*	*	*	*	0	0	0
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	42	*	*	42	0	42
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0

Summary of Violators

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	0	N/A	N/A	N/A
01:00:00	0	N/A	N/A	N/A
02:00:00	0	N/A	N/A	N/A
03:00:00	0	N/A	N/A	N/A
04:00:00	0	N/A	N/A	N/A
05:00:00	0	N/A	N/A	N/A
06:00:00	0	N/A	N/A	N/A
07:00:00	0	N/A	N/A	N/A
08:00:00	0	N/A	N/A	N/A
09:00:00	0	N/A	N/A	N/A
10:00:00	0	N/A	N/A	N/A
11:00:00	0	N/A	N/A	N/A
12:00:00	0	N/A	N/A	N/A
13:00:00	0	N/A	N/A	N/A
14:00:00	0	N/A	N/A	N/A
15:00:00	0	N/A	N/A	N/A
16:00:00	0	N/A	N/A	N/A
17:00:00	1	42.0	0	0.0
18:00:00	0	N/A	N/A	N/A
19:00:00	0	N/A	N/A	N/A
20:00:00	0	N/A	N/A	N/A
21:00:00	0	N/A	N/A	N/A
22:00:00	0	N/A	N/A	N/A
23:00:00	0	N/A	N/A	N/A

Outgoing Histogram

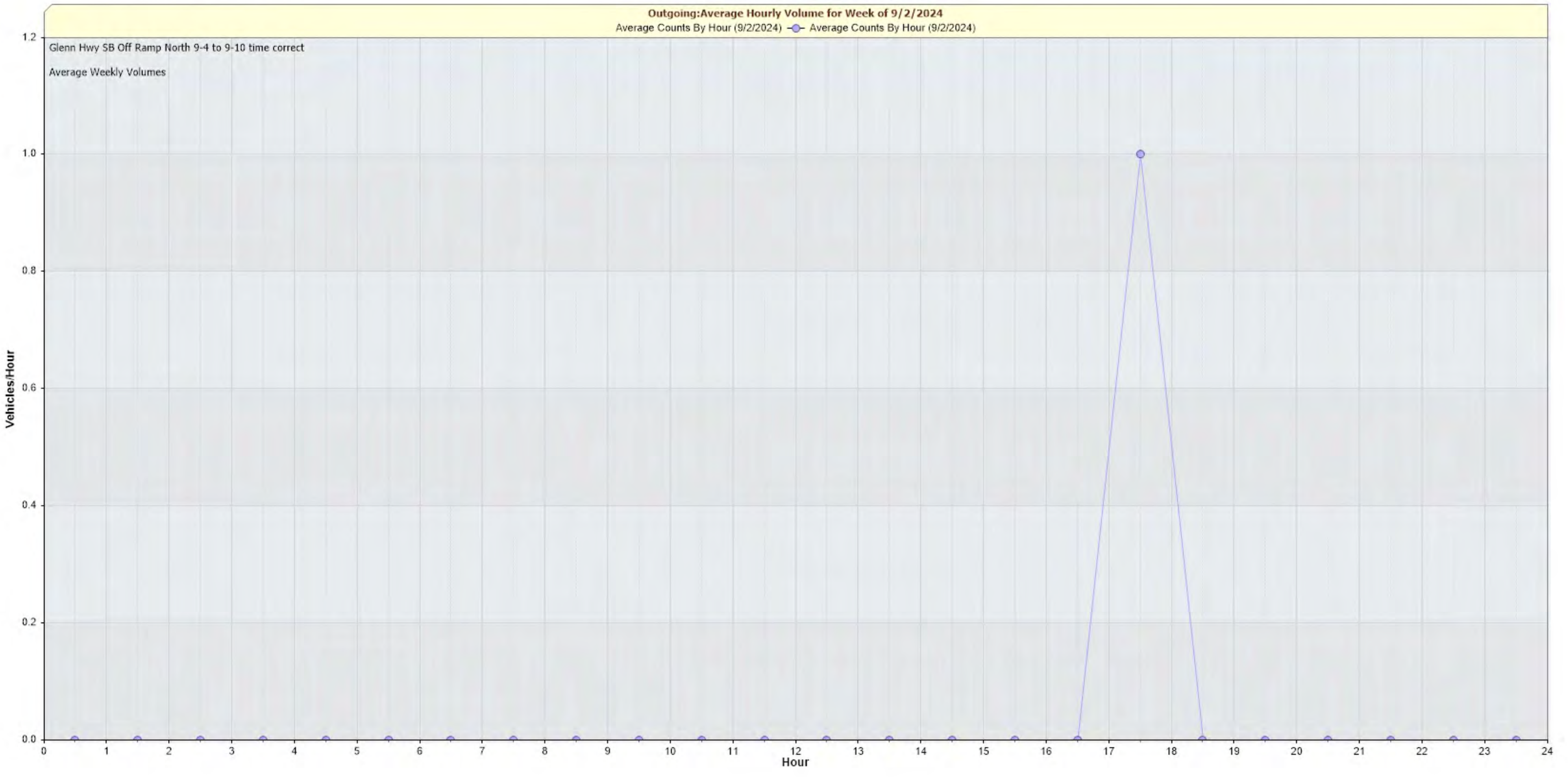
Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

from Fri-Sep-06-2024-05-00-PM to Fri-Sep-06-2024-05-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/6/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	17:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	42	42	32 to 42	100.0	*	*	0	1	0
9/6/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/6/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	42	42	32 to 42	100.0	*	*	0	1	0

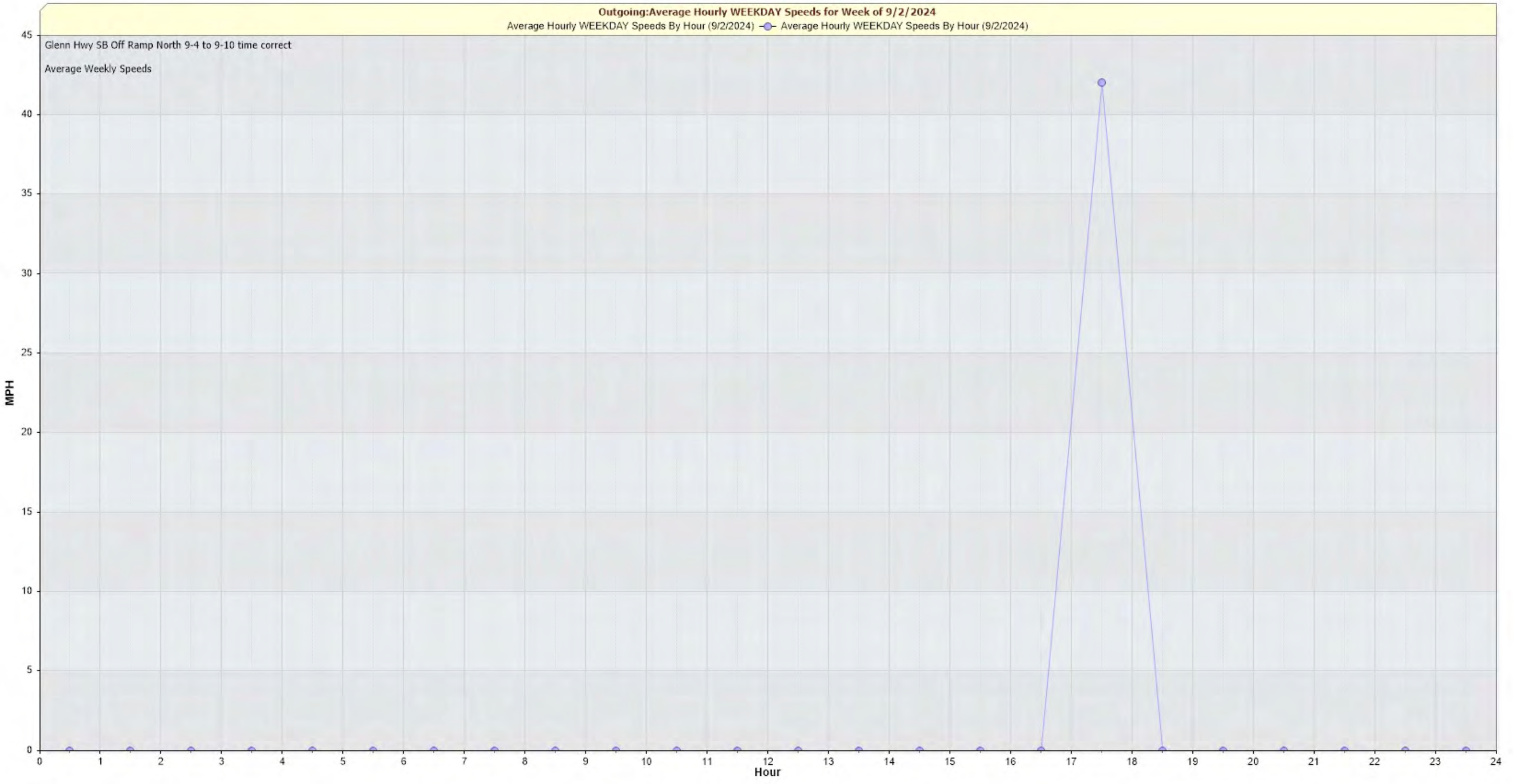
Outgoing: Average Hourly Volume for Week of 9/2/2024
Average Counts By Hour (9/2/2024) — Average Counts By Hour (9/2/2024)

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/2/2024
Average Hourly WEEKDAY Speeds By Hour (9/2/2024) — Average Hourly WEEKDAY Speeds By Hour (9/2/2024)

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct
Average Weekly Speeds

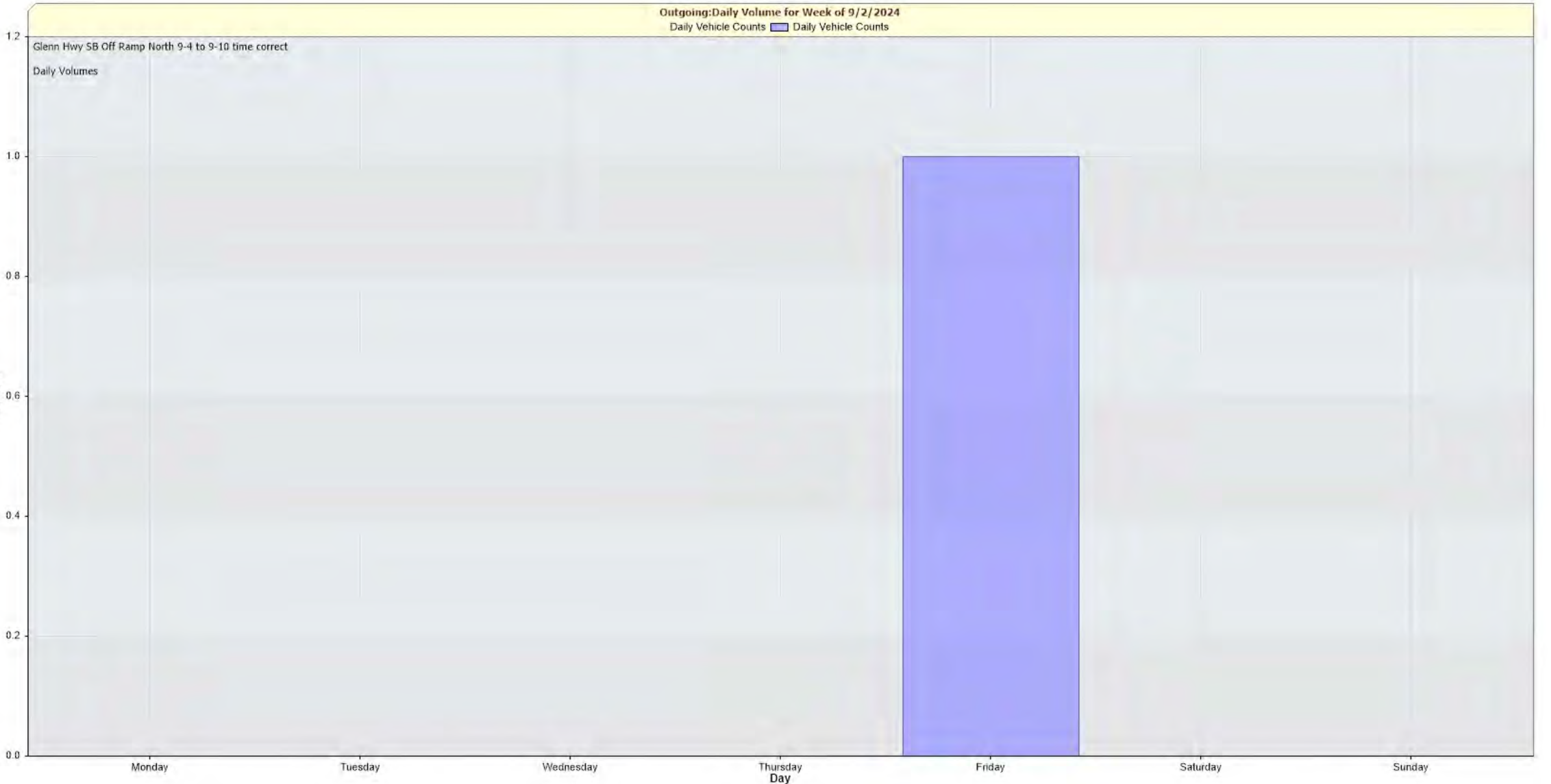


Outgoing: Daily Volume for Week of 9/2/2024

Daily Vehicle Counts

Glenn Hwy SB Off Ramp North 9-4 to 9-10 time correct

Daily Volumes



For Project: Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:49:26 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/10/2024 3:00:00 PM through 9/16/2024 8:59:59 AM

85th Percentile Speed 40 MPH

85th Percentile Vehicles 8926

Max Speed 53 MPH on 9/15/2024 6:30:37 PM

Total Vehicles 10501

AADT: 1826

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1500	1500
AM Peak 8:00 AM	118	89
PM Peak 6:00 PM	217	181

Speed

Speed Limit: 65

85th Percentile Speed: 40

50th Percentile Speed: 35

10 MPH Pace Interval: 31.0 MPH to 41.0 MPH

Average Speed: 33.87

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	0	0	0	0	0	0	0
% over limit	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speeder	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Avg Speed	34.1	32.7	34.1	32.9	34.8	33.3	35.0

Class Counts

	Number	%
VEH_SM	24	0.2
VEH_MED	10205	97.2
VEH_LG	272	2.6
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/10/2024 4:00:00 PM	39.0	101	119	49	0.0	0.0%	32.4
9/10/2024 5:00:00 PM	40.0	133	156	46	0.0	0.0%	32.7
9/10/2024 6:00:00 PM	39.0	183	215	45	0.0	0.0%	32.7
9/10/2024 7:00:00 PM	39.0	151	178	49	0.0	0.0%	33.3
9/10/2024 8:00:00 PM	38.0	150	177	45	0.0	0.0%	31.7
9/10/2024 9:00:00 PM	40.0	94	110	52	0.0	0.0%	33.7
9/10/2024 10:00:00 PM	38.0	54	64	44	0.0	0.0%	33.9
9/10/2024 11:00:00 PM	37.0	48	56	46	0.0	0.0%	31.3
9/11/2024 12:00:00 AM	37.0	20	23	40	0.0	0.0%	32.3
9/11/2024 1:00:00 AM	35.0	14	16	41	0.0	0.0%	28.6
9/11/2024 2:00:00 AM	36.0	3	3	36	0.0	0.0%	30.3
9/11/2024 3:00:00 AM	31.0	4	5	39	0.0	0.0%	26.8
9/11/2024 4:00:00 AM	34.0	4	5	40	0.0	0.0%	28.0
9/11/2024 5:00:00 AM	34.0	1	1	34	0.0	0.0%	34.0
9/11/2024 6:00:00 AM	35.0	10	12	41	0.0	0.0%	30.4
9/11/2024 7:00:00 AM	40.0	8	9	43	0.0	0.0%	33.6
9/11/2024 8:00:00 AM	37.0	21	25	39	0.0	0.0%	33.2
9/11/2024 9:00:00 AM	39.0	130	153	48	0.0	0.0%	31.7
9/11/2024 10:00:00 AM	38.0	61	72	52	0.0	0.0%	33.3
9/11/2024 11:00:00 AM	39.0	71	84	45	0.0	0.0%	33.7
9/11/2024 12:00:00 PM	41.0	61	72	46	0.0	0.0%	33.0
9/11/2024 1:00:00 PM	41.0	59	69	47	0.0	0.0%	35.9
9/11/2024 2:00:00 PM	40.0	96	113	46	0.0	0.0%	33.8
9/11/2024 3:00:00 PM	41.0	116	136	48	0.0	0.0%	34.1
9/11/2024 4:00:00 PM	40.0	153	180	47	0.0	0.0%	33.6
9/11/2024 5:00:00 PM	40.0	130	153	45	0.0	0.0%	34.0
9/11/2024 6:00:00 PM	42.0	166	195	49	0.0	0.0%	35.9
9/11/2024 7:00:00 PM	41.0	202	238	48	0.0	0.0%	35.7
9/11/2024 8:00:00 PM	40.0	182	214	47	0.0	0.0%	34.2
9/11/2024 9:00:00 PM	41.0	95	112	48	0.0	0.0%	36.0
9/11/2024 10:00:00 PM	40.0	83	98	46	0.0	0.0%	33.5
9/11/2024 11:00:00 PM	40.0	37	43	46	0.0	0.0%	33.1
9/12/2024 12:00:00 AM	37.0	20	24	44	0.0	0.0%	31.9
9/12/2024 1:00:00 AM	37.0	11	13	41	0.0	0.0%	34.6
9/12/2024 2:00:00 AM	34.0	5	6	35	0.0	0.0%	28.8
9/12/2024 3:00:00 AM	32.0	4	5	36	0.0	0.0%	27.8
9/12/2024 4:00:00 AM	34.0	1	1	34	0.0	0.0%	34.0
9/12/2024 5:00:00 AM	26.0	2	2	26	0.0	0.0%	18.5
9/12/2024 6:00:00 AM	38.0	3	3	38	0.0	0.0%	34.7
9/12/2024 7:00:00 AM	38.0	12	14	42	0.0	0.0%	32.6
9/12/2024 8:00:00 AM	39.0	20	23	46	0.0	0.0%	34.1
9/12/2024 9:00:00 AM	38.0	137	161	44	0.0	0.0%	32.7
9/12/2024 10:00:00 AM	39.0	82	96	46	0.0	0.0%	32.8
9/12/2024 11:00:00 AM	38.0	62	73	47	0.0	0.0%	33.2
9/12/2024 12:00:00 PM	39.0	67	79	43	0.0	0.0%	32.9
9/12/2024 1:00:00 PM	39.0	76	89	51	0.0	0.0%	32.7
9/12/2024 2:00:00 PM	38.0	89	105	44	0.0	0.0%	31.0

Incoming Summary
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

9/12/2024 3:00:00 PM	39.0	99	116	44	0.0	0.0%	32.7
9/12/2024 4:00:00 PM	39.0	141	166	49	0.0	0.0%	33.6
9/12/2024 5:00:00 PM	40.0	155	182	46	0.0	0.0%	32.7
9/12/2024 6:00:00 PM	39.0	179	211	47	0.0	0.0%	33.9
9/12/2024 7:00:00 PM	38.0	169	199	47	0.0	0.0%	32.7
9/12/2024 8:00:00 PM	39.0	150	177	49	0.0	0.0%	34.2
9/12/2024 9:00:00 PM	38.0	95	112	48	0.0	0.0%	33.2
9/12/2024 10:00:00 PM	37.0	65	76	47	0.0	0.0%	32.1
9/12/2024 11:00:00 PM	35.0	47	55	41	0.0	0.0%	30.9
9/13/2024 12:00:00 AM	36.0	23	27	39	0.0	0.0%	30.1
9/13/2024 1:00:00 AM	37.0	15	18	41	0.0	0.0%	28.3
9/13/2024 2:00:00 AM	42.0	7	8	43	0.0	0.0%	36.4
9/13/2024 3:00:00 AM	37.0	7	8	42	0.0	0.0%	31.1
9/13/2024 4:00:00 AM	34.0	3	3	34	0.0	0.0%	29.7
9/13/2024 5:00:00 AM	**No Data**						
9/13/2024 6:00:00 AM	33.0	3	3	33	0.0	0.0%	31.7
9/13/2024 7:00:00 AM	38.0	10	12	47	0.0	0.0%	31.3
9/13/2024 8:00:00 AM	40.0	27	32	45	0.0	0.0%	31.7
9/13/2024 9:00:00 AM	40.0	125	147	49	0.0	0.0%	34.8
9/13/2024 10:00:00 AM	41.0	65	77	48	0.0	0.0%	35.2
9/13/2024 11:00:00 AM	41.0	65	77	47	0.0	0.0%	35.2
9/13/2024 12:00:00 PM	40.0	94	111	49	0.0	0.0%	33.5
9/13/2024 1:00:00 PM	40.0	86	101	48	0.0	0.0%	34.8
9/13/2024 2:00:00 PM	41.0	127	149	49	0.0	0.0%	34.9
9/13/2024 3:00:00 PM	41.0	163	192	52	0.0	0.0%	35.8
9/13/2024 4:00:00 PM	41.0	163	192	49	0.0	0.0%	35.5
9/13/2024 5:00:00 PM	40.0	164	193	49	0.0	0.0%	34.8
9/13/2024 6:00:00 PM	41.0	190	224	52	0.0	0.0%	35.3
9/13/2024 7:00:00 PM	41.0	218	256	48	0.0	0.0%	35.0
9/13/2024 8:00:00 PM	41.0	150	176	50	0.0	0.0%	34.6
9/13/2024 9:00:00 PM	41.0	91	107	47	0.0	0.0%	35.1
9/13/2024 10:00:00 PM	40.0	94	110	47	0.0	0.0%	34.9
9/13/2024 11:00:00 PM	40.0	51	60	45	0.0	0.0%	33.7
9/14/2024 12:00:00 AM	38.0	26	31	45	0.0	0.0%	32.2
9/14/2024 1:00:00 AM	40.0	16	19	43	0.0	0.0%	34.4
9/14/2024 2:00:00 AM	39.0	12	14	46	0.0	0.0%	36.6
9/14/2024 3:00:00 AM	35.0	10	12	39	0.0	0.0%	30.4
9/14/2024 4:00:00 AM	34.0	2	2	34	0.0	0.0%	33.5
9/14/2024 5:00:00 AM	30.0	1	1	30	0.0	0.0%	30.0
9/14/2024 6:00:00 AM	35.0	10	12	43	0.0	0.0%	28.8
9/14/2024 7:00:00 AM	40.0	8	9	44	0.0	0.0%	32.1
9/14/2024 8:00:00 AM	42.0	19	22	47	0.0	0.0%	34.5
9/14/2024 9:00:00 AM	43.0	37	43	50	0.0	0.0%	36.3
9/14/2024 10:00:00 AM	41.0	61	72	48	0.0	0.0%	35.2
9/14/2024 11:00:00 AM	41.0	76	89	50	0.0	0.0%	35.3
9/14/2024 12:00:00 PM	38.0	70	82	47	0.0	0.0%	33.6
9/14/2024 1:00:00 PM	40.0	72	85	45	0.0	0.0%	33.9
9/14/2024 2:00:00 PM	40.0	101	119	47	0.0	0.0%	34.7

Incoming Summary
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

9/14/2024 3:00:00 PM	38.0	102	120	42	0.0	0.0%	32.3
9/14/2024 4:00:00 PM	39.0	112	132	47	0.0	0.0%	32.4
9/14/2024 5:00:00 PM	39.0	121	142	47	0.0	0.0%	31.8
9/14/2024 6:00:00 PM	38.0	98	115	41	0.0	0.0%	32.9
9/14/2024 7:00:00 PM	40.0	90	106	49	0.0	0.0%	33.1
9/14/2024 8:00:00 PM	40.0	75	88	46	0.0	0.0%	34.0
9/14/2024 9:00:00 PM	39.0	68	80	48	0.0	0.0%	34.3
9/14/2024 10:00:00 PM	38.0	65	76	48	0.0	0.0%	31.3
9/14/2024 11:00:00 PM	38.0	42	49	43	0.0	0.0%	30.1
9/15/2024 12:00:00 AM	37.0	26	31	46	0.0	0.0%	31.8
9/15/2024 1:00:00 AM	33.0	18	21	37	0.0	0.0%	27.9
9/15/2024 2:00:00 AM	36.0	7	8	45	0.0	0.0%	34.9
9/15/2024 3:00:00 AM	29.0	3	4	38	0.0	0.0%	29.8
9/15/2024 4:00:00 AM	47.0	3	3	47	0.0	0.0%	35.0
9/15/2024 5:00:00 AM	32.0	4	5	33	0.0	0.0%	26.8
9/15/2024 6:00:00 AM	36.0	6	7	42	0.0	0.0%	30.7
9/15/2024 7:00:00 AM	34.0	3	4	40	0.0	0.0%	34.3
9/15/2024 8:00:00 AM	34.0	9	11	37	0.0	0.0%	31.8
9/15/2024 9:00:00 AM	39.0	18	21	46	0.0	0.0%	34.5
9/15/2024 10:00:00 AM	39.0	48	57	45	0.0	0.0%	33.4
9/15/2024 11:00:00 AM	40.0	86	101	46	0.0	0.0%	34.2
9/15/2024 12:00:00 PM	40.0	87	102	47	0.0	0.0%	34.2
9/15/2024 1:00:00 PM	41.0	110	130	48	0.0	0.0%	34.6
9/15/2024 2:00:00 PM	40.0	99	116	51	0.0	0.0%	35.3
9/15/2024 3:00:00 PM	42.0	101	119	46	0.0	0.0%	36.2
9/15/2024 4:00:00 PM	41.0	99	117	48	0.0	0.0%	35.6
9/15/2024 5:00:00 PM	40.0	105	124	48	0.0	0.0%	34.6
9/15/2024 6:00:00 PM	42.0	110	130	48	0.0	0.0%	36.2
9/15/2024 7:00:00 PM	43.0	88	104	53	0.0	0.0%	37.2
9/15/2024 8:00:00 PM	42.0	81	95	51	0.0	0.0%	37.1
9/15/2024 9:00:00 PM	40.0	60	71	50	0.0	0.0%	33.8
9/15/2024 10:00:00 PM	40.0	65	77	45	0.0	0.0%	33.5
9/15/2024 11:00:00 PM	42.0	29	34	47	0.0	0.0%	35.7
9/16/2024 12:00:00 AM	39.0	16	19	43	0.0	0.0%	32.2
9/16/2024 1:00:00 AM	39.0	12	14	41	0.0	0.0%	36.0
9/16/2024 2:00:00 AM	40.0	9	11	43	0.0	0.0%	33.9
9/16/2024 3:00:00 AM	36.0	4	5	41	0.0	0.0%	34.4
9/16/2024 4:00:00 AM	37.0	6	7	38	0.0	0.0%	27.7
9/16/2024 5:00:00 AM	40.0	3	4	44	0.0	0.0%	33.3
9/16/2024 6:00:00 AM	40.0	3	4	40	0.0	0.0%	38.3
9/16/2024 7:00:00 AM	39.0	8	9	45	0.0	0.0%	28.7
9/16/2024 8:00:00 AM	40.0	22	26	41	0.0	0.0%	34.7
9/16/2024 9:00:00 AM	41.0	11	13	51	0.0	0.0%	37.0

Incoming Summary
Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/11/2024 12:00:00 AM	39.0	933	1098	52	0.0	0.0%	32.7
9/12/2024 12:00:00 AM	40.0	1727	2032	52	0.0	0.0%	34.1
9/13/2024 12:00:00 AM	39.0	1692	1991	51	0.0	0.0%	32.9
9/14/2024 12:00:00 AM	41.0	1944	2287	52	0.0	0.0%	34.8
9/15/2024 12:00:00 AM	39.0	1292	1520	50	0.0	0.0%	33.3
9/16/2024 12:00:00 AM	41.0	1258	1480	53	0.0	0.0%	35.0
9/16/2024 8:59:59 AM	40.0	79	93	51	0.0	0.0%	34.1

Incoming Weekly Counts

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	16	13	18	19	21	15.67	20	36.33
1 - 2	*	*	3	6	8	14	8	5.67	11	37.33
2 - 3	*	*	5	5	8	12	4	6	8	33.33
3 - 4	*	*	5	1	3	2	3	3	2.5	34
4 - 5	*	*	1	2	0	1	5	1	3	30
5 - 6	*	*	12	3	3	12	7	6	9.5	35.33
6 - 7	*	*	9	14	12	9	4	11.67	6.5	38.67
7 - 8	*	*	25	23	32	22	11	26.67	16.5	38.67
8 - 9	*	*	153	161	147	43	21	153.67	32	38.37
9 - 10	*	*	72	96	77	72	57	81.67	64.5	39.3
10 - 11	*	*	84	73	77	89	101	78	95	39.07
11 - 12	*	*	72	79	111	82	102	87.33	92	39.47
12 - 13	*	*	69	89	101	85	130	86.33	107.5	39.57
13 - 14	*	*	113	105	149	119	116	122.33	117.5	39.13
14 - 15	*	*	136	116	192	120	119	148	119.5	39.83
15 - 16	*	119	180	166	192	132	117	164.25	124.5	39.28
16 - 17	*	156	153	182	193	142	124	171	133	39.35
17 - 18	*	215	195	211	224	115	130	211.25	122.5	39.7
18 - 19	*	178	238	199	256	106	104	217.75	105	39.38
19 - 20	*	177	214	177	176	88	95	186	91.5	39.1
20 - 21	*	110	112	112	107	80	71	110.25	75.5	39.7
21 - 22	*	64	98	76	110	76	77	87	76.5	38.45
22 - 23	*	56	43	55	60	49	34	53.5	41.5	37.83
23 - 24	*	23	24	27	31	31	19	26.25	25	36.92
Totals	0	1098	2032	1991	2287	1520	1480			
% of Total	0%	10.55%	19.52%	19.13%	21.97%	14.6%	14.22%			

Incoming Weekly Counts

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	14	*	*	*	*	*	*	14	0	39
1 - 2	11	*	*	*	*	*	*	11	0	39.5
2 - 3	5	*	*	*	*	*	*	5	0	36
3 - 4	7	*	*	*	*	*	*	7	0	37
4 - 5	4	*	*	*	*	*	*	4	0	40
5 - 6	4	*	*	*	*	*	*	4	0	39.5
6 - 7	9	*	*	*	*	*	*	9	0	39
7 - 8	26	*	*	*	*	*	*	26	0	39.5
8 - 9	13	*	*	*	*	*	*	13	0	41
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	*	*	*	0	0	0
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	93	0	0	0	0	0	0			
% of Total	100%	0%	0%	0%	0%	0%	0%			

Incoming Monthly Counts

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	14	*	16	13	18	19	21	15.25	20	37
1 - 2	11	*	3	6	8	14	8	7	11	37.88
2 - 3	5	*	5	5	8	12	4	5.75	8	34
3 - 4	7	*	5	1	3	2	3	4	2.5	34.75
4 - 5	4	*	1	2	0	1	5	1.75	3	33.33
5 - 6	4	*	12	3	3	12	7	5.5	9.5	36.38
6 - 7	9	*	9	14	12	9	4	11	6.5	38.75
7 - 8	26	*	25	23	32	22	11	26.5	16.5	38.88
8 - 9	13	*	153	161	147	43	21	118.5	32	39.02
9 - 10	*	*	72	96	77	72	57	81.67	64.5	39.3
10 - 11	*	*	84	73	77	89	101	78	95	39.07
11 - 12	*	*	72	79	111	82	102	87.33	92	39.47
12 - 13	*	*	69	89	101	85	130	86.33	107.5	39.57
13 - 14	*	*	113	105	149	119	116	122.33	117.5	39.13
14 - 15	*	*	136	116	192	120	119	148	119.5	39.83
15 - 16	*	119	180	166	192	132	117	164.25	124.5	39.28
16 - 17	*	156	153	182	193	142	124	171	133	39.35
17 - 18	*	215	195	211	224	115	130	211.25	122.5	39.7
18 - 19	*	178	238	199	256	106	104	217.75	105	39.38
19 - 20	*	177	214	177	176	88	95	186	91.5	39.1
20 - 21	*	110	112	112	107	80	71	110.25	75.5	39.7
21 - 22	*	64	98	76	110	76	77	87	76.5	38.45
22 - 23	*	56	43	55	60	49	34	53.5	41.5	37.83
23 - 24	*	23	24	27	31	31	19	26.25	25	36.92
Totals	93	1098	2032	1991	2287	1520	1480			
% of Total	0.89%	10.46%	19.35%	18.96%	21.78%	14.47%	14.09%			

Incoming Weekly Speeds

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	28.56	34.62	28.33	34.42	27.9	30.15	31	36.33
1 - 2	*	*	30.33	28.83	36.38	36.57	34.88	32.65	35.95	37.33
2 - 3	*	*	26.8	27.8	31.12	30.42	29.75	29	30.25	33.33
3 - 4	*	*	28	34	29.67	33.5	35	29.22	34.4	34
4 - 5	*	*	34	18.5	0	30	26.8	23.67	27.33	30
5 - 6	*	*	30.42	34.67	31.67	28.75	30.71	31.33	29.47	35.33
6 - 7	*	*	33.56	32.57	31.25	32.11	34.25	32.37	32.77	38.67
7 - 8	*	*	33.2	34.13	31.72	34.45	31.82	32.88	33.58	38.67
8 - 9	*	*	31.68	32.73	34.8	36.35	34.48	33.04	35.73	38.37
9 - 10	*	*	33.26	32.84	35.21	35.19	33.44	33.71	34.42	39.3
10 - 11	*	*	33.69	33.22	35.17	35.3	34.21	34.03	34.72	39.07
11 - 12	*	*	32.99	32.87	33.53	33.57	34.21	33.18	33.92	39.47
12 - 13	*	*	35.88	32.73	34.83	33.94	34.55	34.39	34.31	39.57
13 - 14	*	*	33.78	31.05	34.86	34.72	35.34	33.44	35.03	39.13
14 - 15	*	*	34.14	32.72	35.76	32.25	36.24	34.47	34.24	39.83
15 - 16	*	32.39	33.56	33.63	35.46	32.44	35.62	33.92	33.94	39.28
16 - 17	*	32.73	33.97	32.67	34.83	31.82	34.56	33.58	33.09	39.35
17 - 18	*	32.66	35.86	33.89	35.27	32.87	36.25	34.4	34.66	39.7
18 - 19	*	33.3	35.69	32.74	34.95	33.1	37.24	34.31	35.15	39.38
19 - 20	*	31.7	34.17	34.23	34.56	33.97	37.05	33.69	35.57	39.1
20 - 21	*	33.65	36.03	33.23	35.06	34.26	33.77	34.49	34.03	39.7
21 - 22	*	33.88	33.52	32.13	34.89	31.3	33.47	33.72	32.39	38.45
22 - 23	*	31.27	33.14	30.91	33.68	30.14	35.71	32.23	32.42	37.83
23 - 24	*	32.26	31.88	30.11	32.16	31.84	32.21	31.59	31.98	36.92
Totals	0	32.7	34.1	32.9	34.8	33.3	35			
% of Total	0%	16.12%	16.81%	16.22%	17.16%	16.42%	17.26%			

Incoming Weekly Speeds
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	36	*	*	*	*	*	*	36	*	39
1 - 2	33.91	*	*	*	*	*	*	33.91	*	39.5
2 - 3	34.4	*	*	*	*	*	*	34.4	*	36
3 - 4	27.71	*	*	*	*	*	*	27.71	*	37
4 - 5	33.25	*	*	*	*	*	*	33.25	*	40
5 - 6	38.25	*	*	*	*	*	*	38.25	*	39.5
6 - 7	28.67	*	*	*	*	*	*	28.67	*	39
7 - 8	34.69	*	*	*	*	*	*	34.69	*	39.5
8 - 9	37	*	*	*	*	*	*	37	*	41
9 - 10	*	*	*	*	*	*	*	*	*	0
10 - 11	*	*	*	*	*	*	*	*	*	0
11 - 12	*	*	*	*	*	*	*	*	*	0
12 - 13	*	*	*	*	*	*	*	*	*	0
13 - 14	*	*	*	*	*	*	*	*	*	0
14 - 15	*	*	*	*	*	*	*	*	*	0
15 - 16	*	*	*	*	*	*	*	*	*	0
16 - 17	*	*	*	*	*	*	*	*	*	0
17 - 18	*	*	*	*	*	*	*	*	*	0
18 - 19	*	*	*	*	*	*	*	*	*	0
19 - 20	*	*	*	*	*	*	*	*	*	0
20 - 21	*	*	*	*	*	*	*	*	*	0
21 - 22	*	*	*	*	*	*	*	*	*	0
22 - 23	*	*	*	*	*	*	*	*	*	0
23 - 24	*	*	*	*	*	*	*	*	*	0
Totals	34.1	0	0	0	0	0	0			
% of Total	100%	0%	0%	0%	0%	0%	0%			

Incoming Monthly Speeds

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	36	*	28.56	34.62	28.33	34.42	27.9	31.49	31	37
1 - 2	33.91	*	30.33	28.83	36.38	36.57	34.88	33.14	35.95	37.88
2 - 3	34.4	*	26.8	27.8	31.12	30.42	29.75	30.17	30.25	34
3 - 4	27.71	*	28	34	29.67	33.5	35	28.56	34.4	34.75
4 - 5	33.25	*	34	18.5	0	30	26.8	29.14	27.33	33.33
5 - 6	38.25	*	30.42	34.67	31.67	28.75	30.71	32.59	29.47	36.38
6 - 7	28.67	*	33.56	32.57	31.25	32.11	34.25	31.61	32.77	38.75
7 - 8	34.69	*	33.2	34.13	31.72	34.45	31.82	33.32	33.58	38.88
8 - 9	37	*	31.68	32.73	34.8	36.35	34.48	33.15	35.73	39.02
9 - 10	*	*	33.26	32.84	35.21	35.19	33.44	33.71	34.42	39.3
10 - 11	*	*	33.69	33.22	35.17	35.3	34.21	34.03	34.72	39.07
11 - 12	*	*	32.99	32.87	33.53	33.57	34.21	33.18	33.92	39.47
12 - 13	*	*	35.88	32.73	34.83	33.94	34.55	34.39	34.31	39.57
13 - 14	*	*	33.78	31.05	34.86	34.72	35.34	33.44	35.03	39.13
14 - 15	*	*	34.14	32.72	35.76	32.25	36.24	34.47	34.24	39.83
15 - 16	*	32.39	33.56	33.63	35.46	32.44	35.62	33.92	33.94	39.28
16 - 17	*	32.73	33.97	32.67	34.83	31.82	34.56	33.58	33.09	39.35
17 - 18	*	32.66	35.86	33.89	35.27	32.87	36.25	34.4	34.66	39.7
18 - 19	*	33.3	35.69	32.74	34.95	33.1	37.24	34.31	35.15	39.38
19 - 20	*	31.7	34.17	34.23	34.56	33.97	37.05	33.69	35.57	39.1
20 - 21	*	33.65	36.03	33.23	35.06	34.26	33.77	34.49	34.03	39.7
21 - 22	*	33.88	33.52	32.13	34.89	31.3	33.47	33.72	32.39	38.45
22 - 23	*	31.27	33.14	30.91	33.68	30.14	35.71	32.23	32.42	37.83
23 - 24	*	32.26	31.88	30.11	32.16	31.84	32.21	31.59	31.98	36.92
Totals	34.1	32.7	34.1	32.9	34.8	33.3	35			
% of Total	14.39%	13.8%	14.39%	13.89%	14.69%	14.06%	14.77%			

Incoming Weekly EightyFifthSpeeds

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	35	37	37	40	32.7	36.33	36.35	36.33
1 - 2	*	*	36	34	42	39	36	37.33	37.5	37.33
2 - 3	*	*	31	32	37	35	29	33.33	32	33.33
3 - 4	*	*	34	34	34	34	47	34	40.5	34
4 - 5	*	*	34	26	0	30	32	30	31	30
5 - 6	*	*	35	38	33	35	36	35.33	35.5	35.33
6 - 7	*	*	40	38	38	40	34	38.67	37	38.67
7 - 8	*	*	37	39	40	42	34	38.67	38	38.67
8 - 9	*	*	38.1	37.7	39.3	42.3	38.5	38.37	40.4	38.37
9 - 10	*	*	37.9	39	41	41	38.2	39.3	39.6	39.3
10 - 11	*	*	38.8	38	40.4	41	39.5	39.07	40.25	39.07
11 - 12	*	*	40.5	38.5	39.4	38	39.7	39.47	38.85	39.47
12 - 13	*	*	40.5	38.2	40	39.7	40.9	39.57	40.3	39.57
13 - 14	*	*	39.1	37.6	40.7	39.8	39.8	39.13	39.8	39.13
14 - 15	*	*	40.1	38.5	40.9	37.4	41.2	39.83	39.3	39.83
15 - 16	*	38.5	39.5	38.5	40.6	38.4	40.9	39.28	39.65	39.28
16 - 17	*	39.3	39.1	39.1	39.9	38.1	39.6	39.35	38.85	39.35
17 - 18	*	38.2	41.3	38.6	40.7	37.7	41.4	39.7	39.55	39.7
18 - 19	*	38.9	41	37.5	40.1	39.5	42.8	39.38	41.15	39.38
19 - 20	*	37.5	39.7	38.5	40.7	39.5	41.6	39.1	40.55	39.1
20 - 21	*	39.5	40.5	38	40.8	38.2	39.6	39.7	38.9	39.7
21 - 22	*	38	39.2	37	39.6	38	39.8	38.45	38.9	38.45
22 - 23	*	37	40	34.8	39.5	38	42	37.83	40	37.83
23 - 24	*	36.7	37	36	38	37	39	36.92	38	36.92
Totals	0	343.6	914.3	883.5	902.6	918.6	925.2			
% of Total	0%	7.03%	18.71%	18.08%	18.47%	18.79%	18.93%			

Incoming Weekly EightyFifthSpeeds
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	39	*	*	*	*	*	*	39	0	39
1 - 2	39.5	*	*	*	*	*	*	39.5	0	39.5
2 - 3	36	*	*	*	*	*	*	36	0	36
3 - 4	37	*	*	*	*	*	*	37	0	37
4 - 5	40	*	*	*	*	*	*	40	0	40
5 - 6	39.5	*	*	*	*	*	*	39.5	0	39.5
6 - 7	39	*	*	*	*	*	*	39	0	39
7 - 8	39.5	*	*	*	*	*	*	39.5	0	39.5
8 - 9	41	*	*	*	*	*	*	41	0	41
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	*	*	*	0	0	0
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	350.5	0	0	0	0	0	0			
% of Total	100%	0%	0%	0%	0%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	39	*	35	37	37	40	32.7	37	36.35	37
1 - 2	39.5	*	36	34	42	39	36	37.88	37.5	37.88
2 - 3	36	*	31	32	37	35	29	34	32	34
3 - 4	37	*	34	34	34	34	47	34.75	40.5	34.75
4 - 5	40	*	34	26	0	30	32	33.33	31	33.33
5 - 6	39.5	*	35	38	33	35	36	36.38	35.5	36.38
6 - 7	39	*	40	38	38	40	34	38.75	37	38.75
7 - 8	39.5	*	37	39	40	42	34	38.88	38	38.88
8 - 9	41	*	38.1	37.7	39.3	42.3	38.5	39.02	40.4	39.02
9 - 10	*	*	37.9	39	41	41	38.2	39.3	39.6	39.3
10 - 11	*	*	38.8	38	40.4	41	39.5	39.07	40.25	39.07
11 - 12	*	*	40.5	38.5	39.4	38	39.7	39.47	38.85	39.47
12 - 13	*	*	40.5	38.2	40	39.7	40.9	39.57	40.3	39.57
13 - 14	*	*	39.1	37.6	40.7	39.8	39.8	39.13	39.8	39.13
14 - 15	*	*	40.1	38.5	40.9	37.4	41.2	39.83	39.3	39.83
15 - 16	*	38.5	39.5	38.5	40.6	38.4	40.9	39.28	39.65	39.28
16 - 17	*	39.3	39.1	39.1	39.9	38.1	39.6	39.35	38.85	39.35
17 - 18	*	38.2	41.3	38.6	40.7	37.7	41.4	39.7	39.55	39.7
18 - 19	*	38.9	41	37.5	40.1	39.5	42.8	39.38	41.15	39.38
19 - 20	*	37.5	39.7	38.5	40.7	39.5	41.6	39.1	40.55	39.1
20 - 21	*	39.5	40.5	38	40.8	38.2	39.6	39.7	38.9	39.7
21 - 22	*	38	39.2	37	39.6	38	39.8	38.45	38.9	38.45
22 - 23	*	37	40	34.8	39.5	38	42	37.83	40	37.83
23 - 24	*	36.7	37	36	38	37	39	36.92	38	36.92

Summary of Violators

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	101	31.3	0	0.0
01:00:00	50	34.4	0	0.0
02:00:00	39	30.2	0	0.0
03:00:00	21	30.0	0	0.0
04:00:00	13	28.3	0	0.0
05:00:00	41	31.1	0	0.0
06:00:00	57	31.9	0	0.0
07:00:00	139	33.4	0	0.0
08:00:00	538	33.5	0	0.0
09:00:00	374	34.0	0	0.0
10:00:00	424	34.3	0	0.0
11:00:00	446	33.5	0	0.0
12:00:00	474	34.4	0	0.0
13:00:00	602	34.1	0	0.0
14:00:00	683	34.4	0	0.0
15:00:00	906	33.9	0	0.0
16:00:00	950	33.4	0	0.0
17:00:00	1090	34.5	0	0.0
18:00:00	1081	34.5	0	0.0
19:00:00	927	34.1	0	0.0
20:00:00	592	34.4	0	0.0
21:00:00	501	33.3	0	0.0
22:00:00	297	32.3	0	0.0
23:00:00	155	31.7	0	0.0

Incoming Histogram

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	15:00	8	1	7	12	40	36	13	2	0	0	0	0	0	0	0	0	0	0	0	119	32.4	38.5	29 to 39	69.7	*	*	1	115	3
9/10/2024	16:00	12	4	1	16	43	53	26	1	0	0	0	0	0	0	0	0	0	0	0	156	32.7	39.3	31 to 41	70.5	*	*	0	150	6
9/10/2024	17:00	12	4	6	22	61	87	22	1	0	0	0	0	0	0	0	0	0	0	0	215	32.7	38.2	30 to 40	72.1	*	*	1	211	3
9/10/2024	18:00	11	0	1	15	60	65	23	3	0	0	0	0	0	0	0	0	0	0	0	178	33.3	38.9	30 to 40	74.7	*	*	0	175	3
9/10/2024	19:00	16	2	3	22	60	61	12	1	0	0	0	0	0	0	0	0	0	0	0	177	31.7	37.5	29 to 39	73.4	*	*	0	175	2
9/10/2024	20:00	7	2	2	7	34	40	14	2	2	0	0	0	0	0	0	0	0	0	0	110	33.7	39.5	30 to 40	70.9	*	*	0	107	3
9/10/2024	21:00	1	0	1	4	32	21	5	0	0	0	0	0	0	0	0	0	0	0	0	64	33.9	38	30 to 40	87.5	*	*	0	63	1
9/10/2024	22:00	6	0	1	9	17	17	5	1	0	0	0	0	0	0	0	0	0	0	0	56	31.3	37	26 to 36	69.6	*	*	0	55	1
9/10/2024	23:00	0	2	1	3	8	7	2	0	0	0	0	0	0	0	0	0	0	0	0	23	32.3	36.7	27 to 37	78.3	*	*	0	23	0
24 Hr Summary		73	15	23	110	355	387	122	11	2	0	0	0	0	0	0	0	0	0	1098	32.7	39	29 to 39	71.2	*	*	2	1074	22	

Incoming Histogram

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/11/2024	00:00	1	1	1	6	4	1	2	0	0	0	0	0	0	0	0	0	0	0	16	28.6	35	23 to 33	68.8	*	*	0	16	0
9/11/2024	01:00	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	3	30.3	36	21 to 31	66.7	*	*	0	3	0
9/11/2024	02:00	1	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	5	26.8	31	21 to 31	60.0	*	*	0	5	0
9/11/2024	03:00	1	0	1	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	5	28	34	30 to 40	60.0	*	*	0	5	0
9/11/2024	04:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	34	34	24 to 34	100.0	*	*	0	1	0
9/11/2024	05:00	1	0	0	4	4	1	2	0	0	0	0	0	0	0	0	0	0	0	12	30.4	35	25 to 35	75.0	*	*	0	10	2
9/11/2024	06:00	0	0	1	1	3	2	2	0	0	0	0	0	0	0	0	0	0	0	9	33.6	40	30 to 40	66.7	*	*	0	9	0
9/11/2024	07:00	1	0	0	2	11	11	0	0	0	0	0	0	0	0	0	0	0	0	25	33.2	37	29 to 39	92.0	*	*	0	25	0
9/11/2024	08:00	6	3	13	31	40	43	15	2	0	0	0	0	0	0	0	0	0	0	153	31.7	38.1	28 to 38	61.4	*	*	0	150	3
9/11/2024	09:00	2	1	5	8	19	28	6	2	1	0	0	0	0	0	0	0	0	0	72	33.3	37.9	28 to 38	69.4	*	*	1	66	5
9/11/2024	10:00	2	0	2	11	31	26	11	1	0	0	0	0	0	0	0	0	0	0	84	33.7	38.8	30 to 40	77.4	*	*	0	80	4
9/11/2024	11:00	5	1	3	7	20	19	15	2	0	0	0	0	0	0	0	0	0	0	72	33	40.5	30 to 40	61.1	*	*	0	69	3
9/11/2024	12:00	0	0	0	4	27	24	10	4	0	0	0	0	0	0	0	0	0	0	69	35.9	40.5	30 to 40	78.3	*	*	0	60	9
9/11/2024	13:00	3	0	4	16	33	39	17	1	0	0	0	0	0	0	0	0	0	0	113	33.8	39.1	29 to 39	71.7	*	*	1	105	7
9/11/2024	14:00	6	3	5	10	30	57	22	3	0	0	0	0	0	0	0	0	0	0	136	34.1	40.1	31 to 41	70.6	*	*	0	126	10
9/11/2024	15:00	8	1	4	21	55	60	28	3	0	0	0	0	0	0	0	0	0	0	180	33.6	39.5	31 to 41	70.0	*	*	0	176	4
9/11/2024	16:00	7	4	2	9	40	67	23	1	0	0	0	0	0	0	0	0	0	0	153	34	39.1	30 to 40	74.5	*	*	0	152	1
9/11/2024	17:00	6	2	2	9	45	77	44	10	0	0	0	0	0	0	0	0	0	0	195	35.9	41.3	31 to 41	74.4	*	*	0	193	2
9/11/2024	18:00	7	0	4	16	52	100	47	12	0	0	0	0	0	0	0	0	0	0	238	35.7	41	33 to 43	71.4	*	*	0	233	5
9/11/2024	19:00	8	2	6	18	61	77	38	4	0	0	0	0	0	0	0	0	0	0	214	34.2	39.7	30 to 40	71.0	*	*	1	211	2
9/11/2024	20:00	2	1	2	4	25	52	22	4	0	0	0	0	0	0	0	0	0	0	112	36	40.5	31 to 41	77.7	*	*	0	108	4
9/11/2024	21:00	4	1	3	10	31	33	15	1	0	0	0	0	0	0	0	0	0	0	98	33.5	39.2	30 to 40	71.4	*	*	0	97	1
9/11/2024	22:00	0	3	3	3	14	13	5	2	0	0	0	0	0	0	0	0	0	0	43	33.1	40	29 to 39	65.1	*	*	0	42	1
9/11/2024	23:00	1	0	2	3	8	9	1	0	0	0	0	0	0	0	0	0	0	0	24	31.9	37	28 to 38	79.2	*	*	0	24	0
24 Hr Summary		72	23	64	194	559	741	326	52	1	0	0	0	0	0	0	0	0	0	2032	34.1	40	31 to 41	69.2	*	*	3	1966	63

Incoming Histogram
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/12/2024	00:00	0	0	0	1	6	5	1	0	0	0	0	0	0	0	0	0	0	0	13	34.6	37	28 to 38	92.3	*	*	0	13	0
9/12/2024	01:00	1	0	0	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	6	28.8	34	25 to 35	83.3	*	*	0	5	1
9/12/2024	02:00	0	0	2	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	5	27.8	32	22 to 32	80.0	*	*	0	5	0
9/12/2024	03:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	34	34	24 to 34	100.0	*	*	0	1	0
9/12/2024	04:00	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	18.5	26	1 to 11	50.0	*	*	0	2	0
9/12/2024	05:00	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	3	34.7	38	28 to 38	100.0	*	*	0	2	1
9/12/2024	06:00	1	0	0	3	4	5	1	0	0	0	0	0	0	0	0	0	0	0	14	32.6	38	28 to 38	78.6	*	*	0	12	2
9/12/2024	07:00	1	0	0	2	7	10	1	2	0	0	0	0	0	0	0	0	0	0	23	34.1	39	29 to 39	73.9	*	*	0	21	2
9/12/2024	08:00	4	3	3	27	55	55	14	0	0	0	0	0	0	0	0	0	0	0	161	32.7	37.7	29 to 39	74.5	*	*	0	157	4
9/12/2024	09:00	2	4	6	8	32	30	13	1	0	0	0	0	0	0	0	0	0	0	96	32.8	39	31 to 41	70.8	*	*	2	83	11
9/12/2024	10:00	2	2	2	8	25	26	6	2	0	0	0	0	0	0	0	0	0	0	73	33.2	38	28 to 38	74.0	*	*	0	70	3
9/12/2024	11:00	1	2	2	14	27	22	11	0	0	0	0	0	0	0	0	0	0	0	79	32.9	38.5	27 to 37	73.4	*	*	0	74	5
9/12/2024	12:00	3	1	3	14	31	27	8	1	1	0	0	0	0	0	0	0	0	0	89	32.7	38.2	27 to 37	73.0	*	*	2	82	5
9/12/2024	13:00	6	4	5	18	28	35	9	0	0	0	0	0	0	0	0	0	0	0	105	31	37.6	29 to 39	63.8	*	*	0	100	5
9/12/2024	14:00	1	2	4	23	44	28	14	0	0	0	0	0	0	0	0	0	0	0	116	32.7	38.5	27 to 37	69.8	*	*	0	113	3
9/12/2024	15:00	2	2	6	20	52	63	18	3	0	0	0	0	0	0	0	0	0	0	166	33.6	38.5	30 to 40	73.5	*	*	0	162	4
9/12/2024	16:00	10	3	7	18	63	53	26	2	0	0	0	0	0	0	0	0	0	0	182	32.7	39.1	31 to 41	70.3	*	*	0	177	5
9/12/2024	17:00	8	0	4	22	60	90	24	3	0	0	0	0	0	0	0	0	0	0	211	33.9	38.6	30 to 40	76.3	*	*	0	206	5
9/12/2024	18:00	8	3	7	17	75	73	14	2	0	0	0	0	0	0	0	0	0	0	199	32.7	37.5	29 to 39	76.4	*	*	0	197	2
9/12/2024	19:00	3	2	4	18	55	74	18	3	0	0	0	0	0	0	0	0	0	0	177	34.2	38.5	29 to 39	76.8	*	*	0	175	2
9/12/2024	20:00	5	2	2	10	31	51	9	2	0	0	0	0	0	0	0	0	0	0	112	33.2	38	30 to 40	77.7	*	*	0	110	2
9/12/2024	21:00	3	1	0	14	29	24	4	1	0	0	0	0	0	0	0	0	0	0	76	32.1	37	28 to 38	75.0	*	*	0	74	2
9/12/2024	22:00	2	0	6	9	25	10	3	0	0	0	0	0	0	0	0	0	0	0	55	30.9	34.8	25 to 35	72.7	*	*	0	55	0
9/12/2024	23:00	3	0	0	6	10	8	0	0	0	0	0	0	0	0	0	0	0	0	27	30.1	36	27 to 37	77.8	*	*	0	27	0
24 Hr Summary		67	31	63	254	666	693	194	22	1	0	0	0	0	0	0	0	0	0	1991	32.9	39	29 to 39	72.1	*	*	4	1923	64

Incoming Histogram
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/13/2024	00:00	3	0	1	5	5	2	2	0	0	0	0	0	0	0	0	0	0	0	18	28.3	37	23 to 33	55.6	*	*	0	17	1	
9/13/2024	01:00	0	0	0	1	2	2	3	0	0	0	0	0	0	0	0	0	0	0	8	36.4	42	33 to 43	75.0	*	*	0	7	1	
9/13/2024	02:00	1	0	0	1	3	2	1	0	0	0	0	0	0	0	0	0	0	0	8	31.1	37	27 to 37	75.0	*	*	0	8	0	
9/13/2024	03:00	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	29.7	34	24 to 34	100.0	*	*	0	3	0	
9/13/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/13/2024	05:00	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	3	31.7	33	23 to 33	100.0	*	*	0	3	0	
9/13/2024	06:00	1	0	1	3	3	3	0	1	0	0	0	0	0	0	0	0	0	0	12	31.3	38	28 to 38	66.7	*	*	0	10	2	
9/13/2024	07:00	1	4	1	5	8	7	5	1	0	0	0	0	0	0	0	0	0	0	32	31.7	40	28 to 38	56.3	*	*	0	31	1	
9/13/2024	08:00	0	1	2	17	48	55	19	5	0	0	0	0	0	0	0	0	0	0	147	34.8	39.3	29 to 39	74.1	*	*	0	144	3	
9/13/2024	09:00	1	1	2	8	21	25	16	3	0	0	0	0	0	0	0	0	0	0	77	35.2	41	32 to 42	67.5	*	*	1	73	3	
9/13/2024	10:00	3	1	2	6	16	26	20	3	0	0	0	0	0	0	0	0	0	0	77	35.2	40.4	31 to 41	71.4	*	*	0	72	5	
9/13/2024	11:00	5	0	7	13	27	40	14	5	0	0	0	0	0	0	0	0	0	0	111	33.5	39.4	31 to 41	66.7	*	*	0	104	7	
9/13/2024	12:00	6	1	0	4	22	49	15	4	0	0	0	0	0	0	0	0	0	0	101	34.8	40	31 to 41	75.2	*	*	1	95	5	
9/13/2024	13:00	7	2	2	7	36	58	34	3	0	0	0	0	0	0	0	0	0	0	149	34.9	40.7	31 to 41	72.5	*	*	0	147	2	
9/13/2024	14:00	5	3	5	5	37	88	37	10	2	0	0	0	0	0	0	0	0	0	192	35.8	40.9	31 to 41	72.4	*	*	0	185	7	
9/13/2024	15:00	7	1	3	6	53	75	41	6	0	0	0	0	0	0	0	0	0	0	192	35.5	40.6	31 to 41	76.6	*	*	0	190	2	
9/13/2024	16:00	5	3	4	15	50	78	30	8	0	0	0	0	0	0	0	0	0	0	193	34.8	39.9	31 to 41	73.6	*	*	0	186	7	
9/13/2024	17:00	9	2	1	19	51	82	53	6	1	0	0	0	0	0	0	0	0	0	224	35.3	40.7	32 to 42	72.3	*	*	0	221	3	
9/13/2024	18:00	10	4	2	18	59	101	55	7	0	0	0	0	0	0	0	0	0	0	256	35	40.1	31 to 41	72.7	*	*	1	250	5	
9/13/2024	19:00	8	1	6	16	41	56	39	8	1	0	0	0	0	0	0	0	0	0	176	34.6	40.7	31 to 41	68.2	*	*	0	172	4	
9/13/2024	20:00	4	1	0	7	28	45	18	4	0	0	0	0	0	0	0	0	0	0	107	35.1	40.8	30 to 40	70.1	*	*	1	105	1	
9/13/2024	21:00	4	1	0	11	22	50	18	4	0	0	0	0	0	0	0	0	0	0	110	34.9	39.6	31 to 41	76.4	*	*	0	109	1	
9/13/2024	22:00	2	1	3	4	19	21	7	3	0	0	0	0	0	0	0	0	0	0	60	33.7	39.5	28 to 38	71.7	*	*	0	60	0	
9/13/2024	23:00	1	2	0	5	9	10	3	1	0	0	0	0	0	0	0	0	0	0	31	32.2	38	29 to 39	67.7	*	*	0	31	0	
24 Hr Summary		83	29	42	177	565	875	430	82	4	0	0	0	0	0	0	0	0	0	2287	34.8	41	31 to 41	71.0	*	*	4	2223	60	

Incoming Histogram
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/14/2024	00:00	1	0	1	1	4	8	4	0	0	0	0	0	0	0	0	0	0	0	19	34.4	40	33 to 43	84.2	*	*	0	18	1
9/14/2024	01:00	0	0	0	0	3	9	1	1	0	0	0	0	0	0	0	0	0	0	14	36.6	39	30 to 40	92.9	*	*	0	14	0
9/14/2024	02:00	0	1	2	1	5	3	0	0	0	0	0	0	0	0	0	0	0	0	12	30.4	35	27 to 37	66.7	*	*	0	12	0
9/14/2024	03:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	33.5	34	24 to 34	100.0	*	*	0	2	0
9/14/2024	04:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	30	30	20 to 30	100.0	*	*	0	1	0
9/14/2024	05:00	2	1	0	1	4	3	1	0	0	0	0	0	0	0	0	0	0	0	12	28.8	35	27 to 37	66.7	*	*	0	12	0
9/14/2024	06:00	1	0	0	1	2	3	2	0	0	0	0	0	0	0	0	0	0	0	9	32.1	40	30 to 40	66.7	*	*	0	9	0
9/14/2024	07:00	1	1	0	0	8	6	4	2	0	0	0	0	0	0	0	0	0	0	22	34.5	42	30 to 40	72.7	*	*	0	19	3
9/14/2024	08:00	1	0	0	2	13	17	7	2	1	0	0	0	0	0	0	0	0	0	43	36.3	42.3	31 to 41	76.7	*	*	0	42	1
9/14/2024	09:00	5	0	0	7	8	24	24	4	0	0	0	0	0	0	0	0	0	0	72	35.2	41	35 to 45	69.4	*	*	1	68	3
9/14/2024	10:00	5	0	1	6	21	32	17	6	1	0	0	0	0	0	0	0	0	0	89	35.3	41	33 to 43	68.5	*	*	1	86	2
9/14/2024	11:00	2	2	2	8	30	30	6	2	0	0	0	0	0	0	0	0	0	0	82	33.6	38	31 to 41	79.3	*	*	0	77	5
9/14/2024	12:00	2	1	2	10	28	25	16	1	0	0	0	0	0	0	0	0	0	0	85	33.9	39.7	28 to 38	70.6	*	*	2	79	4
9/14/2024	13:00	2	1	1	12	39	43	16	5	0	0	0	0	0	0	0	0	0	0	119	34.7	39.8	30 to 40	72.3	*	*	0	115	4
9/14/2024	14:00	4	3	7	14	38	44	10	0	0	0	0	0	0	0	0	0	0	0	120	32.3	37.4	30 to 40	74.2	*	*	1	112	7
9/14/2024	15:00	7	4	4	16	40	46	14	1	0	0	0	0	0	0	0	0	0	0	132	32.4	38.4	29 to 39	67.4	*	*	0	128	4
9/14/2024	16:00	9	3	5	18	48	47	11	1	0	0	0	0	0	0	0	0	0	0	142	31.8	38.1	29 to 39	70.4	*	*	0	138	4
9/14/2024	17:00	3	1	7	12	33	50	9	0	0	0	0	0	0	0	0	0	0	0	115	32.9	37.7	29 to 39	75.7	*	*	0	114	1
9/14/2024	18:00	7	3	1	11	30	36	15	3	0	0	0	0	0	0	0	0	0	0	106	33.1	39.5	31 to 41	67.9	*	*	0	105	1
9/14/2024	19:00	3	0	2	8	31	29	14	1	0	0	0	0	0	0	0	0	0	0	88	34	39.5	30 to 40	72.7	*	*	0	87	1
9/14/2024	20:00	0	1	1	6	33	30	7	2	0	0	0	0	0	0	0	0	0	0	80	34.3	38.2	30 to 40	83.8	*	*	0	80	0
9/14/2024	21:00	6	1	6	10	22	25	5	1	0	0	0	0	0	0	0	0	0	0	76	31.3	38	31 to 41	67.1	*	*	0	76	0
9/14/2024	22:00	3	1	7	9	12	14	3	0	0	0	0	0	0	0	0	0	0	0	49	30.1	38	26 to 36	55.1	*	*	0	49	0
9/14/2024	23:00	1	0	2	6	12	7	2	1	0	0	0	0	0	0	0	0	0	0	31	31.8	37	25 to 35	71.0	*	*	0	31	0
24 Hr Summary		65	24	51	159	467	531	188	33	2	0	0	0	0	0	0	0	0	0	1520	33.3	39	30 to 40	69.7	*	*	5	1474	41

Incoming Histogram
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/15/2024	00:00	2	0	2	6	10	1	0	0	0	0	0	0	0	0	0	0	0	0	21	27.9	32.7	24 to 34	81.0	*	*	0	21	0
9/15/2024	01:00	0	0	0	0	5	2	0	1	0	0	0	0	0	0	0	0	0	0	8	34.9	36	26 to 36	87.5	*	*	0	8	0
9/15/2024	02:00	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	0	0	0	4	29.8	29	19 to 29	75.0	*	*	0	4	0
9/15/2024	03:00	0	0	0	1	1	0	0	1	0	0	0	0	0	0	0	0	0	0	3	35	47	21 to 31	66.7	*	*	0	2	1
9/15/2024	04:00	1	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	0	5	26.8	32	23 to 33	80.0	*	*	0	5	0
9/15/2024	05:00	1	0	0	0	3	2	1	0	0	0	0	0	0	0	0	0	0	0	7	30.7	36	26 to 36	71.4	*	*	0	6	1
9/15/2024	06:00	0	0	0	1	2	0	1	0	0	0	0	0	0	0	0	0	0	0	4	34.3	34	24 to 34	75.0	*	*	0	4	0
9/15/2024	07:00	0	0	1	1	7	2	0	0	0	0	0	0	0	0	0	0	0	0	11	31.8	34	24 to 34	81.8	*	*	0	11	0
9/15/2024	08:00	0	1	0	4	2	12	0	2	0	0	0	0	0	0	0	0	0	0	21	34.5	38.5	29 to 39	71.4	*	*	0	20	1
9/15/2024	09:00	2	0	0	7	21	22	4	1	0	0	0	0	0	0	0	0	0	0	57	33.4	38.2	29 to 39	77.2	*	*	1	54	2
9/15/2024	10:00	5	0	2	2	34	40	17	1	0	0	0	0	0	0	0	0	0	0	101	34.2	39.5	30 to 40	79.2	*	*	1	100	0
9/15/2024	11:00	4	1	3	8	27	36	22	1	0	0	0	0	0	0	0	0	0	0	102	34.2	39.7	31 to 41	74.5	*	*	0	101	1
9/15/2024	12:00	4	5	0	8	42	40	23	8	0	0	0	0	0	0	0	0	0	0	130	34.6	40.9	31 to 41	67.7	*	*	1	127	2
9/15/2024	13:00	3	1	1	7	32	48	22	1	1	0	0	0	0	0	0	0	0	0	116	35.3	39.8	32 to 42	79.3	*	*	0	115	1
9/15/2024	14:00	0	2	0	6	33	49	25	4	0	0	0	0	0	0	0	0	0	0	119	36.2	41.2	30 to 40	73.1	*	*	0	116	3
9/15/2024	15:00	5	0	1	8	24	42	34	3	0	0	0	0	0	0	0	0	0	0	117	35.6	40.9	33 to 43	75.2	*	*	1	115	1
9/15/2024	16:00	6	3	2	4	33	52	20	4	0	0	0	0	0	0	0	0	0	0	124	34.6	39.6	30 to 40	75.8	*	*	0	123	1
9/15/2024	17:00	4	1	1	3	21	66	26	8	0	0	0	0	0	0	0	0	0	0	130	36.2	41.4	32 to 42	78.5	*	*	0	127	3
9/15/2024	18:00	2	0	0	3	20	42	27	9	1	0	0	0	0	0	0	0	0	0	104	37.2	42.8	34 to 44	71.2	*	*	0	103	1
9/15/2024	19:00	1	2	1	3	14	41	26	6	1	0	0	0	0	0	0	0	0	0	95	37.1	41.6	32 to 42	76.8	*	*	0	95	0
9/15/2024	20:00	5	0	1	4	25	22	12	1	1	0	0	0	0	0	0	0	0	0	71	33.8	39.6	30 to 40	73.2	*	*	2	68	1
9/15/2024	21:00	5	2	2	5	22	26	14	1	0	0	0	0	0	0	0	0	0	0	77	33.5	39.8	32 to 42	70.1	*	*	0	77	0
9/15/2024	22:00	1	0	1	3	7	12	9	1	0	0	0	0	0	0	0	0	0	0	34	35.7	42	33 to 43	73.5	*	*	0	34	0
9/15/2024	23:00	1	0	1	4	3	7	3	0	0	0	0	0	0	0	0	0	0	0	19	32.2	39	27 to 37	57.9	*	*	0	19	0
24 Hr Summary		52	18	19	92	391	565	286	53	4	0	0	0	0	0	0	0	0	0	1480	35	41	31 to 41	71.9	*	*	6	1455	19

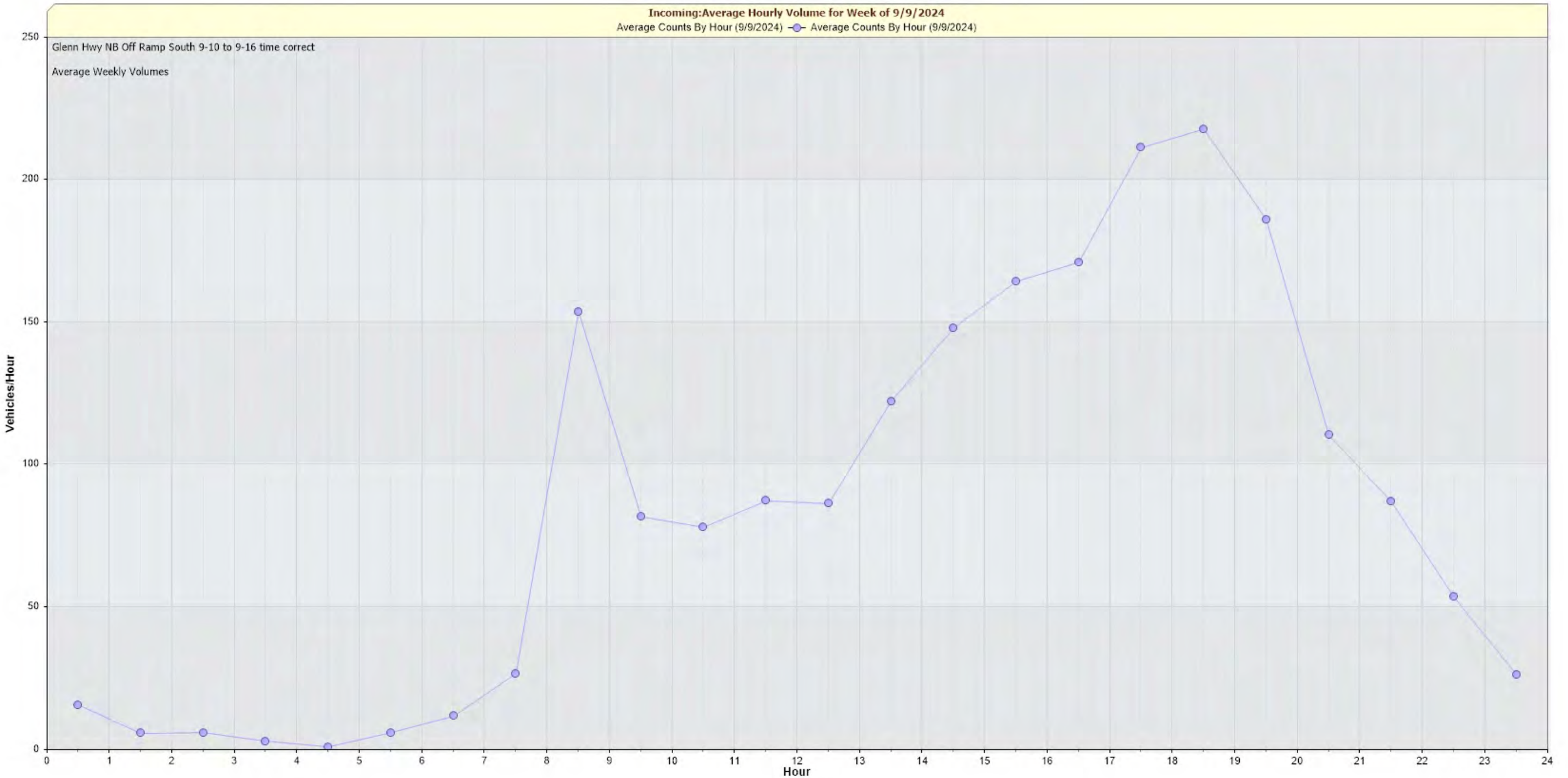
Incoming Histogram
 Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

from Tue-Sep-10-2024-03-00-PM to Mon-Sep-16-2024-08-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	0	0	0	1	2	9	2	0	0	0	0	0	0	0	0	0	0	0	14	36	39	31 to 41	92.9	*	*	0	14	0	
9/16/2024	01:00	0	1	1	0	1	5	3	0	0	0	0	0	0	0	0	0	0	0	11	33.9	39.5	30 to 40	72.7	*	*	0	11	0	
9/16/2024	02:00	0	0	0	1	2	1	1	0	0	0	0	0	0	0	0	0	0	0	5	34.4	36	26 to 36	80.0	*	*	0	4	1	
9/16/2024	03:00	0	3	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	0	7	27.7	37	28 to 38	57.1	*	*	0	7	0	
9/16/2024	04:00	0	0	1	1	0	0	2	0	0	0	0	0	0	0	0	0	0	0	4	33.3	40	16 to 26	50.0	*	*	0	3	1	
9/16/2024	05:00	0	0	0	0	0	2	2	0	0	0	0	0	0	0	0	0	0	0	4	38.3	39.5	30 to 40	100.0	*	*	0	4	0	
9/16/2024	06:00	2	0	0	2	2	2	0	1	0	0	0	0	0	0	0	0	0	0	9	28.7	39	25 to 35	55.6	*	*	0	9	0	
9/16/2024	07:00	1	0	0	2	10	8	5	0	0	0	0	0	0	0	0	0	0	0	26	34.7	39.5	31 to 41	88.5	*	*	0	25	1	
9/16/2024	08:00	0	0	0	3	1	4	4	0	1	0	0	0	0	0	0	0	0	0	13	37	41	32 to 42	69.2	*	*	0	13	0	
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		3	4	2	10	19	34	19	1	1	0	0	0	0	0	0	0	0	0	93	34.1	40	31 to 41	73.1	*	*	0	90	3	

Incoming: Average Hourly Volume for Week of 9/9/2024
Average Counts By Hour (9/9/2024) — Average Counts By Hour (9/9/2024)

Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct
Average Weekly Volumes



Incoming: Average Hourly WEEKDAY Speeds for Week of 9/9/2024
Average Hourly WEEKDAY Speeds By Hour (9/9/2024) — Average Hourly WEEKDAY Speeds By Hour (9/9/2024)

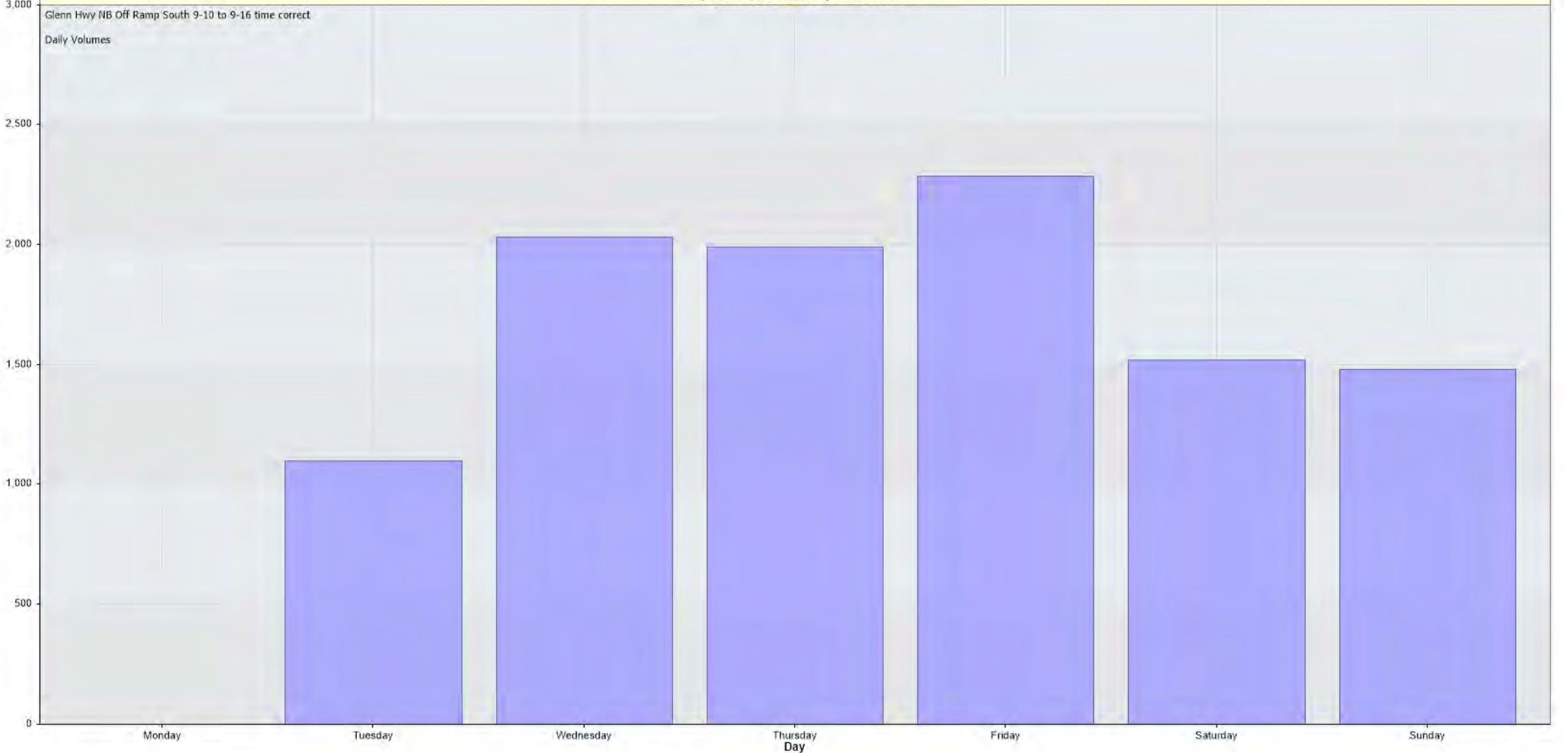
Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct
Average Weekly Speeds



Incoming:Daily Volume for Week of 9/9/2024

Daily Vehicle Counts

Daily Vehicle Counts



Glenn Hwy NB Off Ramp South 9-10 to 9-16 time correct

Daily Volumes

Vehicles

Day

For Project: Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:52:29 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/10/2024 2:00:00 PM through 9/15/2024 2:59:59 PM

85th Percentile Speed 40 MPH

85th Percentile Vehicles 5006

Max Speed 64 MPH on 9/13/2024 10:45:18 PM

Total Vehicles 5890

AADT: 1168

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1141	981
AM Peak 7:00 AM	110	76
PM Peak 2:00 PM	117	105

Speed

Speed Limit: 45

85th Percentile Speed: 40

50th Percentile Speed: 34

10 MPH Pace Interval: 31.0 MPH to 41.0 MPH

Average Speed: 32.94

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	N/A	8	15	20	26	15	12
% over limit	N/A	1.1	1.1	1.7	1.9	1.6	2.9
Avg Speeder	N/A	47.8	47.6	47.5	48.2	48.3	46.8
Avg Speed	N/A	32.4	32.9	33.0	32.5	33.4	34.4

Class Counts

	Number	%
VEH_SM	2	0
VEH_MED	5511	93.6
VEH_LG	377	6.4
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Incoming Summary
Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/10/2024 3:00:00 PM	32.0	105	123	40	0.0	0.0%	26.5
9/10/2024 4:00:00 PM	38.0	94	110	45	0.0	0.0%	31.4
9/10/2024 5:00:00 PM	40.0	116	136	50	49.3	2.2%	33.4
9/10/2024 6:00:00 PM	41.0	96	113	47	46.5	1.8%	34.8
9/10/2024 7:00:00 PM	40.0	64	75	44	0.0	0.0%	33.5
9/10/2024 8:00:00 PM	42.0	59	69	49	47.5	2.9%	35.2
9/10/2024 9:00:00 PM	41.0	39	46	46	46.0	2.2%	35.5
9/10/2024 10:00:00 PM	37.0	23	27	40	0.0	0.0%	32.3
9/10/2024 11:00:00 PM	42.0	6	7	45	0.0	0.0%	37.7
9/11/2024 12:00:00 AM	41.0	3	4	43	0.0	0.0%	35.0
9/11/2024 1:00:00 AM	31.0	4	5	42	0.0	0.0%	28.6
9/11/2024 2:00:00 AM	32.0	4	5	45	0.0	0.0%	27.2
9/11/2024 3:00:00 AM	43.0	3	3	43	0.0	0.0%	39.3
9/11/2024 4:00:00 AM	37.0	1	1	37	0.0	0.0%	37.0
9/11/2024 5:00:00 AM	35.0	4	5	38	0.0	0.0%	29.6
9/11/2024 6:00:00 AM	37.0	9	11	43	0.0	0.0%	33.6
9/11/2024 7:00:00 AM	39.0	36	42	45	0.0	0.0%	36.3
9/11/2024 8:00:00 AM	38.0	103	121	45	0.0	0.0%	30.7
9/11/2024 9:00:00 AM	40.0	70	82	45	0.0	0.0%	32.5
9/11/2024 10:00:00 AM	40.0	58	68	47	47.0	1.5%	33.8
9/11/2024 11:00:00 AM	41.0	59	69	48	47.5	2.9%	34.3
9/11/2024 12:00:00 PM	40.0	60	71	46	46.0	1.4%	34.7
9/11/2024 1:00:00 PM	39.0	68	80	53	49.5	2.5%	32.1
9/11/2024 2:00:00 PM	41.0	74	87	47	46.5	2.3%	32.7
9/11/2024 3:00:00 PM	35.0	109	128	44	0.0	0.0%	28.2
9/11/2024 4:00:00 PM	38.0	80	94	44	0.0	0.0%	32.1
9/11/2024 5:00:00 PM	40.0	111	131	50	49.0	1.5%	33.9
9/11/2024 6:00:00 PM	41.0	85	100	48	47.3	3.0%	34.8
9/11/2024 7:00:00 PM	42.0	55	65	44	0.0	0.0%	34.4
9/11/2024 8:00:00 PM	40.0	45	53	47	47.0	1.9%	34.3
9/11/2024 9:00:00 PM	39.0	43	51	43	0.0	0.0%	34.4
9/11/2024 10:00:00 PM	38.0	15	18	43	0.0	0.0%	34.1
9/11/2024 11:00:00 PM	40.0	8	10	42	0.0	0.0%	36.4
9/12/2024 12:00:00 AM	35.0	5	6	47	47.0	16.7%	35.3
9/12/2024 1:00:00 AM	34.0	3	3	34	0.0	0.0%	27.0
9/12/2024 2:00:00 AM	46.0	3	3	46	46.0	33.3%	32.0
9/12/2024 3:00:00 AM	39.0	1	1	39	0.0	0.0%	39.0
9/12/2024 4:00:00 AM	15.0	1	1	15	0.0	0.0%	15.0
9/12/2024 5:00:00 AM	42.0	5	6	44	0.0	0.0%	36.5
9/12/2024 6:00:00 AM	39.0	8	9	44	0.0	0.0%	32.7
9/12/2024 7:00:00 AM	42.0	44	52	48	47.5	3.8%	35.8
9/12/2024 8:00:00 AM	36.0	93	109	49	49.0	0.9%	29.3
9/12/2024 9:00:00 AM	38.0	71	84	45	0.0	0.0%	32.0
9/12/2024 10:00:00 AM	41.0	53	62	51	48.5	3.2%	34.5
9/12/2024 11:00:00 AM	40.0	61	72	50	48.0	2.8%	33.4
9/12/2024 12:00:00 PM	39.0	65	77	45	0.0	0.0%	32.8
9/12/2024 1:00:00 PM	41.0	64	75	45	0.0	0.0%	33.3

Incoming Summary
 Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

9/12/2024 2:00:00 PM	40.0	65	77	48	47.0	2.6%	32.8
9/12/2024 3:00:00 PM	37.0	77	91	46	46.0	1.1%	28.6
9/12/2024 4:00:00 PM	40.0	62	73	47	47.0	1.4%	33.1
9/12/2024 5:00:00 PM	42.0	73	86	48	46.8	5.8%	34.5
9/12/2024 6:00:00 PM	39.0	82	96	45	0.0	0.0%	33.4
9/12/2024 7:00:00 PM	42.0	60	70	50	48.0	2.9%	35.5
9/12/2024 8:00:00 PM	41.0	37	43	45	0.0	0.0%	35.9
9/12/2024 9:00:00 PM	40.0	27	32	49	49.0	3.1%	36.6
9/12/2024 10:00:00 PM	41.0	14	17	44	0.0	0.0%	35.1
9/12/2024 11:00:00 PM	39.0	7	8	41	0.0	0.0%	33.5
9/13/2024 12:00:00 AM	35.0	2	2	35	0.0	0.0%	33.0
9/13/2024 1:00:00 AM	31.0	2	2	31	0.0	0.0%	25.5
9/13/2024 2:00:00 AM	38.0	1	1	38	0.0	0.0%	38.0
9/13/2024 3:00:00 AM	24.0	2	2	24	0.0	0.0%	20.0
9/13/2024 4:00:00 AM	39.0	2	2	39	0.0	0.0%	37.5
9/13/2024 5:00:00 AM	39.0	4	5	41	0.0	0.0%	33.4
9/13/2024 6:00:00 AM	39.0	10	12	41	0.0	0.0%	30.9
9/13/2024 7:00:00 AM	41.0	43	51	47	47.0	2.0%	36.0
9/13/2024 8:00:00 AM	37.0	87	102	43	0.0	0.0%	29.4
9/13/2024 9:00:00 AM	37.0	74	87	48	48.0	1.1%	31.2
9/13/2024 10:00:00 AM	39.0	69	81	47	46.5	2.5%	31.7
9/13/2024 11:00:00 AM	38.0	46	54	50	48.3	5.6%	32.6
9/13/2024 12:00:00 PM	40.0	74	87	46	46.0	1.1%	32.3
9/13/2024 1:00:00 PM	39.0	82	96	47	47.0	1.0%	31.7
9/13/2024 2:00:00 PM	40.0	103	121	47	47.0	2.5%	32.5
9/13/2024 3:00:00 PM	33.0	108	127	44	0.0	0.0%	27.1
9/13/2024 4:00:00 PM	40.0	111	131	49	47.7	2.3%	32.6
9/13/2024 5:00:00 PM	41.0	81	95	49	47.0	3.2%	34.8
9/13/2024 6:00:00 PM	42.0	91	107	52	49.3	2.8%	34.2
9/13/2024 7:00:00 PM	42.0	70	82	47	47.0	1.2%	36.0
9/13/2024 8:00:00 PM	41.0	54	63	49	49.0	1.6%	33.9
9/13/2024 9:00:00 PM	40.0	39	46	44	0.0	0.0%	35.2
9/13/2024 10:00:00 PM	40.0	22	26	45	0.0	0.0%	35.1
9/13/2024 11:00:00 PM	43.0	10	12	64	55.5	16.7%	38.7
9/14/2024 12:00:00 AM	43.0	6	7	46	46.0	14.3%	39.4
9/14/2024 1:00:00 AM	32.0	2	2	32	0.0	0.0%	24.0
9/14/2024 2:00:00 AM	48.0	3	3	48	48.0	33.3%	39.0
9/14/2024 3:00:00 AM	**No Data**						
9/14/2024 4:00:00 AM	43.0	2	2	43	0.0	0.0%	37.5
9/14/2024 5:00:00 AM	40.0	9	11	45	0.0	0.0%	34.9
9/14/2024 6:00:00 AM	39.0	8	10	45	0.0	0.0%	30.1
9/14/2024 7:00:00 AM	40.0	14	16	45	0.0	0.0%	33.3
9/14/2024 8:00:00 AM	40.0	24	28	45	0.0	0.0%	34.1
9/14/2024 9:00:00 AM	42.0	31	37	49	48.5	5.4%	33.7
9/14/2024 10:00:00 AM	41.0	56	66	48	48.0	3.0%	34.7
9/14/2024 11:00:00 AM	39.0	71	84	46	46.0	1.2%	32.0
9/14/2024 12:00:00 PM	40.0	71	83	50	50.0	1.2%	32.4
9/14/2024 1:00:00 PM	40.0	82	96	47	47.0	1.0%	33.1

Incoming Summary
 Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

9/14/2024 2:00:00 PM	39.0	70	82	51	51.0	1.2%	32.1
9/14/2024 3:00:00 PM	41.0	57	67	52	52.0	1.5%	34.4
9/14/2024 4:00:00 PM	42.0	42	49	46	46.0	4.1%	34.1
9/14/2024 5:00:00 PM	41.0	69	81	48	48.0	1.2%	33.9
9/14/2024 6:00:00 PM	39.0	63	74	43	0.0	0.0%	32.3
9/14/2024 7:00:00 PM	40.0	30	35	50	50.0	2.9%	34.7
9/14/2024 8:00:00 PM	40.0	27	32	45	0.0	0.0%	35.3
9/14/2024 9:00:00 PM	39.0	27	32	45	0.0	0.0%	32.6
9/14/2024 10:00:00 PM	41.0	8	9	47	47.0	11.1%	38.9
9/14/2024 11:00:00 PM	41.0	8	10	44	0.0	0.0%	34.1
9/15/2024 12:00:00 AM	37.0	8	9	38	0.0	0.0%	32.7
9/15/2024 1:00:00 AM	36.0	5	6	38	0.0	0.0%	33.2
9/15/2024 2:00:00 AM	30.0	2	2	30	0.0	0.0%	27.5
9/15/2024 3:00:00 AM	33.0	1	1	33	0.0	0.0%	33.0
9/15/2024 4:00:00 AM	39.0	2	2	39	0.0	0.0%	34.5
9/15/2024 5:00:00 AM	31.0	2	2	31	0.0	0.0%	27.0
9/15/2024 6:00:00 AM	46.0	4	5	47	46.5	40.0%	37.2
9/15/2024 7:00:00 AM	34.0	4	5	38	0.0	0.0%	29.0
9/15/2024 8:00:00 AM	41.0	11	13	43	0.0	0.0%	36.2
9/15/2024 9:00:00 AM	39.0	25	29	46	46.0	3.4%	34.8
9/15/2024 10:00:00 AM	41.0	35	41	46	46.0	2.4%	35.9
9/15/2024 11:00:00 AM	39.0	37	43	44	0.0	0.0%	32.8
9/15/2024 12:00:00 PM	40.0	52	61	46	46.0	3.3%	34.6
9/15/2024 1:00:00 PM	40.0	71	84	47	46.5	2.4%	34.9
9/15/2024 2:00:00 PM	40.0	73	86	49	48.0	3.5%	34.3
9/15/2024 3:00:00 PM	42.0	24	28	47	47.0	3.6%	34.2

Incoming Summary

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/11/2024 12:00:00 AM	40.0	604	710	50	47.8	1.1%	32.4
9/12/2024 12:00:00 AM	40.0	1110	1306	53	47.6	1.1%	32.9
9/13/2024 12:00:00 AM	40.0	977	1149	51	47.5	1.7%	33.0
9/14/2024 12:00:00 AM	40.0	1189	1399	64	48.2	1.9%	32.5
9/15/2024 12:00:00 AM	40.0	780	918	52	48.3	1.6%	33.4
9/15/2024 2:59:59 PM	40.0	347	408	49	46.8	2.9%	34.4

Incoming Weekly Counts

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	5	3	2	2	6	3.33	4	32
1 - 2	*	*	5	3	1	3	2	3	2.5	38.67
2 - 3	*	*	3	1	2	0	1	2	0.5	35.33
3 - 4	*	*	1	1	2	2	2	1.33	2	30.33
4 - 5	*	*	5	6	5	11	2	5.33	6.5	38.67
5 - 6	*	*	11	9	12	10	5	10.67	7.5	38.17
6 - 7	*	*	42	52	51	16	5	48.33	10.5	40.23
7 - 8	*	*	121	109	102	28	13	110.67	20.5	36.37
8 - 9	*	*	82	84	87	37	29	84.33	33	37.9
9 - 10	*	*	68	62	81	66	41	70.33	53.5	39.53
10 - 11	*	*	69	72	54	84	43	65	63.5	39.47
11 - 12	*	*	71	77	87	83	61	78.33	72	39.57
12 - 13	*	*	80	75	96	96	84	83.67	90	39.37
13 - 14	*	*	87	77	121	82	86	95	84	39.8
14 - 15	*	123	128	91	127	67	28	117.25	47.5	34.02
15 - 16	*	110	94	73	131	49	*	102	49	38.45
16 - 17	*	136	131	86	95	81	*	112	81	40.2
17 - 18	*	113	100	96	107	74	*	104	74	40.23
18 - 19	*	75	65	70	82	35	*	73	35	40.85
19 - 20	*	69	53	43	63	32	*	57	32	40.72
20 - 21	*	46	51	32	46	32	*	43.75	32	39.67
21 - 22	*	27	18	17	26	9	*	22	9	38.8
22 - 23	*	7	10	8	12	10	*	9.25	10	41
23 - 24	*	4	6	2	7	9	*	4.75	9	38.5
Totals	0	710	1306	1149	1399	918	408			
% of Total	0%	12.05%	22.17%	19.51%	23.75%	15.59%	6.93%			

Incoming Monthly Counts

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	5	3	2	2	6	3.33	4	32
1 - 2	*	*	5	3	1	3	2	3	2.5	38.67
2 - 3	*	*	3	1	2	0	1	2	0.5	35.33
3 - 4	*	*	1	1	2	2	2	1.33	2	30.33
4 - 5	*	*	5	6	5	11	2	5.33	6.5	38.67
5 - 6	*	*	11	9	12	10	5	10.67	7.5	38.17
6 - 7	*	*	42	52	51	16	5	48.33	10.5	40.23
7 - 8	*	*	121	109	102	28	13	110.67	20.5	36.37
8 - 9	*	*	82	84	87	37	29	84.33	33	37.9
9 - 10	*	*	68	62	81	66	41	70.33	53.5	39.53
10 - 11	*	*	69	72	54	84	43	65	63.5	39.47
11 - 12	*	*	71	77	87	83	61	78.33	72	39.57
12 - 13	*	*	80	75	96	96	84	83.67	90	39.37
13 - 14	*	*	87	77	121	82	86	95	84	39.8
14 - 15	*	123	128	91	127	67	28	117.25	47.5	34.02
15 - 16	*	110	94	73	131	49	*	102	49	38.45
16 - 17	*	136	131	86	95	81	*	112	81	40.2
17 - 18	*	113	100	96	107	74	*	104	74	40.23
18 - 19	*	75	65	70	82	35	*	73	35	40.85
19 - 20	*	69	53	43	63	32	*	57	32	40.72
20 - 21	*	46	51	32	46	32	*	43.75	32	39.67
21 - 22	*	27	18	17	26	9	*	22	9	38.8
22 - 23	*	7	10	8	12	10	*	9.25	10	41
23 - 24	*	4	6	2	7	9	*	4.75	9	38.5
Totals	0	710	1306	1149	1399	918	408			
% of Total	0%	12.05%	22.17%	19.51%	23.75%	15.59%	6.93%			

Incoming Weekly Speeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	28.6	27	25.5	24	33.17	27.5	30.88	32
1 - 2	*	*	27.2	32	38	39	27.5	30	34.4	38.67
2 - 3	*	*	39.33	39	20	0	33	32.83	33	35.33
3 - 4	*	*	37	15	37.5	37.5	34.5	31.75	36	30.33
4 - 5	*	*	29.6	36.5	33.4	34.91	27	33.38	33.69	38.67
5 - 6	*	*	33.64	32.67	30.92	30.1	37.2	32.34	32.47	38.17
6 - 7	*	*	36.31	35.75	36.04	33.31	29	36.01	32.29	40.23
7 - 8	*	*	30.71	29.28	29.4	34.07	36.15	29.84	34.73	36.37
8 - 9	*	*	32.51	31.96	31.23	33.73	34.79	31.89	34.2	37.9
9 - 10	*	*	33.78	34.45	31.7	34.74	35.9	33.18	35.19	39.53
10 - 11	*	*	34.26	33.42	32.63	32.01	32.81	33.5	32.28	39.47
11 - 12	*	*	34.73	32.77	32.34	32.36	34.59	33.2	33.31	39.57
12 - 13	*	*	32.14	33.35	31.69	33.1	34.9	32.33	33.94	39.37
13 - 14	*	*	32.71	32.77	32.51	32.15	34.26	32.64	33.23	39.8
14 - 15	*	26.51	28.22	28.62	27.13	34.42	34.18	27.55	34.35	34.02
15 - 16	*	31.42	32.1	33.05	32.58	34.12	*	32.24	34.12	38.45
16 - 17	*	33.35	33.9	34.47	34.84	33.93	*	34.04	33.93	40.2
17 - 18	*	34.78	34.83	33.43	34.19	32.26	*	34.33	32.26	40.23
18 - 19	*	33.55	34.42	35.53	35.96	34.71	*	34.89	34.71	40.85
19 - 20	*	35.2	34.3	35.91	33.92	35.25	*	34.77	35.25	40.72
20 - 21	*	35.5	34.43	36.59	35.22	32.59	*	35.31	32.59	39.67
21 - 22	*	32.3	34.06	35.12	35.12	38.89	*	34.03	38.89	38.8
22 - 23	*	37.71	36.4	33.5	38.67	34.1	*	36.76	34.1	41
23 - 24	*	35	35.33	33	39.43	32.67	*	36.53	32.67	38.5
Totals	0	32.4	32.9	33	32.5	33.4	34.4			
% of Total	0%	16.31%	16.57%	16.62%	16.36%	16.82%	17.32%			

Incoming Monthly Speeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	28.6	27	25.5	24	33.17	27.5	30.88	32
1 - 2	*	*	27.2	32	38	39	27.5	30	34.4	38.67
2 - 3	*	*	39.33	39	20	0	33	32.83	33	35.33
3 - 4	*	*	37	15	37.5	37.5	34.5	31.75	36	30.33
4 - 5	*	*	29.6	36.5	33.4	34.91	27	33.38	33.69	38.67
5 - 6	*	*	33.64	32.67	30.92	30.1	37.2	32.34	32.47	38.17
6 - 7	*	*	36.31	35.75	36.04	33.31	29	36.01	32.29	40.23
7 - 8	*	*	30.71	29.28	29.4	34.07	36.15	29.84	34.73	36.37
8 - 9	*	*	32.51	31.96	31.23	33.73	34.79	31.89	34.2	37.9
9 - 10	*	*	33.78	34.45	31.7	34.74	35.9	33.18	35.19	39.53
10 - 11	*	*	34.26	33.42	32.63	32.01	32.81	33.5	32.28	39.47
11 - 12	*	*	34.73	32.77	32.34	32.36	34.59	33.2	33.31	39.57
12 - 13	*	*	32.14	33.35	31.69	33.1	34.9	32.33	33.94	39.37
13 - 14	*	*	32.71	32.77	32.51	32.15	34.26	32.64	33.23	39.8
14 - 15	*	26.51	28.22	28.62	27.13	34.42	34.18	27.55	34.35	34.02
15 - 16	*	31.42	32.1	33.05	32.58	34.12	*	32.24	34.12	38.45
16 - 17	*	33.35	33.9	34.47	34.84	33.93	*	34.04	33.93	40.2
17 - 18	*	34.78	34.83	33.43	34.19	32.26	*	34.33	32.26	40.23
18 - 19	*	33.55	34.42	35.53	35.96	34.71	*	34.89	34.71	40.85
19 - 20	*	35.2	34.3	35.91	33.92	35.25	*	34.77	35.25	40.72
20 - 21	*	35.5	34.43	36.59	35.22	32.59	*	35.31	32.59	39.67
21 - 22	*	32.3	34.06	35.12	35.12	38.89	*	34.03	38.89	38.8
22 - 23	*	37.71	36.4	33.5	38.67	34.1	*	36.76	34.1	41
23 - 24	*	35	35.33	33	39.43	32.67	*	36.53	32.67	38.5
Totals	0	32.4	32.9	33	32.5	33.4	34.4			
% of Total	0%	16.31%	16.57%	16.62%	16.36%	16.82%	17.32%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	31	34	31	32	36	32	34	32
1 - 2	*	*	32	46	38	48	30	38.67	39	38.67
2 - 3	*	*	43	39	24	0	33	35.33	33	35.33
3 - 4	*	*	37	15	39	43	39	30.33	41	30.33
4 - 5	*	*	35	42	39	40	31	38.67	35.5	38.67
5 - 6	*	*	37	39	38.5	39	46	38.17	42.5	38.17
6 - 7	*	*	39	41.2	40.5	40	34	40.23	37	40.23
7 - 8	*	*	37.7	35.2	36.2	40	41	36.37	40.5	36.37
8 - 9	*	*	39.3	37.4	37	41.7	38.7	37.9	40.2	37.9
9 - 10	*	*	39.5	40.6	38.5	41	40.5	39.53	40.75	39.53
10 - 11	*	*	40.5	39.9	38	39	39	39.47	39	39.47
11 - 12	*	*	40	38.7	40	39.8	39.9	39.57	39.85	39.57
12 - 13	*	*	38.5	40.4	39.2	40	39.5	39.37	39.75	39.37
13 - 14	*	*	40.4	39.5	39.5	39	39.8	39.8	39.4	39.8
14 - 15	*	31.6	35	36.5	33	40.7	42	34.02	41.35	34.02
15 - 16	*	37.7	37.5	39.5	39.1	41.3	*	38.45	41.3	38.45
16 - 17	*	39.3	39.4	42	40.1	40.2	*	40.2	40.2	40.2
17 - 18	*	40.1	40.7	38.4	41.7	38.3	*	40.23	38.3	40.23
18 - 19	*	39.2	41.5	41.2	41.5	40	*	40.85	40	40.85
19 - 20	*	41.2	40	40.7	41	39.7	*	40.72	39.7	40.72
20 - 21	*	40.8	38.4	39.5	40	39	*	39.67	39	39.67
21 - 22	*	36.5	38	40.7	40	41	*	38.8	41	38.8
22 - 23	*	42	40	39	43	41	*	41	41	41
23 - 24	*	41	35	35	43	37	*	38.5	37	38.5
Totals	0	389.4	915.4	920.4	920.8	920.7	569.4			
% of Total	0%	8.4%	19.75%	19.85%	19.86%	19.86%	12.28%			

Incoming Monthly EightyFifthSpeeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	31	34	31	32	36	32	34	32
1 - 2	*	*	32	46	38	48	30	38.67	39	38.67
2 - 3	*	*	43	39	24	0	33	35.33	33	35.33
3 - 4	*	*	37	15	39	43	39	30.33	41	30.33
4 - 5	*	*	35	42	39	40	31	38.67	35.5	38.67
5 - 6	*	*	37	39	38.5	39	46	38.17	42.5	38.17
6 - 7	*	*	39	41.2	40.5	40	34	40.23	37	40.23
7 - 8	*	*	37.7	35.2	36.2	40	41	36.37	40.5	36.37
8 - 9	*	*	39.3	37.4	37	41.7	38.7	37.9	40.2	37.9
9 - 10	*	*	39.5	40.6	38.5	41	40.5	39.53	40.75	39.53
10 - 11	*	*	40.5	39.9	38	39	39	39.47	39	39.47
11 - 12	*	*	40	38.7	40	39.8	39.9	39.57	39.85	39.57
12 - 13	*	*	38.5	40.4	39.2	40	39.5	39.37	39.75	39.37
13 - 14	*	*	40.4	39.5	39.5	39	39.8	39.8	39.4	39.8
14 - 15	*	31.6	35	36.5	33	40.7	42	34.02	41.35	34.02
15 - 16	*	37.7	37.5	39.5	39.1	41.3	*	38.45	41.3	38.45
16 - 17	*	39.3	39.4	42	40.1	40.2	*	40.2	40.2	40.2
17 - 18	*	40.1	40.7	38.4	41.7	38.3	*	40.23	38.3	40.23
18 - 19	*	39.2	41.5	41.2	41.5	40	*	40.85	40	40.85
19 - 20	*	41.2	40	40.7	41	39.7	*	40.72	39.7	40.72
20 - 21	*	40.8	38.4	39.5	40	39	*	39.67	39	39.67
21 - 22	*	36.5	38	40.7	40	41	*	38.8	41	38.8
22 - 23	*	42	40	39	43	41	*	41	41	41
23 - 24	*	41	35	35	43	37	*	38.5	37	38.5

Summary of Violators

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM
time correct

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	18	29.0	0	0.0
01:00:00	14	31.6	2	47.0
02:00:00	7	32.9	0	0.0
03:00:00	8	33.9	0	0.0
04:00:00	29	33.5	0	0.0
05:00:00	47	32.4	2	46.5
06:00:00	166	35.5	3	47.3
07:00:00	373	30.4	1	49.0
08:00:00	319	32.4	4	47.8
09:00:00	318	33.9	8	47.4
10:00:00	322	33.0	8	47.8
11:00:00	379	33.2	5	46.8
12:00:00	431	33.0	6	47.7
13:00:00	453	32.9	11	47.5
14:00:00	564	28.7	3	48.3
15:00:00	457	32.4	6	47.0
16:00:00	529	34.0	14	47.8
17:00:00	490	34.0	8	47.9
18:00:00	327	34.9	4	48.3
19:00:00	260	34.8	4	47.8
20:00:00	207	34.9	2	47.5
21:00:00	97	34.5	1	47.0
22:00:00	47	36.2	2	55.5
23:00:00	28	35.3	2	46.5

Incoming Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	14:00	1	4	44	43	22	7	2	0	0	0	0	0	0	0	0	0	0	0	123	26.5	31.6	20 to 30	74.0	*	*	2	112	9	
9/10/2024	15:00	0	6	17	16	26	35	9	1	0	0	0	0	0	0	0	0	0	0	110	31.4	37.7	28 to 38	58.2	*	*	0	99	11	
9/10/2024	16:00	0	4	19	12	28	50	18	3	2	0	0	0	0	0	0	0	0	0	136	33.4	39.3	31 to 41	68.4	3	2.2	0	119	17	
9/10/2024	17:00	0	3	8	9	27	38	26	2	0	0	0	0	0	0	0	0	0	0	113	34.8	40.1	31 to 41	69.9	2	1.8	0	103	10	
9/10/2024	18:00	1	3	8	5	18	28	12	0	0	0	0	0	0	0	0	0	0	0	75	33.5	39.2	30 to 40	68.0	*	*	0	62	13	
9/10/2024	19:00	0	2	5	4	16	26	13	3	0	0	0	0	0	0	0	0	0	0	69	35.2	41.2	32 to 42	66.7	2	2.9	0	59	10	
9/10/2024	20:00	0	2	2	2	8	16	15	1	0	0	0	0	0	0	0	0	0	0	46	35.5	40.8	32 to 42	76.1	1	2.2	0	37	9	
9/10/2024	21:00	0	0	4	2	9	10	2	0	0	0	0	0	0	0	0	0	0	0	27	32.3	36.5	30 to 40	77.8	*	*	0	26	1	
9/10/2024	22:00	0	0	0	0	1	4	1	1	0	0	0	0	0	0	0	0	0	0	7	37.7	42	35 to 45	85.7	*	*	0	7	0	
9/10/2024	23:00	0	1	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	4	35	41	33 to 43	75.0	*	*	0	4	0	
24 Hr Summary		2	25	107	93	155	215	100	11	2	0	0	0	0	0	0	0	0	0	710	32.4	40	31 to 41	58.6	8	1.1	2	628	80	

Incoming Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/11/2024	00:00	0	1	1	0	2	0	1	0	0	0	0	0	0	0	0	0	0	0	5	28.6	31	21 to 31	60.0	*	*	0	5	0
9/11/2024	01:00	1	1	0	1	1	0	0	1	0	0	0	0	0	0	0	0	0	0	5	27.2	32	8 to 18	40.0	*	*	0	5	0
9/11/2024	02:00	0	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3	39.3	43	33 to 43	100.0	*	*	0	3	0
9/11/2024	03:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	37	37	27 to 37	100.0	*	*	0	1	0
9/11/2024	04:00	0	1	1	0	1	2	0	0	0	0	0	0	0	0	0	0	0	0	5	29.6	35	28 to 38	60.0	*	*	0	4	1
9/11/2024	05:00	0	0	2	0	3	5	1	0	0	0	0	0	0	0	0	0	0	0	11	33.6	37	28 to 38	72.7	*	*	0	10	1
9/11/2024	06:00	0	0	0	1	10	25	5	1	0	0	0	0	0	0	0	0	0	0	42	36.3	39	30 to 40	90.5	*	*	0	41	1
9/11/2024	07:00	1	1	25	26	30	25	12	1	0	0	0	0	0	0	0	0	0	0	121	30.7	37.7	22 to 32	57.9	*	*	0	116	5
9/11/2024	08:00	0	1	15	11	19	23	10	3	0	0	0	0	0	0	0	0	0	0	82	32.5	39.3	29 to 39	54.9	*	*	0	78	4
9/11/2024	09:00	1	3	8	3	15	25	12	1	0	0	0	0	0	0	0	0	0	0	68	33.8	39.5	32 to 42	72.1	1	1.5	0	59	9
9/11/2024	10:00	0	2	11	2	13	28	11	2	0	0	0	0	0	0	0	0	0	0	69	34.3	40.5	33 to 43	68.1	2	2.9	0	60	9
9/11/2024	11:00	0	2	5	4	20	26	13	1	0	0	0	0	0	0	0	0	0	0	71	34.7	40	32 to 42	74.6	1	1.4	0	66	5
9/11/2024	12:00	0	3	12	12	16	27	8	1	1	0	0	0	0	0	0	0	0	0	80	32.1	38.5	29 to 39	57.5	2	2.5	0	76	4
9/11/2024	13:00	0	3	20	5	18	17	20	4	0	0	0	0	0	0	0	0	0	0	87	32.7	40.4	31 to 41	51.7	2	2.3	0	83	4
9/11/2024	14:00	1	9	28	36	33	16	5	0	0	0	0	0	0	0	0	0	0	0	128	28.2	35	22 to 32	65.6	*	*	0	122	6
9/11/2024	15:00	1	1	18	9	22	34	9	0	0	0	0	0	0	0	0	0	0	0	94	32.1	37.5	30 to 40	63.8	*	*	0	92	2
9/11/2024	16:00	0	0	17	8	35	48	20	2	1	0	0	0	0	0	0	0	0	0	131	33.9	39.4	30 to 40	68.7	2	1.5	0	122	9
9/11/2024	17:00	1	2	11	4	18	39	21	4	0	0	0	0	0	0	0	0	0	0	100	34.8	40.7	30 to 40	65.0	3	3.0	0	91	9
9/11/2024	18:00	0	4	6	5	11	18	21	0	0	0	0	0	0	0	0	0	0	0	65	34.4	41.5	34 to 44	66.2	*	*	0	60	5
9/11/2024	19:00	0	2	4	4	14	17	11	1	0	0	0	0	0	0	0	0	0	0	53	34.3	40	30 to 40	66.0	1	1.9	0	52	1
9/11/2024	20:00	0	1	3	4	11	28	4	0	0	0	0	0	0	0	0	0	0	0	51	34.4	38.4	31 to 41	82.4	*	*	0	49	2
9/11/2024	21:00	0	0	2	1	5	7	3	0	0	0	0	0	0	0	0	0	0	0	18	34.1	38	31 to 41	77.8	*	*	0	18	0
9/11/2024	22:00	0	0	1	0	2	4	3	0	0	0	0	0	0	0	0	0	0	0	10	36.4	40	32 to 42	90.0	*	*	0	10	0
9/11/2024	23:00	0	0	0	0	4	1	0	1	0	0	0	0	0	0	0	0	0	0	6	35.3	35	25 to 35	83.3	1	16.7	0	6	0
24 Hr Summary		6	37	190	136	303	417	192	23	2	0	0	0	0	0	0	0	0	0	1306	32.9	40	31 to 41	60.4	15	1.1	0	1229	77

Incoming Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/12/2024	00:00	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	27	34	24 to 34	66.7	*	*	0	3	0
9/12/2024	01:00	0	1	0	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	3	32	46	6 to 16	33.3	1	33.3	0	3	0
9/12/2024	02:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	39	39	29 to 39	100.0	*	*	0	1	0
9/12/2024	03:00	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	15	15	5 to 15	100.0	*	*	0	1	0
9/12/2024	04:00	0	0	1	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	6	36.5	42	34 to 44	83.3	*	*	0	4	2
9/12/2024	05:00	0	0	3	0	0	5	1	0	0	0	0	0	0	0	0	0	0	0	9	32.7	39	34 to 44	66.7	*	*	0	7	2
9/12/2024	06:00	0	1	3	2	8	25	11	2	0	0	0	0	0	0	0	0	0	0	52	35.8	41.2	33 to 43	80.8	2	3.8	0	51	1
9/12/2024	07:00	0	2	23	33	31	12	7	1	0	0	0	0	0	0	0	0	0	0	109	29.3	35.2	22 to 32	64.2	1	0.9	0	106	3
9/12/2024	08:00	0	3	10	12	22	34	2	1	0	0	0	0	0	0	0	0	0	0	84	32	37.4	29 to 39	69.0	*	*	0	78	6
9/12/2024	09:00	0	2	5	7	13	17	16	1	1	0	0	0	0	0	0	0	0	0	62	34.5	40.6	31 to 41	64.5	2	3.2	0	57	5
9/12/2024	10:00	0	2	11	4	19	19	15	1	1	0	0	0	0	0	0	0	0	0	72	33.4	39.9	32 to 42	68.1	2	2.8	0	62	10
9/12/2024	11:00	0	2	11	7	24	23	6	4	0	0	0	0	0	0	0	0	0	0	77	32.8	38.7	31 to 41	63.6	*	*	0	69	8
9/12/2024	12:00	0	1	12	11	8	26	15	2	0	0	0	0	0	0	0	0	0	0	75	33.3	40.4	32 to 42	58.7	*	*	0	69	6
9/12/2024	13:00	0	4	15	5	12	27	11	3	0	0	0	0	0	0	0	0	0	0	77	32.8	39.5	32 to 42	57.1	2	2.6	0	69	8
9/12/2024	14:00	1	6	23	25	15	13	7	1	0	0	0	0	0	0	0	0	0	0	91	28.6	36.5	21 to 31	54.9	1	1.1	0	88	3
9/12/2024	15:00	0	3	10	8	13	27	10	2	0	0	0	0	0	0	0	0	0	0	73	33.1	39.5	32 to 42	60.3	1	1.4	0	64	9
9/12/2024	16:00	1	2	9	10	14	30	14	6	0	0	0	0	0	0	0	0	0	0	86	34.5	42	31 to 41	55.8	5	5.8	0	81	5
9/12/2024	17:00	0	0	13	12	21	39	10	1	0	0	0	0	0	0	0	0	0	0	96	33.4	38.4	29 to 39	68.8	*	*	0	88	8
9/12/2024	18:00	0	2	4	3	16	27	16	1	1	0	0	0	0	0	0	0	0	0	70	35.5	41.2	32 to 42	71.4	2	2.9	0	65	5
9/12/2024	19:00	0	0	2	3	10	15	12	1	0	0	0	0	0	0	0	0	0	0	43	35.9	40.7	33 to 43	74.4	*	*	0	40	3
9/12/2024	20:00	0	1	0	1	9	14	6	1	0	0	0	0	0	0	0	0	0	0	32	36.6	39.5	31 to 41	90.6	1	3.1	0	28	4
9/12/2024	21:00	0	1	1	1	2	7	5	0	0	0	0	0	0	0	0	0	0	0	17	35.1	40.7	34 to 44	76.5	*	*	0	15	2
9/12/2024	22:00	0	0	1	0	4	2	1	0	0	0	0	0	0	0	0	0	0	0	8	33.5	39	29 to 39	75.0	*	*	0	8	0
9/12/2024	23:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	33	35	25 to 35	100.0	*	*	0	2	0
24 Hr Summary		2	35	157	145	244	367	167	29	3	0	0	0	0	0	0	0	0	0	1149	33	40	31 to 41	58.3	20	1.7	0	1059	90

Incoming Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/13/2024	00:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25.5	31	10 to 20	50.0	*	*	0	2	0
9/13/2024	01:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	38	38	28 to 38	100.0	*	*	0	1	0
9/13/2024	02:00	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	20	24	14 to 24	100.0	*	*	0	1	1
9/13/2024	03:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	37.5	39	29 to 39	100.0	*	*	0	2	0
9/13/2024	04:00	0	0	1	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	5	33.4	39	31 to 41	80.0	*	*	0	5	0
9/13/2024	05:00	0	2	1	1	3	4	1	0	0	0	0	0	0	0	0	0	0	0	12	30.9	38.5	29 to 39	58.3	*	*	0	10	2
9/13/2024	06:00	0	0	2	2	15	18	12	2	0	0	0	0	0	0	0	0	0	0	51	36	40.5	30 to 40	74.5	1	2.0	0	51	0
9/13/2024	07:00	0	3	16	41	23	10	9	0	0	0	0	0	0	0	0	0	0	0	102	29.4	36.2	23 to 33	67.6	*	*	0	95	7
9/13/2024	08:00	0	4	12	19	20	24	6	2	0	0	0	0	0	0	0	0	0	0	87	31.2	37	27 to 37	60.9	1	1.1	0	85	2
9/13/2024	09:00	0	3	16	8	20	24	8	2	0	0	0	0	0	0	0	0	0	0	81	31.7	38.5	31 to 41	60.5	2	2.5	0	78	3
9/13/2024	10:00	0	2	11	2	12	21	3	2	1	0	0	0	0	0	0	0	0	0	54	32.6	38	29 to 39	61.1	3	5.6	0	49	5
9/13/2024	11:00	1	6	11	12	11	28	15	3	0	0	0	0	0	0	0	0	0	0	87	32.3	40	32 to 42	54.0	1	1.1	0	84	3
9/13/2024	12:00	0	1	26	9	20	25	13	2	0	0	0	0	0	0	0	0	0	0	96	31.7	39.2	31 to 41	55.2	1	1.0	0	92	4
9/13/2024	13:00	1	8	17	11	25	39	16	4	0	0	0	0	0	0	0	0	0	0	121	32.5	39.5	32 to 42	59.5	3	2.5	0	114	7
9/13/2024	14:00	0	5	42	43	23	8	6	0	0	0	0	0	0	0	0	0	0	0	127	27.1	33	21 to 31	72.4	*	*	0	125	2
9/13/2024	15:00	1	9	15	14	23	48	18	3	0	0	0	0	0	0	0	0	0	0	131	32.6	39.1	31 to 41	64.9	3	2.3	0	122	9
9/13/2024	16:00	0	1	8	10	18	39	15	4	0	0	0	0	0	0	0	0	0	0	95	34.8	40.1	32 to 42	73.7	3	3.2	0	90	5
9/13/2024	17:00	0	3	15	7	21	35	22	3	1	0	0	0	0	0	0	0	0	0	107	34.2	41.7	31 to 41	57.9	3	2.8	0	105	2
9/13/2024	18:00	0	1	7	5	6	38	24	1	0	0	0	0	0	0	0	0	0	0	82	36	41.5	34 to 44	78.0	1	1.2	0	79	3
9/13/2024	19:00	0	5	8	3	10	18	18	1	0	0	0	0	0	0	0	0	0	0	63	33.9	41	33 to 43	60.3	1	1.6	0	58	5
9/13/2024	20:00	0	1	2	1	14	20	8	0	0	0	0	0	0	0	0	0	0	0	46	35.2	40	31 to 41	78.3	*	*	0	43	3
9/13/2024	21:00	0	1	1	2	5	11	5	1	0	0	0	0	0	0	0	0	0	0	26	35.1	40	33 to 43	73.1	*	*	0	26	0
9/13/2024	22:00	0	0	1	0	3	4	2	1	0	0	1	0	0	0	0	0	0	0	12	38.7	43	29 to 39	58.3	2	16.7	0	11	1
9/13/2024	23:00	0	0	0	1	1	0	4	1	0	0	0	0	0	0	0	0	0	0	7	39.4	43	33 to 43	71.4	1	14.3	0	7	0
24 Hr Summary		3	56	214	191	275	419	206	32	2	0	1	0	0	0	0	0	0	0	1399	32.5	40	31 to 41	55.5	26	1.9	0	1335	64

Incoming Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/14/2024	00:00	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	24	32	6 to 16	50.0	*	*	0	1	1	
9/14/2024	01:00	0	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	3	39	48	28 to 38	66.7	1	33.3	0	3	0	
9/14/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/14/2024	03:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	37.5	43	22 to 32	50.0	*	*	0	2	0	
9/14/2024	04:00	0	0	2	0	3	3	2	1	0	0	0	0	0	0	0	0	0	0	11	34.9	40	30 to 40	63.6	*	*	0	10	1	
9/14/2024	05:00	0	2	3	0	1	2	1	1	0	0	0	0	0	0	0	0	0	0	10	30.1	39	14 to 24	50.0	*	*	0	9	1	
9/14/2024	06:00	0	0	4	1	1	7	2	1	0	0	0	0	0	0	0	0	0	0	16	33.3	40	35 to 45	62.5	*	*	0	13	3	
9/14/2024	07:00	0	0	5	1	5	10	6	1	0	0	0	0	0	0	0	0	0	0	28	34.1	40	31 to 41	67.9	*	*	0	23	5	
9/14/2024	08:00	0	3	4	5	4	12	5	4	0	0	0	0	0	0	0	0	0	0	37	33.7	41.7	32 to 42	54.1	2	5.4	0	34	3	
9/14/2024	09:00	0	3	5	6	10	28	9	5	0	0	0	0	0	0	0	0	0	0	66	34.7	41	31 to 41	62.1	2	3.0	0	65	1	
9/14/2024	10:00	0	3	17	9	15	27	10	3	0	0	0	0	0	0	0	0	0	0	84	32	39	31 to 41	53.6	1	1.2	0	78	6	
9/14/2024	11:00	0	8	11	8	15	24	16	0	1	0	0	0	0	0	0	0	0	0	83	32.4	39.8	32 to 42	59.0	1	1.2	0	79	4	
9/14/2024	12:00	0	1	18	11	16	31	16	3	0	0	0	0	0	0	0	0	0	0	96	33.1	40	32 to 42	58.3	1	1.0	0	90	6	
9/14/2024	13:00	0	3	17	9	13	28	11	0	1	0	0	0	0	0	0	0	0	0	82	32.1	39	31 to 41	54.9	1	1.2	0	78	4	
9/14/2024	14:00	0	4	5	8	10	22	15	2	1	0	0	0	0	0	0	0	0	0	67	34.4	40.7	31 to 41	62.7	1	1.5	0	60	7	
9/14/2024	15:00	2	1	4	3	11	16	9	3	0	0	0	0	0	0	0	0	0	0	49	34.1	41.3	32 to 42	65.3	2	4.1	0	49	0	
9/14/2024	16:00	1	5	8	3	17	31	12	4	0	0	0	0	0	0	0	0	0	0	81	33.9	40.2	31 to 41	66.7	1	1.2	0	75	6	
9/14/2024	17:00	1	1	12	10	14	29	7	0	0	0	0	0	0	0	0	0	0	0	74	32.3	38.3	31 to 41	63.5	*	*	0	72	2	
9/14/2024	18:00	0	2	2	2	8	13	7	0	1	0	0	0	0	0	0	0	0	0	35	34.7	40	31 to 41	74.3	1	2.9	0	35	0	
9/14/2024	19:00	0	2	2	1	4	16	6	1	0	0	0	0	0	0	0	0	0	0	32	35.3	39.7	33 to 43	81.3	*	*	0	30	2	
9/14/2024	20:00	0	3	1	6	8	9	4	1	0	0	0	0	0	0	0	0	0	0	32	32.6	39	28 to 38	62.5	*	*	0	32	0	
9/14/2024	21:00	0	0	0	0	2	2	4	1	0	0	0	0	0	0	0	0	0	0	9	38.9	41	31 to 41	88.9	1	11.1	0	9	0	
9/14/2024	22:00	0	0	1	1	3	2	3	0	0	0	0	0	0	0	0	0	0	0	10	34.1	41	26 to 36	60.0	*	*	0	10	0	
9/14/2024	23:00	0	0	1	1	3	4	0	0	0	0	0	0	0	0	0	0	0	0	9	32.7	37	28 to 38	77.8	*	*	0	9	0	
24 Hr Summary		4	42	122	85	166	317	146	32	4	0	0	0	0	0	0	0	0	0	918	33.4	40	31 to 41	60.6	15	1.6	0	866	52	

Incoming Histogram

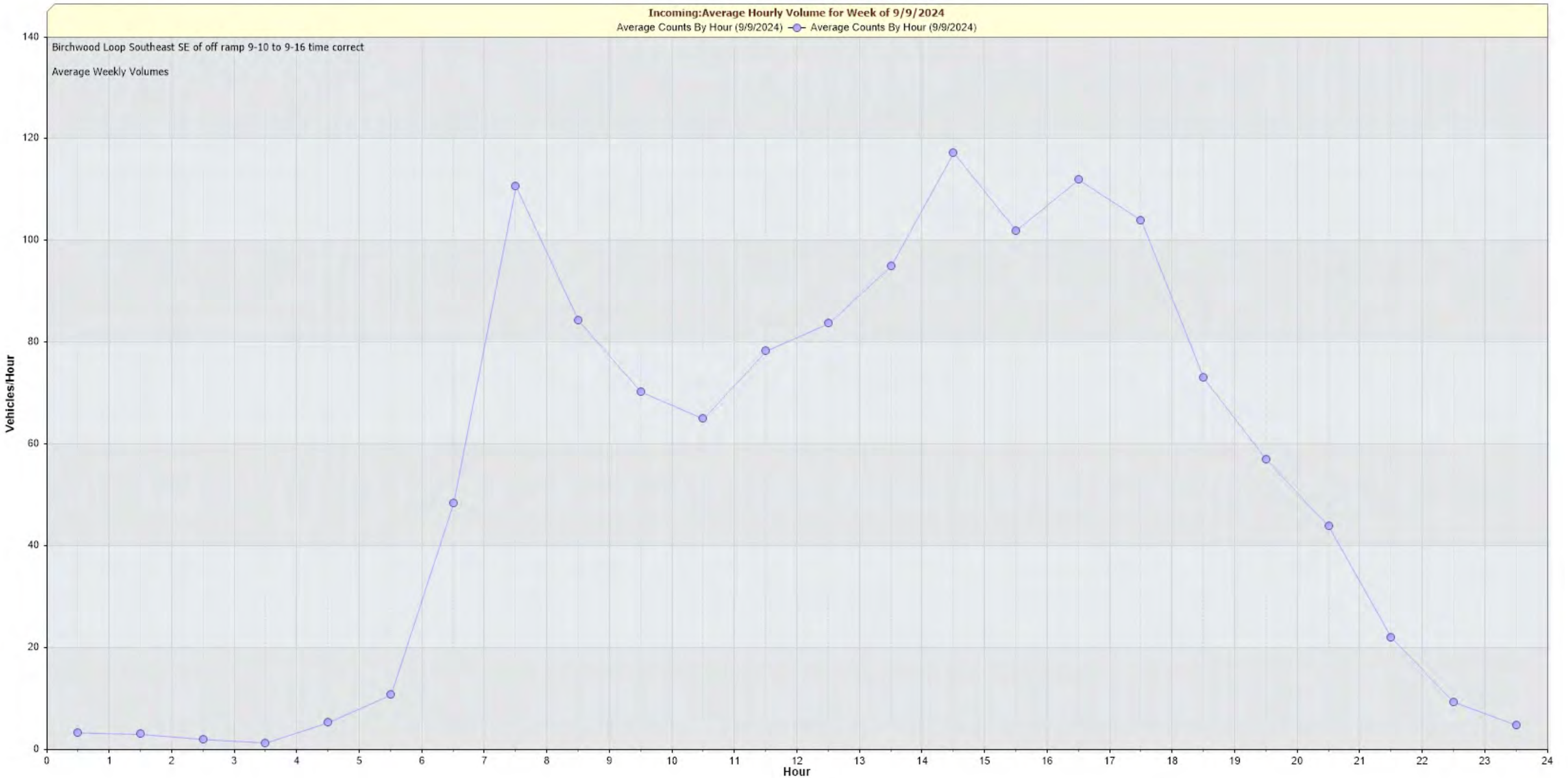
Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/15/2024	00:00	0	0	0	1	2	3	0	0	0	0	0	0	0	0	0	0	0	0	6	33.2	36	26 to 36	83.3	*	*	0	6	0	
9/15/2024	01:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27.5	30	20 to 30	100.0	*	*	0	2	0	
9/15/2024	02:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	33	33	23 to 33	100.0	*	*	0	1	0	
9/15/2024	03:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34.5	39	29 to 39	100.0	*	*	0	2	0	
9/15/2024	04:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27	31	21 to 31	100.0	*	*	0	2	0	
9/15/2024	05:00	0	0	0	2	0	1	0	2	0	0	0	0	0	0	0	0	0	0	5	37.2	46	25 to 35	60.0	2	40.0	0	5	0	
9/15/2024	06:00	0	1	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	5	29	34	28 to 38	60.0	*	*	0	5	0	
9/15/2024	07:00	0	0	0	1	3	5	4	0	0	0	0	0	0	0	0	0	0	0	13	36.2	41	31 to 41	76.9	*	*	0	13	0	
9/15/2024	08:00	0	0	1	1	11	13	2	1	0	0	0	0	0	0	0	0	0	0	29	34.8	38.7	29 to 39	82.8	1	3.4	0	28	1	
9/15/2024	09:00	0	1	2	2	7	18	10	1	0	0	0	0	0	0	0	0	0	0	41	35.9	40.5	32 to 42	82.9	1	2.4	0	40	1	
9/15/2024	10:00	0	0	5	8	11	13	6	0	0	0	0	0	0	0	0	0	0	0	43	32.8	39	30 to 40	60.5	*	*	0	41	2	
9/15/2024	11:00	0	1	3	9	13	20	13	2	0	0	0	0	0	0	0	0	0	0	61	34.6	39.9	32 to 42	68.9	2	3.3	0	60	1	
9/15/2024	12:00	0	1	9	4	17	35	16	2	0	0	0	0	0	0	0	0	0	0	84	34.9	39.5	30 to 40	73.8	2	2.4	0	83	1	
9/15/2024	13:00	0	4	7	8	16	30	17	4	0	0	0	0	0	0	0	0	0	0	86	34.3	39.8	31 to 41	66.3	3	3.5	0	80	6	
9/15/2024	14:00	0	1	3	3	6	8	5	2	0	0	0	0	0	0	0	0	0	0	28	34.2	42	32 to 42	60.7	1	3.6	0	26	2	
9/15/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	9	32	40	92	148	73	14	0	0	0	0	0	0	0	0	0	0	408	34.4	40	31 to 41	67.9	12	2.9	0	394	14	

Incoming:Average Hourly Volume for Week of 9/9/2024
Average Counts By Hour (9/9/2024) — Average Counts By Hour (9/9/2024)

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct
Average Weekly Volumes



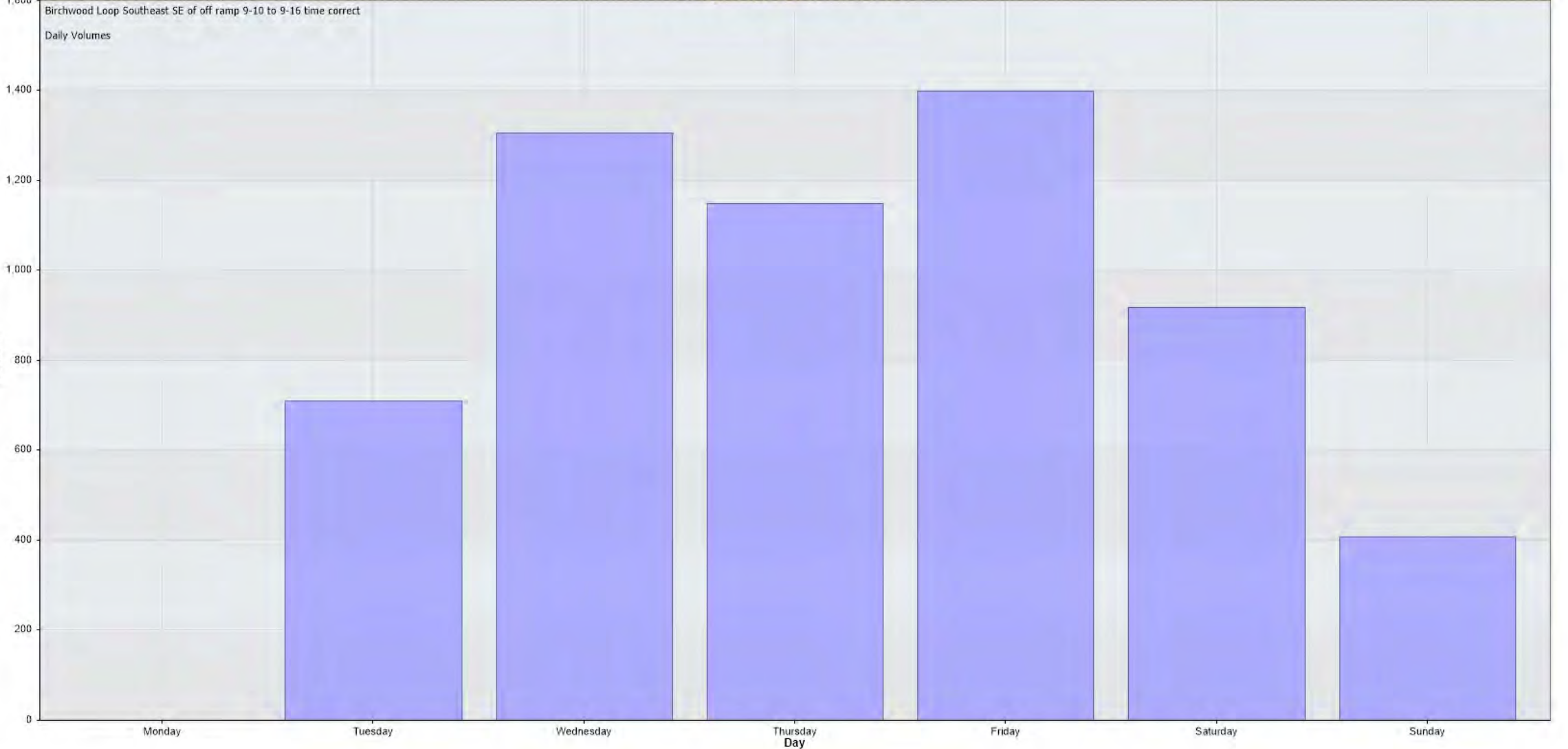
Incoming: Average Hourly WEEKDAY Speeds for Week of 9/9/2024
Average Hourly WEEKDAY Speeds By Hour (9/9/2024) — Average Hourly WEEKDAY Speeds By Hour (9/9/2024)

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct
Average Weekly Speeds



Incoming:Daily Volume for Week of 9/9/2024

Daily Vehicle Counts



Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

Daily Volumes

Vehicles

Day

For Project: Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:52:30 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/10/2024 2:00:00 PM through 9/15/2024 2:59:59 PM

85th Percentile Speed 39 MPH

85th Percentile Vehicles 8129

Max Speed 70 MPH on 9/13/2024 1:58:22 AM

Total Vehicles 9563

AADT: 1896

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1857	1593
AM Peak 7:00 AM	161	107
PM Peak 5:00 PM	196	177

Speed

Speed Limit: 45

85th Percentile Speed: 39

50th Percentile Speed: 31

10 MPH Pace Interval: 23.0 MPH to 33.0 MPH

Average Speed: 30.5

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	N/A	16	34	30	32	28	18
% over limit	N/A	1.4	1.6	1.4	1.5	1.8	2.9
Avg Speeder	N/A	48.1	47.2	47.3	48.5	49.0	47.4
Avg Speed	N/A	30.7	30.4	29.8	30.0	31.1	33.0

Class Counts

	Number	%
VEH_SM	63	0.7
VEH_MED	9046	94.6
VEH_LG	454	4.7
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Outgoing Summary
Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/10/2024 3:00:00 PM	32.0	93	109	41	0.0	0.0%	25.3
9/10/2024 4:00:00 PM	38.0	138	162	51	51.0	0.6%	30.7
9/10/2024 5:00:00 PM	38.0	172	202	47	46.5	1.0%	30.2
9/10/2024 6:00:00 PM	39.0	156	183	49	47.7	1.6%	31.2
9/10/2024 7:00:00 PM	41.0	125	147	46	46.0	2.0%	32.7
9/10/2024 8:00:00 PM	40.0	112	132	55	51.7	2.3%	32.3
9/10/2024 9:00:00 PM	41.0	77	91	49	47.3	4.4%	33.6
9/10/2024 10:00:00 PM	35.0	47	55	42	0.0	0.0%	29.5
9/10/2024 11:00:00 PM	37.0	13	15	40	0.0	0.0%	26.2
9/11/2024 12:00:00 AM	36.0	8	10	44	0.0	0.0%	25.5
9/11/2024 1:00:00 AM	33.0	3	3	33	0.0	0.0%	27.3
9/11/2024 2:00:00 AM	21.0	3	4	27	0.0	0.0%	20.0
9/11/2024 3:00:00 AM	32.0	4	5	33	0.0	0.0%	29.4
9/11/2024 4:00:00 AM	28.0	3	4	36	0.0	0.0%	27.3
9/11/2024 5:00:00 AM	33.0	9	11	36	0.0	0.0%	26.3
9/11/2024 6:00:00 AM	43.0	14	16	46	46.0	6.3%	34.1
9/11/2024 7:00:00 AM	42.0	53	62	48	46.4	8.1%	35.1
9/11/2024 8:00:00 AM	34.0	137	161	51	48.5	1.2%	26.4
9/11/2024 9:00:00 AM	39.0	86	101	46	46.0	2.0%	30.7
9/11/2024 10:00:00 AM	38.0	68	80	44	0.0	0.0%	30.5
9/11/2024 11:00:00 AM	40.0	84	99	46	46.0	2.0%	32.5
9/11/2024 12:00:00 PM	38.0	89	105	43	0.0	0.0%	32.0
9/11/2024 1:00:00 PM	40.0	94	110	48	47.5	1.8%	29.8
9/11/2024 2:00:00 PM	39.0	114	134	46	46.0	0.7%	29.7
9/11/2024 3:00:00 PM	34.0	140	165	49	49.0	0.6%	26.6
9/11/2024 4:00:00 PM	38.0	161	189	47	46.5	1.1%	30.1
9/11/2024 5:00:00 PM	39.0	141	166	52	48.3	1.8%	30.1
9/11/2024 6:00:00 PM	40.0	177	208	50	47.4	2.4%	31.5
9/11/2024 7:00:00 PM	40.0	176	207	46	46.0	1.0%	31.7
9/11/2024 8:00:00 PM	40.0	81	95	51	48.5	2.1%	32.0
9/11/2024 9:00:00 PM	40.0	79	93	46	46.0	1.1%	31.6
9/11/2024 10:00:00 PM	40.0	35	41	48	47.5	4.9%	31.5
9/11/2024 11:00:00 PM	37.0	15	18	52	52.0	5.6%	31.1
9/12/2024 12:00:00 AM	31.0	6	7	35	0.0	0.0%	25.1
9/12/2024 1:00:00 AM	32.0	3	4	47	47.0	25.0%	35.3
9/12/2024 2:00:00 AM	33.0	2	2	33	0.0	0.0%	26.0
9/12/2024 3:00:00 AM	40.0	3	3	40	0.0	0.0%	26.7
9/12/2024 4:00:00 AM	38.0	2	2	38	0.0	0.0%	28.0
9/12/2024 5:00:00 AM	31.0	6	7	31	0.0	0.0%	25.0
9/12/2024 6:00:00 AM	40.0	15	18	44	0.0	0.0%	30.1
9/12/2024 7:00:00 AM	42.0	49	58	45	0.0	0.0%	34.6
9/12/2024 8:00:00 AM	34.0	144	170	47	46.5	1.2%	26.5
9/12/2024 9:00:00 AM	39.0	78	92	47	47.0	1.1%	30.2
9/12/2024 10:00:00 AM	37.0	89	105	45	0.0	0.0%	30.1
9/12/2024 11:00:00 AM	39.0	84	99	45	0.0	0.0%	30.8
9/12/2024 12:00:00 PM	40.0	115	135	46	46.0	1.5%	31.7
9/12/2024 1:00:00 PM	38.0	102	120	49	46.8	3.3%	29.8

Outgoing Summary
Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

9/12/2024 2:00:00 PM	38.0	109	128	49	49.0	0.8%	29.2
9/12/2024 3:00:00 PM	33.0	156	183	50	49.0	1.1%	26.3
9/12/2024 4:00:00 PM	38.0	154	181	48	46.7	1.7%	28.8
9/12/2024 5:00:00 PM	38.0	176	207	46	46.0	0.5%	29.1
9/12/2024 6:00:00 PM	39.0	171	201	50	46.8	3.0%	30.4
9/12/2024 7:00:00 PM	41.0	137	161	47	46.5	1.2%	32.3
9/12/2024 8:00:00 PM	40.0	68	80	48	48.0	1.3%	33.1
9/12/2024 9:00:00 PM	38.0	61	72	49	48.0	4.2%	31.7
9/12/2024 10:00:00 PM	39.0	54	63	51	51.0	3.2%	31.2
9/12/2024 11:00:00 PM	36.0	15	18	45	0.0	0.0%	29.1
9/13/2024 12:00:00 AM	38.0	14	17	44	0.0	0.0%	30.2
9/13/2024 1:00:00 AM	30.0	8	9	34	0.0	0.0%	25.3
9/13/2024 2:00:00 AM	38.0	11	13	70	70.0	7.7%	33.5
9/13/2024 3:00:00 AM	26.0	3	3	26	0.0	0.0%	18.0
9/13/2024 4:00:00 AM	26.0	2	2	26	0.0	0.0%	21.5
9/13/2024 5:00:00 AM	22.0	5	6	36	0.0	0.0%	21.5
9/13/2024 6:00:00 AM	39.0	6	7	40	0.0	0.0%	29.0
9/13/2024 7:00:00 AM	41.0	49	58	47	47.0	1.7%	33.4
9/13/2024 8:00:00 AM	34.0	130	153	48	48.0	0.7%	26.6
9/13/2024 9:00:00 AM	39.0	71	83	48	47.0	2.4%	30.8
9/13/2024 10:00:00 AM	38.0	74	87	51	49.5	2.3%	30.8
9/13/2024 11:00:00 AM	40.0	80	94	49	49.0	1.1%	30.6
9/13/2024 12:00:00 PM	38.0	91	107	49	49.0	0.9%	29.5
9/13/2024 1:00:00 PM	40.0	128	150	47	46.7	2.0%	31.1
9/13/2024 2:00:00 PM	38.0	146	172	50	47.7	1.7%	29.9
9/13/2024 3:00:00 PM	34.0	148	174	45	0.0	0.0%	25.9
9/13/2024 4:00:00 PM	38.0	137	161	60	50.3	2.5%	30.1
9/13/2024 5:00:00 PM	39.0	169	199	48	47.0	1.0%	30.1
9/13/2024 6:00:00 PM	40.0	165	194	46	46.0	0.5%	30.7
9/13/2024 7:00:00 PM	41.0	128	151	49	47.0	2.0%	32.4
9/13/2024 8:00:00 PM	39.0	82	96	46	46.0	1.0%	31.2
9/13/2024 9:00:00 PM	39.0	75	88	47	46.7	3.4%	31.6
9/13/2024 10:00:00 PM	39.0	46	54	50	48.5	3.7%	33.1
9/13/2024 11:00:00 PM	37.0	31	36	44	0.0	0.0%	29.8
9/14/2024 12:00:00 AM	43.0	14	16	48	48.0	6.3%	30.8
9/14/2024 1:00:00 AM	35.0	12	14	40	0.0	0.0%	26.8
9/14/2024 2:00:00 AM	40.0	8	10	56	56.0	10.0%	30.5
9/14/2024 3:00:00 AM	49.0	1	1	49	49.0	100.0%	49.0
9/14/2024 4:00:00 AM	45.0	3	3	45	0.0	0.0%	29.7
9/14/2024 5:00:00 AM	31.0	8	10	41	0.0	0.0%	25.5
9/14/2024 6:00:00 AM	22.0	5	6	35	0.0	0.0%	22.7
9/14/2024 7:00:00 AM	39.0	19	22	42	0.0	0.0%	30.0
9/14/2024 8:00:00 AM	38.0	37	44	45	0.0	0.0%	29.3
9/14/2024 9:00:00 AM	40.0	50	59	50	48.3	5.1%	32.5
9/14/2024 10:00:00 AM	38.0	77	91	45	0.0	0.0%	31.1
9/14/2024 11:00:00 AM	41.0	80	94	44	0.0	0.0%	30.8
9/14/2024 12:00:00 PM	40.0	90	106	49	49.0	0.9%	30.9
9/14/2024 1:00:00 PM	38.0	131	154	47	47.0	0.6%	30.1

Outgoing Summary
 Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

9/14/2024 2:00:00 PM	38.0	110	129	46	46.0	0.8%	30.8
9/14/2024 3:00:00 PM	39.0	116	136	55	49.3	2.2%	30.9
9/14/2024 4:00:00 PM	41.0	105	124	52	47.5	3.2%	31.9
9/14/2024 5:00:00 PM	38.0	94	110	48	47.7	2.7%	31.9
9/14/2024 6:00:00 PM	41.0	88	103	55	48.6	4.9%	32.3
9/14/2024 7:00:00 PM	40.0	82	96	45	0.0	0.0%	32.1
9/14/2024 8:00:00 PM	39.0	59	69	67	57.0	2.9%	32.0
9/14/2024 9:00:00 PM	40.0	50	59	47	47.0	1.7%	31.9
9/14/2024 10:00:00 PM	37.0	35	41	48	48.0	2.4%	29.5
9/14/2024 11:00:00 PM	39.0	22	26	45	0.0	0.0%	29.9
9/15/2024 12:00:00 AM	39.0	12	14	48	48.0	7.1%	32.4
9/15/2024 1:00:00 AM	36.0	9	11	41	0.0	0.0%	29.7
9/15/2024 2:00:00 AM	38.0	2	2	38	0.0	0.0%	35.0
9/15/2024 3:00:00 AM	35.0	3	3	35	0.0	0.0%	32.7
9/15/2024 4:00:00 AM	31.0	2	2	31	0.0	0.0%	27.5
9/15/2024 5:00:00 AM	35.0	3	3	35	0.0	0.0%	34.0
9/15/2024 6:00:00 AM	38.0	6	7	45	0.0	0.0%	32.9
9/15/2024 7:00:00 AM	37.0	7	8	40	0.0	0.0%	28.0
9/15/2024 8:00:00 AM	47.0	8	10	50	48.3	30.0%	38.1
9/15/2024 9:00:00 AM	43.0	26	30	44	0.0	0.0%	34.0
9/15/2024 10:00:00 AM	43.0	50	59	49	47.4	8.5%	34.9
9/15/2024 11:00:00 AM	42.0	72	85	49	47.0	3.5%	32.5
9/15/2024 12:00:00 PM	42.0	68	80	50	48.3	3.8%	32.3
9/15/2024 1:00:00 PM	40.0	128	150	47	47.0	0.7%	33.6
9/15/2024 2:00:00 PM	40.0	113	133	47	46.3	2.3%	32.4
9/15/2024 3:00:00 PM	38.0	26	30	43	0.0	0.0%	33.2

Outgoing Summary

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/11/2024 12:00:00 AM	39.0	940	1106	55	48.1	1.4%	30.7
9/12/2024 12:00:00 AM	39.0	1771	2084	52	47.2	1.6%	30.4
9/13/2024 12:00:00 AM	39.0	1807	2126	51	47.3	1.4%	29.8
9/14/2024 12:00:00 AM	39.0	1796	2113	70	48.5	1.5%	30.0
9/15/2024 12:00:00 AM	39.0	1293	1521	67	49.0	1.8%	31.1
9/15/2024 2:59:59 PM	41.0	521	613	50	47.4	2.9%	33.0

Outgoing Weekly Counts

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	3	4	9	14	11	5.33	12.5	31.67
1 - 2	*	*	4	2	13	10	2	6.33	6	30.67
2 - 3	*	*	5	3	3	1	3	3.67	2	32.67
3 - 4	*	*	4	2	2	3	2	2.67	2.5	30.67
4 - 5	*	*	11	7	6	10	3	8	6.5	28.4
5 - 6	*	*	16	18	7	6	7	13.67	6.5	40.5
6 - 7	*	*	62	58	58	22	8	59.33	15	41.33
7 - 8	*	*	161	170	153	44	10	161.33	27	33.73
8 - 9	*	*	101	92	83	59	30	92	44.5	38.83
9 - 10	*	*	80	105	87	91	59	90.67	75	37.07
10 - 11	*	*	99	99	94	94	85	97.33	89.5	39.4
11 - 12	*	*	105	135	107	106	80	115.67	93	38.17
12 - 13	*	*	110	120	150	154	150	126.67	152	38.93
13 - 14	*	*	134	128	172	129	133	144.67	131	37.57
14 - 15	*	109	165	183	174	136	30	157.75	83	32.8
15 - 16	*	162	189	181	161	124	*	173.25	124	37.55
16 - 17	*	202	166	207	199	110	*	193.5	110	37.93
17 - 18	*	183	208	201	194	103	*	196.5	103	39.22
18 - 19	*	147	207	161	151	96	*	166.5	96	40.33
19 - 20	*	132	95	80	96	69	*	100.75	69	39.58
20 - 21	*	91	93	72	88	59	*	86	59	39.12
21 - 22	*	55	41	63	54	41	*	53.25	41	38.05
22 - 23	*	15	18	18	36	26	*	21.75	26	36.62
23 - 24	*	10	7	17	16	14	*	12.5	14	36.88
Totals	0	1106	2084	2126	2113	1521	613			
% of Total	0%	11.57%	21.79%	22.23%	22.1%	15.91%	6.41%			

Outgoing Monthly Counts

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	*	3	4	9	14	11	5.33	12.5	31.67
1 - 2	*	*	4	2	13	10	2	6.33	6	30.67
2 - 3	*	*	5	3	3	1	3	3.67	2	32.67
3 - 4	*	*	4	2	2	3	2	2.67	2.5	30.67
4 - 5	*	*	11	7	6	10	3	8	6.5	28.4
5 - 6	*	*	16	18	7	6	7	13.67	6.5	40.5
6 - 7	*	*	62	58	58	22	8	59.33	15	41.33
7 - 8	*	*	161	170	153	44	10	161.33	27	33.73
8 - 9	*	*	101	92	83	59	30	92	44.5	38.83
9 - 10	*	*	80	105	87	91	59	90.67	75	37.07
10 - 11	*	*	99	99	94	94	85	97.33	89.5	39.4
11 - 12	*	*	105	135	107	106	80	115.67	93	38.17
12 - 13	*	*	110	120	150	154	150	126.67	152	38.93
13 - 14	*	*	134	128	172	129	133	144.67	131	37.57
14 - 15	*	109	165	183	174	136	30	157.75	83	32.8
15 - 16	*	162	189	181	161	124	*	173.25	124	37.55
16 - 17	*	202	166	207	199	110	*	193.5	110	37.93
17 - 18	*	183	208	201	194	103	*	196.5	103	39.22
18 - 19	*	147	207	161	151	96	*	166.5	96	40.33
19 - 20	*	132	95	80	96	69	*	100.75	69	39.58
20 - 21	*	91	93	72	88	59	*	86	59	39.12
21 - 22	*	55	41	63	54	41	*	53.25	41	38.05
22 - 23	*	15	18	18	36	26	*	21.75	26	36.62
23 - 24	*	10	7	17	16	14	*	12.5	14	36.88
Totals	0	1106	2084	2126	2113	1521	613			
% of Total	0%	11.57%	21.79%	22.23%	22.1%	15.91%	6.41%			

Outgoing Weekly Speeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	27.33	35.25	25.33	26.79	29.73	28.19	28.08	31.67
1 - 2	*	*	20	26	33.54	30.5	35	29.89	31.25	30.67
2 - 3	*	*	29.4	26.67	18	49	32.67	25.55	36.75	32.67
3 - 4	*	*	27.25	28	21.5	29.67	27.5	26	28.8	30.67
4 - 5	*	*	26.27	25	21.5	25.5	34	24.71	27.46	28.4
5 - 6	*	*	34.12	30.06	29	22.67	32.86	31.46	28.15	40.5
6 - 7	*	*	35.13	34.59	33.38	29.95	28	34.38	29.43	41.33
7 - 8	*	*	26.42	26.51	26.58	29.34	38.1	26.5	30.96	33.73
8 - 9	*	*	30.72	30.24	30.81	32.53	34	30.59	33.02	38.83
9 - 10	*	*	30.54	30.13	30.79	31.14	34.93	30.46	32.63	37.07
10 - 11	*	*	32.49	30.84	30.56	30.8	32.53	31.31	31.62	39.4
11 - 12	*	*	32.03	31.65	29.54	30.94	32.33	31.12	31.54	38.17
12 - 13	*	*	29.76	29.77	31.12	30.08	33.56	30.3	31.8	38.93
13 - 14	*	*	29.73	29.22	29.92	30.84	32.37	29.65	31.62	37.57
14 - 15	*	25.36	26.63	26.33	25.89	30.9	33.2	26.12	31.32	32.8
15 - 16	*	30.75	30.05	28.82	30.14	31.93	*	29.91	31.93	37.55
16 - 17	*	30.18	30.08	29.05	30.07	31.91	*	29.83	31.91	37.93
17 - 18	*	31.17	31.53	30.43	30.68	32.29	*	30.96	32.29	39.22
18 - 19	*	32.69	31.71	32.32	32.38	32.11	*	32.23	32.11	40.33
19 - 20	*	32.29	31.99	33.12	31.21	32.04	*	32.13	32.04	39.58
20 - 21	*	33.57	31.63	31.68	31.6	31.9	*	32.15	31.9	39.12
21 - 22	*	29.49	31.49	31.16	33.06	29.51	*	31.27	29.51	38.05
22 - 23	*	26.2	31.06	29.11	29.83	29.92	*	29.31	29.92	36.62
23 - 24	*	25.5	25.14	30.18	30.81	32.36	*	28.74	32.36	36.88
Totals	0	30.7	30.4	29.8	30.1	31.1	33			
% of Total	0%	16.59%	16.42%	16.1%	16.26%	16.8%	17.83%			

Outgoing Monthly Speeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	27.33	35.25	25.33	26.79	29.73	28.19	28.08	31.67
1 - 2	*	*	20	26	33.54	30.5	35	29.89	31.25	30.67
2 - 3	*	*	29.4	26.67	18	49	32.67	25.55	36.75	32.67
3 - 4	*	*	27.25	28	21.5	29.67	27.5	26	28.8	30.67
4 - 5	*	*	26.27	25	21.5	25.5	34	24.71	27.46	28.4
5 - 6	*	*	34.12	30.06	29	22.67	32.86	31.46	28.15	40.5
6 - 7	*	*	35.13	34.59	33.38	29.95	28	34.38	29.43	41.33
7 - 8	*	*	26.42	26.51	26.58	29.34	38.1	26.5	30.96	33.73
8 - 9	*	*	30.72	30.24	30.81	32.53	34	30.59	33.02	38.83
9 - 10	*	*	30.54	30.13	30.79	31.14	34.93	30.46	32.63	37.07
10 - 11	*	*	32.49	30.84	30.56	30.8	32.53	31.31	31.62	39.4
11 - 12	*	*	32.03	31.65	29.54	30.94	32.33	31.12	31.54	38.17
12 - 13	*	*	29.76	29.77	31.12	30.08	33.56	30.3	31.8	38.93
13 - 14	*	*	29.73	29.22	29.92	30.84	32.37	29.65	31.62	37.57
14 - 15	*	25.36	26.63	26.33	25.89	30.9	33.2	26.12	31.32	32.8
15 - 16	*	30.75	30.05	28.82	30.14	31.93	*	29.91	31.93	37.55
16 - 17	*	30.18	30.08	29.05	30.07	31.91	*	29.83	31.91	37.93
17 - 18	*	31.17	31.53	30.43	30.68	32.29	*	30.96	32.29	39.22
18 - 19	*	32.69	31.71	32.32	32.38	32.11	*	32.23	32.11	40.33
19 - 20	*	32.29	31.99	33.12	31.21	32.04	*	32.13	32.04	39.58
20 - 21	*	33.57	31.63	31.68	31.6	31.9	*	32.15	31.9	39.12
21 - 22	*	29.49	31.49	31.16	33.06	29.51	*	31.27	29.51	38.05
22 - 23	*	26.2	31.06	29.11	29.83	29.92	*	29.31	29.92	36.62
23 - 24	*	25.5	25.14	30.18	30.81	32.36	*	28.74	32.36	36.88
Totals	0	30.7	30.4	29.8	30.1	31.1	33			
% of Total	0%	16.59%	16.42%	16.1%	16.26%	16.8%	17.83%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	33	32	30	35	36	31.67	35.5	31.67
1 - 2	*	*	21	33	38	40	38	30.67	39	30.67
2 - 3	*	*	32	40	26	49	35	32.67	42	32.67
3 - 4	*	*	28	38	26	45	31	30.67	38	30.67
4 - 5	*	*	32.7	30.5	22	31	35	28.4	33	28.4
5 - 6	*	*	42.5	40	39	22	38	40.5	30	40.5
6 - 7	*	*	42	41.2	40.8	38.5	37	41.33	37.75	41.33
7 - 8	*	*	34	33.4	33.8	37.5	47	33.73	42.25	33.73
8 - 9	*	*	38.8	39	38.7	39.3	42.5	38.83	40.9	38.83
9 - 10	*	*	37.2	36.3	37.7	38	42.7	37.07	40.35	37.07
10 - 11	*	*	40	38.2	40	40.6	41.2	39.4	40.9	39.4
11 - 12	*	*	37.7	39.4	37.4	39.2	41.5	38.17	40.35	38.17
12 - 13	*	*	39.4	37.7	39.7	37.8	39.4	38.93	38.6	38.93
13 - 14	*	*	38.2	37.4	37.1	38	39.6	37.57	38.8	37.57
14 - 15	*	31.7	33.3	32.6	33.6	38.3	37.7	32.8	38	32.8
15 - 16	*	37.9	37.7	37.4	37.2	41	*	37.55	41	37.55
16 - 17	*	37.1	38.2	37.7	38.7	38	*	37.93	38	37.93
17 - 18	*	38.7	39.3	39	39.9	40.3	*	39.22	40.3	39.22
18 - 19	*	40.7	39.5	40.6	40.5	40	*	40.33	40	40.33
19 - 20	*	40	39.3	40	39	38.7	*	39.58	38.7	39.58
20 - 21	*	40.8	39.7	37.8	38.2	40	*	39.12	40	39.12
21 - 22	*	35	40	38.4	38.8	37	*	38.05	37	38.05
22 - 23	*	36.5	37	36	37	39	*	36.62	39	36.62
23 - 24	*	36	31	38	42.5	39	*	36.88	39	36.88
Totals	0	374.4	871.5	893.6	871.6	922.2	581.6			
% of Total	0%	8.29%	19.3%	19.79%	19.3%	20.43%	12.88%			

Outgoing Monthly EightyFifthSpeeds

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	*	33	32	30	35	36	31.67	35.5	31.67
1 - 2	*	*	21	33	38	40	38	30.67	39	30.67
2 - 3	*	*	32	40	26	49	35	32.67	42	32.67
3 - 4	*	*	28	38	26	45	31	30.67	38	30.67
4 - 5	*	*	32.7	30.5	22	31	35	28.4	33	28.4
5 - 6	*	*	42.5	40	39	22	38	40.5	30	40.5
6 - 7	*	*	42	41.2	40.8	38.5	37	41.33	37.75	41.33
7 - 8	*	*	34	33.4	33.8	37.5	47	33.73	42.25	33.73
8 - 9	*	*	38.8	39	38.7	39.3	42.5	38.83	40.9	38.83
9 - 10	*	*	37.2	36.3	37.7	38	42.7	37.07	40.35	37.07
10 - 11	*	*	40	38.2	40	40.6	41.2	39.4	40.9	39.4
11 - 12	*	*	37.7	39.4	37.4	39.2	41.5	38.17	40.35	38.17
12 - 13	*	*	39.4	37.7	39.7	37.8	39.4	38.93	38.6	38.93
13 - 14	*	*	38.2	37.4	37.1	38	39.6	37.57	38.8	37.57
14 - 15	*	31.7	33.3	32.6	33.6	38.3	37.7	32.8	38	32.8
15 - 16	*	37.9	37.7	37.4	37.2	41	*	37.55	41	37.55
16 - 17	*	37.1	38.2	37.7	38.7	38	*	37.93	38	37.93
17 - 18	*	38.7	39.3	39	39.9	40.3	*	39.22	40.3	39.22
18 - 19	*	40.7	39.5	40.6	40.5	40	*	40.33	40	40.33
19 - 20	*	40	39.3	40	39	38.7	*	39.58	38.7	39.58
20 - 21	*	40.8	39.7	37.8	38.2	40	*	39.12	40	39.12
21 - 22	*	35	40	38.4	38.8	37	*	38.05	37	38.05
22 - 23	*	36.5	37	36	37	39	*	36.62	39	36.62
23 - 24	*	36	31	38	42.5	39	*	36.88	39	36.88

Summary of Violators

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM
time correct

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	41	28.1	1	47.0
01:00:00	31	30.4	2	63.0
02:00:00	15	28.5	1	49.0
03:00:00	13	27.1	0	0.0
04:00:00	37	25.7	0	0.0
05:00:00	54	30.7	1	46.0
06:00:00	208	33.7	6	46.5
07:00:00	538	27.0	8	47.9
08:00:00	365	31.2	8	47.3
09:00:00	422	31.2	7	48.0
10:00:00	471	31.4	6	47.0
11:00:00	533	31.3	7	47.9
12:00:00	684	31.0	11	46.9
13:00:00	696	30.4	9	47.0
14:00:00	797	27.2	5	49.4
15:00:00	817	30.2	14	48.2
16:00:00	884	30.1	11	47.4
17:00:00	889	31.1	20	47.5
18:00:00	762	32.2	10	46.4
19:00:00	472	32.1	9	51.1
20:00:00	403	32.1	12	47.2
21:00:00	254	31.0	7	48.9
22:00:00	113	29.5	1	52.0
23:00:00	64	29.5	2	48.0

Outgoing Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	14:00	1	13	46	21	23	4	1	0	0	0	0	0	0	0	0	0	0	0	109	25.4	31.7	19 to 29	67.0	*	*	0	101	8	
9/10/2024	15:00	0	10	31	31	30	46	13	0	1	0	0	0	0	0	0	0	0	0	162	30.7	37.9	29 to 39	53.1	1	0.6	5	142	15	
9/10/2024	16:00	0	10	41	48	42	42	17	2	0	0	0	0	0	0	0	0	0	0	202	30.2	37.1	23 to 33	51.5	2	1.0	1	186	15	
9/10/2024	17:00	0	6	38	31	46	37	20	5	0	0	0	0	0	0	0	0	0	0	183	31.2	38.7	23 to 33	49.7	3	1.6	1	166	16	
9/10/2024	18:00	0	4	23	27	24	37	23	9	0	0	0	0	0	0	0	0	0	0	147	32.7	40.7	31 to 41	47.6	3	2.0	2	134	11	
9/10/2024	19:00	0	5	21	19	36	26	22	1	1	1	0	0	0	0	0	0	0	0	132	32.3	40	31 to 41	51.5	3	2.3	2	119	11	
9/10/2024	20:00	0	2	6	18	20	23	16	6	0	0	0	0	0	0	0	0	0	0	91	33.6	40.8	31 to 41	53.8	4	4.4	1	78	12	
9/10/2024	21:00	0	2	12	13	16	10	2	0	0	0	0	0	0	0	0	0	0	0	55	29.5	35	23 to 33	65.5	*	*	0	53	2	
9/10/2024	22:00	0	3	6	2	1	2	1	0	0	0	0	0	0	0	0	0	0	0	15	26.2	36.5	16 to 26	66.7	*	*	0	15	0	
9/10/2024	23:00	1	2	4	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	10	25.5	36	14 to 24	70.0	*	*	0	10	0	
24 Hr Summary		2	57	228	210	238	229	116	23	2	1	0	0	0	0	0	0	0	0	1106	30.7	39	23 to 33	46.7	16	1.4	12	1004	90	

Outgoing Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/11/2024	00:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	27.3	33	18 to 28	66.7	*	*	0	3	0
9/11/2024	01:00	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	20	21	11 to 21	75.0	*	*	0	2	2
9/11/2024	02:00	0	0	0	2	3	0	0	0	0	0	0	0	0	0	0	0	0	0	5	29.4	32	23 to 33	100.0	*	*	0	5	0
9/11/2024	03:00	0	0	2	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	4	27.3	28	18 to 28	75.0	*	*	0	4	0
9/11/2024	04:00	1	2	2	1	4	1	0	0	0	0	0	0	0	0	0	0	0	0	11	26.3	32.7	23 to 33	54.5	*	*	0	10	1
9/11/2024	05:00	0	1	2	1	4	4	3	1	0	0	0	0	0	0	0	0	0	0	16	34.1	42.5	33 to 43	68.8	1	6.3	0	16	0
9/11/2024	06:00	0	4	7	2	8	24	10	7	0	0	0	0	0	0	0	0	0	0	62	35.1	42	32 to 42	62.9	5	8.1	1	55	6
9/11/2024	07:00	0	25	51	36	25	17	4	2	1	0	0	0	0	0	0	0	0	0	161	26.4	34	18 to 28	60.9	2	1.2	0	158	3
9/11/2024	08:00	0	11	17	16	20	23	11	3	0	0	0	0	0	0	0	0	0	0	101	30.7	38.8	28 to 38	46.5	2	2.0	0	94	7
9/11/2024	09:00	1	3	14	13	22	20	7	0	0	0	0	0	0	0	0	0	0	0	80	30.5	37.2	28 to 38	60.0	*	*	0	73	7
9/11/2024	10:00	0	5	15	10	24	27	14	4	0	0	0	0	0	0	0	0	0	0	99	32.5	40	31 to 41	57.6	2	2.0	2	89	8
9/11/2024	11:00	0	2	14	20	26	34	9	0	0	0	0	0	0	0	0	0	0	0	105	32	37.7	30 to 40	60.0	*	*	0	96	9
9/11/2024	12:00	0	13	23	18	22	16	16	2	0	0	0	0	0	0	0	0	0	0	110	29.8	39.4	23 to 33	41.8	2	1.8	1	103	6
9/11/2024	13:00	0	11	26	33	27	20	15	2	0	0	0	0	0	0	0	0	0	0	134	29.7	38.2	19 to 29	47.8	1	0.7	0	129	5
9/11/2024	14:00	0	21	42	54	25	19	3	1	0	0	0	0	0	0	0	0	0	0	165	26.6	33.3	19 to 29	61.8	1	0.6	0	162	3
9/11/2024	15:00	0	21	29	44	35	37	20	3	0	0	0	0	0	0	0	0	0	0	189	30.1	37.7	24 to 34	47.6	2	1.1	1	177	11
9/11/2024	16:00	1	12	38	29	33	31	16	5	1	0	0	0	0	0	0	0	0	0	166	30.1	38.2	19 to 29	45.8	3	1.8	0	160	6
9/11/2024	17:00	0	11	31	44	40	48	29	4	1	0	0	0	0	0	0	0	0	0	208	31.5	39.3	28 to 38	48.1	5	2.4	0	202	6
9/11/2024	18:00	0	8	34	44	34	51	28	8	0	0	0	0	0	0	0	0	0	0	207	31.7	39.5	31 to 41	47.8	2	1.0	2	196	9
9/11/2024	19:00	0	5	9	22	23	21	12	2	1	0	0	0	0	0	0	0	0	0	95	32	39.3	24 to 34	52.6	2	2.1	0	93	2
9/11/2024	20:00	0	2	21	16	14	24	12	4	0	0	0	0	0	0	0	0	0	0	93	31.6	39.7	31 to 41	49.5	1	1.1	1	92	0
9/11/2024	21:00	0	1	11	5	8	8	4	4	0	0	0	0	0	0	0	0	0	0	41	31.5	40	20 to 30	43.9	2	4.9	0	40	1
9/11/2024	22:00	0	1	4	4	2	4	2	0	1	0	0	0	0	0	0	0	0	0	18	31.1	37	19 to 29	50.0	1	5.6	0	18	0
9/11/2024	23:00	0	1	4	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	7	25.1	31	14 to 24	71.4	*	*	0	7	0
24 Hr Summary		4	161	398	417	401	431	215	52	5	0	0	0	0	0	0	0	0	0	2084	30.4	39	27 to 37	45.0	34	1.6	8	1984	92

Outgoing Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/12/2024	00:00	0	0	0	0	3	0	0	1	0	0	0	0	0	0	0	0	0	0	4	35.3	32	22 to 32	75.0	1	25.0	0	4	0
9/12/2024	01:00	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	26	33	9 to 19	50.0	*	*	1	1	0
9/12/2024	02:00	0	0	2	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	3	26.7	40	10 to 20	66.7	*	*	0	3	0
9/12/2024	03:00	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	28	38	8 to 18	50.0	*	*	0	2	0
9/12/2024	04:00	0	1	2	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	7	25	30.5	21 to 31	85.7	*	*	0	7	0
9/12/2024	05:00	0	5	1	1	4	3	4	0	0	0	0	0	0	0	0	0	0	0	18	30.1	40	31 to 41	55.6	*	*	0	16	2
9/12/2024	06:00	0	2	6	5	9	22	13	1	0	0	0	0	0	0	0	0	0	0	58	34.6	41.2	34 to 44	69.0	*	*	0	54	4
9/12/2024	07:00	0	11	72	44	20	12	8	3	0	0	0	0	0	0	0	0	0	0	170	26.5	33.4	20 to 30	70.6	2	1.2	2	162	6
9/12/2024	08:00	2	5	17	16	22	16	13	1	0	0	0	0	0	0	0	0	0	0	92	30.2	39	25 to 35	48.9	1	1.1	2	85	5
9/12/2024	09:00	0	7	16	19	34	22	6	1	0	0	0	0	0	0	0	0	0	0	105	30.1	36.3	28 to 38	57.1	*	*	2	97	6
9/12/2024	10:00	0	4	21	15	27	21	9	2	0	0	0	0	0	0	0	0	0	0	99	30.8	38.2	25 to 35	50.5	*	*	0	90	9
9/12/2024	11:00	0	5	29	23	19	37	19	3	0	0	0	0	0	0	0	0	0	0	135	31.7	39.4	29 to 39	46.7	2	1.5	2	122	11
9/12/2024	12:00	0	11	30	21	20	25	8	5	0	0	0	0	0	0	0	0	0	0	120	29.8	37.7	19 to 29	48.3	4	3.3	0	113	7
9/12/2024	13:00	0	13	29	26	22	27	8	3	0	0	0	0	0	0	0	0	0	0	128	29.2	37.4	19 to 29	46.9	1	0.8	1	120	7
9/12/2024	14:00	1	18	63	56	24	15	5	0	1	0	0	0	0	0	0	0	0	0	183	26.3	32.6	19 to 29	71.0	1	0.5	0	172	11
9/12/2024	15:00	0	14	44	47	30	28	15	3	0	0	0	0	0	0	0	0	0	0	181	28.8	37.4	20 to 30	53.0	3	1.7	1	169	11
9/12/2024	16:00	0	13	54	47	44	29	19	1	0	0	0	0	0	0	0	0	0	0	207	29.1	37.7	18 to 28	51.7	1	0.5	3	194	10
9/12/2024	17:00	1	9	41	52	29	39	23	6	1	0	0	0	0	0	0	0	0	0	201	30.4	39	22 to 32	53.2	6	3.0	0	193	8
9/12/2024	18:00	0	10	19	35	23	38	32	4	0	0	0	0	0	0	0	0	0	0	161	32.3	40.6	34 to 44	46.6	2	1.2	0	154	7
9/12/2024	19:00	0	3	10	13	11	26	15	2	0	0	0	0	0	0	0	0	0	0	80	33.1	40	32 to 42	57.5	1	1.3	0	76	4
9/12/2024	20:00	2	4	7	13	19	18	4	5	0	0	0	0	0	0	0	0	0	0	72	31.7	37.8	27 to 37	54.2	3	4.2	0	67	5
9/12/2024	21:00	0	2	12	13	14	16	4	0	2	0	0	0	0	0	0	0	0	0	63	31.2	38.4	22 to 32	55.6	2	3.2	1	60	2
9/12/2024	22:00	0	1	5	5	2	4	0	1	0	0	0	0	0	0	0	0	0	0	18	29.1	36	23 to 33	66.7	*	*	0	18	0
9/12/2024	23:00	0	1	4	4	3	3	2	0	0	0	0	0	0	0	0	0	0	0	17	30.2	38	18 to 28	52.9	*	*	0	17	0
24 Hr Summary		6	141	484	457	382	402	208	42	4	0	0	0	0	0	0	0	0	0	2126	29.8	39	22 to 32	47.2	30	1.4	15	1996	115

Outgoing Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/13/2024	00:00	0	0	4	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	9	25.3	30	20 to 30	88.9	*	*	0	8	1
9/13/2024	01:00	0	0	1	4	5	1	1	0	0	0	0	0	1	0	0	0	0	0	13	33.5	38	23 to 33	76.9	1	7.7	0	13	0
9/13/2024	02:00	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	18	26	5 to 15	66.7	*	*	0	2	1
9/13/2024	03:00	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	21.5	26	16 to 26	100.0	*	*	0	2	0
9/13/2024	04:00	0	3	2	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	6	21.5	22	12 to 22	83.3	*	*	0	5	1
9/13/2024	05:00	0	1	2	1	0	2	1	0	0	0	0	0	0	0	0	0	0	0	7	29	39	11 to 21	42.9	*	*	0	6	1
9/13/2024	06:00	0	2	9	5	13	14	13	2	0	0	0	0	0	0	0	0	0	0	58	33.4	40.8	31 to 41	55.2	1	1.7	0	53	5
9/13/2024	07:00	0	15	58	38	20	14	6	2	0	0	0	0	0	0	0	0	0	0	153	26.6	33.8	19 to 29	67.3	1	0.7	2	147	4
9/13/2024	08:00	1	5	14	17	17	19	8	2	0	0	0	0	0	0	0	0	0	0	83	30.8	38.7	24 to 34	47.0	2	2.4	0	80	3
9/13/2024	09:00	0	6	19	11	17	24	8	1	1	0	0	0	0	0	0	0	0	0	87	30.8	37.7	28 to 38	49.4	2	2.3	0	84	3
9/13/2024	10:00	1	9	16	17	13	20	17	1	0	0	0	0	0	0	0	0	0	0	94	30.6	40	32 to 42	44.7	1	1.1	0	90	4
9/13/2024	11:00	0	7	26	22	24	18	8	2	0	0	0	0	0	0	0	0	0	0	107	29.5	37.4	21 to 31	49.5	1	0.9	2	98	7
9/13/2024	12:00	0	3	34	36	23	25	24	5	0	0	0	0	0	0	0	0	0	0	150	31.1	39.7	21 to 31	52.0	3	2.0	1	144	5
9/13/2024	13:00	0	12	40	36	28	42	10	3	1	0	0	0	0	0	0	0	0	0	172	29.9	37.1	22 to 32	48.8	3	1.7	0	163	9
9/13/2024	14:00	1	25	60	45	20	19	3	1	0	0	0	0	0	0	0	0	0	0	174	25.9	33.6	18 to 28	67.2	*	*	3	166	5
9/13/2024	15:00	0	12	28	36	33	36	12	3	0	0	1	0	0	0	0	0	0	0	161	30.1	37.2	25 to 35	50.9	4	2.5	1	153	7
9/13/2024	16:00	0	12	47	42	38	31	24	5	0	0	0	0	0	0	0	0	0	0	199	30.1	38.7	22 to 32	50.3	2	1.0	2	190	7
9/13/2024	17:00	0	9	45	49	22	30	34	5	0	0	0	0	0	0	0	0	0	0	194	30.7	39.9	19 to 29	51.0	1	0.5	2	187	5
9/13/2024	18:00	0	5	24	29	30	32	27	4	0	0	0	0	0	0	0	0	0	0	151	32.4	40.5	32 to 42	49.7	3	2.0	1	143	7
9/13/2024	19:00	1	10	12	16	15	28	11	3	0	0	0	0	0	0	0	0	0	0	96	31.2	39	31 to 41	53.1	1	1.0	1	90	5
9/13/2024	20:00	0	2	13	21	18	24	7	3	0	0	0	0	0	0	0	0	0	0	88	31.6	38.2	26 to 36	55.7	3	3.4	2	84	2
9/13/2024	21:00	0	0	8	8	12	19	4	2	1	0	0	0	0	0	0	0	0	0	54	33.1	38.8	29 to 39	59.3	2	3.7	1	52	1
9/13/2024	22:00	0	3	7	8	8	6	4	0	0	0	0	0	0	0	0	0	0	0	36	29.8	37	23 to 33	55.6	*	*	1	33	2
9/13/2024	23:00	0	2	4	3	1	1	4	1	0	0	0	0	0	0	0	0	0	0	16	30.8	42.5	19 to 29	56.3	1	6.3	0	15	1
24 Hr Summary		5	145	473	449	359	406	226	45	3	0	1	0	1	0	0	0	0	0	2113	30	39	21 to 31	46.6	32	1.5	19	2008	86

Outgoing Histogram

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/14/2024	00:00	0	2	5	2	2	2	1	0	0	0	0	0	0	0	0	0	0	0	14	26.8	35	16 to 26	57.1	*	*	0	13	1
9/14/2024	01:00	1	2	1	0	2	1	2	0	0	1	0	0	0	0	0	0	0	0	10	30.5	40	13 to 23	40.0	1	10.0	0	8	2
9/14/2024	02:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	49	49	39 to 49	100.0	1	100.0	0	1	0
9/14/2024	03:00	0	1	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	3	29.7	45	15 to 25	66.7	*	*	0	3	0
9/14/2024	04:00	0	2	4	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	10	25.5	31	16 to 26	70.0	*	*	0	9	1
9/14/2024	05:00	0	1	4	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	6	22.7	22	12 to 22	83.3	*	*	0	5	1
9/14/2024	06:00	0	2	5	3	4	6	2	0	0	0	0	0	0	0	0	0	0	0	22	30	38.5	30 to 40	50.0	*	*	1	18	3
9/14/2024	07:00	0	4	9	13	6	6	5	1	0	0	0	0	0	0	0	0	0	0	44	29.3	37.5	21 to 31	59.1	*	*	1	41	2
9/14/2024	08:00	0	2	8	12	9	18	7	2	1	0	0	0	0	0	0	0	0	0	59	32.5	39.3	30 to 40	50.8	3	5.1	0	56	3
9/14/2024	09:00	1	3	17	16	15	29	8	2	0	0	0	0	0	0	0	0	0	0	91	31.1	38	28 to 38	51.6	*	*	0	90	1
9/14/2024	10:00	0	3	23	21	14	14	19	0	0	0	0	0	0	0	0	0	0	0	94	30.8	40.6	19 to 29	47.9	*	*	1	88	5
9/14/2024	11:00	0	5	23	17	26	18	13	4	0	0	0	0	0	0	0	0	0	0	106	30.9	39.2	23 to 33	48.1	1	0.9	0	104	2
9/14/2024	12:00	0	8	28	41	32	32	11	2	0	0	0	0	0	0	0	0	0	0	154	30.1	37.8	24 to 34	50.6	1	0.6	0	146	8
9/14/2024	13:00	0	6	28	22	22	40	10	1	0	0	0	0	0	0	0	0	0	0	129	30.8	38	29 to 39	50.4	1	0.8	0	123	6
9/14/2024	14:00	1	4	20	36	36	21	12	5	0	1	0	0	0	0	0	0	0	0	136	30.9	38.3	24 to 34	56.6	3	2.2	0	131	5
9/14/2024	15:00	0	6	21	26	14	31	21	4	1	0	0	0	0	0	0	0	0	0	124	31.9	41	33 to 43	45.2	4	3.2	0	118	6
9/14/2024	16:00	0	6	14	14	37	25	11	3	0	0	0	0	0	0	0	0	0	0	110	31.9	38	28 to 38	63.6	3	2.7	2	104	4
9/14/2024	17:00	0	4	22	12	18	26	15	5	0	1	0	0	0	0	0	0	0	0	103	32.3	40.3	32 to 42	54.4	5	4.9	0	99	4
9/14/2024	18:00	0	2	20	12	21	17	23	1	0	0	0	0	0	0	0	0	0	0	96	32.1	40	31 to 41	56.3	*	*	1	92	3
9/14/2024	19:00	0	4	13	8	14	22	6	1	0	0	0	1	0	0	0	0	0	0	69	32	38.7	31 to 41	55.1	2	2.9	0	68	1
9/14/2024	20:00	0	1	10	12	15	8	10	3	0	0	0	0	0	0	0	0	0	0	59	31.9	40	27 to 37	54.2	1	1.7	0	59	0
9/14/2024	21:00	0	3	9	10	6	11	1	1	0	0	0	0	0	0	0	0	0	0	41	29.5	37	18 to 28	51.2	1	2.4	0	41	0
9/14/2024	22:00	0	1	6	7	4	4	3	1	0	0	0	0	0	0	0	0	0	0	26	29.9	39	22 to 32	57.7	*	*	0	25	1
9/14/2024	23:00	0	1	1	5	1	4	1	1	0	0	0	0	0	0	0	0	0	0	14	32.4	39	27 to 37	57.1	1	7.1	0	14	0
24 Hr Summary		3	73	291	291	299	337	182	39	2	3	0	1	0	0	0	0	0	0	1521	31.1	39	28 to 38	45.6	28	1.8	6	1456	59

Outgoing Histogram

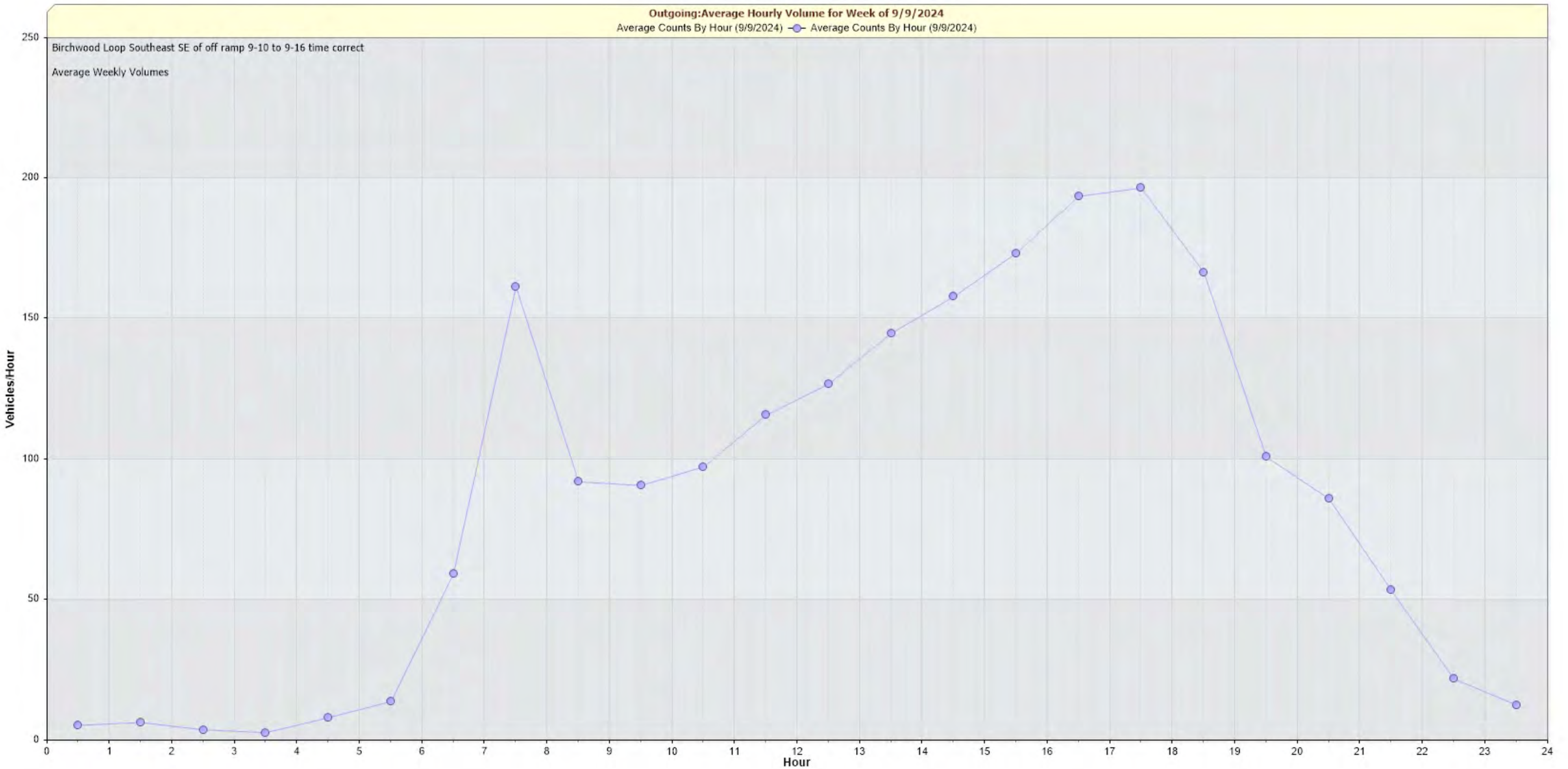
Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Sun-Sep-15-2024-02-59-PM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG			
9/15/2024	00:00	0	1	2	2	3	2	1	0	0	0	0	0	0	0	0	0	0	0	0	11	29.7	36	23 to 33	54.5	*	*	0	10	1		
9/15/2024	01:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	35	38	28 to 38	100.0	*	*	0	2	0		
9/15/2024	02:00	0	0	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	3	32.7	35	25 to 35	100.0	*	*	0	2	1		
9/15/2024	03:00	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	27.5	31	21 to 31	100.0	*	*	0	2	0		
9/15/2024	04:00	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	34	35	25 to 35	100.0	*	*	0	3	0		
9/15/2024	05:00	0	0	1	1	2	2	0	1	0	0	0	0	0	0	0	0	0	0	0	7	32.9	38	25 to 35	57.1	*	*	0	7	0		
9/15/2024	06:00	0	1	3	1	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	8	28	37	16 to 26	62.5	*	*	0	8	0		
9/15/2024	07:00	0	0	0	1	3	3	0	2	1	0	0	0	0	0	0	0	0	0	0	10	38.1	47	26 to 36	60.0	3	30.0	0	10	0		
9/15/2024	08:00	0	0	6	2	8	3	11	0	0	0	0	0	0	0	0	0	0	0	0	30	34	42.5	30 to 40	53.3	*	*	0	30	0		
9/15/2024	09:00	0	0	8	10	8	16	12	5	0	0	0	0	0	0	0	0	0	0	0	59	34.9	42.7	34 to 44	50.8	5	8.5	0	59	0		
9/15/2024	10:00	0	5	9	14	22	18	13	4	0	0	0	0	0	0	0	0	0	0	0	85	32.5	41.2	30 to 40	50.6	3	3.5	0	84	1		
9/15/2024	11:00	0	5	16	12	10	16	14	6	1	0	0	0	0	0	0	0	0	0	0	80	32.3	41.5	32 to 42	43.8	3	3.8	0	77	3		
9/15/2024	12:00	0	1	18	22	32	51	24	2	0	0	0	0	0	0	0	0	0	0	0	150	33.6	39.4	31 to 41	66.7	1	0.7	1	147	2		
9/15/2024	13:00	0	2	16	34	21	34	22	4	0	0	0	0	0	0	0	0	0	0	0	133	32.4	39.6	30 to 40	48.9	3	2.3	2	130	1		
9/15/2024	14:00	0	0	3	6	5	14	2	0	0	0	0	0	0	0	0	0	0	0	0	30	33.2	37.7	28 to 38	70.0	*	*	0	27	3		
9/15/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/15/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/15/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/15/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/15/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/15/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/15/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/15/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/15/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		0	15	83	106	119	164	100	24	2	0	0	0	0	0	0	0	0	0	0	613	33	41	31 to 41	52.0	18	2.9	3	598	12		

Outgoing: Average Hourly Volume for Week of 9/9/2024
Average Counts By Hour (9/9/2024) ● Average Counts By Hour (9/9/2024)

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/9/2024
Average Hourly WEEKDAY Speeds By Hour (9/9/2024) — Average Hourly WEEKDAY Speeds By Hour (9/9/2024)

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct
Average Weekly Speeds

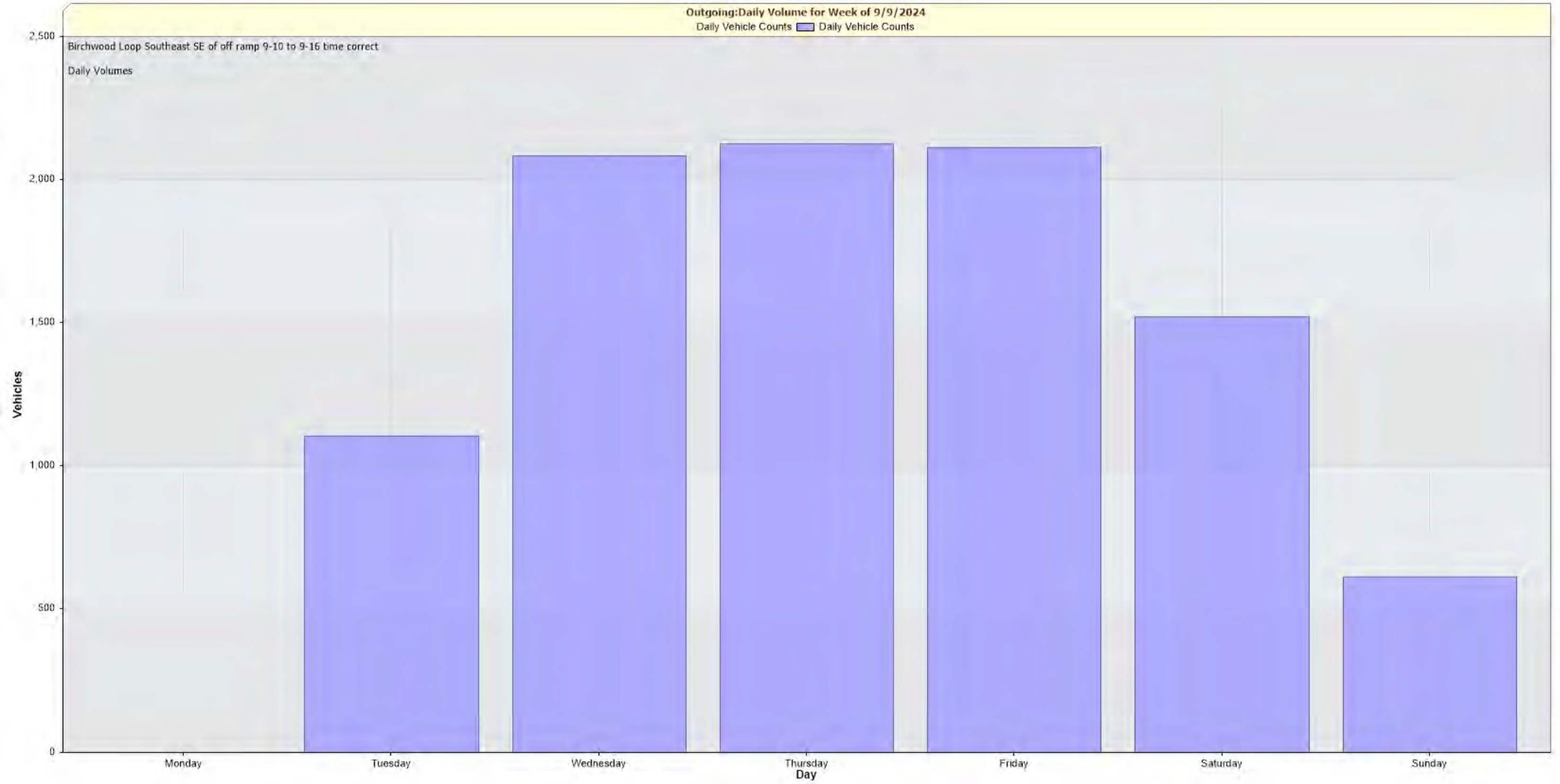


Outgoing: Daily Volume for Week of 9/9/2024

Daily Vehicle Counts

Birchwood Loop Southeast SE of off ramp 9-10 to 9-16 time correct

Daily Volumes



For Project: Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:54:21 PM

Speed Intervals: 1 MPH

Time Intervals: Instant

Traffic Report From: 9/10/2024 2:00:00 PM through 9/16/2024 7:59:59 AM

85th Percentile Speed: 48 MPH

85th Percentile Vehicles: 7031

Max Speed: 76 MPH on 9/14/2024 11:53:10 AM

Total Vehicles: 8272

AADT: 1438

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1181	1181
AM Peak 11:00 AM	82	83
PM Peak 5:00 PM	173	146

Speed

Speed Limit: 65

85th Percentile Speed: 48

50th Percentile Speed: 43

10 MPH Pace Interval: 38.0 MPH to 48.0 MPH

Average Speed: 43.11

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	1	1	0	1	2	2	2
% over limit	1.8	0.1	0.0	0.1	0.1	0.2	0.2
Avg Speeder	70.0	66.0	0.0	66.0	70.5	72.0	73.5
Avg Speed	42.6	43.3	43.4	42.9	43.1	42.6	43.5

Class Counts

	Number	%
VEH_SM	23	0.3
VEH_MED	7800	94.3
VEH_LG	449	5.4
[VEH_SM=motorcycle, VEH_MED = sedan, VEH_LG = truck]		

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/10/2024 3:00:00 PM	47.0	133	157	55	0.0	0.0%	41.7
9/10/2024 4:00:00 PM	47.0	148	174	63	0.0	0.0%	42.2
9/10/2024 5:00:00 PM	49.0	158	186	59	0.0	0.0%	44.1
9/10/2024 6:00:00 PM	51.0	149	175	61	0.0	0.0%	45.0
9/10/2024 7:00:00 PM	49.0	98	115	66	66.0	0.9%	44.1
9/10/2024 8:00:00 PM	49.0	71	84	54	0.0	0.0%	43.8
9/10/2024 9:00:00 PM	47.0	55	65	64	0.0	0.0%	42.8
9/10/2024 10:00:00 PM	45.0	22	26	52	0.0	0.0%	42.4
9/10/2024 11:00:00 PM	48.0	11	13	52	0.0	0.0%	42.4
9/11/2024 12:00:00 AM	36.0	4	5	36	0.0	0.0%	34.0
9/11/2024 1:00:00 AM	44.0	3	3	44	0.0	0.0%	42.0
9/11/2024 2:00:00 AM	48.0	4	5	48	0.0	0.0%	43.2
9/11/2024 3:00:00 AM	47.0	3	3	47	0.0	0.0%	42.3
9/11/2024 4:00:00 AM	**No Data**						
9/11/2024 5:00:00 AM	43.0	2	2	43	0.0	0.0%	39.0
9/11/2024 6:00:00 AM	43.0	5	6	53	0.0	0.0%	42.3
9/11/2024 7:00:00 AM	48.0	19	22	51	0.0	0.0%	44.7
9/11/2024 8:00:00 AM	47.0	73	86	55	0.0	0.0%	42.3
9/11/2024 9:00:00 AM	47.0	78	92	58	0.0	0.0%	42.3
9/11/2024 10:00:00 AM	48.0	71	83	55	0.0	0.0%	42.0
9/11/2024 11:00:00 AM	48.0	56	66	55	0.0	0.0%	43.9
9/11/2024 12:00:00 PM	49.0	68	80	55	0.0	0.0%	43.4
9/11/2024 1:00:00 PM	49.0	80	94	57	0.0	0.0%	43.4
9/11/2024 2:00:00 PM	47.0	76	89	53	0.0	0.0%	42.8
9/11/2024 3:00:00 PM	47.0	132	155	59	0.0	0.0%	42.6
9/11/2024 4:00:00 PM	48.0	122	144	64	0.0	0.0%	43.7
9/11/2024 5:00:00 PM	49.0	139	164	57	0.0	0.0%	44.6
9/11/2024 6:00:00 PM	48.0	139	163	59	0.0	0.0%	43.9
9/11/2024 7:00:00 PM	50.0	106	125	59	0.0	0.0%	44.7
9/11/2024 8:00:00 PM	48.0	71	83	59	0.0	0.0%	43.6
9/11/2024 9:00:00 PM	48.0	73	86	57	0.0	0.0%	42.8
9/11/2024 10:00:00 PM	49.0	26	30	61	0.0	0.0%	42.1
9/11/2024 11:00:00 PM	45.0	9	11	49	0.0	0.0%	41.5
9/12/2024 12:00:00 AM	48.0	8	10	55	0.0	0.0%	42.7
9/12/2024 1:00:00 AM	45.0	6	7	51	0.0	0.0%	39.4
9/12/2024 2:00:00 AM	43.0	3	3	43	0.0	0.0%	37.7
9/12/2024 3:00:00 AM	40.0	2	2	40	0.0	0.0%	38.0
9/12/2024 4:00:00 AM	39.0	3	4	44	0.0	0.0%	38.5
9/12/2024 5:00:00 AM	45.0	3	3	45	0.0	0.0%	42.0
9/12/2024 6:00:00 AM	52.0	8	9	53	0.0	0.0%	39.0
9/12/2024 7:00:00 AM	49.0	19	22	54	0.0	0.0%	43.6
9/12/2024 8:00:00 AM	45.0	76	90	56	0.0	0.0%	41.2
9/12/2024 9:00:00 AM	47.0	70	82	59	0.0	0.0%	42.2
9/12/2024 10:00:00 AM	47.0	55	65	57	0.0	0.0%	42.8
9/12/2024 11:00:00 AM	47.0	59	69	54	0.0	0.0%	41.3
9/12/2024 12:00:00 PM	46.0	73	86	51	0.0	0.0%	42.3
9/12/2024 1:00:00 PM	48.0	81	95	59	0.0	0.0%	43.1

Outgoing Summary
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

9/12/2024 2:00:00 PM	48.0	89	105	60	0.0	0.0%	43.1
9/12/2024 3:00:00 PM	48.0	122	143	58	0.0	0.0%	42.5
9/12/2024 4:00:00 PM	48.0	128	151	58	0.0	0.0%	43.0
9/12/2024 5:00:00 PM	50.0	126	148	66	66.0	0.7%	44.7
9/12/2024 6:00:00 PM	48.0	138	162	57	0.0	0.0%	44.0
9/12/2024 7:00:00 PM	49.0	114	134	64	0.0	0.0%	43.7
9/12/2024 8:00:00 PM	48.0	70	82	57	0.0	0.0%	43.5
9/12/2024 9:00:00 PM	46.0	49	58	56	0.0	0.0%	42.0
9/12/2024 10:00:00 PM	46.0	22	26	56	0.0	0.0%	41.8
9/12/2024 11:00:00 PM	46.0	10	12	53	0.0	0.0%	41.4
9/13/2024 12:00:00 AM	37.0	3	4	57	0.0	0.0%	39.5
9/13/2024 1:00:00 AM	46.0	4	5	54	0.0	0.0%	45.0
9/13/2024 2:00:00 AM	44.0	2	2	44	0.0	0.0%	38.0
9/13/2024 3:00:00 AM	44.0	3	3	44	0.0	0.0%	39.7
9/13/2024 4:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
9/13/2024 5:00:00 AM	50.0	5	6	54	0.0	0.0%	42.3
9/13/2024 6:00:00 AM	40.0	5	6	45	0.0	0.0%	39.2
9/13/2024 7:00:00 AM	49.0	21	25	57	0.0	0.0%	45.0
9/13/2024 8:00:00 AM	46.0	80	94	57	0.0	0.0%	41.8
9/13/2024 9:00:00 AM	47.0	52	61	54	0.0	0.0%	42.6
9/13/2024 10:00:00 AM	48.0	68	80	53	0.0	0.0%	41.4
9/13/2024 11:00:00 AM	48.0	55	65	52	0.0	0.0%	42.1
9/13/2024 12:00:00 PM	47.0	68	80	53	0.0	0.0%	42.7
9/13/2024 1:00:00 PM	48.0	80	94	56	0.0	0.0%	43.5
9/13/2024 2:00:00 PM	47.0	105	123	63	0.0	0.0%	43.3
9/13/2024 3:00:00 PM	48.0	139	164	55	0.0	0.0%	43.4
9/13/2024 4:00:00 PM	49.0	116	136	73	73.0	0.7%	43.6
9/13/2024 5:00:00 PM	48.0	133	156	64	0.0	0.0%	43.5
9/13/2024 6:00:00 PM	48.0	163	192	68	68.0	0.5%	43.8
9/13/2024 7:00:00 PM	48.0	121	142	58	0.0	0.0%	44.2
9/13/2024 8:00:00 PM	50.0	90	106	61	0.0	0.0%	44.2
9/13/2024 9:00:00 PM	49.0	75	88	54	0.0	0.0%	42.4
9/13/2024 10:00:00 PM	46.0	28	33	53	0.0	0.0%	40.2
9/13/2024 11:00:00 PM	49.0	13	15	57	0.0	0.0%	41.8
9/14/2024 12:00:00 AM	44.0	8	10	50	0.0	0.0%	38.9
9/14/2024 1:00:00 AM	43.0	3	4	47	0.0	0.0%	41.0
9/14/2024 2:00:00 AM	45.0	9	11	58	0.0	0.0%	41.8
9/14/2024 3:00:00 AM	68.0	3	3	68	68.0	33.3%	49.3
9/14/2024 4:00:00 AM	53.0	2	2	53	0.0	0.0%	46.5
9/14/2024 5:00:00 AM	48.0	7	8	51	0.0	0.0%	42.9
9/14/2024 6:00:00 AM	41.0	4	5	42	0.0	0.0%	37.8
9/14/2024 7:00:00 AM	43.0	11	13	50	0.0	0.0%	40.9
9/14/2024 8:00:00 AM	47.0	18	21	50	0.0	0.0%	41.9
9/14/2024 9:00:00 AM	47.0	36	42	62	0.0	0.0%	41.3
9/14/2024 10:00:00 AM	45.0	55	65	52	0.0	0.0%	41.3
9/14/2024 11:00:00 AM	48.0	99	117	62	0.0	0.0%	42.6
9/14/2024 12:00:00 PM	47.0	77	91	76	76.0	1.1%	42.7
9/14/2024 1:00:00 PM	47.0	84	99	55	0.0	0.0%	41.8

Outgoing Summary
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

9/14/2024 2:00:00 PM	48.0	88	103	58	0.0	0.0%	42.8
9/14/2024 3:00:00 PM	48.0	86	101	55	0.0	0.0%	43.4
9/14/2024 4:00:00 PM	47.0	84	99	58	0.0	0.0%	42.0
9/14/2024 5:00:00 PM	48.0	83	98	56	0.0	0.0%	42.8
9/14/2024 6:00:00 PM	48.0	87	102	56	0.0	0.0%	43.3
9/14/2024 7:00:00 PM	49.0	67	79	64	0.0	0.0%	44.5
9/14/2024 8:00:00 PM	47.0	52	61	57	0.0	0.0%	42.7
9/14/2024 9:00:00 PM	46.0	48	56	58	0.0	0.0%	41.5
9/14/2024 10:00:00 PM	46.0	29	34	56	0.0	0.0%	41.7
9/14/2024 11:00:00 PM	51.0	11	13	58	0.0	0.0%	45.8
9/15/2024 12:00:00 AM	47.0	8	10	50	0.0	0.0%	41.5
9/15/2024 1:00:00 AM	61.0	3	4	63	0.0	0.0%	48.8
9/15/2024 2:00:00 AM	45.0	3	3	45	0.0	0.0%	39.7
9/15/2024 3:00:00 AM	49.0	3	4	50	0.0	0.0%	46.5
9/15/2024 4:00:00 AM	45.0	1	1	45	0.0	0.0%	45.0
9/15/2024 5:00:00 AM	44.0	3	4	46	0.0	0.0%	44.5
9/15/2024 6:00:00 AM	42.0	1	1	42	0.0	0.0%	42.0
9/15/2024 7:00:00 AM	49.0	5	6	49	0.0	0.0%	42.7
9/15/2024 8:00:00 AM	47.0	13	15	54	0.0	0.0%	42.5
9/15/2024 9:00:00 AM	45.0	19	22	54	0.0	0.0%	41.2
9/15/2024 10:00:00 AM	46.0	33	39	53	0.0	0.0%	43.0
9/15/2024 11:00:00 AM	46.0	67	79	62	0.0	0.0%	42.3
9/15/2024 12:00:00 PM	47.0	67	79	54	0.0	0.0%	41.6
9/15/2024 1:00:00 PM	47.0	111	131	57	0.0	0.0%	43.1
9/15/2024 2:00:00 PM	48.0	90	106	56	0.0	0.0%	44.2
9/15/2024 3:00:00 PM	48.0	87	102	57	0.0	0.0%	43.6
9/15/2024 4:00:00 PM	50.0	69	81	71	71.0	1.2%	44.1
9/15/2024 5:00:00 PM	48.0	82	96	61	0.0	0.0%	43.5
9/15/2024 6:00:00 PM	49.0	72	85	60	0.0	0.0%	43.9
9/15/2024 7:00:00 PM	49.0	68	80	62	0.0	0.0%	44.6
9/15/2024 8:00:00 PM	49.0	63	74	76	76.0	1.4%	44.7
9/15/2024 9:00:00 PM	47.0	63	74	61	0.0	0.0%	43.4
9/15/2024 10:00:00 PM	49.0	15	18	59	0.0	0.0%	44.5
9/15/2024 11:00:00 PM	51.0	14	16	54	0.0	0.0%	42.9
9/16/2024 12:00:00 AM	51.0	7	8	56	0.0	0.0%	41.4
9/16/2024 1:00:00 AM	42.0	3	4	42	0.0	0.0%	38.8
9/16/2024 2:00:00 AM	48.0	3	3	48	0.0	0.0%	38.3
9/16/2024 3:00:00 AM	64.0	3	4	70	70.0	25.0%	53.3
9/16/2024 4:00:00 AM	**No Data**						
9/16/2024 5:00:00 AM	45.0	4	5	48	0.0	0.0%	41.6
9/16/2024 6:00:00 AM	45.0	4	5	49	0.0	0.0%	40.4
9/16/2024 7:00:00 AM	47.0	21	25	52	0.0	0.0%	43.4
9/16/2024 8:00:00 AM	44.0	8	10	48	0.0	0.0%	40.6

Outgoing Summary
Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/11/2024 12:00:00 AM	49.0	850	1000	66	66.0	0.1%	43.3
9/12/2024 12:00:00 AM	48.0	1362	1602	64	0.0	0.0%	43.4
9/13/2024 12:00:00 AM	48.0	1328	1562	66	66.0	0.1%	42.9
9/14/2024 12:00:00 AM	48.0	1434	1687	73	70.5	0.1%	43.1
9/15/2024 12:00:00 AM	48.0	1051	1237	76	72.0	0.2%	42.6
9/16/2024 12:00:00 AM	48.0	959	1128	76	73.5	0.2%	43.5
9/16/2024 7:59:59 AM	48.0	48	56	70	70.0	1.8%	42.6

Outgoing Weekly Counts

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	3	7	5	4	4	5	4	45
1 - 2	*	*	5	3	2	11	3	3.33	7	44.83
2 - 3	*	*	3	2	3	3	4	2.67	3.5	43.67
3 - 4	*	*	0	4	1	2	1	1.67	1.5	37
4 - 5	*	*	2	3	6	8	4	3.67	6	46
5 - 6	*	*	6	9	6	5	1	7	3	45
6 - 7	*	*	22	22	25	13	6	23	9.5	48.5
7 - 8	*	*	86	90	94	21	15	90	18	45.6
8 - 9	*	*	92	82	61	42	22	78.33	32	46.67
9 - 10	*	*	83	65	80	65	39	76	52	47.23
10 - 11	*	*	66	69	65	117	79	66.67	98	47.4
11 - 12	*	*	80	86	80	91	79	82	85	47.13
12 - 13	*	*	94	95	94	99	131	94.33	115	47.63
13 - 14	*	*	89	105	123	103	106	105.67	104.5	47.27
14 - 15	*	157	155	143	164	101	102	154.75	101.5	47.1
15 - 16	*	174	144	151	136	99	81	151.25	90	47.7
16 - 17	*	186	164	148	156	98	96	163.5	97	48.6
17 - 18	*	175	163	162	192	102	85	173	93.5	48.5
18 - 19	*	115	125	134	142	79	80	129	79.5	48.42
19 - 20	*	84	83	82	106	61	74	88.75	67.5	48.35
20 - 21	*	65	86	58	88	56	74	74.25	65	46.98
21 - 22	*	26	30	26	33	34	18	28.75	26	46.38
22 - 23	*	13	11	12	15	13	16	12.75	14.5	46.92
23 - 24	*	5	10	4	10	10	8	7.25	9	41.12
Totals	0	1000	1602	1562	1687	1237	1128			
% of Total	0%	12.17%	19.5%	19.01%	20.53%	15.06%	13.73%			

Outgoing Weekly Counts

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	4	*	*	*	*	*	*	4	0	41.5
1 - 2	3	*	*	*	*	*	*	3	0	48
2 - 3	4	*	*	*	*	*	*	4	0	64
3 - 4	0	*	*	*	*	*	*	0	0	0
4 - 5	5	*	*	*	*	*	*	5	0	45
5 - 6	5	*	*	*	*	*	*	5	0	45
6 - 7	25	*	*	*	*	*	*	25	0	46.5
7 - 8	10	*	*	*	*	*	*	10	0	44
8 - 9	*	*	*	*	*	*	*	0	0	0
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	*	*	*	0	0	0
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	56	0	0	0	0	0	0			
% of Total	100%	0%	0%	0%	0%	0%	0%			

Outgoing Monthly Counts

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	4	*	3	7	5	4	4	4.75	4	44.12
1 - 2	3	*	5	3	2	11	3	3.25	7	45.62
2 - 3	4	*	3	2	3	3	4	3	3.5	48.75
3 - 4	0	*	0	4	1	2	1	1.25	1.5	37
4 - 5	5	*	2	3	6	8	4	4	6	45.75
5 - 6	5	*	6	9	6	5	1	6.5	3	45
6 - 7	25	*	22	22	25	13	6	23.5	9.5	48
7 - 8	10	*	86	90	94	21	15	70	18	45.2
8 - 9	*	*	92	82	61	42	22	78.33	32	46.67
9 - 10	*	*	83	65	80	65	39	76	52	47.23
10 - 11	*	*	66	69	65	117	79	66.67	98	47.4
11 - 12	*	*	80	86	80	91	79	82	85	47.13
12 - 13	*	*	94	95	94	99	131	94.33	115	47.63
13 - 14	*	*	89	105	123	103	106	105.67	104.5	47.27
14 - 15	*	157	155	143	164	101	102	154.75	101.5	47.1
15 - 16	*	174	144	151	136	99	81	151.25	90	47.7
16 - 17	*	186	164	148	156	98	96	163.5	97	48.6
17 - 18	*	175	163	162	192	102	85	173	93.5	48.5
18 - 19	*	115	125	134	142	79	80	129	79.5	48.42
19 - 20	*	84	83	82	106	61	74	88.75	67.5	48.35
20 - 21	*	65	86	58	88	56	74	74.25	65	46.98
21 - 22	*	26	30	26	33	34	18	28.75	26	46.38
22 - 23	*	13	11	12	15	13	16	12.75	14.5	46.92
23 - 24	*	5	10	4	10	10	8	7.25	9	41.12
Totals	56	1000	1602	1562	1687	1237	1128			
% of Total	0.68%	12.09%	19.37%	18.88%	20.39%	14.95%	13.64%			

Outgoing Weekly Speeds

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	42	39.43	45	41	48.75	41.8	44.88	45
1 - 2	*	*	43.2	37.67	38	41.82	39.67	40.5	41.36	44.83
2 - 3	*	*	42.33	38	39.67	49.33	46.5	40.25	47.71	43.67
3 - 4	*	*	0	38.5	35	46.5	45	37.8	46	37
4 - 5	*	*	39	42	42.33	42.88	44.5	41.64	43.42	46
5 - 6	*	*	42.33	39	39.17	37.8	42	40	38.5	45
6 - 7	*	*	44.68	43.59	45.04	40.92	42.67	44.46	41.47	48.5
7 - 8	*	*	42.27	41.16	41.81	41.86	42.53	41.74	42.14	45.6
8 - 9	*	*	42.28	42.17	42.59	41.31	41.23	42.32	41.28	46.67
9 - 10	*	*	41.98	42.77	41.44	41.28	42.97	42.01	41.91	47.23
10 - 11	*	*	43.86	41.33	42.06	42.65	42.33	42.4	42.52	47.4
11 - 12	*	*	43.42	42.27	42.69	42.75	41.57	42.78	42.2	47.13
12 - 13	*	*	43.43	43.06	43.54	41.79	43.14	43.34	42.56	47.63
13 - 14	*	*	42.8	43.12	43.28	42.83	44.15	43.09	43.5	47.27
14 - 15	*	41.71	42.64	42.53	43.38	43.39	43.63	42.58	43.51	47.1
15 - 16	*	42.23	43.69	42.95	43.65	42.03	44.07	43.08	42.95	47.7
16 - 17	*	44.11	44.62	44.7	43.53	42.83	43.51	44.23	43.16	48.6
17 - 18	*	44.97	43.92	43.95	43.75	43.28	43.91	44.14	43.57	48.5
18 - 19	*	44.08	44.69	43.69	44.15	44.54	44.64	44.15	44.59	48.42
19 - 20	*	43.75	43.6	43.48	44.18	42.67	44.73	43.78	43.8	48.35
20 - 21	*	42.8	42.81	42	42.42	41.52	43.36	42.54	42.57	46.98
21 - 22	*	42.42	42.13	41.81	40.21	41.68	44.5	41.57	42.65	46.38
22 - 23	*	42.38	41.55	41.42	41.8	45.85	42.88	41.8	44.21	46.92
23 - 24	*	34	42.7	39.5	38.9	41.5	41.38	39.45	41.44	41.12
Totals	0	43.3	43.4	42.9	43.1	42.6	43.5			
% of Total	0%	16.73%	16.77%	16.58%	16.65%	16.46%	16.81%			

Outgoing Weekly Speeds

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	38.75	*	*	*	*	*	*	38.75	*	41.5
1 - 2	38.33	*	*	*	*	*	*	38.33	*	48
2 - 3	53.25	*	*	*	*	*	*	53.25	*	64
3 - 4	0	*	*	*	*	*	*	*	*	0
4 - 5	41.6	*	*	*	*	*	*	41.6	*	45
5 - 6	40.4	*	*	*	*	*	*	40.4	*	45
6 - 7	43.44	*	*	*	*	*	*	43.44	*	46.5
7 - 8	40.6	*	*	*	*	*	*	40.6	*	44
8 - 9	*	*	*	*	*	*	*	*	*	0
9 - 10	*	*	*	*	*	*	*	*	*	0
10 - 11	*	*	*	*	*	*	*	*	*	0
11 - 12	*	*	*	*	*	*	*	*	*	0
12 - 13	*	*	*	*	*	*	*	*	*	0
13 - 14	*	*	*	*	*	*	*	*	*	0
14 - 15	*	*	*	*	*	*	*	*	*	0
15 - 16	*	*	*	*	*	*	*	*	*	0
16 - 17	*	*	*	*	*	*	*	*	*	0
17 - 18	*	*	*	*	*	*	*	*	*	0
18 - 19	*	*	*	*	*	*	*	*	*	0
19 - 20	*	*	*	*	*	*	*	*	*	0
20 - 21	*	*	*	*	*	*	*	*	*	0
21 - 22	*	*	*	*	*	*	*	*	*	0
22 - 23	*	*	*	*	*	*	*	*	*	0
23 - 24	*	*	*	*	*	*	*	*	*	0
Totals	42.6	0	0	0	0	0	0			
% of Total	100%	0%	0%	0%	0%	0%	0%			

Outgoing Monthly Speeds

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	38.75	*	42	39.43	45	41	48.75	41.16	44.88	44.12
1 - 2	38.33	*	43.2	37.67	38	41.82	39.67	40	41.36	45.62
2 - 3	53.25	*	42.33	38	39.67	49.33	46.5	44.58	47.71	48.75
3 - 4	0	*	0	38.5	35	46.5	45	37.8	46	37
4 - 5	41.6	*	39	42	42.33	42.88	44.5	41.62	43.42	45.75
5 - 6	40.4	*	42.33	39	39.17	37.8	42	40.08	38.5	45
6 - 7	43.44	*	44.68	43.59	45.04	40.92	42.67	44.19	41.47	48
7 - 8	40.6	*	42.27	41.16	41.81	41.86	42.53	41.7	42.14	45.2
8 - 9	*	*	42.28	42.17	42.59	41.31	41.23	42.32	41.28	46.67
9 - 10	*	*	41.98	42.77	41.44	41.28	42.97	42.01	41.91	47.23
10 - 11	*	*	43.86	41.33	42.06	42.65	42.33	42.4	42.52	47.4
11 - 12	*	*	43.42	42.27	42.69	42.75	41.57	42.78	42.2	47.13
12 - 13	*	*	43.43	43.06	43.54	41.79	43.14	43.34	42.56	47.63
13 - 14	*	*	42.8	43.12	43.28	42.83	44.15	43.09	43.5	47.27
14 - 15	*	41.71	42.64	42.53	43.38	43.39	43.63	42.58	43.51	47.1
15 - 16	*	42.23	43.69	42.95	43.65	42.03	44.07	43.08	42.95	47.7
16 - 17	*	44.11	44.62	44.7	43.53	42.83	43.51	44.23	43.16	48.6
17 - 18	*	44.97	43.92	43.95	43.75	43.28	43.91	44.14	43.57	48.5
18 - 19	*	44.08	44.69	43.69	44.15	44.54	44.64	44.15	44.59	48.42
19 - 20	*	43.75	43.6	43.48	44.18	42.67	44.73	43.78	43.8	48.35
20 - 21	*	42.8	42.81	42	42.42	41.52	43.36	42.54	42.57	46.98
21 - 22	*	42.42	42.13	41.81	40.21	41.68	44.5	41.57	42.65	46.38
22 - 23	*	42.38	41.55	41.42	41.8	45.85	42.88	41.8	44.21	46.92
23 - 24	*	34	42.7	39.5	38.9	41.5	41.38	39.45	41.44	41.12
Totals	42.6	43.3	43.4	42.9	43.1	42.6	43.5			
% of Total	14.13%	14.37%	14.4%	14.23%	14.3%	14.13%	14.43%			

Outgoing Weekly EightyFifthSpeeds
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	9/9/2024	to	9/15/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/9/2024	9/10/2024	9/11/2024	9/12/2024	9/13/2024	9/14/2024	9/15/2024	Day Avg	Avg	Avg Speed
0 - 1	*	*	44	45	46	43	61	45	52	45
1 - 2	*	*	47.5	43	44	45	45	44.83	45	44.83
2 - 3	*	*	47	40	44	68	49	43.67	58.5	43.67
3 - 4	*	*	0	39	35	53	45	37	49	37
4 - 5	*	*	43	45	50	48	44	46	46	46
5 - 6	*	*	43	52	40	41	42	45	41.5	45
6 - 7	*	*	48	48.5	49	43	48.5	48.5	45.75	48.5
7 - 8	*	*	46.5	44.9	45.4	46.5	47	45.6	46.75	45.6
8 - 9	*	*	46.2	47	46.8	47	45	46.67	46	46.67
9 - 10	*	*	47.5	46.5	47.7	44.4	46	47.23	45.2	47.23
10 - 11	*	*	48	46.2	48	47.2	46	47.4	46.6	47.4
11 - 12	*	*	48.8	46	46.6	47	46.3	47.13	46.65	47.13
12 - 13	*	*	48.3	47.1	47.5	46.2	46.7	47.63	46.45	47.63
13 - 14	*	*	46.8	48	47	47.3	47.3	47.27	47.3	47.27
14 - 15	*	46.5	46.7	47.6	47.6	48	48	47.1	48	47.1
15 - 16	*	46.6	47.5	47.7	49	46.3	49.5	47.7	47.9	47.7
16 - 17	*	48.7	48.7	49.8	47.2	47.5	47.3	48.6	47.4	48.6
17 - 18	*	50.5	48	47.6	47.9	47.3	48.7	48.5	48	48.5
18 - 19	*	48.4	49.1	48.3	47.9	48.8	48.8	48.42	48.8	48.42
19 - 20	*	48.5	48	47.6	49.3	46.5	49	48.35	47.75	48.35
20 - 21	*	46.3	47.7	45.8	48.1	46	47	46.98	46.5	46.98
21 - 22	*	45	49	45.5	46	45.7	49	46.38	47.35	46.38
22 - 23	*	48	45	45.7	49	51	51	46.92	51	46.92
23 - 24	*	35.5	48	37	44	46.5	51	41.12	48.75	41.12
Totals	0	464	1082.3	1100.8	1113	1140.2	1148.1			
% of Total	0%	7.67%	17.89%	18.2%	18.4%	18.85%	18.98%			

Outgoing Weekly EightyFifthSpeeds
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	41.5	*	*	*	*	*	*	41.5	0	41.5
1 - 2	48	*	*	*	*	*	*	48	0	48
2 - 3	64	*	*	*	*	*	*	64	0	64
3 - 4	0	*	*	*	*	*	*	0	0	0
4 - 5	45	*	*	*	*	*	*	45	0	45
5 - 6	45	*	*	*	*	*	*	45	0	45
6 - 7	46.5	*	*	*	*	*	*	46.5	0	46.5
7 - 8	44	*	*	*	*	*	*	44	0	44
8 - 9	*	*	*	*	*	*	*	0	0	0
9 - 10	*	*	*	*	*	*	*	0	0	0
10 - 11	*	*	*	*	*	*	*	0	0	0
11 - 12	*	*	*	*	*	*	*	0	0	0
12 - 13	*	*	*	*	*	*	*	0	0	0
13 - 14	*	*	*	*	*	*	*	0	0	0
14 - 15	*	*	*	*	*	*	*	0	0	0
15 - 16	*	*	*	*	*	*	*	0	0	0
16 - 17	*	*	*	*	*	*	*	0	0	0
17 - 18	*	*	*	*	*	*	*	0	0	0
18 - 19	*	*	*	*	*	*	*	0	0	0
19 - 20	*	*	*	*	*	*	*	0	0	0
20 - 21	*	*	*	*	*	*	*	0	0	0
21 - 22	*	*	*	*	*	*	*	0	0	0
22 - 23	*	*	*	*	*	*	*	0	0	0
23 - 24	*	*	*	*	*	*	*	0	0	0
Totals	334	0	0	0	0	0	0			
% of Total	100%	0%	0%	0%	0%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

	Sep 2024									
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
0 - 1	41.5	*	44	45	46	43	61	44.12	52	44.12
1 - 2	48	*	47.5	43	44	45	45	45.62	45	45.62
2 - 3	64	*	47	40	44	68	49	48.75	58.5	48.75
3 - 4	0	*	0	39	35	53	45	37	49	37
4 - 5	45	*	43	45	50	48	44	45.75	46	45.75
5 - 6	45	*	43	52	40	41	42	45	41.5	45
6 - 7	46.5	*	48	48.5	49	43	48.5	48	45.75	48
7 - 8	44	*	46.5	44.9	45.4	46.5	47	45.2	46.75	45.2
8 - 9	*	*	46.2	47	46.8	47	45	46.67	46	46.67
9 - 10	*	*	47.5	46.5	47.7	44.4	46	47.23	45.2	47.23
10 - 11	*	*	48	46.2	48	47.2	46	47.4	46.6	47.4
11 - 12	*	*	48.8	46	46.6	47	46.3	47.13	46.65	47.13
12 - 13	*	*	48.3	47.1	47.5	46.2	46.7	47.63	46.45	47.63
13 - 14	*	*	46.8	48	47	47.3	47.3	47.27	47.3	47.27
14 - 15	*	46.5	46.7	47.6	47.6	48	48	47.1	48	47.1
15 - 16	*	46.6	47.5	47.7	49	46.3	49.5	47.7	47.9	47.7
16 - 17	*	48.7	48.7	49.8	47.2	47.5	47.3	48.6	47.4	48.6
17 - 18	*	50.5	48	47.6	47.9	47.3	48.7	48.5	48	48.5
18 - 19	*	48.4	49.1	48.3	47.9	48.8	48.8	48.42	48.8	48.42
19 - 20	*	48.5	48	47.6	49.3	46.5	49	48.35	47.75	48.35
20 - 21	*	46.3	47.7	45.8	48.1	46	47	46.98	46.5	46.98
21 - 22	*	45	49	45.5	46	45.7	49	46.38	47.35	46.38
22 - 23	*	48	45	45.7	49	51	51	46.92	51	46.92
23 - 24	*	35.5	48	37	44	46.5	51	41.12	48.75	41.12

Summary of Violators

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	27	42.3	0	0.0
01:00:00	27	40.7	0	0.0
02:00:00	19	45.7	2	69.0
03:00:00	8	40.9	0	0.0
04:00:00	28	42.4	0	0.0
05:00:00	32	39.8	0	0.0
06:00:00	113	43.7	0	0.0
07:00:00	316	41.7	0	0.0
08:00:00	299	42.1	0	0.0
09:00:00	332	42.0	0	0.0
10:00:00	396	42.5	0	0.0
11:00:00	416	42.5	1	76.0
12:00:00	513	43.0	0	0.0
13:00:00	526	43.3	0	0.0
14:00:00	822	42.8	0	0.0
15:00:00	785	43.0	2	72.0
16:00:00	848	44.0	1	66.0
17:00:00	879	44.0	1	68.0
18:00:00	675	44.3	1	66.0
19:00:00	490	43.8	1	76.0
20:00:00	427	42.5	0	0.0
21:00:00	167	41.9	0	0.0
22:00:00	80	42.7	0	0.0
23:00:00	47	40.2	0	0.0

Outgoing Histogram
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/10/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/10/2024	14:00	0	0	1	0	13	35	67	29	11	1	0	0	0	0	0	0	0	0	157	41.7	46.5	36 to 46	73.2	*	*	0	146	11	
9/10/2024	15:00	0	0	1	0	6	38	83	37	7	1	1	0	0	0	0	0	0	0	174	42.2	46.6	38 to 48	77.6	*	*	0	159	15	
9/10/2024	16:00	0	0	0	1	3	28	69	58	22	5	0	0	0	0	0	0	0	0	186	44.1	48.7	38 to 48	78.0	*	*	0	175	11	
9/10/2024	17:00	0	0	0	0	4	24	60	54	19	13	1	0	0	0	0	0	0	0	175	45	50.5	38 to 48	69.7	*	*	0	167	8	
9/10/2024	18:00	0	0	0	0	2	18	44	38	10	1	1	1	0	0	0	0	0	0	115	44.1	48.4	39 to 49	74.8	1	0.9	0	110	5	
9/10/2024	19:00	0	0	0	0	0	16	34	24	10	0	0	0	0	0	0	0	0	0	84	43.8	48.5	39 to 49	77.4	*	*	0	81	3	
9/10/2024	20:00	0	0	0	0	0	13	32	15	3	0	2	0	0	0	0	0	0	0	65	42.8	46.3	38 to 48	81.5	*	*	0	62	3	
9/10/2024	21:00	0	0	0	0	2	3	15	3	3	0	0	0	0	0	0	0	0	0	26	42.4	45	35 to 45	76.9	*	*	0	25	1	
9/10/2024	22:00	0	0	0	1	0	4	2	4	2	0	0	0	0	0	0	0	0	0	13	42.4	48	38 to 48	69.2	*	*	0	12	1	
9/10/2024	23:00	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	5	34	35.5	26 to 36	100.0	*	*	0	5	0	
24 Hr Summary		0	0	2	2	33	181	406	262	87	21	5	1	0	0	0	0	0	0	1000	43.3	49	38 to 48	73.1	1	0.1	0	942	58	

Outgoing Histogram
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/11/2024	00:00	0	0	0	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	3	42	44	34 to 44	100.0	*	*	0	3	0	
9/11/2024	01:00	0	0	0	0	0	1	2	2	0	0	0	0	0	0	0	0	0	0	5	43.2	47.5	38 to 48	80.0	*	*	1	3	1	
9/11/2024	02:00	0	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	3	42.3	47	37 to 47	100.0	*	*	0	3	0	
9/11/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/11/2024	04:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	39	43	33 to 43	100.0	*	*	0	1	1	
9/11/2024	05:00	0	0	0	0	0	1	4	0	1	0	0	0	0	0	0	0	0	0	6	42.3	43	33 to 43	83.3	*	*	0	6	0	
9/11/2024	06:00	0	0	0	0	0	1	11	7	3	0	0	0	0	0	0	0	0	0	22	44.7	48	41 to 51	90.9	*	*	0	22	0	
9/11/2024	07:00	0	0	0	1	9	15	34	18	8	1	0	0	0	0	0	0	0	0	86	42.3	46.5	37 to 47	72.1	*	*	0	78	8	
9/11/2024	08:00	0	0	1	0	3	21	43	18	5	1	0	0	0	0	0	0	0	0	92	42.3	46.2	37 to 47	78.3	*	*	0	82	10	
9/11/2024	09:00	0	0	0	0	4	24	32	14	8	1	0	0	0	0	0	0	0	0	83	42	47.5	35 to 45	73.5	*	*	0	76	7	
9/11/2024	10:00	0	0	0	0	2	9	27	18	8	2	0	0	0	0	0	0	0	0	66	43.9	48	38 to 48	75.8	*	*	0	63	3	
9/11/2024	11:00	0	0	0	1	2	16	27	23	10	1	0	0	0	0	0	0	0	0	80	43.4	48.8	37 to 47	73.8	*	*	0	74	6	
9/11/2024	12:00	0	0	0	1	5	17	29	30	9	3	0	0	0	0	0	0	0	0	94	43.4	48.3	38 to 48	73.4	*	*	0	92	2	
9/11/2024	13:00	0	0	0	0	6	14	36	24	9	0	0	0	0	0	0	0	0	0	89	42.8	46.8	37 to 47	76.4	*	*	2	81	6	
9/11/2024	14:00	0	0	0	1	5	36	62	36	11	4	0	0	0	0	0	0	0	0	155	42.6	46.7	36 to 46	75.5	*	*	0	143	12	
9/11/2024	15:00	0	0	0	1	4	20	57	45	12	4	1	0	0	0	0	0	0	0	144	43.7	47.5	37 to 47	74.3	*	*	1	138	5	
9/11/2024	16:00	0	0	0	0	1	16	72	52	20	3	0	0	0	0	0	0	0	0	164	44.6	48.7	41 to 51	79.3	*	*	0	159	5	
9/11/2024	17:00	0	0	0	0	2	24	63	58	13	3	0	0	0	0	0	0	0	0	163	43.9	48	38 to 48	80.4	*	*	0	158	5	
9/11/2024	18:00	0	0	0	1	1	13	51	39	16	4	0	0	0	0	0	0	0	0	125	44.7	49.1	40 to 50	77.6	*	*	0	123	2	
9/11/2024	19:00	0	0	0	0	3	10	40	21	7	2	0	0	0	0	0	0	0	0	83	43.6	48	39 to 49	79.5	*	*	1	77	5	
9/11/2024	20:00	0	0	0	0	4	18	34	21	8	1	0	0	0	0	0	0	0	0	86	42.8	47.7	38 to 48	74.4	*	*	0	82	4	
9/11/2024	21:00	0	0	1	0	2	9	7	7	3	0	1	0	0	0	0	0	0	0	30	42.1	49	36 to 46	63.3	*	*	0	27	3	
9/11/2024	22:00	0	0	0	0	2	1	5	3	0	0	0	0	0	0	0	0	0	0	11	41.5	45	36 to 46	72.7	*	*	0	11	0	
9/11/2024	23:00	0	0	1	0	0	2	2	3	1	1	0	0	0	0	0	0	0	0	10	42.7	48	38 to 48	60.0	*	*	0	9	1	
24 Hr Summary		0	0	3	6	55	270	643	440	152	31	2	0	0	0	0	0	0	0	1602	43.4	48	38 to 48	74.6	*	*	5	1511	86	

Outgoing Histogram

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/12/2024	00:00	0	0	0	0	2	3	0	1	1	0	0	0	0	0	0	0	0	0	7	39.4	45	29 to 39	71.4	*	*	0	6	1
9/12/2024	01:00	0	0	0	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	3	37.7	43	30 to 40	66.7	*	*	0	2	1
9/12/2024	02:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	38	40	30 to 40	100.0	*	*	0	2	0
9/12/2024	03:00	0	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	4	38.5	39	34 to 44	100.0	*	*	0	4	0
9/12/2024	04:00	0	0	0	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	3	42	45	35 to 45	100.0	*	*	0	3	0
9/12/2024	05:00	0	0	0	1	2	3	0	1	2	0	0	0	0	0	0	0	0	0	9	39	52	27 to 37	66.7	*	*	1	6	2
9/12/2024	06:00	0	0	0	0	1	2	12	5	2	0	0	0	0	0	0	0	0	0	22	43.6	48.5	36 to 46	77.3	*	*	0	20	2
9/12/2024	07:00	0	0	0	1	9	24	34	16	3	3	0	0	0	0	0	0	0	0	90	41.2	44.9	35 to 45	74.4	*	*	1	79	10
9/12/2024	08:00	0	0	0	0	4	20	36	17	3	2	0	0	0	0	0	0	0	0	82	42.2	47	38 to 48	78.0	*	*	0	74	8
9/12/2024	09:00	0	0	0	0	4	11	30	14	5	1	0	0	0	0	0	0	0	0	65	42.8	46.5	37 to 47	76.9	*	*	0	55	10
9/12/2024	10:00	0	0	0	1	6	16	27	16	3	0	0	0	0	0	0	0	0	0	69	41.3	46.2	35 to 45	69.6	*	*	0	64	5
9/12/2024	11:00	0	0	0	0	2	15	44	22	3	0	0	0	0	0	0	0	0	0	86	42.3	46	38 to 48	83.7	*	*	1	79	6
9/12/2024	12:00	0	0	0	0	6	12	39	33	2	3	0	0	0	0	0	0	0	0	95	43.1	47.1	38 to 48	81.1	*	*	0	91	4
9/12/2024	13:00	0	0	0	0	2	19	54	20	6	3	1	0	0	0	0	0	0	0	105	43.1	48	36 to 46	78.1	*	*	1	97	7
9/12/2024	14:00	0	0	0	0	5	31	59	34	13	1	0	0	0	0	0	0	0	0	143	42.5	47.6	37 to 47	74.1	*	*	1	132	10
9/12/2024	15:00	0	0	0	3	6	22	67	34	17	2	0	0	0	0	0	0	0	0	151	43	47.7	40 to 50	74.8	*	*	1	142	8
9/12/2024	16:00	0	0	0	0	2	20	59	39	23	1	3	1	0	0	0	0	0	0	148	44.7	49.8	39 to 49	73.0	1	0.7	0	143	5
9/12/2024	17:00	0	0	0	0	0	27	66	51	13	5	0	0	0	0	0	0	0	0	162	44	47.6	38 to 48	79.6	*	*	0	160	2
9/12/2024	18:00	0	0	0	3	1	20	53	41	15	0	1	0	0	0	0	0	0	0	134	43.7	48.3	38 to 48	74.6	*	*	1	127	6
9/12/2024	19:00	0	0	0	0	1	16	34	25	4	2	0	0	0	0	0	0	0	0	82	43.5	47.6	38 to 48	81.7	*	*	0	80	2
9/12/2024	20:00	0	0	0	0	2	19	17	16	3	1	0	0	0	0	0	0	0	0	58	42	45.8	36 to 46	81.0	*	*	0	54	4
9/12/2024	21:00	0	0	0	0	2	7	9	6	1	1	0	0	0	0	0	0	0	0	26	41.8	45.5	37 to 47	80.8	*	*	0	26	0
9/12/2024	22:00	0	0	0	1	0	3	4	3	1	0	0	0	0	0	0	0	0	0	12	41.4	45.7	36 to 46	75.0	*	*	0	11	1
9/12/2024	23:00	0	0	0	0	2	1	0	0	0	1	0	0	0	0	0	0	0	0	4	39.5	37	27 to 37	75.0	*	*	0	3	1
24 Hr Summary		0	0	0	10	61	295	649	395	120	26	5	1	0	0	0	0	0	0	1562	42.9	48	38 to 48	74.8	1	0.1	7	1460	95

Outgoing Histogram

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/13/2024	00:00	0	0	0	0	0	1	1	2	1	0	0	0	0	0	0	0	0	0	5	45	46	36 to 46	80.0	*	*	0	5	0
9/13/2024	01:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	38	44	22 to 32	50.0	*	*	0	2	0
9/13/2024	02:00	0	0	0	0	1	0	2	0	0	0	0	0	0	0	0	0	0	0	3	39.7	44	34 to 44	66.7	*	*	0	2	1
9/13/2024	03:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	0	1
9/13/2024	04:00	0	0	0	0	0	4	0	0	2	0	0	0	0	0	0	0	0	0	6	42.3	50	29 to 39	66.7	*	*	0	6	0
9/13/2024	05:00	0	0	0	0	0	4	1	1	0	0	0	0	0	0	0	0	0	0	6	39.2	40	35 to 45	100.0	*	*	0	4	2
9/13/2024	06:00	0	0	0	0	1	4	4	12	2	2	0	0	0	0	0	0	0	0	25	45	49	39 to 49	68.0	*	*	0	24	1
9/13/2024	07:00	0	0	0	0	6	22	41	21	3	1	0	0	0	0	0	0	0	0	94	41.8	45.4	36 to 46	84.0	*	*	0	84	10
9/13/2024	08:00	0	0	0	1	4	7	28	16	5	0	0	0	0	0	0	0	0	0	61	42.6	46.8	40 to 50	75.4	*	*	0	60	1
9/13/2024	09:00	0	0	1	1	5	21	34	10	8	0	0	0	0	0	0	0	0	0	80	41.4	47.7	34 to 44	71.3	*	*	0	69	11
9/13/2024	10:00	0	0	0	0	3	17	25	16	4	0	0	0	0	0	0	0	0	0	65	42.1	48	36 to 46	73.8	*	*	1	60	4
9/13/2024	11:00	0	0	0	0	6	16	24	30	4	0	0	0	0	0	0	0	0	0	80	42.7	46.6	37 to 47	73.8	*	*	0	75	5
9/13/2024	12:00	0	0	0	0	3	14	40	30	5	2	0	0	0	0	0	0	0	0	94	43.5	47.5	38 to 48	81.9	*	*	1	89	4
9/13/2024	13:00	0	0	0	0	2	23	56	29	9	3	1	0	0	0	0	0	0	0	123	43.3	47	37 to 47	79.7	*	*	0	118	5
9/13/2024	14:00	0	0	0	0	5	23	78	41	15	2	0	0	0	0	0	0	0	0	164	43.4	47.6	38 to 48	78.7	*	*	1	155	8
9/13/2024	15:00	0	0	0	3	4	22	52	35	12	5	2	0	1	0	0	0	0	0	136	43.6	49	38 to 48	69.9	1	0.7	0	125	11
9/13/2024	16:00	0	0	0	0	2	22	72	46	11	2	1	0	0	0	0	0	0	0	156	43.5	47.2	37 to 47	80.8	*	*	0	152	4
9/13/2024	17:00	0	0	0	1	6	29	65	73	14	2	1	1	0	0	0	0	0	0	192	43.8	47.9	38 to 48	75.5	1	0.5	0	183	9
9/13/2024	18:00	0	0	0	0	0	16	63	47	14	2	0	0	0	0	0	0	0	0	142	44.2	47.9	39 to 49	82.4	*	*	0	137	5
9/13/2024	19:00	0	0	0	0	2	19	35	32	16	1	1	0	0	0	0	0	0	0	106	44.2	49.3	38 to 48	69.8	*	*	0	102	4
9/13/2024	20:00	0	0	0	0	4	24	30	23	7	0	0	0	0	0	0	0	0	0	88	42.4	48.1	36 to 46	72.7	*	*	0	84	4
9/13/2024	21:00	0	0	0	0	3	16	7	3	4	0	0	0	0	0	0	0	0	0	33	40.2	46	32 to 42	75.8	*	*	0	31	2
9/13/2024	22:00	0	0	0	0	2	4	4	3	1	1	0	0	0	0	0	0	0	0	15	41.8	49	36 to 46	66.7	*	*	0	15	0
9/13/2024	23:00	0	0	0	0	4	1	3	1	1	0	0	0	0	0	0	0	0	0	10	38.9	44	25 to 35	50.0	*	*	0	7	3
24 Hr Summary		0	0	1	6	64	310	666	471	138	23	6	1	1	0	0	0	0	0	1687	43.1	48	38 to 48	73.3	2	0.1	3	1589	95

Outgoing Histogram

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/14/2024	00:00	0	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	4	41	43	37 to 47	75.0	*	*	0	4	0
9/14/2024	01:00	0	0	0	0	0	6	1	2	1	1	0	0	0	0	0	0	0	0	11	41.8	45	35 to 45	81.8	*	*	0	10	1
9/14/2024	02:00	0	0	0	0	0	1	1	0	0	0	0	1	0	0	0	0	0	0	3	49.3	68	32 to 42	66.7	1	33.3	0	3	0
9/14/2024	03:00	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	2	46.5	53	30 to 40	50.0	*	*	0	2	0
9/14/2024	04:00	0	0	0	0	0	1	5	1	1	0	0	0	0	0	0	0	0	0	8	42.9	48	33 to 43	75.0	*	*	0	7	1
9/14/2024	05:00	0	0	0	0	1	2	2	0	0	0	0	0	0	0	0	0	0	0	5	37.8	41	31 to 41	80.0	*	*	0	3	2
9/14/2024	06:00	0	0	0	0	1	4	6	1	1	0	0	0	0	0	0	0	0	0	13	40.9	43	33 to 43	84.6	*	*	0	12	1
9/14/2024	07:00	0	0	0	0	1	7	6	6	1	0	0	0	0	0	0	0	0	0	21	41.9	46.5	37 to 47	76.2	*	*	0	20	1
9/14/2024	08:00	0	0	0	1	6	10	12	11	1	0	1	0	0	0	0	0	0	0	42	41.3	47	36 to 46	64.3	*	*	1	37	4
9/14/2024	09:00	0	0	1	0	4	13	35	9	3	0	0	0	0	0	0	0	0	0	65	41.3	44.4	35 to 45	81.5	*	*	1	59	5
9/14/2024	10:00	0	0	0	0	5	25	48	31	6	1	1	0	0	0	0	0	0	0	117	42.6	47.2	39 to 49	74.4	*	*	0	109	8
9/14/2024	11:00	0	0	0	2	2	21	36	24	5	0	0	0	0	1	0	0	0	0	91	42.7	47	37 to 47	73.6	1	1.1	0	88	3
9/14/2024	12:00	0	0	0	0	7	21	44	22	4	1	0	0	0	0	0	0	0	0	99	41.8	46.2	37 to 47	75.8	*	*	0	93	6
9/14/2024	13:00	0	0	1	1	2	20	44	28	4	3	0	0	0	0	0	0	0	0	103	42.8	47.3	38 to 48	76.7	*	*	0	94	9
9/14/2024	14:00	0	0	0	1	1	14	45	29	10	1	0	0	0	0	0	0	0	0	101	43.4	48	39 to 49	77.2	*	*	0	92	9
9/14/2024	15:00	0	0	0	3	5	21	38	26	4	2	0	0	0	0	0	0	0	0	99	42	46.3	37 to 47	78.8	*	*	0	96	3
9/14/2024	16:00	0	0	0	0	4	18	43	20	11	2	0	0	0	0	0	0	0	0	98	42.8	47.5	37 to 47	75.5	*	*	0	94	4
9/14/2024	17:00	0	0	0	0	1	20	42	27	8	4	0	0	0	0	0	0	0	0	102	43.3	47.3	37 to 47	80.4	*	*	1	93	8
9/14/2024	18:00	0	0	0	0	1	13	29	25	7	3	1	0	0	0	0	0	0	0	79	44.5	48.8	39 to 49	74.7	*	*	0	77	2
9/14/2024	19:00	0	0	0	2	1	11	27	14	5	1	0	0	0	0	0	0	0	0	61	42.7	46.5	37 to 47	80.3	*	*	1	56	4
9/14/2024	20:00	0	0	0	0	3	18	21	12	1	1	0	0	0	0	0	0	0	0	56	41.5	46	35 to 45	76.8	*	*	0	55	1
9/14/2024	21:00	0	0	0	0	4	7	13	7	2	1	0	0	0	0	0	0	0	0	34	41.7	45.7	36 to 46	76.5	*	*	0	33	1
9/14/2024	22:00	0	0	0	0	0	1	5	3	3	1	0	0	0	0	0	0	0	0	13	45.8	51	37 to 47	69.2	*	*	0	13	0
9/14/2024	23:00	0	0	0	0	1	2	4	2	1	0	0	0	0	0	0	0	0	0	10	41.5	46.5	37 to 47	70.0	*	*	0	10	0
24 Hr Summary		0	0	2	10	51	256	510	301	80	22	3	1	0	1	0	0	0	0	1237	42.6	48	37 to 47	74.0	2	0.2	4	1160	73

Outgoing Histogram

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/15/2024	00:00	0	0	0	0	1	1	0	0	0	0	2	0	0	0	0	0	0	0	4	48.8	61	28 to 38	50.0	*	*	0	3	1
9/15/2024	01:00	0	0	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	3	39.7	45	32 to 42	66.7	*	*	0	3	0
9/15/2024	02:00	0	0	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	4	46.5	49	40 to 50	75.0	*	*	0	4	0
9/15/2024	03:00	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	45	45	35 to 45	100.0	*	*	0	1	0
9/15/2024	04:00	0	0	0	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	4	44.5	44	36 to 46	100.0	*	*	0	3	1
9/15/2024	05:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	42	42	32 to 42	100.0	*	*	0	1	0
9/15/2024	06:00	0	0	0	0	1	0	3	2	0	0	0	0	0	0	0	0	0	0	6	42.7	48.5	39 to 49	83.3	*	*	0	6	0
9/15/2024	07:00	0	0	0	0	2	3	5	3	2	0	0	0	0	0	0	0	0	0	15	42.5	47	37 to 47	66.7	*	*	0	14	1
9/15/2024	08:00	0	0	0	0	0	7	11	2	2	0	0	0	0	0	0	0	0	0	22	41.2	45	35 to 45	86.4	*	*	0	22	0
9/15/2024	09:00	0	0	0	0	1	5	18	13	2	0	0	0	0	0	0	0	0	0	39	43	46	36 to 46	82.1	*	*	0	37	2
9/15/2024	10:00	0	0	0	0	4	14	38	19	2	1	1	0	0	0	0	0	0	0	79	42.3	46	36 to 46	79.7	*	*	0	78	1
9/15/2024	11:00	0	1	1	0	3	19	32	18	5	0	0	0	0	0	0	0	0	0	79	41.6	46.3	37 to 47	74.7	*	*	0	76	3
9/15/2024	12:00	0	0	0	0	2	20	67	34	7	1	0	0	0	0	0	0	0	0	131	43.1	46.7	38 to 48	81.7	*	*	0	129	2
9/15/2024	13:00	0	0	0	0	2	12	42	38	8	4	0	0	0	0	0	0	0	0	106	44.2	47.3	38 to 48	82.1	*	*	0	99	7
9/15/2024	14:00	0	0	0	0	6	12	40	31	11	2	0	0	0	0	0	0	0	0	102	43.6	48	38 to 48	73.5	*	*	1	97	4
9/15/2024	15:00	0	0	0	1	2	13	33	19	9	3	0	0	1	0	0	0	0	0	81	44.1	49.5	38 to 48	72.8	1	1.2	0	80	1
9/15/2024	16:00	0	0	0	1	4	13	37	32	7	1	1	0	0	0	0	0	0	0	96	43.5	47.3	38 to 48	78.1	*	*	1	93	2
9/15/2024	17:00	0	0	0	1	1	15	30	27	8	2	1	0	0	0	0	0	0	0	85	43.9	48.7	39 to 49	70.6	*	*	0	81	4
9/15/2024	18:00	0	0	0	0	1	9	31	28	8	2	1	0	0	0	0	0	0	0	80	44.6	48.8	39 to 49	78.8	*	*	0	79	1
9/15/2024	19:00	0	0	0	0	1	6	32	24	8	1	1	0	0	1	0	0	0	0	74	44.7	49	39 to 49	78.4	1	1.4	1	73	0
9/15/2024	20:00	0	0	0	0	1	15	29	21	5	2	1	0	0	0	0	0	0	0	74	43.4	47	37 to 47	81.1	*	*	0	74	0
9/15/2024	21:00	0	0	0	0	0	3	7	5	1	2	0	0	0	0	0	0	0	0	18	44.5	49	37 to 47	77.8	*	*	0	17	1
9/15/2024	22:00	0	0	0	0	1	2	7	3	3	0	0	0	0	0	0	0	0	0	16	42.9	51	35 to 45	75.0	*	*	0	14	2
9/15/2024	23:00	0	0	0	1	2	0	2	1	1	1	0	0	0	0	0	0	0	0	8	41.4	51	24 to 34	37.5	*	*	0	8	0
24 Hr Summary		0	1	1	4	36	170	469	325	90	22	8	0	1	1	0	0	0	0	1128	43.5	48	38 to 48	75.4	2	0.2	3	1092	33

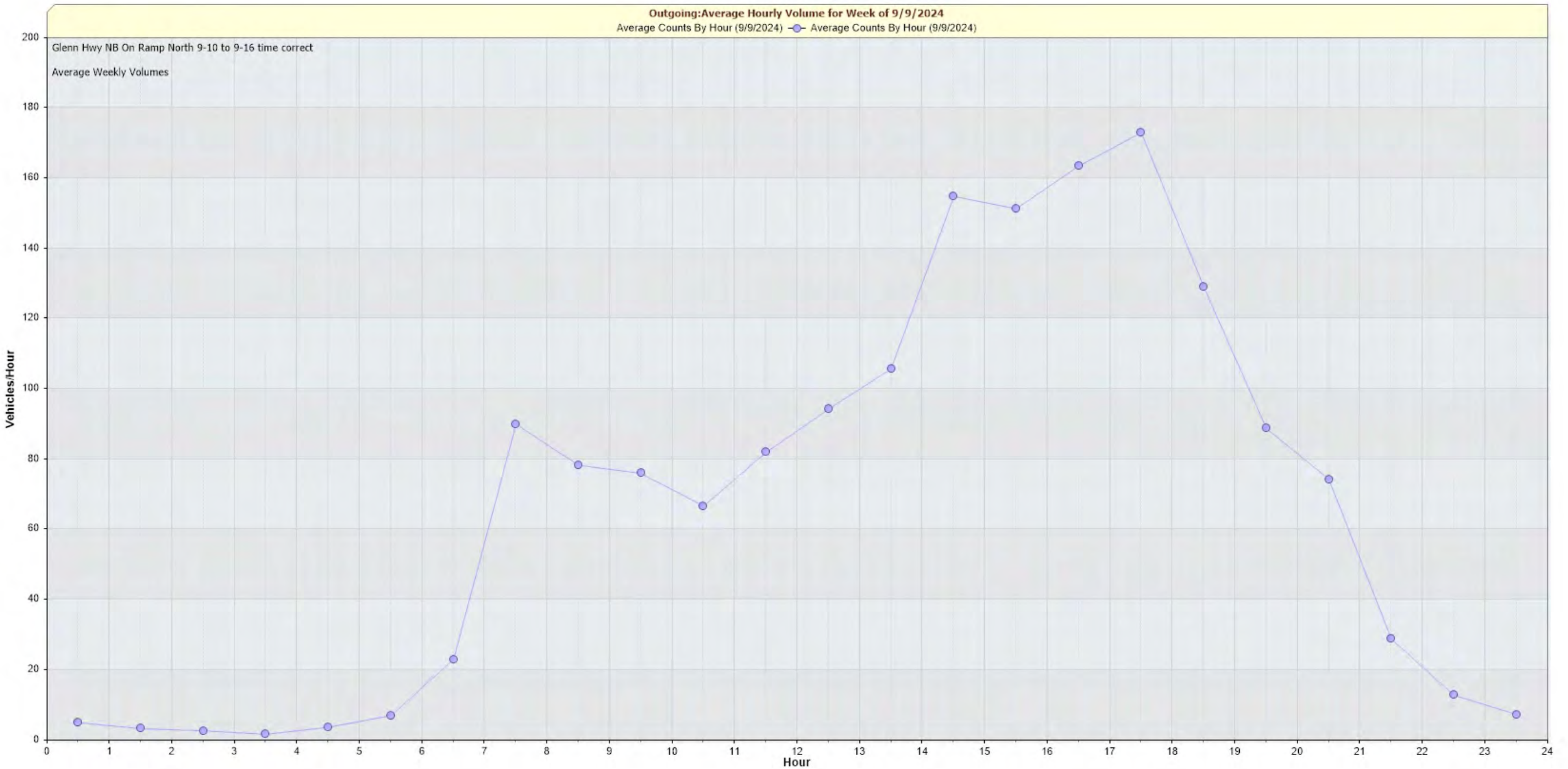
Outgoing Histogram
 Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

from Tue-Sep-10-2024-02-00-PM to Mon-Sep-16-2024-07-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	0	0	0	0	1	0	3	0	0	0	0	0	0	0	0	0	0	0	4	38.8	41.5	32 to 42	75.0	*	*	1	2	1	
9/16/2024	01:00	0	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	3	38.3	48	25 to 35	66.7	*	*	0	2	1	
9/16/2024	02:00	0	0	0	0	0	1	1	0	0	0	1	0	1	0	0	0	0	0	4	53.3	64	30 to 40	50.0	1	25.0	0	4	0	
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	0	0	0	0	0	2	1	2	0	0	0	0	0	0	0	0	0	0	5	41.6	45	35 to 45	80.0	*	*	0	4	1	
9/16/2024	05:00	0	0	0	0	1	2	0	2	0	0	0	0	0	0	0	0	0	0	5	40.4	45	28 to 38	60.0	*	*	0	4	1	
9/16/2024	06:00	0	0	0	0	1	4	7	11	2	0	0	0	0	0	0	0	0	0	25	43.4	46.5	37 to 47	84.0	*	*	0	22	3	
9/16/2024	07:00	0	0	0	0	1	3	4	2	0	0	0	0	0	0	0	0	0	0	10	40.6	44	34 to 44	70.0	*	*	0	8	2	
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	0	0	0	5	13	16	18	2	0	1	0	1	0	0	0	0	0	56	42.6	48	38 to 48	67.9	1	1.8	1	46	9	

Outgoing: Average Hourly Volume for Week of 9/9/2024
Average Counts By Hour (9/9/2024) ● Average Counts By Hour (9/9/2024)

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct
Average Weekly Volumes



Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/9/2024
Average Hourly WEEKDAY Speeds By Hour (9/9/2024) — Average Hourly WEEKDAY Speeds By Hour (9/9/2024)

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct
Average Weekly Speeds

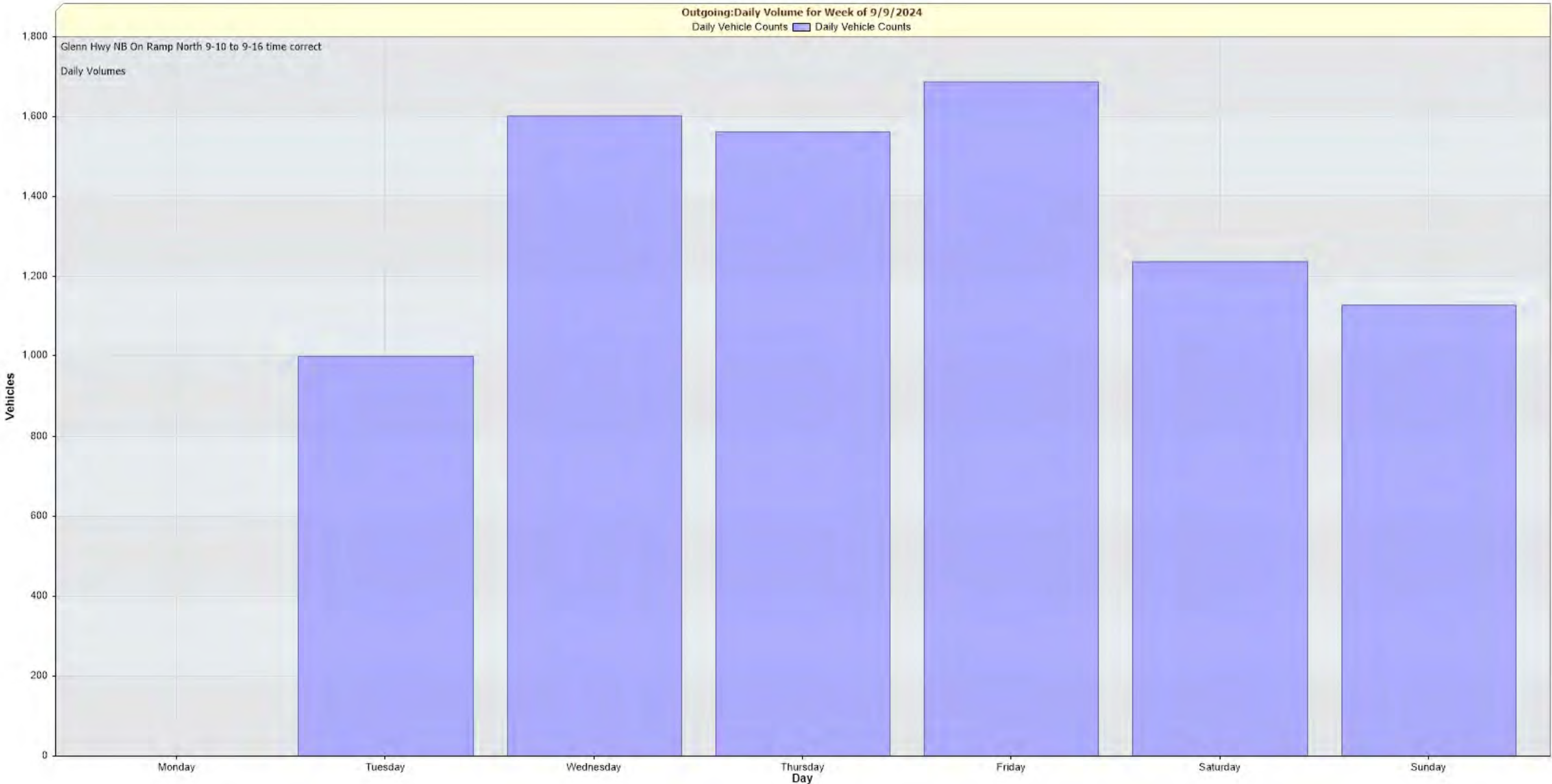


Outgoing:Daily Volume for Week of 9/9/2024

Daily Vehicle Counts

Glenn Hwy NB On Ramp North 9-10 to 9-16 time correct

Daily Volumes



For Project: Old Glenn Hwy South South 9-16 to-9-20 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:56:18 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/16/2024 1:00:00 PM through 9/20/2024 10:59:59 AM

85th Percentile Speed 45 MPH

85th Percentile Vehicles 10649

Max Speed 71 MPH on 9/17/2024 6:00:00 PM

Total Vehicles 12528

AADT: 3198

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	2505	2505
AM Peak	8:00 AM 212	212
PM Peak	6:00 PM 314	314

Speed

Speed Limit: 45

85th Percentile Speed: 45

50th Percentile Speed: 37

10 MPH Pace Interval: 37.0 MPH to 47.0 MPH

Average Speed: 36.36

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	258	452	508	538	62	N/A	N/A
% over limit	13.5	13.9	15.6	15.5	9.5	N/A	N/A
Avg Speeder	47.9	48.2	48.1	48.3	48.9	N/A	N/A
Avg Speed	36.2	36.3	36.5	36.7	34.0	N/A	N/A

Class Counts

	Number	%
VEH_SM	54	0.4
VEH_MED	11770	93.9
VEH_LG	704	5.6
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/16/2024 2:00:00 PM	45.0	98	115	52	47.6	12.2%	35.6
9/16/2024 3:00:00 PM	43.0	198	233	53	47.9	6.0%	33.3
9/16/2024 4:00:00 PM	43.0	222	261	53	48.5	7.7%	32.2
9/16/2024 5:00:00 PM	45.0	231	272	52	47.9	14.0%	37.3
9/16/2024 6:00:00 PM	46.0	233	274	57	48.6	15.3%	37.4
9/16/2024 7:00:00 PM	46.0	252	296	54	47.4	19.6%	38.3
9/16/2024 8:00:00 PM	47.0	167	196	54	47.9	19.4%	38.4
9/16/2024 9:00:00 PM	45.0	99	116	51	47.9	14.7%	37.7
9/16/2024 10:00:00 PM	44.0	70	82	50	46.9	8.5%	35.5
9/16/2024 11:00:00 PM	46.0	41	48	48	46.9	16.7%	38.0
9/17/2024 12:00:00 AM	42.0	21	25	52	49.0	8.0%	33.9
9/17/2024 1:00:00 AM	42.0	14	17	47	46.5	11.8%	36.5
9/17/2024 2:00:00 AM	47.0	8	9	52	49.5	22.2%	39.3
9/17/2024 3:00:00 AM	46.0	6	7	53	49.5	28.6%	38.4
9/17/2024 4:00:00 AM	48.0	7	8	58	53.0	25.0%	41.9
9/17/2024 5:00:00 AM	32.0	2	2	32	0.0	0.0%	30.0
9/17/2024 6:00:00 AM	46.0	14	16	49	46.8	31.3%	41.6
9/17/2024 7:00:00 AM	46.0	62	73	54	49.2	17.8%	38.9
9/17/2024 8:00:00 AM	43.0	104	122	52	48.1	9.0%	31.4
9/17/2024 9:00:00 AM	41.0	184	216	53	48.3	6.0%	29.8
9/17/2024 10:00:00 AM	44.0	156	183	52	47.3	8.7%	34.4
9/17/2024 11:00:00 AM	45.0	163	192	56	48.9	12.5%	36.3
9/17/2024 12:00:00 PM	45.0	160	188	63	50.2	12.2%	36.9
9/17/2024 1:00:00 PM	46.0	161	189	55	48.1	15.9%	37.0
9/17/2024 2:00:00 PM	46.0	201	236	53	48.0	16.1%	37.8
9/17/2024 3:00:00 PM	44.0	159	187	51	47.3	10.7%	33.4
9/17/2024 4:00:00 PM	43.0	204	240	55	48.2	6.7%	32.6
9/17/2024 5:00:00 PM	45.0	230	271	57	48.1	14.4%	37.8
9/17/2024 6:00:00 PM	46.0	258	303	71	48.0	18.4%	38.2
9/17/2024 7:00:00 PM	46.0	249	293	71	48.5	18.8%	38.8
9/17/2024 8:00:00 PM	46.0	180	212	51	47.6	18.9%	39.1
9/17/2024 9:00:00 PM	46.0	110	130	52	47.8	16.2%	38.6
9/17/2024 10:00:00 PM	45.0	61	72	52	48.3	15.3%	38.0
9/17/2024 11:00:00 PM	45.0	48	56	53	49.4	12.5%	38.3
9/18/2024 12:00:00 AM	48.0	21	25	50	48.1	28.0%	38.0
9/18/2024 1:00:00 AM	47.0	6	7	50	47.7	42.9%	41.0
9/18/2024 2:00:00 AM	37.0	6	7	51	51.0	14.3%	33.3
9/18/2024 3:00:00 AM	45.0	6	7	46	46.0	14.3%	40.7
9/18/2024 4:00:00 AM	60.0	1	1	60	60.0	100.0%	60.0
9/18/2024 5:00:00 AM	40.0	7	8	43	0.0	0.0%	34.8
9/18/2024 6:00:00 AM	46.0	20	23	53	48.5	17.4%	37.9
9/18/2024 7:00:00 AM	46.0	63	74	52	48.2	18.9%	38.1
9/18/2024 8:00:00 AM	43.0	99	117	51	47.3	6.0%	33.1
9/18/2024 9:00:00 AM	41.0	176	207	52	47.2	6.3%	30.4
9/18/2024 10:00:00 AM	46.0	146	172	59	48.3	16.3%	35.5
9/18/2024 11:00:00 AM	46.0	135	159	56	48.4	17.0%	37.1
9/18/2024 12:00:00 PM	46.0	125	147	55	48.4	19.0%	38.2

Incoming Summary
Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

9/18/2024 1:00:00 PM	46.0	173	204	53	47.9	17.6%	37.8
9/18/2024 2:00:00 PM	46.0	173	203	51	47.6	18.2%	38.3
9/18/2024 3:00:00 PM	43.0	186	219	51	48.3	6.4%	32.7
9/18/2024 4:00:00 PM	44.0	205	241	55	48.9	9.5%	32.7
9/18/2024 5:00:00 PM	46.0	238	280	59	48.4	19.6%	38.8
9/18/2024 6:00:00 PM	46.0	250	294	62	48.0	18.0%	38.9
9/18/2024 7:00:00 PM	46.0	264	310	54	47.9	20.6%	38.4
9/18/2024 8:00:00 PM	47.0	172	202	65	48.5	19.3%	38.9
9/18/2024 9:00:00 PM	45.0	105	124	53	48.0	15.3%	36.7
9/18/2024 10:00:00 PM	46.0	112	132	55	47.9	15.9%	36.4
9/18/2024 11:00:00 PM	45.0	56	66	51	47.2	15.2%	36.6
9/19/2024 12:00:00 AM	47.0	37	43	57	48.8	23.3%	38.7
9/19/2024 1:00:00 AM	48.0	19	22	49	47.9	31.8%	37.5
9/19/2024 2:00:00 AM	46.0	8	9	48	46.7	33.3%	35.1
9/19/2024 3:00:00 AM	49.0	7	8	55	52.0	25.0%	37.0
9/19/2024 4:00:00 AM	41.0	11	13	50	50.0	7.7%	33.5
9/19/2024 5:00:00 AM	45.0	7	8	45	0.0	0.0%	37.8
9/19/2024 6:00:00 AM	45.0	18	21	57	50.0	14.3%	39.9
9/19/2024 7:00:00 AM	46.0	57	67	54	48.3	22.1%	39.1
9/19/2024 8:00:00 AM	42.0	92	108	52	48.4	4.6%	31.6
9/19/2024 9:00:00 AM	43.0	184	217	50	47.2	9.2%	30.4
9/19/2024 10:00:00 AM	44.0	162	191	53	47.9	13.1%	35.4
9/19/2024 11:00:00 AM	46.0	140	165	56	48.6	15.8%	36.3
9/19/2024 12:00:00 PM	46.0	162	190	54	47.9	15.3%	37.9
9/19/2024 1:00:00 PM	47.0	201	237	56	48.2	24.1%	38.9
9/19/2024 2:00:00 PM	46.0	185	218	55	47.9	15.6%	37.8
9/19/2024 3:00:00 PM	44.0	180	212	56	48.5	9.9%	33.6
9/19/2024 4:00:00 PM	43.0	239	281	53	48.1	6.0%	32.7
9/19/2024 5:00:00 PM	45.0	228	268	54	48.5	13.4%	38.4
9/19/2024 6:00:00 PM	46.0	271	319	56	47.9	20.7%	38.2
9/19/2024 7:00:00 PM	46.0	305	359	55	47.9	20.1%	39.3
9/19/2024 8:00:00 PM	47.0	182	214	61	49.0	20.6%	38.6
9/19/2024 9:00:00 PM	46.0	119	140	63	49.1	16.4%	39.2
9/19/2024 10:00:00 PM	48.0	86	101	60	49.7	18.8%	39.7
9/19/2024 11:00:00 PM	45.0	55	65	63	49.4	13.8%	36.3
9/20/2024 12:00:00 AM	46.0	25	29	52	48.8	17.2%	38.4
9/20/2024 1:00:00 AM	47.0	11	13	48	47.3	23.1%	35.9
9/20/2024 2:00:00 AM	41.0	4	5	44	0.0	0.0%	39.4
9/20/2024 3:00:00 AM	41.0	2	2	41	0.0	0.0%	37.5
9/20/2024 4:00:00 AM	31.0	5	6	53	53.0	16.7%	31.5
9/20/2024 5:00:00 AM	47.0	5	6	47	47.0	33.3%	42.5
9/20/2024 6:00:00 AM	44.0	14	17	47	47.0	5.9%	38.6
9/20/2024 7:00:00 AM	45.0	56	66	57	49.6	15.2%	38.4
9/20/2024 8:00:00 AM	44.0	82	96	53	49.3	12.5%	32.7
9/20/2024 9:00:00 AM	42.0	178	210	52	48.5	6.2%	31.0
9/20/2024 10:00:00 AM	44.0	127	149	50	48.1	7.4%	35.1
9/20/2024 11:00:00 AM	45.0	71	84	58	50.0	10.7%	35.4

Incoming Summary
Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/17/2024 12:00:00 AM	45.0	1630	1918	57	47.9	13.5%	36.2
9/18/2024 12:00:00 AM	45.0	2760	3247	71	48.2	13.9%	36.3
9/19/2024 12:00:00 AM	46.0	2760	3247	65	48.1	15.6%	36.5
9/20/2024 12:00:00 AM	46.0	2943	3462	63	48.3	15.5%	36.7
9/20/2024 10:59:59 AM	44.0	556	654	58	48.9	9.5%	34.0

Incoming Weekly Counts

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	17	7	22	13	*	*	14.75	0	45.75
1 - 2	*	9	7	9	5	*	*	7.5	0	42.75
2 - 3	*	7	7	8	2	*	*	6	0	45.25
3 - 4	*	8	1	13	6	*	*	7	0	45
4 - 5	*	2	8	8	6	*	*	6	0	40.75
5 - 6	*	16	23	21	17	*	*	19.25	0	45.12
6 - 7	*	73	74	67	66	*	*	70	0	45.62
7 - 8	*	122	117	108	96	*	*	110.75	0	42.8
8 - 9	*	216	207	217	210	*	*	212.5	0	41.47
9 - 10	*	183	172	191	149	*	*	173.75	0	44.18
10 - 11	*	192	159	165	84	*	*	150	0	44.82
11 - 12	*	188	147	190	*	*	*	175	0	45.27
12 - 13	*	189	204	237	*	*	*	210	0	45.7
13 - 14	115	236	203	218	*	*	*	193	0	45.1
14 - 15	233	187	219	212	*	*	*	212.75	0	43.1
15 - 16	261	240	241	281	*	*	*	255.75	0	42.9
16 - 17	272	271	280	268	*	*	*	272.75	0	45.08
17 - 18	274	303	294	319	*	*	*	297.5	0	45.45
18 - 19	296	293	310	359	*	*	*	314.5	0	45.85
19 - 20	196	212	202	214	*	*	*	206	0	45.97
20 - 21	116	130	124	140	*	*	*	127.5	0	45.1
21 - 22	82	72	132	101	*	*	*	96.75	0	45.25
22 - 23	48	56	66	65	*	*	*	58.75	0	44.88
23 - 24	25	25	43	29	*	*	*	30.5	0	45.62
Totals	1918	3247	3247	3462	654	0	0			
% of Total	15.31%	25.92%	25.92%	27.63%	5.22%	0%	0%			

Incoming Monthly Counts

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	17	7	22	13	*	*	14.75	0	45.75
1 - 2	*	9	7	9	5	*	*	7.5	0	42.75
2 - 3	*	7	7	8	2	*	*	6	0	45.25
3 - 4	*	8	1	13	6	*	*	7	0	45
4 - 5	*	2	8	8	6	*	*	6	0	40.75
5 - 6	*	16	23	21	17	*	*	19.25	0	45.12
6 - 7	*	73	74	67	66	*	*	70	0	45.62
7 - 8	*	122	117	108	96	*	*	110.75	0	42.8
8 - 9	*	216	207	217	210	*	*	212.5	0	41.47
9 - 10	*	183	172	191	149	*	*	173.75	0	44.18
10 - 11	*	192	159	165	84	*	*	150	0	44.82
11 - 12	*	188	147	190	*	*	*	175	0	45.27
12 - 13	*	189	204	237	*	*	*	210	0	45.7
13 - 14	115	236	203	218	*	*	*	193	0	45.1
14 - 15	233	187	219	212	*	*	*	212.75	0	43.1
15 - 16	261	240	241	281	*	*	*	255.75	0	42.9
16 - 17	272	271	280	268	*	*	*	272.75	0	45.08
17 - 18	274	303	294	319	*	*	*	297.5	0	45.45
18 - 19	296	293	310	359	*	*	*	314.5	0	45.85
19 - 20	196	212	202	214	*	*	*	206	0	45.97
20 - 21	116	130	124	140	*	*	*	127.5	0	45.1
21 - 22	82	72	132	101	*	*	*	96.75	0	45.25
22 - 23	48	56	66	65	*	*	*	58.75	0	44.88
23 - 24	25	25	43	29	*	*	*	30.5	0	45.62
Totals	1918	3247	3247	3462	654	0	0			
% of Total	15.31%	25.92%	25.92%	27.63%	5.22%	0%	0%			

Incoming Weekly Speeds

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	36.53	41	37.5	35.92	*	*	37.29	*	45.75
1 - 2	*	39.33	33.29	35.11	39.4	*	*	36.67	*	42.75
2 - 3	*	38.43	40.71	37	37.5	*	*	38.54	*	45.25
3 - 4	*	41.88	60	33.54	31.5	*	*	36.43	*	45
4 - 5	*	30	34.75	37.75	42.5	*	*	37.29	*	40.75
5 - 6	*	41.56	37.87	39.9	38.65	*	*	39.36	*	45.12
6 - 7	*	38.95	38.14	39.1	38.39	*	*	38.64	*	45.62
7 - 8	*	31.44	33.06	31.59	32.73	*	*	32.19	*	42.8
8 - 9	*	29.8	30.43	30.38	31	*	*	30.4	*	41.47
9 - 10	*	34.4	35.53	35.41	35.13	*	*	35.11	*	44.18
10 - 11	*	36.26	37.11	36.31	35.39	*	*	36.38	*	44.82
11 - 12	*	36.93	38.16	37.89	*	*	*	37.62	*	45.27
12 - 13	*	36.96	37.75	38.93	*	*	*	37.96	*	45.7
13 - 14	35.89	37.8	38.26	37.77	*	*	*	37.63	*	45.1
14 - 15	33.26	33.45	32.69	33.62	*	*	*	33.25	*	43.1
15 - 16	32.25	32.64	32.72	32.71	*	*	*	32.58	*	42.9
16 - 17	37.29	37.78	38.84	38.45	*	*	*	38.09	*	45.08
17 - 18	37.41	38.23	38.94	38.18	*	*	*	38.2	*	45.45
18 - 19	38.26	38.77	38.35	39.29	*	*	*	38.7	*	45.85
19 - 20	38.38	39.12	38.86	38.56	*	*	*	38.74	*	45.97
20 - 21	37.71	38.58	36.67	39.16	*	*	*	38.08	*	45.1
21 - 22	35.49	38.03	36.4	39.72	*	*	*	37.38	*	45.25
22 - 23	38.02	38.27	36.58	36.34	*	*	*	37.21	*	44.88
23 - 24	33.92	38	38.74	38.41	*	*	*	37.52	*	45.62
Totals	36.2	36.3	36.5	36.7	34	0	0			
% of Total	20.14%	20.2%	20.31%	20.42%	18.92%	0%	0%			

Incoming Monthly Speeds
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	36.53	41	37.5	35.92	*	*	37.29	*	45.75
1 - 2	*	39.33	33.29	35.11	39.4	*	*	36.67	*	42.75
2 - 3	*	38.43	40.71	37	37.5	*	*	38.54	*	45.25
3 - 4	*	41.88	60	33.54	31.5	*	*	36.43	*	45
4 - 5	*	30	34.75	37.75	42.5	*	*	37.29	*	40.75
5 - 6	*	41.56	37.87	39.9	38.65	*	*	39.36	*	45.12
6 - 7	*	38.95	38.14	39.1	38.39	*	*	38.64	*	45.62
7 - 8	*	31.44	33.06	31.59	32.73	*	*	32.19	*	42.8
8 - 9	*	29.8	30.43	30.38	31	*	*	30.4	*	41.47
9 - 10	*	34.4	35.53	35.41	35.13	*	*	35.11	*	44.18
10 - 11	*	36.26	37.11	36.31	35.39	*	*	36.38	*	44.82
11 - 12	*	36.93	38.16	37.89	*	*	*	37.62	*	45.27
12 - 13	*	36.96	37.75	38.93	*	*	*	37.96	*	45.7
13 - 14	35.89	37.8	38.26	37.77	*	*	*	37.63	*	45.1
14 - 15	33.26	33.45	32.69	33.62	*	*	*	33.25	*	43.1
15 - 16	32.25	32.64	32.72	32.71	*	*	*	32.58	*	42.9
16 - 17	37.29	37.78	38.84	38.45	*	*	*	38.09	*	45.08
17 - 18	37.41	38.23	38.94	38.18	*	*	*	38.2	*	45.45
18 - 19	38.26	38.77	38.35	39.29	*	*	*	38.7	*	45.85
19 - 20	38.38	39.12	38.86	38.56	*	*	*	38.74	*	45.97
20 - 21	37.71	38.58	36.67	39.16	*	*	*	38.08	*	45.1
21 - 22	35.49	38.03	36.4	39.72	*	*	*	37.38	*	45.25
22 - 23	38.02	38.27	36.58	36.34	*	*	*	37.21	*	44.88
23 - 24	33.92	38	38.74	38.41	*	*	*	37.52	*	45.62
Totals	36.2	36.3	36.5	36.7	34	0	0			
% of Total	20.14%	20.2%	20.31%	20.42%	18.92%	0%	0%			

Incoming Weekly EightyFifthSpeeds
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	42	47	47.5	46.5	*	*	45.75	0	45.75
1 - 2	*	47	37	46	41	*	*	42.75	0	42.75
2 - 3	*	46	45	49	41	*	*	45.25	0	45.25
3 - 4	*	48	60	41	31	*	*	45	0	45
4 - 5	*	32	40	44.5	46.5	*	*	40.75	0	40.75
5 - 6	*	46	46	45	43.5	*	*	45.12	0	45.12
6 - 7	*	45.5	46	46	45	*	*	45.62	0	45.62
7 - 8	*	43	43	41.2	44	*	*	42.8	0	42.8
8 - 9	*	40.8	40.8	42.8	41.5	*	*	41.47	0	41.47
9 - 10	*	43.7	45.2	44	43.8	*	*	44.18	0	44.18
10 - 11	*	44.4	45.4	45.2	44.3	*	*	44.82	0	44.82
11 - 12	*	44.7	46	45.1	*	*	*	45.27	0	45.27
12 - 13	*	45.2	45.5	46.4	*	*	*	45.7	0	45.7
13 - 14	44.4	45.3	45.6	45.1	*	*	*	45.1	0	45.1
14 - 15	43	43.4	42.5	43.5	*	*	*	43.1	0	43.1
15 - 16	42.7	42.6	43.8	42.5	*	*	*	42.9	0	42.9
16 - 17	44.8	44.9	45.8	44.8	*	*	*	45.08	0	45.08
17 - 18	45.1	45.4	45.5	45.8	*	*	*	45.45	0	45.45
18 - 19	45.6	45.9	45.9	46	*	*	*	45.85	0	45.85
19 - 20	46.1	45.5	46.1	46.2	*	*	*	45.97	0	45.97
20 - 21	45	45.2	45	45.2	*	*	*	45.1	0	45.1
21 - 22	43.6	45	45.2	47.2	*	*	*	45.25	0	45.25
22 - 23	45.3	44.7	45	44.5	*	*	*	44.88	0	44.88
23 - 24	42	48	46.5	46	*	*	*	45.62	0	45.62
Totals	487.6	1064.2	1083.8	1080.5	468.1	0	0			
% of Total	11.65%	25.43%	25.9%	25.82%	11.19%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	42	47	47.5	46.5	*	*	45.75	0	45.75
1 - 2	*	47	37	46	41	*	*	42.75	0	42.75
2 - 3	*	46	45	49	41	*	*	45.25	0	45.25
3 - 4	*	48	60	41	31	*	*	45	0	45
4 - 5	*	32	40	44.5	46.5	*	*	40.75	0	40.75
5 - 6	*	46	46	45	43.5	*	*	45.12	0	45.12
6 - 7	*	45.5	46	46	45	*	*	45.62	0	45.62
7 - 8	*	43	43	41.2	44	*	*	42.8	0	42.8
8 - 9	*	40.8	40.8	42.8	41.5	*	*	41.47	0	41.47
9 - 10	*	43.7	45.2	44	43.8	*	*	44.18	0	44.18
10 - 11	*	44.4	45.4	45.2	44.3	*	*	44.82	0	44.82
11 - 12	*	44.7	46	45.1	*	*	*	45.27	0	45.27
12 - 13	*	45.2	45.5	46.4	*	*	*	45.7	0	45.7
13 - 14	44.4	45.3	45.6	45.1	*	*	*	45.1	0	45.1
14 - 15	43	43.4	42.5	43.5	*	*	*	43.1	0	43.1
15 - 16	42.7	42.6	43.8	42.5	*	*	*	42.9	0	42.9
16 - 17	44.8	44.9	45.8	44.8	*	*	*	45.08	0	45.08
17 - 18	45.1	45.4	45.5	45.8	*	*	*	45.45	0	45.45
18 - 19	45.6	45.9	45.9	46	*	*	*	45.85	0	45.85
19 - 20	46.1	45.5	46.1	46.2	*	*	*	45.97	0	45.97
20 - 21	45	45.2	45	45.2	*	*	*	45.1	0	45.1
21 - 22	43.6	45	45.2	47.2	*	*	*	45.25	0	45.25
22 - 23	45.3	44.7	45	44.5	*	*	*	44.88	0	44.88
23 - 24	42	48	46.5	46	*	*	*	45.62	0	45.62

Summary of Violators

Old Glenn Hwy South South 9-16 to-9-20 time correct from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	59	37.3	15	47.5
01:00:00	30	36.7	6	48.3
02:00:00	24	38.5	5	49.8
03:00:00	28	36.4	5	53.8
04:00:00	24	37.3	2	47.0
05:00:00	77	39.4	13	48.1
06:00:00	280	38.6	51	48.8
07:00:00	443	32.2	35	48.4
08:00:00	850	30.4	59	47.7
09:00:00	695	35.1	80	48.0
10:00:00	600	36.4	86	48.8
11:00:00	525	37.6	80	48.8
12:00:00	630	38.0	123	48.1
13:00:00	772	37.6	123	47.8
14:00:00	851	33.2	69	48.0
15:00:00	1023	32.6	76	48.5
16:00:00	1091	38.1	168	48.2
17:00:00	1190	38.2	216	48.0
18:00:00	1258	38.7	249	47.9
19:00:00	824	38.7	161	48.3
20:00:00	510	38.1	80	48.2
21:00:00	387	37.4	58	48.4
22:00:00	235	37.2	34	48.2
23:00:00	122	37.5	24	48.6

Incoming Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	13:00	0	0	3	20	33	21	19	16	3	0	0	0	0	0	0	0	0	0	115	35.9	44.4	26 to 36	54.8	14	12.2	1	107	7	
9/16/2024	14:00	2	4	29	43	67	22	43	22	1	0	0	0	0	0	0	0	0	0	233	33.3	43	24 to 34	49.8	14	6.0	3	207	23	
9/16/2024	15:00	2	10	65	38	39	27	55	20	5	0	0	0	0	0	0	0	0	0	261	32.2	42.7	20 to 30	42.9	20	7.7	2	239	20	
9/16/2024	16:00	0	0	10	42	49	46	74	44	7	0	0	0	0	0	0	0	0	0	272	37.3	44.8	35 to 45	48.9	38	14.0	2	253	17	
9/16/2024	17:00	0	0	7	41	59	45	71	39	11	1	0	0	0	0	0	0	0	0	274	37.4	45.1	36 to 46	46.7	42	15.3	1	257	16	
9/16/2024	18:00	1	2	11	28	55	48	79	66	6	0	0	0	0	0	0	0	0	0	296	38.3	45.6	37 to 47	55.1	58	19.6	1	283	12	
9/16/2024	19:00	1	0	4	25	34	30	54	43	5	0	0	0	0	0	0	0	0	0	196	38.4	46.1	39 to 49	52.6	38	19.4	0	186	10	
9/16/2024	20:00	0	1	1	22	13	22	34	19	4	0	0	0	0	0	0	0	0	0	116	37.7	45	37 to 47	57.8	17	14.7	0	111	5	
9/16/2024	21:00	2	3	3	12	13	13	26	9	1	0	0	0	0	0	0	0	0	0	82	35.5	43.6	36 to 46	54.9	7	8.5	0	81	1	
9/16/2024	22:00	0	1	4	4	1	17	11	10	0	0	0	0	0	0	0	0	0	0	48	38	45.3	36 to 46	66.7	8	16.7	0	48	0	
9/16/2024	23:00	0	0	3	6	5	4	4	2	1	0	0	0	0	0	0	0	0	0	25	33.9	42	21 to 31	48.0	2	8.0	0	24	1	
24 Hr Summary		8	21	140	281	368	295	470	290	44	1	0	0	0	0	0	0	0	0	1918	36.2	45	37 to 47	45.4	258	13.5	10	1796	112	

Incoming Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/17/2024	00:00	0	0	1	3	2	4	4	3	0	0	0	0	0	0	0	0	0	0	17	36.5	42	37 to 47	58.8	2	11.8	0	17	0
9/17/2024	01:00	0	0	0	2	1	0	4	1	1	0	0	0	0	0	0	0	0	0	9	39.3	47	33 to 43	55.6	2	22.2	0	9	0
9/17/2024	02:00	0	0	0	1	1	2	1	1	1	0	0	0	0	0	0	0	0	0	7	38.4	46	27 to 37	57.1	2	28.6	0	7	0
9/17/2024	03:00	0	0	0	1	0	1	4	1	0	1	0	0	0	0	0	0	0	0	8	41.9	48	34 to 44	62.5	2	25.0	0	8	0
9/17/2024	04:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	30	32	22 to 32	100.0	*	*	0	2	0
9/17/2024	05:00	0	0	0	1	2	1	7	5	0	0	0	0	0	0	0	0	0	0	16	41.6	46	39 to 49	81.3	5	31.3	0	15	1
9/17/2024	06:00	1	1	2	5	9	16	20	13	6	0	0	0	0	0	0	0	0	0	73	38.9	45.5	36 to 46	58.9	13	17.8	0	71	2
9/17/2024	07:00	2	8	25	23	20	12	19	11	2	0	0	0	0	0	0	0	0	0	122	31.4	43	21 to 31	44.3	11	9.0	0	113	9
9/17/2024	08:00	3	15	54	50	30	21	26	15	2	0	0	0	0	0	0	0	0	0	216	29.8	40.8	20 to 30	52.8	13	6.0	0	197	19
9/17/2024	09:00	3	6	10	40	35	26	39	22	2	0	0	0	0	0	0	0	0	0	183	34.4	43.7	25 to 35	44.3	16	8.7	1	160	22
9/17/2024	10:00	2	3	11	28	34	38	43	25	6	2	0	0	0	0	0	0	0	0	192	36.3	44.4	35 to 45	46.9	24	12.5	1	166	25
9/17/2024	11:00	1	6	4	25	38	36	40	28	7	2	1	0	0	0	0	0	0	0	188	36.9	44.7	35 to 45	48.4	23	12.2	2	176	10
9/17/2024	12:00	3	2	6	28	38	28	45	31	7	1	0	0	0	0	0	0	0	0	189	37	45.2	37 to 47	48.7	30	15.9	1	179	9
9/17/2024	13:00	2	2	5	19	57	43	59	40	9	0	0	0	0	0	0	0	0	0	236	37.8	45.3	37 to 47	49.6	38	16.1	1	219	16
9/17/2024	14:00	1	5	25	43	34	20	36	21	2	0	0	0	0	0	0	0	0	0	187	33.4	43.4	23 to 33	46.5	20	10.7	2	170	15
9/17/2024	15:00	3	10	45	38	40	34	47	19	3	1	0	0	0	0	0	0	0	0	240	32.6	42.6	21 to 31	42.1	16	6.7	3	220	17
9/17/2024	16:00	2	3	5	28	56	46	76	47	7	1	0	0	0	0	0	0	0	0	271	37.8	44.9	37 to 47	51.7	39	14.4	0	250	21
9/17/2024	17:00	2	1	5	36	52	47	95	57	7	1	0	0	0	0	0	0	0	0	303	38.2	45.4	36 to 46	56.1	55	18.2	2	288	13
9/17/2024	18:00	1	1	4	38	47	45	82	62	12	0	0	0	1	0	0	0	0	0	293	38.8	45.9	38 to 48	52.6	55	18.8	0	285	8
9/17/2024	19:00	1	0	1	28	27	30	71	49	5	0	0	0	0	0	0	0	0	0	212	39.1	45.5	38 to 48	61.8	40	18.9	1	207	4
9/17/2024	20:00	0	0	0	21	18	20	45	23	3	0	0	0	0	0	0	0	0	0	130	38.6	45.2	38 to 48	62.3	21	16.2	0	126	4
9/17/2024	21:00	0	0	3	8	10	15	22	11	3	0	0	0	0	0	0	0	0	0	72	38	45	35 to 45	55.6	11	15.3	0	69	3
9/17/2024	22:00	0	0	2	6	10	8	20	6	4	0	0	0	0	0	0	0	0	0	56	38.3	44.7	37 to 47	58.9	7	12.5	0	53	3
9/17/2024	23:00	0	0	3	1	5	4	4	7	1	0	0	0	0	0	0	0	0	0	25	38	48	39 to 49	48.0	7	28.0	0	25	0
24 Hr Summary		27	63	211	474	567	497	809	498	90	9	1	0	1	0	0	0	0	0	3247	36.3	45	37 to 47	46.2	452	13.9	14	3032	201

Incoming Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/18/2024	00:00	0	0	0	0	2	1	1	2	1	0	0	0	0	0	0	0	0	0	7	41	47	40 to 50	57.1	3	42.9	0	7	0
9/18/2024	01:00	0	0	1	2	2	1	0	0	1	0	0	0	0	0	0	0	0	0	7	33.3	37	24 to 34	71.4	1	14.3	0	7	0
9/18/2024	02:00	0	0	0	0	1	1	3	2	0	0	0	0	0	0	0	0	0	0	7	40.7	45	36 to 46	85.7	1	14.3	0	7	0
9/18/2024	03:00	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	60	60	50 to 60	100.0	1	100.0	0	1	0
9/18/2024	04:00	0	0	0	2	1	3	2	0	0	0	0	0	0	0	0	0	0	0	8	34.8	40	27 to 37	75.0	*	*	0	7	1
9/18/2024	05:00	0	0	1	4	2	5	7	3	1	0	0	0	0	0	0	0	0	0	23	37.9	46	34 to 44	52.2	4	17.4	0	23	0
9/18/2024	06:00	1	2	2	6	11	14	23	12	3	0	0	0	0	0	0	0	0	0	74	38.1	46	34 to 44	52.7	14	18.9	0	70	4
9/18/2024	07:00	1	3	13	26	27	14	21	11	1	0	0	0	0	0	0	0	0	0	117	33.1	43	25 to 35	49.6	7	6.0	0	109	8
9/18/2024	08:00	1	13	58	36	24	35	24	15	1	0	0	0	0	0	0	0	0	0	207	30.4	40.8	19 to 29	49.3	13	6.3	0	196	11
9/18/2024	09:00	2	7	10	25	33	29	35	26	2	3	0	0	0	0	0	0	0	0	172	35.5	45.2	30 to 40	43.6	28	16.3	0	161	11
9/18/2024	10:00	0	0	10	18	35	31	31	28	4	2	0	0	0	0	0	0	0	0	159	37.1	45.4	30 to 40	47.2	27	17.0	0	143	16
9/18/2024	11:00	1	0	3	17	31	27	29	31	7	1	0	0	0	0	0	0	0	0	147	38.2	46	38 to 48	46.9	28	19.0	1	130	16
9/18/2024	12:00	1	1	7	24	40	39	47	39	6	0	0	0	0	0	0	0	0	0	204	37.8	45.5	36 to 46	47.5	36	17.6	0	192	12
9/18/2024	13:00	1	2	7	22	31	34	60	39	7	0	0	0	0	0	0	0	0	0	203	38.3	45.6	37 to 47	54.7	37	18.2	1	183	19
9/18/2024	14:00	3	9	41	23	48	41	32	18	4	0	0	0	0	0	0	0	0	0	219	32.7	42.5	28 to 38	42.5	14	6.4	3	200	16
9/18/2024	15:00	6	13	47	32	35	34	40	25	8	1	0	0	0	0	0	0	0	0	241	32.7	43.8	19 to 29	35.3	23	9.5	2	227	12
9/18/2024	16:00	1	2	7	33	36	52	74	62	9	4	0	0	0	0	0	0	0	0	280	38.8	45.8	37 to 47	53.6	55	19.6	1	259	20
9/18/2024	17:00	1	2	2	27	52	50	91	60	6	2	1	0	0	0	0	0	0	0	294	38.9	45.5	38 to 48	56.5	53	18.0	1	284	9
9/18/2024	18:00	0	1	2	41	62	62	63	68	11	0	0	0	0	0	0	0	0	0	310	38.4	45.9	38 to 48	47.7	64	20.6	0	296	14
9/18/2024	19:00	1	0	3	17	40	39	53	38	10	0	0	1	0	0	0	0	0	0	202	38.9	46.1	37 to 47	53.0	39	19.3	0	198	4
9/18/2024	20:00	1	0	3	20	29	28	21	18	4	0	0	0	0	0	0	0	0	0	124	36.7	45	29 to 39	51.6	19	15.3	0	121	3
9/18/2024	21:00	1	7	4	16	26	20	35	20	2	1	0	0	0	0	0	0	0	0	132	36.4	45.2	38 to 48	47.0	21	15.9	0	130	2
9/18/2024	22:00	1	2	5	8	9	8	18	14	1	0	0	0	0	0	0	0	0	0	66	36.6	45	36 to 46	54.5	10	15.2	0	62	4
9/18/2024	23:00	0	1	0	5	10	5	8	11	2	1	0	0	0	0	0	0	0	0	43	38.7	46.5	37 to 47	46.5	10	23.3	0	42	1
24 Hr Summary		23	65	226	404	587	573	718	542	91	15	2	1	0	0	0	0	0	0	3247	36.5	46	37 to 47	44.6	508	15.6	9	3055	183

Incoming Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

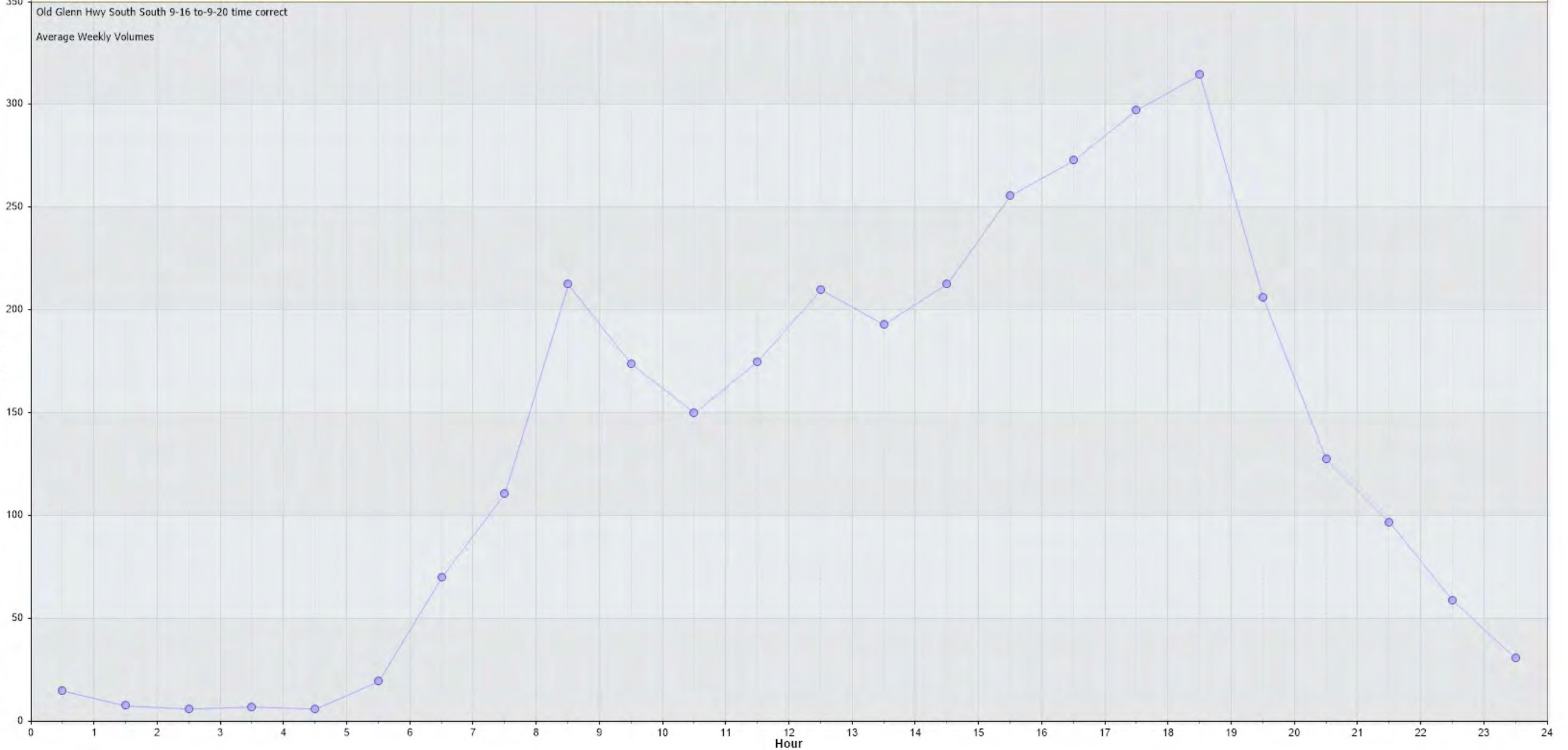
Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/19/2024	00:00	0	0	1	4	5	2	3	7	0	0	0	0	0	0	0	0	0	0	22	37.5	47.5	39 to 49	50.0	7	31.8	0	22	0
9/19/2024	01:00	1	0	2	0	1	0	1	4	0	0	0	0	0	0	0	0	0	0	9	35.1	46	38 to 48	55.6	3	33.3	0	9	0
9/19/2024	02:00	0	0	1	1	2	1	0	2	0	1	0	0	0	0	0	0	0	0	8	37	49	21 to 31	50.0	2	25.0	0	7	1
9/19/2024	03:00	0	0	2	2	3	3	2	0	1	0	0	0	0	0	0	0	0	0	13	33.5	41	21 to 31	53.8	1	7.7	0	12	1
9/19/2024	04:00	0	0	0	1	0	4	1	2	0	0	0	0	0	0	0	0	0	0	8	37.8	44.5	35 to 45	87.5	*	*	0	8	0
9/19/2024	05:00	0	0	1	1	3	3	8	4	0	1	0	0	0	0	0	0	0	0	21	39.9	45	36 to 46	66.7	3	14.3	0	21	0
9/19/2024	06:00	0	0	1	4	14	16	15	13	4	0	0	0	0	0	0	0	0	0	67	39.1	46	32 to 42	55.2	14	20.9	1	63	3
9/19/2024	07:00	1	2	22	23	23	17	14	4	2	0	0	0	0	0	0	0	0	0	108	31.6	41.2	21 to 31	52.8	5	4.6	1	100	7
9/19/2024	08:00	5	10	67	35	30	15	27	26	2	0	0	0	0	0	0	0	0	0	217	30.4	42.8	20 to 30	48.8	20	9.2	1	199	17
9/19/2024	09:00	1	4	13	29	43	29	43	25	4	0	0	0	0	0	0	0	0	0	191	35.4	44	27 to 37	42.9	25	13.1	1	181	9
9/19/2024	10:00	5	2	6	23	36	23	37	26	6	1	0	0	0	0	0	0	0	0	165	36.3	45.2	37 to 47	44.2	26	15.8	4	147	14
9/19/2024	11:00	0	2	4	18	38	41	53	27	7	0	0	0	0	0	0	0	0	0	190	37.9	45.1	36 to 46	54.7	29	15.3	1	180	9
9/19/2024	12:00	1	2	7	22	39	40	56	57	12	1	0	0	0	0	0	0	0	0	237	38.9	46.4	37 to 47	52.3	57	24.1	1	217	19
9/19/2024	13:00	1	1	3	36	38	28	67	37	6	1	0	0	0	0	0	0	0	0	218	37.8	45.1	37 to 47	52.3	34	15.6	0	206	12
9/19/2024	14:00	1	5	34	39	40	32	35	20	3	3	0	0	0	0	0	0	0	0	212	33.6	43.5	22 to 32	44.8	21	9.9	3	195	14
9/19/2024	15:00	5	21	45	36	37	56	58	20	3	0	0	0	0	0	0	0	0	0	281	32.7	42.5	33 to 43	42.7	17	6.0	2	264	15
9/19/2024	16:00	1	1	5	25	44	63	73	45	11	0	0	0	0	0	0	0	0	0	268	38.4	44.8	35 to 45	58.2	36	13.4	0	256	12
9/19/2024	17:00	4	2	5	46	53	47	78	70	13	1	0	0	0	0	0	0	0	0	319	38.2	45.8	38 to 48	50.5	66	20.7	1	306	12
9/19/2024	18:00	2	2	2	26	62	69	103	80	12	1	0	0	0	0	0	0	0	0	359	39.3	46	37 to 47	55.4	72	20.1	1	351	7
9/19/2024	19:00	2	1	5	30	32	30	57	44	10	2	1	0	0	0	0	0	0	0	214	38.6	46.2	39 to 49	49.1	44	20.6	0	213	1
9/19/2024	20:00	1	1	1	10	28	24	41	26	7	0	1	0	0	0	0	0	0	0	140	39.2	45.2	36 to 46	57.9	23	16.4	0	138	2
9/19/2024	21:00	1	0	0	11	14	12	34	22	5	1	1	0	0	0	0	0	0	0	101	39.7	47.2	39 to 49	59.4	19	18.8	0	98	3
9/19/2024	22:00	0	1	3	16	9	10	15	8	2	0	1	0	0	0	0	0	0	0	65	36.3	44.5	37 to 47	47.7	9	13.8	0	64	1
9/19/2024	23:00	0	0	1	4	6	2	9	5	2	0	0	0	0	0	0	0	0	0	29	38.4	46	39 to 49	51.7	5	17.2	0	29	0
24 Hr Summary		32	57	231	442	600	567	830	574	112	13	4	0	0	0	0	0	0	0	3462	36.7	46	37 to 47	46.4	538	15.5	17	3286	159

Incoming Histogram
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/20/2024	00:00	0	0	1	1	5	2	1	3	0	0	0	0	0	0	0	0	0	0	13	35.9	46.5	25 to 35	61.5	3	23.1	0	13	0	
9/20/2024	01:00	0	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	5	39.4	41	34 to 44	100.0	*	*	0	5	0	
9/20/2024	02:00	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	2	37.5	41	31 to 41	100.0	*	*	0	2	0	
9/20/2024	03:00	0	0	1	3	1	0	0	0	1	0	0	0	0	0	0	0	0	0	6	31.5	31	21 to 31	83.3	1	16.7	0	5	1	
9/20/2024	04:00	0	0	0	0	1	1	2	2	0	0	0	0	0	0	0	0	0	0	6	42.5	46.5	37 to 47	83.3	2	33.3	0	6	0	
9/20/2024	05:00	0	0	0	3	1	3	9	1	0	0	0	0	0	0	0	0	0	0	17	38.6	43.5	34 to 44	76.5	1	5.9	0	15	2	
9/20/2024	06:00	1	1	0	6	10	18	17	9	3	1	0	0	0	0	0	0	0	0	66	38.4	45	35 to 45	57.6	10	15.2	1	62	3	
9/20/2024	07:00	1	2	17	23	16	10	13	9	5	0	0	0	0	0	0	0	0	0	96	32.7	44	22 to 32	47.9	12	12.5	1	90	5	
9/20/2024	08:00	3	10	50	40	33	25	32	12	5	0	0	0	0	0	0	0	0	0	210	31	41.5	19 to 29	45.7	13	6.2	2	190	18	
9/20/2024	09:00	1	4	10	21	38	19	35	18	3	0	0	0	0	0	0	0	0	0	149	35.1	43.8	26 to 36	43.6	11	7.4	0	137	12	
9/20/2024	10:00	0	0	3	14	28	12	12	12	1	2	0	0	0	0	0	0	0	0	84	35.4	44.3	26 to 36	57.1	9	10.7	0	76	8	
9/20/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
24 Hr Summary		6	17	82	111	134	93	124	66	18	3	0	0	0	0	0	0	0	0	654	34	44	26 to 36	41.3	62	9.5	4	601	49	

Incoming: Average Hourly Volume for Week of 9/16/2024
Average Counts By Hour (9/16/2024) — Average Counts By Hour (9/16/2024)



Incoming: Average Hourly WEEKDAY Speeds for Week of 9/16/2024
Average Hourly WEEKDAY Speeds By Hour (9/16/2024) — Average Hourly WEEKDAY Speeds By Hour (9/16/2024)

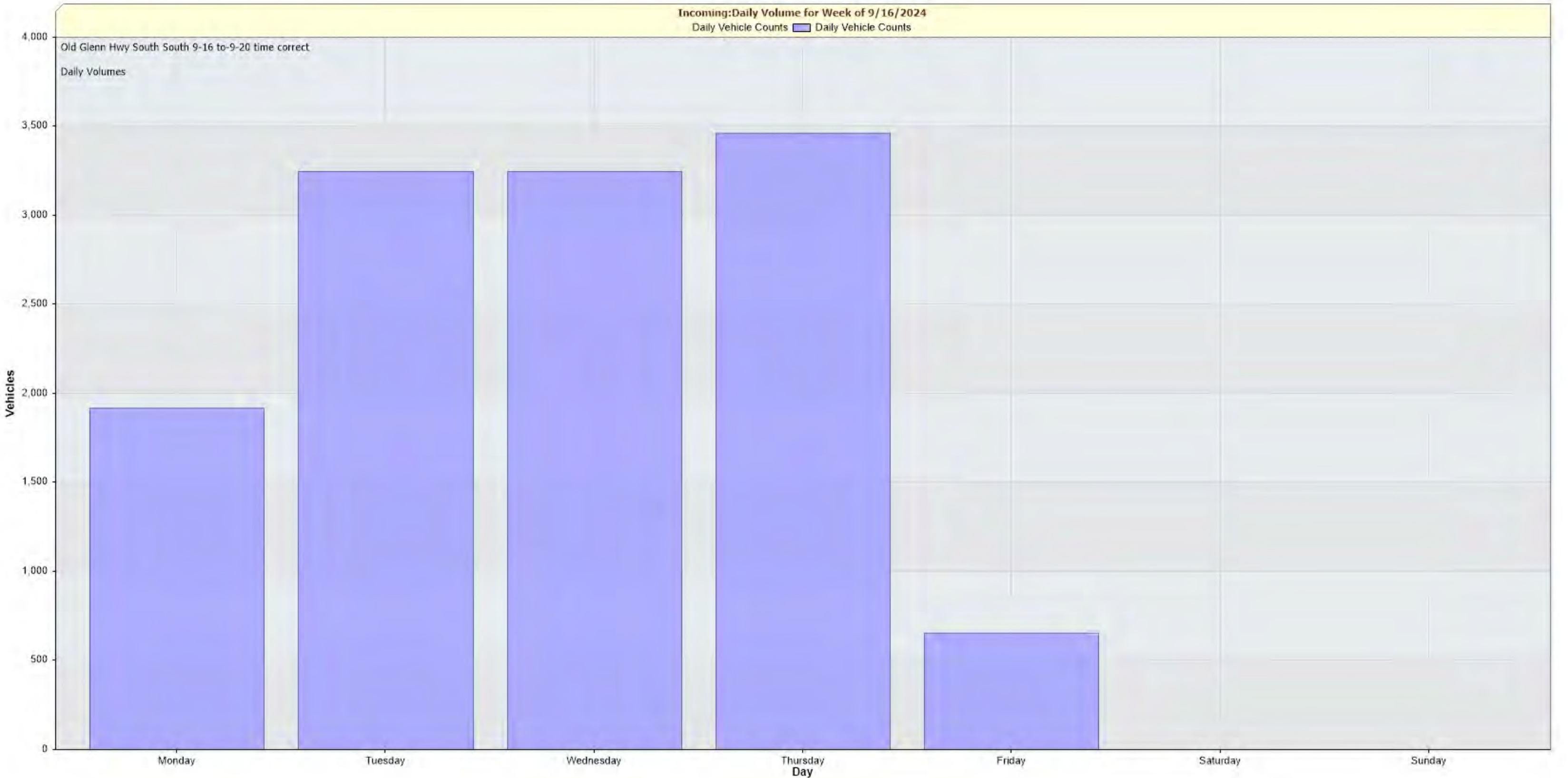
Old Glenn Hwy South South 9-16 to 9-20 time correct
Average Weekly Speeds



Incoming:Daily Volume for Week of 9/16/2024

Daily Vehicle Counts

Old Glenn Hwy South South 9-16 to-9-20 time correct
Daily Volumes



For Project: Old Glenn Hwy South South 9-16 to-9-20 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:56:18 PM

Speed Intervals: 1 MPH

Time Intervals: Instant

Traffic Report From: 9/16/2024 1:00:00 PM through 9/20/2024 10:59:59 AM

85th Percentile Speed: 43 MPH

85th Percentile Vehicles: 9308

Max Speed: 58 MPH on 9/18/2024 8:15:43 AM

Total Vehicles: 10951

AADT: 2796

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	2190	2190
AM Peak	7:00 AM 172	172
PM Peak	5:00 PM 294	294

Speed

Speed Limit: 45

85th Percentile Speed: 43

50th Percentile Speed: 33

10 MPH Pace Interval: 36.0 MPH to 46.0 MPH

Average Speed: 33.27

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	90	178	183	181	16	N/A	N/A
% over limit	5.2	6.2	6.5	6.0	3.0	N/A	N/A
Avg Speeder	47.4	47.6	47.7	47.7	48.0	N/A	N/A
Avg Speed	33.1	33.3	33.5	33.6	30.8	N/A	N/A

Class Counts

	Number	%
VEH_SM	6	0.1
VEH_MED	10145	92.6
VEH_LG	800	7.3
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/16/2024 2:00:00 PM	44.0	99	116	49	47.4	6.9%	34.9
9/16/2024 3:00:00 PM	39.0	210	247	51	47.5	2.4%	29.0
9/16/2024 4:00:00 PM	42.0	221	260	49	46.7	4.6%	31.9
9/16/2024 5:00:00 PM	43.0	236	278	56	47.6	6.1%	33.9
9/16/2024 6:00:00 PM	44.0	226	266	52	47.6	6.0%	36.0
9/16/2024 7:00:00 PM	43.0	175	206	53	47.3	7.3%	34.1
9/16/2024 8:00:00 PM	45.0	97	114	50	47.5	9.6%	34.6
9/16/2024 9:00:00 PM	42.0	75	88	45	0.0	0.0%	33.0
9/16/2024 10:00:00 PM	43.0	60	70	47	46.7	4.3%	32.6
9/16/2024 11:00:00 PM	40.0	42	50	48	48.0	4.0%	32.1
9/17/2024 12:00:00 AM	37.0	21	25	45	0.0	0.0%	31.1
9/17/2024 1:00:00 AM	45.0	8	9	54	54.0	11.1%	32.4
9/17/2024 2:00:00 AM	41.0	10	12	49	49.0	8.3%	36.3
9/17/2024 3:00:00 AM	44.0	7	8	48	48.0	12.5%	35.6
9/17/2024 4:00:00 AM	28.0	1	1	28	0.0	0.0%	28.0
9/17/2024 5:00:00 AM	44.0	1	1	44	0.0	0.0%	44.0
9/17/2024 6:00:00 AM	45.0	3	4	48	48.0	25.0%	40.3
9/17/2024 7:00:00 AM	43.0	21	25	47	46.5	7.7%	34.1
9/17/2024 8:00:00 AM	41.0	138	162	52	49.7	1.9%	29.0
9/17/2024 9:00:00 AM	42.0	104	122	51	47.8	4.9%	30.1
9/17/2024 10:00:00 AM	42.0	131	154	52	48.4	5.2%	32.2
9/17/2024 11:00:00 AM	43.0	131	154	53	47.8	5.2%	32.6
9/17/2024 12:00:00 PM	44.0	133	156	55	48.2	5.8%	33.7
9/17/2024 1:00:00 PM	45.0	148	174	51	47.5	10.3%	35.3
9/17/2024 2:00:00 PM	43.0	158	186	51	47.5	7.0%	33.8
9/17/2024 3:00:00 PM	39.0	199	234	48	46.7	2.6%	28.9
9/17/2024 4:00:00 PM	42.0	230	270	56	48.2	4.8%	31.4
9/17/2024 5:00:00 PM	42.0	235	277	50	47.6	4.3%	34.4
9/17/2024 6:00:00 PM	44.0	275	323	54	47.4	8.4%	35.8
9/17/2024 7:00:00 PM	45.0	212	250	53	46.7	12.4%	36.9
9/17/2024 8:00:00 PM	43.0	120	141	52	48.8	2.8%	34.5
9/17/2024 9:00:00 PM	41.0	78	92	49	48.3	4.3%	33.8
9/17/2024 10:00:00 PM	43.0	50	59	50	49.0	6.8%	34.9
9/17/2024 11:00:00 PM	46.0	26	31	47	46.2	19.4%	33.6
9/18/2024 12:00:00 AM	45.0	8	10	45	0.0	0.0%	35.2
9/18/2024 1:00:00 AM	36.0	12	14	46	46.0	7.1%	30.0
9/18/2024 2:00:00 AM	44.0	4	5	47	47.0	20.0%	36.8
9/18/2024 3:00:00 AM	**No Data**						
9/18/2024 4:00:00 AM	40.0	2	2	40	0.0	0.0%	39.0
9/18/2024 5:00:00 AM	26.0	2	2	26	0.0	0.0%	23.0
9/18/2024 6:00:00 AM	43.0	4	5	44	0.0	0.0%	35.0
9/18/2024 7:00:00 AM	44.0	20	23	50	48.5	8.7%	35.1
9/18/2024 8:00:00 AM	39.0	139	163	53	48.6	3.0%	28.5
9/18/2024 9:00:00 AM	43.0	103	121	58	50.0	6.6%	31.2
9/18/2024 10:00:00 AM	42.0	116	137	49	47.3	2.9%	32.5
9/18/2024 11:00:00 AM	43.0	106	125	54	48.2	7.2%	34.1
9/18/2024 12:00:00 PM	44.0	144	170	51	48.3	8.8%	35.1

Outgoing Summary
Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

9/18/2024 1:00:00 PM	44.0	125	147	48	46.6	8.2%	35.4
9/18/2024 2:00:00 PM	42.0	133	157	54	47.5	8.9%	33.5
9/18/2024 3:00:00 PM	40.0	206	242	51	48.0	2.5%	29.4
9/18/2024 4:00:00 PM	44.0	215	253	51	47.5	8.3%	33.6
9/18/2024 5:00:00 PM	44.0	224	264	58	47.9	6.1%	35.4
9/18/2024 6:00:00 PM	44.0	244	287	51	47.1	9.8%	35.7
9/18/2024 7:00:00 PM	44.0	195	229	52	47.6	8.3%	34.6
9/18/2024 8:00:00 PM	42.0	139	163	53	48.0	5.5%	33.0
9/18/2024 9:00:00 PM	44.0	112	132	53	47.9	6.8%	35.3
9/18/2024 10:00:00 PM	42.0	82	97	48	47.0	2.1%	32.3
9/18/2024 11:00:00 PM	44.0	35	41	49	48.0	4.9%	35.6
9/19/2024 12:00:00 AM	44.0	23	27	45	0.0	0.0%	34.0
9/19/2024 1:00:00 AM	43.0	8	9	43	0.0	0.0%	31.1
9/19/2024 2:00:00 AM	44.0	8	10	46	46.0	10.0%	31.3
9/19/2024 3:00:00 AM	39.0	5	6	41	0.0	0.0%	28.2
9/19/2024 4:00:00 AM	43.0	4	5	49	49.0	20.0%	35.4
9/19/2024 5:00:00 AM	35.0	5	6	51	51.0	16.7%	34.3
9/19/2024 6:00:00 AM	43.0	5	6	50	50.0	16.7%	38.0
9/19/2024 7:00:00 AM	42.0	19	22	46	46.0	4.5%	35.4
9/19/2024 8:00:00 AM	39.0	156	184	52	48.0	3.3%	28.4
9/19/2024 9:00:00 AM	40.0	96	113	49	47.7	2.7%	29.7
9/19/2024 10:00:00 AM	44.0	123	145	49	47.7	4.8%	32.9
9/19/2024 11:00:00 AM	42.0	141	166	50	48.0	4.8%	33.3
9/19/2024 12:00:00 PM	44.0	147	173	50	47.6	8.7%	35.2
9/19/2024 1:00:00 PM	43.0	142	167	48	46.9	4.8%	34.4
9/19/2024 2:00:00 PM	44.0	167	196	53	47.8	9.7%	34.8
9/19/2024 3:00:00 PM	41.0	221	260	50	47.5	3.1%	30.6
9/19/2024 4:00:00 PM	42.0	230	271	52	47.5	5.5%	33.0
9/19/2024 5:00:00 PM	43.0	240	282	50	47.3	5.7%	34.5
9/19/2024 6:00:00 PM	44.0	257	302	50	47.1	8.3%	35.7
9/19/2024 7:00:00 PM	44.0	214	252	58	47.2	7.9%	35.0
9/19/2024 8:00:00 PM	43.0	157	185	58	49.5	5.9%	35.3
9/19/2024 9:00:00 PM	44.0	101	119	55	49.2	10.1%	36.7
9/19/2024 10:00:00 PM	42.0	66	78	50	48.5	2.6%	32.6
9/19/2024 11:00:00 PM	41.0	42	50	49	49.0	2.0%	30.7
9/20/2024 12:00:00 AM	44.0	19	22	45	0.0	0.0%	33.2
9/20/2024 1:00:00 AM	43.0	3	3	43	0.0	0.0%	33.0
9/20/2024 2:00:00 AM	28.0	4	5	29	0.0	0.0%	25.2
9/20/2024 3:00:00 AM	35.0	1	1	35	0.0	0.0%	35.0
9/20/2024 4:00:00 AM	38.0	4	5	44	0.0	0.0%	29.8
9/20/2024 5:00:00 AM	25.0	2	2	25	0.0	0.0%	23.5
9/20/2024 6:00:00 AM	43.0	5	6	45	0.0	0.0%	38.2
9/20/2024 7:00:00 AM	43.0	16	19	49	47.5	10.5%	34.2
9/20/2024 8:00:00 AM	38.0	154	181	49	47.3	2.2%	28.5
9/20/2024 9:00:00 AM	42.0	100	118	50	47.3	3.4%	30.7
9/20/2024 10:00:00 AM	42.0	116	136	54	49.0	3.7%	33.0
9/20/2024 11:00:00 AM	40.0	55	65	50	50.0	1.5%	31.2

Outgoing Summary
Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/17/2024 12:00:00 AM	43.0	1462	1720	56	47.4	5.2%	33.1
9/18/2024 12:00:00 AM	43.0	2427	2855	56	47.6	6.2%	33.3
9/19/2024 12:00:00 AM	43.0	2385	2806	58	47.7	6.5%	33.5
9/20/2024 12:00:00 AM	43.0	2575	3029	58	47.7	6.0%	33.6
9/20/2024 10:59:59 AM	41.0	460	541	54	48.0	3.0%	30.8

Outgoing Weekly Counts
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	9	14	9	3	*	*	8.75	0	41.68
1 - 2	*	12	5	10	5	*	*	8	0	39.25
2 - 3	*	8	0	6	1	*	*	3.75	0	39.33
3 - 4	*	1	2	5	5	*	*	3.25	0	37.25
4 - 5	*	1	2	6	2	*	*	2.75	0	32.5
5 - 6	*	4	5	6	6	*	*	5.25	0	43.5
6 - 7	*	25	23	22	19	*	*	22.25	0	42.62
7 - 8	*	162	163	184	181	*	*	172.5	0	38.88
8 - 9	*	122	121	113	118	*	*	118.5	0	41.47
9 - 10	*	154	137	145	136	*	*	143	0	42.08
10 - 11	*	154	125	166	65	*	*	127.5	0	41.7
11 - 12	*	156	170	173	*	*	*	166.33	0	43.4
12 - 13	*	174	147	167	*	*	*	162.67	0	43.53
13 - 14	116	186	157	196	*	*	*	163.75	0	42.85
14 - 15	247	234	242	260	*	*	*	245.75	0	39.48
15 - 16	260	270	253	271	*	*	*	263.5	0	42.15
16 - 17	278	277	264	282	*	*	*	275.25	0	42.5
17 - 18	266	323	287	302	*	*	*	294.5	0	43.48
18 - 19	206	250	229	252	*	*	*	234.25	0	43.4
19 - 20	114	141	163	185	*	*	*	150.75	0	42.75
20 - 21	88	92	132	119	*	*	*	107.75	0	42.3
21 - 22	70	59	97	78	*	*	*	76	0	41.92
22 - 23	50	31	41	50	*	*	*	43	0	42.38
23 - 24	25	10	27	22	*	*	*	21	0	42.32
Totals	1720	2855	2806	3029	541	0	0			
% of Total	15.71%	26.07%	25.62%	27.66%	4.94%	0%	0%			

Outgoing Monthly Counts
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	9	14	9	3	*	*	8.75	0	41.68
1 - 2	*	12	5	10	5	*	*	8	0	39.25
2 - 3	*	8	0	6	1	*	*	3.75	0	39.33
3 - 4	*	1	2	5	5	*	*	3.25	0	37.25
4 - 5	*	1	2	6	2	*	*	2.75	0	32.5
5 - 6	*	4	5	6	6	*	*	5.25	0	43.5
6 - 7	*	25	23	22	19	*	*	22.25	0	42.62
7 - 8	*	162	163	184	181	*	*	172.5	0	38.88
8 - 9	*	122	121	113	118	*	*	118.5	0	41.47
9 - 10	*	154	137	145	136	*	*	143	0	42.08
10 - 11	*	154	125	166	65	*	*	127.5	0	41.7
11 - 12	*	156	170	173	*	*	*	166.33	0	43.4
12 - 13	*	174	147	167	*	*	*	162.67	0	43.53
13 - 14	116	186	157	196	*	*	*	163.75	0	42.85
14 - 15	247	234	242	260	*	*	*	245.75	0	39.48
15 - 16	260	270	253	271	*	*	*	263.5	0	42.15
16 - 17	278	277	264	282	*	*	*	275.25	0	42.5
17 - 18	266	323	287	302	*	*	*	294.5	0	43.48
18 - 19	206	250	229	252	*	*	*	234.25	0	43.4
19 - 20	114	141	163	185	*	*	*	150.75	0	42.75
20 - 21	88	92	132	119	*	*	*	107.75	0	42.3
21 - 22	70	59	97	78	*	*	*	76	0	41.92
22 - 23	50	31	41	50	*	*	*	43	0	42.38
23 - 24	25	10	27	22	*	*	*	21	0	42.32
Totals	1720	2855	2806	3029	541	0	0			
% of Total	15.71%	26.07%	25.62%	27.66%	4.94%	0%	0%			

Outgoing Weekly Speeds

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	32.44	30	31.11	33	*	*	31.17	*	41.68
1 - 2	*	36.25	36.8	31.3	25.2	*	*	33.06	*	39.25
2 - 3	*	35.62	0	28.17	35	*	*	32.6	*	39.33
3 - 4	*	28	39	35.4	29.8	*	*	33.23	*	37.25
4 - 5	*	44	23	34.33	23.5	*	*	31.18	*	32.5
5 - 6	*	40.25	35	38	38.17	*	*	37.76	*	43.5
6 - 7	*	34.08	35.13	35.36	34.21	*	*	34.7	*	42.62
7 - 8	*	28.96	28.5	28.42	28.5	*	*	28.59	*	38.88
8 - 9	*	30.12	31.24	29.73	30.73	*	*	30.46	*	41.47
9 - 10	*	32.21	32.5	32.9	33.02	*	*	32.65	*	42.08
10 - 11	*	32.62	34.13	33.32	31.17	*	*	33.03	*	41.7
11 - 12	*	33.72	35.11	35.21	*	*	*	34.71	*	43.4
12 - 13	*	35.31	35.38	34.41	*	*	*	35.02	*	43.53
13 - 14	34.69	33.85	33.55	34.83	*	*	*	34.22	*	42.85
14 - 15	28.96	28.89	29.37	30.6	*	*	*	29.48	*	39.48
15 - 16	31.9	31.42	33.63	32.96	*	*	*	32.46	*	42.15
16 - 17	33.93	34.45	35.36	34.49	*	*	*	34.55	*	42.5
17 - 18	35.98	35.82	35.7	35.74	*	*	*	35.81	*	43.48
18 - 19	34.07	36.88	34.56	34.99	*	*	*	35.19	*	43.4
19 - 20	34.55	34.5	32.98	35.26	*	*	*	34.33	*	42.75
20 - 21	33.05	33.84	35.33	36.71	*	*	*	34.93	*	42.3
21 - 22	32.63	34.9	32.34	32.62	*	*	*	32.97	*	41.92
22 - 23	32.12	33.61	35.63	30.72	*	*	*	32.82	*	42.38
23 - 24	31.12	35.2	33.96	33.23	*	*	*	33.07	*	42.32
Totals	33.2	33.3	33.5	33.5	30.7	0	0			
% of Total	20.22%	20.28%	20.4%	20.4%	18.7%	0%	0%			

Outgoing Monthly Speeds

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	32.44	30	31.11	33	*	*	31.17	*	41.68
1 - 2	*	36.25	36.8	31.3	25.2	*	*	33.06	*	39.25
2 - 3	*	35.62	0	28.17	35	*	*	32.6	*	39.33
3 - 4	*	28	39	35.4	29.8	*	*	33.23	*	37.25
4 - 5	*	44	23	34.33	23.5	*	*	31.18	*	32.5
5 - 6	*	40.25	35	38	38.17	*	*	37.76	*	43.5
6 - 7	*	34.08	35.13	35.36	34.21	*	*	34.7	*	42.62
7 - 8	*	28.96	28.5	28.42	28.5	*	*	28.59	*	38.88
8 - 9	*	30.12	31.24	29.73	30.73	*	*	30.46	*	41.47
9 - 10	*	32.21	32.5	32.9	33.02	*	*	32.65	*	42.08
10 - 11	*	32.62	34.13	33.32	31.17	*	*	33.03	*	41.7
11 - 12	*	33.72	35.11	35.21	*	*	*	34.71	*	43.4
12 - 13	*	35.31	35.38	34.41	*	*	*	35.02	*	43.53
13 - 14	34.69	33.85	33.55	34.83	*	*	*	34.22	*	42.85
14 - 15	28.96	28.89	29.37	30.6	*	*	*	29.48	*	39.48
15 - 16	31.9	31.42	33.63	32.96	*	*	*	32.46	*	42.15
16 - 17	33.93	34.45	35.36	34.49	*	*	*	34.55	*	42.5
17 - 18	35.98	35.82	35.7	35.74	*	*	*	35.81	*	43.48
18 - 19	34.07	36.88	34.56	34.99	*	*	*	35.19	*	43.4
19 - 20	34.55	34.5	32.98	35.26	*	*	*	34.33	*	42.75
20 - 21	33.05	33.84	35.33	36.71	*	*	*	34.93	*	42.3
21 - 22	32.63	34.9	32.34	32.62	*	*	*	32.97	*	41.92
22 - 23	32.12	33.61	35.63	30.72	*	*	*	32.82	*	42.38
23 - 24	31.12	35.2	33.96	33.23	*	*	*	33.07	*	42.32
Totals	33.2	33.3	33.5	33.5	30.7	0	0			
% of Total	20.22%	20.28%	20.4%	20.4%	18.7%	0%	0%			

Outgoing Weekly EightyFifthSpeeds
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	45	36	42.7	43	*	*	41.68	0	41.68
1 - 2	*	41	44	44	28	*	*	39.25	0	39.25
2 - 3	*	44	0	39	35	*	*	39.33	0	39.33
3 - 4	*	28	40	43	38	*	*	37.25	0	37.25
4 - 5	*	44	26	35	25	*	*	32.5	0	32.5
5 - 6	*	45	43	43	43	*	*	43.5	0	43.5
6 - 7	*	42.3	43.7	42	42.5	*	*	42.62	0	42.62
7 - 8	*	40.6	38.5	39	37.4	*	*	38.88	0	38.88
8 - 9	*	41.9	42.2	39.8	42	*	*	41.47	0	41.47
9 - 10	*	41.7	41.2	43.4	42	*	*	42.08	0	42.08
10 - 11	*	42.2	42.9	41.7	40	*	*	41.7	0	41.7
11 - 12	*	43.1	43.2	43.9	*	*	*	43.4	0	43.4
12 - 13	*	44.1	43.5	43	*	*	*	43.53	0	43.53
13 - 14	43.3	42.5	42	43.6	*	*	*	42.85	0	42.85
14 - 15	38.6	38.7	40	40.6	*	*	*	39.48	0	39.48
15 - 16	41.8	41.8	43.1	41.9	*	*	*	42.15	0	42.15
16 - 17	42.4	42	43.1	42.5	*	*	*	42.5	0	42.5
17 - 18	43.5	43.3	43.7	43.4	*	*	*	43.48	0	43.48
18 - 19	42.9	44.3	43.2	43.2	*	*	*	43.4	0	43.4
19 - 20	44.1	42.6	41.7	42.6	*	*	*	42.75	0	42.75
20 - 21	41.8	40.5	43.3	43.6	*	*	*	42.3	0	42.3
21 - 22	42.2	42.5	41.4	41.6	*	*	*	41.92	0	41.92
22 - 23	40	45.2	44	40.3	*	*	*	42.38	0	42.38
23 - 24	37	44.3	44	44	*	*	*	42.32	0	42.32
Totals	457.6	1010.6	953.7	1006.8	415.9	0	0			
% of Total	11.9%	26.29%	24.81%	26.19%	10.82%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	45	36	42.7	43	*	*	41.68	0	41.68
1 - 2	*	41	44	44	28	*	*	39.25	0	39.25
2 - 3	*	44	0	39	35	*	*	39.33	0	39.33
3 - 4	*	28	40	43	38	*	*	37.25	0	37.25
4 - 5	*	44	26	35	25	*	*	32.5	0	32.5
5 - 6	*	45	43	43	43	*	*	43.5	0	43.5
6 - 7	*	42.3	43.7	42	42.5	*	*	42.62	0	42.62
7 - 8	*	40.6	38.5	39	37.4	*	*	38.88	0	38.88
8 - 9	*	41.9	42.2	39.8	42	*	*	41.47	0	41.47
9 - 10	*	41.7	41.2	43.4	42	*	*	42.08	0	42.08
10 - 11	*	42.2	42.9	41.7	40	*	*	41.7	0	41.7
11 - 12	*	43.1	43.2	43.9	*	*	*	43.4	0	43.4
12 - 13	*	44.1	43.5	43	*	*	*	43.53	0	43.53
13 - 14	43.3	42.5	42	43.6	*	*	*	42.85	0	42.85
14 - 15	38.6	38.7	40	40.6	*	*	*	39.48	0	39.48
15 - 16	41.8	41.8	43.1	41.9	*	*	*	42.15	0	42.15
16 - 17	42.4	42	43.1	42.5	*	*	*	42.5	0	42.5
17 - 18	43.5	43.3	43.7	43.4	*	*	*	43.48	0	43.48
18 - 19	42.9	44.3	43.2	43.2	*	*	*	43.4	0	43.4
19 - 20	44.1	42.6	41.7	42.6	*	*	*	42.75	0	42.75
20 - 21	41.8	40.5	43.3	43.6	*	*	*	42.3	0	42.3
21 - 22	42.2	42.5	41.4	41.6	*	*	*	41.92	0	41.92
22 - 23	40	45.2	44	40.3	*	*	*	42.38	0	42.38
23 - 24	37	44.3	44	44	*	*	*	42.32	0	42.32

Summary of Violators

Old Glenn Hwy South South 9-16 to-9-20 time correct from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	35	31.2	2	50.0
01:00:00	32	33.1	3	47.3
02:00:00	15	32.6	1	48.0
03:00:00	13	33.2	1	49.0
04:00:00	11	31.2	1	51.0
05:00:00	21	37.8	2	49.0
06:00:00	89	34.7	7	47.3
07:00:00	690	28.6	18	48.3
08:00:00	474	30.5	21	48.5
09:00:00	572	32.6	24	48.1
10:00:00	510	33.0	26	48.1
11:00:00	499	34.7	39	48.0
12:00:00	488	35.0	38	47.1
13:00:00	655	34.2	54	47.6
14:00:00	983	29.5	26	47.4
15:00:00	1054	32.5	61	47.5
16:00:00	1101	34.5	61	47.6
17:00:00	1178	35.8	96	47.3
18:00:00	937	35.2	85	47.1
19:00:00	603	34.3	35	48.4
20:00:00	431	34.9	25	48.6
21:00:00	304	33.0	11	47.9
22:00:00	172	32.8	11	47.1
23:00:00	84	33.1	0	0.0

Outgoing Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	13:00	0	2	15	20	20	15	32	12	0	0	0	0	0	0	0	0	0	0	116	34.7	43.3	36 to 46	45.7	8	6.9	6	105	5	
9/16/2024	14:00	0	23	74	52	27	38	23	9	1	0	0	0	0	0	0	0	0	0	247	29	38.6	19 to 29	55.5	6	2.4	0	227	20	
9/16/2024	15:00	2	16	38	57	49	32	49	17	0	0	0	0	0	0	0	0	0	0	260	31.9	41.8	22 to 32	43.5	12	4.6	0	242	18	
9/16/2024	16:00	1	10	34	46	52	34	76	23	1	1	0	0	0	0	0	0	0	0	278	33.9	42.4	36 to 46	43.5	17	6.1	0	249	29	
9/16/2024	17:00	0	2	31	26	45	52	78	31	1	0	0	0	0	0	0	0	0	0	266	36	43.5	35 to 45	54.9	16	6.0	0	252	14	
9/16/2024	18:00	1	2	21	49	36	29	49	18	1	0	0	0	0	0	0	0	0	0	206	34.1	42.9	23 to 33	43.2	15	7.3	0	195	11	
9/16/2024	19:00	1	2	14	20	15	21	23	16	2	0	0	0	0	0	0	0	0	0	114	34.6	44.1	38 to 48	47.4	11	9.6	0	108	6	
9/16/2024	20:00	0	2	13	22	12	11	25	3	0	0	0	0	0	0	0	0	0	0	88	33	41.8	22 to 32	47.7	*	*	0	86	2	
9/16/2024	21:00	2	6	8	7	14	15	14	4	0	0	0	0	0	0	0	0	0	0	70	32.6	42.2	34 to 44	48.6	3	4.3	0	65	5	
9/16/2024	22:00	0	0	8	15	4	12	9	2	0	0	0	0	0	0	0	0	0	0	50	32.1	40	21 to 31	48.0	2	4.0	0	50	0	
9/16/2024	23:00	0	1	4	6	5	6	2	1	0	0	0	0	0	0	0	0	0	0	25	31.1	37	26 to 36	60.0	*	*	0	24	1	
24 Hr Summary		7	66	260	320	279	265	380	136	6	1	0	0	0	0	0	0	0	0	1720	33.1	43	35 to 45	40.6	90	5.2	6	1603	111	

Outgoing Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/17/2024	00:00	0	0	3	3	0	0	1	1	1	0	0	0	0	0	0	0	0	0	9	32.4	45	19 to 29	66.7	1	11.1	0	7	2
9/17/2024	01:00	0	0	1	2	1	3	4	1	0	0	0	0	0	0	0	0	0	0	12	36.3	41	31 to 41	58.3	1	8.3	0	12	0
9/17/2024	02:00	0	0	0	3	0	1	3	1	0	0	0	0	0	0	0	0	0	0	8	35.6	44	34 to 44	50.0	1	12.5	0	8	0
9/17/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	28	28	18 to 28	100.0	*	*	0	1	0
9/17/2024	04:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	44	44	34 to 44	100.0	*	*	0	1	0
9/17/2024	05:00	0	0	0	1	0	0	1	2	0	0	0	0	0	0	0	0	0	0	4	40.3	45	38 to 48	75.0	1	25.0	0	4	0
9/17/2024	06:00	0	0	3	6	4	4	6	2	0	0	0	0	0	0	0	0	0	0	25	34.1	42.3	21 to 31	52.0	2	8.0	0	23	2
9/17/2024	07:00	0	17	49	34	15	17	22	6	2	0	0	0	0	0	0	0	0	0	162	29	40.6	18 to 28	55.6	3	1.9	0	151	11
9/17/2024	08:00	0	20	28	22	7	13	21	10	1	0	0	0	0	0	0	0	0	0	122	30.1	41.9	17 to 27	49.2	6	4.9	0	108	14
9/17/2024	09:00	0	14	24	28	21	23	33	9	2	0	0	0	0	0	0	0	0	0	154	32.2	41.7	22 to 32	39.0	8	5.2	0	131	23
9/17/2024	10:00	0	9	31	22	23	25	31	12	1	0	0	0	0	0	0	0	0	0	154	32.6	42.2	37 to 47	40.3	8	5.2	0	137	17
9/17/2024	11:00	1	6	22	31	16	31	34	14	0	1	0	0	0	0	0	0	0	0	156	33.7	43.1	35 to 45	45.5	9	5.8	0	141	15
9/17/2024	12:00	1	9	16	21	30	23	47	24	3	0	0	0	0	0	0	0	0	0	174	35.3	44.1	36 to 46	48.3	18	10.3	0	155	19
9/17/2024	13:00	0	10	17	34	38	23	48	13	3	0	0	0	0	0	0	0	0	0	186	33.8	42.5	23 to 33	41.9	13	7.0	0	172	14
9/17/2024	14:00	1	19	64	61	26	30	23	10	0	0	0	0	0	0	0	0	0	0	234	28.9	38.7	18 to 28	57.7	6	2.6	0	215	19
9/17/2024	15:00	3	26	55	35	40	34	60	15	1	1	0	0	0	0	0	0	0	0	270	31.4	41.8	19 to 29	37.0	13	4.8	0	250	20
9/17/2024	16:00	1	5	27	55	38	64	63	22	2	0	0	0	0	0	0	0	0	0	277	34.4	42	35 to 45	50.2	12	4.3	0	253	24
9/17/2024	17:00	2	5	24	49	53	50	100	37	3	0	0	0	0	0	0	0	0	0	323	35.8	43.3	36 to 46	50.8	27	8.4	0	308	15
9/17/2024	18:00	0	4	11	35	37	48	74	40	1	0	0	0	0	0	0	0	0	0	250	36.9	44.3	37 to 47	58.4	31	12.4	0	237	13
9/17/2024	19:00	0	5	13	30	12	32	38	9	2	0	0	0	0	0	0	0	0	0	141	34.5	42.6	35 to 45	54.6	4	2.8	0	133	8
9/17/2024	20:00	0	1	10	17	21	16	21	6	0	0	0	0	0	0	0	0	0	0	92	33.8	40.5	31 to 41	52.2	4	4.3	0	89	3
9/17/2024	21:00	0	0	8	10	8	16	10	5	2	0	0	0	0	0	0	0	0	0	59	34.9	42.5	32 to 42	49.2	4	6.8	0	59	0
9/17/2024	22:00	0	0	8	5	3	3	5	7	0	0	0	0	0	0	0	0	0	0	31	33.6	45.2	21 to 31	45.2	6	19.4	0	30	1
9/17/2024	23:00	0	0	2	1	2	1	1	3	0	0	0	0	0	0	0	0	0	0	10	35.2	44.3	35 to 45	50.0	*	*	0	9	1
24 Hr Summary		9	150	416	506	395	457	647	249	24	2	0	0	0	0	0	0	0	0	2855	33.3	43	36 to 46	42.3	178	6.2	0	2634	221

Outgoing Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/18/2024	00:00	0	0	4	4	1	4	0	1	0	0	0	0	0	0	0	0	0	0	14	30	36	20 to 30	64.3	1	7.1	0	14	0	
9/18/2024	01:00	0	0	0	1	1	1	1	1	0	0	0	0	0	0	0	0	0	0	5	36.8	44	26 to 36	60.0	1	20.0	0	5	0	
9/18/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/18/2024	03:00	0	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	2	39	40	30 to 40	100.0	*	*	0	2	0	
9/18/2024	04:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	23	26	16 to 26	100.0	*	*	0	2	0	
9/18/2024	05:00	0	0	1	1	0	1	2	0	0	0	0	0	0	0	0	0	0	0	5	35	43	34 to 44	60.0	*	*	0	5	0	
9/18/2024	06:00	0	0	3	5	3	2	8	1	1	0	0	0	0	0	0	0	0	0	23	35.1	43.7	34 to 44	43.5	2	8.7	0	20	3	
9/18/2024	07:00	0	15	55	30	24	18	15	5	1	0	0	0	0	0	0	0	0	0	163	28.5	38.5	18 to 28	56.4	5	3.1	0	150	13	
9/18/2024	08:00	0	12	27	22	14	15	22	6	2	1	0	0	0	0	0	0	0	0	121	31.2	42.2	19 to 29	44.6	8	6.6	0	115	6	
9/18/2024	09:00	2	7	19	25	20	29	26	9	0	0	0	0	0	0	0	0	0	0	137	32.5	41.2	33 to 43	45.3	4	2.9	0	120	17	
9/18/2024	10:00	1	5	18	17	20	18	33	11	2	0	0	0	0	0	0	0	0	0	125	34.1	42.9	37 to 47	44.8	9	7.2	0	115	10	
9/18/2024	11:00	0	3	18	40	16	23	48	18	4	0	0	0	0	0	0	0	0	0	170	35.1	43.2	36 to 46	47.1	15	8.8	0	152	18	
9/18/2024	12:00	0	5	12	25	20	25	42	18	0	0	0	0	0	0	0	0	0	0	147	35.4	43.5	36 to 46	53.1	12	8.2	0	126	21	
9/18/2024	13:00	0	4	14	40	29	31	23	14	2	0	0	0	0	0	0	0	0	0	157	33.5	42	24 to 34	46.5	14	8.9	0	141	16	
9/18/2024	14:00	1	22	66	50	36	28	31	6	2	0	0	0	0	0	0	0	0	0	242	29.4	40	19 to 29	51.7	6	2.5	0	223	19	
9/18/2024	15:00	1	15	34	42	40	33	59	25	4	0	0	0	0	0	0	0	0	0	253	33.6	43.1	36 to 46	43.9	21	8.3	0	235	18	
9/18/2024	16:00	1	10	20	42	37	46	82	23	2	1	0	0	0	0	0	0	0	0	264	35.4	43.1	35 to 45	52.3	16	6.1	0	244	20	
9/18/2024	17:00	1	6	17	55	41	52	77	36	2	0	0	0	0	0	0	0	0	0	287	35.7	43.7	37 to 47	49.5	28	9.8	0	273	14	
9/18/2024	18:00	0	4	23	48	42	26	58	25	3	0	0	0	0	0	0	0	0	0	229	34.6	43.2	24 to 34	43.7	19	8.3	0	213	16	
9/18/2024	19:00	0	6	19	37	31	27	33	7	3	0	0	0	0	0	0	0	0	0	163	33	41.7	23 to 33	46.0	9	5.5	0	152	11	
9/18/2024	20:00	0	4	11	19	23	19	42	13	1	0	0	0	0	0	0	0	0	0	132	35.3	43.3	35 to 45	50.0	9	6.8	0	128	4	
9/18/2024	21:00	0	0	21	23	10	18	23	2	0	0	0	0	0	0	0	0	0	0	97	32.3	41.4	20 to 30	47.4	2	2.1	0	93	4	
9/18/2024	22:00	0	0	6	6	4	8	11	6	0	0	0	0	0	0	0	0	0	0	41	35.6	44	35 to 45	56.1	2	4.9	0	39	2	
9/18/2024	23:00	0	1	5	6	1	2	8	4	0	0	0	0	0	0	0	0	0	0	27	34	44	35 to 45	51.9	*	*	0	27	0	
24 Hr Summary		7	119	394	539	413	427	645	231	29	2	0	0	0	0	0	0	0	0	2806	33.5	43	36 to 46	41.2	183	6.5	0	2594	212	

Outgoing Histogram

Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/19/2024	00:00	0	0	2	4	0	0	3	0	0	0	0	0	0	0	0	0	0	0	9	31.1	42.7	19 to 29	66.7	*	*	0	8	1
9/19/2024	01:00	0	0	3	3	1	0	1	2	0	0	0	0	0	0	0	0	0	0	10	31.3	44	17 to 27	60.0	1	10.0	0	9	1
9/19/2024	02:00	0	1	2	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	6	28.2	39	17 to 27	66.7	*	*	0	4	2
9/19/2024	03:00	0	0	2	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	5	35.4	43	39 to 49	60.0	1	20.0	0	5	0
9/19/2024	04:00	0	0	0	3	1	1	0	0	1	0	0	0	0	0	0	0	0	0	6	34.3	35	25 to 35	83.3	1	16.7	0	6	0
9/19/2024	05:00	0	0	1	0	1	1	2	0	1	0	0	0	0	0	0	0	0	0	6	38	43	33 to 43	66.7	1	16.7	0	6	0
9/19/2024	06:00	0	0	2	3	4	5	7	1	0	0	0	0	0	0	0	0	0	0	22	35.4	42	34 to 44	59.1	1	4.5	0	19	3
9/19/2024	07:00	0	12	66	44	25	9	17	10	1	0	0	0	0	0	0	0	0	0	184	28.4	39	19 to 29	64.1	6	3.3	0	175	9
9/19/2024	08:00	1	12	23	25	17	14	14	7	0	0	0	0	0	0	0	0	0	0	113	29.7	39.8	20 to 30	48.7	3	2.7	0	104	9
9/19/2024	09:00	2	8	27	26	12	23	28	19	0	0	0	0	0	0	0	0	0	0	145	32.9	43.4	35 to 45	43.4	7	4.8	0	129	16
9/19/2024	10:00	1	6	24	30	23	32	41	8	1	0	0	0	0	0	0	0	0	0	166	33.3	41.7	33 to 43	46.4	8	4.8	0	144	22
9/19/2024	11:00	2	9	11	29	22	27	48	23	2	0	0	0	0	0	0	0	0	0	173	35.2	43.9	35 to 45	49.1	15	8.7	0	155	18
9/19/2024	12:00	2	8	19	28	13	31	52	14	0	0	0	0	0	0	0	0	0	0	167	34.4	43	35 to 45	53.3	8	4.8	0	156	11
9/19/2024	13:00	0	4	16	46	31	26	48	20	5	0	0	0	0	0	0	0	0	0	196	34.8	43.6	23 to 33	42.9	19	9.7	0	175	21
9/19/2024	14:00	1	14	58	68	38	27	39	14	1	0	0	0	0	0	0	0	0	0	260	30.6	40.6	20 to 30	51.9	8	3.1	0	241	19
9/19/2024	15:00	2	19	30	47	47	50	58	15	3	0	0	0	0	0	0	0	0	0	271	33	41.9	34 to 44	43.9	15	5.5	0	253	18
9/19/2024	16:00	1	7	29	43	62	38	77	24	1	0	0	0	0	0	0	0	0	0	282	34.5	42.5	33 to 43	45.7	16	5.7	0	262	20
9/19/2024	17:00	1	6	21	42	58	52	84	37	1	0	0	0	0	0	0	0	0	0	302	35.7	43.4	36 to 46	50.7	25	8.3	0	291	11
9/19/2024	18:00	0	7	20	49	38	48	57	31	1	1	0	0	0	0	0	0	0	0	252	35	43.2	36 to 46	50.4	20	7.9	0	243	9
9/19/2024	19:00	0	3	16	33	27	37	52	13	2	2	0	0	0	0	0	0	0	0	185	35.3	42.6	35 to 45	51.4	11	5.9	0	180	5
9/19/2024	20:00	0	2	6	18	19	16	43	12	2	1	0	0	0	0	0	0	0	0	119	36.7	43.6	36 to 46	52.9	12	10.1	0	114	5
9/19/2024	21:00	0	3	15	13	11	14	19	2	1	0	0	0	0	0	0	0	0	0	78	32.6	41.6	33 to 43	46.2	2	2.6	0	77	1
9/19/2024	22:00	0	2	16	7	6	6	12	1	0	0	0	0	0	0	0	0	0	0	50	30.7	40.3	21 to 31	54.0	1	2.0	0	49	1
9/19/2024	23:00	0	0	3	6	4	3	3	3	0	0	0	0	0	0	0	0	0	0	22	33.2	44	24 to 34	50.0	*	*	0	21	1
24 Hr Summary		13	123	412	568	460	461	708	257	23	4	0	0	0	0	0	0	0	0	3029	33.6	43	36 to 46	42.2	181	6.0	0	2826	203

Outgoing Histogram

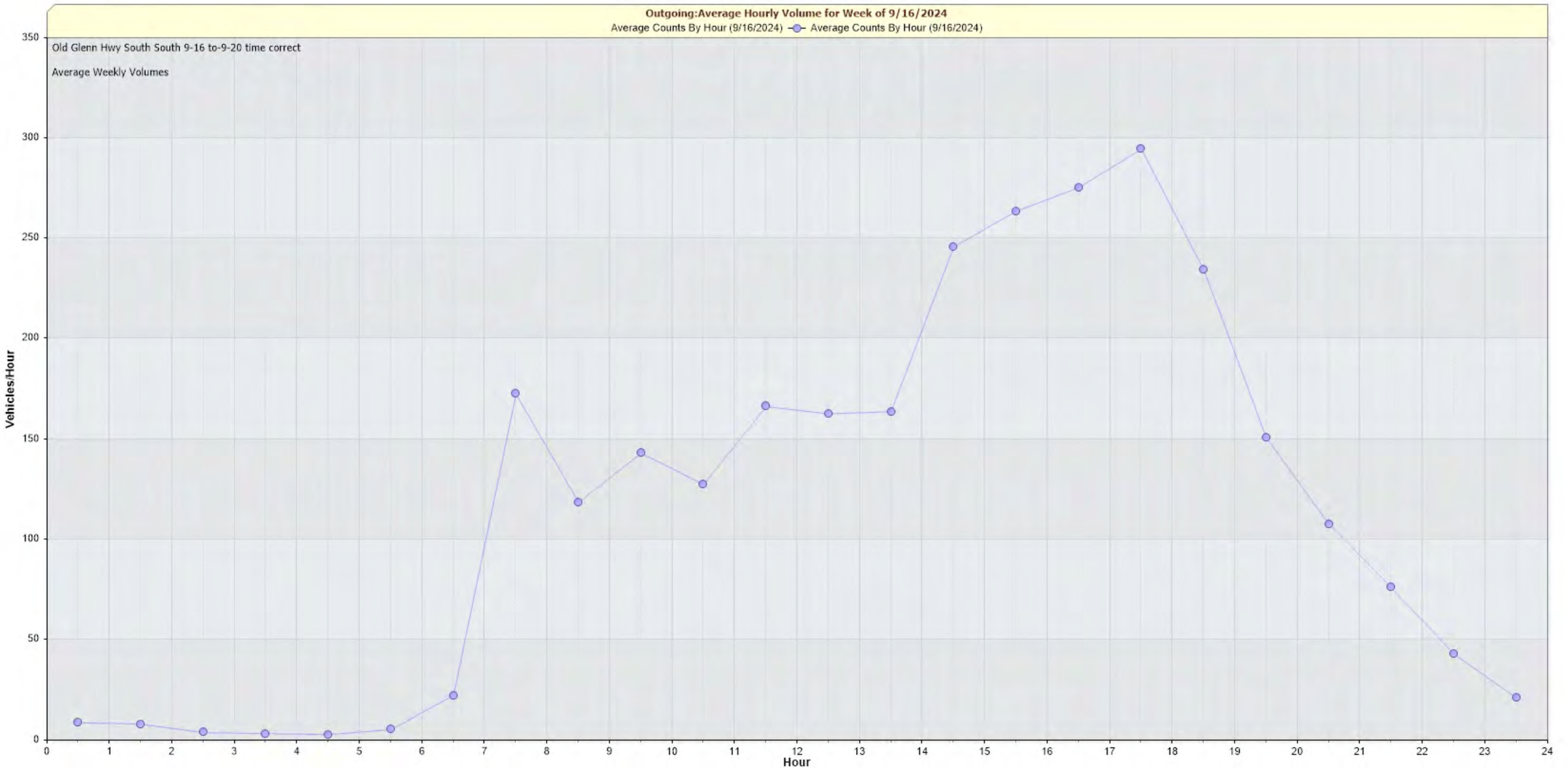
Old Glenn Hwy South South 9-16 to-9-20 time correct

from Mon-Sep-16-2024-01-00-PM to Fri-Sep-20-2024-10-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/20/2024	00:00	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	0	0	3	33	43	20 to 30	66.7	*	*	0	3	0	
9/20/2024	01:00	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	25.2	28	19 to 29	100.0	*	*	0	5	0	
9/20/2024	02:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	35	35	25 to 35	100.0	*	*	0	1	0	
9/20/2024	03:00	0	1	1	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	5	29.8	38	15 to 25	60.0	*	*	0	4	1	
9/20/2024	04:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	23.5	25	15 to 25	100.0	*	*	0	1	1	
9/20/2024	05:00	0	0	1	0	0	1	3	1	0	0	0	0	0	0	0	0	0	0	6	38.2	43	35 to 45	83.3	*	*	0	6	0	
9/20/2024	06:00	0	0	3	3	4	2	5	2	0	0	0	0	0	0	0	0	0	0	19	34.2	42.5	30 to 40	47.4	2	10.5	0	17	2	
9/20/2024	07:00	0	12	64	38	24	21	17	5	0	0	0	0	0	0	0	0	0	0	181	28.5	37.4	19 to 29	59.7	4	2.2	0	170	11	
9/20/2024	08:00	3	15	22	17	13	19	21	7	1	0	0	0	0	0	0	0	0	0	118	30.7	42	19 to 29	39.0	4	3.4	0	107	11	
9/20/2024	09:00	0	5	21	26	22	27	26	8	1	0	0	0	0	0	0	0	0	0	136	33	42	33 to 43	43.4	5	3.7	0	115	21	
9/20/2024	10:00	0	4	13	15	6	14	10	2	1	0	0	0	0	0	0	0	0	0	65	31.2	40	19 to 29	49.2	1	1.5	0	59	6	
9/20/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		3	37	129	104	70	86	84	25	3	0	0	0	0	0	0	0	0	0	541	30.8	41	19 to 29	46.6	16	3.0	0	488	53	

Outgoing: Average Hourly Volume for Week of 9/16/2024
Average Counts By Hour (9/16/2024) ● Average Counts By Hour (9/16/2024)

Old Glenn Hwy South South 9-16 to-9-20 time correct
Average Weekly Volumes



Outgoing:Average Hourly WEEKDAY Speeds for Week of 9/16/2024
Average Hourly WEEKDAY Speeds By Hour (9/16/2024) — Average Hourly WEEKDAY Speeds By Hour (9/16/2024)

Old Glenn Hwy South South 9-16 to-9-20 time correct
Average Weekly Speeds

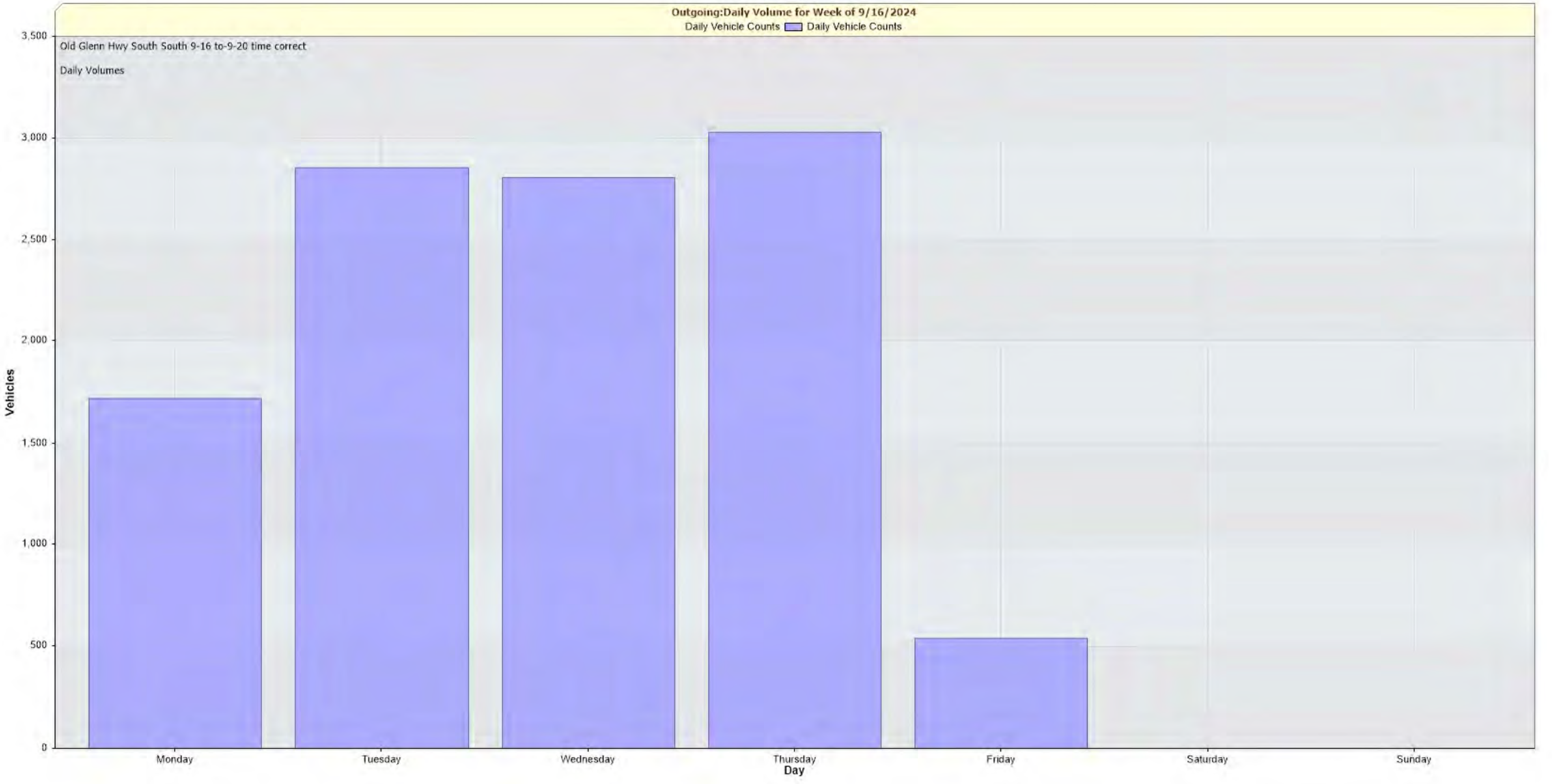


Outgoing: Daily Volume for Week of 9/16/2024

Daily Vehicle Counts

Old Glenn Hwy South South 9-16 to 9-20 time correct

Daily Volumes



For Project: Old Glenn Hwy North North 9-16 to 9-20 time correct

Project Notes:

Location/Name: Incoming

Report Generated: 10/3/2024 3:57:57 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/16/2024 12:00:00 PM through 9/20/2024 9:59:59 AM

85th Percentile Speed 46 MPH

85th Percentile Vehicles 3832

Max Speed 65 MPH on 9/18/2024 7:23:38 PM

Total Vehicles 4508

AADT: 1150

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	901	901
AM Peak 11:00 AM	79	79
PM Peak 5:00 PM	93	93

Speed

Speed Limit: 45

85th Percentile Speed: 46

50th Percentile Speed: 40

10 MPH Pace Interval: 37.0 MPH to 47.0 MPH

Average Speed: 38.49

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	117	203	236	256	39	N/A	N/A
% over limit	17.7	17.7	20.2	20.5	13.6	N/A	N/A
Avg Speeder	48.1	48.2	48.2	48.4	49.2	N/A	N/A
Avg Speed	38.6	38.3	38.5	39.0	37.0	N/A	N/A

Class Counts

	Number	%
VEH_SM	56	1.2
VEH_MED	4203	93.2
VEH_LG	249	5.5
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/16/2024 1:00:00 PM	45.0	38	45	50	48.4	15.6%	39.5
9/16/2024 2:00:00 PM	47.0	70	82	52	47.8	23.2%	40.2
9/16/2024 3:00:00 PM	39.0	58	68	46	46.0	1.5%	28.7
9/16/2024 4:00:00 PM	47.0	76	90	52	48.1	18.9%	39.8
9/16/2024 5:00:00 PM	45.0	80	94	52	47.9	12.8%	39.4
9/16/2024 6:00:00 PM	48.0	75	88	57	49.0	23.9%	40.0
9/16/2024 7:00:00 PM	46.0	58	68	52	47.9	20.6%	39.4
9/16/2024 8:00:00 PM	48.0	46	54	54	48.1	31.5%	41.7
9/16/2024 9:00:00 PM	45.0	28	33	50	48.0	12.1%	38.9
9/16/2024 10:00:00 PM	42.0	24	28	50	47.7	10.7%	37.0
9/16/2024 11:00:00 PM	44.0	8	10	47	47.0	20.0%	41.0
9/17/2024 12:00:00 AM	33.0	1	1	33	0.0	0.0%	33.0
9/17/2024 1:00:00 AM	41.0	4	5	42	0.0	0.0%	36.8
9/17/2024 2:00:00 AM	42.0	3	4	43	0.0	0.0%	39.5
9/17/2024 3:00:00 AM	**No Data**						
9/17/2024 4:00:00 AM	58.0	3	3	58	58.0	33.3%	47.0
9/17/2024 5:00:00 AM	32.0	1	1	32	0.0	0.0%	32.0
9/17/2024 6:00:00 AM	46.0	11	13	49	46.8	30.8%	43.0
9/17/2024 7:00:00 AM	46.0	52	61	54	49.1	19.7%	39.9
9/17/2024 8:00:00 AM	44.0	68	80	52	48.0	12.5%	31.8
9/17/2024 9:00:00 AM	45.0	60	70	53	48.1	12.9%	35.9
9/17/2024 10:00:00 AM	46.0	64	75	50	47.3	16.0%	38.8
9/17/2024 11:00:00 AM	46.0	67	79	53	48.3	16.5%	40.0
9/17/2024 12:00:00 PM	45.0	71	84	63	50.7	15.5%	39.7
9/17/2024 1:00:00 PM	46.0	60	70	55	47.9	22.9%	40.1
9/17/2024 2:00:00 PM	46.0	81	95	52	47.9	20.0%	39.0
9/17/2024 3:00:00 PM	42.0	59	69	49	47.0	8.7%	29.2
9/17/2024 4:00:00 PM	46.0	77	91	55	48.3	16.5%	38.5
9/17/2024 5:00:00 PM	47.0	78	92	57	49.0	17.4%	39.5
9/17/2024 6:00:00 PM	46.0	82	97	55	47.6	28.9%	41.3
9/17/2024 7:00:00 PM	47.0	41	48	52	48.4	18.8%	39.9
9/17/2024 8:00:00 PM	45.0	42	50	50	47.5	16.0%	39.2
9/17/2024 9:00:00 PM	46.0	24	28	52	47.5	21.4%	39.9
9/17/2024 10:00:00 PM	45.0	16	19	48	46.7	15.8%	40.1
9/17/2024 11:00:00 PM	44.0	7	8	52	52.0	12.5%	41.9
9/18/2024 12:00:00 AM	46.0	5	6	49	47.5	33.3%	37.8
9/18/2024 1:00:00 AM	50.0	2	2	50	50.0	50.0%	40.0
9/18/2024 2:00:00 AM	**No Data**						
9/18/2024 3:00:00 AM	42.0	3	4	45	0.0	0.0%	42.5
9/18/2024 4:00:00 AM	60.0	1	1	60	60.0	100.0%	60.0
9/18/2024 5:00:00 AM	40.0	4	5	43	0.0	0.0%	36.4
9/18/2024 6:00:00 AM	47.0	12	14	53	48.5	28.6%	41.9
9/18/2024 7:00:00 AM	47.0	53	62	52	48.6	19.4%	39.1
9/18/2024 8:00:00 AM	44.0	60	70	51	47.4	7.1%	33.1
9/18/2024 9:00:00 AM	45.0	68	80	52	47.2	15.0%	36.2
9/18/2024 10:00:00 AM	46.0	64	75	55	47.2	25.3%	38.8
9/18/2024 11:00:00 AM	48.0	54	63	56	48.8	30.2%	41.5

Incoming Summary
Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

9/18/2024 12:00:00 PM	46.0	61	72	51	47.9	18.1%	38.6
9/18/2024 1:00:00 PM	46.0	63	74	50	47.8	17.6%	39.4
9/18/2024 2:00:00 PM	46.0	78	92	51	47.7	19.6%	39.2
9/18/2024 3:00:00 PM	40.0	71	84	47	47.0	3.6%	28.0
9/18/2024 4:00:00 PM	47.0	71	83	55	49.1	20.5%	39.4
9/18/2024 5:00:00 PM	48.0	78	92	55	48.7	26.1%	41.0
9/18/2024 6:00:00 PM	47.0	76	90	55	48.3	25.6%	40.4
9/18/2024 7:00:00 PM	48.0	56	66	53	48.4	33.3%	42.4
9/18/2024 8:00:00 PM	47.0	50	59	65	49.1	28.8%	41.3
9/18/2024 9:00:00 PM	46.0	31	36	49	47.0	16.7%	38.0
9/18/2024 10:00:00 PM	43.0	17	20	50	48.0	10.0%	41.3
9/18/2024 11:00:00 PM	46.0	12	14	46	46.0	21.4%	38.1
9/19/2024 12:00:00 AM	46.0	8	9	46	46.0	22.2%	37.6
9/19/2024 1:00:00 AM	47.0	5	6	49	48.0	33.3%	39.0
9/19/2024 2:00:00 AM	46.0	3	3	46	46.0	33.3%	27.3
9/19/2024 3:00:00 AM	55.0	2	2	55	55.0	50.0%	45.5
9/19/2024 4:00:00 AM	39.0	3	4	50	50.0	25.0%	38.8
9/19/2024 5:00:00 AM	36.0	3	4	45	0.0	0.0%	35.8
9/19/2024 6:00:00 AM	46.0	13	15	57	50.0	20.0%	43.2
9/19/2024 7:00:00 AM	47.0	44	52	52	47.9	26.4%	40.9
9/19/2024 8:00:00 AM	43.0	53	62	52	48.5	6.5%	32.2
9/19/2024 9:00:00 AM	46.0	66	78	50	47.2	20.5%	36.9
9/19/2024 10:00:00 AM	46.0	71	84	53	48.1	19.0%	39.8
9/19/2024 11:00:00 AM	46.0	56	66	56	49.4	21.2%	40.3
9/19/2024 12:00:00 PM	46.0	69	81	54	48.0	19.8%	40.4
9/19/2024 1:00:00 PM	47.0	84	99	56	48.6	32.3%	41.7
9/19/2024 2:00:00 PM	47.0	72	85	55	48.3	24.7%	41.1
9/19/2024 3:00:00 PM	40.0	67	79	56	48.7	7.6%	29.6
9/19/2024 4:00:00 PM	44.0	86	101	53	48.3	8.8%	37.6
9/19/2024 5:00:00 PM	48.0	73	86	54	48.7	24.4%	41.2
9/19/2024 6:00:00 PM	48.0	85	100	56	48.1	37.0%	42.1
9/19/2024 7:00:00 PM	47.0	85	100	55	48.3	22.0%	40.5
9/19/2024 8:00:00 PM	46.0	50	59	58	49.6	16.9%	38.8
9/19/2024 9:00:00 PM	46.0	33	39	52	47.9	17.9%	39.7
9/19/2024 10:00:00 PM	43.0	21	25	50	50.0	4.0%	36.8
9/19/2024 11:00:00 PM	43.0	10	12	48	48.0	8.3%	36.5
9/20/2024 12:00:00 AM	47.0	3	4	49	48.0	50.0%	42.0
9/20/2024 1:00:00 AM	35.0	4	5	35	0.0	0.0%	33.6
9/20/2024 2:00:00 AM	37.0	2	2	37	0.0	0.0%	37.0
9/20/2024 3:00:00 AM	41.0	1	1	41	0.0	0.0%	41.0
9/20/2024 4:00:00 AM	53.0	1	1	53	53.0	100.0%	53.0
9/20/2024 5:00:00 AM	44.0	3	4	47	47.0	25.0%	42.3
9/20/2024 6:00:00 AM	44.0	12	14	47	47.0	7.1%	41.1
9/20/2024 7:00:00 AM	46.0	48	56	57	49.8	16.1%	39.1
9/20/2024 8:00:00 AM	45.0	61	72	53	49.7	13.9%	33.0
9/20/2024 9:00:00 AM	45.0	68	80	52	48.9	13.8%	37.2
9/20/2024 10:00:00 AM	45.0	43	51	50	48.2	11.8%	38.6

Incoming Summary

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/17/2024 12:00:00 AM	46.0	562	661	57	48.1	17.7%	38.6
9/18/2024 12:00:00 AM	46.0	976	1148	63	48.2	17.7%	38.3
9/19/2024 12:00:00 AM	46.0	992	1167	65	48.2	20.2%	38.5
9/20/2024 12:00:00 AM	46.0	1059	1246	58	48.4	20.5%	39.0
9/20/2024 9:59:59 AM	45.0	243	286	57	49.2	13.6%	37.0

Incoming Weekly Counts

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	5	2	6	5	*	*	4.5	0	43.12
1 - 2	*	4	0	3	2	*	*	2.25	0	41.67
2 - 3	*	0	4	2	1	*	*	1.75	0	46
3 - 4	*	3	1	4	1	*	*	2.25	0	52.5
4 - 5	*	1	5	4	4	*	*	3.5	0	38
5 - 6	*	13	14	15	14	*	*	14	0	45.62
6 - 7	*	61	62	52	56	*	*	57.75	0	46.18
7 - 8	*	80	70	62	72	*	*	71	0	43.58
8 - 9	*	70	80	78	80	*	*	77	0	44.97
9 - 10	*	75	75	84	51	*	*	71.25	0	45.35
10 - 11	*	79	63	66	*	*	*	69.33	0	46.27
11 - 12	*	84	72	81	*	*	*	79	0	45.47
12 - 13	45	70	74	99	*	*	*	72	0	45.9
13 - 14	82	95	92	85	*	*	*	88.5	0	46.18
14 - 15	68	69	84	79	*	*	*	75	0	40.25
15 - 16	90	91	83	101	*	*	*	91.25	0	45.42
16 - 17	94	92	92	86	*	*	*	91	0	46.4
17 - 18	88	97	90	100	*	*	*	93.75	0	46.7
18 - 19	68	48	66	100	*	*	*	70.5	0	46.65
19 - 20	54	50	59	59	*	*	*	55.5	0	46.08
20 - 21	33	28	36	39	*	*	*	34	0	45.25
21 - 22	28	19	20	25	*	*	*	23	0	43.18
22 - 23	10	8	14	12	*	*	*	11	0	44.08
23 - 24	1	6	9	4	*	*	*	5	0	42.88
Totals	661	1148	1167	1246	286	0	0			
% of Total	12.33%	21.42%	21.78%	23.25%	5.34%	0%	0%			

Incoming Monthly Counts
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	5	2	6	5	*	*	4.5	0	43.12
1 - 2	*	4	0	3	2	*	*	2.25	0	41.67
2 - 3	*	0	4	2	1	*	*	1.75	0	46
3 - 4	*	3	1	4	1	*	*	2.25	0	52.5
4 - 5	*	1	5	4	4	*	*	3.5	0	38
5 - 6	*	13	14	15	14	*	*	14	0	45.62
6 - 7	*	61	62	52	56	*	*	57.75	0	46.18
7 - 8	*	80	70	62	72	*	*	71	0	43.58
8 - 9	*	70	80	78	80	*	*	77	0	44.97
9 - 10	*	75	75	84	51	*	*	71.25	0	45.35
10 - 11	*	79	63	66	*	*	*	69.33	0	46.27
11 - 12	*	84	72	81	*	*	*	79	0	45.47
12 - 13	45	70	74	99	*	*	*	72	0	45.9
13 - 14	82	95	92	85	*	*	*	88.5	0	46.18
14 - 15	68	69	84	79	*	*	*	75	0	40.25
15 - 16	90	91	83	101	*	*	*	91.25	0	45.42
16 - 17	94	92	92	86	*	*	*	91	0	46.4
17 - 18	88	97	90	100	*	*	*	93.75	0	46.7
18 - 19	68	48	66	100	*	*	*	70.5	0	46.65
19 - 20	54	50	59	59	*	*	*	55.5	0	46.08
20 - 21	33	28	36	39	*	*	*	34	0	45.25
21 - 22	28	19	20	25	*	*	*	23	0	43.18
22 - 23	10	8	14	12	*	*	*	11	0	44.08
23 - 24	1	6	9	4	*	*	*	5	0	42.88
Totals	661	1148	1167	1246	286	0	0			
% of Total	14.66%	25.47%	25.89%	27.64%	6.34%	0%	0%			

Incoming Weekly Speeds

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	36.8	40	39	33.6	*	*	37	*	43.12
1 - 2	*	39.5	0	27.33	37	*	*	34.89	*	41.67
2 - 3	*	0	42.5	45.5	41	*	*	43.14	*	46
3 - 4	*	47	60	38.75	53	*	*	45.44	*	52.5
4 - 5	*	32	36.4	35.75	42.25	*	*	37.57	*	38
5 - 6	*	43	41.86	43.2	41.07	*	*	42.29	*	45.62
6 - 7	*	39.87	39.08	40.88	39.09	*	*	39.7	*	46.18
7 - 8	*	31.79	33.09	32.23	32.97	*	*	32.5	*	43.58
8 - 9	*	35.93	36.16	36.91	37.19	*	*	36.56	*	44.97
9 - 10	*	38.81	38.83	39.82	38.57	*	*	39.07	*	45.35
10 - 11	*	39.96	41.49	40.35	*	*	*	40.55	*	46.27
11 - 12	*	39.69	38.57	40.36	*	*	*	39.58	*	45.47
12 - 13	39.67	40.13	39.42	41.66	*	*	*	40.4	*	45.9
13 - 14	40.17	39.01	39.22	41.09	*	*	*	39.83	*	46.18
14 - 15	28.69	29.19	28.02	29.59	*	*	*	28.86	*	40.25
15 - 16	39.83	38.45	39.4	37.58	*	*	*	38.77	*	45.42
16 - 17	39.45	39.5	41	41.19	*	*	*	40.26	*	46.4
17 - 18	40.02	41.28	40.41	42.1	*	*	*	40.99	*	46.7
18 - 19	39.41	39.88	42.44	40.48	*	*	*	40.58	*	46.65
19 - 20	41.69	39.24	41.29	38.85	*	*	*	40.27	*	46.08
20 - 21	38.94	39.93	37.97	39.74	*	*	*	39.12	*	45.25
21 - 22	36.96	40.05	41.25	36.76	*	*	*	38.48	*	43.18
22 - 23	41	41.88	38.14	36.5	*	*	*	39.02	*	44.08
23 - 24	33	37.83	37.56	42	*	*	*	38.3	*	42.88
Totals	38.6	38.3	38.4	39	37	0	0			
% of Total	20.18%	20.02%	20.07%	20.39%	19.34%	0%	0%			

Incoming Monthly Speeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	36.8	40	39	33.6	*	*	37	*	43.12
1 - 2	*	39.5	0	27.33	37	*	*	34.89	*	41.67
2 - 3	*	0	42.5	45.5	41	*	*	43.14	*	46
3 - 4	*	47	60	38.75	53	*	*	45.44	*	52.5
4 - 5	*	32	36.4	35.75	42.25	*	*	37.57	*	38
5 - 6	*	43	41.86	43.2	41.07	*	*	42.29	*	45.62
6 - 7	*	39.87	39.08	40.88	39.09	*	*	39.7	*	46.18
7 - 8	*	31.79	33.09	32.23	32.97	*	*	32.5	*	43.58
8 - 9	*	35.93	36.16	36.91	37.19	*	*	36.56	*	44.97
9 - 10	*	38.81	38.83	39.82	38.57	*	*	39.07	*	45.35
10 - 11	*	39.96	41.49	40.35	*	*	*	40.55	*	46.27
11 - 12	*	39.69	38.57	40.36	*	*	*	39.58	*	45.47
12 - 13	39.67	40.13	39.42	41.66	*	*	*	40.4	*	45.9
13 - 14	40.17	39.01	39.22	41.09	*	*	*	39.83	*	46.18
14 - 15	28.69	29.19	28.02	29.59	*	*	*	28.86	*	40.25
15 - 16	39.83	38.45	39.4	37.58	*	*	*	38.77	*	45.42
16 - 17	39.45	39.5	41	41.19	*	*	*	40.26	*	46.4
17 - 18	40.02	41.28	40.41	42.1	*	*	*	40.99	*	46.7
18 - 19	39.41	39.88	42.44	40.48	*	*	*	40.58	*	46.65
19 - 20	41.69	39.24	41.29	38.85	*	*	*	40.27	*	46.08
20 - 21	38.94	39.93	37.97	39.74	*	*	*	39.12	*	45.25
21 - 22	36.96	40.05	41.25	36.76	*	*	*	38.48	*	43.18
22 - 23	41	41.88	38.14	36.5	*	*	*	39.02	*	44.08
23 - 24	33	37.83	37.56	42	*	*	*	38.3	*	42.88
Totals	38.6	38.3	38.4	39	37	0	0			
% of Total	20.18%	20.02%	20.07%	20.39%	19.34%	0%	0%			

Incoming Weekly EightyFifthSpeeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	41	50	47	34.5	*	*	43.12	0	43.12
1 - 2	*	42	0	46	37	*	*	41.67	0	41.67
2 - 3	*	0	42	55	41	*	*	46	0	46
3 - 4	*	58	60	39	53	*	*	52.5	0	52.5
4 - 5	*	32	40	36	44	*	*	38	0	38
5 - 6	*	45.7	47	46	43.8	*	*	45.62	0	45.62
6 - 7	*	45.8	46.7	46.2	46	*	*	46.18	0	46.18
7 - 8	*	44	43.4	42.4	44.5	*	*	43.58	0	43.58
8 - 9	*	44.5	45	45.7	44.7	*	*	44.97	0	44.97
9 - 10	*	45.2	45.8	45.8	44.6	*	*	45.35	0	45.35
10 - 11	*	45.3	47.5	46	*	*	*	46.27	0	46.27
11 - 12	*	45	45.7	45.7	*	*	*	45.47	0	45.47
12 - 13	45	45.9	45.7	47	*	*	*	45.9	0	45.9
13 - 14	46.4	46	45.8	46.5	*	*	*	46.18	0	46.18
14 - 15	39	42	40	40	*	*	*	40.25	0	40.25
15 - 16	46.2	45.2	47	43.3	*	*	*	45.42	0	45.42
16 - 17	44.6	46.2	47.5	47.3	*	*	*	46.4	0	46.4
17 - 18	47.2	45.9	46.4	47.3	*	*	*	46.7	0	46.7
18 - 19	46	46.5	47.7	46.4	*	*	*	46.65	0	46.65
19 - 20	47.2	45	46.8	45.3	*	*	*	46.08	0	46.08
20 - 21	44.5	45.7	45.5	45.3	*	*	*	45.25	0	45.25
21 - 22	42	45	43	42.7	*	*	*	43.18	0	43.18
22 - 23	44	44	45.3	43	*	*	*	44.08	0	44.08
23 - 24	33	46	45.5	47	*	*	*	42.88	0	42.88
Totals	525.1	1031.9	1059.3	1081.9	433.1	0	0			
% of Total	12.71%	24.98%	25.64%	26.19%	10.48%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	41	50	47	34.5	*	*	43.12	0	43.12
1 - 2	*	42	0	46	37	*	*	41.67	0	41.67
2 - 3	*	0	42	55	41	*	*	46	0	46
3 - 4	*	58	60	39	53	*	*	52.5	0	52.5
4 - 5	*	32	40	36	44	*	*	38	0	38
5 - 6	*	45.7	47	46	43.8	*	*	45.62	0	45.62
6 - 7	*	45.8	46.7	46.2	46	*	*	46.18	0	46.18
7 - 8	*	44	43.4	42.4	44.5	*	*	43.58	0	43.58
8 - 9	*	44.5	45	45.7	44.7	*	*	44.97	0	44.97
9 - 10	*	45.2	45.8	45.8	44.6	*	*	45.35	0	45.35
10 - 11	*	45.3	47.5	46	*	*	*	46.27	0	46.27
11 - 12	*	45	45.7	45.7	*	*	*	45.47	0	45.47
12 - 13	45	45.9	45.7	47	*	*	*	45.9	0	45.9
13 - 14	46.4	46	45.8	46.5	*	*	*	46.18	0	46.18
14 - 15	39	42	40	40	*	*	*	40.25	0	40.25
15 - 16	46.2	45.2	47	43.3	*	*	*	45.42	0	45.42
16 - 17	44.6	46.2	47.5	47.3	*	*	*	46.4	0	46.4
17 - 18	47.2	45.9	46.4	47.3	*	*	*	46.7	0	46.7
18 - 19	46	46.5	47.7	46.4	*	*	*	46.65	0	46.65
19 - 20	47.2	45	46.8	45.3	*	*	*	46.08	0	46.08
20 - 21	44.5	45.7	45.5	45.3	*	*	*	45.25	0	45.25
21 - 22	42	45	43	42.7	*	*	*	43.18	0	43.18
22 - 23	44	44	45.3	43	*	*	*	44.08	0	44.08
23 - 24	33	46	45.5	47	*	*	*	42.88	0	42.88

Summary of Violators

Old Glenn Hwy North North 9-16 to 9-20 time correct from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	18	37.0	3	48.7
01:00:00	9	34.9	1	46.0
02:00:00	7	43.1	1	55.0
03:00:00	9	45.4	4	55.3
04:00:00	14	37.6	1	47.0
05:00:00	56	42.3	12	48.2
06:00:00	231	39.7	46	48.8
07:00:00	284	32.5	29	48.6
08:00:00	308	36.6	48	47.8
09:00:00	285	39.1	53	47.6
10:00:00	208	40.5	46	48.8
11:00:00	237	39.6	42	48.8
12:00:00	288	40.4	68	48.3
13:00:00	354	39.8	77	47.9
14:00:00	300	28.9	16	47.6
15:00:00	365	38.8	58	48.5
16:00:00	364	40.3	73	48.6
17:00:00	375	41.0	109	48.2
18:00:00	282	40.6	67	48.3
19:00:00	222	40.3	52	48.6
20:00:00	136	39.1	23	47.6
21:00:00	92	38.5	9	47.7
22:00:00	44	39.0	7	47.4
23:00:00	20	38.3	6	47.2

Incoming Histogram
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/16/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	2	0	0	1	2	16	12	10	2	0	0	0	0	0	0	0	0	0	45	39.7	45	35 to 45	73.3	7	15.6	0	45	0
9/16/2024	13:00	0	1	0	2	13	21	18	23	4	0	0	0	0	0	0	0	0	0	82	40.2	46.4	35 to 45	57.3	19	23.2	2	72	8
9/16/2024	14:00	1	4	25	11	8	9	9	1	0	0	0	0	0	0	0	0	0	0	68	28.7	39	19 to 29	55.9	1	1.5	3	60	5
9/16/2024	15:00	1	0	2	3	14	13	35	19	3	0	0	0	0	0	0	0	0	0	90	39.8	46.2	38 to 48	64.4	17	18.9	2	82	6
9/16/2024	16:00	0	0	1	5	11	26	34	15	2	0	0	0	0	0	0	0	0	0	94	39.4	44.6	34 to 44	70.2	12	12.8	2	89	3
9/16/2024	17:00	0	0	0	4	15	25	19	17	7	1	0	0	0	0	0	0	0	0	88	40	47.2	36 to 46	56.8	21	23.9	1	84	3
9/16/2024	18:00	1	2	1	1	8	18	17	16	4	0	0	0	0	0	0	0	0	0	68	39.4	46	37 to 47	64.7	14	20.6	1	65	2
9/16/2024	19:00	0	0	0	2	4	11	19	15	3	0	0	0	0	0	0	0	0	0	54	41.7	47.2	38 to 48	64.8	17	31.5	0	52	2
9/16/2024	20:00	0	1	0	4	1	8	13	5	1	0	0	0	0	0	0	0	0	0	33	38.9	44.5	36 to 46	72.7	4	12.1	0	32	1
9/16/2024	21:00	1	1	0	1	4	9	9	2	1	0	0	0	0	0	0	0	0	0	28	37	42	32 to 42	71.4	3	10.7	0	27	1
9/16/2024	22:00	0	0	0	0	0	5	3	2	0	0	0	0	0	0	0	0	0	0	10	41	44	37 to 47	90.0	2	20.0	0	10	0
9/16/2024	23:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	33	33	23 to 33	100.0	*	*	0	1	0
24 Hr Summary		6	9	29	34	81	161	188	125	27	1	0	0	0	0	0	0	0	0	661	38.6	46	35 to 45	58.2	117	17.7	11	619	31

Incoming Histogram

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/17/2024	00:00	0	0	1	0	0	1	3	0	0	0	0	0	0	0	0	0	0	0	5	36.8	41	32 to 42	80.0	*	*	0	5	0	
9/17/2024	01:00	0	0	0	0	1	0	3	0	0	0	0	0	0	0	0	0	0	0	4	39.5	42	33 to 43	100.0	*	*	0	4	0	
9/17/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/17/2024	03:00	0	0	0	0	0	0	2	0	0	1	0	0	0	0	0	0	0	0	3	47	58	33 to 43	66.7	1	33.3	0	3	0	
9/17/2024	04:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	32	32	22 to 32	100.0	*	*	0	1	0	
9/17/2024	05:00	0	0	0	0	1	1	7	4	0	0	0	0	0	0	0	0	0	0	13	43	45.7	39 to 49	92.3	4	30.8	0	12	1	
9/17/2024	06:00	1	1	1	1	7	15	17	13	5	0	0	0	0	0	0	0	0	0	61	39.9	45.8	36 to 46	63.9	12	19.7	0	60	1	
9/17/2024	07:00	2	6	24	6	7	8	15	10	2	0	0	0	0	0	0	0	0	0	80	31.8	44	17 to 27	42.5	10	12.5	0	74	6	
9/17/2024	08:00	0	4	4	10	9	13	19	10	1	0	0	0	0	0	0	0	0	0	70	35.9	44.5	38 to 48	51.4	9	12.9	0	63	7	
9/17/2024	09:00	1	1	0	5	12	15	26	14	1	0	0	0	0	0	0	0	0	0	75	38.8	45.2	36 to 46	62.7	12	16.0	1	68	6	
9/17/2024	10:00	0	1	2	2	5	23	27	15	4	0	0	0	0	0	0	0	0	0	79	40	45.3	35 to 45	70.9	13	16.5	1	70	8	
9/17/2024	11:00	1	4	0	2	9	18	26	19	2	2	1	0	0	0	0	0	0	0	84	39.7	45	35 to 45	65.5	13	15.5	2	80	2	
9/17/2024	12:00	1	2	0	2	8	12	24	18	2	1	0	0	0	0	0	0	0	0	70	40.1	45.9	37 to 47	72.9	16	22.9	1	67	2	
9/17/2024	13:00	2	2	0	3	15	23	26	20	4	0	0	0	0	0	0	0	0	0	95	39	46	37 to 47	60.0	19	20.0	1	90	4	
9/17/2024	14:00	1	6	21	14	8	5	8	6	0	0	0	0	0	0	0	0	0	0	69	29.2	42	18 to 28	53.6	6	8.7	2	61	6	
9/17/2024	15:00	2	1	3	5	13	18	28	17	3	1	0	0	0	0	0	0	0	0	91	38.5	45.2	37 to 47	62.6	15	16.5	3	79	9	
9/17/2024	16:00	1	1	0	2	18	19	29	16	5	1	0	0	0	0	0	0	0	0	92	39.5	46.2	37 to 47	59.8	16	17.4	0	85	7	
9/17/2024	17:00	2	1	0	0	9	15	36	30	3	1	0	0	0	0	0	0	0	0	97	41.3	45.9	37 to 47	73.2	28	28.9	2	90	5	
9/17/2024	18:00	1	0	0	3	5	10	19	8	2	0	0	0	0	0	0	0	0	0	48	39.9	46.5	38 to 48	64.6	9	18.8	0	46	2	
9/17/2024	19:00	0	0	0	3	6	18	13	9	1	0	0	0	0	0	0	0	0	0	50	39.2	45	36 to 46	72.0	8	16.0	1	48	1	
9/17/2024	20:00	0	0	0	3	2	6	11	5	1	0	0	0	0	0	0	0	0	0	28	39.9	45.7	36 to 46	71.4	6	21.4	0	28	0	
9/17/2024	21:00	0	0	0	1	1	5	8	4	0	0	0	0	0	0	0	0	0	0	19	40.1	45	36 to 46	84.2	3	15.8	0	18	1	
9/17/2024	22:00	0	0	0	1	0	1	5	0	1	0	0	0	0	0	0	0	0	0	8	41.9	44	34 to 44	75.0	1	12.5	0	6	2	
9/17/2024	23:00	0	0	1	0	0	3	0	2	0	0	0	0	0	0	0	0	0	0	6	37.8	46	29 to 39	50.0	2	33.3	0	6	0	
24 Hr Summary		15	30	57	63	137	229	352	220	37	7	1	0	0	0	0	0	0	0	1148	38.3	46	37 to 47	58.8	203	17.7	14	1064	70	

Incoming Histogram

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/18/2024	00:00	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	2	40	50	20 to 30	50.0	1	50.0	0	2	0
9/18/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/18/2024	02:00	0	0	0	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	4	42.5	42	35 to 45	100.0	*	*	0	4	0
9/18/2024	03:00	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	60	60	50 to 60	100.0	1	100.0	0	1	0
9/18/2024	04:00	0	0	0	1	0	2	2	0	0	0	0	0	0	0	0	0	0	0	5	36.4	40	33 to 43	80.0	*	*	0	4	1
9/18/2024	05:00	0	0	0	1	0	3	6	3	1	0	0	0	0	0	0	0	0	0	14	41.9	47	38 to 48	71.4	4	28.6	0	14	0
9/18/2024	06:00	1	2	1	3	7	13	22	10	3	0	0	0	0	0	0	0	0	0	62	39.1	46.7	34 to 44	59.7	12	19.4	0	58	4
9/18/2024	07:00	1	3	11	15	7	8	18	6	1	0	0	0	0	0	0	0	0	0	70	33.1	43.4	21 to 31	42.9	5	7.1	0	66	4
9/18/2024	08:00	0	3	11	2	9	24	16	14	1	0	0	0	0	0	0	0	0	0	80	36.2	45	33 to 43	55.0	12	15.0	0	78	2
9/18/2024	09:00	1	2	1	5	6	18	23	18	0	1	0	0	0	0	0	0	0	0	75	38.8	45.8	37 to 47	64.0	19	25.3	0	70	5
9/18/2024	10:00	0	0	0	2	8	13	18	17	3	2	0	0	0	0	0	0	0	0	63	41.5	47.5	38 to 48	66.7	19	30.2	0	60	3
9/18/2024	11:00	1	0	1	4	16	16	16	15	3	0	0	0	0	0	0	0	0	0	72	38.6	45.7	33 to 43	58.3	13	18.1	1	63	8
9/18/2024	12:00	1	1	0	2	11	19	22	17	1	0	0	0	0	0	0	0	0	0	74	39.4	45.7	35 to 45	62.2	13	17.6	0	69	5
9/18/2024	13:00	1	2	1	3	12	20	32	18	3	0	0	0	0	0	0	0	0	0	92	39.2	45.8	36 to 46	63.0	18	19.6	1	81	10
9/18/2024	14:00	3	9	34	7	5	11	11	4	0	0	0	0	0	0	0	0	0	0	84	28	40	16 to 26	56.0	3	3.6	3	74	7
9/18/2024	15:00	4	1	0	2	11	17	25	16	6	1	0	0	0	0	0	0	0	0	83	39.4	47	35 to 45	57.8	17	20.5	2	76	5
9/18/2024	16:00	1	1	0	3	7	21	30	22	6	1	0	0	0	0	0	0	0	0	92	41	47.5	36 to 46	65.2	24	26.1	1	84	7
9/18/2024	17:00	0	2	0	4	9	23	25	22	3	2	0	0	0	0	0	0	0	0	90	40.4	46.4	37 to 47	61.1	23	25.6	1	86	3
9/18/2024	18:00	0	0	0	4	3	13	19	21	6	0	0	0	0	0	0	0	0	0	66	42.4	47.7	38 to 48	69.7	22	33.3	1	58	7
9/18/2024	19:00	0	0	1	2	4	15	20	11	5	0	0	1	0	0	0	0	0	0	59	41.3	46.8	37 to 47	67.8	17	28.8	0	58	1
9/18/2024	20:00	1	0	0	4	5	11	7	8	0	0	0	0	0	0	0	0	0	0	36	38	45.5	35 to 45	55.6	6	16.7	0	36	0
9/18/2024	21:00	0	0	0	0	1	5	12	1	1	0	0	0	0	0	0	0	0	0	20	41.3	43	34 to 44	90.0	2	10.0	0	19	1
9/18/2024	22:00	0	0	1	2	2	0	5	4	0	0	0	0	0	0	0	0	0	0	14	38.1	45.3	36 to 46	64.3	3	21.4	0	14	0
9/18/2024	23:00	0	1	0	0	2	2	0	4	0	0	0	0	0	0	0	0	0	0	9	37.6	45.5	36 to 46	66.7	2	22.2	0	8	1
24 Hr Summary		15	27	62	66	126	254	332	232	44	7	1	1	0	0	0	0	0	0	1167	38.5	46	37 to 47	56.0	236	20.2	10	1083	74

Incoming Histogram

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/19/2024	00:00	0	0	0	0	2	2	0	2	0	0	0	0	0	0	0	0	0	0	6	39	47	29 to 39	66.7	2	33.3	0	6	0
9/19/2024	01:00	1	0	1	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	3	27.3	46	2 to 12	33.3	1	33.3	0	3	0
9/19/2024	02:00	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	2	45.5	55	26 to 36	50.0	1	50.0	0	2	0
9/19/2024	03:00	0	0	0	0	1	2	0	0	1	0	0	0	0	0	0	0	0	0	4	38.8	39	29 to 39	75.0	1	25.0	0	3	1
9/19/2024	04:00	0	0	0	1	0	2	0	1	0	0	0	0	0	0	0	0	0	0	4	35.8	36	26 to 36	75.0	*	*	0	4	0
9/19/2024	05:00	0	0	0	0	1	2	7	4	0	1	0	0	0	0	0	0	0	0	15	43.2	46	37 to 47	86.7	3	20.0	0	15	0
9/19/2024	06:00	0	0	0	0	7	16	13	13	3	0	0	0	0	0	0	0	0	0	52	40.9	46.2	37 to 47	67.3	13	25.0	1	49	2
9/19/2024	07:00	1	2	16	8	7	10	13	3	2	0	0	0	0	0	0	0	0	0	62	32.2	42.4	21 to 31	45.2	4	6.5	1	59	2
9/19/2024	08:00	2	2	11	5	7	6	22	21	2	0	0	0	0	0	0	0	0	0	78	36.9	45.7	37 to 47	59.0	16	20.5	1	72	5
9/19/2024	09:00	1	2	0	4	4	20	35	15	3	0	0	0	0	0	0	0	0	0	84	39.8	45.8	37 to 47	70.2	16	19.0	1	79	4
9/19/2024	10:00	1	1	0	1	10	14	23	10	5	1	0	0	0	0	0	0	0	0	66	40.3	46	34 to 44	65.2	14	21.2	4	57	5
9/19/2024	11:00	0	2	0	2	7	22	29	15	4	0	0	0	0	0	0	0	0	0	81	40.4	45.7	36 to 46	72.8	16	19.8	1	76	4
9/19/2024	12:00	0	1	1	3	8	21	27	27	10	1	0	0	0	0	0	0	0	0	99	41.7	47	37 to 47	64.6	32	32.3	1	88	10
9/19/2024	13:00	1	0	0	4	6	17	32	19	5	1	0	0	0	0	0	0	0	0	85	41.1	46.5	37 to 47	64.7	21	24.7	0	80	5
9/19/2024	14:00	1	5	28	11	11	9	7	5	1	1	0	0	0	0	0	0	0	0	79	29.6	40	21 to 31	54.4	6	7.6	3	71	5
9/19/2024	15:00	1	2	0	7	16	36	28	9	2	0	0	0	0	0	0	0	0	0	101	37.6	43.3	33 to 43	67.3	9	8.9	2	93	6
9/19/2024	16:00	1	0	0	0	10	25	22	22	6	0	0	0	0	0	0	0	0	0	86	41.2	47.3	37 to 47	64.0	21	24.4	0	85	1
9/19/2024	17:00	2	0	1	3	4	18	29	33	9	1	0	0	0	0	0	0	0	0	100	42.1	47.3	37 to 47	67.0	37	37.0	1	95	4
9/19/2024	18:00	2	1	0	2	8	25	35	22	4	1	0	0	0	0	0	0	0	0	100	40.5	46.4	37 to 47	67.0	22	22.0	1	98	1
9/19/2024	19:00	2	0	1	2	11	9	22	9	1	2	0	0	0	0	0	0	0	0	59	38.8	45.3	34 to 44	55.9	10	16.9	0	59	0
9/19/2024	20:00	1	1	0	0	6	8	9	13	1	0	0	0	0	0	0	0	0	0	39	39.7	45.3	36 to 46	66.7	7	17.9	0	38	1
9/19/2024	21:00	1	0	0	2	6	4	9	2	1	0	0	0	0	0	0	0	0	0	25	36.8	42.7	33 to 43	60.0	1	4.0	0	23	2
9/19/2024	22:00	0	0	1	2	1	2	5	1	0	0	0	0	0	0	0	0	0	0	12	36.5	43	34 to 44	58.3	1	8.3	0	12	0
9/19/2024	23:00	0	0	0	0	1	1	0	2	0	0	0	0	0	0	0	0	0	0	4	42	47	39 to 49	75.0	2	50.0	0	4	0
24 Hr Summary		18	19	60	57	134	272	367	249	60	10	0	0	0	0	0	0	0	0	1246	39	46	37 to 47	59.2	256	20.5	17	1171	58

Incoming Histogram
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/20/2024	00:00	0	0	0	0	3	2	0	0	0	0	0	0	0	0	0	0	0	0	5	33.6	34.5	25 to 35	100.0	*	*	0	5	0	
9/20/2024	01:00	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	2	37	37	27 to 37	100.0	*	*	0	2	0	
9/20/2024	02:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	41	41	31 to 41	100.0	*	*	0	1	0	
9/20/2024	03:00	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	1	53	53	43 to 53	100.0	1	100.0	0	1	0	
9/20/2024	04:00	0	0	0	0	1	0	2	1	0	0	0	0	0	0	0	0	0	0	4	42.3	44	34 to 44	75.0	1	25.0	0	4	0	
9/20/2024	05:00	0	0	0	0	1	3	9	1	0	0	0	0	0	0	0	0	0	0	14	41.1	43.8	34 to 44	92.9	1	7.1	0	12	2	
9/20/2024	06:00	1	1	0	2	9	16	16	7	3	1	0	0	0	0	0	0	0	0	56	39.1	46	35 to 45	60.7	9	16.1	1	52	3	
9/20/2024	07:00	1	2	16	14	8	7	12	7	5	0	0	0	0	0	0	0	0	0	72	33	44.5	20 to 30	43.1	10	13.9	1	69	2	
9/20/2024	08:00	2	1	4	8	8	18	25	9	5	0	0	0	0	0	0	0	0	0	80	37.2	44.7	34 to 44	57.5	11	13.8	2	73	5	
9/20/2024	09:00	1	2	0	1	9	10	17	9	2	0	0	0	0	0	0	0	0	0	51	38.6	44.6	35 to 45	62.7	6	11.8	0	47	4	
9/20/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		5	6	20	25	39	58	82	34	16	1	0	0	0	0	0	0	0	0	286	37	45	35 to 45	53.1	39	13.6	4	266	16	

Incoming: Average Hourly Volume for Week of 9/16/2024

Average Counts By Hour (9/16/2024) ● Average Counts By Hour (9/16/2024)

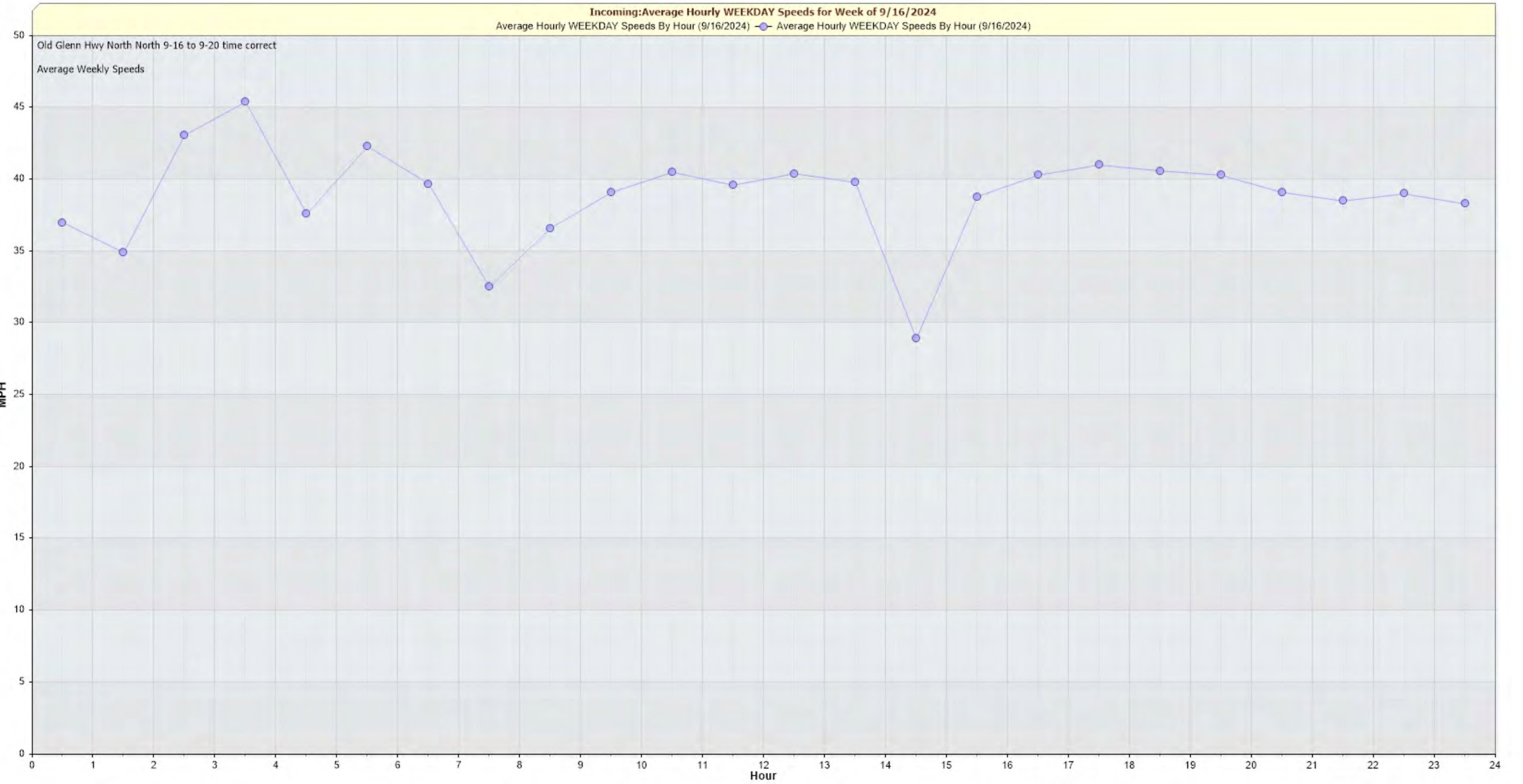
Old Glenn Hwy North North 9-16 to 9-20 time correct

Average Weekly Volumes



Incoming: Average Hourly WEEKDAY Speeds for Week of 9/16/2024
Average Hourly WEEKDAY Speeds By Hour (9/16/2024) — Average Hourly WEEKDAY Speeds By Hour (9/16/2024)

Old Glenn Hwy North North 9-16 to 9-20 time correct
Average Weekly Speeds

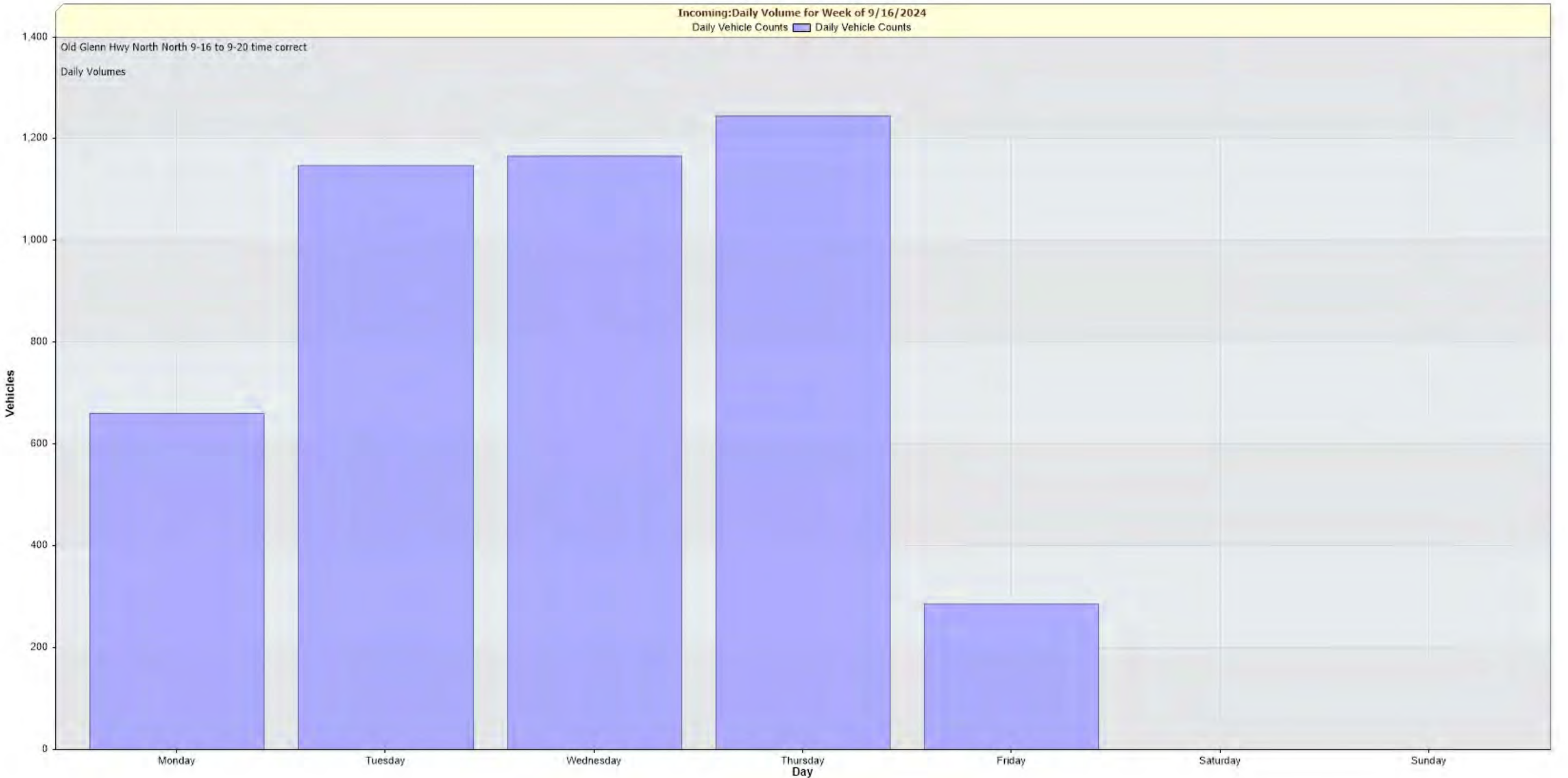


Incoming: Daily Volume for Week of 9/16/2024

Daily Vehicle Counts

Old Glenn Hwy North North 9-16 to 9-20 time correct

Daily Volumes



For Project: Old Glenn Hwy North North 9-16 to 9-20 time correct

Project Notes:

Location/Name: Outgoing

Report Generated: 10/3/2024 3:57:57 PM

Speed Intervals 1 MPH

Time Intervals Instant

Traffic Report From 9/16/2024 12:00:00 PM through 9/20/2024 9:59:59 AM

85th Percentile Speed 44 MPH

85th Percentile Vehicles 5601

Max Speed 58 MPH on 9/18/2024 8:15:43 AM

Total Vehicles 6589

AADT: 1682

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1317	1317
AM Peak	7:00 AM 125	125
PM Peak	5:00 PM 205	205

Speed

Speed Limit: 45

85th Percentile Speed: 44

50th Percentile Speed: 37

10 MPH Pace Interval: 36.0 MPH to 46.0 MPH

Average Speed: 35.35

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	72	139	133	130	8	N/A	N/A
% over limit	6.3	8.2	8.0	7.1	3.0	N/A	N/A
Avg Speeder	47.2	47.6	47.7	47.7	48.0	N/A	N/A
Avg Speed	35.4	35.7	35.6	35.4	31.0	N/A	N/A

Class Counts

	Number	%
VEH_SM	1	0
VEH_MED	6246	94.8
VEH_LG	342	5.2
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/16/2024 1:00:00 PM	41.0	48	57	44	0.0	0.0%	33.5
9/16/2024 2:00:00 PM	43.0	98	115	49	47.3	6.1%	36.1
9/16/2024 3:00:00 PM	37.0	132	155	47	46.5	1.3%	27.9
9/16/2024 4:00:00 PM	44.0	144	170	49	46.7	7.1%	35.6
9/16/2024 5:00:00 PM	44.0	144	169	56	47.5	8.9%	37.0
9/16/2024 6:00:00 PM	45.0	155	182	52	47.6	8.2%	38.5
9/16/2024 7:00:00 PM	44.0	99	117	49	46.7	8.5%	37.0
9/16/2024 8:00:00 PM	45.0	54	63	50	47.4	14.3%	38.3
9/16/2024 9:00:00 PM	42.0	38	45	45	0.0	0.0%	34.7
9/16/2024 10:00:00 PM	43.0	37	43	47	46.5	4.7%	35.0
9/16/2024 11:00:00 PM	42.0	14	17	43	0.0	0.0%	34.9
9/17/2024 12:00:00 AM	42.0	11	13	45	0.0	0.0%	32.9
9/17/2024 1:00:00 AM	45.0	6	7	54	54.0	14.3%	34.4
9/17/2024 2:00:00 AM	44.0	5	6	49	49.0	16.7%	39.7
9/17/2024 3:00:00 AM	48.0	2	2	48	48.0	50.0%	42.0
9/17/2024 4:00:00 AM	**No Data**						
9/17/2024 5:00:00 AM	44.0	1	1	44	0.0	0.0%	44.0
9/17/2024 6:00:00 AM	48.0	3	3	48	48.0	33.3%	45.0
9/17/2024 7:00:00 AM	43.0	12	14	46	46.0	6.7%	34.9
9/17/2024 8:00:00 AM	38.0	102	120	50	50.0	0.8%	27.7
9/17/2024 9:00:00 AM	43.0	48	56	51	49.0	3.6%	33.7
9/17/2024 10:00:00 AM	44.0	57	67	52	48.7	10.4%	37.1
9/17/2024 11:00:00 AM	44.0	54	64	53	47.8	9.4%	36.9
9/17/2024 12:00:00 PM	44.0	76	90	55	48.6	7.8%	35.4
9/17/2024 1:00:00 PM	45.0	90	106	50	47.5	14.2%	37.9
9/17/2024 2:00:00 PM	43.0	88	104	50	47.3	8.7%	36.1
9/17/2024 3:00:00 PM	37.0	127	149	48	47.0	1.3%	28.0
9/17/2024 4:00:00 PM	43.0	141	166	56	48.2	7.8%	36.5
9/17/2024 5:00:00 PM	43.0	157	185	50	47.5	5.4%	36.7
9/17/2024 6:00:00 PM	45.0	192	226	54	47.4	11.1%	38.3
9/17/2024 7:00:00 PM	45.0	138	162	53	46.7	14.8%	38.6
9/17/2024 8:00:00 PM	43.0	67	79	50	47.7	3.8%	38.0
9/17/2024 9:00:00 PM	43.0	38	45	49	48.7	6.7%	37.4
9/17/2024 10:00:00 PM	43.0	29	34	50	48.7	8.8%	37.6
9/17/2024 11:00:00 PM	46.0	8	10	47	46.3	40.0%	42.7
9/18/2024 12:00:00 AM	45.0	4	5	45	0.0	0.0%	38.8
9/18/2024 1:00:00 AM	35.0	5	6	36	0.0	0.0%	28.7
9/18/2024 2:00:00 AM	44.0	3	4	47	47.0	25.0%	39.3
9/18/2024 3:00:00 AM	**No Data**						
9/18/2024 4:00:00 AM	**No Data**						
						
9/18/2024 6:00:00 AM	44.0	3	3	44	0.0	0.0%	41.0
9/18/2024 7:00:00 AM	44.0	11	13	44	0.0	0.0%	37.9
9/18/2024 8:00:00 AM	34.0	100	118	48	48.0	0.8%	26.7
9/18/2024 9:00:00 AM	44.0	48	57	58	50.0	10.5%	35.0
9/18/2024 10:00:00 AM	43.0	48	57	45	0.0	0.0%	36.0
9/18/2024 11:00:00 AM	45.0	48	57	50	47.4	12.3%	38.0

Outgoing Summary
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

9/18/2024 12:00:00 PM	44.0	77	91	51	49.0	8.8%	38.0
9/18/2024 1:00:00 PM	45.0	72	85	48	46.6	10.6%	38.2
9/18/2024 2:00:00 PM	43.0	78	92	54	47.8	13.0%	36.4
9/18/2024 3:00:00 PM	39.0	139	163	51	51.0	0.6%	28.2
9/18/2024 4:00:00 PM	45.0	146	172	51	47.6	11.6%	37.8
9/18/2024 5:00:00 PM	44.0	158	186	58	47.6	7.0%	37.5
9/18/2024 6:00:00 PM	45.0	161	189	51	47.0	11.1%	37.7
9/18/2024 7:00:00 PM	45.0	104	122	50	47.3	12.3%	37.6
9/18/2024 8:00:00 PM	43.0	69	81	53	48.0	11.1%	35.9
9/18/2024 9:00:00 PM	44.0	67	79	53	48.1	8.9%	37.3
9/18/2024 10:00:00 PM	42.0	40	47	48	48.0	2.1%	35.0
9/18/2024 11:00:00 PM	45.0	20	23	49	48.0	8.7%	39.9
9/19/2024 12:00:00 AM	45.0	7	8	45	0.0	0.0%	38.0
9/19/2024 1:00:00 AM	43.0	3	3	43	0.0	0.0%	31.0
9/19/2024 2:00:00 AM	44.0	3	4	46	46.0	25.0%	34.8
9/19/2024 3:00:00 AM	39.0	2	2	39	0.0	0.0%	33.0
9/19/2024 4:00:00 AM	43.0	2	2	43	0.0	0.0%	41.5
9/19/2024 5:00:00 AM	33.0	1	1	33	0.0	0.0%	33.0
9/19/2024 6:00:00 AM	50.0	3	3	50	50.0	33.3%	43.0
9/19/2024 7:00:00 AM	41.0	6	7	46	46.0	14.3%	37.1
9/19/2024 8:00:00 AM	32.0	112	132	48	47.5	1.5%	26.5
9/19/2024 9:00:00 AM	43.0	47	55	48	47.0	3.6%	33.1
9/19/2024 10:00:00 AM	45.0	54	63	49	47.8	9.5%	36.7
9/19/2024 11:00:00 AM	42.0	58	68	50	48.7	4.4%	37.0
9/19/2024 12:00:00 PM	45.0	80	94	50	47.6	9.6%	38.7
9/19/2024 1:00:00 PM	44.0	82	97	47	46.8	4.1%	36.2
9/19/2024 2:00:00 PM	43.0	96	113	53	48.1	8.0%	36.0
9/19/2024 3:00:00 PM	40.0	151	178	49	47.0	1.7%	29.8
9/19/2024 4:00:00 PM	43.0	173	204	52	47.4	6.9%	35.6
9/19/2024 5:00:00 PM	44.0	156	183	50	47.2	7.7%	37.4
9/19/2024 6:00:00 PM	45.0	190	224	50	47.0	9.8%	37.5
9/19/2024 7:00:00 PM	45.0	128	150	58	47.4	11.3%	37.0
9/19/2024 8:00:00 PM	44.0	83	98	58	49.7	9.2%	37.3
9/19/2024 9:00:00 PM	45.0	65	76	54	48.7	13.2%	38.3
9/19/2024 10:00:00 PM	43.0	34	40	50	48.5	5.0%	35.8
9/19/2024 11:00:00 PM	41.0	19	22	49	49.0	4.5%	35.8
9/20/2024 12:00:00 AM	45.0	6	7	45	0.0	0.0%	36.9
9/20/2024 1:00:00 AM	43.0	1	1	43	0.0	0.0%	43.0
9/20/2024 2:00:00 AM	28.0	2	2	28	0.0	0.0%	25.0
9/20/2024 3:00:00 AM	**No Data**						
9/20/2024 4:00:00 AM	44.0	1	1	44	0.0	0.0%	44.0
9/20/2024 5:00:00 AM	**No Data**						
9/20/2024 6:00:00 AM	43.0	3	4	45	0.0	0.0%	41.0
9/20/2024 7:00:00 AM	40.0	8	10	49	47.5	20.0%	35.3
9/20/2024 8:00:00 AM	34.0	112	132	44	0.0	0.0%	26.9
9/20/2024 9:00:00 AM	43.0	54	64	47	46.5	3.1%	33.8
9/20/2024 10:00:00 AM	44.0	42	49	54	49.0	8.2%	36.4

Outgoing Summary
Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/17/2024 12:00:00 AM	43.0	974	1146	56	47.2	6.3%	35.4
9/18/2024 12:00:00 AM	44.0	1446	1701	56	47.6	8.2%	35.7
9/19/2024 12:00:00 AM	44.0	1405	1653	58	47.7	8.0%	35.6
9/20/2024 12:00:00 AM	44.0	1552	1826	58	47.7	7.1%	35.4
9/20/2024 9:59:59 AM	41.0	224	263	54	48.0	3.0%	31.0

Outgoing Weekly Counts

Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	7	6	3	1	*	*	4.25	0	41.5
1 - 2	*	6	4	4	2	*	*	4	0	40
2 - 3	*	2	0	2	0	*	*	1	0	43.5
3 - 4	*	0	0	2	1	*	*	0.75	0	43.5
4 - 5	*	1	0	1	0	*	*	0.5	0	38.5
5 - 6	*	3	3	3	4	*	*	3.25	0	46.25
6 - 7	*	14	13	7	10	*	*	11	0	41.58
7 - 8	*	120	118	132	132	*	*	125.5	0	34.38
8 - 9	*	56	57	55	64	*	*	58	0	42.65
9 - 10	*	67	57	63	49	*	*	59	0	43.6
10 - 11	*	64	57	68	*	*	*	63	0	43.33
11 - 12	*	90	91	94	*	*	*	91.67	0	43.87
12 - 13	57	106	85	97	*	*	*	86.25	0	43.3
13 - 14	115	104	92	113	*	*	*	106	0	42.9
14 - 15	155	149	163	178	*	*	*	161.25	0	37.88
15 - 16	170	166	172	204	*	*	*	178	0	43.18
16 - 17	169	185	186	183	*	*	*	180.75	0	43.3
17 - 18	182	226	189	224	*	*	*	205.25	0	44.18
18 - 19	117	162	122	150	*	*	*	137.75	0	44.35
19 - 20	63	79	81	98	*	*	*	80.25	0	43.58
20 - 21	45	45	79	76	*	*	*	61.25	0	43.2
21 - 22	43	34	47	40	*	*	*	41	0	42.62
22 - 23	17	10	23	22	*	*	*	18	0	43.25
23 - 24	13	5	8	7	*	*	*	8.25	0	43.88
Totals	1146	1701	1653	1826	263	0	0			
% of Total	17.39%	25.82%	25.09%	27.71%	3.99%	0%	0%			

Outgoing Monthly Counts
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	7	6	3	1	*	*	4.25	0	41.5
1 - 2	*	6	4	4	2	*	*	4	0	40
2 - 3	*	2	0	2	0	*	*	1	0	43.5
3 - 4	*	0	0	2	1	*	*	0.75	0	43.5
4 - 5	*	1	0	1	0	*	*	0.5	0	38.5
5 - 6	*	3	3	3	4	*	*	3.25	0	46.25
6 - 7	*	14	13	7	10	*	*	11	0	41.58
7 - 8	*	120	118	132	132	*	*	125.5	0	34.38
8 - 9	*	56	57	55	64	*	*	58	0	42.65
9 - 10	*	67	57	63	49	*	*	59	0	43.6
10 - 11	*	64	57	68	*	*	*	63	0	43.33
11 - 12	*	90	91	94	*	*	*	91.67	0	43.87
12 - 13	57	106	85	97	*	*	*	86.25	0	43.3
13 - 14	115	104	92	113	*	*	*	106	0	42.9
14 - 15	155	149	163	178	*	*	*	161.25	0	37.88
15 - 16	170	166	172	204	*	*	*	178	0	43.18
16 - 17	169	185	186	183	*	*	*	180.75	0	43.3
17 - 18	182	226	189	224	*	*	*	205.25	0	44.18
18 - 19	117	162	122	150	*	*	*	137.75	0	44.35
19 - 20	63	79	81	98	*	*	*	80.25	0	43.58
20 - 21	45	45	79	76	*	*	*	61.25	0	43.2
21 - 22	43	34	47	40	*	*	*	41	0	42.62
22 - 23	17	10	23	22	*	*	*	18	0	43.25
23 - 24	13	5	8	7	*	*	*	8.25	0	43.88
Totals	1146	1701	1653	1826	263	0	0			
% of Total	17.39%	25.82%	25.09%	27.71%	3.99%	0%	0%			

Outgoing Weekly Speeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	34.43	28.67	31	43	*	*	32.29	*	41.5
1 - 2	*	39.67	39.25	34.75	25	*	*	36.5	*	40
2 - 3	*	42	0	33	0	*	*	37.5	*	43.5
3 - 4	*	0	0	41.5	44	*	*	42.33	*	43.5
4 - 5	*	44	0	33	0	*	*	38.5	*	38.5
5 - 6	*	45	41	43	41	*	*	42.38	*	46.25
6 - 7	*	34.86	37.92	37.14	35.3	*	*	36.23	*	41.58
7 - 8	*	27.67	26.74	26.46	26.86	*	*	26.92	*	34.38
8 - 9	*	33.66	34.96	33.11	33.84	*	*	33.9	*	42.65
9 - 10	*	37.13	36	36.67	36.37	*	*	36.58	*	43.6
10 - 11	*	36.92	38	36.99	*	*	*	37.27	*	43.33
11 - 12	*	35.43	38.03	38.69	*	*	*	37.41	*	43.87
12 - 13	33.51	37.87	38.16	36.23	*	*	*	36.76	*	43.3
13 - 14	36.12	36.08	36.36	35.96	*	*	*	36.12	*	42.9
14 - 15	27.86	28.04	28.25	29.81	*	*	*	28.54	*	37.88
15 - 16	35.58	36.52	37.81	35.64	*	*	*	36.36	*	43.18
16 - 17	37.05	36.67	37.54	37.44	*	*	*	37.18	*	43.3
17 - 18	38.55	38.28	37.7	37.51	*	*	*	38	*	44.18
18 - 19	37.02	38.58	37.61	36.97	*	*	*	37.6	*	44.35
19 - 20	38.32	38.03	35.93	37.31	*	*	*	37.33	*	43.58
20 - 21	34.69	37.38	37.27	38.32	*	*	*	37.14	*	43.2
21 - 22	35	37.56	34.98	35.75	*	*	*	35.71	*	42.62
22 - 23	34.94	42.7	39.91	35.77	*	*	*	37.86	*	43.25
23 - 24	32.92	38.8	38	36.86	*	*	*	35.88	*	43.88
Totals	35.4	35.7	35.6	35.4	31	0	0			
% of Total	20.45%	20.62%	20.57%	20.45%	17.91%	0%	0%			

Outgoing Monthly Speeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	34.43	28.67	31	43	*	*	32.29	*	41.5
1 - 2	*	39.67	39.25	34.75	25	*	*	36.5	*	40
2 - 3	*	42	0	33	0	*	*	37.5	*	43.5
3 - 4	*	0	0	41.5	44	*	*	42.33	*	43.5
4 - 5	*	44	0	33	0	*	*	38.5	*	38.5
5 - 6	*	45	41	43	41	*	*	42.38	*	46.25
6 - 7	*	34.86	37.92	37.14	35.3	*	*	36.23	*	41.58
7 - 8	*	27.67	26.74	26.46	26.86	*	*	26.92	*	34.38
8 - 9	*	33.66	34.96	33.11	33.84	*	*	33.9	*	42.65
9 - 10	*	37.13	36	36.67	36.37	*	*	36.58	*	43.6
10 - 11	*	36.92	38	36.99	*	*	*	37.27	*	43.33
11 - 12	*	35.43	38.03	38.69	*	*	*	37.41	*	43.87
12 - 13	33.51	37.87	38.16	36.23	*	*	*	36.76	*	43.3
13 - 14	36.12	36.08	36.36	35.96	*	*	*	36.12	*	42.9
14 - 15	27.86	28.04	28.25	29.81	*	*	*	28.54	*	37.88
15 - 16	35.58	36.52	37.81	35.64	*	*	*	36.36	*	43.18
16 - 17	37.05	36.67	37.54	37.44	*	*	*	37.18	*	43.3
17 - 18	38.55	38.28	37.7	37.51	*	*	*	38	*	44.18
18 - 19	37.02	38.58	37.61	36.97	*	*	*	37.6	*	44.35
19 - 20	38.32	38.03	35.93	37.31	*	*	*	37.33	*	43.58
20 - 21	34.69	37.38	37.27	38.32	*	*	*	37.14	*	43.2
21 - 22	35	37.56	34.98	35.75	*	*	*	35.71	*	42.62
22 - 23	34.94	42.7	39.91	35.77	*	*	*	37.86	*	43.25
23 - 24	32.92	38.8	38	36.86	*	*	*	35.88	*	43.88
Totals	35.4	35.7	35.6	35.4	31	0	0			
% of Total	20.45%	20.62%	20.57%	20.45%	17.91%	0%	0%			

Outgoing Weekly EightyFifthSpeeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	45	35	43	43	*	*	41.5	0	41.5
1 - 2	*	44	44	44	28	*	*	40	0	40
2 - 3	*	48	0	39	0	*	*	43.5	0	43.5
3 - 4	*	0	0	43	44	*	*	43.5	0	43.5
4 - 5	*	44	0	33	0	*	*	38.5	0	38.5
5 - 6	*	48	44	50	43	*	*	46.25	0	46.25
6 - 7	*	42	43.3	41	40	*	*	41.58	0	41.58
7 - 8	*	37.5	34	32	34	*	*	34.38	0	34.38
8 - 9	*	42.7	43.3	42.5	42.1	*	*	42.65	0	42.65
9 - 10	*	44	42.3	44.6	43.5	*	*	43.6	0	43.6
10 - 11	*	43.8	44.3	41.9	*	*	*	43.33	0	43.33
11 - 12	*	43.7	43.5	44.4	*	*	*	43.87	0	43.87
12 - 13	41	44.9	44.2	43.1	*	*	*	43.3	0	43.3
13 - 14	43	42.8	42.8	43	*	*	*	42.9	0	42.9
14 - 15	36.8	36.8	38.2	39.7	*	*	*	37.88	0	37.88
15 - 16	43.1	43	44.2	42.4	*	*	*	43.18	0	43.18
16 - 17	43.5	43	43.5	43.2	*	*	*	43.3	0	43.3
17 - 18	44.2	44.2	44.2	44.1	*	*	*	44.18	0	44.18
18 - 19	43.4	45	44.6	44.4	*	*	*	44.35	0	44.35
19 - 20	45	43	43	43.3	*	*	*	43.58	0	43.58
20 - 21	42	42.3	44	44.5	*	*	*	43.2	0	43.2
21 - 22	43	43	42	42.5	*	*	*	42.62	0	42.62
22 - 23	41.5	45.7	44.8	41	*	*	*	43.25	0	43.25
23 - 24	42	44.5	44.5	44.5	*	*	*	43.88	0	43.88
Totals	508.5	1000.9	893.7	1014.1	317.6	0	0			
% of Total	13.62%	26.8%	23.93%	27.15%	8.5%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	45	35	43	43	*	*	41.5	0	41.5
1 - 2	*	44	44	44	28	*	*	40	0	40
2 - 3	*	48	0	39	0	*	*	43.5	0	43.5
3 - 4	*	0	0	43	44	*	*	43.5	0	43.5
4 - 5	*	44	0	33	0	*	*	38.5	0	38.5
5 - 6	*	48	44	50	43	*	*	46.25	0	46.25
6 - 7	*	42	43.3	41	40	*	*	41.58	0	41.58
7 - 8	*	37.5	34	32	34	*	*	34.38	0	34.38
8 - 9	*	42.7	43.3	42.5	42.1	*	*	42.65	0	42.65
9 - 10	*	44	42.3	44.6	43.5	*	*	43.6	0	43.6
10 - 11	*	43.8	44.3	41.9	*	*	*	43.33	0	43.33
11 - 12	*	43.7	43.5	44.4	*	*	*	43.87	0	43.87
12 - 13	41	44.9	44.2	43.1	*	*	*	43.3	0	43.3
13 - 14	43	42.8	42.8	43	*	*	*	42.9	0	42.9
14 - 15	36.8	36.8	38.2	39.7	*	*	*	37.88	0	37.88
15 - 16	43.1	43	44.2	42.4	*	*	*	43.18	0	43.18
16 - 17	43.5	43	43.5	43.2	*	*	*	43.3	0	43.3
17 - 18	44.2	44.2	44.2	44.1	*	*	*	44.18	0	44.18
18 - 19	43.4	45	44.6	44.4	*	*	*	44.35	0	44.35
19 - 20	45	43	43	43.3	*	*	*	43.58	0	43.58
20 - 21	42	42.3	44	44.5	*	*	*	43.2	0	43.2
21 - 22	43	43	42	42.5	*	*	*	42.62	0	42.62
22 - 23	41.5	45.7	44.8	41	*	*	*	43.25	0	43.25
23 - 24	42	44.5	44.5	44.5	*	*	*	43.88	0	43.88

Summary of Violators

Old Glenn Hwy North North 9-16 to 9-20 time correct from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	17	32.3	1	54.0
01:00:00	16	36.5	3	47.3
02:00:00	4	37.5	1	48.0
03:00:00	3	42.3	0	0.0
04:00:00	2	38.5	0	0.0
05:00:00	13	42.4	2	49.0
06:00:00	44	36.2	4	46.8
07:00:00	502	26.9	4	48.3
08:00:00	232	33.9	12	48.8
09:00:00	236	36.6	17	48.5
10:00:00	189	37.3	16	47.8
11:00:00	275	37.4	24	48.3
12:00:00	345	36.8	28	47.1
13:00:00	424	36.1	37	47.7
14:00:00	645	28.5	8	47.4
15:00:00	712	36.4	59	47.5
16:00:00	723	37.2	52	47.4
17:00:00	821	38.0	83	47.2
18:00:00	551	37.6	66	47.0
19:00:00	321	37.3	30	48.3
20:00:00	245	37.1	20	48.5
21:00:00	164	35.7	8	48.0
22:00:00	72	37.9	7	47.1
23:00:00	33	35.9	0	0.0

Outgoing Histogram
Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	0	3	6	10	8	16	14	0	0	0	0	0	0	0	0	0	0	0	57	33.5	41	33 to 43	61.4	*	*	1	54	2	
9/16/2024	13:00	0	0	8	16	28	14	40	9	0	0	0	0	0	0	0	0	0	0	115	36.1	43	32 to 42	54.8	7	6.1	0	108	7	
9/16/2024	14:00	0	17	49	30	22	24	11	2	0	0	0	0	0	0	0	0	0	0	155	27.9	36.8	17 to 27	56.1	2	1.3	0	149	6	
9/16/2024	15:00	0	1	4	36	43	26	43	17	0	0	0	0	0	0	0	0	0	0	170	35.6	43.1	27 to 37	52.9	12	7.1	0	166	4	
9/16/2024	16:00	1	1	0	20	44	25	56	21	0	1	0	0	0	0	0	0	0	0	169	37	43.5	33 to 43	55.0	15	8.9	0	159	10	
9/16/2024	17:00	0	0	3	14	33	36	66	29	1	0	0	0	0	0	0	0	0	0	182	38.5	44.2	35 to 45	64.3	15	8.2	0	175	7	
9/16/2024	18:00	1	1	2	14	26	19	42	12	0	0	0	0	0	0	0	0	0	0	117	37	43.4	34 to 44	57.3	10	8.5	0	113	4	
9/16/2024	19:00	0	1	2	3	8	19	19	10	1	0	0	0	0	0	0	0	0	0	63	38.3	45	38 to 48	68.3	9	14.3	0	62	1	
9/16/2024	20:00	0	1	0	13	8	8	12	3	0	0	0	0	0	0	0	0	0	0	45	34.7	42	35 to 45	51.1	*	*	0	45	0	
9/16/2024	21:00	2	1	1	3	11	12	10	3	0	0	0	0	0	0	0	0	0	0	43	35	43	34 to 44	60.5	2	4.7	0	42	1	
9/16/2024	22:00	0	0	2	3	0	5	7	0	0	0	0	0	0	0	0	0	0	0	17	34.9	41.5	33 to 43	70.6	*	*	0	17	0	
9/16/2024	23:00	0	1	0	3	3	3	2	1	0	0	0	0	0	0	0	0	0	0	13	32.9	42	25 to 35	61.5	*	*	0	13	0	
24 Hr Summary		4	27	77	165	234	207	322	107	2	1	0	0	0	0	0	0	0	0	1146	35.4	43	34 to 44	50.1	72	6.3	1	1103	42	

Outgoing Histogram
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/17/2024	00:00	0	0	2	2	0	0	1	1	1	0	0	0	0	0	0	0	0	0	7	34.4	45	19 to 29	57.1	1	14.3	0	5	2	
9/17/2024	01:00	0	0	0	0	1	2	2	1	0	0	0	0	0	0	0	0	0	0	6	39.7	44	31 to 41	66.7	1	16.7	0	6	0	
9/17/2024	02:00	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	2	42	48	26 to 36	50.0	1	50.0	0	2	0	
9/17/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/17/2024	04:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	44	44	34 to 44	100.0	*	*	0	1	0	
9/17/2024	05:00	0	0	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	3	45	48	38 to 48	100.0	1	33.3	0	3	0	
9/17/2024	06:00	0	0	1	3	2	4	3	1	0	0	0	0	0	0	0	0	0	0	14	34.9	42	27 to 37	57.1	1	7.1	0	13	1	
9/17/2024	07:00	0	15	41	24	12	14	9	4	1	0	0	0	0	0	0	0	0	0	120	27.7	37.5	18 to 28	59.2	1	0.8	0	114	6	
9/17/2024	08:00	0	5	5	12	6	7	17	3	1	0	0	0	0	0	0	0	0	0	56	33.7	42.7	37 to 47	48.2	2	3.6	0	51	5	
9/17/2024	09:00	0	3	1	6	12	15	20	8	2	0	0	0	0	0	0	0	0	0	67	37.1	44	33 to 43	58.2	7	10.4	0	56	11	
9/17/2024	10:00	0	0	2	9	14	13	17	8	1	0	0	0	0	0	0	0	0	0	64	36.9	43.8	37 to 47	53.1	6	9.4	0	60	4	
9/17/2024	11:00	0	3	8	16	10	21	20	11	0	1	0	0	0	0	0	0	0	0	90	35.4	43.7	35 to 45	51.1	7	7.8	0	85	5	
9/17/2024	12:00	1	1	4	9	21	13	35	20	2	0	0	0	0	0	0	0	0	0	106	37.9	44.9	36 to 46	56.6	15	14.2	0	99	7	
9/17/2024	13:00	0	1	6	14	22	18	32	9	2	0	0	0	0	0	0	0	0	0	104	36.1	42.8	31 to 41	51.9	9	8.7	0	98	6	
9/17/2024	14:00	1	14	43	34	21	22	12	2	0	0	0	0	0	0	0	0	0	0	149	28	36.8	18 to 28	57.0	2	1.3	0	141	8	
9/17/2024	15:00	0	1	7	20	36	31	54	15	1	1	0	0	0	0	0	0	0	0	166	36.5	43	34 to 44	54.8	13	7.8	0	157	9	
9/17/2024	16:00	1	1	4	26	32	49	52	19	1	0	0	0	0	0	0	0	0	0	185	36.7	43	35 to 45	60.0	10	5.4	0	171	14	
9/17/2024	17:00	1	1	3	17	45	37	85	34	3	0	0	0	0	0	0	0	0	0	226	38.3	44.2	36 to 46	60.2	25	11.1	0	219	7	
9/17/2024	18:00	0	2	0	13	27	32	56	31	1	0	0	0	0	0	0	0	0	0	162	38.6	45	36 to 46	65.4	24	14.8	0	157	5	
9/17/2024	19:00	0	1	0	8	9	23	31	6	1	0	0	0	0	0	0	0	0	0	79	38	43	35 to 45	73.4	3	3.8	0	76	3	
9/17/2024	20:00	0	0	1	3	10	10	16	5	0	0	0	0	0	0	0	0	0	0	45	37.4	42.3	31 to 41	68.9	3	6.7	0	44	1	
9/17/2024	21:00	0	0	2	3	4	12	9	3	1	0	0	0	0	0	0	0	0	0	34	37.6	43	35 to 45	64.7	3	8.8	0	34	0	
9/17/2024	22:00	0	0	0	0	0	2	4	4	0	0	0	0	0	0	0	0	0	0	10	42.7	45.7	37 to 47	90.0	4	40.0	0	10	0	
9/17/2024	23:00	0	0	0	0	2	1	0	2	0	0	0	0	0	0	0	0	0	0	5	38.8	44.5	26 to 36	60.0	*	*	0	5	0	
24 Hr Summary		4	48	130	219	286	327	477	190	18	2	0	0	0	0	0	0	0	0	1701	35.7	44	36 to 46	51.6	139	8.2	0	1607	94	

Outgoing Histogram
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/18/2024	00:00	0	0	2	1	1	2	0	0	0	0	0	0	0	0	0	0	0	0	6	28.7	35	20 to 30	66.7	*	*	0	6	0	
9/18/2024	01:00	0	0	0	0	1	1	1	1	0	0	0	0	0	0	0	0	0	0	4	39.3	44	26 to 36	50.0	1	25.0	0	4	0	
9/18/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/18/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/18/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/18/2024	05:00	0	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	0	0	3	41	44	34 to 44	100.0	*	*	0	3	0	
9/18/2024	06:00	0	0	0	2	2	2	7	0	0	0	0	0	0	0	0	0	0	0	13	37.9	43.3	34 to 44	69.2	*	*	0	11	2	
9/18/2024	07:00	0	15	44	18	23	12	5	1	0	0	0	0	0	0	0	0	0	0	118	26.7	34	18 to 28	60.2	1	0.8	0	112	6	
9/18/2024	08:00	0	1	8	9	9	8	15	5	1	1	0	0	0	0	0	0	0	0	57	35	43.3	30 to 40	42.1	6	10.5	0	54	3	
9/18/2024	09:00	2	1	0	5	11	17	19	2	0	0	0	0	0	0	0	0	0	0	57	36	42.3	34 to 44	68.4	*	*	0	47	10	
9/18/2024	10:00	1	0	2	1	13	10	20	9	1	0	0	0	0	0	0	0	0	0	57	38	44.3	37 to 47	61.4	7	12.3	0	54	3	
9/18/2024	11:00	0	0	1	16	11	14	38	8	3	0	0	0	0	0	0	0	0	0	91	38	43.5	35 to 45	60.4	8	8.8	0	83	8	
9/18/2024	12:00	0	1	2	8	14	15	31	14	0	0	0	0	0	0	0	0	0	0	85	38.2	44.2	36 to 46	65.9	9	10.6	0	74	11	
9/18/2024	13:00	0	3	2	13	19	22	21	10	2	0	0	0	0	0	0	0	0	0	92	36.4	42.8	33 to 43	55.4	12	13.0	0	84	8	
9/18/2024	14:00	1	18	52	27	26	19	17	2	1	0	0	0	0	0	0	0	0	0	163	28.2	38.2	19 to 29	53.4	1	0.6	0	156	7	
9/18/2024	15:00	0	1	1	23	34	29	56	24	4	0	0	0	0	0	0	0	0	0	172	37.8	44.2	36 to 46	59.9	20	11.6	0	162	10	
9/18/2024	16:00	1	4	4	18	29	36	72	20	1	1	0	0	0	0	0	0	0	0	186	37.5	43.5	36 to 46	63.4	13	7.0	0	179	7	
9/18/2024	17:00	1	3	3	20	30	41	61	29	1	0	0	0	0	0	0	0	0	0	189	37.7	44.2	37 to 47	59.8	21	11.1	0	182	7	
9/18/2024	18:00	0	2	2	12	26	20	38	21	1	0	0	0	0	0	0	0	0	0	122	37.6	44.6	36 to 46	54.1	15	12.3	0	117	5	
9/18/2024	19:00	0	2	2	10	20	20	18	6	3	0	0	0	0	0	0	0	0	0	81	35.9	43	30 to 40	56.8	9	11.1	0	80	1	
9/18/2024	20:00	0	2	3	5	17	12	28	11	1	0	0	0	0	0	0	0	0	0	79	37.3	44	35 to 45	57.0	7	8.9	0	79	0	
9/18/2024	21:00	0	0	6	6	6	13	15	1	0	0	0	0	0	0	0	0	0	0	47	35	42	33 to 43	61.7	1	2.1	0	45	2	
9/18/2024	22:00	0	0	1	1	3	2	10	6	0	0	0	0	0	0	0	0	0	0	23	39.9	44.8	35 to 45	69.6	2	8.7	0	23	0	
9/18/2024	23:00	0	1	0	1	0	0	4	2	0	0	0	0	0	0	0	0	0	0	8	38	44.5	35 to 45	75.0	*	*	0	8	0	
24 Hr Summary		6	54	135	196	295	296	478	172	19	2	0	0	0	0	0	0	0	0	1653	35.6	44	36 to 46	50.7	133	8.0	0	1563	90	

Outgoing Histogram
 Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/19/2024	00:00	0	0	1	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	3	31	43	19 to 29	66.7	*	*	0	3	0
9/19/2024	01:00	0	0	1	1	0	0	1	1	0	0	0	0	0	0	0	0	0	0	4	34.8	44	15 to 25	50.0	1	25.0	0	4	0
9/19/2024	02:00	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	2	33	39	17 to 27	50.0	*	*	0	2	0
9/19/2024	03:00	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	2	41.5	43	33 to 43	100.0	*	*	0	2	0
9/19/2024	04:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	33	33	23 to 33	100.0	*	*	0	1	0
9/19/2024	05:00	0	0	0	0	0	1	1	0	1	0	0	0	0	0	0	0	0	0	3	43	50	31 to 41	66.7	1	33.3	0	3	0
9/19/2024	06:00	0	0	0	1	1	3	1	1	0	0	0	0	0	0	0	0	0	0	7	37.1	41	28 to 38	71.4	1	14.3	0	4	3
9/19/2024	07:00	0	12	55	28	24	3	6	4	0	0	0	0	0	0	0	0	0	0	132	26.5	32	19 to 29	68.9	2	1.5	0	126	6
9/19/2024	08:00	0	0	9	11	12	8	10	5	0	0	0	0	0	0	0	0	0	0	55	33.1	42.5	23 to 33	49.1	2	3.6	0	49	6
9/19/2024	09:00	1	1	3	10	8	14	12	14	0	0	0	0	0	0	0	0	0	0	63	36.7	44.6	35 to 45	54.0	6	9.5	0	57	6
9/19/2024	10:00	1	1	2	4	13	18	25	3	1	0	0	0	0	0	0	0	0	0	68	37	41.9	33 to 43	67.6	3	4.4	0	61	7
9/19/2024	11:00	0	2	1	9	12	13	40	16	1	0	0	0	0	0	0	0	0	0	94	38.7	44.4	38 to 48	66.0	9	9.6	0	85	9
9/19/2024	12:00	1	6	3	12	9	21	37	8	0	0	0	0	0	0	0	0	0	0	97	36.2	43.1	35 to 45	63.9	4	4.1	0	91	6
9/19/2024	13:00	0	2	3	17	28	22	29	9	3	0	0	0	0	0	0	0	0	0	113	36	43	30 to 40	49.6	9	8.0	0	102	11
9/19/2024	14:00	1	13	48	40	21	21	28	6	0	0	0	0	0	0	0	0	0	0	178	29.8	39.7	19 to 29	52.8	3	1.7	0	167	11
9/19/2024	15:00	0	3	9	34	42	46	53	15	2	0	0	0	0	0	0	0	0	0	204	35.6	42.4	34 to 44	53.9	14	6.9	0	196	8
9/19/2024	16:00	0	1	1	16	52	27	65	20	1	0	0	0	0	0	0	0	0	0	183	37.4	43.2	32 to 42	57.9	14	7.7	0	175	8
9/19/2024	17:00	1	4	2	20	49	43	70	34	1	0	0	0	0	0	0	0	0	0	224	37.5	44.1	36 to 46	58.9	22	9.8	0	217	7
9/19/2024	18:00	0	5	1	17	29	37	35	24	1	1	0	0	0	0	0	0	0	0	150	37	44.4	36 to 46	59.3	17	11.3	0	145	5
9/19/2024	19:00	0	1	2	13	18	18	33	10	1	2	0	0	0	0	0	0	0	0	98	37.3	43.3	35 to 45	56.1	9	9.2	0	98	0
9/19/2024	20:00	0	2	1	6	14	11	30	10	2	0	0	0	0	0	0	0	0	0	76	38.3	44.5	36 to 46	60.5	10	13.2	0	74	2
9/19/2024	21:00	0	3	1	3	7	11	12	2	1	0	0	0	0	0	0	0	0	0	40	35.8	42.5	33 to 43	60.0	2	5.0	0	40	0
9/19/2024	22:00	0	1	3	0	3	6	8	1	0	0	0	0	0	0	0	0	0	0	22	35.8	41	31 to 41	68.2	1	4.5	0	22	0
9/19/2024	23:00	0	0	0	2	1	1	1	2	0	0	0	0	0	0	0	0	0	0	7	36.9	44.5	35 to 45	57.1	*	*	0	7	0
24 Hr Summary		5	57	146	246	344	325	500	185	15	3	0	0	0	0	0	0	0	0	1826	35.4	44	36 to 46	49.9	130	7.1	0	1731	95

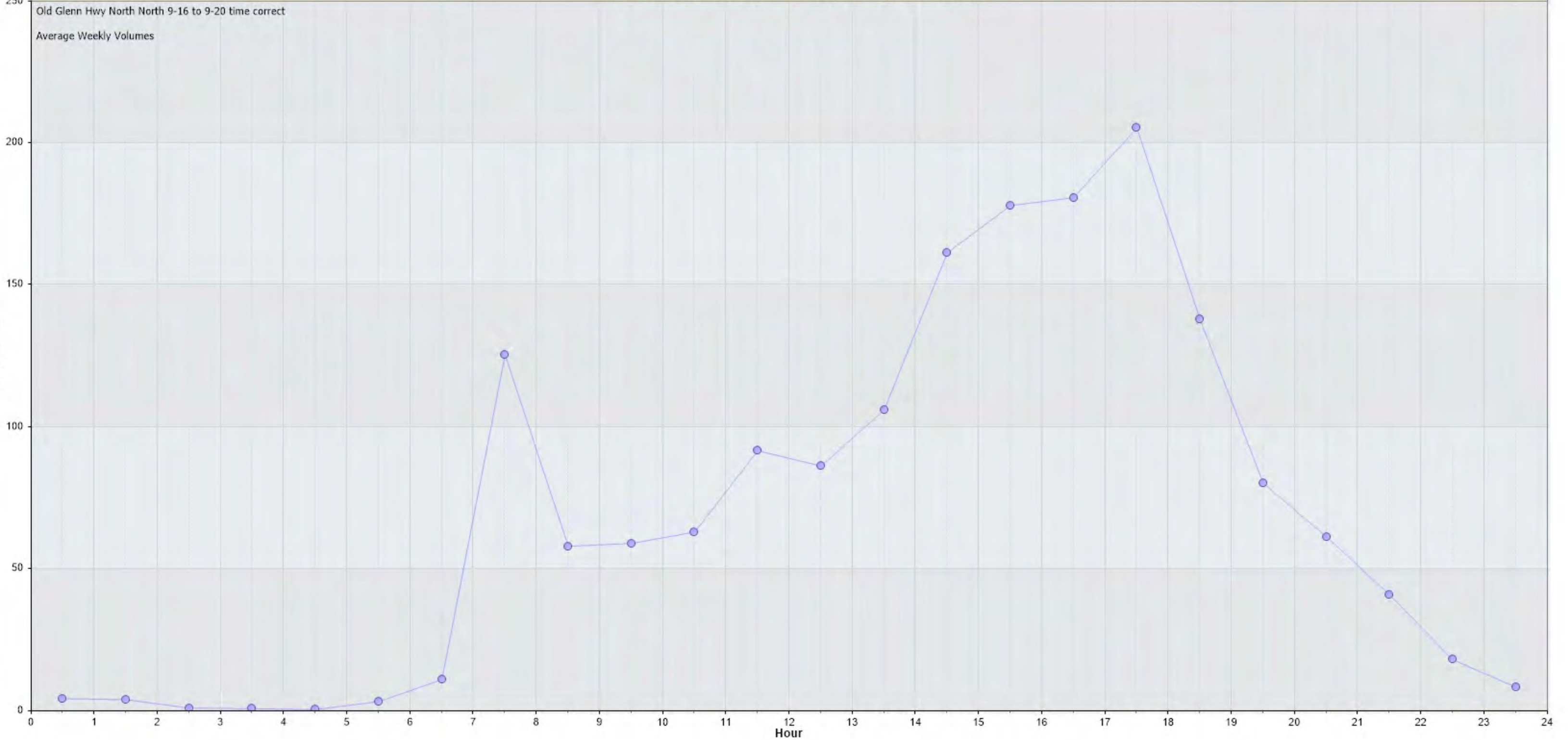
Outgoing Histogram
Old Glenn Hwy North North 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/20/2024	00:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	43	43	33 to 43	100.0	*	*	0	1	0	
9/20/2024	01:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25	28	18 to 28	100.0	*	*	0	2	0	
9/20/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	03:00	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	1	44	44	34 to 44	100.0	*	*	0	1	0	
9/20/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	05:00	0	0	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	4	41	43	35 to 45	100.0	*	*	0	4	0	
9/20/2024	06:00	0	0	0	2	4	1	1	2	0	0	0	0	0	0	0	0	0	0	10	35.3	40	25 to 35	70.0	2	20.0	0	9	1	
9/20/2024	07:00	0	11	56	24	21	13	7	0	0	0	0	0	0	0	0	0	0	0	132	26.9	34	18 to 28	65.2	*	*	0	125	7	
9/20/2024	08:00	1	3	6	10	10	13	17	4	0	0	0	0	0	0	0	0	0	0	64	33.8	42.1	33 to 43	53.1	2	3.1	0	57	7	
9/20/2024	09:00	0	2	2	7	6	14	13	4	1	0	0	0	0	0	0	0	0	0	49	36.4	43.5	34 to 44	57.1	4	8.2	0	43	6	
9/20/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		1	16	65	44	41	42	42	11	1	0	0	0	0	0	0	0	0	0	263	31	41	21 to 31	44.1	8	3.0	0	242	21	

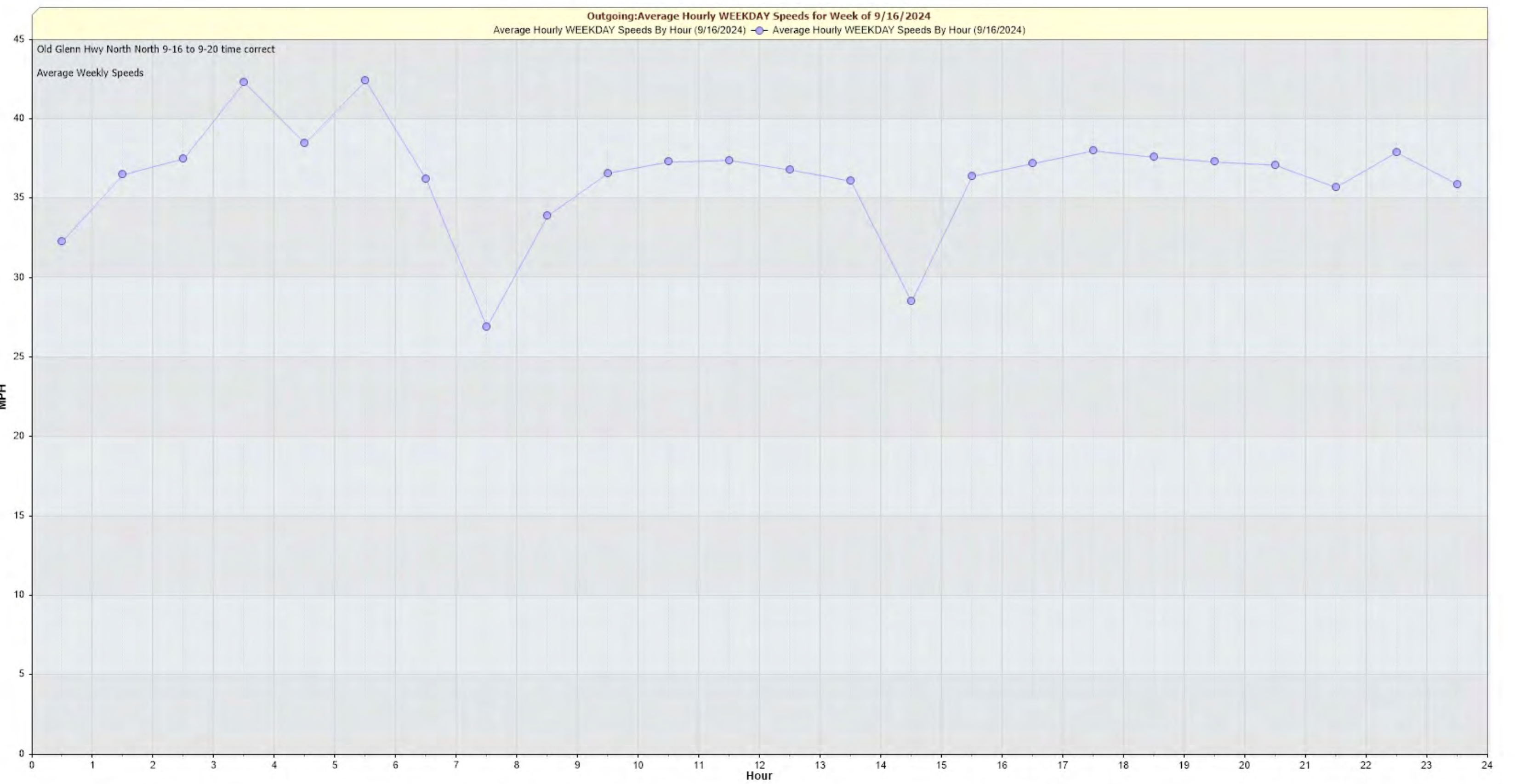
Outgoing: Average Hourly Volume for Week of 9/16/2024

Average Counts By Hour (9/16/2024) ● Average Counts By Hour (9/16/2024)



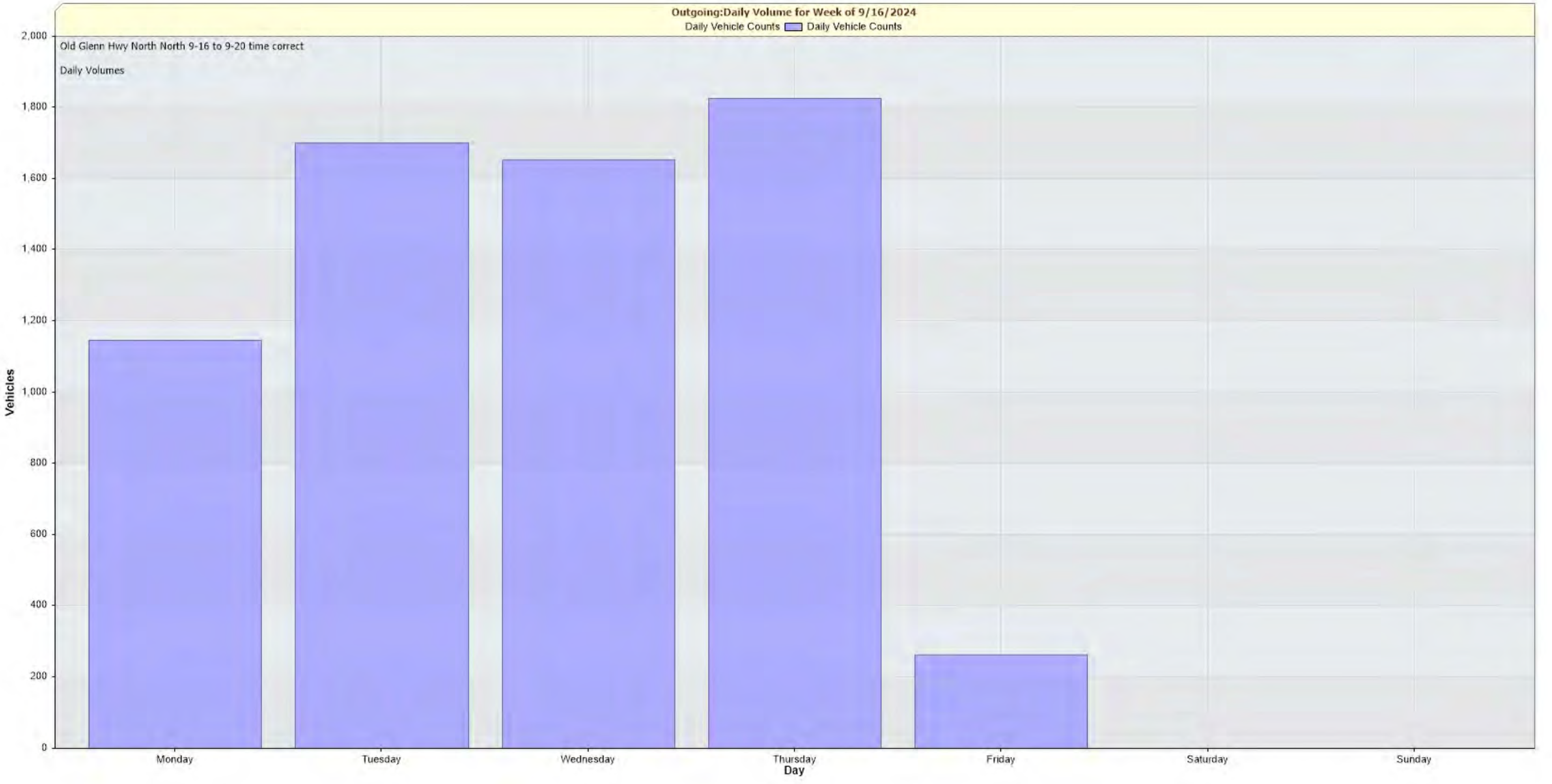
Outgoing: Average Hourly WEEKDAY Speeds for Week of 9/16/2024
Average Hourly WEEKDAY Speeds By Hour (9/16/2024) — Average Hourly WEEKDAY Speeds By Hour (9/16/2024)

Old Glenn Hwy North North 9-16 to 9-20 time correct
Average Weekly Speeds



Outgoing: Daily Volume for Week of 9/16/2024

Daily Vehicle Counts



Old Glenn Hwy North North 9-16 to 9-20 time correct

Daily Volumes

Vehicles

Monday

Tuesday

Wednesday

Thursday
Day

Friday

Saturday

Sunday

For Project: Birchwood Loop northwest northwest 9-16 to 9-20 time correct
 Project Notes:
 Location/Name: Incoming
 Report Generated: 10/3/2024 3:59:16 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 9/16/2024 12:00:00 PM through 9/20/2024 9:59:59 AM
 85th Percentile Speed: 35 MPH
 85th Percentile Vehicles: 6138
 Max Speed: 55 MPH on 9/19/2024 2:00:12 PM
 Total Vehicles: 7221
 AADT: 1843

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1444	1444
AM Peak	7:00 AM 120	120
PM Peak	5:00 PM 189	189

Speed

Speed Limit: 45
 85th Percentile Speed: 35
 50th Percentile Speed: 30
 10 MPH Pace Interval: 25.0 MPH to 35.0 MPH
 Average Speed: 29.04

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	3	1	3	1	0	N/A	N/A
% over limit	0.2	0.1	0.2	0.1	0.0	N/A	N/A
Avg Speeder	47.7	46.0	47.3	55.0	0.0	N/A	N/A
Avg Speed	28.8	29.0	29.3	29.3	27.2	N/A	N/A

Class Counts

	Number	%
VEH_SM	38	0.5
VEH_MED	6906	95.6
VEH_LG	277	3.8
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Incoming Summary
Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/16/2024 1:00:00 PM	35.0	63	74	39	0.0	0.0%	29.2
9/16/2024 2:00:00 PM	35.0	125	147	47	47.0	0.7%	29.6
9/16/2024 3:00:00 PM	31.0	147	173	39	0.0	0.0%	24.9
9/16/2024 4:00:00 PM	35.0	162	190	40	0.0	0.0%	28.8
9/16/2024 5:00:00 PM	34.0	148	174	42	0.0	0.0%	29.9
9/16/2024 6:00:00 PM	35.0	148	174	44	0.0	0.0%	29.7
9/16/2024 7:00:00 PM	35.0	96	113	50	50.0	0.9%	30.1
9/16/2024 8:00:00 PM	34.0	63	74	46	46.0	1.4%	29.4
9/16/2024 9:00:00 PM	34.0	59	69	40	0.0	0.0%	28.5
9/16/2024 10:00:00 PM	33.0	41	48	40	0.0	0.0%	29.1
9/16/2024 11:00:00 PM	33.0	16	19	36	0.0	0.0%	29.1
9/17/2024 12:00:00 AM	31.0	7	8	34	0.0	0.0%	29.0
9/17/2024 1:00:00 AM	38.0	5	6	41	0.0	0.0%	31.2
9/17/2024 2:00:00 AM	30.0	4	5	32	0.0	0.0%	29.8
9/17/2024 3:00:00 AM	30.0	2	2	30	0.0	0.0%	29.0
9/17/2024 4:00:00 AM	25.0	1	1	25	0.0	0.0%	25.0
9/17/2024 5:00:00 AM	38.0	1	1	38	0.0	0.0%	38.0
9/17/2024 6:00:00 AM	34.0	7	8	39	0.0	0.0%	29.4
9/17/2024 7:00:00 AM	36.0	32	38	40	0.0	0.0%	30.3
9/17/2024 8:00:00 AM	30.0	88	104	43	0.0	0.0%	24.0
9/17/2024 9:00:00 AM	35.0	88	104	43	0.0	0.0%	29.3
9/17/2024 10:00:00 AM	36.0	74	87	43	0.0	0.0%	32.2
9/17/2024 11:00:00 AM	35.0	67	79	43	0.0	0.0%	30.4
9/17/2024 12:00:00 PM	34.0	80	94	39	0.0	0.0%	29.6
9/17/2024 1:00:00 PM	35.0	103	121	40	0.0	0.0%	29.4
9/17/2024 2:00:00 PM	35.0	112	132	46	46.0	0.8%	29.9
9/17/2024 3:00:00 PM	31.0	166	195	37	0.0	0.0%	23.9
9/17/2024 4:00:00 PM	35.0	134	158	41	0.0	0.0%	29.6
9/17/2024 5:00:00 PM	35.0	152	179	43	0.0	0.0%	30.5
9/17/2024 6:00:00 PM	35.0	157	185	44	0.0	0.0%	30.0
9/17/2024 7:00:00 PM	34.0	126	148	40	0.0	0.0%	29.7
9/17/2024 8:00:00 PM	33.0	54	64	40	0.0	0.0%	30.3
9/17/2024 9:00:00 PM	34.0	48	56	37	0.0	0.0%	29.3
9/17/2024 10:00:00 PM	32.0	30	35	37	0.0	0.0%	29.0
9/17/2024 11:00:00 PM	32.0	12	14	37	0.0	0.0%	29.4
9/18/2024 12:00:00 AM	34.0	7	8	37	0.0	0.0%	29.8
9/18/2024 1:00:00 AM	34.0	7	8	39	0.0	0.0%	28.3
9/18/2024 2:00:00 AM	37.0	3	3	37	0.0	0.0%	33.3
9/18/2024 3:00:00 AM	30.0	1	1	30	0.0	0.0%	30.0
9/18/2024 4:00:00 AM	32.0	3	4	33	0.0	0.0%	25.3
9/18/2024 5:00:00 AM	34.0	3	4	35	0.0	0.0%	33.5
9/18/2024 6:00:00 AM	31.0	9	11	37	0.0	0.0%	30.6
9/18/2024 7:00:00 AM	34.0	37	43	40	0.0	0.0%	30.2
9/18/2024 8:00:00 AM	31.0	103	121	40	0.0	0.0%	24.6
9/18/2024 9:00:00 AM	35.0	76	89	42	0.0	0.0%	28.8
9/18/2024 10:00:00 AM	36.0	63	74	41	0.0	0.0%	29.6
9/18/2024 11:00:00 AM	34.0	76	89	41	0.0	0.0%	29.6

Incoming Summary
Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

9/18/2024 12:00:00 PM	34.0	75	88	49	48.0	2.3%	29.4
9/18/2024 1:00:00 PM	35.0	90	106	42	0.0	0.0%	29.5
9/18/2024 2:00:00 PM	36.0	97	114	46	46.0	0.9%	30.7
9/18/2024 3:00:00 PM	29.0	151	178	40	0.0	0.0%	24.1
9/18/2024 4:00:00 PM	35.0	133	156	40	0.0	0.0%	29.8
9/18/2024 5:00:00 PM	35.0	148	174	42	0.0	0.0%	31.1
9/18/2024 6:00:00 PM	35.0	157	185	41	0.0	0.0%	30.6
9/18/2024 7:00:00 PM	36.0	136	160	45	0.0	0.0%	32.1
9/18/2024 8:00:00 PM	36.0	78	92	45	0.0	0.0%	30.6
9/18/2024 9:00:00 PM	35.0	74	87	45	0.0	0.0%	31.0
9/18/2024 10:00:00 PM	33.0	26	30	36	0.0	0.0%	29.4
9/18/2024 11:00:00 PM	31.0	16	19	38	0.0	0.0%	28.5
9/19/2024 12:00:00 AM	31.0	8	9	36	0.0	0.0%	26.7
9/19/2024 1:00:00 AM	31.0	8	10	35	0.0	0.0%	29.1
9/19/2024 2:00:00 AM	36.0	5	6	40	0.0	0.0%	28.8
9/19/2024 3:00:00 AM	28.0	4	5	30	0.0	0.0%	27.6
9/19/2024 4:00:00 AM	34.0	2	2	34	0.0	0.0%	34.0
9/19/2024 5:00:00 AM	35.0	2	2	35	0.0	0.0%	34.0
9/19/2024 6:00:00 AM	37.0	8	10	38	0.0	0.0%	33.1
9/19/2024 7:00:00 AM	35.0	33	39	43	0.0	0.0%	30.3
9/19/2024 8:00:00 AM	31.0	118	139	40	0.0	0.0%	24.6
9/19/2024 9:00:00 AM	34.0	84	99	41	0.0	0.0%	28.5
9/19/2024 10:00:00 AM	34.0	88	104	43	0.0	0.0%	30.3
9/19/2024 11:00:00 AM	36.0	70	82	42	0.0	0.0%	31.6
9/19/2024 12:00:00 PM	36.0	82	97	43	0.0	0.0%	30.6
9/19/2024 1:00:00 PM	35.0	89	105	42	0.0	0.0%	29.5
9/19/2024 2:00:00 PM	36.0	115	135	41	0.0	0.0%	31.0
9/19/2024 3:00:00 PM	32.0	139	164	55	55.0	0.6%	25.1
9/19/2024 4:00:00 PM	34.0	190	223	40	0.0	0.0%	29.1
9/19/2024 5:00:00 PM	35.0	140	165	45	0.0	0.0%	30.7
9/19/2024 6:00:00 PM	36.0	180	212	41	0.0	0.0%	30.7
9/19/2024 7:00:00 PM	35.0	114	134	43	0.0	0.0%	30.8
9/19/2024 8:00:00 PM	34.0	60	71	42	0.0	0.0%	30.6
9/19/2024 9:00:00 PM	35.0	57	67	38	0.0	0.0%	28.5
9/19/2024 10:00:00 PM	34.0	43	51	35	0.0	0.0%	28.3
9/19/2024 11:00:00 PM	35.0	19	22	39	0.0	0.0%	29.9
9/20/2024 12:00:00 AM	34.0	8	9	37	0.0	0.0%	30.2
9/20/2024 1:00:00 AM	34.0	3	4	37	0.0	0.0%	32.8
9/20/2024 2:00:00 AM	28.0	2	2	28	0.0	0.0%	28.0
9/20/2024 3:00:00 AM	31.0	4	5	35	0.0	0.0%	25.6
9/20/2024 4:00:00 AM	25.0	3	4	36	0.0	0.0%	26.3
9/20/2024 5:00:00 AM	34.0	2	2	34	0.0	0.0%	32.0
9/20/2024 6:00:00 AM	32.0	7	8	35	0.0	0.0%	27.4
9/20/2024 7:00:00 AM	35.0	36	42	37	0.0	0.0%	29.3
9/20/2024 8:00:00 AM	29.0	99	117	39	0.0	0.0%	23.5
9/20/2024 9:00:00 AM	34.0	74	87	39	0.0	0.0%	28.8
9/20/2024 10:00:00 AM	35.0	55	65	43	0.0	0.0%	30.0

Incoming Summary

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/17/2024 12:00:00 AM	34.0	1074	1263	50	47.7	0.2%	28.8
9/18/2024 12:00:00 AM	34.0	1550	1824	46	46.0	0.1%	29.0
9/19/2024 12:00:00 AM	35.0	1568	1845	49	47.3	0.2%	29.3
9/20/2024 12:00:00 AM	35.0	1660	1953	55	55.0	0.1%	29.3
9/20/2024 9:59:59 AM	34.0	286	336	43	0.0	0.0%	27.2

Incoming Weekly Counts

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	6	8	10	4	*	*	7	0	34.25
1 - 2	*	5	3	6	2	*	*	4	0	32.75
2 - 3	*	2	1	5	5	*	*	3.25	0	29.75
3 - 4	*	1	4	2	4	*	*	2.75	0	29
4 - 5	*	1	4	2	2	*	*	2.25	0	35.25
5 - 6	*	8	11	10	8	*	*	9.25	0	33.38
6 - 7	*	38	43	39	42	*	*	40.5	0	34.6
7 - 8	*	104	121	139	117	*	*	120.25	0	30.2
8 - 9	*	104	89	99	87	*	*	94.75	0	34.15
9 - 10	*	87	74	104	65	*	*	82.5	0	34.92
10 - 11	*	79	89	82	*	*	*	83.33	0	34.67
11 - 12	*	94	88	97	*	*	*	93	0	34.57
12 - 13	74	121	106	105	*	*	*	101.5	0	34.3
13 - 14	147	132	114	135	*	*	*	132	0	35.08
14 - 15	173	195	178	164	*	*	*	177.5	0	30.18
15 - 16	190	158	156	223	*	*	*	181.75	0	34.28
16 - 17	174	179	174	165	*	*	*	173	0	34.52
17 - 18	174	185	185	212	*	*	*	189	0	34.68
18 - 19	113	148	160	134	*	*	*	138.75	0	34.52
19 - 20	74	64	92	71	*	*	*	75.25	0	33.92
20 - 21	69	56	87	67	*	*	*	69.75	0	33.9
21 - 22	48	35	30	51	*	*	*	41	0	32.62
22 - 23	19	14	19	22	*	*	*	18.5	0	32.75
23 - 24	8	8	9	9	*	*	*	8.5	0	32.5
Totals	1263	1824	1845	1953	336	0	0			
% of Total	17.49%	25.26%	25.55%	27.05%	4.65%	0%	0%			

Incoming Monthly Counts

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Hour	Sep 2024							Week Day Avg	Weekend Avg	Week Day 85% Avg Speed
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday			
0 - 1	*	6	8	10	4	*	*	7	0	34.25
1 - 2	*	5	3	6	2	*	*	4	0	32.75
2 - 3	*	2	1	5	5	*	*	3.25	0	29.75
3 - 4	*	1	4	2	4	*	*	2.75	0	29
4 - 5	*	1	4	2	2	*	*	2.25	0	35.25
5 - 6	*	8	11	10	8	*	*	9.25	0	33.38
6 - 7	*	38	43	39	42	*	*	40.5	0	34.6
7 - 8	*	104	121	139	117	*	*	120.25	0	30.2
8 - 9	*	104	89	99	87	*	*	94.75	0	34.15
9 - 10	*	87	74	104	65	*	*	82.5	0	34.92
10 - 11	*	79	89	82	*	*	*	83.33	0	34.67
11 - 12	*	94	88	97	*	*	*	93	0	34.57
12 - 13	74	121	106	105	*	*	*	101.5	0	34.3
13 - 14	147	132	114	135	*	*	*	132	0	35.08
14 - 15	173	195	178	164	*	*	*	177.5	0	30.18
15 - 16	190	158	156	223	*	*	*	181.75	0	34.28
16 - 17	174	179	174	165	*	*	*	173	0	34.52
17 - 18	174	185	185	212	*	*	*	189	0	34.68
18 - 19	113	148	160	134	*	*	*	138.75	0	34.52
19 - 20	74	64	92	71	*	*	*	75.25	0	33.92
20 - 21	69	56	87	67	*	*	*	69.75	0	33.9
21 - 22	48	35	30	51	*	*	*	41	0	32.62
22 - 23	19	14	19	22	*	*	*	18.5	0	32.75
23 - 24	8	8	9	9	*	*	*	8.5	0	32.5
Totals	1263	1824	1845	1953	336	0	0			
% of Total	17.49%	25.26%	25.55%	27.05%	4.65%	0%	0%			

Incoming Weekly Speeds

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	31.17	28.25	29.1	32.75	*	*	29.82	*	34.25
1 - 2	*	29.8	33.33	28.83	28	*	*	29.87	*	32.75
2 - 3	*	29	30	27.6	25.6	*	*	27.23	*	29.75
3 - 4	*	25	25.25	34	26.25	*	*	27.18	*	29
4 - 5	*	38	33.5	34	32	*	*	33.78	*	35.25
5 - 6	*	29.38	30.64	33.1	27.38	*	*	30.32	*	33.38
6 - 7	*	30.29	30.21	30.28	29.29	*	*	30.01	*	34.6
7 - 8	*	23.96	24.55	24.57	23.49	*	*	24.17	*	30.2
8 - 9	*	29.28	28.8	28.47	28.82	*	*	28.85	*	34.15
9 - 10	*	32.22	29.59	30.29	29.97	*	*	30.58	*	34.92
10 - 11	*	30.42	29.58	31.59	*	*	*	30.5	*	34.67
11 - 12	*	29.57	29.36	30.61	*	*	*	29.87	*	34.57
12 - 13	29.34	29.37	29.49	29.52	*	*	*	29.44	*	34.3
13 - 14	29.61	29.89	30.7	31.02	*	*	*	30.28	*	35.08
14 - 15	24.88	23.93	24.05	25.09	*	*	*	24.46	*	30.18
15 - 16	28.82	29.63	29.81	29.07	*	*	*	29.28	*	34.28
16 - 17	29.85	30.5	31.08	30.73	*	*	*	30.54	*	34.52
17 - 18	29.72	30.03	30.56	30.73	*	*	*	30.28	*	34.68
18 - 19	30.06	29.66	32.09	30.84	*	*	*	30.72	*	34.52
19 - 20	29.38	30.31	30.6	30.61	*	*	*	30.24	*	33.92
20 - 21	28.48	29.32	31.03	28.48	*	*	*	29.44	*	33.9
21 - 22	29.12	29	29.4	28.29	*	*	*	28.89	*	32.62
22 - 23	29.05	29.36	28.47	29.91	*	*	*	29.22	*	32.75
23 - 24	29	29.75	26.67	30.22	*	*	*	28.88	*	32.5
Totals	28.8	29	29.3	29.3	27.2	0	0			
% of Total	20.06%	20.19%	20.4%	20.4%	18.94%	0%	0%			

Incoming Monthly Speeds

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	31.17	28.25	29.1	32.75	*	*	29.82	*	34.25
1 - 2	*	29.8	33.33	28.83	28	*	*	29.87	*	32.75
2 - 3	*	29	30	27.6	25.6	*	*	27.23	*	29.75
3 - 4	*	25	25.25	34	26.25	*	*	27.18	*	29
4 - 5	*	38	33.5	34	32	*	*	33.78	*	35.25
5 - 6	*	29.38	30.64	33.1	27.38	*	*	30.32	*	33.38
6 - 7	*	30.29	30.21	30.28	29.29	*	*	30.01	*	34.6
7 - 8	*	23.96	24.55	24.57	23.49	*	*	24.17	*	30.2
8 - 9	*	29.28	28.8	28.47	28.82	*	*	28.85	*	34.15
9 - 10	*	32.22	29.59	30.29	29.97	*	*	30.58	*	34.92
10 - 11	*	30.42	29.58	31.59	*	*	*	30.5	*	34.67
11 - 12	*	29.57	29.36	30.61	*	*	*	29.87	*	34.57
12 - 13	29.34	29.37	29.49	29.52	*	*	*	29.44	*	34.3
13 - 14	29.61	29.89	30.7	31.02	*	*	*	30.28	*	35.08
14 - 15	24.88	23.93	24.05	25.09	*	*	*	24.46	*	30.18
15 - 16	28.82	29.63	29.81	29.07	*	*	*	29.28	*	34.28
16 - 17	29.85	30.5	31.08	30.73	*	*	*	30.54	*	34.52
17 - 18	29.72	30.03	30.56	30.73	*	*	*	30.28	*	34.68
18 - 19	30.06	29.66	32.09	30.84	*	*	*	30.72	*	34.52
19 - 20	29.38	30.31	30.6	30.61	*	*	*	30.24	*	33.92
20 - 21	28.48	29.32	31.03	28.48	*	*	*	29.44	*	33.9
21 - 22	29.12	29	29.4	28.29	*	*	*	28.89	*	32.62
22 - 23	29.05	29.36	28.47	29.91	*	*	*	29.22	*	32.75
23 - 24	29	29.75	26.67	30.22	*	*	*	28.88	*	32.5
Totals	28.8	29	29.3	29.3	27.2	0	0			
% of Total	20.06%	20.19%	20.4%	20.4%	18.94%	0%	0%			

Incoming Weekly EightyFifthSpeeds

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	38	34	31	34	*	*	34.25	0	34.25
1 - 2	*	30	37	36	28	*	*	32.75	0	32.75
2 - 3	*	30	30	28	31	*	*	29.75	0	29.75
3 - 4	*	25	32	34	25	*	*	29	0	29
4 - 5	*	38	34	35	34	*	*	35.25	0	35.25
5 - 6	*	34	31	36.5	32	*	*	33.38	0	33.38
6 - 7	*	35.5	34	34.7	34.2	*	*	34.6	0	34.6
7 - 8	*	30	31	31	28.8	*	*	30.2	0	30.2
8 - 9	*	35	34.3	33.6	33.7	*	*	34.15	0	34.15
9 - 10	*	35.8	35.2	34	34.7	*	*	34.92	0	34.92
10 - 11	*	34.8	33.7	35.5	*	*	*	34.67	0	34.67
11 - 12	*	34	34	35.7	*	*	*	34.57	0	34.57
12 - 13	34.4	34.1	34.2	34.5	*	*	*	34.3	0	34.3
13 - 14	34.1	34.7	35.5	36	*	*	*	35.08	0	35.08
14 - 15	30.6	30.2	28.7	31.2	*	*	*	30.18	0	30.18
15 - 16	34.1	34.6	34.6	33.8	*	*	*	34.28	0	34.28
16 - 17	34	34.7	34.8	34.6	*	*	*	34.52	0	34.52
17 - 18	34.6	34.2	34.8	35.1	*	*	*	34.68	0	34.68
18 - 19	34.2	33.8	35.7	34.4	*	*	*	34.52	0	34.52
19 - 20	33.6	33	35.4	33.7	*	*	*	33.92	0	33.92
20 - 21	33.2	33.4	34.8	34.2	*	*	*	33.9	0	33.9
21 - 22	32.5	31.7	33	33.3	*	*	*	32.62	0	32.62
22 - 23	33	32	31	35	*	*	*	32.75	0	32.75
23 - 24	31	34	31	34	*	*	*	32.5	0	32.5
Totals	399.3	800.5	803.7	814.8	315.4	0	0			
% of Total	12.74%	25.54%	25.65%	26%	10.06%	0%	0%			

Incoming Monthly EightyFifthSpeeds
 Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	38	34	31	34	*	*	34.25	0	34.25
1 - 2	*	30	37	36	28	*	*	32.75	0	32.75
2 - 3	*	30	30	28	31	*	*	29.75	0	29.75
3 - 4	*	25	32	34	25	*	*	29	0	29
4 - 5	*	38	34	35	34	*	*	35.25	0	35.25
5 - 6	*	34	31	36.5	32	*	*	33.38	0	33.38
6 - 7	*	35.5	34	34.7	34.2	*	*	34.6	0	34.6
7 - 8	*	30	31	31	28.8	*	*	30.2	0	30.2
8 - 9	*	35	34.3	33.6	33.7	*	*	34.15	0	34.15
9 - 10	*	35.8	35.2	34	34.7	*	*	34.92	0	34.92
10 - 11	*	34.8	33.7	35.5	*	*	*	34.67	0	34.67
11 - 12	*	34	34	35.7	*	*	*	34.57	0	34.57
12 - 13	34.4	34.1	34.2	34.5	*	*	*	34.3	0	34.3
13 - 14	34.1	34.7	35.5	36	*	*	*	35.08	0	35.08
14 - 15	30.6	30.2	28.7	31.2	*	*	*	30.18	0	30.18
15 - 16	34.1	34.6	34.6	33.8	*	*	*	34.28	0	34.28
16 - 17	34	34.7	34.8	34.6	*	*	*	34.52	0	34.52
17 - 18	34.6	34.2	34.8	35.1	*	*	*	34.68	0	34.68
18 - 19	34.2	33.8	35.7	34.4	*	*	*	34.52	0	34.52
19 - 20	33.6	33	35.4	33.7	*	*	*	33.92	0	33.92
20 - 21	33.2	33.4	34.8	34.2	*	*	*	33.9	0	33.9
21 - 22	32.5	31.7	33	33.3	*	*	*	32.62	0	32.62
22 - 23	33	32	31	35	*	*	*	32.75	0	32.75
23 - 24	31	34	31	34	*	*	*	32.5	0	32.5

Summary of Violators

Birchwood Loop northwest northwest 9-16 to 9-20 time from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM
correct

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	28	29.8	0	0.0
01:00:00	16	29.9	0	0.0
02:00:00	13	27.2	0	0.0
03:00:00	11	27.2	0	0.0
04:00:00	9	33.8	0	0.0
05:00:00	37	30.3	0	0.0
06:00:00	162	30.0	0	0.0
07:00:00	481	24.2	0	0.0
08:00:00	379	28.8	0	0.0
09:00:00	330	30.6	0	0.0
10:00:00	250	30.5	0	0.0
11:00:00	279	29.9	2	48.0
12:00:00	406	29.4	0	0.0
13:00:00	528	30.3	3	46.3
14:00:00	710	24.5	1	55.0
15:00:00	727	29.3	0	0.0
16:00:00	692	30.5	0	0.0
17:00:00	756	30.3	0	0.0
18:00:00	555	30.7	1	50.0
19:00:00	301	30.2	1	46.0
20:00:00	279	29.4	0	0.0
21:00:00	164	28.9	0	0.0
22:00:00	74	29.2	0	0.0
23:00:00	34	28.9	0	0.0

Incoming Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	2	1	6	28	24	13	0	0	0	0	0	0	0	0	0	0	0	0	74	29.3	34.4	26 to 36	78.4	*	*	3	61	10	
9/16/2024	13:00	2	12	8	39	63	18	3	2	0	0	0	0	0	0	0	0	0	0	147	29.6	34.1	25 to 35	75.5	1	0.7	1	131	15	
9/16/2024	14:00	4	31	54	38	37	9	0	0	0	0	0	0	0	0	0	0	0	0	173	24.9	30.6	21 to 31	61.8	*	*	1	163	9	
9/16/2024	15:00	3	10	27	56	65	27	2	0	0	0	0	0	0	0	0	0	0	0	190	28.8	34.1	23 to 33	69.5	*	*	0	176	14	
9/16/2024	16:00	2	8	15	44	79	23	3	0	0	0	0	0	0	0	0	0	0	0	174	29.9	34	26 to 36	77.0	*	*	0	166	8	
9/16/2024	17:00	4	5	13	52	66	31	3	0	0	0	0	0	0	0	0	0	0	0	174	29.7	34.6	26 to 36	77.0	*	*	0	170	4	
9/16/2024	18:00	1	6	8	30	49	16	1	1	1	0	0	0	0	0	0	0	0	0	113	30.1	34.2	26 to 36	77.9	1	0.9	0	108	5	
9/16/2024	19:00	0	3	12	20	31	6	1	1	0	0	0	0	0	0	0	0	0	0	74	29.4	33.6	26 to 36	74.3	1	1.4	0	74	0	
9/16/2024	20:00	1	3	11	24	23	6	1	0	0	0	0	0	0	0	0	0	0	0	69	28.5	33.2	24 to 34	75.4	*	*	1	68	0	
9/16/2024	21:00	0	0	4	21	19	3	1	0	0	0	0	0	0	0	0	0	0	0	48	29.1	32.5	24 to 34	87.5	*	*	0	48	0	
9/16/2024	22:00	0	0	3	7	6	3	0	0	0	0	0	0	0	0	0	0	0	0	19	29.1	33	26 to 36	84.2	*	*	0	19	0	
9/16/2024	23:00	0	0	1	3	4	0	0	0	0	0	0	0	0	0	0	0	0	0	8	29	31	24 to 34	100.0	*	*	0	7	1	
24 Hr Summary		19	79	162	362	466	155	15	4	1	0	0	0	0	0	0	0	0	0	1263	28.8	34	25 to 35	70.5	3	0.2	6	1191	66	

Incoming Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/17/2024	00:00	0	0	2	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	6	31.2	38	20 to 30	50.0	*	*	0	5	1
9/17/2024	01:00	0	0	0	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	5	29.8	30	22 to 32	100.0	*	*	0	5	0
9/17/2024	02:00	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	2	29	30	20 to 30	100.0	*	*	0	2	0
9/17/2024	03:00	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	25	25	15 to 25	100.0	*	*	0	1	0
9/17/2024	04:00	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	38	38	28 to 38	100.0	*	*	0	1	0
9/17/2024	05:00	0	0	1	3	3	1	0	0	0	0	0	0	0	0	0	0	0	0	8	29.4	34	20 to 30	75.0	*	*	0	8	0
9/17/2024	06:00	1	1	2	11	15	7	1	0	0	0	0	0	0	0	0	0	0	0	38	30.3	35.5	24 to 34	73.7	*	*	0	36	2
9/17/2024	07:00	3	20	45	16	13	5	2	0	0	0	0	0	0	0	0	0	0	0	104	24	30	17 to 27	68.3	*	*	0	100	4
9/17/2024	08:00	2	6	13	24	40	14	5	0	0	0	0	0	0	0	0	0	0	0	104	29.3	35	26 to 36	67.3	*	*	4	88	12
9/17/2024	09:00	0	1	4	16	40	22	4	0	0	0	0	0	0	0	0	0	0	0	87	32.2	35.8	27 to 37	85.1	*	*	6	69	12
9/17/2024	10:00	1	5	3	19	36	11	4	0	0	0	0	0	0	0	0	0	0	0	79	30.4	34.8	26 to 36	75.9	*	*	4	74	1
9/17/2024	11:00	0	6	5	27	42	14	0	0	0	0	0	0	0	0	0	0	0	0	94	29.6	34	25 to 35	79.8	*	*	1	87	6
9/17/2024	12:00	5	7	9	27	54	17	2	0	0	0	0	0	0	0	0	0	0	0	121	29.4	34.1	27 to 37	76.9	*	*	0	114	7
9/17/2024	13:00	2	11	8	29	60	18	3	1	0	0	0	0	0	0	0	0	0	0	132	29.9	34.7	27 to 37	75.0	1	0.8	3	117	12
9/17/2024	14:00	7	42	58	51	30	7	0	0	0	0	0	0	0	0	0	0	0	0	195	23.9	30.2	17 to 27	64.1	*	*	2	186	7
9/17/2024	15:00	1	10	16	42	60	23	6	0	0	0	0	0	0	0	0	0	0	0	158	29.6	34.6	25 to 35	70.3	*	*	0	153	5
9/17/2024	16:00	0	2	17	57	67	29	7	0	0	0	0	0	0	0	0	0	0	0	179	30.5	34.7	25 to 35	76.5	*	*	0	174	5
9/17/2024	17:00	2	7	8	58	79	28	3	0	0	0	0	0	0	0	0	0	0	0	185	30	34.2	25 to 35	80.5	*	*	0	181	4
9/17/2024	18:00	2	8	11	39	69	18	1	0	0	0	0	0	0	0	0	0	0	0	148	29.7	33.8	26 to 36	78.4	*	*	0	143	5
9/17/2024	19:00	0	2	3	18	31	9	1	0	0	0	0	0	0	0	0	0	0	0	64	30.3	33	25 to 35	82.8	*	*	0	62	2
9/17/2024	20:00	1	0	4	21	26	4	0	0	0	0	0	0	0	0	0	0	0	0	56	29.3	33.4	25 to 35	87.5	*	*	0	56	0
9/17/2024	21:00	0	0	5	13	16	1	0	0	0	0	0	0	0	0	0	0	0	0	35	29	31.7	23 to 33	94.3	*	*	0	35	0
9/17/2024	22:00	0	0	2	5	5	2	0	0	0	0	0	0	0	0	0	0	0	0	14	29.4	32	22 to 32	85.7	*	*	0	14	0
9/17/2024	23:00	0	0	2	2	3	1	0	0	0	0	0	0	0	0	0	0	0	0	8	29.8	34	24 to 34	75.0	*	*	0	8	0
24 Hr Summary		27	128	218	481	696	233	40	1	0	0	0	0	0	0	0	0	0	0	1824	29	34	25 to 35	69.2	1	0.1	20	1719	85

Incoming Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/18/2024	00:00	0	0	3	1	3	1	0	0	0	0	0	0	0	0	0	0	0	0	8	28.3	34	21 to 31	75.0	*	*	0	8	0
9/18/2024	01:00	0	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	3	33.3	37	27 to 37	100.0	*	*	0	3	0
9/18/2024	02:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	1	30	30	20 to 30	100.0	*	*	0	1	0
9/18/2024	03:00	0	1	1	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	4	25.3	32	11 to 21	50.0	*	*	0	4	0
9/18/2024	04:00	0	0	0	0	3	1	0	0	0	0	0	0	0	0	0	0	0	0	4	33.5	34	25 to 35	100.0	*	*	0	4	0
9/18/2024	05:00	0	0	0	3	6	2	0	0	0	0	0	0	0	0	0	0	0	0	11	30.6	31	25 to 35	90.9	*	*	0	10	1
9/18/2024	06:00	1	2	2	6	26	5	1	0	0	0	0	0	0	0	0	0	0	0	43	30.2	34	25 to 35	83.7	*	*	0	41	2
9/18/2024	07:00	5	17	45	23	22	8	1	0	0	0	0	0	0	0	0	0	0	0	121	24.6	31	20 to 30	61.2	*	*	0	121	0
9/18/2024	08:00	0	4	20	22	28	13	2	0	0	0	0	0	0	0	0	0	0	0	89	28.8	34.3	27 to 37	65.2	*	*	0	86	3
9/18/2024	09:00	1	5	6	21	27	11	3	0	0	0	0	0	0	0	0	0	0	0	74	29.6	35.2	26 to 36	71.6	*	*	0	69	5
9/18/2024	10:00	1	4	9	26	38	10	1	0	0	0	0	0	0	0	0	0	0	0	89	29.6	33.7	26 to 36	78.7	*	*	2	82	5
9/18/2024	11:00	2	4	6	28	35	11	0	2	0	0	0	0	0	0	0	0	0	0	88	29.4	34	25 to 35	75.0	2	2.3	0	86	2
9/18/2024	12:00	2	5	8	32	42	15	2	0	0	0	0	0	0	0	0	0	0	0	106	29.5	34.2	25 to 35	75.5	*	*	0	102	4
9/18/2024	13:00	0	5	10	27	47	20	4	1	0	0	0	0	0	0	0	0	0	0	114	30.7	35.5	26 to 36	71.9	1	0.9	0	107	7
9/18/2024	14:00	2	29	72	50	20	4	1	0	0	0	0	0	0	0	0	0	0	0	178	24.1	28.7	18 to 28	73.0	*	*	0	174	4
9/18/2024	15:00	2	7	10	44	67	25	1	0	0	0	0	0	0	0	0	0	0	0	156	29.8	34.6	26 to 36	77.6	*	*	0	147	9
9/18/2024	16:00	1	4	5	45	84	28	7	0	0	0	0	0	0	0	0	0	0	0	174	31.1	34.8	26 to 36	81.6	*	*	0	170	4
9/18/2024	17:00	1	2	23	39	79	38	3	0	0	0	0	0	0	0	0	0	0	0	185	30.6	34.8	26 to 36	74.6	*	*	0	180	5
9/18/2024	18:00	1	0	10	29	72	40	7	1	0	0	0	0	0	0	0	0	0	0	160	32.1	35.7	26 to 36	78.1	*	*	0	155	5
9/18/2024	19:00	1	2	5	26	38	18	1	1	0	0	0	0	0	0	0	0	0	0	92	30.6	35.4	26 to 36	75.0	*	*	0	90	2
9/18/2024	20:00	1	0	3	28	37	15	2	1	0	0	0	0	0	0	0	0	0	0	87	31	34.8	26 to 36	83.9	*	*	0	86	1
9/18/2024	21:00	0	0	4	11	12	3	0	0	0	0	0	0	0	0	0	0	0	0	30	29.4	33	26 to 36	86.7	*	*	0	30	0
9/18/2024	22:00	0	0	3	9	6	1	0	0	0	0	0	0	0	0	0	0	0	0	19	28.5	31	23 to 33	89.5	*	*	0	19	0
9/18/2024	23:00	1	1	0	4	2	1	0	0	0	0	0	0	0	0	0	0	0	0	9	26.7	31	26 to 36	77.8	*	*	0	9	0
24 Hr Summary		22	92	245	474	699	271	36	6	0	0	0	0	0	0	0	0	0	0	1845	29.3	35	26 to 36	69.5	3	0.2	2	1784	59

Incoming Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/19/2024	00:00	0	0	0	7	2	1	0	0	0	0	0	0	0	0	0	0	0	0	10	29.1	31	25 to 35	100.0	*	*	0	10	0
9/19/2024	01:00	0	1	1	2	0	1	1	0	0	0	0	0	0	0	0	0	0	0	6	28.8	36	18 to 28	66.7	*	*	0	5	1
9/19/2024	02:00	0	0	0	4	1	0	0	0	0	0	0	0	0	0	0	0	0	0	5	27.6	28	20 to 30	100.0	*	*	0	5	0
9/19/2024	03:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	34	34	24 to 34	100.0	*	*	0	2	0
9/19/2024	04:00	0	0	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	2	34	35	25 to 35	100.0	*	*	0	2	0
9/19/2024	05:00	0	0	0	1	5	4	0	0	0	0	0	0	0	0	0	0	0	0	10	33.1	36.5	27 to 37	90.0	*	*	0	9	1
9/19/2024	06:00	1	2	2	8	18	7	1	0	0	0	0	0	0	0	0	0	0	0	39	30.3	34.7	27 to 37	82.1	*	*	0	38	1
9/19/2024	07:00	2	27	49	29	21	10	1	0	0	0	0	0	0	0	0	0	0	0	139	24.6	31	16 to 26	63.3	*	*	0	135	4
9/19/2024	08:00	0	8	16	29	34	10	2	0	0	0	0	0	0	0	0	0	0	0	99	28.5	33.6	25 to 35	68.7	*	*	0	97	2
9/19/2024	09:00	2	1	5	30	50	15	1	0	0	0	0	0	0	0	0	0	0	0	104	30.3	34	25 to 35	79.8	*	*	1	97	6
9/19/2024	10:00	1	4	4	12	34	23	4	0	0	0	0	0	0	0	0	0	0	0	82	31.6	35.5	27 to 37	78.0	*	*	1	78	3
9/19/2024	11:00	0	6	7	22	39	22	1	0	0	0	0	0	0	0	0	0	0	0	97	30.6	35.7	26 to 36	71.1	*	*	1	89	7
9/19/2024	12:00	0	6	11	30	39	16	3	0	0	0	0	0	0	0	0	0	0	0	105	29.5	34.5	25 to 35	71.4	*	*	0	104	1
9/19/2024	13:00	0	5	8	33	58	28	3	0	0	0	0	0	0	0	0	0	0	0	135	31	36	27 to 37	74.8	*	*	0	132	3
9/19/2024	14:00	4	30	51	35	35	6	2	0	0	1	0	0	0	0	0	0	0	0	164	25.1	31.2	18 to 28	60.4	1	0.6	3	155	6
9/19/2024	15:00	0	9	25	84	74	28	3	0	0	0	0	0	0	0	0	0	0	0	223	29.1	33.8	25 to 35	76.7	*	*	1	215	7
9/19/2024	16:00	1	5	7	46	74	27	4	1	0	0	0	0	0	0	0	0	0	0	165	30.7	34.6	26 to 36	80.6	*	*	1	159	5
9/19/2024	17:00	3	5	12	52	96	40	4	0	0	0	0	0	0	0	0	0	0	0	212	30.7	35.1	26 to 36	79.7	*	*	0	210	2
9/19/2024	18:00	0	4	7	30	69	23	1	0	0	0	0	0	0	0	0	0	0	0	134	30.8	34.4	25 to 35	80.6	*	*	0	130	4
9/19/2024	19:00	1	2	3	15	41	6	3	0	0	0	0	0	0	0	0	0	0	0	71	30.6	33.7	26 to 36	84.5	*	*	0	71	0
9/19/2024	20:00	3	3	8	22	20	11	0	0	0	0	0	0	0	0	0	0	0	0	67	28.5	34.2	26 to 36	76.1	*	*	0	67	0
9/19/2024	21:00	0	2	8	19	18	4	0	0	0	0	0	0	0	0	0	0	0	0	51	28.3	33.3	25 to 35	80.4	*	*	0	50	1
9/19/2024	22:00	0	1	2	5	10	4	0	0	0	0	0	0	0	0	0	0	0	0	22	29.9	35	24 to 34	72.7	*	*	0	22	0
9/19/2024	23:00	0	0	0	4	4	1	0	0	0	0	0	0	0	0	0	0	0	0	9	30.2	34	24 to 34	88.9	*	*	0	9	0
24 Hr Summary		18	121	226	519	745	288	34	1	0	1	0	0	0	0	0	0	0	0	1953	29.3	35	25 to 35	69.9	1	0.1	8	1891	54

Incoming Histogram

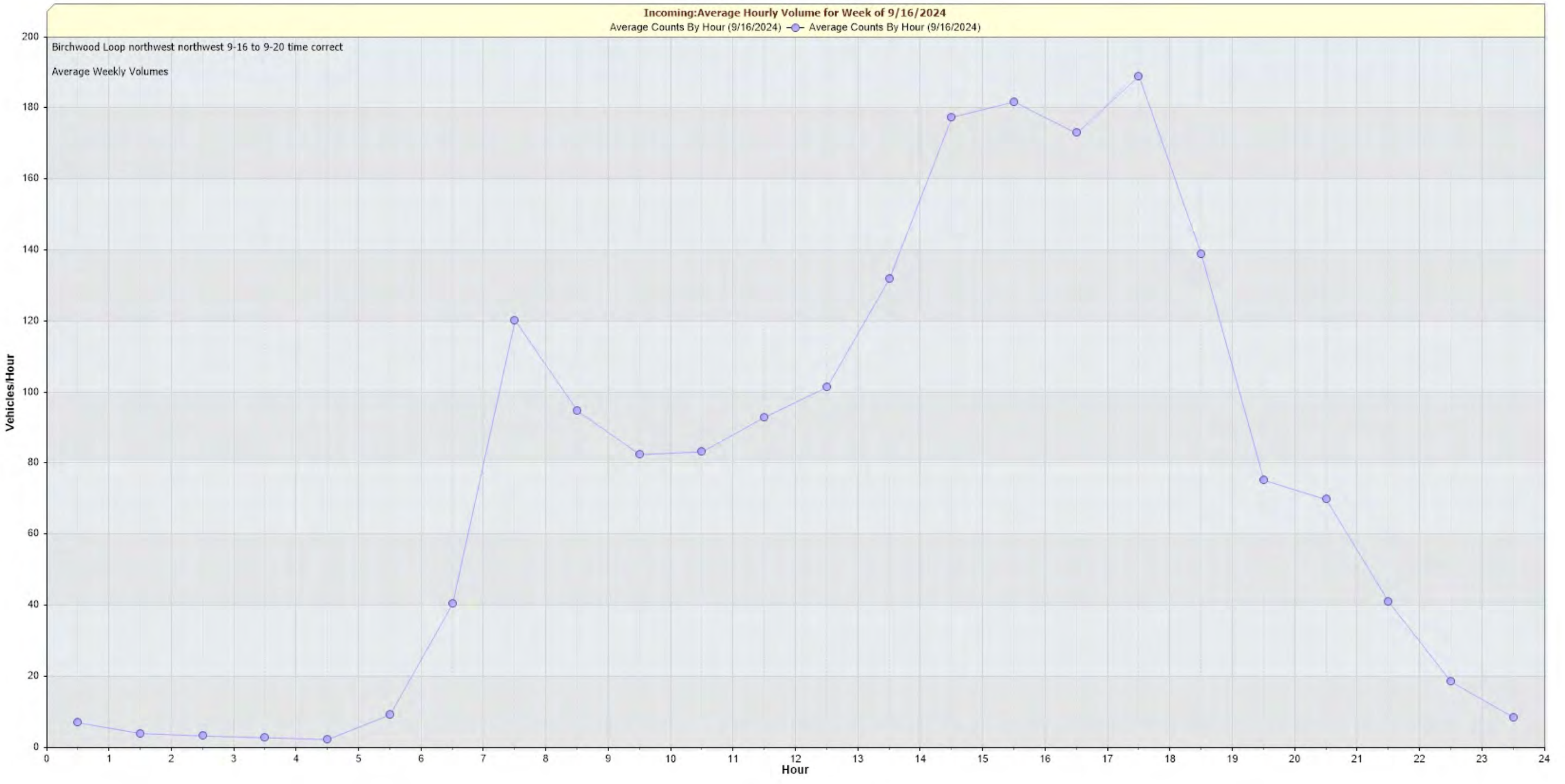
Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/20/2024	00:00	0	0	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	4	32.8	34	27 to 37	100.0	*	*	0	4	0	
9/20/2024	01:00	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	28	28	18 to 28	100.0	*	*	0	2	0	
9/20/2024	02:00	0	1	2	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	5	25.6	31	13 to 23	60.0	*	*	0	5	0	
9/20/2024	03:00	0	0	2	1	0	1	0	0	0	0	0	0	0	0	0	0	0	0	4	26.3	25	15 to 25	75.0	*	*	0	4	0	
9/20/2024	04:00	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	2	32	34	24 to 34	100.0	*	*	0	2	0	
9/20/2024	05:00	0	0	2	3	2	1	0	0	0	0	0	0	0	0	0	0	0	0	8	27.4	32	22 to 32	75.0	*	*	0	8	0	
9/20/2024	06:00	2	2	5	9	17	7	0	0	0	0	0	0	0	0	0	0	0	0	42	29.3	34.2	27 to 37	76.2	*	*	0	40	2	
9/20/2024	07:00	3	20	52	25	11	6	0	0	0	0	0	0	0	0	0	0	0	0	117	23.5	28.8	18 to 28	73.5	*	*	0	116	1	
9/20/2024	08:00	0	5	15	27	29	11	0	0	0	0	0	0	0	0	0	0	0	0	87	28.8	33.7	23 to 33	71.3	*	*	0	80	7	
9/20/2024	09:00	0	1	8	19	25	10	2	0	0	0	0	0	0	0	0	0	0	0	65	30	34.7	25 to 35	72.3	*	*	2	60	3	
9/20/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	12:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		5	29	86	87	89	38	2	0	0	0	0	0	0	0	0	0	0	0	336	27.2	34	20 to 30	58.6	*	*	2	321	13	

Incoming: Average Hourly Volume for Week of 9/16/2024
Average Counts By Hour (9/16/2024) — Average Counts By Hour (9/16/2024)

Birchwood Loop northwest northwest 9-16 to 9-20 time correct
Average Weekly Volumes



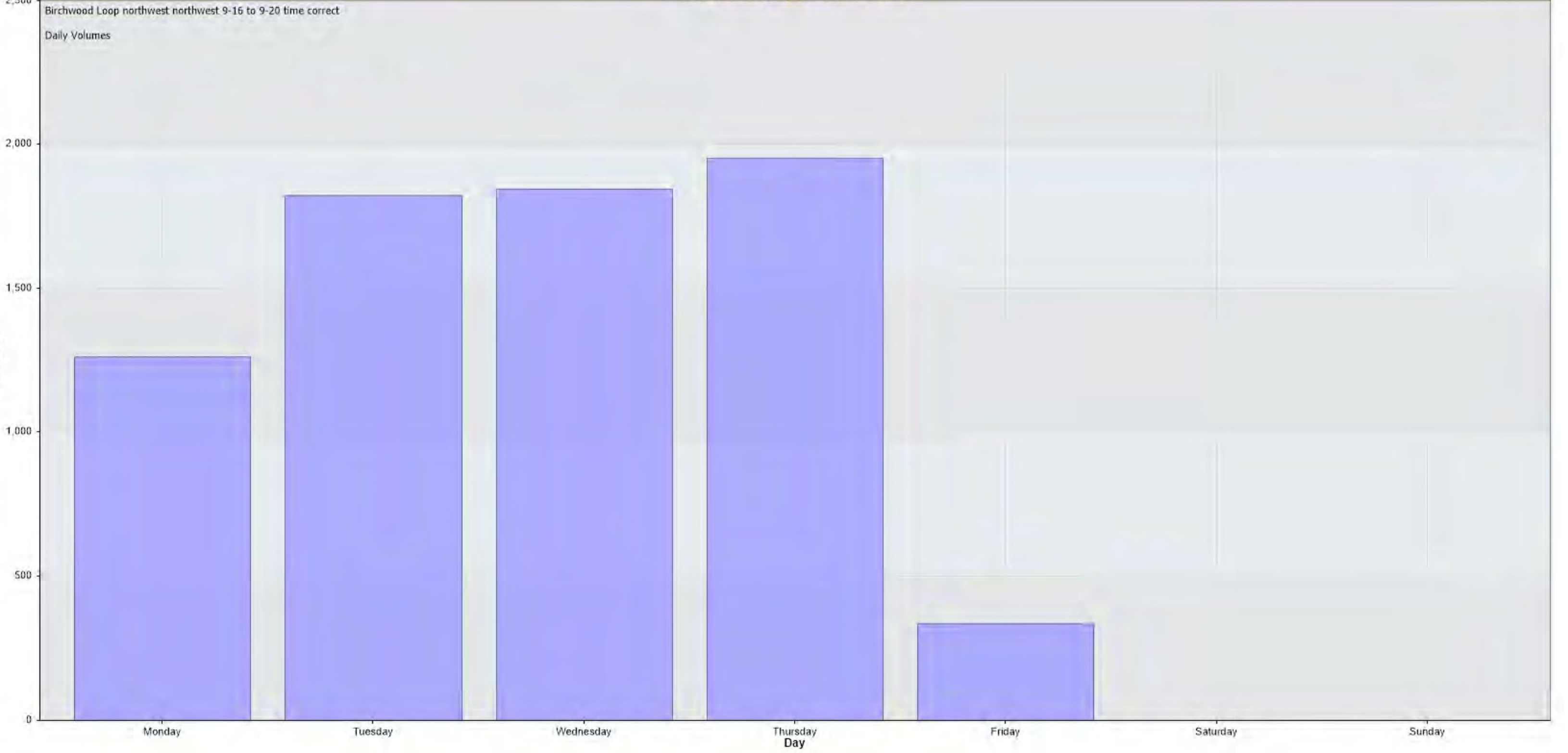
Incoming: Average Hourly WEEKDAY Speeds for Week of 9/16/2024
Average Hourly WEEKDAY Speeds By Hour (9/16/2024) — Average Hourly WEEKDAY Speeds By Hour (9/16/2024)

Birchwood Loop northwest northwest 9-16 to 9-20 time correct
Average Weekly Speeds



Incoming: Daily Volume for Week of 9/16/2024

Daily Vehicle Counts



For Project: Birchwood Loop northwest northwest 9-16 to 9-20 time correct
 Project Notes:
 Location/Name: Outgoing
 Report Generated: 10/3/2024 3:59:16 PM
 Speed Intervals: 1 MPH
 Time Intervals: Instant
 Traffic Report From: 9/16/2024 12:00:00 PM through 9/20/2024 9:59:59 AM
 85th Percentile Speed: 31 MPH
 85th Percentile Vehicles: 5092
 Max Speed: 49 MPH on 9/16/2024 3:32:07 PM
 Total Vehicles: 5990
 AADT: 1529

Volumes - weekly counts

Time	5 Day	7 Day
Average Daily	1198	1198
AM Peak	7:00 AM 105	105
PM Peak	5:00 PM 145	145

Speed

Speed Limit: 45
 85th Percentile Speed: 31
 50th Percentile Speed: 27
 10 MPH Pace Interval: 22.0 MPH to 32.0 MPH
 Average Speed: 27.01

	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday
Count over limit	1	0	0	1	0	N/A	N/A
% over limit	0.1	0.0	0.0	0.1	0.0	N/A	N/A
Avg Speeder	49.0	0.0	0.0	46.0	0.0	N/A	N/A
Avg Speed	26.9	27.0	27.1	27.1	26.3	N/A	N/A

Class Counts

	Number	%
VEH_SM	5	0.1
VEH_MED	5576	93.1
VEH_LG	409	6.8
[VEH_SM=motorcycle,	VEH_MED = sedan,	VEH_LG = truck]

Outgoing Summary
Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/16/2024 1:00:00 PM	31.0	67	79	34	0.0	0.0%	26.7
9/16/2024 2:00:00 PM	31.0	115	135	37	0.0	0.0%	27.2
9/16/2024 3:00:00 PM	27.0	94	111	32	0.0	0.0%	23.3
9/16/2024 4:00:00 PM	32.0	98	115	49	49.0	0.9%	27.8
9/16/2024 5:00:00 PM	31.0	106	125	42	0.0	0.0%	27.5
9/16/2024 6:00:00 PM	32.0	119	140	38	0.0	0.0%	28.1
9/16/2024 7:00:00 PM	32.0	80	94	37	0.0	0.0%	27.9
9/16/2024 8:00:00 PM	31.0	47	55	35	0.0	0.0%	26.5
9/16/2024 9:00:00 PM	30.0	32	38	34	0.0	0.0%	26.3
9/16/2024 10:00:00 PM	31.0	20	23	39	0.0	0.0%	26.8
9/16/2024 11:00:00 PM	29.0	14	17	33	0.0	0.0%	26.0
9/17/2024 12:00:00 AM	27.0	5	6	28	0.0	0.0%	24.5
9/17/2024 1:00:00 AM	24.0	3	4	28	0.0	0.0%	23.8
9/17/2024 2:00:00 AM	29.0	3	3	29	0.0	0.0%	25.3
9/17/2024 3:00:00 AM	28.0	3	3	28	0.0	0.0%	26.7
9/17/2024 4:00:00 AM	34.0	3	3	34	0.0	0.0%	29.0
9/17/2024 5:00:00 AM	26.0	3	4	37	0.0	0.0%	28.3
9/17/2024 6:00:00 AM	28.0	8	10	31	0.0	0.0%	26.1
9/17/2024 7:00:00 AM	29.0	60	71	35	0.0	0.0%	26.5
9/17/2024 8:00:00 AM	29.0	94	111	34	0.0	0.0%	25.0
9/17/2024 9:00:00 AM	30.0	87	102	38	0.0	0.0%	25.6
9/17/2024 10:00:00 AM	31.0	89	105	37	0.0	0.0%	26.3
9/17/2024 11:00:00 AM	30.0	84	99	37	0.0	0.0%	26.8
9/17/2024 12:00:00 PM	30.0	86	101	38	0.0	0.0%	26.2
9/17/2024 1:00:00 PM	32.0	80	94	40	0.0	0.0%	27.3
9/17/2024 2:00:00 PM	31.0	101	119	41	0.0	0.0%	26.7
9/17/2024 3:00:00 PM	30.0	76	90	39	0.0	0.0%	24.8
9/17/2024 4:00:00 PM	32.0	98	115	39	0.0	0.0%	27.4
9/17/2024 5:00:00 PM	32.0	129	152	37	0.0	0.0%	28.2
9/17/2024 6:00:00 PM	33.0	123	145	41	0.0	0.0%	29.0
9/17/2024 7:00:00 PM	32.0	63	74	38	0.0	0.0%	28.6
9/17/2024 8:00:00 PM	33.0	59	69	41	0.0	0.0%	28.6
9/17/2024 9:00:00 PM	29.0	27	32	36	0.0	0.0%	26.5
9/17/2024 10:00:00 PM	30.0	23	27	33	0.0	0.0%	26.9
9/17/2024 11:00:00 PM	30.0	11	13	37	0.0	0.0%	27.4
9/18/2024 12:00:00 AM	32.0	3	3	32	0.0	0.0%	28.7
9/18/2024 1:00:00 AM	27.0	4	5	29	0.0	0.0%	26.2
9/18/2024 2:00:00 AM	29.0	3	4	35	0.0	0.0%	30.0
9/18/2024 3:00:00 AM	32.0	3	3	32	0.0	0.0%	27.7
9/18/2024 4:00:00 AM	31.0	3	3	31	0.0	0.0%	26.7
9/18/2024 5:00:00 AM	30.0	9	11	32	0.0	0.0%	27.4
9/18/2024 6:00:00 AM	30.0	14	16	33	0.0	0.0%	27.0
9/18/2024 7:00:00 AM	31.0	64	75	43	0.0	0.0%	27.5
9/18/2024 8:00:00 AM	28.0	81	95	33	0.0	0.0%	25.2
9/18/2024 9:00:00 AM	31.0	84	99	38	0.0	0.0%	26.6
9/18/2024 10:00:00 AM	33.0	79	93	39	0.0	0.0%	27.1
9/18/2024 11:00:00 AM	30.0	60	71	34	0.0	0.0%	26.4

Outgoing Summary
Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

9/18/2024 12:00:00 PM	30.0	77	91	38	0.0	0.0%	26.5
9/18/2024 1:00:00 PM	31.0	71	84	36	0.0	0.0%	27.8
9/18/2024 2:00:00 PM	31.0	93	109	41	0.0	0.0%	27.4
9/18/2024 3:00:00 PM	29.0	88	103	38	0.0	0.0%	24.5
9/18/2024 4:00:00 PM	31.0	101	119	44	0.0	0.0%	27.3
9/18/2024 5:00:00 PM	31.0	100	118	39	0.0	0.0%	27.7
9/18/2024 6:00:00 PM	33.0	136	160	41	0.0	0.0%	28.5
9/18/2024 7:00:00 PM	33.0	81	95	41	0.0	0.0%	28.5
9/18/2024 8:00:00 PM	31.0	67	79	36	0.0	0.0%	27.9
9/18/2024 9:00:00 PM	32.0	67	79	36	0.0	0.0%	27.6
9/18/2024 10:00:00 PM	29.0	15	18	32	0.0	0.0%	25.7
9/18/2024 11:00:00 PM	32.0	14	16	36	0.0	0.0%	28.8
9/19/2024 12:00:00 AM	30.0	13	15	31	0.0	0.0%	26.1
9/19/2024 1:00:00 AM	33.0	4	5	46	46.0	20.0%	32.6
9/19/2024 2:00:00 AM	29.0	3	4	30	0.0	0.0%	25.3
9/19/2024 3:00:00 AM	28.0	8	9	29	0.0	0.0%	24.2
9/19/2024 4:00:00 AM	40.0	3	3	40	0.0	0.0%	34.0
9/19/2024 5:00:00 AM	31.0	7	8	32	0.0	0.0%	28.0
9/19/2024 6:00:00 AM	28.0	13	15	32	0.0	0.0%	26.9
9/19/2024 7:00:00 AM	30.0	62	73	36	0.0	0.0%	27.2
9/19/2024 8:00:00 AM	30.0	94	110	37	0.0	0.0%	25.7
9/19/2024 9:00:00 AM	32.0	97	114	40	0.0	0.0%	27.7
9/19/2024 10:00:00 AM	30.0	72	85	35	0.0	0.0%	26.8
9/19/2024 11:00:00 AM	31.0	71	84	34	0.0	0.0%	27.2
9/19/2024 12:00:00 PM	31.0	96	113	38	0.0	0.0%	27.3
9/19/2024 1:00:00 PM	30.0	91	107	37	0.0	0.0%	26.6
9/19/2024 2:00:00 PM	31.0	82	96	36	0.0	0.0%	26.5
9/19/2024 3:00:00 PM	29.0	94	110	39	0.0	0.0%	24.1
9/19/2024 4:00:00 PM	32.0	103	121	41	0.0	0.0%	27.4
9/19/2024 5:00:00 PM	32.0	138	162	41	0.0	0.0%	28.3
9/19/2024 6:00:00 PM	33.0	115	135	39	0.0	0.0%	29.0
9/19/2024 7:00:00 PM	33.0	79	93	41	0.0	0.0%	28.2
9/19/2024 8:00:00 PM	33.0	58	68	37	0.0	0.0%	27.8
9/19/2024 9:00:00 PM	31.0	29	34	34	0.0	0.0%	26.7
9/19/2024 10:00:00 PM	31.0	35	41	35	0.0	0.0%	26.5
9/19/2024 11:00:00 PM	28.0	10	12	31	0.0	0.0%	25.9
9/20/2024 12:00:00 AM	29.0	8	10	29	0.0	0.0%	25.2
9/20/2024 1:00:00 AM	29.0	5	6	40	0.0	0.0%	27.7
9/20/2024 2:00:00 AM	26.0	2	2	26	0.0	0.0%	25.0
9/20/2024 3:00:00 AM	23.0	3	4	24	0.0	0.0%	20.5
9/20/2024 4:00:00 AM	33.0	1	1	33	0.0	0.0%	33.0
9/20/2024 5:00:00 AM	31.0	4	5	34	0.0	0.0%	28.0
9/20/2024 6:00:00 AM	28.0	8	10	29	0.0	0.0%	26.4
9/20/2024 7:00:00 AM	30.0	42	50	34	0.0	0.0%	26.8
9/20/2024 8:00:00 AM	30.0	88	104	37	0.0	0.0%	26.2
9/20/2024 9:00:00 AM	31.0	69	81	33	0.0	0.0%	26.8
9/20/2024 10:00:00 AM	29.0	57	67	37	0.0	0.0%	25.7

Outgoing Summary

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Day/Time Ending	85th pctl (MPH)	85th pctl cnts	Total Cnts	Max Speed	Avg Speeder	% Speeders	Avg Speed
9/17/2024 12:00:00 AM	31.0	797	938	49	49.0	0.1%	26.9
9/18/2024 12:00:00 AM	31.0	1317	1549	41	0.0	0.0%	27.0
9/19/2024 12:00:00 AM	31.0	1327	1561	44	0.0	0.0%	27.1
9/20/2024 12:00:00 AM	32.0	1370	1612	46	46.0	0.1%	27.1
9/20/2024 9:59:59 AM	30.0	280	330	40	0.0	0.0%	26.3

Outgoing Weekly Counts

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	4	5	5	6	*	*	5	0	28.25
1 - 2	*	3	4	4	2	*	*	3.25	0	28.25
2 - 3	*	3	3	9	4	*	*	4.75	0	27.75
3 - 4	*	3	3	3	1	*	*	2.5	0	34.5
4 - 5	*	4	11	8	5	*	*	7	0	29.5
5 - 6	*	10	16	15	10	*	*	12.75	0	28.5
6 - 7	*	71	75	73	50	*	*	67.25	0	29.88
7 - 8	*	111	95	110	104	*	*	105	0	28.98
8 - 9	*	102	99	114	81	*	*	99	0	30.48
9 - 10	*	105	93	85	67	*	*	87.5	0	30.32
10 - 11	*	99	71	84	*	*	*	84.67	0	29.93
11 - 12	*	101	91	113	*	*	*	101.67	0	29.87
12 - 13	79	94	84	107	*	*	*	91	0	30.7
13 - 14	135	119	109	96	*	*	*	114.75	0	30.45
14 - 15	111	90	103	110	*	*	*	103.5	0	28.52
15 - 16	115	115	119	121	*	*	*	117.5	0	31.5
16 - 17	125	152	118	162	*	*	*	139.25	0	31.17
17 - 18	140	145	160	135	*	*	*	145	0	32.35
18 - 19	94	74	95	93	*	*	*	89	0	32.2
19 - 20	55	69	79	68	*	*	*	67.75	0	31.68
20 - 21	38	32	79	34	*	*	*	45.75	0	30.12
21 - 22	23	27	18	41	*	*	*	27.25	0	30.08
22 - 23	17	13	16	12	*	*	*	14.5	0	29.75
23 - 24	6	3	15	10	*	*	*	8.5	0	29.25
Totals	938	1549	1561	1612	330	0	0			
% of Total	15.66%	25.86%	26.06%	26.91%	5.51%	0%	0%			

Outgoing Monthly Counts

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	4	5	5	6	*	*	5	0	28.25
1 - 2	*	3	4	4	2	*	*	3.25	0	28.25
2 - 3	*	3	3	9	4	*	*	4.75	0	27.75
3 - 4	*	3	3	3	1	*	*	2.5	0	34.5
4 - 5	*	4	11	8	5	*	*	7	0	29.5
5 - 6	*	10	16	15	10	*	*	12.75	0	28.5
6 - 7	*	71	75	73	50	*	*	67.25	0	29.88
7 - 8	*	111	95	110	104	*	*	105	0	28.98
8 - 9	*	102	99	114	81	*	*	99	0	30.48
9 - 10	*	105	93	85	67	*	*	87.5	0	30.32
10 - 11	*	99	71	84	*	*	*	84.67	0	29.93
11 - 12	*	101	91	113	*	*	*	101.67	0	29.87
12 - 13	79	94	84	107	*	*	*	91	0	30.7
13 - 14	135	119	109	96	*	*	*	114.75	0	30.45
14 - 15	111	90	103	110	*	*	*	103.5	0	28.52
15 - 16	115	115	119	121	*	*	*	117.5	0	31.5
16 - 17	125	152	118	162	*	*	*	139.25	0	31.17
17 - 18	140	145	160	135	*	*	*	145	0	32.35
18 - 19	94	74	95	93	*	*	*	89	0	32.2
19 - 20	55	69	79	68	*	*	*	67.75	0	31.68
20 - 21	38	32	79	34	*	*	*	45.75	0	30.12
21 - 22	23	27	18	41	*	*	*	27.25	0	30.08
22 - 23	17	13	16	12	*	*	*	14.5	0	29.75
23 - 24	6	3	15	10	*	*	*	8.5	0	29.25
Totals	938	1549	1561	1612	330	0	0			
% of Total	15.66%	25.86%	26.06%	26.91%	5.51%	0%	0%			

Outgoing Weekly Speeds

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	23.75	26.2	32.6	27.67	*	*	27.75	*	28.25
1 - 2	*	25.33	30	25.25	25	*	*	26.69	*	28.25
2 - 3	*	26.67	27.67	24.22	20.5	*	*	24.37	*	27.75
3 - 4	*	29	26.67	34	33	*	*	30.2	*	34.5
4 - 5	*	28.25	27.36	28	28	*	*	27.79	*	29.5
5 - 6	*	26.1	27	26.93	26.4	*	*	26.69	*	28.5
6 - 7	*	26.51	27.45	27.15	26.82	*	*	27	*	29.88
7 - 8	*	25.01	25.19	25.72	26.17	*	*	25.52	*	28.98
8 - 9	*	25.63	26.59	27.7	26.81	*	*	26.71	*	30.48
9 - 10	*	26.27	27.13	26.78	25.69	*	*	26.51	*	30.32
10 - 11	*	26.8	26.37	27.23	*	*	*	26.82	*	29.93
11 - 12	*	26.16	26.45	27.33	*	*	*	26.68	*	29.87
12 - 13	26.72	27.3	27.82	26.59	*	*	*	27.09	*	30.7
13 - 14	27.18	26.66	27.45	26.48	*	*	*	26.96	*	30.45
14 - 15	23.34	24.8	24.47	24.05	*	*	*	24.13	*	28.52
15 - 16	27.77	27.43	27.28	27.37	*	*	*	27.46	*	31.5
16 - 17	27.52	28.19	27.66	28.28	*	*	*	27.95	*	31.17
17 - 18	28.15	28.99	28.49	28.99	*	*	*	28.65	*	32.35
18 - 19	27.87	28.58	28.53	28.16	*	*	*	28.27	*	32.2
19 - 20	26.55	28.61	27.86	27.82	*	*	*	27.77	*	31.68
20 - 21	26.29	26.53	27.61	26.68	*	*	*	26.97	*	30.12
21 - 22	26.83	26.93	25.67	26.46	*	*	*	26.52	*	30.08
22 - 23	26	27.38	28.81	25.92	*	*	*	27.07	*	29.75
23 - 24	24.5	28.67	26.07	25.2	*	*	*	25.76	*	29.25
Totals	26.9	27	27.1	27.1	26.3	0	0			
% of Total	20.01%	20.09%	20.16%	20.16%	19.57%	0%	0%			

Outgoing Monthly Speeds

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	23.75	26.2	32.6	27.67	*	*	27.75	*	28.25
1 - 2	*	25.33	30	25.25	25	*	*	26.69	*	28.25
2 - 3	*	26.67	27.67	24.22	20.5	*	*	24.37	*	27.75
3 - 4	*	29	26.67	34	33	*	*	30.2	*	34.5
4 - 5	*	28.25	27.36	28	28	*	*	27.79	*	29.5
5 - 6	*	26.1	27	26.93	26.4	*	*	26.69	*	28.5
6 - 7	*	26.51	27.45	27.15	26.82	*	*	27	*	29.88
7 - 8	*	25.01	25.19	25.72	26.17	*	*	25.52	*	28.98
8 - 9	*	25.63	26.59	27.7	26.81	*	*	26.71	*	30.48
9 - 10	*	26.27	27.13	26.78	25.69	*	*	26.51	*	30.32
10 - 11	*	26.8	26.37	27.23	*	*	*	26.82	*	29.93
11 - 12	*	26.16	26.45	27.33	*	*	*	26.68	*	29.87
12 - 13	26.72	27.3	27.82	26.59	*	*	*	27.09	*	30.7
13 - 14	27.18	26.66	27.45	26.48	*	*	*	26.96	*	30.45
14 - 15	23.34	24.8	24.47	24.05	*	*	*	24.13	*	28.52
15 - 16	27.77	27.43	27.28	27.37	*	*	*	27.46	*	31.5
16 - 17	27.52	28.19	27.66	28.28	*	*	*	27.95	*	31.17
17 - 18	28.15	28.99	28.49	28.99	*	*	*	28.65	*	32.35
18 - 19	27.87	28.58	28.53	28.16	*	*	*	28.27	*	32.2
19 - 20	26.55	28.61	27.86	27.82	*	*	*	27.77	*	31.68
20 - 21	26.29	26.53	27.61	26.68	*	*	*	26.97	*	30.12
21 - 22	26.83	26.93	25.67	26.46	*	*	*	26.52	*	30.08
22 - 23	26	27.38	28.81	25.92	*	*	*	27.07	*	29.75
23 - 24	24.5	28.67	26.07	25.2	*	*	*	25.76	*	29.25
Totals	26.9	27	27.1	27.1	26.3	0	0			
% of Total	20.01%	20.09%	20.16%	20.16%	19.57%	0%	0%			

Outgoing Weekly EightyFifthSpeeds

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	9/16/2024	to	9/22/2024							
	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Week	Weekend	Week Day 85%
Hour	9/16/2024	9/17/2024	9/18/2024	9/19/2024	9/20/2024	9/21/2024	9/22/2024	Day Avg	Avg	Avg Speed
0 - 1	*	24	27	33	29	*	*	28.25	0	28.25
1 - 2	*	29	29	29	26	*	*	28.25	0	28.25
2 - 3	*	28	32	28	23	*	*	27.75	0	27.75
3 - 4	*	34	31	40	33	*	*	34.5	0	34.5
4 - 5	*	26	30	31	31	*	*	29.5	0	29.5
5 - 6	*	28	30	28	28	*	*	28.5	0	28.5
6 - 7	*	29	30.7	30	29.8	*	*	29.88	0	29.88
7 - 8	*	28.6	27.8	29.7	29.8	*	*	28.98	0	28.98
8 - 9	*	29.4	30.7	31.2	30.6	*	*	30.48	0	30.48
9 - 10	*	30.4	32.1	30	28.8	*	*	30.32	0	30.32
10 - 11	*	29.4	29.6	30.8	*	*	*	29.93	0	29.93
11 - 12	*	29.5	29.7	30.4	*	*	*	29.87	0	29.87
12 - 13	30.8	31.3	31	29.7	*	*	*	30.7	0	30.7
13 - 14	30.4	30.2	31	30.2	*	*	*	30.45	0	30.45
14 - 15	27	29.6	28.8	28.7	*	*	*	28.52	0	28.52
15 - 16	31.8	31.8	31	31.4	*	*	*	31.5	0	31.5
16 - 17	30.7	31.9	30.2	31.9	*	*	*	31.17	0	31.17
17 - 18	31.7	32.7	32.3	32.7	*	*	*	32.35	0	32.35
18 - 19	31.9	32	32.4	32.5	*	*	*	32.2	0	32.2
19 - 20	30.6	33	30.8	32.3	*	*	*	31.68	0	31.68
20 - 21	29.5	28.8	31.2	31	*	*	*	30.12	0	30.12
21 - 22	31	30	29	30.3	*	*	*	30.08	0	30.08
22 - 23	29	30	32	28	*	*	*	29.75	0	29.75
23 - 24	27	32	29.7	28.3	*	*	*	29.25	0	29.25
Totals	361.4	718.6	729	738.1	289	0	0			
% of Total	12.74%	25.34%	25.7%	26.03%	10.19%	0%	0%			

Outgoing Monthly EightyFifthSpeeds
 Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

	Sep 2024							Week	Weekend	Week Day 85%
Hour	Monday	Tuesday	Wednesday	Thursday	Friday	Saturday	Sunday	Day Avg	Avg	Avg Speed
0 - 1	*	24	27	33	29	*	*	28.25	0	28.25
1 - 2	*	29	29	29	26	*	*	28.25	0	28.25
2 - 3	*	28	32	28	23	*	*	27.75	0	27.75
3 - 4	*	34	31	40	33	*	*	34.5	0	34.5
4 - 5	*	26	30	31	31	*	*	29.5	0	29.5
5 - 6	*	28	30	28	28	*	*	28.5	0	28.5
6 - 7	*	29	30.7	30	29.8	*	*	29.88	0	29.88
7 - 8	*	28.6	27.8	29.7	29.8	*	*	28.98	0	28.98
8 - 9	*	29.4	30.7	31.2	30.6	*	*	30.48	0	30.48
9 - 10	*	30.4	32.1	30	28.8	*	*	30.32	0	30.32
10 - 11	*	29.4	29.6	30.8	*	*	*	29.93	0	29.93
11 - 12	*	29.5	29.7	30.4	*	*	*	29.87	0	29.87
12 - 13	30.8	31.3	31	29.7	*	*	*	30.7	0	30.7
13 - 14	30.4	30.2	31	30.2	*	*	*	30.45	0	30.45
14 - 15	27	29.6	28.8	28.7	*	*	*	28.52	0	28.52
15 - 16	31.8	31.8	31	31.4	*	*	*	31.5	0	31.5
16 - 17	30.7	31.9	30.2	31.9	*	*	*	31.17	0	31.17
17 - 18	31.7	32.7	32.3	32.7	*	*	*	32.35	0	32.35
18 - 19	31.9	32	32.4	32.5	*	*	*	32.2	0	32.2
19 - 20	30.6	33	30.8	32.3	*	*	*	31.68	0	31.68
20 - 21	29.5	28.8	31.2	31	*	*	*	30.12	0	30.12
21 - 22	31	30	29	30.3	*	*	*	30.08	0	30.08
22 - 23	29	30	32	28	*	*	*	29.75	0	29.75
23 - 24	27	32	29.7	28.3	*	*	*	29.25	0	29.25

Summary of Violators

Birchwood Loop northwest northwest 9-16 to 9-20 time from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM
correct

Starting Hour	Count	Average Speed of all Traffic	Violator Counts	Average Speed of Violators
00:00:00	20	27.8	1	46.0
01:00:00	13	26.7	0	0.0
02:00:00	19	24.4	0	0.0
03:00:00	10	30.2	0	0.0
04:00:00	28	27.8	0	0.0
05:00:00	51	26.7	0	0.0
06:00:00	269	27.0	0	0.0
07:00:00	420	25.5	0	0.0
08:00:00	396	26.7	0	0.0
09:00:00	350	26.5	0	0.0
10:00:00	254	26.8	0	0.0
11:00:00	305	26.7	0	0.0
12:00:00	364	27.1	0	0.0
13:00:00	459	27.0	0	0.0
14:00:00	414	24.1	0	0.0
15:00:00	470	27.5	1	49.0
16:00:00	557	28.0	0	0.0
17:00:00	580	28.7	0	0.0
18:00:00	356	28.3	0	0.0
19:00:00	271	27.8	0	0.0
20:00:00	183	27.0	0	0.0
21:00:00	109	26.5	0	0.0
22:00:00	58	27.1	0	0.0
23:00:00	34	25.8	0	0.0

Outgoing Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG	
9/16/2024	00:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/16/2024	12:00	1	3	16	38	21	0	0	0	0	0	0	0	0	0	0	0	0	0	0	79	26.7	30.8	22 to 32	86.1	*	*	1	74	4
9/16/2024	13:00	0	2	26	70	35	2	0	0	0	0	0	0	0	0	0	0	0	0	0	135	27.2	30.4	21 to 31	88.9	*	*	0	121	14
9/16/2024	14:00	0	18	56	28	9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	111	23.3	27	18 to 28	85.6	*	*	1	98	12
9/16/2024	15:00	0	1	24	55	28	5	1	1	0	0	0	0	0	0	0	0	0	0	0	115	27.8	31.8	23 to 33	80.9	1	0.9	0	108	7
9/16/2024	16:00	0	2	28	56	34	4	1	0	0	0	0	0	0	0	0	0	0	0	0	125	27.5	30.7	21 to 31	82.4	*	*	0	111	14
9/16/2024	17:00	0	2	15	77	36	10	0	0	0	0	0	0	0	0	0	0	0	0	0	140	28.1	31.7	23 to 33	86.4	*	*	0	136	4
9/16/2024	18:00	0	2	12	50	26	4	0	0	0	0	0	0	0	0	0	0	0	0	0	94	27.9	31.9	24 to 34	83.0	*	*	1	84	9
9/16/2024	19:00	0	3	16	23	12	1	0	0	0	0	0	0	0	0	0	0	0	0	0	55	26.5	30.6	22 to 32	85.5	*	*	0	53	2
9/16/2024	20:00	0	4	9	18	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	38	26.3	29.5	22 to 32	86.8	*	*	0	37	1
9/16/2024	21:00	0	1	5	13	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	23	26.8	31	22 to 32	87.0	*	*	0	22	1
9/16/2024	22:00	0	1	5	8	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	17	26	29	20 to 30	88.2	*	*	0	16	1
9/16/2024	23:00	0	1	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	24.5	27	18 to 28	100.0	*	*	0	6	0
24 Hr Summary		1	40	213	440	213	28	2	1	0	0	0	0	0	0	0	0	0	0	0	938	26.9	31	22 to 32	81.8	1	0.1	3	866	69

Outgoing Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/17/2024	00:00	0	1	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	23.8	24	18 to 28	100.0	*	*	0	4	0
9/17/2024	01:00	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	25.3	29	19 to 29	100.0	*	*	0	3	0
9/17/2024	02:00	0	0	0	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26.7	28	18 to 28	100.0	*	*	0	3	0
9/17/2024	03:00	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	29	34	24 to 34	100.0	*	*	0	3	0
9/17/2024	04:00	0	0	1	2	0	1	0	0	0	0	0	0	0	0	0	0	0	0	4	28.3	26	16 to 26	75.0	*	*	0	3	1
9/17/2024	05:00	0	0	2	7	1	0	0	0	0	0	0	0	0	0	0	0	0	0	10	26.1	28	21 to 31	100.0	*	*	0	9	1
9/17/2024	06:00	0	2	14	44	10	1	0	0	0	0	0	0	0	0	0	0	0	0	71	26.5	29	22 to 32	90.1	*	*	0	68	3
9/17/2024	07:00	0	9	43	45	14	0	0	0	0	0	0	0	0	0	0	0	0	0	111	25	28.6	19 to 29	83.8	*	*	0	103	8
9/17/2024	08:00	1	9	32	42	13	5	0	0	0	0	0	0	0	0	0	0	0	0	102	25.6	29.4	20 to 30	79.4	*	*	0	84	18
9/17/2024	09:00	0	5	29	46	23	2	0	0	0	0	0	0	0	0	0	0	0	0	105	26.3	30.4	22 to 32	82.9	*	*	1	88	16
9/17/2024	10:00	0	0	23	58	16	2	0	0	0	0	0	0	0	0	0	0	0	0	99	26.8	29.4	21 to 31	92.9	*	*	0	91	8
9/17/2024	11:00	0	8	25	52	13	3	0	0	0	0	0	0	0	0	0	0	0	0	101	26.2	29.5	22 to 32	85.1	*	*	0	92	9
9/17/2024	12:00	0	1	27	40	21	4	1	0	0	0	0	0	0	0	0	0	0	0	94	27.3	31.3	23 to 33	80.9	*	*	0	88	6
9/17/2024	13:00	0	6	32	54	24	2	1	0	0	0	0	0	0	0	0	0	0	0	119	26.7	30.2	21 to 31	84.0	*	*	0	110	9
9/17/2024	14:00	1	11	34	27	14	3	0	0	0	0	0	0	0	0	0	0	0	0	90	24.8	29.6	19 to 29	73.3	*	*	0	83	7
9/17/2024	15:00	0	0	34	48	28	5	0	0	0	0	0	0	0	0	0	0	0	0	115	27.4	31.8	21 to 31	79.1	*	*	1	103	11
9/17/2024	16:00	0	3	22	76	41	10	0	0	0	0	0	0	0	0	0	0	0	0	152	28.2	31.9	23 to 33	84.2	*	*	0	148	4
9/17/2024	17:00	0	1	19	63	49	11	2	0	0	0	0	0	0	0	0	0	0	0	145	29	32.7	24 to 34	81.4	*	*	0	139	6
9/17/2024	18:00	1	1	6	36	26	4	0	0	0	0	0	0	0	0	0	0	0	0	74	28.6	32	23 to 33	85.1	*	*	0	72	2
9/17/2024	19:00	0	1	7	37	17	6	1	0	0	0	0	0	0	0	0	0	0	0	69	28.6	33	24 to 34	84.1	*	*	0	64	5
9/17/2024	20:00	0	1	6	21	3	1	0	0	0	0	0	0	0	0	0	0	0	0	32	26.5	28.8	23 to 33	90.6	*	*	0	31	1
9/17/2024	21:00	0	0	6	14	7	0	0	0	0	0	0	0	0	0	0	0	0	0	27	26.9	30	21 to 31	88.9	*	*	0	26	1
9/17/2024	22:00	0	0	3	7	2	1	0	0	0	0	0	0	0	0	0	0	0	0	13	27.4	30	20 to 30	84.6	*	*	0	12	1
9/17/2024	23:00	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	28.7	32	22 to 32	100.0	*	*	0	3	0
24 Hr Summary		3	59	369	728	324	61	5	0	0	0	0	0	0	0	0	0	0	0	1549	27	31	22 to 32	80.1	*	*	2	1430	117

Outgoing Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/18/2024	00:00	0	0	1	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	26.2	27	19 to 29	100.0	*	*	0	5	0
9/18/2024	01:00	0	0	0	3	0	1	0	0	0	0	0	0	0	0	0	0	0	0	4	30	29	25 to 35	100.0	*	*	0	4	0
9/18/2024	02:00	0	0	0	2	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	27.7	32	22 to 32	100.0	*	*	0	3	0
9/18/2024	03:00	0	0	1	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	3	26.7	31	21 to 31	100.0	*	*	0	3	0
9/18/2024	04:00	0	0	3	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	11	27.4	30	22 to 32	100.0	*	*	0	10	1
9/18/2024	05:00	0	0	3	10	3	0	0	0	0	0	0	0	0	0	0	0	0	0	16	27	30	22 to 32	93.8	*	*	0	16	0
9/18/2024	06:00	0	1	14	41	16	1	2	0	0	0	0	0	0	0	0	0	0	0	75	27.5	30.7	22 to 32	92.0	*	*	0	72	3
9/18/2024	07:00	0	6	31	52	6	0	0	0	0	0	0	0	0	0	0	0	0	0	95	25.2	27.8	20 to 30	92.6	*	*	0	90	5
9/18/2024	08:00	0	2	32	38	23	4	0	0	0	0	0	0	0	0	0	0	0	0	99	26.6	30.7	21 to 31	78.8	*	*	0	92	7
9/18/2024	09:00	0	2	26	39	19	7	0	0	0	0	0	0	0	0	0	0	0	0	93	27.1	32.1	20 to 30	76.3	*	*	0	85	8
9/18/2024	10:00	0	2	15	40	14	0	0	0	0	0	0	0	0	0	0	0	0	0	71	26.4	29.6	21 to 31	87.3	*	*	0	63	8
9/18/2024	11:00	0	4	25	46	13	3	0	0	0	0	0	0	0	0	0	0	0	0	91	26.5	29.7	22 to 32	83.5	*	*	0	84	7
9/18/2024	12:00	0	1	14	42	24	3	0	0	0	0	0	0	0	0	0	0	0	0	84	27.8	31	24 to 34	88.1	*	*	0	79	5
9/18/2024	13:00	0	0	25	54	26	3	1	0	0	0	0	0	0	0	0	0	0	0	109	27.4	31	23 to 33	89.9	*	*	0	104	5
9/18/2024	14:00	0	14	41	35	9	4	0	0	0	0	0	0	0	0	0	0	0	0	103	24.5	28.8	19 to 29	80.6	*	*	0	93	10
9/18/2024	15:00	0	7	28	49	27	7	1	0	0	0	0	0	0	0	0	0	0	0	119	27.3	31	23 to 33	81.5	*	*	0	110	9
9/18/2024	16:00	0	1	19	67	27	4	0	0	0	0	0	0	0	0	0	0	0	0	118	27.7	30.2	23 to 33	89.0	*	*	0	110	8
9/18/2024	17:00	0	3	21	78	45	10	3	0	0	0	0	0	0	0	0	0	0	0	160	28.5	32.3	24 to 34	83.1	*	*	0	149	11
9/18/2024	18:00	0	1	12	49	26	6	1	0	0	0	0	0	0	0	0	0	0	0	95	28.5	32.4	23 to 33	83.2	*	*	0	89	6
9/18/2024	19:00	0	2	8	44	22	3	0	0	0	0	0	0	0	0	0	0	0	0	79	27.9	30.8	24 to 34	86.1	*	*	0	73	6
9/18/2024	20:00	0	0	18	36	23	2	0	0	0	0	0	0	0	0	0	0	0	0	79	27.6	31.2	23 to 33	86.1	*	*	0	77	2
9/18/2024	21:00	0	2	4	9	3	0	0	0	0	0	0	0	0	0	0	0	0	0	18	25.7	29	20 to 30	83.3	*	*	0	16	2
9/18/2024	22:00	0	0	2	8	4	2	0	0	0	0	0	0	0	0	0	0	0	0	16	28.8	32	22 to 32	87.5	*	*	0	16	0
9/18/2024	23:00	1	0	2	8	4	0	0	0	0	0	0	0	0	0	0	0	0	0	15	26.1	29.7	21 to 31	93.3	*	*	0	14	1
24 Hr Summary		1	48	345	760	339	60	8	0	0	0	0	0	0	0	0	0	0	0	1561	27.1	31	22 to 32	81.7	*	*	0	1457	104

Outgoing Histogram

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG
9/19/2024	00:00	0	0	0	2	2	0	0	1	0	0	0	0	0	0	0	0	0	0	5	32.6	33	23 to 33	80.0	1	20.0	0	5	0
9/19/2024	01:00	0	0	2	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	4	25.3	29	20 to 30	100.0	*	*	0	3	1
9/19/2024	02:00	0	1	5	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	9	24.2	28	18 to 28	88.9	*	*	0	9	0
9/19/2024	03:00	0	0	0	1	0	1	1	0	0	0	0	0	0	0	0	0	0	0	3	34	40	26 to 36	66.7	*	*	0	2	1
9/19/2024	04:00	0	0	1	4	3	0	0	0	0	0	0	0	0	0	0	0	0	0	8	28	31	22 to 32	100.0	*	*	0	8	0
9/19/2024	05:00	0	0	3	10	2	0	0	0	0	0	0	0	0	0	0	0	0	0	15	26.9	28	22 to 32	100.0	*	*	0	14	1
9/19/2024	06:00	0	1	18	34	18	2	0	0	0	0	0	0	0	0	0	0	0	0	73	27.2	30	23 to 33	87.7	*	*	0	68	5
9/19/2024	07:00	0	7	36	49	15	3	0	0	0	0	0	0	0	0	0	0	0	0	110	25.7	29.7	19 to 29	81.8	*	*	0	104	6
9/19/2024	08:00	0	6	13	60	29	5	1	0	0	0	0	0	0	0	0	0	0	0	114	27.7	31.2	23 to 33	84.2	*	*	0	105	9
9/19/2024	09:00	0	0	24	44	14	3	0	0	0	0	0	0	0	0	0	0	0	0	85	26.8	30	23 to 33	89.4	*	*	0	77	8
9/19/2024	10:00	0	3	18	35	28	0	0	0	0	0	0	0	0	0	0	0	0	0	84	27.2	30.8	22 to 32	84.5	*	*	0	77	7
9/19/2024	11:00	0	3	20	63	22	5	0	0	0	0	0	0	0	0	0	0	0	0	113	27.3	30.4	21 to 31	83.2	*	*	0	105	8
9/19/2024	12:00	1	3	22	60	19	2	0	0	0	0	0	0	0	0	0	0	0	0	107	26.6	29.7	23 to 33	86.9	*	*	0	100	7
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9/19/2024	14:00	0	19	47	30	12	2	0	0	0	0	0	0	0	0	0	0	0	0	110	24.1	28.7	19 to 29	77.3	*	*	0	104	6
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9/19/2024	16:00	0	2	24	76	48	9	3	0	0	0	0	0	0	0	0	0	0	0	162	28.3	31.9	23 to 33	83.3	*	*	0	153	9
9/19/2024	17:00	0	1	14	63	45	12	0	0	0	0	0	0	0	0	0	0	0	0	135	29	32.7	23 to 33	83.7	*	*	0	129	6
9/19/2024	18:00	0	4	15	40	28	5	1	0	0	0	0	0	0	0	0	0	0	0	93	28.2	32.5	23 to 33	78.5	*	*	0	90	3
9/19/2024	19:00	0	0	18	29	16	5	0	0	0	0	0	0	0	0	0	0	0	0	68	27.8	32.3	22 to 32	82.4	*	*	0	66	2
9/19/2024	20:00	0	3	6	15	10	0	0	0	0	0	0	0	0	0	0	0	0	0	34	26.7	31	22 to 32	82.4	*	*	0	34	0
9/19/2024	21:00	0	0	10	23	7	1	0	0	0	0	0	0	0	0	0	0	0	0	41	26.5	30.3	21 to 31	90.2	*	*	0	38	3
9/19/2024	22:00	0	0	5	5	2	0	0	0	0	0	0	0	0	0	0	0	0	0	12	25.9	28	21 to 31	100.0	*	*	0	12	0
9/19/2024	23:00	0	1	2	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	25.2	28.3	19 to 29	90.0	*	*	0	9	1
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Outgoing Histogram

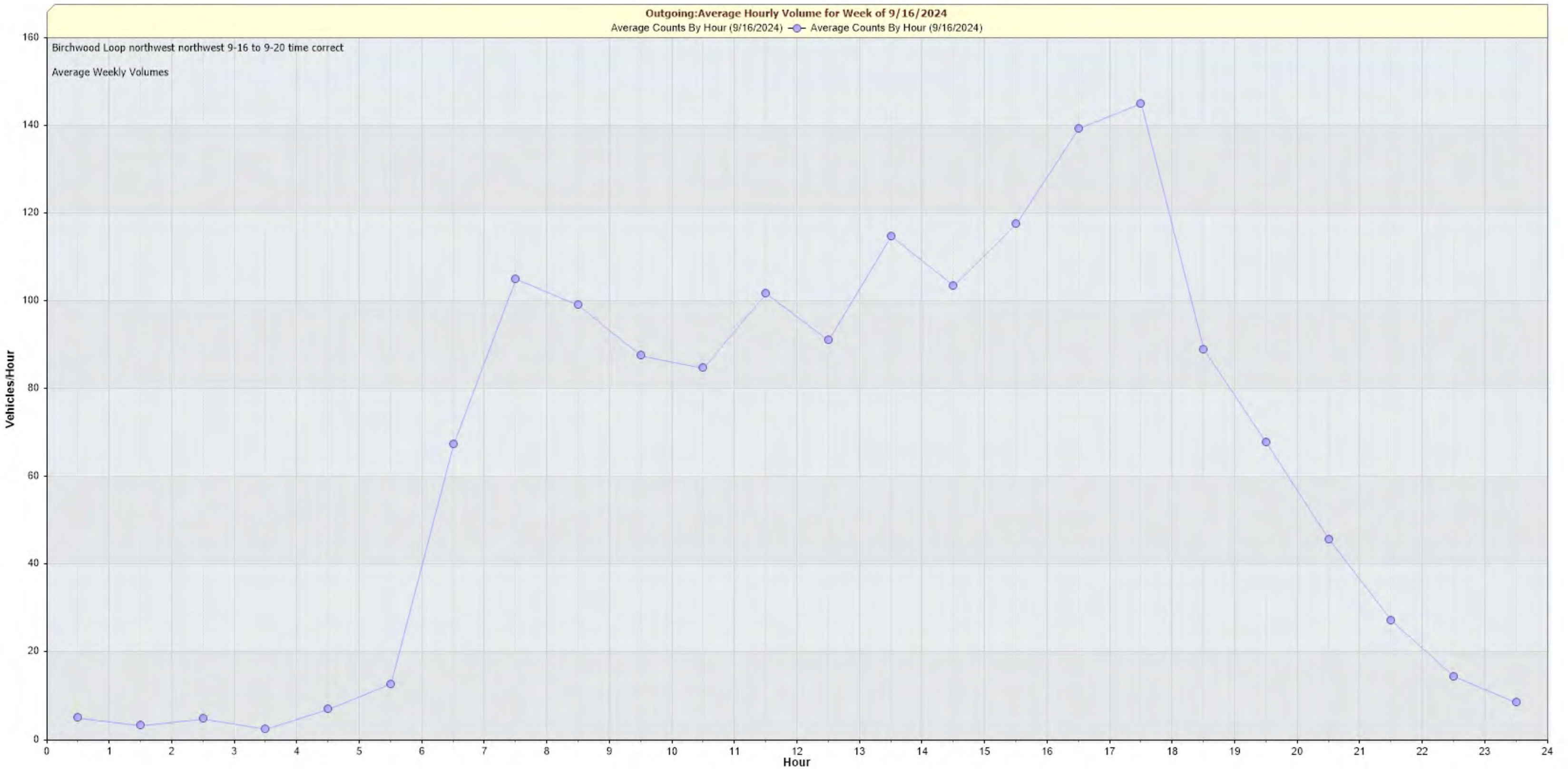
Birchwood Loop northwest northwest 9-16 to 9-20 time correct

from Mon-Sep-16-2024-12-00-PM to Fri-Sep-20-2024-09-59-AM

Date	Starting Hr:min	<15	15 to <20	20 to <25	25 to <30	30 to <35	35 to <40	40 to <45	45 to <50	50 to <55	55 to <60	60 to <65	65 to <70	70 to <75	75 to <80	80 to <85	85 to <90	90 to <95	95 to >100	Total Counts	Avg Speed (MPH)	85pct Speed	10MPH Pace	% in pace	# of Speeders	% Speeders	VEH_SM	VEH_MED	VEH_LG			
9/20/2024	00:00	0	1	1	3	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	6	27.7	29	19 to 29	66.7	*	*	0	6	0		
9/20/2024	01:00	0	0	1	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	25	26	16 to 26	100.0	*	*	0	2	0		
9/20/2024	02:00	0	2	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4	20.5	23	14 to 24	100.0	*	*	0	3	1		
9/20/2024	03:00	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	33	33	23 to 33	100.0	*	*	0	1	0		
9/20/2024	04:00	0	0	2	1	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5	28	31	21 to 31	80.0	*	*	0	5	0		
9/20/2024	05:00	0	0	0	10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	10	26.4	28	19 to 29	100.0	*	*	0	9	1		
9/20/2024	06:00	0	2	10	26	12	0	0	0	0	0	0	0	0	0	0	0	0	0	0	50	26.8	29.8	22 to 32	92.0	*	*	0	49	1		
9/20/2024	07:00	0	5	32	47	16	4	0	0	0	0	0	0	0	0	0	0	0	0	0	104	26.2	29.8	21 to 31	82.7	*	*	0	100	4		
9/20/2024	08:00	0	2	18	42	19	0	0	0	0	0	0	0	0	0	0	0	0	0	0	81	26.8	30.6	23 to 33	90.1	*	*	0	71	10		
9/20/2024	09:00	0	5	19	34	7	2	0	0	0	0	0	0	0	0	0	0	0	0	0	67	25.7	28.8	22 to 32	85.1	*	*	0	59	8		
9/20/2024	10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
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9/20/2024	13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	
9/20/2024	14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
9/20/2024	23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
24 Hr Summary		0	17	85	164	57	6	1	0	0	0	0	0	0	0	0	0	0	0	0	330	26.3	30	22 to 32	85.2	*	*	0	305	25		

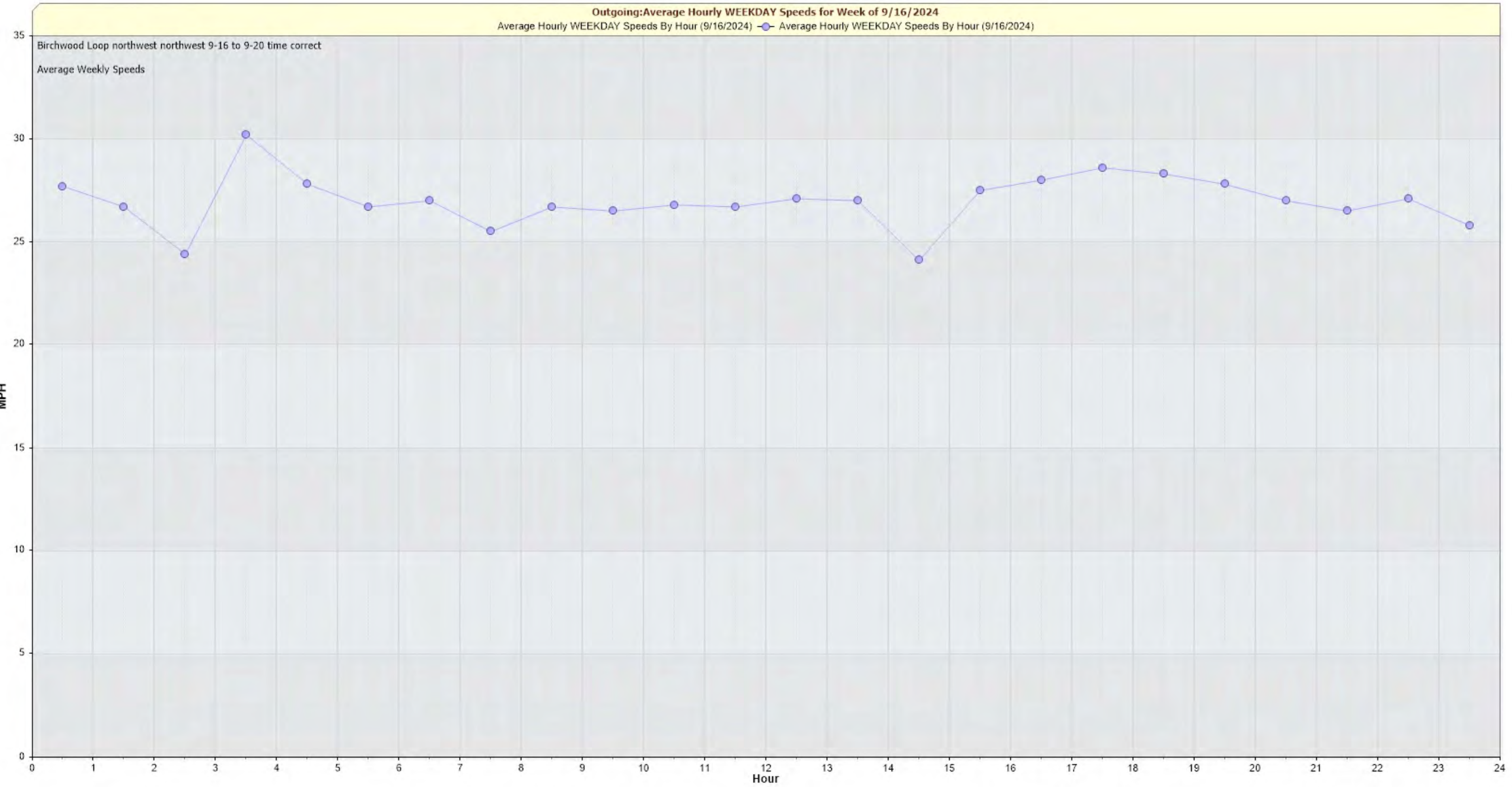
Outgoing: Average Hourly Volume for Week of 9/16/2024
Average Counts By Hour (9/16/2024) — Average Counts By Hour (9/16/2024)

Birchwood Loop northwest northwest 9-16 to 9-20 time correct
Average Weekly Volumes



Outgoing:Average Hourly WEEKDAY Speeds for Week of 9/16/2024
Average Hourly WEEKDAY Speeds By Hour (9/16/2024) — Average Hourly WEEKDAY Speeds By Hour (9/16/2024)

Birchwood Loop northwest northwest 9-16 to 9-20 time correct
Average Weekly Speeds

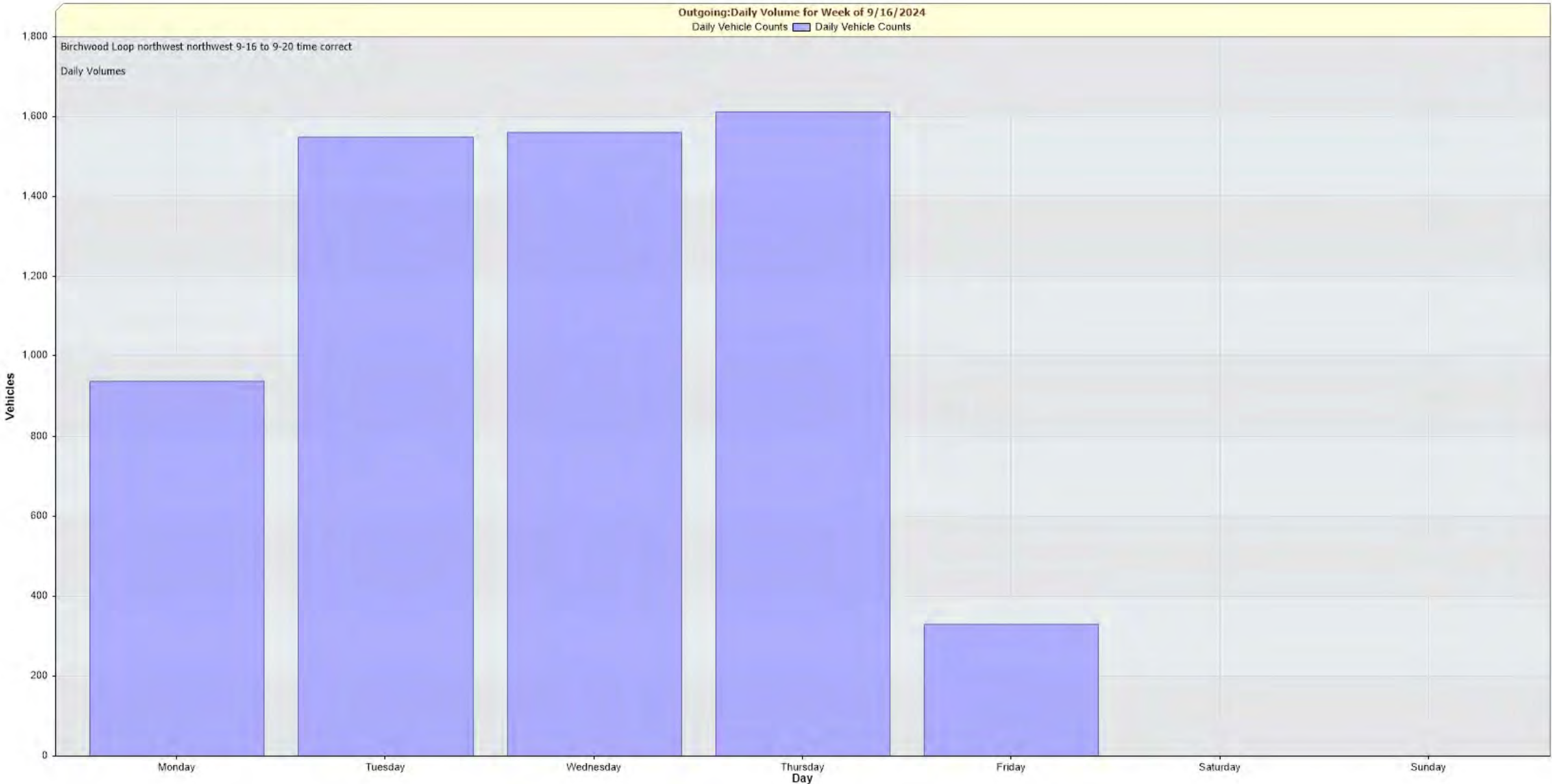


Outgoing:Daily Volume for Week of 9/16/2024

Daily Vehicle Counts

Birchwood Loop northwest northwest 9-16 to 9-20 time correct

Daily Volumes



Appendix B

Grading and Drainage Study

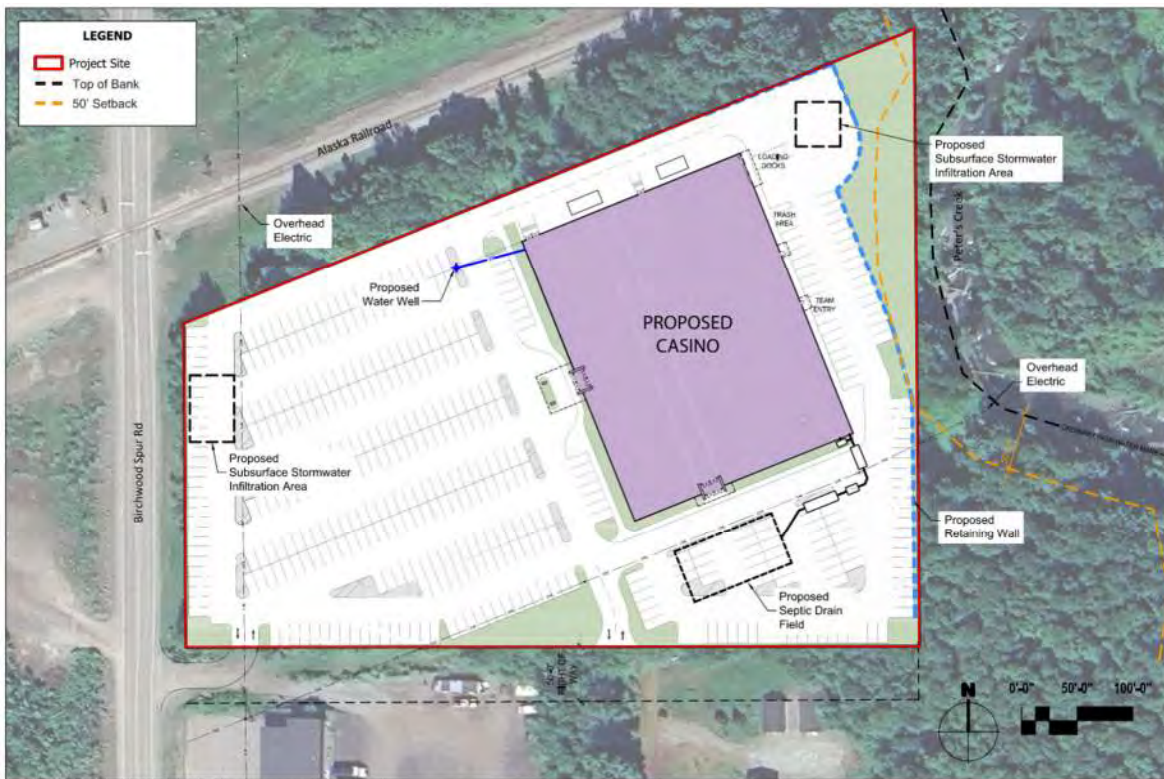
EKLUTNA NATIVE VILLAGE GAMING HALL PROJECT

CHUGIAK, ALASKA

Grading and Drainage Study

Final Submission

January 10, 2025



Source: Marnell Companies, 1/7/2025, Acorn Environmental, 1/10/2025

Prepared By:



3601 C St., Suite 650, Anchorage, Alaska 99503

Kyle Markel, PE – Civil Engineer
Carl Bassler, PE – Principal, Civil Engineer

EI Project No. 10336
Acorn Project No. 2415

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APPENDICES

Concept Site Plan and Existing Site Topography.....	Appendix A
Concept Grading and Drainage Sketch	Appendix B
Pre-Development Runoff Calculation	Appendix C
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1.0 PROJECT INTRODUCTION

1.1 Project description

The proposed project development includes a new 58,000 SF gaming hall building, with supporting utilities, paved parking, driveways, and landscaping. The purpose of this Grading and Drainage Study is to analyze the existing topography and drainage patterns, develop a conceptual grading and drainage design, investigate potential stormwater management strategies, and evaluate potential drainage impacts for the proposed site development.

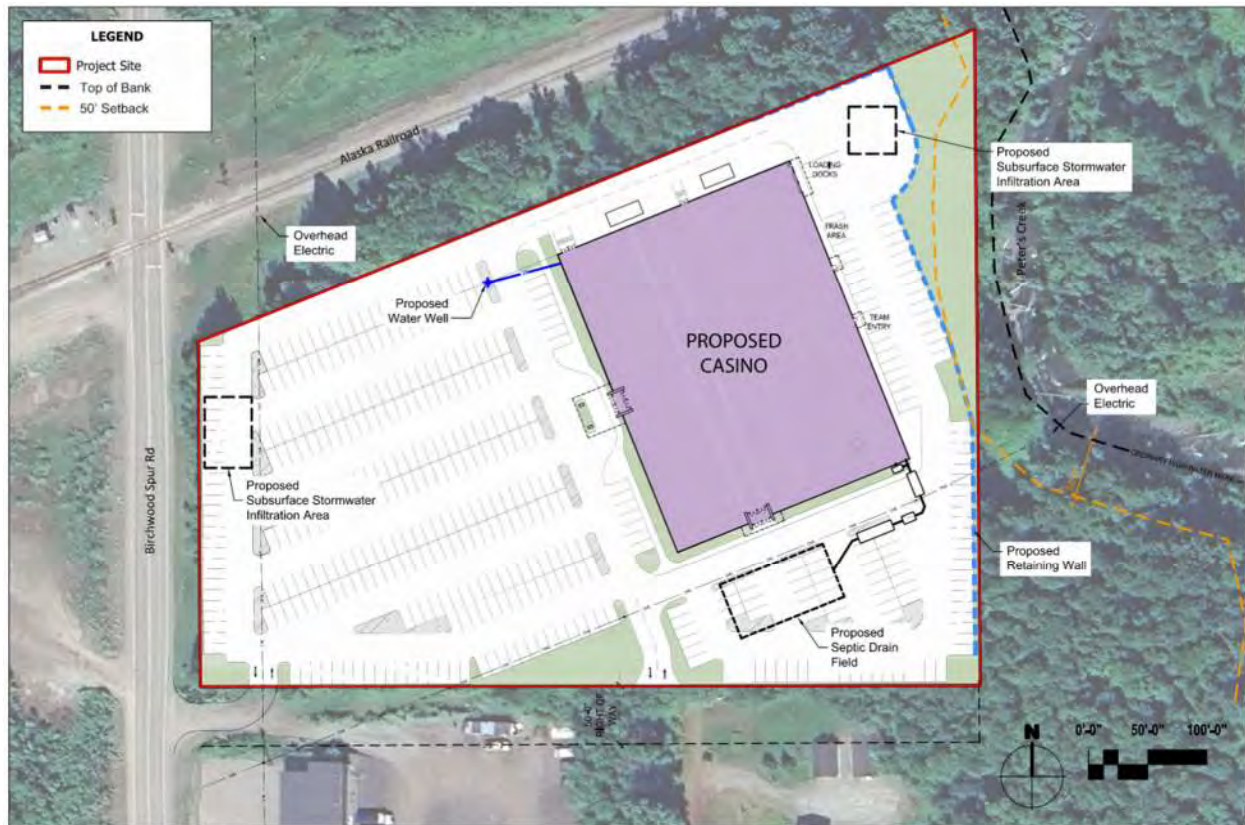


Figure 1: Concept Site Plan

2.0 EXISTING SITE CONDITIONS

2.1 Existing Conditions

The project site is located about 25 miles north of Anchorage, off Birchwood Spur Road in Chugiak, Alaska. The 6.4-acre property includes three lots which are heavily vegetated with mature deciduous and coniferous trees and thick underbrush, and formerly one small (roughly 1,000 SF) single story house, which has been demolished. The topography mostly slopes gradually down to the north. The highest portion of the site is the southeast corner at elevation 88, and the lowest part of the site is in the northeast corner at elevation 73. There is a bench cutting across the northeast corner of the property that drops about 8 feet and then flattens out toward the creek bank. Aside from the steep slope along the bench, typical slopes across the site are generally around 1% to 3%. Peters Creek runs along the east

side of the site and is ultimately where the stormwater, that does not infiltrate, discharges. A topographic survey was not performed for this study, however LiDAR contours from the Municipality of Anchorage (MOA) Geographic Data Information Center provide sufficient information to understand the general drainage patterns. An exhibit of the existing site topography can be found in Appendix A.

The United States Geological Survey (USGS) published a generalized geologic map of the Eagle River-Birchwood Area in 1971 which shows the project site to be in a geological area classified as "F" defined as "Large alluvial-fan deposits. Gravel and sand; cobbles and small boulders common; small amounts of silt. Good water-bearing unit; may yield large quantities of water to wells." Though this information comes from a generalization of large areas, it is a good indication that gravel and sand can be anticipated beneath the top overburden layer.

A preliminary geotechnical investigation was performed in July of 2024 which included visual observation of four test pits and three percolation tests. The following conditions were noted:

A surface layer of vegetation and other organics about 6 inches thick was observed in all the test pits. Beneath the organics is a Sandy Silt layer, extending to a depth of 1 to 3.5 feet below ground surface (bgs). A Sandy Gravel material underlies these surface layers in all pits, and extends to the bottom of the test pits, 6 to 12.5 feet bgs. This granular soil is moist, and medium dense to dense. The maximum particle size ranges from 2 inches to large cobbles and boulders over 1.5 feet in diameter. Groundwater was encountered at 9 to 12.5 ft below ground surface (bgs) in three of the four pits. The test pit in the northeast corner of the site encountered groundwater at 6 ft bgs which was not surprising considering the much lower elevation and close proximity to Peters Creek. Because of the high water table encountered in the northeast test pit, a percolation test was not performed.

3.0 GRADING AND EARTHWORK

3.1 Conceptual Grading

A concept grading plan was developed and can be found in Appendix B. The developer's desire to minimize the length and height of retaining walls was factored into the grading design, however the site plan is laid out such that the pavement limits are within a couple feet of the property line across much of the site to maximize parking spaces. General grading parameters for the site include the following:

- Positive slope away from the building.
- Matching existing grades at both driveway locations.
- Asphalt Parking Lot Slopes: 1.5% - 4.5%
- Unpaved slopes not to exceed 3H:1V.

The current site layout and concept grading will necessitate retaining walls on the north and east side of the property. Walls on the north side are to contain parking lot fill within the property boundaries. Walls on the east side are to contain parking lot fill outside of the 50-foot stream bank setbacks along Peters Creek, or outside the 500-year flood zone. The northwest, southwest, and southeast corners of the site will closely match existing grades, however the northeast corner of the site will need to be raised by 3 to 7 feet above existing grades as the northeast portion of the site has a steep drop-off of about 7 to 10 feet towards the east and Peters Creek. Retaining walls will likely not be needed along the west and south property lines as the proposed grades are very close to existing grades.

Since some proposed retaining walls along the east side are just outside the 50-foot stream setback, and some are not far outside the 500-year flood zone, their design will need to take into account potential stream scour, high flood water and groundwater levels, and storm water runoff associated with extreme flooding or storm events. Preliminary grading indicates the walls will need to retain a fill height up to 6 feet. The walls will be of durable material, such as cast-in-place concrete, segmental block/reinforced earth, or similar.

The finished grade along the north and east sides of the building was dropped about 4 feet below the building finished floor. This lowered area is intended to facilitate the truck loading bay area, reduce the amount of fill required for the northeast corner of the site, and minimize the retaining wall heights. The disadvantage of the lowered grade is that staircases (about 6 steps) will be required to access the doorways on those sides of the building. It is understood that these doorways do not serve as main entrances and would be used much less frequently.

Grading for the west half of the proposed site will closely follow the slope of the existing site towards a low point near the northwest corner. The east half of the site will slope towards the northeast corner of the site. At this conceptual level of site grading, the finished floor elevation for the building is set at elevation 84. This will allow for positive drainage away from the building while still minimizing the fill quantities as much as possible. The southeast corner of the site will be the highest elevation (matching existing grades) and will be above the finished floor of the building. Drainage will sheet flow toward the building but will be intercepted by a grade break or curb and gutter along the south side of the building to direct the drainage around the building to the north. Storm drain catch basins will be located at low points across the site with storm drain pipes connecting them and directing runoff to the buried infiltration beds. The concept grading and drainage sketch can be found in Appendix B.

3.2 Earthwork Quantities

It is assumed that the top 1 foot of soil will need to be stripped across the entire site to remove tree roots, organics, and topsoil/peat. Stripping 1 foot over the entire property (6.42 acres) equates to 10,350 cubic yards of unusable excavation that will need to be hauled off-site.

The assumed pavement section for this site is 2.5 ft thick to include pavement and classified fill. The silty layer of soil encountered onsite in the top couple feet does not meet the requirements for classified fill and must be removed where it is within the new pavement section. This results in about 15,800 CY of poor silty material to be excavated. Of that, 4,200 CY can be reused across the site to fill the lower areas. This results in a net volume of 11,600 CY of unusable excavation. 20,500 CY of classified material will be imported to the site for the pavement section. It is possible that a 'mining' approach could be used to reduce the quantity of imported classified fill, where classified fill is obtained from on-site excavations, and replaced with excess silty material.

It is assumed the building section will include classified material to 3.5 feet below finished floor as necessary to remove the shallow silty layer. This will result in an additional 5,400 CY of excavation under building after site stripping, which is all unusable excavation. 6,500 CY of classified material will then be imported to the site for the building section.

During design, the cut/fill calculations and sections would be more accurately defined, and grading could be adjusted, if possible, to minimize this additional fill that would be needed. Depending on the results of the future geotechnical investigation, the amount of usable excavation could change. Additionally, where the subgrade below the pavement section is a silty material, a geotextile is recommended to prevent silt from migrating up into the gravel fill.

A breakdown of estimated earthwork and pavement quantities is as follows:

- Unclassified Fill – 4,200 cubic yards (reused material from site excavations)
- Classified Fill (aggregate base course, structural fill, etc.) – 27,000 cubic yards
- Concrete (sidewalks, curbs, building floor slab) – 1,150 cubic yards
- Asphalt (2" thick) – 1,100 cubic yards
- Topsoil – 500 cubic yards

3.3 Flooding

With Peters Creek running along the east side of the property, flooding potential must be considered for this site. The Federal Emergency Management Agency (FEMA) administers the National Flood Insurance Program, which includes mapping floodplains and delineating boundaries of major rivers for different design storm events. FEMA publishes Flood Insurance Rate Maps (FIRM) which identify flood hazard areas including the floodway, and base flood (100-year) and 500-year flood limits. The FIRM for the project site is included in Appendix E. Figure 2 below shows an approximate overlay of the proposed building footprint and property lines on the FIRM.



Figure 2: Proposed Site in Relation to FEMA Floodplains

The critical items to note for this project are that the building is set at elevation 84, and all other proposed site development is outside of the 100-year floodplain. Also, the current site design avoids the placement of any fill or retaining walls within the 100-year floodplain.

FEMA typically reviews developments that include placement of fill within the Special Flood Hazard Area (SFHA), which is identified by the blue and blue/red striped areas in Figure 2. More precise mapping with the FIRM would be beneficial in design, but it is expected that the project would not place any fill or retaining walls within the SFHA. The Alternative A Site Plan (1/10/25) reduced the eastward extent of the parking area, which appears will avoid any fill or wall encroachment within Zone X, or to a very small extent. The brown zone (Zone X) represents a 0.2% annual chance (500-year) of flood hazard. Refer to the FIRM in Appendix E for more detail.

4.0 DRAINAGE (HYDROLOGY AND HYDRAULICS)

4.1 Methodology

In the Municipality of Anchorage (MOA), site peak stormwater runoff from the post-development condition is typically designed to be less than or equal to 1.05 times the pre-development peak flow. For this project, the amount of impervious surface area proposed for the site is substantially more than the existing pre-development condition, which will result in additional stormwater runoff. Onsite subsurface

infiltration will be the main method of reducing stormwater runoff from the site. Infiltration can be accomplished in various ways, including vegetated swales, dry wells, infiltration basins, perforated storm drain pipes, or a combination thereof. Stormwater detention was also considered as an option for this site, however it would likely be less cost effective than infiltration.

For the purposes of this study, we have calculated a total quantity of stormwater to be infiltrated and used an infiltration rate of 1 minute per inch based on initial percolation tests. The type and configuration of infiltration structures and methods will be dependent on the final site layout and must be ultimately determined in the design phase. Further hydraulic analysis of the site and stormwater system would be performed during design and could likely incorporate detention methods with infiltration methods and potentially decrease the footprint of the stormwater infrastructure.

4.2 Results of Analysis

The Rational Method was used to analyze the stormwater runoff for the pre-development condition and a 24-hour stormwater modeling calculation was used for the post-development condition. To be conservative, the 100-year 24-hour storm event was used as the design storm. The Anchorage Stormwater Manual (ASM) provides design guidance for stormwater runoff within the MOA, including design storm depths with location adjustment factors. The total rainfall depth for the 100-year, 24-hour design storm event is 3.59 inches.

Pre-Development Condition:

The Rational Method for peak stormwater runoff states that the flowrate is equal to the runoff coefficient multiplied by the peak rainfall intensity, multiplied by the contributing area. The pre-developed site was analyzed as a single 6.42-acre drainage basin of forest with minimal slopes which results in a runoff coefficient of 0.20. The peak rainfall intensity factor, adjusted for the site location, is 0.75 inches per hour. The result for the pre-development peak stormwater runoff for the 100-year storm event is 0.96 cfs. The full pre-development runoff calculation can be found in Appendix C.

Post-Development Condition:

The site post-development condition was analyzed for the amount of stormwater runoff produced in the 100-year storm event. Because the existing site is so heavily forested, the pre-development runoff is very low compared to the post-development condition. It is probable that some stormwater could run off the site, likely up to 5% more than the amount calculated in the pre-development condition. However, considering the small amount of pre-development runoff and for the intent of being conservative at this stage of the project, stormwater infiltration systems were sized to infiltrate the entire 100-year design storm event.

The site was split into two different basins, Basin A (west side) and Basin B (east side) and analyzed for the 100-year storm event. Runoff coefficients for the different land cover surfaces were incorporated per the proposed concept site plan. An infiltration rate of 60 inches per hour was used for the infiltration system founded in the in-situ soils, correlating with the percolation tests performed in July of 2024. The calculation resulted in an infiltration field or “trench” size of 2,400 sf by 3 ft deep for Basin A, and 1,600 sf by 3 ft deep for Basin B. The full infiltration calculation can be found in Appendix D.

The proposed stormwater infiltration system would include rows of buried half round pipe or chambers that would distribute the stormwater over a bed of well-draining rock material. The system would be designed to withstand vehicle traffic loading and be installed beneath the paved parking lot, however the first chamber is typically designed to be accessible for periodically removing sediment. The system

would be designed so that an overflow point would be near the low end of the site. If the infiltration bed backed up, drainage would overflow offsite at an elevation lower than the building finished floor.

As an alternative to an infiltration system, a stormwater detention system could be provided, designed to limit the peak offsite flowrate to no greater than the predevelopment peak. The detention storage would be provided by buried pipes or chambers, rather than a pond, given the site limitations. Preliminary calculations were performed for a stormwater detention system. In general, the detention system would require 4,000 – 5,000 linear feet of 24-inch diameter pipe to store the site stormwater for the 10-year storm event and allow controlled discharge similar to the peak pre-development rate. Larger diameter pipe would decrease the total length of pipe required, however there is a limit on how deep the pipe can be buried as it has to gravity drain to a discharge point near the low end of the site. Based on preliminary calculations, a stormwater detention system for this site would be more costly than an infiltration system.

If additional stormwater treatment is desired, sediment traps and grit chambers can be provided upstream of infiltration or detention systems, to remove detrimental levels of sediment and other debris. These measures can increase the level of treatment the system provides and facilitate maintenance. An operations and maintenance plan should also be considered for the facility to include bi-annual sweeping of the paved surfaces, especially in the springtime, to remove sand/gravel spread for traction throughout the winter months. Storm drain inlet structures should also be routinely cleaned out to further reduce the amount of sediment and debris migrating to the infiltration or detention system. Pretreatment and regular maintenance is recommended as it will extend the life of the stormwater system.

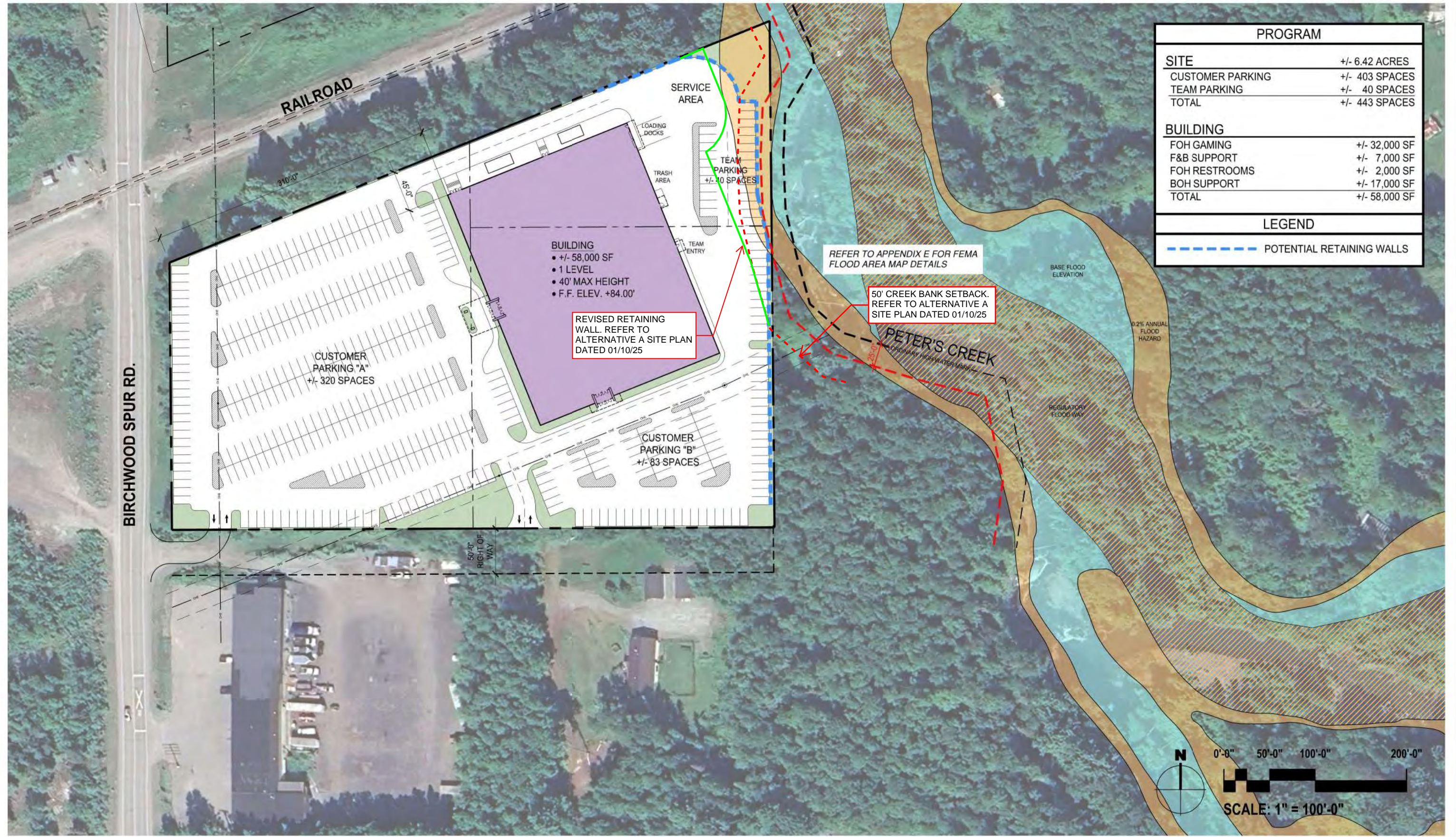
5.0 REFERENCES

Anchorage Stormwater Manual (ASM), Volume 1 Management and Design Criteria, Version 1.0 December 2017, Municipality of Anchorage (MOA).

Federal Emergency Management Agency (FEMA), July 8, 2020

APPENDIX A

1. Concept Site Plan
2. Existing Site Topography



PROGRAM	
SITE	+/- 6.42 ACRES
CUSTOMER PARKING	+/- 403 SPACES
TEAM PARKING	+/- 40 SPACES
TOTAL	+/- 443 SPACES
BUILDING	
FOH GAMING	+/- 32,000 SF
F&B SUPPORT	+/- 7,000 SF
FOH RESTROOMS	+/- 2,000 SF
BOH SUPPORT	+/- 17,000 SF
TOTAL	+/- 58,000 SF
LEGEND	
	POTENTIAL RETAINING WALLS

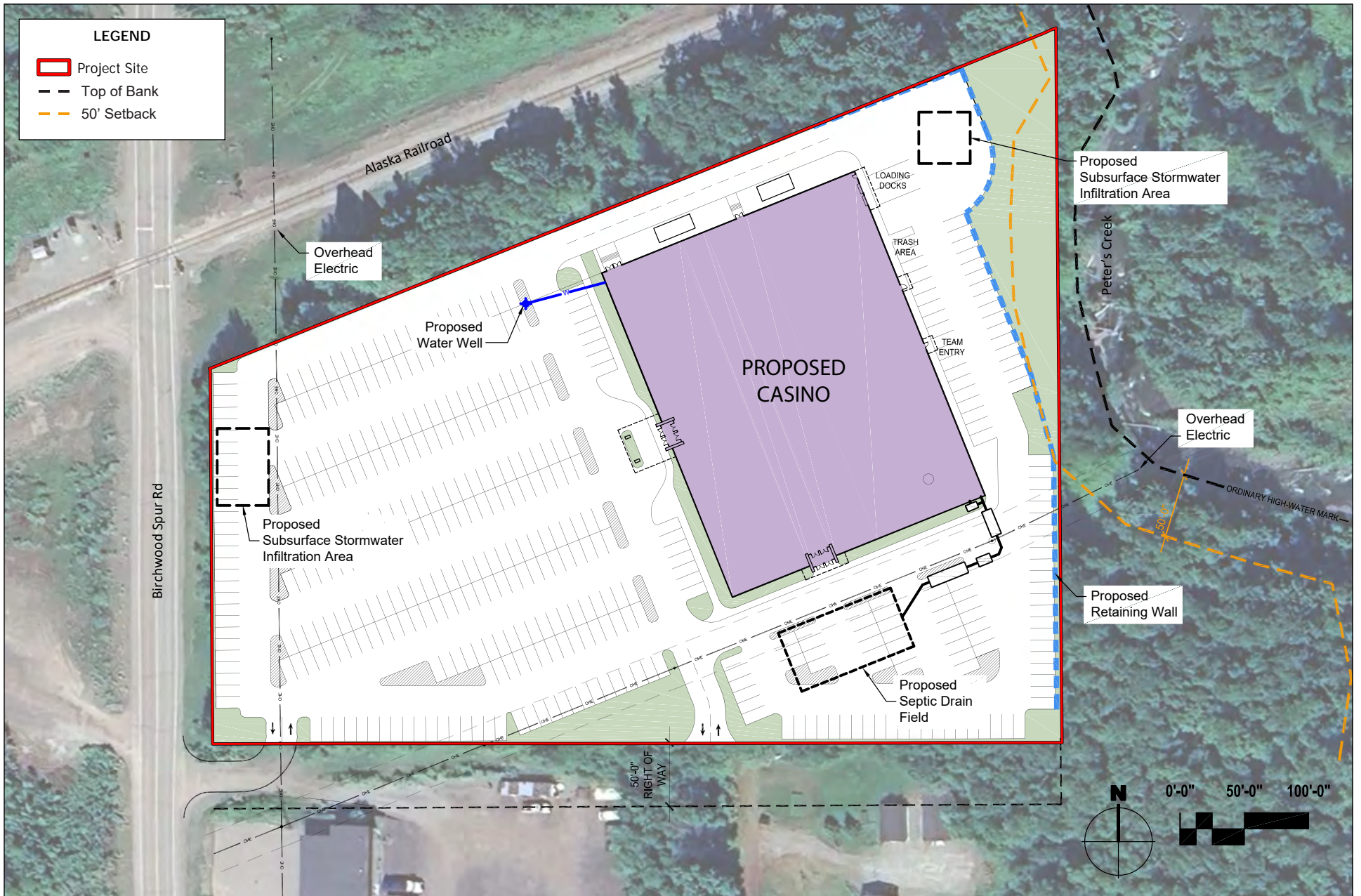
BUILDING
 • +/- 58,000 SF
 • 1 LEVEL
 • 40' MAX HEIGHT
 • F.F. ELEV. +84.00'

REVISED RETAINING WALL. REFER TO ALTERNATIVE A SITE PLAN DATED 01/10/25

REFER TO APPENDIX E FOR FEMA FLOOD AREA MAP DETAILS

50' CREEK BANK SETBACK. REFER TO ALTERNATIVE A SITE PLAN DATED 01/10/25

EKLUTNA GAMING HALL - SITE PLAN STUDY
 CONCEPT SITE PLAN



Source: Marnell Companies, 1/7/2025, Acorn Environmental, 1/10/2025

FIGURE 2.1-1
ALTERNATIVE A SITE PLAN



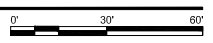
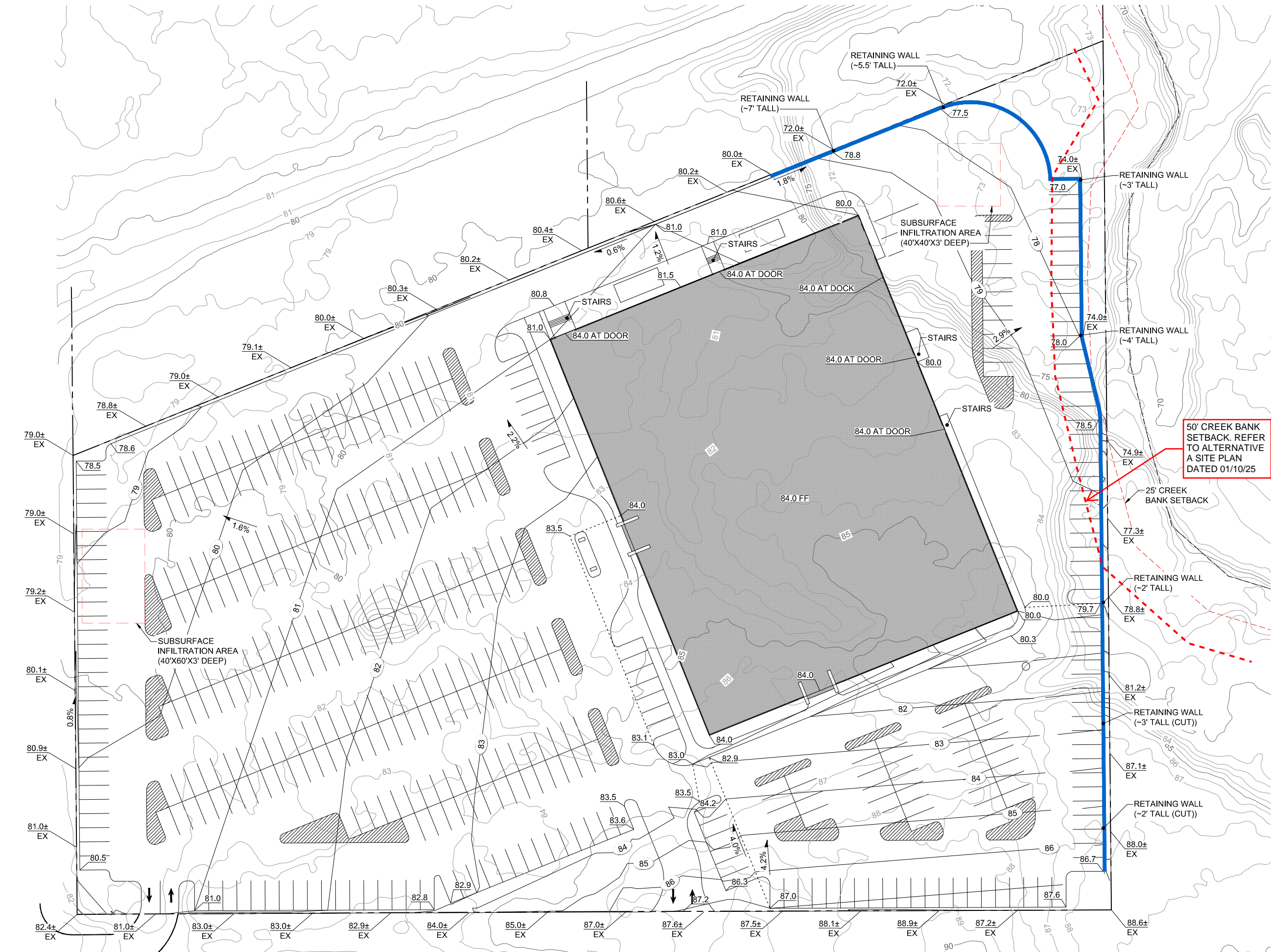
APPENDIX B

Concept Grading and Drainage Sketch

NOTES

- 1. STORM DRAIN SYSTEM NOT SHOWN.

50' CREEK BANK SETBACK. REFER TO ALTERNATIVE A SITE PLAN DATED 01/10/25



APPENDIX C

Pre-Development Runoff Calculation

DRAINAGE CALCULATION WORKSHEET
Pre-development, 100 yr

Date: 7-Jul-24

By: RPP/KIM

Project Name: Eklutna Native Village Gaming Hall - Grading and Drainage Study EEI Project No.: 10336

DRAINAGE BASIN

On-Site Drainage Basin		
Subbasin I.D.	Area, sf	Area, ac
Basin	279,696	6.42
Totals	279,696	6.42

Calculated weighted C value per MOA ASM Vol. 1 Table 4.4-2

Basin	Characterization	Area, acres	% Area	Avg HSG	Avg Slope	C (Factor)	C (Weighted)
Forest, Brush		279,696	6.42	100.0%	D	2-6%	0.20
Totals		279,696	6.42	100%			0.200

TIME OF CONCENTRATION

Basin
 Total Length (ft) 800

Overland Flow: $T_c = 0.225(L^{0.42})(S^{-0.19})(C^{-1.0})$

Length, L (ft) 150
 Average Slope, S (ft/ft) 0.012
 Runoff Coefficient, C 0.160
 Flow Time, T_c (min) 26.83

Shallow Concentrated Flow: $T = L/33kS^{0.5}$

Length, L (ft) 650
 Average Slope, S (ft/ft) 0.012
 Surface type forest
 k value 0.076
 Flow Time, T (min) 39.82

Time of Concentration, D (min): 66.65

RAINFALL INTENSITY

The peak rainfalls are interpolated from Fig. 4.2-1, MOA ASM Vol. 1, IDF Values. This storm is adjusted by the Orographic Factor, adjusting specific site to Anchorage data.

Peak rainfall intensity (Figure 4.2-1) **100 -YEAR** 0.75
Basin
 Orographic Factor (Fig 4.2-3 MOA ASM Vol.1) 1.000
 Adjusted Peak Rainfall (I_{pa}=I_p x PI): 0.75

PEAK STORM RUNOFF

Peak runoff is calculated using the Rational Method formula, $Q=C \times I \times A$

Where Q=peak runoff (c.f.s.)

C=Runoff Coefficient

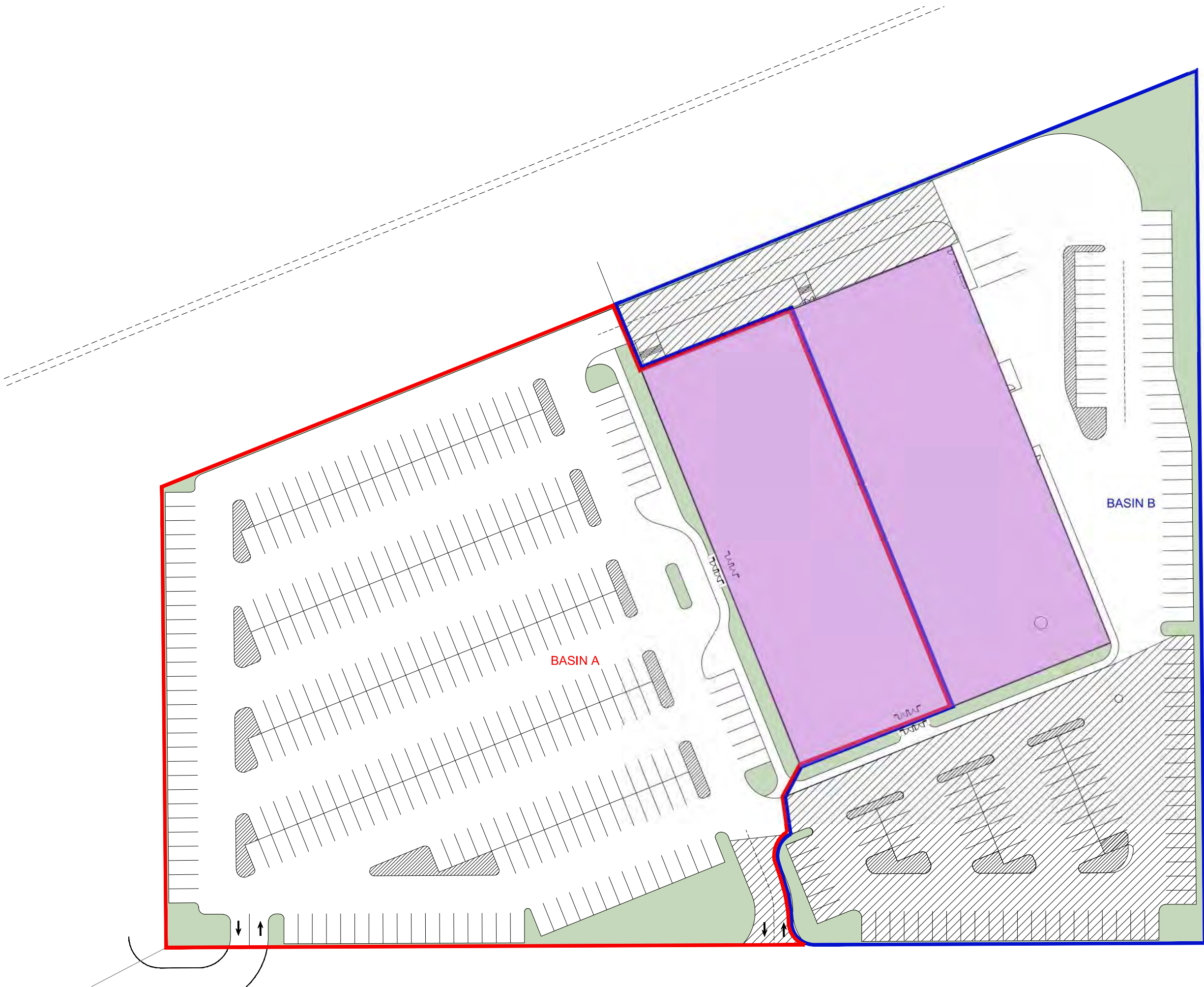
I=Peak rainfall intensity (I_{pa}, in/hr)

A=Area of contributing basin (acres)







	Total	Basin
		0.200
		0.75
		6.42
Q (cfs)	0.96	0.96

APPENDIX D

Stormwater Infiltration Calculation and Basins Exhibit



LEGEND

-  LANDSCAPE (0-2%)
-  PAVEMENT (0-2%)
-  PAVEMENT (2-6%)
-  ROOF
-  BASIN A
-  BASIN B

NOTE: THE "ALTERNATIVE A SITE PLAN" DATED 01/10/25 INCORPORATES A 50' SETBACK FROM THE CREEK WHICH REDUCES THE AMOUNT OF PAVEMENT IN BASIN B. THE FOLLOWING CALCULATIONS ARE THEREFORE CONSERVATIVE.

Project: Eklutna Native Village Gaming Hall - Grading and Drainage Study
 EEI Project No: 10336
 Calculated By: Ryan Parpan Date: 8/13/2024
 Checked By: Carl Bassler Date: 8/14/2024

INFILTRATION CALCULATION - BASIN A

Determine Size of Infiltration Basin. Runoff based on rational method coefficients. 100-year, 24-hour storm used due to being only means of drainage (i.e. no storm drain system backup). Storm data from Anchorage Design Criteria Manual Chapter 2 Drainage. Calculation uses time-step intervals.

Basin A Landcover

Landcover	Area (SF)		Runoff Coefficient	
	A	C	C*A	
Landscape (0-2%)	17,228	0.17	2,929	
Pavement (0-2%)	113,392	0.95	107,722	
Pavement (2-6%)	1,870	0.96	1,795	
Roof	29,000	1.00	29,000	
			0	
Total	161,490		141,446	
Composite Runoff Coefficient		0.876		

Basin A Parameters

Infiltration Pipe and Trench				Storm Data	
Infil Rate	K	60	IN/HR	Select storm	100
Perf Pipe Diam	D _p	0	FT	Total Rainfall	3.59
Trench Width	W _r	40	FT		
Rock Height	H _r	3	FT		
Trench Length	L _t	60	FT		
Pipe Length	L _p	0	FT	Area factor	1.00
Porosity	n	0.35	ratio	Time interval	6.00
Additional Storage Volume	V _a	0	FT ³		

Calculated System Data

Pipe Volume	0	V_p	FT^3	$L_p * \pi * (D_p/2)^2$
Rock Void Volume	2,520	V_r	FT^3	$(L_r * W_r * H_r - V_p) * n$
Total Store Volume	2,520	V_t	FT^3	$V_p + V_r + V_a$
Bottom Area	2,400	A_b	FT^3	$L * W$
Base Infiltration Rate	0.001389	I_b	FT/S	$k/(12*3600)$
Base Flow Rate	3.33	Q_b	FT^3/S	$I_b * A_b$

Basin Performance Results

On-site Peak Flow	8.86	FT^3/S	Q_r Max
Off-site Peak Flow	0.00	FT^3/S	Q_o Max
Off-site Volume	0	FT^3	V_o Max
Trench Inf Volume	42,424	FT^3	Sum Infiltration Rate * Time Interval

Off-site volume is 0. Entire 100-year, 24-hour storm is infiltrated.

Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	Volume Totals, Accumulated		
									On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q_r	Q_{inf}	Q_e	V_e	V_s	Q_o	V_{inf}	V_o	V_r
HR		IN/HR	FT^3/S	FT^3/S	FT^3/S	FT^3	FT^3	FT^3/S	FT^3	FT^3	FT^3
t = timeprev + timeint		I(t)	$Q(t) = C * I(t) * A$	$Q_{inf} = I_b * A_b * (head\ increase)$	$Q_e = Q_r - Q_{inf}$	$V_e = Q_e * timeint$	$V_s = \Sigma V_e$ (if $< V_t$)	$Q_o = Q_e$ or 0	$V_{inf} = V_{inf}prev + Q_{inf} * timeint$	$V_o = Q_o * timeint + V_o prev - \Delta V_s$	$V_r = V_r prev + Q_r * timeint$
0	0.000	0.000	0.000	0.000	0.000	0.00	0.00	0.00	0.0	0.0	0.0
0.1	0.002	0.072	0.235	0.235	0.000	0.00	0.00	0.00	84.6	0.0	84.6
0.2	0.004	0.068	0.223	0.223	0.000	0.00	0.00	0.00	165.0	0.0	165.0
0.3	0.006	0.072	0.235	0.235	0.000	0.00	0.00	0.00	249.7	0.0	249.7
0.4	0.008	0.072	0.235	0.235	0.000	0.00	0.00	0.00	334.3	0.0	334.3
0.5	0.010	0.072	0.235	0.235	0.000	0.00	0.00	0.00	418.9	0.0	418.9
0.6	0.012	0.072	0.235	0.235	0.000	0.00	0.00	0.00	503.6	0.0	503.6
0.7	0.014	0.075	0.247	0.247	0.000	0.00	0.00	0.00	592.4	0.0	592.4
0.8	0.016	0.072	0.235	0.235	0.000	0.00	0.00	0.00	677.1	0.0	677.1
0.9	0.018	0.072	0.235	0.235	0.000	0.00	0.00	0.00	761.7	0.0	761.7
1	0.020	0.075	0.247	0.247	0.000	0.00	0.00	0.00	850.6	0.0	850.6
1.1	0.022	0.072	0.235	0.235	0.000	0.00	0.00	0.00	935.2	0.0	935.2
1.2	0.024	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,024.0	0.0	1,024.0
1.3	0.026	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,112.9	0.0	1,112.9
1.4	0.028	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,201.8	0.0	1,201.8
1.5	0.031	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,290.6	0.0	1,290.6
1.6	0.033	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,379.5	0.0	1,379.5

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
1.7	0.035	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,468.4	0.0	1,468.4
1.8	0.037	0.079	0.259	0.259	0.000	0.00	0.00	0.00	1,561.5	0.0	1,561.5
1.9	0.039	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,650.3	0.0	1,650.3
2	0.041	0.079	0.259	0.259	0.000	0.00	0.00	0.00	1,743.4	0.0	1,743.4
2.1	0.043	0.075	0.247	0.247	0.000	0.00	0.00	0.00	1,832.3	0.0	1,832.3
2.2	0.046	0.079	0.259	0.259	0.000	0.00	0.00	0.00	1,925.4	0.0	1,925.4
2.3	0.048	0.079	0.259	0.259	0.000	0.00	0.00	0.00	2,018.5	0.0	2,018.5
2.4	0.050	0.079	0.259	0.259	0.000	0.00	0.00	0.00	2,111.6	0.0	2,111.6
2.5	0.052	0.079	0.259	0.259	0.000	0.00	0.00	0.00	2,204.7	0.0	2,204.7
2.6	0.054	0.079	0.259	0.259	0.000	0.00	0.00	0.00	2,297.8	0.0	2,297.8
2.7	0.057	0.083	0.270	0.270	0.000	0.00	0.00	0.00	2,395.1	0.0	2,395.1
2.8	0.059	0.079	0.259	0.259	0.000	0.00	0.00	0.00	2,488.2	0.0	2,488.2
2.9	0.061	0.083	0.270	0.270	0.000	0.00	0.00	0.00	2,585.5	0.0	2,585.5
3	0.063	0.083	0.270	0.270	0.000	0.00	0.00	0.00	2,682.8	0.0	2,682.8
3.1	0.066	0.083	0.270	0.270	0.000	0.00	0.00	0.00	2,780.2	0.0	2,780.2
3.2	0.068	0.083	0.270	0.270	0.000	0.00	0.00	0.00	2,877.5	0.0	2,877.5
3.3	0.070	0.083	0.270	0.270	0.000	0.00	0.00	0.00	2,974.8	0.0	2,974.8
3.4	0.073	0.083	0.270	0.270	0.000	0.00	0.00	0.00	3,072.1	0.0	3,072.1
3.5	0.075	0.086	0.282	0.282	0.000	0.00	0.00	0.00	3,173.7	0.0	3,173.7
3.6	0.077	0.083	0.270	0.270	0.000	0.00	0.00	0.00	3,271.0	0.0	3,271.0
3.7	0.080	0.086	0.282	0.282	0.000	0.00	0.00	0.00	3,372.6	0.0	3,372.6
3.8	0.082	0.086	0.282	0.282	0.000	0.00	0.00	0.00	3,474.1	0.0	3,474.1
3.9	0.085	0.086	0.282	0.282	0.000	0.00	0.00	0.00	3,575.7	0.0	3,575.7
4	0.087	0.086	0.282	0.282	0.000	0.00	0.00	0.00	3,677.3	0.0	3,677.3
4.1	0.089	0.090	0.294	0.294	0.000	0.00	0.00	0.00	3,783.1	0.0	3,783.1
4.2	0.092	0.086	0.282	0.282	0.000	0.00	0.00	0.00	3,884.6	0.0	3,884.6
4.3	0.094	0.090	0.294	0.294	0.000	0.00	0.00	0.00	3,990.4	0.0	3,990.4
4.4	0.097	0.090	0.294	0.294	0.000	0.00	0.00	0.00	4,096.2	0.0	4,096.2
4.5	0.099	0.090	0.294	0.294	0.000	0.00	0.00	0.00	4,202.0	0.0	4,202.0
4.6	0.102	0.090	0.294	0.294	0.000	0.00	0.00	0.00	4,307.8	0.0	4,307.8
4.7	0.104	0.090	0.294	0.294	0.000	0.00	0.00	0.00	4,413.6	0.0	4,413.6
4.8	0.107	0.093	0.306	0.306	0.000	0.00	0.00	0.00	4,523.6	0.0	4,523.6
4.9	0.110	0.093	0.306	0.306	0.000	0.00	0.00	0.00	4,633.6	0.0	4,633.6
5	0.112	0.093	0.306	0.306	0.000	0.00	0.00	0.00	4,743.6	0.0	4,743.6
5.1	0.115	0.093	0.306	0.306	0.000	0.00	0.00	0.00	4,853.6	0.0	4,853.6
5.2	0.117	0.093	0.306	0.306	0.000	0.00	0.00	0.00	4,963.7	0.0	4,963.7
5.3	0.120	0.097	0.317	0.317	0.000	0.00	0.00	0.00	5,077.9	0.0	5,077.9
5.4	0.123	0.097	0.317	0.317	0.000	0.00	0.00	0.00	5,192.2	0.0	5,192.2
5.5	0.125	0.097	0.317	0.317	0.000	0.00	0.00	0.00	5,306.4	0.0	5,306.4
5.6	0.128	0.097	0.317	0.317	0.000	0.00	0.00	0.00	5,420.7	0.0	5,420.7

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
5.7	0.131	0.097	0.317	0.317	0.000	0.00	0.00	0.00	5,534.9	0.0	5,534.9
5.8	0.134	0.101	0.329	0.329	0.000	0.00	0.00	0.00	5,653.4	0.0	5,653.4
5.9	0.136	0.101	0.329	0.329	0.000	0.00	0.00	0.00	5,771.9	0.0	5,771.9
6	0.139	0.101	0.329	0.329	0.000	0.00	0.00	0.00	5,890.4	0.0	5,890.4
6.1	0.142	0.104	0.341	0.341	0.000	0.00	0.00	0.00	6,013.1	0.0	6,013.1
6.2	0.145	0.101	0.329	0.329	0.000	0.00	0.00	0.00	6,131.6	0.0	6,131.6
6.3	0.148	0.104	0.341	0.341	0.000	0.00	0.00	0.00	6,254.3	0.0	6,254.3
6.4	0.151	0.104	0.341	0.341	0.000	0.00	0.00	0.00	6,377.0	0.0	6,377.0
6.5	0.154	0.108	0.353	0.353	0.000	0.00	0.00	0.00	6,504.0	0.0	6,504.0
6.6	0.157	0.108	0.353	0.353	0.000	0.00	0.00	0.00	6,630.9	0.0	6,630.9
6.7	0.160	0.108	0.353	0.353	0.000	0.00	0.00	0.00	6,757.9	0.0	6,757.9
6.8	0.163	0.108	0.353	0.353	0.000	0.00	0.00	0.00	6,884.8	0.0	6,884.8
6.9	0.166	0.111	0.364	0.364	0.000	0.00	0.00	0.00	7,016.0	0.0	7,016.0
7	0.169	0.111	0.364	0.364	0.000	0.00	0.00	0.00	7,147.2	0.0	7,147.2
7.1	0.172	0.111	0.364	0.364	0.000	0.00	0.00	0.00	7,278.4	0.0	7,278.4
7.2	0.175	0.115	0.376	0.376	0.000	0.00	0.00	0.00	7,413.8	0.0	7,413.8
7.3	0.178	0.115	0.376	0.376	0.000	0.00	0.00	0.00	7,549.2	0.0	7,549.2
7.4	0.182	0.115	0.376	0.376	0.000	0.00	0.00	0.00	7,684.6	0.0	7,684.6
7.5	0.185	0.118	0.388	0.388	0.000	0.00	0.00	0.00	7,824.2	0.0	7,824.2
7.6	0.188	0.118	0.388	0.388	0.000	0.00	0.00	0.00	7,963.9	0.0	7,963.9
7.7	0.192	0.118	0.388	0.388	0.000	0.00	0.00	0.00	8,103.5	0.0	8,103.5
7.8	0.195	0.122	0.400	0.400	0.000	0.00	0.00	0.00	8,247.4	0.0	8,247.4
7.9	0.198	0.122	0.400	0.400	0.000	0.00	0.00	0.00	8,391.3	0.0	8,391.3
8	0.202	0.126	0.411	0.411	0.000	0.00	0.00	0.00	8,539.4	0.0	8,539.4
8.1	0.205	0.126	0.411	0.411	0.000	0.00	0.00	0.00	8,687.5	0.0	8,687.5
8.2	0.209	0.129	0.423	0.423	0.000	0.00	0.00	0.00	8,839.8	0.0	8,839.8
8.3	0.213	0.129	0.423	0.423	0.000	0.00	0.00	0.00	8,992.2	0.0	8,992.2
8.4	0.216	0.133	0.435	0.435	0.000	0.00	0.00	0.00	9,148.7	0.0	9,148.7
8.5	0.220	0.133	0.435	0.435	0.000	0.00	0.00	0.00	9,305.3	0.0	9,305.3
8.6	0.224	0.136	0.447	0.447	0.000	0.00	0.00	0.00	9,466.1	0.0	9,466.1
8.7	0.228	0.140	0.458	0.458	0.000	0.00	0.00	0.00	9,631.1	0.0	9,631.1
8.8	0.232	0.140	0.458	0.458	0.000	0.00	0.00	0.00	9,796.2	0.0	9,796.2
8.9	0.235	0.140	0.458	0.458	0.000	0.00	0.00	0.00	9,961.2	0.0	9,961.2
9	0.240	0.147	0.482	0.482	0.000	0.00	0.00	0.00	10,134.7	0.0	10,134.7
9.1	0.244	0.147	0.482	0.482	0.000	0.00	0.00	0.00	10,308.2	0.0	10,308.2
9.2	0.248	0.151	0.494	0.494	0.000	0.00	0.00	0.00	10,485.9	0.0	10,485.9
9.3	0.252	0.151	0.494	0.494	0.000	0.00	0.00	0.00	10,663.6	0.0	10,663.6
9.4	0.256	0.158	0.517	0.517	0.000	0.00	0.00	0.00	10,849.8	0.0	10,849.8
9.5	0.261	0.158	0.517	0.517	0.000	0.00	0.00	0.00	11,036.0	0.0	11,036.0
9.6	0.265	0.162	0.529	0.529	0.000	0.00	0.00	0.00	11,226.4	0.0	11,226.4

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
9.7	0.270	0.165	0.541	0.541	0.000	0.00	0.00	0.00	11,421.1	0.0	11,421.1
9.8	0.275	0.172	0.564	0.564	0.000	0.00	0.00	0.00	11,624.2	0.0	11,624.2
9.9	0.280	0.172	0.564	0.564	0.000	0.00	0.00	0.00	11,827.3	0.0	11,827.3
10	0.285	0.179	0.588	0.588	0.000	0.00	0.00	0.00	12,038.9	0.0	12,038.9
10.1	0.290	0.183	0.599	0.599	0.000	0.00	0.00	0.00	12,254.7	0.0	12,254.7
10.2	0.295	0.187	0.611	0.611	0.000	0.00	0.00	0.00	12,474.8	0.0	12,474.8
10.3	0.300	0.194	0.635	0.635	0.000	0.00	0.00	0.00	12,703.3	0.0	12,703.3
10.4	0.306	0.201	0.658	0.658	0.000	0.00	0.00	0.00	12,940.2	0.0	12,940.2
10.5	0.312	0.205	0.670	0.670	0.000	0.00	0.00	0.00	13,181.4	0.0	13,181.4
10.6	0.318	0.215	0.705	0.705	0.000	0.00	0.00	0.00	13,435.3	0.0	13,435.3
10.7	0.324	0.219	0.717	0.717	0.000	0.00	0.00	0.00	13,693.5	0.0	13,693.5
10.8	0.330	0.233	0.764	0.764	0.000	0.00	0.00	0.00	13,968.5	0.0	13,968.5
10.9	0.337	0.241	0.788	0.788	0.000	0.00	0.00	0.00	14,252.0	0.0	14,252.0
11	0.344	0.251	0.823	0.823	0.000	0.00	0.00	0.00	14,548.3	0.0	14,548.3
11.1	0.351	0.266	0.870	0.870	0.000	0.00	0.00	0.00	14,861.4	0.0	14,861.4
11.2	0.359	0.284	0.929	0.929	0.000	0.00	0.00	0.00	15,195.7	0.0	15,195.7
11.3	0.367	0.298	0.976	0.976	0.000	0.00	0.00	0.00	15,546.9	0.0	15,546.9
11.4	0.376	0.323	1.058	1.058	0.000	0.00	0.00	0.00	15,927.8	0.0	15,927.8
11.5	0.386	0.352	1.152	1.152	0.000	0.00	0.00	0.00	16,342.5	0.0	16,342.5
11.6	0.397	0.388	1.269	1.269	0.000	0.00	0.00	0.00	16,799.5	0.0	16,799.5
11.7	0.409	0.442	1.446	1.446	0.000	0.00	0.00	0.00	17,320.0	0.0	17,320.0
11.8	0.424	0.521	1.704	1.704	0.000	0.00	0.00	0.00	17,933.5	0.0	17,933.5
11.9	0.442	0.657	2.151	2.151	0.000	0.00	0.00	0.00	18,707.9	0.0	18,707.9
12	0.471	1.048	3.432	3.333	0.099	35.63	35.63	0.00	19,907.9	0.0	19,943.5
12.1	0.547	2.707	8.863	3.380	5.482	1973.66	2,009.29	0.00	21,124.9	0.0	23,134.2
12.2	0.569	0.790	2.586	5.991	-3.405	-1225.85	783.44	0.00	23,281.7	0.0	24,065.1
12.3	0.585	0.578	1.892	4.370	-2.477	-891.78	0.00	0.00	24,854.8	0.0	24,746.4
12.4	0.598	0.474	1.552	1.552	0.000	0.00	0.00	0.00	25,413.3	0.0	25,305.0
12.5	0.610	0.413	1.352	1.352	0.000	0.00	0.00	0.00	25,900.0	0.0	25,791.6
12.6	0.620	0.370	1.211	1.211	0.000	0.00	0.00	0.00	26,335.8	0.0	26,227.5
12.7	0.629	0.337	1.105	1.105	0.000	0.00	0.00	0.00	26,733.6	0.0	26,625.2
12.8	0.638	0.309	1.011	1.011	0.000	0.00	0.00	0.00	27,097.5	0.0	26,989.2
12.9	0.646	0.291	0.952	0.952	0.000	0.00	0.00	0.00	27,440.3	0.0	27,331.9
13	0.654	0.273	0.893	0.893	0.000	0.00	0.00	0.00	27,761.9	0.0	27,653.5
13.1	0.661	0.258	0.846	0.846	0.000	0.00	0.00	0.00	28,066.5	0.0	27,958.2
13.2	0.668	0.248	0.811	0.811	0.000	0.00	0.00	0.00	28,358.5	0.0	28,250.2
13.3	0.674	0.237	0.776	0.776	0.000	0.00	0.00	0.00	28,637.8	0.0	28,529.5
13.4	0.681	0.226	0.741	0.741	0.000	0.00	0.00	0.00	28,904.4	0.0	28,796.1
13.5	0.687	0.215	0.705	0.705	0.000	0.00	0.00	0.00	29,158.3	0.0	29,050.0
13.6	0.692	0.212	0.694	0.694	0.000	0.00	0.00	0.00	29,408.0	0.0	29,299.6

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
13.7	0.698	0.201	0.658	0.658	0.000	0.00	0.00	0.00	29,644.9	0.0	29,536.6
13.8	0.704	0.197	0.646	0.646	0.000	0.00	0.00	0.00	29,877.7	0.0	29,769.3
13.9	0.709	0.190	0.623	0.623	0.000	0.00	0.00	0.00	30,101.9	0.0	29,993.6
14	0.714	0.187	0.611	0.611	0.000	0.00	0.00	0.00	30,322.0	0.0	30,213.6
14.1	0.719	0.180	0.588	0.588	0.000	0.00	0.00	0.00	30,533.6	0.0	30,425.2
14.2	0.724	0.176	0.576	0.576	0.000	0.00	0.00	0.00	30,740.9	0.0	30,632.6
14.3	0.729	0.172	0.564	0.564	0.000	0.00	0.00	0.00	30,944.0	0.0	30,835.7
14.4	0.733	0.169	0.552	0.552	0.000	0.00	0.00	0.00	31,142.9	0.0	31,034.6
14.5	0.738	0.162	0.529	0.529	0.000	0.00	0.00	0.00	31,333.3	0.0	31,225.0
14.6	0.742	0.162	0.529	0.529	0.000	0.00	0.00	0.00	31,523.8	0.0	31,415.4
14.7	0.747	0.158	0.517	0.517	0.000	0.00	0.00	0.00	31,710.0	0.0	31,601.6
14.8	0.751	0.154	0.505	0.505	0.000	0.00	0.00	0.00	31,891.9	0.0	31,783.6
14.9	0.755	0.151	0.494	0.494	0.000	0.00	0.00	0.00	32,069.6	0.0	31,961.3
15	0.760	0.151	0.494	0.494	0.000	0.00	0.00	0.00	32,247.4	0.0	32,139.0
15.1	0.764	0.144	0.470	0.470	0.000	0.00	0.00	0.00	32,416.6	0.0	32,308.3
15.2	0.768	0.144	0.470	0.470	0.000	0.00	0.00	0.00	32,585.9	0.0	32,477.6
15.3	0.772	0.144	0.470	0.470	0.000	0.00	0.00	0.00	32,755.2	0.0	32,646.8
15.4	0.775	0.140	0.458	0.458	0.000	0.00	0.00	0.00	32,920.2	0.0	32,811.9
15.5	0.779	0.136	0.447	0.447	0.000	0.00	0.00	0.00	33,081.0	0.0	32,972.7
15.6	0.783	0.133	0.435	0.435	0.000	0.00	0.00	0.00	33,237.6	0.0	33,129.2
15.7	0.787	0.133	0.435	0.435	0.000	0.00	0.00	0.00	33,394.1	0.0	33,285.8
15.8	0.790	0.133	0.435	0.435	0.000	0.00	0.00	0.00	33,550.7	0.0	33,442.4
15.9	0.794	0.129	0.423	0.423	0.000	0.00	0.00	0.00	33,703.0	0.0	33,594.7
16	0.797	0.126	0.411	0.411	0.000	0.00	0.00	0.00	33,851.1	0.0	33,742.8
16.1	0.801	0.126	0.411	0.411	0.000	0.00	0.00	0.00	33,999.3	0.0	33,890.9
16.2	0.804	0.126	0.411	0.411	0.000	0.00	0.00	0.00	34,147.4	0.0	34,039.0
16.3	0.808	0.122	0.400	0.400	0.000	0.00	0.00	0.00	34,291.2	0.0	34,182.9
16.4	0.811	0.118	0.388	0.388	0.000	0.00	0.00	0.00	34,430.9	0.0	34,322.5
16.5	0.815	0.122	0.400	0.400	0.000	0.00	0.00	0.00	34,574.8	0.0	34,466.4
16.6	0.818	0.118	0.388	0.388	0.000	0.00	0.00	0.00	34,714.4	0.0	34,606.1
16.7	0.821	0.115	0.376	0.376	0.000	0.00	0.00	0.00	34,849.8	0.0	34,741.5
16.8	0.824	0.115	0.376	0.376	0.000	0.00	0.00	0.00	34,985.2	0.0	34,876.9
16.9	0.827	0.115	0.376	0.376	0.000	0.00	0.00	0.00	35,120.6	0.0	35,012.3
17	0.831	0.115	0.376	0.376	0.000	0.00	0.00	0.00	35,256.0	0.0	35,147.7
17.1	0.834	0.111	0.364	0.364	0.000	0.00	0.00	0.00	35,387.2	0.0	35,278.9
17.2	0.837	0.108	0.353	0.353	0.000	0.00	0.00	0.00	35,514.2	0.0	35,405.8
17.3	0.840	0.111	0.364	0.364	0.000	0.00	0.00	0.00	35,645.3	0.0	35,537.0
17.4	0.843	0.108	0.353	0.353	0.000	0.00	0.00	0.00	35,772.3	0.0	35,664.0
17.5	0.846	0.108	0.353	0.353	0.000	0.00	0.00	0.00	35,899.2	0.0	35,790.9
17.6	0.849	0.108	0.353	0.353	0.000	0.00	0.00	0.00	36,026.2	0.0	35,917.9

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
17.7	0.852	0.104	0.341	0.341	0.000	0.00	0.00	0.00	36,148.9	0.0	36,040.6
17.8	0.855	0.104	0.341	0.341	0.000	0.00	0.00	0.00	36,271.6	0.0	36,163.3
17.9	0.858	0.104	0.341	0.341	0.000	0.00	0.00	0.00	36,394.3	0.0	36,286.0
18	0.860	0.101	0.329	0.329	0.000	0.00	0.00	0.00	36,512.8	0.0	36,404.5
18.1	0.863	0.104	0.341	0.341	0.000	0.00	0.00	0.00	36,635.5	0.0	36,527.2
18.2	0.866	0.101	0.329	0.329	0.000	0.00	0.00	0.00	36,754.0	0.0	36,645.7
18.3	0.869	0.101	0.329	0.329	0.000	0.00	0.00	0.00	36,872.5	0.0	36,764.2
18.4	0.872	0.097	0.317	0.317	0.000	0.00	0.00	0.00	36,986.8	0.0	36,878.4
18.5	0.874	0.097	0.317	0.317	0.000	0.00	0.00	0.00	37,101.0	0.0	36,992.7
18.6	0.877	0.101	0.329	0.329	0.000	0.00	0.00	0.00	37,219.5	0.0	37,111.2
18.7	0.880	0.093	0.306	0.306	0.000	0.00	0.00	0.00	37,329.5	0.0	37,221.2
18.8	0.882	0.097	0.317	0.317	0.000	0.00	0.00	0.00	37,443.8	0.0	37,335.4
18.9	0.885	0.097	0.317	0.317	0.000	0.00	0.00	0.00	37,558.0	0.0	37,449.7
19	0.888	0.093	0.306	0.306	0.000	0.00	0.00	0.00	37,668.1	0.0	37,559.7
19.1	0.890	0.093	0.306	0.306	0.000	0.00	0.00	0.00	37,778.1	0.0	37,669.7
19.2	0.893	0.093	0.306	0.306	0.000	0.00	0.00	0.00	37,888.1	0.0	37,779.8
19.3	0.895	0.093	0.306	0.306	0.000	0.00	0.00	0.00	37,998.1	0.0	37,889.8
19.4	0.898	0.090	0.294	0.294	0.000	0.00	0.00	0.00	38,103.9	0.0	37,995.6
19.5	0.900	0.090	0.294	0.294	0.000	0.00	0.00	0.00	38,209.7	0.0	38,101.4
19.6	0.903	0.093	0.306	0.306	0.000	0.00	0.00	0.00	38,319.7	0.0	38,211.4
19.7	0.906	0.090	0.294	0.294	0.000	0.00	0.00	0.00	38,425.5	0.0	38,317.2
19.8	0.908	0.086	0.282	0.282	0.000	0.00	0.00	0.00	38,527.1	0.0	38,418.7
19.9	0.910	0.090	0.294	0.294	0.000	0.00	0.00	0.00	38,632.9	0.0	38,524.5
20	0.913	0.086	0.282	0.282	0.000	0.00	0.00	0.00	38,734.4	0.0	38,626.1
20.1	0.915	0.090	0.294	0.294	0.000	0.00	0.00	0.00	38,840.2	0.0	38,731.9
20.2	0.918	0.086	0.282	0.282	0.000	0.00	0.00	0.00	38,941.8	0.0	38,833.4
20.3	0.920	0.086	0.282	0.282	0.000	0.00	0.00	0.00	39,043.3	0.0	38,935.0
20.4	0.923	0.086	0.282	0.282	0.000	0.00	0.00	0.00	39,144.9	0.0	39,036.5
20.5	0.925	0.083	0.270	0.270	0.000	0.00	0.00	0.00	39,242.2	0.0	39,133.9
20.6	0.927	0.086	0.282	0.282	0.000	0.00	0.00	0.00	39,343.8	0.0	39,235.4
20.7	0.930	0.083	0.270	0.270	0.000	0.00	0.00	0.00	39,441.1	0.0	39,332.8
20.8	0.932	0.086	0.282	0.282	0.000	0.00	0.00	0.00	39,542.7	0.0	39,434.3
20.9	0.934	0.083	0.270	0.270	0.000	0.00	0.00	0.00	39,640.0	0.0	39,531.6
21	0.937	0.083	0.270	0.270	0.000	0.00	0.00	0.00	39,737.3	0.0	39,629.0
21.1	0.939	0.083	0.270	0.270	0.000	0.00	0.00	0.00	39,834.6	0.0	39,726.3
21.2	0.941	0.079	0.259	0.259	0.000	0.00	0.00	0.00	39,927.7	0.0	39,819.4
21.3	0.943	0.083	0.270	0.270	0.000	0.00	0.00	0.00	40,025.1	0.0	39,916.7
21.4	0.946	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,118.2	0.0	40,009.8
21.5	0.948	0.083	0.270	0.270	0.000	0.00	0.00	0.00	40,215.5	0.0	40,107.1
21.6	0.950	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,308.6	0.0	40,200.2

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
21.7	0.952	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,401.7	0.0	40,293.3
21.8	0.954	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,494.8	0.0	40,386.4
21.9	0.957	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,587.9	0.0	40,479.5
22	0.959	0.075	0.247	0.247	0.000	0.00	0.00	0.00	40,676.7	0.0	40,568.4
22.1	0.961	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,769.8	0.0	40,661.5
22.2	0.963	0.079	0.259	0.259	0.000	0.00	0.00	0.00	40,862.9	0.0	40,754.6
22.3	0.965	0.075	0.247	0.247	0.000	0.00	0.00	0.00	40,951.8	0.0	40,843.4
22.4	0.967	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,040.6	0.0	40,932.3
22.5	0.969	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,129.5	0.0	41,021.2
22.6	0.972	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,218.4	0.0	41,110.0
22.7	0.974	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,307.2	0.0	41,198.9
22.8	0.976	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,396.1	0.0	41,287.8
22.9	0.978	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,485.0	0.0	41,376.6
23	0.980	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,573.8	0.0	41,465.5
23.1	0.982	0.072	0.235	0.235	0.000	0.00	0.00	0.00	41,658.5	0.0	41,550.1
23.2	0.984	0.075	0.247	0.247	0.000	0.00	0.00	0.00	41,747.3	0.0	41,639.0
23.3	0.986	0.072	0.235	0.235	0.000	0.00	0.00	0.00	41,832.0	0.0	41,723.6
23.4	0.988	0.072	0.235	0.235	0.000	0.00	0.00	0.00	41,916.6	0.0	41,808.2
23.5	0.990	0.075	0.247	0.247	0.000	0.00	0.00	0.00	42,005.4	0.0	41,897.1
23.6	0.992	0.072	0.235	0.235	0.000	0.00	0.00	0.00	42,090.1	0.0	41,981.7
23.7	0.994	0.072	0.235	0.235	0.000	0.00	0.00	0.00	42,174.7	0.0	42,066.4
23.8	0.996	0.068	0.223	0.223	0.000	0.00	0.00	0.00	42,255.1	0.0	42,146.8
23.9	0.998	0.072	0.235	0.235	0.000	0.00	0.00	0.00	42,339.7	0.0	42,231.4
24	1.000	0.072	0.235	0.235	0.000	0.00	0.00	0.00	42,424.4	0.0	42,316.0

Project: Eklutna Native Village Gaming Hall - Grading and Drainage Study
 EEI Project No: 10336
 Calculated By: Ryan Parpan Date: 8/13/2024
 Checked By: Carl Bassler Date: 8/14/2024

INFILTRATION CALCULATION - BASIN B

Determine Size of Infiltration Basin. Runoff based on rational method coefficients. 100-year, 24-hour storm used due to being only means of drainage (i.e. no storm drain system backup). During design it may be possible to utilize detention or a combination detention/infiltration system which has an overflow to Peter's Creek. Storm data from Anchorage Design Criteria Manual Chapter 2 Drainage. Calculation uses time-step intervals.

Basin B Landcover

Landcover	Runoff Coefficient		
	Area (SF)	C	C*A
Landscape (0-2%)	8,907	0.17	1,514
Pavement (0-2%)	36,264	0.95	34,451
Pavement (2-6%)	42,790	0.96	41,078
Roof	29,000	1.00	29,000
			0
Total	116,960		106,043
Composite Runoff Coefficient		0.907	

Basin B Parameters

Infiltration Pipe and Trench				Storm Data		
Infil Rate	K	60	IN/HR	Select storm	100	YR, 24-HOUR
Perf Pipe Diam	D _p	0	FT	Total Rainfall	3.59	IN (MOA DCM CH2 V1, Table 4.2-1) I _p (Anchorage DCM CH 2
Trench Width	W _r	40	FT	Area factor	1.00	V1, Figure 4.2-3)
Rock Height	H _r	3	FT	Time interval	6.00	MIN
Trench Length	L _t	40	FT			
Pipe Length	L _p	0	FT			
Porosity	n	0.35	ratio			
Additional Storage Volume	V _a	0	FT ³			

Calculated System Data

Pipe Volume	0	V_p	FT^3	$L_p * \pi * (D_p/2)^2$
Rock Void Volume	1,680	V_r	FT^3	$(L_r * W_r * H_r - V_p) * n$
Total Store Volume	1,680	V_t	FT^3	$V_p + V_r + V_a$
Bottom Area	1,600	A_b	FT^3	$L * W$
Base Infiltration Rate	0.001389	I_b	FT/S	$k/(12*3600)$
Base Flow Rate	2.22	Q_b	FT^3/S	$I_b * A_b$

Basin Performance Results

On-site Peak Flow	6.64	FT^3/S	Q_r, Max
Off-site Peak Flow	0.00	FT^3/S	Q_o, Max
Off-site Volume	0	FT^3	V_o, Max
Trench Inf Volume	32,047	FT^3	Sum Infiltration Rate * Time Interval

Off-site volume is 0. Entire 100-year, 24-hour storm is infiltrated.

Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	Volume Totals, Accumulated		
									On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q_r	Q_{inf}	Q_e	V_e	V_s	Q_o	V_{inf}	V_o	V_r
HR		IN/HR	FT^3/S	FT^3/S	FT^3/S	FT^3	FT^3	FT^3/S	FT^3	FT^3	FT^3
t = timeprev + timeint		I(t)	$Q(t) = C * I(t) * A$	$Q_{inf} = I_b * A_b * (head\ increase)$	$Q_e = Q_r - Q_{inf}$	$V_e = Q_e * timeint$	$V_s = \Sigma V_e (if < V_t)$	$Q_o = Q_e\ or\ 0$	$V_{inf} = V_{inf}prev + Q_{inf} * timeint$	$V_o = Q_o * timeint + V_o,prev - \Delta V_s$	$V_r = V_r,prev + Q_r * timeint$
0	0.000	0.000	0.000	0.000	0.000	0.00	0.00	0.00	0.0	0.0	0.0
0.1	0.002	0.072	0.176	0.176	0.000	0.00	0.00	0.00	63.4	0.0	63.4
0.2	0.004	0.068	0.167	0.167	0.000	0.00	0.00	0.00	123.7	0.0	123.7
0.3	0.006	0.072	0.176	0.176	0.000	0.00	0.00	0.00	187.2	0.0	187.2
0.4	0.008	0.072	0.176	0.176	0.000	0.00	0.00	0.00	250.6	0.0	250.6
0.5	0.010	0.072	0.176	0.176	0.000	0.00	0.00	0.00	314.1	0.0	314.1
0.6	0.012	0.072	0.176	0.176	0.000	0.00	0.00	0.00	377.5	0.0	377.5
0.7	0.014	0.075	0.185	0.185	0.000	0.00	0.00	0.00	444.1	0.0	444.1
0.8	0.016	0.072	0.176	0.176	0.000	0.00	0.00	0.00	507.6	0.0	507.6
0.9	0.018	0.072	0.176	0.176	0.000	0.00	0.00	0.00	571.0	0.0	571.0
1	0.020	0.075	0.185	0.185	0.000	0.00	0.00	0.00	637.7	0.0	637.7
1.1	0.022	0.072	0.176	0.176	0.000	0.00	0.00	0.00	701.1	0.0	701.1
1.2	0.024	0.075	0.185	0.185	0.000	0.00	0.00	0.00	767.7	0.0	767.7
1.3	0.026	0.075	0.185	0.185	0.000	0.00	0.00	0.00	834.4	0.0	834.4
1.4	0.028	0.075	0.185	0.185	0.000	0.00	0.00	0.00	901.0	0.0	901.0
1.5	0.031	0.075	0.185	0.185	0.000	0.00	0.00	0.00	967.6	0.0	967.6
1.6	0.033	0.075	0.185	0.185	0.000	0.00	0.00	0.00	1,034.2	0.0	1,034.2

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
1.7	0.035	0.075	0.185	0.185	0.000	0.00	0.00	0.00	1,100.8	0.0	1,100.8
1.8	0.037	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,170.6	0.0	1,170.6
1.9	0.039	0.075	0.185	0.185	0.000	0.00	0.00	0.00	1,237.3	0.0	1,237.3
2	0.041	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,307.1	0.0	1,307.1
2.1	0.043	0.075	0.185	0.185	0.000	0.00	0.00	0.00	1,373.7	0.0	1,373.7
2.2	0.046	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,443.5	0.0	1,443.5
2.3	0.048	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,513.3	0.0	1,513.3
2.4	0.050	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,583.1	0.0	1,583.1
2.5	0.052	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,652.9	0.0	1,652.9
2.6	0.054	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,722.6	0.0	1,722.6
2.7	0.057	0.083	0.203	0.203	0.000	0.00	0.00	0.00	1,795.6	0.0	1,795.6
2.8	0.059	0.079	0.194	0.194	0.000	0.00	0.00	0.00	1,865.4	0.0	1,865.4
2.9	0.061	0.083	0.203	0.203	0.000	0.00	0.00	0.00	1,938.4	0.0	1,938.4
3	0.063	0.083	0.203	0.203	0.000	0.00	0.00	0.00	2,011.3	0.0	2,011.3
3.1	0.066	0.083	0.203	0.203	0.000	0.00	0.00	0.00	2,084.3	0.0	2,084.3
3.2	0.068	0.083	0.203	0.203	0.000	0.00	0.00	0.00	2,157.3	0.0	2,157.3
3.3	0.070	0.083	0.203	0.203	0.000	0.00	0.00	0.00	2,230.2	0.0	2,230.2
3.4	0.073	0.083	0.203	0.203	0.000	0.00	0.00	0.00	2,303.2	0.0	2,303.2
3.5	0.075	0.086	0.211	0.211	0.000	0.00	0.00	0.00	2,379.3	0.0	2,379.3
3.6	0.077	0.083	0.203	0.203	0.000	0.00	0.00	0.00	2,452.3	0.0	2,452.3
3.7	0.080	0.086	0.211	0.211	0.000	0.00	0.00	0.00	2,528.4	0.0	2,528.4
3.8	0.082	0.086	0.211	0.211	0.000	0.00	0.00	0.00	2,604.6	0.0	2,604.6
3.9	0.085	0.086	0.211	0.211	0.000	0.00	0.00	0.00	2,680.7	0.0	2,680.7
4	0.087	0.086	0.211	0.211	0.000	0.00	0.00	0.00	2,756.9	0.0	2,756.9
4.1	0.089	0.090	0.220	0.220	0.000	0.00	0.00	0.00	2,836.2	0.0	2,836.2
4.2	0.092	0.086	0.211	0.211	0.000	0.00	0.00	0.00	2,912.3	0.0	2,912.3
4.3	0.094	0.090	0.220	0.220	0.000	0.00	0.00	0.00	2,991.6	0.0	2,991.6
4.4	0.097	0.090	0.220	0.220	0.000	0.00	0.00	0.00	3,070.9	0.0	3,070.9
4.5	0.099	0.090	0.220	0.220	0.000	0.00	0.00	0.00	3,150.2	0.0	3,150.2
4.6	0.102	0.090	0.220	0.220	0.000	0.00	0.00	0.00	3,229.6	0.0	3,229.6
4.7	0.104	0.090	0.220	0.220	0.000	0.00	0.00	0.00	3,308.9	0.0	3,308.9
4.8	0.107	0.093	0.229	0.229	0.000	0.00	0.00	0.00	3,391.4	0.0	3,391.4
4.9	0.110	0.093	0.229	0.229	0.000	0.00	0.00	0.00	3,473.8	0.0	3,473.8
5	0.112	0.093	0.229	0.229	0.000	0.00	0.00	0.00	3,556.3	0.0	3,556.3
5.1	0.115	0.093	0.229	0.229	0.000	0.00	0.00	0.00	3,638.8	0.0	3,638.8
5.2	0.117	0.093	0.229	0.229	0.000	0.00	0.00	0.00	3,721.3	0.0	3,721.3
5.3	0.120	0.097	0.238	0.238	0.000	0.00	0.00	0.00	3,806.9	0.0	3,806.9
5.4	0.123	0.097	0.238	0.238	0.000	0.00	0.00	0.00	3,892.6	0.0	3,892.6
5.5	0.125	0.097	0.238	0.238	0.000	0.00	0.00	0.00	3,978.3	0.0	3,978.3
5.6	0.128	0.097	0.238	0.238	0.000	0.00	0.00	0.00	4,063.9	0.0	4,063.9

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
5.7	0.131	0.097	0.238	0.238	0.000	0.00	0.00	0.00	4,149.6	0.0	4,149.6
5.8	0.134	0.101	0.247	0.247	0.000	0.00	0.00	0.00	4,238.4	0.0	4,238.4
5.9	0.136	0.101	0.247	0.247	0.000	0.00	0.00	0.00	4,327.2	0.0	4,327.2
6	0.139	0.101	0.247	0.247	0.000	0.00	0.00	0.00	4,416.1	0.0	4,416.1
6.1	0.142	0.104	0.256	0.256	0.000	0.00	0.00	0.00	4,508.1	0.0	4,508.1
6.2	0.145	0.101	0.247	0.247	0.000	0.00	0.00	0.00	4,596.9	0.0	4,596.9
6.3	0.148	0.104	0.256	0.256	0.000	0.00	0.00	0.00	4,688.9	0.0	4,688.9
6.4	0.151	0.104	0.256	0.256	0.000	0.00	0.00	0.00	4,780.9	0.0	4,780.9
6.5	0.154	0.108	0.264	0.264	0.000	0.00	0.00	0.00	4,876.1	0.0	4,876.1
6.6	0.157	0.108	0.264	0.264	0.000	0.00	0.00	0.00	4,971.2	0.0	4,971.2
6.7	0.160	0.108	0.264	0.264	0.000	0.00	0.00	0.00	5,066.4	0.0	5,066.4
6.8	0.163	0.108	0.264	0.264	0.000	0.00	0.00	0.00	5,161.6	0.0	5,161.6
6.9	0.166	0.111	0.273	0.273	0.000	0.00	0.00	0.00	5,259.9	0.0	5,259.9
7	0.169	0.111	0.273	0.273	0.000	0.00	0.00	0.00	5,358.3	0.0	5,358.3
7.1	0.172	0.111	0.273	0.273	0.000	0.00	0.00	0.00	5,456.6	0.0	5,456.6
7.2	0.175	0.115	0.282	0.282	0.000	0.00	0.00	0.00	5,558.1	0.0	5,558.1
7.3	0.178	0.115	0.282	0.282	0.000	0.00	0.00	0.00	5,659.7	0.0	5,659.7
7.4	0.182	0.115	0.282	0.282	0.000	0.00	0.00	0.00	5,761.2	0.0	5,761.2
7.5	0.185	0.118	0.291	0.291	0.000	0.00	0.00	0.00	5,865.9	0.0	5,865.9
7.6	0.188	0.118	0.291	0.291	0.000	0.00	0.00	0.00	5,970.6	0.0	5,970.6
7.7	0.192	0.118	0.291	0.291	0.000	0.00	0.00	0.00	6,075.3	0.0	6,075.3
7.8	0.195	0.122	0.300	0.300	0.000	0.00	0.00	0.00	6,183.1	0.0	6,183.1
7.9	0.198	0.122	0.300	0.300	0.000	0.00	0.00	0.00	6,291.0	0.0	6,291.0
8	0.202	0.126	0.308	0.308	0.000	0.00	0.00	0.00	6,402.0	0.0	6,402.0
8.1	0.205	0.126	0.308	0.308	0.000	0.00	0.00	0.00	6,513.1	0.0	6,513.1
8.2	0.209	0.129	0.317	0.317	0.000	0.00	0.00	0.00	6,627.3	0.0	6,627.3
8.3	0.213	0.129	0.317	0.317	0.000	0.00	0.00	0.00	6,741.5	0.0	6,741.5
8.4	0.216	0.133	0.326	0.326	0.000	0.00	0.00	0.00	6,858.9	0.0	6,858.9
8.5	0.220	0.133	0.326	0.326	0.000	0.00	0.00	0.00	6,976.2	0.0	6,976.2
8.6	0.224	0.136	0.335	0.335	0.000	0.00	0.00	0.00	7,096.8	0.0	7,096.8
8.7	0.228	0.140	0.344	0.344	0.000	0.00	0.00	0.00	7,220.5	0.0	7,220.5
8.8	0.232	0.140	0.344	0.344	0.000	0.00	0.00	0.00	7,344.2	0.0	7,344.2
8.9	0.235	0.140	0.344	0.344	0.000	0.00	0.00	0.00	7,468.0	0.0	7,468.0
9	0.240	0.147	0.361	0.361	0.000	0.00	0.00	0.00	7,598.0	0.0	7,598.0
9.1	0.244	0.147	0.361	0.361	0.000	0.00	0.00	0.00	7,728.1	0.0	7,728.1
9.2	0.248	0.151	0.370	0.370	0.000	0.00	0.00	0.00	7,861.3	0.0	7,861.3
9.3	0.252	0.151	0.370	0.370	0.000	0.00	0.00	0.00	7,994.6	0.0	7,994.6
9.4	0.256	0.158	0.388	0.388	0.000	0.00	0.00	0.00	8,134.2	0.0	8,134.2
9.5	0.261	0.158	0.388	0.388	0.000	0.00	0.00	0.00	8,273.8	0.0	8,273.8
9.6	0.265	0.162	0.397	0.397	0.000	0.00	0.00	0.00	8,416.5	0.0	8,416.5

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
9.7	0.270	0.165	0.405	0.405	0.000	0.00	0.00	0.00	8,562.5	0.0	8,562.5
9.8	0.275	0.172	0.423	0.423	0.000	0.00	0.00	0.00	8,714.7	0.0	8,714.7
9.9	0.280	0.172	0.423	0.423	0.000	0.00	0.00	0.00	8,867.0	0.0	8,867.0
10	0.285	0.179	0.441	0.441	0.000	0.00	0.00	0.00	9,025.6	0.0	9,025.6
10.1	0.290	0.183	0.449	0.449	0.000	0.00	0.00	0.00	9,187.4	0.0	9,187.4
10.2	0.295	0.187	0.458	0.458	0.000	0.00	0.00	0.00	9,352.4	0.0	9,352.4
10.3	0.300	0.194	0.476	0.476	0.000	0.00	0.00	0.00	9,523.7	0.0	9,523.7
10.4	0.306	0.201	0.493	0.493	0.000	0.00	0.00	0.00	9,701.4	0.0	9,701.4
10.5	0.312	0.205	0.502	0.502	0.000	0.00	0.00	0.00	9,882.2	0.0	9,882.2
10.6	0.318	0.215	0.529	0.529	0.000	0.00	0.00	0.00	10,072.6	0.0	10,072.6
10.7	0.324	0.219	0.538	0.538	0.000	0.00	0.00	0.00	10,266.1	0.0	10,266.1
10.8	0.330	0.233	0.573	0.573	0.000	0.00	0.00	0.00	10,472.3	0.0	10,472.3
10.9	0.337	0.241	0.590	0.590	0.000	0.00	0.00	0.00	10,684.8	0.0	10,684.8
11	0.344	0.251	0.617	0.617	0.000	0.00	0.00	0.00	10,906.9	0.0	10,906.9
11.1	0.351	0.266	0.652	0.652	0.000	0.00	0.00	0.00	11,141.7	0.0	11,141.7
11.2	0.359	0.284	0.696	0.696	0.000	0.00	0.00	0.00	11,392.3	0.0	11,392.3
11.3	0.367	0.298	0.731	0.731	0.000	0.00	0.00	0.00	11,655.6	0.0	11,655.6
11.4	0.376	0.323	0.793	0.793	0.000	0.00	0.00	0.00	11,941.1	0.0	11,941.1
11.5	0.386	0.352	0.864	0.864	0.000	0.00	0.00	0.00	12,252.0	0.0	12,252.0
11.6	0.397	0.388	0.952	0.952	0.000	0.00	0.00	0.00	12,594.7	0.0	12,594.7
11.7	0.409	0.442	1.084	1.084	0.000	0.00	0.00	0.00	12,984.9	0.0	12,984.9
11.8	0.424	0.521	1.278	1.278	0.000	0.00	0.00	0.00	13,444.9	0.0	13,444.9
11.9	0.442	0.657	1.613	1.613	0.000	0.00	0.00	0.00	14,025.4	0.0	14,025.4
12	0.471	1.048	2.573	2.222	0.351	126.36	126.36	0.00	14,825.4	0.0	14,951.8
12.1	0.547	2.707	6.645	2.389	4.255	1531.86	1,658.22	0.00	15,685.6	0.0	17,343.8
12.2	0.569	0.790	1.939	4.416	-2.477	-891.69	766.53	0.00	17,275.2	0.0	18,041.8
12.3	0.585	0.578	1.419	3.236	-1.817	-654.25	112.28	0.00	18,440.2	0.0	18,552.5
12.4	0.598	0.474	1.163	2.371	-1.208	-434.70	0.00	0.00	19,293.7	0.0	18,971.3
12.5	0.610	0.413	1.013	1.013	0.000	0.00	0.00	0.00	19,658.5	0.0	19,336.1
12.6	0.620	0.370	0.908	0.908	0.000	0.00	0.00	0.00	19,985.3	0.0	19,662.9
12.7	0.629	0.337	0.828	0.828	0.000	0.00	0.00	0.00	20,283.5	0.0	19,961.1
12.8	0.638	0.309	0.758	0.758	0.000	0.00	0.00	0.00	20,556.4	0.0	20,233.9
12.9	0.646	0.291	0.714	0.714	0.000	0.00	0.00	0.00	20,813.3	0.0	20,490.9
13	0.654	0.273	0.670	0.670	0.000	0.00	0.00	0.00	21,054.4	0.0	20,732.0
13.1	0.661	0.258	0.634	0.634	0.000	0.00	0.00	0.00	21,282.8	0.0	20,960.4
13.2	0.668	0.248	0.608	0.608	0.000	0.00	0.00	0.00	21,501.7	0.0	21,179.3
13.3	0.674	0.237	0.582	0.582	0.000	0.00	0.00	0.00	21,711.1	0.0	21,388.7
13.4	0.681	0.226	0.555	0.555	0.000	0.00	0.00	0.00	21,911.0	0.0	21,588.6
13.5	0.687	0.215	0.529	0.529	0.000	0.00	0.00	0.00	22,101.3	0.0	21,778.9
13.6	0.692	0.212	0.520	0.520	0.000	0.00	0.00	0.00	22,288.5	0.0	21,966.1

Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	Volume Totals, Accumulated		
									On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
13.7	0.698	0.201	0.493	0.493	0.000	0.00	0.00	0.00	22,466.2	0.0	22,143.7
13.8	0.704	0.197	0.485	0.485	0.000	0.00	0.00	0.00	22,640.7	0.0	22,318.2
13.9	0.709	0.190	0.467	0.467	0.000	0.00	0.00	0.00	22,808.8	0.0	22,486.4
14	0.714	0.187	0.458	0.458	0.000	0.00	0.00	0.00	22,973.8	0.0	22,651.3
14.1	0.719	0.180	0.441	0.441	0.000	0.00	0.00	0.00	23,132.4	0.0	22,810.0
14.2	0.724	0.176	0.432	0.432	0.000	0.00	0.00	0.00	23,287.8	0.0	22,965.4
14.3	0.729	0.172	0.423	0.423	0.000	0.00	0.00	0.00	23,440.1	0.0	23,117.7
14.4	0.733	0.169	0.414	0.414	0.000	0.00	0.00	0.00	23,589.2	0.0	23,266.8
14.5	0.738	0.162	0.397	0.397	0.000	0.00	0.00	0.00	23,732.0	0.0	23,409.6
14.6	0.742	0.162	0.397	0.397	0.000	0.00	0.00	0.00	23,874.7	0.0	23,552.3
14.7	0.747	0.158	0.388	0.388	0.000	0.00	0.00	0.00	24,014.3	0.0	23,691.9
14.8	0.751	0.154	0.379	0.379	0.000	0.00	0.00	0.00	24,150.7	0.0	23,828.3
14.9	0.755	0.151	0.370	0.370	0.000	0.00	0.00	0.00	24,284.0	0.0	23,961.6
15	0.760	0.151	0.370	0.370	0.000	0.00	0.00	0.00	24,417.2	0.0	24,094.8
15.1	0.764	0.144	0.352	0.352	0.000	0.00	0.00	0.00	24,544.1	0.0	24,221.7
15.2	0.768	0.144	0.352	0.352	0.000	0.00	0.00	0.00	24,671.0	0.0	24,348.6
15.3	0.772	0.144	0.352	0.352	0.000	0.00	0.00	0.00	24,797.9	0.0	24,475.5
15.4	0.775	0.140	0.344	0.344	0.000	0.00	0.00	0.00	24,921.7	0.0	24,599.2
15.5	0.779	0.136	0.335	0.335	0.000	0.00	0.00	0.00	25,042.2	0.0	24,719.8
15.6	0.783	0.133	0.326	0.326	0.000	0.00	0.00	0.00	25,159.6	0.0	24,837.2
15.7	0.787	0.133	0.326	0.326	0.000	0.00	0.00	0.00	25,277.0	0.0	24,954.5
15.8	0.790	0.133	0.326	0.326	0.000	0.00	0.00	0.00	25,394.3	0.0	25,071.9
15.9	0.794	0.129	0.317	0.317	0.000	0.00	0.00	0.00	25,508.6	0.0	25,186.1
16	0.797	0.126	0.308	0.308	0.000	0.00	0.00	0.00	25,619.6	0.0	25,297.2
16.1	0.801	0.126	0.308	0.308	0.000	0.00	0.00	0.00	25,730.6	0.0	25,408.2
16.2	0.804	0.126	0.308	0.308	0.000	0.00	0.00	0.00	25,841.7	0.0	25,519.2
16.3	0.808	0.122	0.300	0.300	0.000	0.00	0.00	0.00	25,949.5	0.0	25,627.1
16.4	0.811	0.118	0.291	0.291	0.000	0.00	0.00	0.00	26,054.2	0.0	25,731.8
16.5	0.815	0.122	0.300	0.300	0.000	0.00	0.00	0.00	26,162.1	0.0	25,839.7
16.6	0.818	0.118	0.291	0.291	0.000	0.00	0.00	0.00	26,266.8	0.0	25,944.4
16.7	0.821	0.115	0.282	0.282	0.000	0.00	0.00	0.00	26,368.3	0.0	26,045.9
16.8	0.824	0.115	0.282	0.282	0.000	0.00	0.00	0.00	26,469.8	0.0	26,147.4
16.9	0.827	0.115	0.282	0.282	0.000	0.00	0.00	0.00	26,571.3	0.0	26,248.9
17	0.831	0.115	0.282	0.282	0.000	0.00	0.00	0.00	26,672.8	0.0	26,350.4
17.1	0.834	0.111	0.273	0.273	0.000	0.00	0.00	0.00	26,771.2	0.0	26,448.8
17.2	0.837	0.108	0.264	0.264	0.000	0.00	0.00	0.00	26,866.4	0.0	26,543.9
17.3	0.840	0.111	0.273	0.273	0.000	0.00	0.00	0.00	26,964.7	0.0	26,642.3
17.4	0.843	0.108	0.264	0.264	0.000	0.00	0.00	0.00	27,059.9	0.0	26,737.5
17.5	0.846	0.108	0.264	0.264	0.000	0.00	0.00	0.00	27,155.1	0.0	26,832.6
17.6	0.849	0.108	0.264	0.264	0.000	0.00	0.00	0.00	27,250.2	0.0	26,927.8

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
17.7	0.852	0.104	0.256	0.256	0.000	0.00	0.00	0.00	27,342.2	0.0	27,019.8
17.8	0.855	0.104	0.256	0.256	0.000	0.00	0.00	0.00	27,434.2	0.0	27,111.8
17.9	0.858	0.104	0.256	0.256	0.000	0.00	0.00	0.00	27,526.2	0.0	27,203.8
18	0.860	0.101	0.247	0.247	0.000	0.00	0.00	0.00	27,615.1	0.0	27,292.6
18.1	0.863	0.104	0.256	0.256	0.000	0.00	0.00	0.00	27,707.1	0.0	27,384.6
18.2	0.866	0.101	0.247	0.247	0.000	0.00	0.00	0.00	27,795.9	0.0	27,473.5
18.3	0.869	0.101	0.247	0.247	0.000	0.00	0.00	0.00	27,884.7	0.0	27,562.3
18.4	0.872	0.097	0.238	0.238	0.000	0.00	0.00	0.00	27,970.4	0.0	27,648.0
18.5	0.874	0.097	0.238	0.238	0.000	0.00	0.00	0.00	28,056.0	0.0	27,733.6
18.6	0.877	0.101	0.247	0.247	0.000	0.00	0.00	0.00	28,144.9	0.0	27,822.4
18.7	0.880	0.093	0.229	0.229	0.000	0.00	0.00	0.00	28,227.4	0.0	27,904.9
18.8	0.882	0.097	0.238	0.238	0.000	0.00	0.00	0.00	28,313.0	0.0	27,990.6
18.9	0.885	0.097	0.238	0.238	0.000	0.00	0.00	0.00	28,398.7	0.0	28,076.2
19	0.888	0.093	0.229	0.229	0.000	0.00	0.00	0.00	28,481.1	0.0	28,158.7
19.1	0.890	0.093	0.229	0.229	0.000	0.00	0.00	0.00	28,563.6	0.0	28,241.2
19.2	0.893	0.093	0.229	0.229	0.000	0.00	0.00	0.00	28,646.1	0.0	28,323.7
19.3	0.895	0.093	0.229	0.229	0.000	0.00	0.00	0.00	28,728.6	0.0	28,406.2
19.4	0.898	0.090	0.220	0.220	0.000	0.00	0.00	0.00	28,807.9	0.0	28,485.5
19.5	0.900	0.090	0.220	0.220	0.000	0.00	0.00	0.00	28,887.2	0.0	28,564.8
19.6	0.903	0.093	0.229	0.229	0.000	0.00	0.00	0.00	28,969.7	0.0	28,647.3
19.7	0.906	0.090	0.220	0.220	0.000	0.00	0.00	0.00	29,049.0	0.0	28,726.6
19.8	0.908	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,125.2	0.0	28,802.7
19.9	0.910	0.090	0.220	0.220	0.000	0.00	0.00	0.00	29,204.5	0.0	28,882.0
20	0.913	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,280.6	0.0	28,958.2
20.1	0.915	0.090	0.220	0.220	0.000	0.00	0.00	0.00	29,359.9	0.0	29,037.5
20.2	0.918	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,436.1	0.0	29,113.6
20.3	0.920	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,512.2	0.0	29,189.8
20.4	0.923	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,588.3	0.0	29,265.9
20.5	0.925	0.083	0.203	0.203	0.000	0.00	0.00	0.00	29,661.3	0.0	29,338.9
20.6	0.927	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,737.4	0.0	29,415.0
20.7	0.930	0.083	0.203	0.203	0.000	0.00	0.00	0.00	29,810.4	0.0	29,488.0
20.8	0.932	0.086	0.211	0.211	0.000	0.00	0.00	0.00	29,886.5	0.0	29,564.1
20.9	0.934	0.083	0.203	0.203	0.000	0.00	0.00	0.00	29,959.5	0.0	29,637.1
21	0.937	0.083	0.203	0.203	0.000	0.00	0.00	0.00	30,032.5	0.0	29,710.1
21.1	0.939	0.083	0.203	0.203	0.000	0.00	0.00	0.00	30,105.4	0.0	29,783.0
21.2	0.941	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,175.2	0.0	29,852.8
21.3	0.943	0.083	0.203	0.203	0.000	0.00	0.00	0.00	30,248.2	0.0	29,925.8
21.4	0.946	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,318.0	0.0	29,995.6
21.5	0.948	0.083	0.203	0.203	0.000	0.00	0.00	0.00	30,391.0	0.0	30,068.5
21.6	0.950	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,460.8	0.0	30,138.3

									Volume Totals, Accumulated		
Time	Accumulation	Intensity	Flow Rate In	Infiltration Rate	Excess Flow	Excess Volume	Storage Volume	Overflow rate	On-site Infiltration Volume	Off-Site Volume	Storm runoff Volume
t		I(t)	Q _r	Q _{inf}	Q _e	V _e	V _s	Q _o	V _{inf}	V _o	V _r
HR		IN/HR	FT ³ /S	FT ³ /S	FT ³ /S	FT ³	FT ³	FT ³ /S	FT ³	FT ³	FT ³
21.7	0.952	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,530.6	0.0	30,208.1
21.8	0.954	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,600.4	0.0	30,277.9
21.9	0.957	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,670.1	0.0	30,347.7
22	0.959	0.075	0.185	0.185	0.000	0.00	0.00	0.00	30,736.8	0.0	30,414.3
22.1	0.961	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,806.6	0.0	30,484.1
22.2	0.963	0.079	0.194	0.194	0.000	0.00	0.00	0.00	30,876.4	0.0	30,553.9
22.3	0.965	0.075	0.185	0.185	0.000	0.00	0.00	0.00	30,943.0	0.0	30,620.6
22.4	0.967	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,009.6	0.0	30,687.2
22.5	0.969	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,076.2	0.0	30,753.8
22.6	0.972	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,142.8	0.0	30,820.4
22.7	0.974	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,209.5	0.0	30,887.0
22.8	0.976	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,276.1	0.0	30,953.7
22.9	0.978	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,342.7	0.0	31,020.3
23	0.980	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,409.3	0.0	31,086.9
23.1	0.982	0.072	0.176	0.176	0.000	0.00	0.00	0.00	31,472.8	0.0	31,150.4
23.2	0.984	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,539.4	0.0	31,217.0
23.3	0.986	0.072	0.176	0.176	0.000	0.00	0.00	0.00	31,602.8	0.0	31,280.4
23.4	0.988	0.072	0.176	0.176	0.000	0.00	0.00	0.00	31,666.3	0.0	31,343.9
23.5	0.990	0.075	0.185	0.185	0.000	0.00	0.00	0.00	31,732.9	0.0	31,410.5
23.6	0.992	0.072	0.176	0.176	0.000	0.00	0.00	0.00	31,796.4	0.0	31,473.9
23.7	0.994	0.072	0.176	0.176	0.000	0.00	0.00	0.00	31,859.8	0.0	31,537.4
23.8	0.996	0.068	0.167	0.167	0.000	0.00	0.00	0.00	31,920.1	0.0	31,597.7
23.9	0.998	0.072	0.176	0.176	0.000	0.00	0.00	0.00	31,983.5	0.0	31,661.1
24	1.000	0.072	0.176	0.176	0.000	0.00	0.00	0.00	32,047.0	0.0	31,724.6

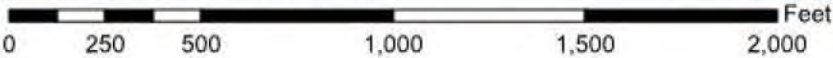
APPENDIX E

FEMA Flood Insurance Rate Map (FIRM)

National Flood Hazard Layer FIRMMette



149°29'52"W 61°25'14"N



1:6,000

149°29'14"W 61°24'56"N

Basemap Imagery Source: USGS National Map 2023

Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AR
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
		Area of Undetermined Flood Hazard Zone D
GENERAL STRUCTURES		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall
OTHER FEATURES		20.2 Cross Sections with 1% Annual Chance Water Surface Elevation
		17.5 Water Surface Elevation
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
		Coastal Transect Baseline
	Profile Baseline	
MAP PANELS		Digital Data Available
		No Digital Data Available
		Unmapped
		The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.



This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 6/12/2024 at 4:44 PM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

Appendix C

Water and Wastewater Study

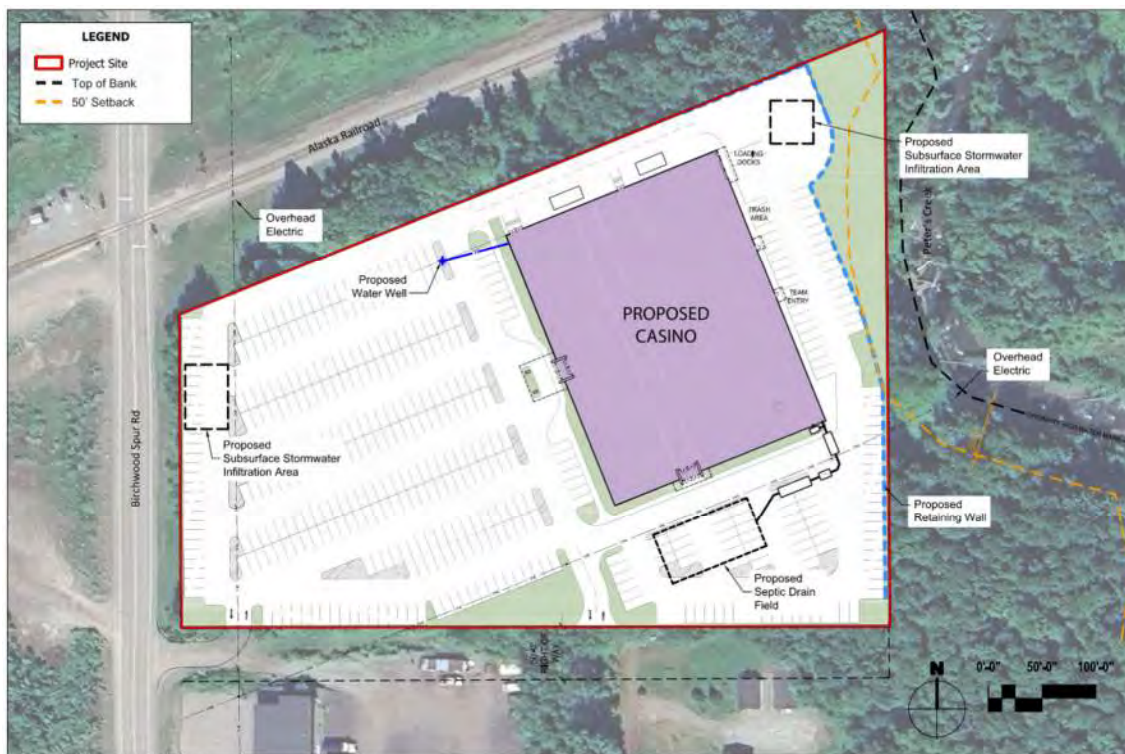
EKLUTNA NATIVE VILLAGE GAMING HALL PROJECT

CHUGIAK, ALASKA

Water and Wastewater Study

Final Submission

January 10, 2025



Source: Marnell Companies, 1/7/2025, Acorn Environmental, 1/10/2025

Prepared By:



3601 C St., Suite 650, Anchorage, Alaska 99503

Kyle Markel, PE – Civil Engineer
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EI Project No. 10336
Acorn Project No. 2415

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1.0 PROJECT INTRODUCTION

1.1 Project Description

The proposed project development includes a new 58,000 SF gaming hall building, with supporting utilities, paved parking, driveways, and landscaping. The purpose of this Water and Wastewater Study is to analyze existing site conditions and facilities relative to water and wastewater, investigate options for new water and wastewater systems to meet the anticipated demands, and evaluate the potential impacts to the proposed site.

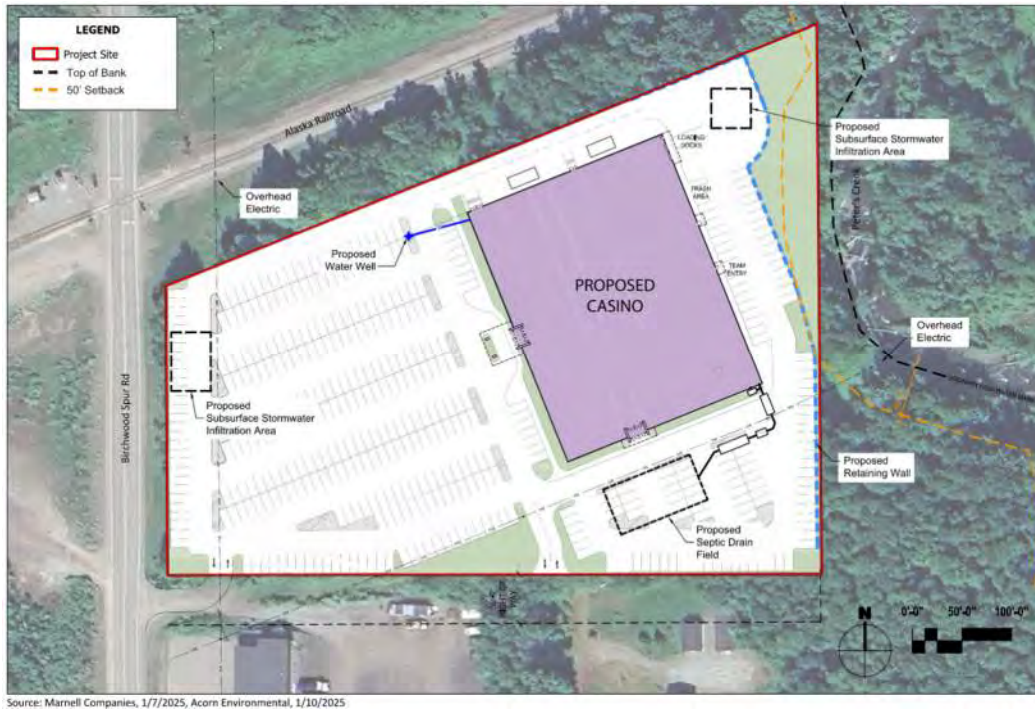


Figure 1: Concept Site Plan

2.0 EXISTING SITE CONDITIONS

The proposed facility will be located on Lots 64, 66, and 67, Township 15 North, Range 1 West, Section 5, off Birchwood Spur Road, in Chugiak, Alaska. Lots 64 and 66 are mostly undeveloped and heavily vegetated. Lot 67 included a small (roughly 1,000 SF) single story house, which has recently been demolished, and a water well and septic system. Refer to Appendix A for the Property Boundary and As-Built Survey.

2.1 Existing Water Supply Conditions

The existing building on Lot 67 is served by an on-site water well. A well log for this well has not been obtained at the time of this study, such that specific details are unknown. The local property owner reported that water occasionally flows from a leak in the top of the well. Evidence of the well leak was present at the time of the site visit in June of 2024, indicating the well was in an 'artesian' state. An artesian well is indicative of an aquifer under pressure due to confining impermeable layers. The existing

well is not adequate to serve the proposed gaming hall facility. It will be abandoned as part of this project, in accordance with environmental standards.

On-site water wells are the most common water source for this area. The Alaska Department of Natural Resources (DNR) tracks water well locations and basic information. Below is a figure showing the documented wells in proximity to the project site.

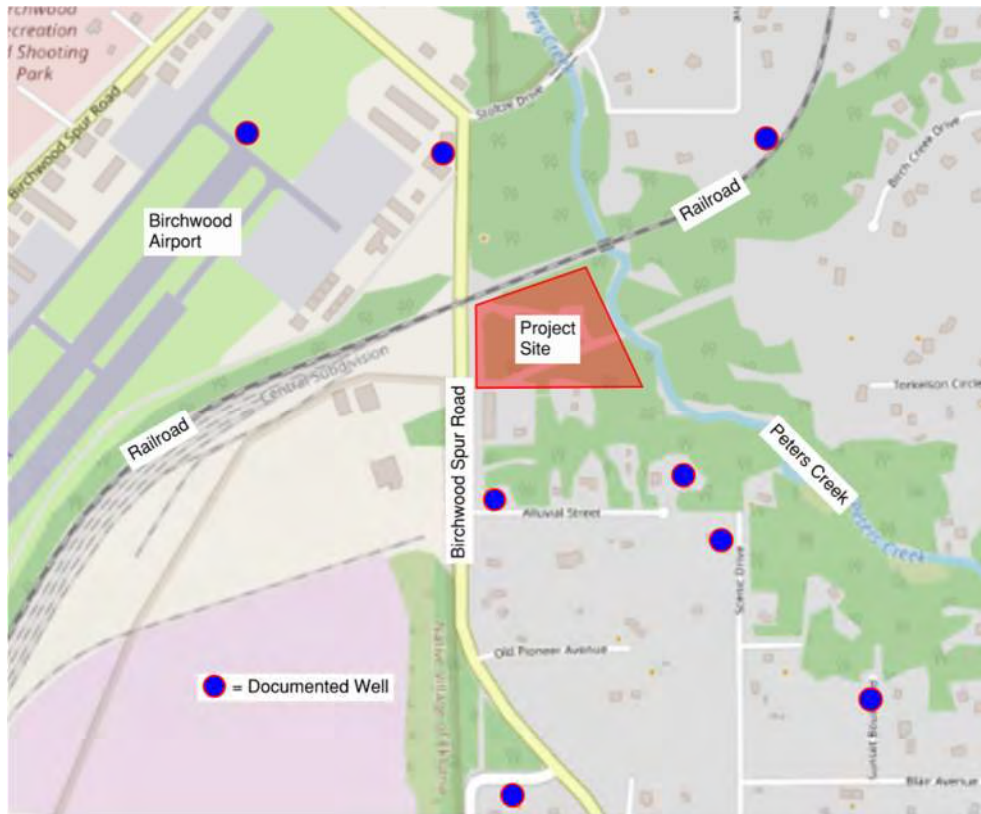


Figure 2: Documented Water Well Locations Map

Table 1 below shows basic data from all well logs documented within about one-half mile of the project site. Well depths range from 38 feet deep to 223 feet deep. Recorded static water levels range from 5 feet to 120 feet below ground surface. These wells are generally 6-inch diameter with an open bottom and no well screens. Reported pumping rates ranged from 4 gpm to 40 gpm. The lower flowrate recordings of 4 and 5 gpm are likely attributed to small residential wells where higher flowrates are not needed, and these rates are not reflective of what the aquifer could produce.

Table 1: Documented Well Log Data

Log Id	Date of Completion	Lot No.	Well Depth	Static Water Depth	MTRS	Pumping Rate
244		L084	157		S015N001W5SW SE NW SE	
3089	06/02/1981	L086	223		S015N001W5	
3846	02/28/1981	L038	98		S015N001W5SW NE SW NE	
3967	09/26/1983	L009	81		S015N001W5	
3979	09/14/1983	L003 B1	80		S015N001W5	
5817	07/08/1984	L075	161		S015N001W5SE	
5883	08/23/1984	L021	55	25	S015N001W5	
10273		L098	188		S015N001W5SW SE	
14425	05/22/1972	L108	105		S015N001W5NE SW SE SE	
14937	09/24/1979	L048	75	68	S015N001W5NW SW SE NE	10 gpm
15644	06/05/1976	L109	102		S015N001W5NW SW SE SE	
22530	12/17/1993	L111	195	120	S015N001W5NW SE SW SE	4 gpm
24843	08/21/1998	L097	205	40	S015N001W5SW SE	4 gpm
25084	08/21/1973	L101	54	24	S015N001W5SW NW SE	5 gpm
25109	06/08/1999	L090	133	5	S015N001W5SE NW NE SE	40 gpm
77942	08/01/1973	L94	150	(artesian)	S015N001W05NE NE SW SE	10 gpm
84535	12/16/2019	LT 1D	38	25	S015N001W05NE SE SE NW	15 gpm

The United States Geological Survey (USGS) published a generalized geologic map of the Eagle River-Birchwood Area in 1971 which shows the project site to be in a geological area classified as “F” defined as “Large alluvial-fan deposits. Gravel and sand; cobbles and small boulders common; small amounts of silt. Good water-bearing unit; may yield large quantities of water to wells.” Though this information comes from a generalization of large areas, it is a good indication that the groundwater will be plentiful.

Four test pits were completed onsite in July of 2024. Groundwater was encountered in all pits at 6 to 12.5 feet below ground surface (bgs), within a permeable gravel layer. This finding confirms that aquifer conditions appear conducive to high-producing water wells at this site, at least for shallow wells.

The Anchorage Water and Wastewater Utility (AWWU) is the water and wastewater utility company, owned and operated by the Municipality of Anchorage (MOA), and is responsible for providing safe and reliable water and wastewater services for the Anchorage area. AWWU does not have any water infrastructure in the immediate area of the project site. AWWU stated that they have no future plans to extend their systems near the site. AWWU also noted that almost all their new infrastructure is installed by developers constructing new subdivisions with a cost-causer/cost-payer relationship where the party creating the cost pays the cost. The closest such water mainline to the site was constructed for a subdivision located approximately 1/2 mile to the northeast, across Peters Creek. There are multiple private properties between the project site and the distribution system, without utility easements, essentially blocking any potential line extensions.

The other public water line nearest the site is AWWU’s Eklutna transmission main, located over a mile away to the southeast, where it crosses Birchwood Loop Road. The line is not permitted to be tapped with service lines. A large vault would be required to connect to the 54” transmission main, and about 7,000 linear feet of water distribution main along Birchwood Loop Road and Birchwood Spur Drive, to the far frontage of the lot. This course of action would require far more effort and costs than an onsite water well and is therefore not the best option for the project.

2.2 Existing Wastewater Disposal Conditions

The former house on Lot 67 was served by an onsite septic system extending to the northwest toward Lot 64. It is a typical gravity septic system including a septic tank with a leach field as is common for the area. The property owner on-site reported that there have not been any known issues with the existing leach field.

There are no known public wastewater systems in the Birchwood area that could be connected to. The nearest public system is located south of the Glenn Highway, roughly 1.5 miles away. AWWU does not have any future plans to extend wastewater infrastructure near the site, similar to the water system as discussed in the previous section. Onsite septic systems are common for the area, and in-situ soils are anticipated to be sufficiently permeable to allow use of on-site infiltration and disposal. The USGS generalized geologic map (mentioned previously) describes the in-situ soils as gravel and sand which work well for infiltration.

The existing septic system is sized for the small house and would not be utilized for the new facility. Part of the site development for the project would include decommissioning the septic system and removing the septic tank.

3.0 WATER SUPPLY ASSESSMENT

3.1 Water Supply Demands

The proposed gaming hall facility will have multiple functions which will require water including a restaurant, bar, restrooms, general maintenance, and for fire suppression systems.

An ADEC document “Best Management Practices for Private Drinking Water Systems” provides a table for typical daily domestic water usage from different sources, which is referenced from the EPA Design Manual. The published facility source types do not include a gaming hall or similar facility. The source types “Bar” or “Assembly Hall” do not appear to accurately represent how the facility will be used. The developer provided water supply information based on similar facilities constructed in other states. In general, the gaming hall facility’s major daily water demands will come from the restrooms, several sinks and a dish washer for the bar and restaurant. The fixtures will generally be low flow units, including waterless urinals, low flow water closets and lavatories, and electronically operated controls, to reduce the water and wastewater demands. The restaurant is planned to be deli style with minimal dishes and other items requiring washing. A letter from the developer which describes the intended fixtures in more detail can be found in Appendix C.

The estimated daily water supply demands are based on the following:

30 Employees x 3 shifts/day x 4.5 gpd	=	405 gpd
3,000 Guests/day x 2.6 gpd	=	7,800 gpd
<u>Additional Water Fixtures</u>	=	<u>476 gpd</u>
Total Water Demand	=	8,681 gpd

The peak demand flowrate was estimated by using the curve from the International Plumbing Code (IPC), Appendix E, for a restaurant facility with approximately 200 fixture units. The anticipated peak demand flowrate was found to be approximately 90 gpm.

Water storage must be provided to be available for use by the building’s fire suppression system, pressurized by a fire pump. The flow and volume of water required for the building sprinkler system is

dependent on multiple factors including the building materials, use of the building, and total square footage. Fire water storage tanks will be sized accordingly and likely installed below ground within the building. The water use for filling of the tanks is effectively a onetime occurrence and is insignificant compared to the domestic water consumption, therefore is not included in the average daily facility water demands. Fire hydrants will likely not be required for this facility as the Alaska Division of Fire and Life Safety consistently waives the requirement where a public water system is not available, and the building is protected with a sprinkler system.

3.2 Water Supply Strategies

Based on the existing site and surrounding site's common use of groundwater wells, and the infeasibility of connecting to a public distribution system, a new onsite water well is recommended to meet the potable water and fire water demands of the facility. Water wells in the surrounding area are typically installed for small residential purposes and do not require high flowrates. To achieve higher flowrates for the proposed gaming hall facility, a well would be designed and constructed to achieve a higher production rate, such as by targeting specific water-bearing formations, installation of appropriate well screens, and likely be 8" diameter or larger, to enhance inflow and to accommodate a larger pump. Until the well is drilled it is unknown if the well could meet the anticipated peak demands, without the need for a storage tank.

The anticipated peak domestic water flowrate is 90 gpm. As this is greater than the known existing well supply rates, storage tank(s) may be required to supplement the well flowrate during periods of peak demand. The total storage volume is dependent on the actual pumping capacity of the well and durations of peak water demands, therefore the water tank(s) must be sized accordingly in the design phase once pumping tests have been performed. Tanks could consist of a single or multiple pressure tanks, or a gravity tank which would require a separate supply pump. If a large gravity tank is required, it would likely be underground so that it would have minimal impact to building or site use.

ADEC 18 AAC 80 Drinking Water provides guidance for drinking water systems in the state of Alaska. Water wells are classified as private or public water systems. Public water systems are then further defined as community or non-community, and transient or non-transient. The well for this project would be considered a public water system and would likely be classified as a non-transient non-community water system, assuming there is a consistent staff of at least 25 people for the majority of the year.

18 AAC 80.1990 defines public water systems as follows:

Public Water System:

- (A) Means a system for the provision to the public of water for human consumption through pipes or other constructed conveyances, if the system has at least 15 service connections or regularly serves an average of at least 25 individuals daily at least 60 days out of the year;
- (B) is either a community water system or a non-community water system;
- (C) includes
 - a. a collection, treatment, storage, or distribution facility, including a water haul vehicle, under control of the operator of the system and used primarily in connection with the system; and
 - b. a collection or pretreatment storage facility not under control of the system operator that are used primarily in connection with the system;

(D) does not include a private water system;

Community Water System: A public water system that serves at least 15 service connections used by year-round residents or regularly serves at least 25 year-round residents.

Transient non-community water system: A non-community water system that serves at least 25 individuals daily for 60 or more days per year but does not regularly serve a daily average of at least 25 of the same individuals for more than six months per year.

Non-transient non-community water system: A public water system that is not a community water system and that regularly serves at least 25 of the same individuals over six months per year

3.3 Water Well Site Constraints

ADEC provides guidance for locating new wells. The general intent is to reduce the risk of contaminating drinking water sources. Public water wells should be at least 200 feet away (horizontally) from wastewater disposal systems, sewer manholes, sewer lines and cleanouts, and wastewater holding tanks. 18 AAC 80.020 provides a minimum separation distances as shown in Table 2 below.

Table 2: ADEC Minimum Separation Distances for Drinking Water

TABLE A. Minimum Separation Distances ^d Between Drinking Water Sources and Potential Sources of Contamination (Measured horizontally in feet)	
Type of Drinking Water System	
Potential Sources of Contamination	Community Water Systems, Non-transient Non-Community Water Systems, and Transient Non-Community Water Systems
Wastewater treatment works, ^b wastewater disposal system, ^b pit privy, ^b sewer manhole, lift station, sewer line, and sewer line cleanout	200
Community sewer line, sewer main, wastewater holding tank, ^b other potential sources of contamination ^c	200
Private sewer line, sewer service line, drain (buried in the ground), petroleum lines and storage tanks, ^d drinking water treatment waste ^c	100

In addition to wastewater systems, ADEC keeps a record of contaminated sites across the state and their cleanup status. There are no documented contaminated sites within 500 feet of the project site, and therefore contaminated sites should not be a concern for a new well on the site.

A concept site plan showing the proposed general location of the water well can be found in Appendix B.

3.4 Cumulative Impacts

When installing a new water well it is important to be aware of the potential impacts to water wells in the surrounding area which are drawing from the same aquifer. In general, impacts to surrounding wells are not a concern for an area of large lot sizes of 1 acre plus. Because of the larger lot sizes, the existing wells in the area are well spread out, as shown in Figure 2. A high recharge rate is expected for the site due to the close proximity to Peters Creek flowing along the east side of the site, and well-draining sandy gravel soil. The anticipated short recovery time (the time for a well's water level to return to the pre-pumping level) would reduce cumulative impacts to surrounding water wells.

4.0 WASTEWATER TREATMENT AND DISPOSAL ASSESSMENT

4.1 Wastewater Generation

Functions of the gaming hall facility that will generate a majority of the wastewater include a restaurant, bar, and restrooms. Similar to the water demand tables, EPA's Onsite Wastewater Treatment Manual and ADEC's Onsite Wastewater Systems Installation Manual do not include a source that accurately represents the proposed gaming hall facility. For simplicity and the purposes of this study, we have assumed the daily wastewater generation quantity to match the water demand quantity of 8,681 gpd. Refer to section 3.1 Water Supply Demands for more detail of the total water demand.

4.2 Wastewater Treatment and Disposal Strategies

There are various methods for treating and disposing of wastewater from a commercial facility with varying levels of siting constraints, construction costs, maintenance costs, and service life expectancy. In general, wastewater is typically handled in one of three ways; connecting to an existing sanitary sewer system that feeds to a large offsite treatment facility, infiltrated onsite, or stored in holding tanks onsite then transported to a treatment facility.

Similar to the water systems, AWWU manages the wastewater network in the greater Anchorage area. Unfortunately, there is no existing wastewater system near the site which could be connected to. Upon inquiry with AWWU over long term master plans for development in the Birchwood area, AWWU expressed they have no plans to extend wastewater infrastructure near the site. This makes a connection to an existing public wastewater system infeasible for the project.

Onsite infiltration of wastewater via septic tanks and leach fields is the most common method of wastewater treatment and disposal in the Birchwood area. This method utilizes a septic tank to settle out solids before the effluent reaches a drain field comprised of well-draining rock fill. The drain rock functions as a dispersal media to the surrounding in-situ soils which serve as treatment for the effluent. The drain fields are typically designed as a deep trench or as a bed. A deep trench is designed to infiltrate effluent through the sidewalls, whereas a bed system is designed to be shallower and infiltrate through the bottom of the bed. A bed type system typically requires a larger footprint than a single deep trench, but it can be advantageous when a high groundwater table is present. When multiple deep trenches are utilized, they must have a horizontal separation of at least two times the depth of the trench.

A holding tank system for wastewater is simply a large tank that offers no treatment or disposal of wastewater. Wastewater is contained in the tank and must be routinely pumped out by a truck and transported to a treatment facility. Holding tank systems are typically only installed in areas where connection to a public system and on-site disposal are not feasible.

For the proposed site and facility, an onsite septic system with advanced treatment is the recommended method of handling wastewater. Connecting to a public wastewater system is not feasible in this location, and an onsite holding tank system would not be practical for the amount of wastewater anticipated.

4.3 Wastewater System Sizing

ADEC 18 AAC 72 Wastewater Disposal provides guidelines for sizing of the septic tanks and absorption areas based on anticipated wastewater daily flowrates, and the soil characteristics.

Per 18 AAC 72.530(e)(2), The net minimum septic tank size for a commercial facility with daily design flows greater than 1,250 gpd is calculated by the following equation: 1,125 gal + (0.75 x Daily Design Flow). With a daily design flow of 8,681 gpd, a total tankage of 7,636 gal is required. The net tank capacity can be accomplished with a single tank, or if that is impractical, multiple tanks in series or parallel could be used. The current estimated design flow indicates a single 8,000 gal nominal tank will be required. A tank of this size in this region would typically be of coated steel, cylindrical, likely 8-foot diameter.

The minimum absorption area is calculated from wastewater application rates provided in the ADEC Onsite Wastewater Systems Installation Manual for systems with secondary treatment as follows:

Table 3: ADEC Wastewater Application Rates with Secondary Treatment

Wastewater Application Rates for soil absorption systems receiving effluent meeting secondary treatment standards				
Percolation Rate ^a (minutes/inch)	Soil Texture (Unified Soil Classification)	with gravity distribution	with timed dose or pressurized distribution	with timed dose or pressurized distribution and nitrogen removal
Faster than 1	Gravel (GW/GP)	Not Suitable	Not Suitable ^b	Not Suitable ^b
1 – 5	Gravel (GW/GP)	1.8	2.0	3.0
1 – 15	Medium to coarse sand (SW/SP)	1.5	1.8	2.8
6 – 15	Fine sand or loamy sand (SP-SM)	1.2	1.5	2.5
16 – 30	Sandy loam, silty gravel (GM), silty sand (SM)	0.9	1.2	2.0
31 – 60	Loam, silt loam, silt (ML)	0.5	0.8	1.0
61 – 90	Silty clay loam, clay loam ^c	0.3	0.4	0.5
91 – 120 ^d	Any soil texture ^c	Not Suitable ^d	0.15	0.25

a. Percolation tests must be performed in accordance with either a method publicly identified by EPA or the department as acceptable. The application rate must be based on either the percolation test or soil texture/classification, whichever is the most conservative.

b. Soils classified as gravel (GW or GP) for which a percolation test has not been conducted or a percolation test result is faster than one minute per inch may still be used if a shallow trench or bed system is installed with a two-foot sand liner below the distribution media and the application rates listed for SW/SP is used; sand must meet the specifications publicly identified by the department; the department may waive the sand liner requirement in a manner set out in 18 AAC 72.540 or disinfection must be included as part of the treatment prior to discharge to the leach field; for treatment that includes disinfection (ex. UV) the application rates listed for gravel (GW/GP) may be used.

c. Soils with expandable clays or soil types not listed in this table require an engineer design and prior department approval.

d. Soils with percolation rates slower than 90 min/inch require prior department approval; soils with percolation rates slower than 120 minutes per inch are considered impermeable.

Four test pits and three percolation tests were completed onsite in July of 2024. Soils encountered across the four test pits were fairly consistent and percolation rates were observed to be faster than 1 minute/inch. Groundwater was encountered at 9 to 12.5 ft below ground surface (bgs) in three of the four pits. The test pit in the northeast corner of the site encountered groundwater at 6 ft bgs which was not surprising considering the much lower elevation and close proximity to Peters Creek. Because of the high water table encountered in the northeast test pit, a percolation test was not performed. With a relatively high groundwater table and fast percolating soils, advanced secondary treatment will be required for the wastewater on this project. Footnote b., in Table 3 above, allows infiltration in soils that percolate faster than 1 min/inch if disinfection is included in the treatment process.

Secondary treatment, as defined in 18 AAC 72.990(79), generally means effluent that is treated to less than 30 mg/L of Biochemical Oxygen Demand (BOD) and Total Suspended Solids (TSS). The application rates for infiltration sizing are increased when there is secondary treatment. Increasing the application rates results in decreasing the required absorption area. Minimizing the absorption area is critical, as the intent of the proposed site design is to pave most of the property and maximize the amount of parking spaces. There is also an option to discharge effluent to the surface if it is treated to a high enough level.

There are several options for secondary treatment of effluent. The treatment systems are typically packaged from a specialized manufacturer and typically operate by recirculating effluent through a series of filter media, or by a biological reaction process. One example of an advanced treatment system that could meet the secondary treatment requirements is the AdvanTex AX-Max manufactured by Orenco Systems, Inc. It would be installed flush with grade in a non-traffic area. Primary treatment would still be accomplished by a traditional septic tank, then effluent would receive secondary treatment by the AdvanTex AX-Max, then it would drain to the absorption area. Another example is the ExtremeSTP Sewage Treatment Plant Containerized MBR by Lifewater Engineering. This system uses a membrane bioreactor treatment process following primary treatment within solids settling tanks. Tanks could be located below grade, and the equipment that required maintenance access would be located above grade in a container structure.

Product information for both advanced treatment systems can be found in Appendix D.

In addition to the effluent treatment system, a grease interceptor will be provided for the restaurant's kitchen to separate grease from the rest of the wastewater, and thereby reduce the load on the treatment system and the absorption and filtering media. Grease would be removed from the interceptor tank periodically and disposed of offsite.

If secondary treatment is provided for the system with timed dosing or pressurized distribution, nitrogen removal, and disinfection, an application rate of 3.0 gpd/sf could be used. For a bed style leach field, this would require a total bed area of approximately 3,000 sf. A concept site plan showing the proposed general location and arrangement of the wastewater system can be found in Appendix B.

For systems with design flows exceeding 2,500 gpd, nitrate calculations should be prepared to demonstrate that nitrate concentrations in the groundwater aquifer will not exceed five milligrams per liter beyond a distance measured from the edge of the soil absorption field to the nearest downgradient property line, or to a point as necessary to protect public health, public and private water systems, and the environment. If the secondary treatment system selected includes nitrogen removal, the nitrogen level in the disposed wastewater is considerably reduced, such that nitrate contamination will likely not be an issue.

4.4 Cumulative Impacts

Impacts from the proposed septic system to the site and surrounding properties are expected to be extremely minimal. Onsite wastewater systems must be sufficiently separated from water wells, streams, groundwater aquifers, lot lines, and slopes exceeding 25%. Furthermore, advanced secondary treatment of the wastewater, including nitrogen removal and disinfection will likely be provided, which will further mitigate the risk of negative impacts to the environment.

5.0 REFERENCES

Appendix E Sizing of Water Piping Systems, 2018, International Plumbing Code (IPC)

Best Management Practices for Private Drinking Water Systems, Alaska Department of Environmental Conservation (ADEC)

Onsite Wastewater Systems Installation Manual - Technical Guidance and Approved Best Management Practices, April 1, 2024, Alaska Department of Environmental Conservation (ADEC)

Onsite Wastewater Treatment System Manual, February 2002, The U.S. Environmental Protection Agency (USEPA)

18 AAC 72 Wastewater Disposal, As Amended through October 1, 2023, Alaska Department of Environmental Conservation (ADEC)

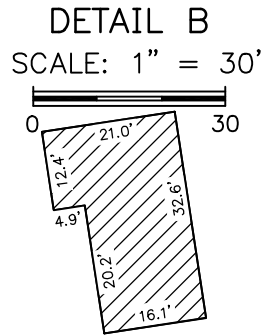
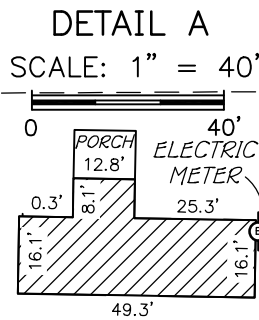
18 AAC 80 Drinking Water, Amended as of October 1, 2023, Alaska Department of Environmental Conservation (ADEC)

APPENDIX A

Property Boundary and As-Built Survey

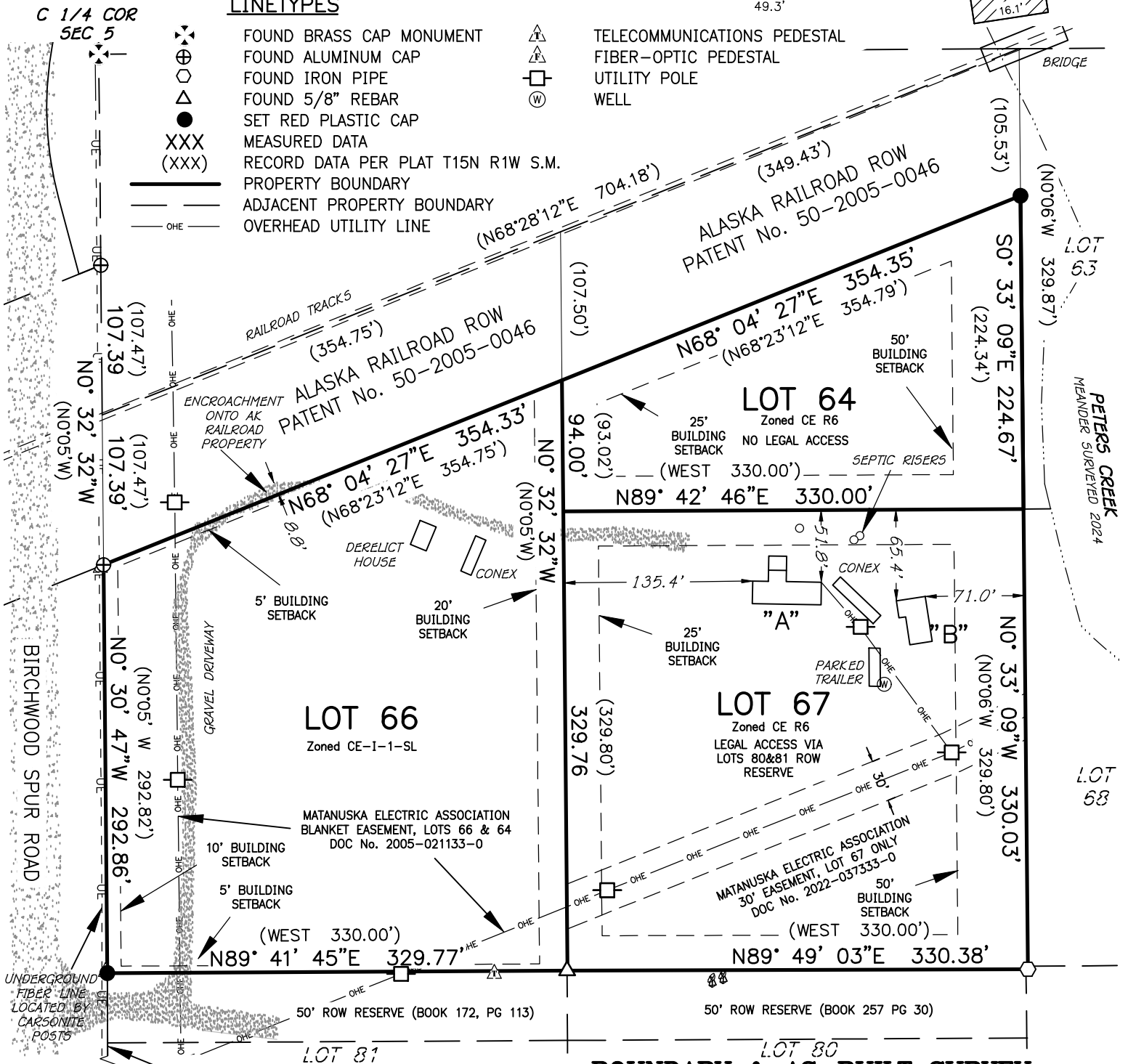
NOTES

1. INFORMATION SHOWN HEREON BASED ON FIELD SURVEY CONDUCTED BY McLINTOCK LAND ASSOCIATES, INC APRIL 2024.
2. THE DISTANCES AND BEARINGS SHOWN ON THIS AS-BUILT SURVEY ARE IN GRID. NAD83(2011) ALASKA STATE PLANE ZONE 4.
3. THIS LOT IS SUBJECT TO ANY EASEMENTS OF RECORD, EVEN IF NOT SHOWN HEREON.



LINETYPES

- | | | | |
|--|------------------------------------|--|-----------------------------|
| | FOUND BRASS CAP MONUMENT | | TELECOMMUNICATIONS PEDESTAL |
| | FOUND ALUMINUM CAP | | FIBER-OPTIC PEDESTAL |
| | FOUND IRON PIPE | | UTILITY POLE |
| | FOUND 5/8" REBAR | | WELL |
| | SET RED PLASTIC CAP | | |
| | MEASURED DATA | | |
| | RECORD DATA PER PLAT T15N R1W S.M. | | |
| | PROPERTY BOUNDARY | | |
| | ADJACENT PROPERTY BOUNDARY | | |
| | OVERHEAD UTILITY LINE | | |



BOUNDARY & AS-BUILT SURVEY

I hereby certify that I have, or someone under my supervision has, inspected the property described as:

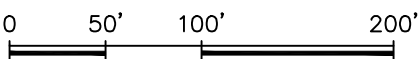
Lots 64 66 & 67
 Township 15 North Range 1 West
 Seward Meridian
 Anchorage Recording District

Monuments have been found and/or set as indicated. The improvements situated thereon are within the property lines and do not encroach or overlap onto adjacent properties, nor do any improvements from adjacent properties encroach or overlap onto this property, except as indicated hereon.

The property dimensions shown in parenthesis are from the record plat and are not necessarily as we may have measured. The improvement locations are approximate and have been determined only to the extent that enables us to determine if there are any encroachments onto or off of the lot. Drafting and reproduction may produce graphic inconsistencies; therefore scaling should not be attempted to determine unshown dimensions. This drawing is not adequate for additional construction or for determining the exact location of property lines. Only easements from the recorded subdivision plat noted above are shown hereon and it is the responsibility of the owner to determine if any other easements, covenants, or restrictions exist.

© McClintock Land Associates, Inc. (MLA) 2024

This document is copyrighted and is authorized to be used for one real property transaction or project only. Any copy is to be considered unauthentic unless it bears an original surveyor's signature (usually in blue ink) or a stamp showing recorder's office data. Copyright restrictions (but not re-use restrictions) are waived if this document has been officially recorded. Lending institutions may also make additional copies for their own records. Liability to MLA is limited to fees received for this project.



SCALE IN FEET

McCLINTOCK LAND ASSOCIATES, INC.
 16942 NORTH EAGLE RIVER LOOP ROAD
 EAGLE RIVER, ALASKA 99577-7801
 (907) 206-5000
 LICENSE No. AECC596

FIELD WORK DATE: APRIL 16&22 2024 PARTY CHIEF: BWS
 DRAWN DATE: APRIL-30-2024 DRAWN BY: BWS

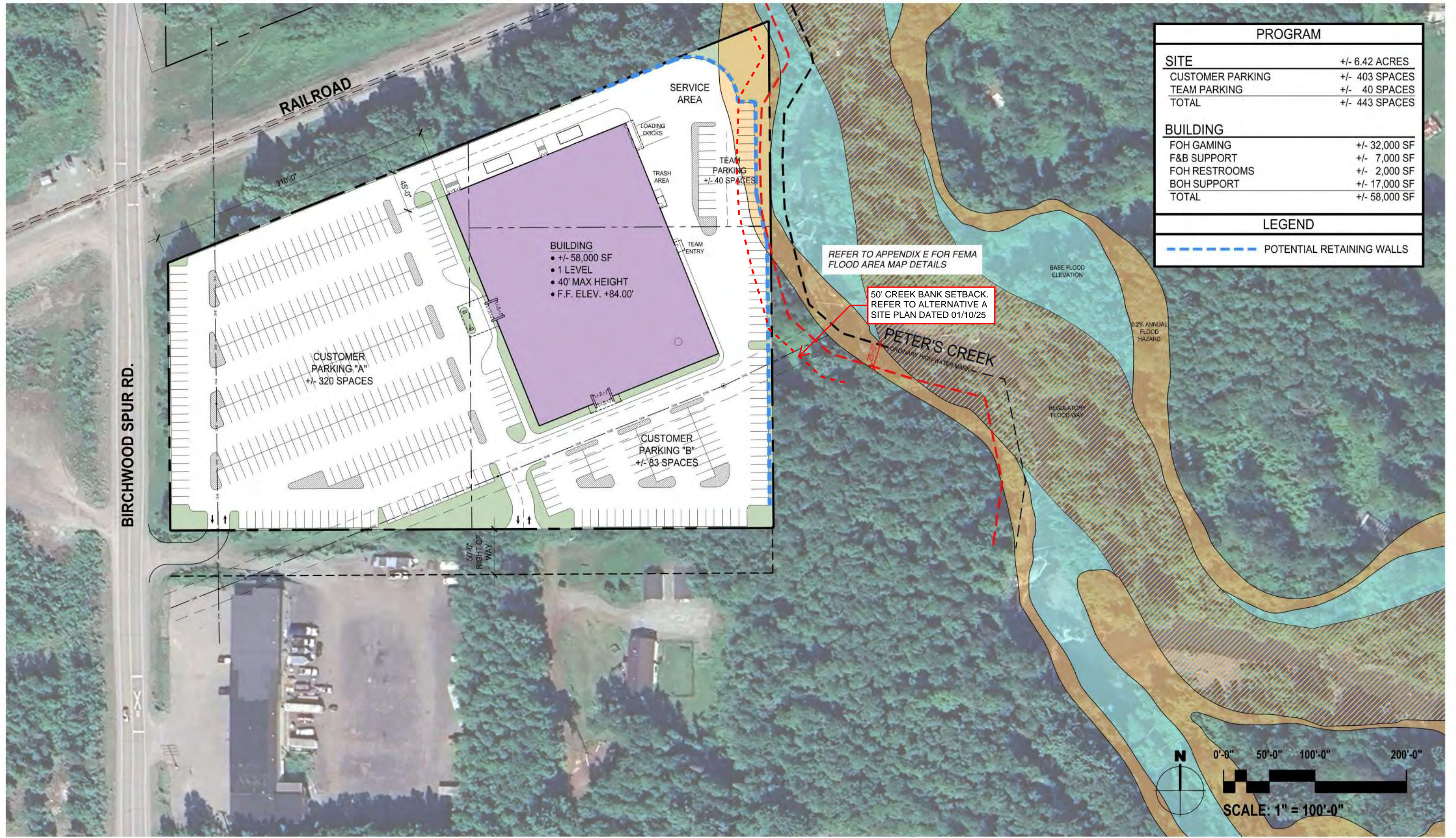
FIELD BOOK: NA
 GRID: NW1457

PLOT SCALE: 1"=100'
 DWG NAME: AB24-117

JOB NO: 24-117

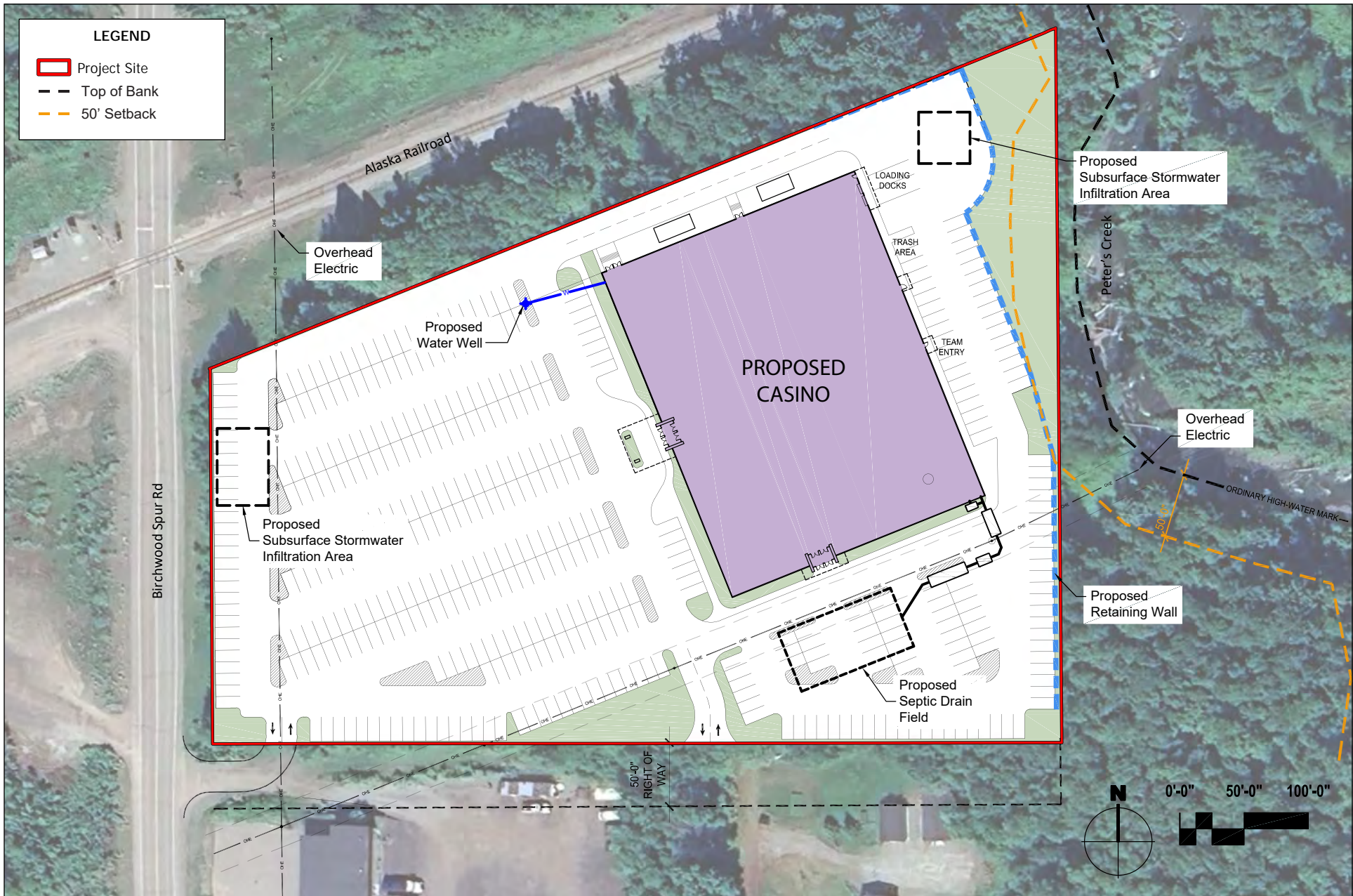
APPENDIX B

1. Concept Site Plan
2. Concept Water and Wastewater Plan



PROGRAM	
SITE	+/- 6.42 ACRES
CUSTOMER PARKING	+/- 403 SPACES
TEAM PARKING	+/- 40 SPACES
TOTAL	+/- 443 SPACES
BUILDING	
FOH GAMING	+/- 32,000 SF
F&B SUPPORT	+/- 7,000 SF
FOH RESTROOMS	+/- 2,000 SF
BOH SUPPORT	+/- 17,000 SF
TOTAL	+/- 58,000 SF
LEGEND	
	POTENTIAL RETAINING WALLS

EKLUTNA GAMING HALL - SITE PLAN STUDY
 CONCEPT SITE PLAN

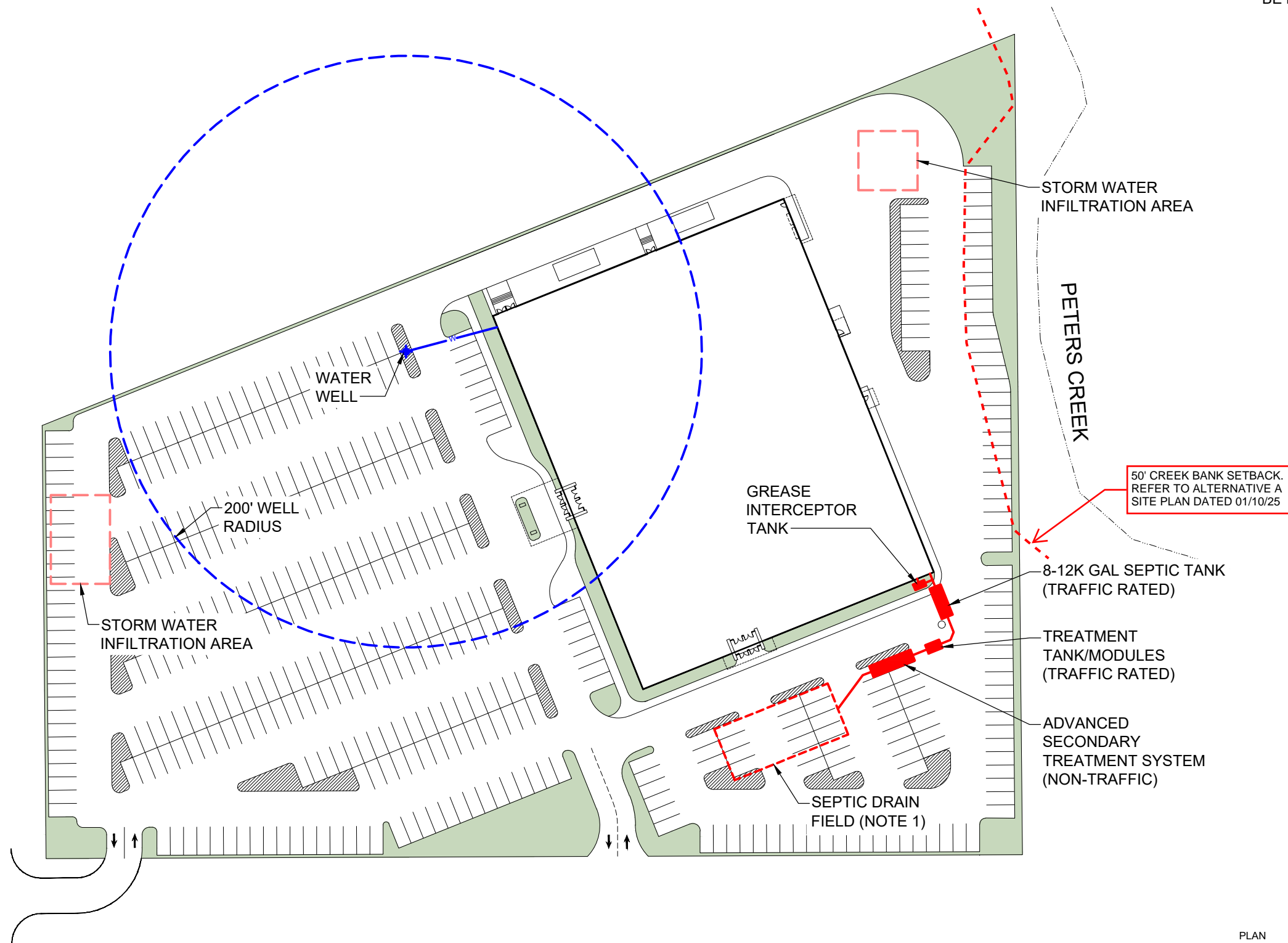


Source: Marnell Companies, 1/7/2025, Acorn Environmental, 1/10/2025

FIGURE 2.1-1
ALTERNATIVE A SITE PLAN

NOTES

- 1. WITH SUFFICIENT ADVANCED TREATMENT, DISCHARGE OF EFFLUENT TO SURFACE WATER COULD BE PERMITTED BY ADEC. IF APPROVED, THE DRAIN FIELD WOULD NOT BE REQUIRED.



APPENDIX C

Water Usage Letter



30 October 2024

Marnell Gaming Management
222 Via Marnell Way
Las Vegas, NV 89119

Attention: Anthony A. Marnell III
Reference: Eklutna Permanent Casino
Water Use Capacity
Consultant's Project No.: 242009

Dear Anthony,

Marnell Consulting has reviewed Enterprise Engineering's anticipated daily water demands at Eklutna Casino, submitted on July 18, 2024 that states the following:

- 30 employees x 3 shifts/day x 13 gpd = 1,170 gpd
 - 4,000 guests/day x 3 gpd = 12,000 gpd
- Total Water Demand = 13,170 gpd

Based on the above we have been asked to investigate the recommended Environmental Protection Agency (EPA) guidelines that have been used by Enterprise Engineering and reevaluate to come up with a more realistic number of Gallons-per-Day use (gpd) that are anticipated in Eklutna Casino.

The EPA or Federal Buildings Metering Guidance suggests that the water usage/day (gpd) by an employee would be between 8-13 gpd and guest usage/day would be between 4-8 gpd. Other similar calculations that were used for casino buildings (without hotel rooms) were based on 4.5 gpd/employee and 2.6 gpd/guest, meaning that each occupant/guest would use the restroom 3x during their stay in the casino. This is excessive and not realistic. In addition, we applied 75% diversity to the numbers, which is a more reasonable representation of water demands in a casino such as Eklutna.

Using Marnell Consulting's logic shown above – anticipated daily water demand would look like this:

- 30 employees x 3 shifts/day x 4.5 gpd = 405 gpd
 - 3,000 guests/day x 2.6 gpd = 7,800 gpd
- (Note: based on waterless urinals that would be installed, along with low flow water closets, lavatories and electronically operated controls)*
- Total Water Demand = 8,205 gpd
Total Water Demand w. 75% diversity applied = 6,154 gpd

Other Anticipated Water Use Fixtures:

- 15-seat bar and an 8-seat Bar with 4-sinks = 25 gpd
- Food court type of a kitchen with 1-hand sink and 3-3-compartment sinks = 35 gpd
- 1 dish machine x 100 GAL at 3 wash cycles per day = 300 gpd
- 1 ice machine at 26.5 GAL at 4-harvesting cycles/day = 106 gpd
- 1 employee dining room sink = 10 gpd

Total Water Demand = 476 gpd
Total Water Demand w. 75% diversity applied = 357 gpd

Grand Total Water Demand = 8,681 gpd
Grand Total Water Demand w. 75% diversity applied = 6,511 gpd

Based on the data that was submitted to us, it appears that we would be fully capable of reducing the Gallons-per-Day (gpd) water demand as projected by Enterprise Engineering, for Eklutna Casino. The data being presented above was based on research of water demands at similar casino settings, around the country.

Please let us know if you require any additional information. Thank you for your time.

Sincerely,

MARNELL CONSULTING



James "Mitch" Milicevic
Director of Systems Integration – MEP

cc: Dave Howryla, Marnell Architecture
Doug Eisarelli, Marnell Architecture

APPENDIX D

Advanced Treatment Systems Brochures



ExtremeSTP™ Sewage Treatment Plant Containerized MBRs (Aboveground, Commercial, Membrane Bioreactors)

Commercial ExtremeSTP™ sewage treatment plants are designed for use anywhere on Earth, including extremely cold climates (-60°F/-51°C). Each system can be customized to meet the client's needs. Our typical membrane bioreactor (MBR) treatment process includes a surge tank, a fine solids removing screen, bioreactor to provide aerobic decomposition, tubular membrane ultrafiltration, UV disinfection, and dosed effluent discharge. However, different processes may be used to better meet the client's needs.



Standard Features:

- Top quality ISO shipping containers provides rugged, weatherproof enclosures. Customer selects the paint color
- Insulation is applied to the interior of the container. Accessible areas are finished with easily cleanable fiberglass reinforced plastic
- PLC controlled, remote monitoring with web cam
- Most process tanks are made of extrusion welded, heavy duty, copolymer polypropylene plastic. Some are rotationally molded plastic tanks, and others are made of welded stainless steel. All are corrosion-free, low maintenance tanks that never need painting
- 'Plug-and-play' (or better expressed as 'plug-and-poop') design makes installation simple

Options:

- Air-transportable design for shipment to remote northern communities
- Portable design that allows the system to be partially drained down, transported to another site, and started up again. This design is especially well suited for ice road and exploration camps that must be quickly and easily relocated
- On-board generator for treatment plant operation
- Other types of membranes besides tubular UF membranes

Sizes:

- Any size from 500 gpd to 100,000 gpd and beyond



AdvanTex[®] Treatment Systems
AX-Max[™]
Manufactured by **Orenco Systems**^{®, Inc.}



This full-sized AdvanTex[®] AX-Max[™] wastewater system was installed at a 50-site campground in the LaPine State Park, LaPine, Oregon, to handle design flows of 7,500 gpd (28.4 m³/day).

Decentralized Wastewater Treatment for Commercial Properties and Communities



814 Airway Avenue, Sutherlin, Oregon, USA 97479
Toll-Free: 800-348-9843 • +1-541-459-4449 • www.orenco.com

Applications:

- Municipal systems
- Subdivisions, apartments
- Golf course developments, resorts
- Manufactured home parks
- Parks, RV parks, campgrounds
- Schools, churches, businesses
- Rest areas, truck stops

AdvanTex® AX-Max™ Treatment System

Reliable, Energy-Efficient Wastewater Treatment



The Yakama Nations Housing Authority in Washington state added five AdvanTex® AX-Max units (background) to its ten AdvanTex AX-100 units, increasing the capacity of its wastewater system by 50%. Photo courtesy of Fextex Systems, Inc.

Everywhere!

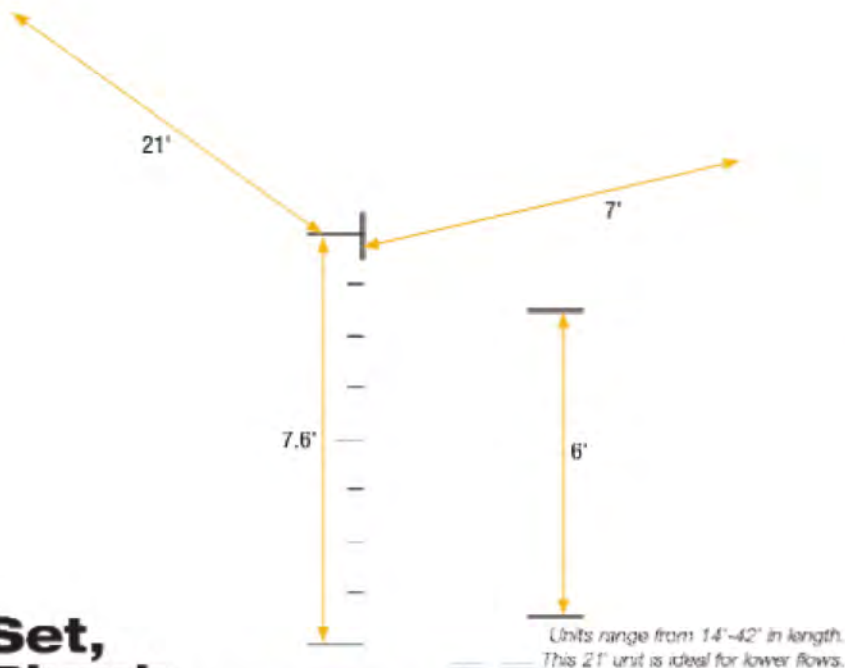
For more than 15 years, Orenco's AdvanTex® Treatment Systems have been providing reliable, energy-efficient wastewater treatment inside and outside the urban core. AdvanTex textile filter technology has been winning awards and coming out on top in field trials and demo projects, all over the world.

Orenco's newest product in the AdvanTex line is the AX-Max™: a completely-integrated, fully-plumbed, and compact wastewater treatment plant that's ideal for commercial properties and communities. It's also ideal for projects with strict discharge limits, limited budgets, and part-time operators.

A Sustainable Solution for Wastewater Treatment

Like all AdvanTex Treatment Systems, the AX-Max is a recirculating media filter that produces outstanding effluent quality suitable for reuse, with significant nutrient removal. AX-Max systems are highly energy-efficient, using less than 2 kWh per 1000 treated gallons (3.785 m³). And they require minimal O&M compared to conventional technologies. Consequently, AdvanTex can earn LEED credits for your projects.

AdvanTex® AX-Max™ Treatment System



Set, Plumb, Wire, and Go

The AX-Max is pre-plumbed and easy to install, so AX-Max projects can meet the tightest deadlines. The entire system — including treatment, recirculation, and discharge — is built inside an insulated fiberglass tank that ranges from 14-42 feet (4.3-12.8 m) in length. AX-Max units can be installed above-ground — for maximum versatility in temporary or variable-flow situations — or in-ground. They can also be installed individually or in multi-tank arrays, treating up to 1 MGD (3,800 m³/day).

For Every Climate and Condition

AX-Max systems provide excellent treatment anywhere, and they have been installed all over the world. For example, AX-Max systems have been installed at Malibu's famous beach parks and New Zealand's Glendhu Bay campground. Several more were installed in Soyo, Africa, to serve a new hospital and school. Other AX-Max systems have been installed on top of Alaska's frozen tundra and St. Lucia's volcanic rock. Still more have been installed in mining camps from Alberta to Texas and, in the Midwest, at a U.S. Department of Defense demo site.



Benefits

- Containerized, fully-plumbed
- Capable of meeting stringent permit limits
 - Reuse-quality effluent
 - Significant reductions in ammonia, total nitrogen
- Compact and versatile
- Above-ground or in-ground installation
- Easy to set
- Simple to operate
- Low energy usage: <2 kWh per 1000 treated gal. (<2 kWh per 3.785 m³)^{*}

^{*} When treating domestic waste



Textile Treatment Media

The treatment medium is a uniform, engineered textile. AdvanTex textile is easy to clean and allows loading rates as high as 50 gpd/ft² (2000 L/day/m²) with primary-treated influent.



Effluent Distribution

High-quality, low-horsepower pumps micro-dose the treatment media at regular intervals, and proprietary spin nozzles efficiently distribute the effluent, optimizing treatment.



Telemetry Controls

Orencia's telemetry-enabled control panels use a dedicated phone line or ethernet connection, ensuring 24/7 monitoring and real-time remote control.

AdvanTex® AX-Max™ Treatment System

Carefully Engineered by Orenco

Orenco Systems has been researching, designing, manufacturing, and selling leading-edge products for small-scale wastewater treatment systems since 1981. The company has grown to become an industry leader, with about 300 employees and 300 points of distribution in North America, Australasia, Europe, Africa, and Southwest Asia. Our systems have been installed in more than 70 countries around the world.

Orenco maintains an environmental lab and employs dozens of civil, electrical, mechanical, and manufacturing engineers, as well as wastewater treatment system operators. Orenco's technologies are based on sound scientific principles of chemistry, biology, mechanical structure, and hydraulics. As a result, our research appears in numerous publications and our engineers are regularly asked to give workshops and trainings.



814 Airway Avenue
Sutherlin, OR 97479 USA

T: 800-348-9843
T: 541-459-4449
F: 541-459-2884

www.orenco.com

ABR-ATX-MAX-1
Rev. 1.5, © 03/17
Orenco Systems®, Inc.

Project Summary



Installation photos courtesy of BioSolutions, Inc.

Point Dume State Beach and Preserve, Southern California

In spring, 2011, Los Angeles County needed to quickly upgrade restrooms at Malibu's Point Dume State Beach in time for the long — and busy — Memorial Day weekend.

The county's engineer specified three AX-Max units, one for each restroom, and all three were installed in a matter of days. The small footprint of this configuration saved the county valuable space for visitor parking. After disinfection, the treated effluent is dispersed right into the sand. Point Dume is part of a large-scale upgrade of L.A. County beach parks, virtually all of which include AdvanTex Treatment Systems of various sizes and configurations.



Fully Supported by Orenco

AdvanTex Treatment Systems are part of a comprehensive program that includes ...

- Designer, installer, and operator training
- Design assistance, technical specifications, and plan reviews
- Installation and operation manuals
- Lifetime technical support

Distributed by:

Appendix D

Socio-economic Impact Analysis



**THE
INNOVATION
GROUP**

**Eklutna Native Village Gaming Facility Project
Socio-Economic Impact Analysis:
Chugiak, Alaska**

Prepared for:

Acorn Environmental

July 2024

Prepared by:

The Innovation Group
7852 South Elati St. Suite 103
Littleton, Colorado 80120
www.theinnovationgroup.com

Eklutna Native Village Gaming Facility Project Socio-Economic Impact Analysis

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fiscal impacts estimated by IMPLAN and illustrated in the tables below exclude any gaming compact contributions generated for Alaska and includes taxes from direct, indirect, and induced effects

Table 2: Local Tax Impact: Eklutna **Casino Ongoing Operations (000's)**

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$0.0	\$0.0	\$0.0	\$0.0
Social Insurance Tax- Employer Contribution	\$0.0	\$0.0	\$0.0	\$0.0
TOPI: Sales Tax	\$0.0	\$38.4	\$44.5	\$82.9
TOPI: Property Tax	\$0.0	\$155.1	\$179.9	\$335.0
TOPI: Motor Vehicle License	\$5.4	\$0.6	\$0.6	\$6.6
TOPI: Severance Tax	\$1.4	\$0.1	\$0.2	\$1.7
TOPI: Other Taxes	\$14.2	\$1.5	\$1.7	\$17.3
TOPI: Special Assessments	\$6.5	\$0.7	\$0.8	\$7.9
OPI: Corporate Profits Tax	\$0.0	\$0.0	\$0.0	\$0.0
Personal Tax: Motor Vehicle License	\$2.2	\$1.2	\$0.8	\$4.2
Personal Tax: Property Taxes	\$0.9	\$0.5	\$0.3	\$1.7
Personal Tax: Other Tax (Fish/Hunt)	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$30.6	\$198.0	\$228.7	\$457.2

Source: IMPLAN Group, LLC

Table 3: State Tax Impact: Eklutna **Casino Ongoing Operations (000's)**

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$1.1	\$0.4	\$0.3	\$1.8
Social Insurance Tax- Employer Contribution	\$1.1	\$0.5	\$0.3	\$1.9
TOPI: Sales Tax	\$0.0	\$26.4	\$30.7	\$57.1
TOPI: Property Tax	\$0.0	\$12.2	\$14.1	\$26.3
TOPI: Motor Vehicle License	\$9.7	\$1.0	\$1.2	\$11.9
TOPI: Severance Tax	\$1,815.4	\$186.8	\$216.6	\$2,218.7
TOPI: Other Taxes	\$60.8	\$6.3	\$7.3	\$74.3
TOPI: Special Assessments	\$0.0	\$0.0	\$0.0	\$0.0
OPI: Corporate Profits Tax	\$0.0	\$66.5	\$54.2	\$120.7
Personal Tax: Motor Vehicle License	\$4.0	\$2.1	\$1.4	\$7.4
Personal Tax: Property Taxes	\$0.1	\$0.0	\$0.0	\$0.1
Personal Tax: Other Tax (Fish/Hunt)	\$45.8	\$25.3	\$16.4	\$87.6
Total	\$1,892.1	\$302.2	\$325.9	\$2,520.2

Source: IMPLAN Group, LLC

Table 4: **Federal Tax Impact: Eklutna Casino Ongoing Operations (000's)**

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$843.0	\$404.0	\$275.0	\$1,522.0
Social Insurance Tax- Employer Contribution	\$752.0	\$306.9	\$217.4	\$1,276.3
TOPI: Excise Tax	\$79.2	\$8.2	\$9.7	\$97.1
TOPI: Custom Duty	\$88.2	\$9.1	\$10.8	\$108.2
OPI: Corporate Profit Tax	\$0.0	\$158.7	\$132.5	\$291.2
Personal Tax: Income Tax	\$1,111.6	\$591.9	\$393.5	\$2,097.0
Personal Tax: Estate and Gift Tax	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$2,874.1	\$1,478.8	\$1,039.0	\$5,391.9

Source: IMPLAN Group, LLC

Construction Economic Impacts

Combining the local and rest of state impacts, the following table displays the total economic impacts from the construction of the development, which is projected to inject total value added of \$45.8 million into the state economy.

Table 5: Eklutna Casino Local Construction Impacts

Impact Type	Employment	Labor Income (MM)	Value Added (MM)	Output (MM)
Direct Effect	336	\$27.6	\$28.4	\$46.7
Indirect Effect	35	\$2.5	\$4.7	\$9.4
Induced Effect	115	\$7.1	\$12.8	\$21.2
Total	487	\$37.2	\$45.8	\$77.3

Source: IMPLAN Group, LLC

Note: Single Year Equivalents

Fiscal impacts resulting from IMPLAN include business taxes (including sales taxes), payroll taxes, property taxes, and other relevant taxes both locally and statewide as well as at the federal level. Based on the construction cost estimates for the Eklutna casino gaming development, the one-time fiscal impacts from construction would contribute \$628,400, \$1.0 million, and \$7.6 million to the local, state, and federal governments, respectively.

Table 6: Local Tax Impact from Construction: Eklutna **Casino (000's)**

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$0.0	\$0.0	\$0.0	\$0.0
Social Insurance Tax- Employer Contribution	\$0.0	\$0.0	\$0.0	\$0.0
TOPI: Sales Tax	\$21.8	\$31.6	\$67.8	\$121.1
TOPI: Property Tax	\$88.0	\$127.6	\$274.0	\$489.5
TOPI: Motor Vehicle License	\$0.3	\$0.5	\$1.0	\$1.8
TOPI: Severance Tax	\$0.1	\$0.1	\$0.2	\$0.4
TOPI: Other Taxes	\$0.8	\$1.2	\$2.6	\$4.6
TOPI: Special Assessments	\$0.4	\$0.5	\$1.2	\$2.1
OPI: Corporate Profits Tax	\$0.0	\$0.0	\$0.0	\$0.0
Personal Tax: Motor Vehicle License	\$4.7	\$0.4	\$1.2	\$6.3
Personal Tax: Property Taxes	\$1.9	\$0.2	\$0.5	\$2.6
Personal Tax: Other Tax (Fish/Hunt)	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$118.0	\$162.1	\$348.4	\$628.4

Source: IMPLAN Group, LLC

Table 7: State Tax Impact from Construction: Eklutna **Casino (000's)**

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$1.6	\$0.2	\$0.5	\$2.2
Social Insurance Tax- Employer Contribution	\$1.6	\$0.2	\$0.5	\$2.3
TOPI: Sales Tax	\$15.0	\$21.8	\$46.7	\$83.5
TOPI: Property Tax	\$6.9	\$10.0	\$21.5	\$38.5
TOPI: Motor Vehicle License	\$0.6	\$0.8	\$1.8	\$3.1
TOPI: Severance Tax	\$105.9	\$153.6	\$329.9	\$589.5
TOPI: Other Taxes	\$3.5	\$5.1	\$11.0	\$19.7
TOPI: Special Assessments	\$0.0	\$0.0	\$0.0	\$0.0
OPI: Corporate Profits Tax	\$9.1	\$29.0	\$82.5	\$120.5
Personal Tax: Motor Vehicle License	\$8.3	\$0.7	\$2.1	\$11.1
Personal Tax: Property Taxes	\$0.2	\$0.0	\$0.0	\$0.2
Personal Tax: Other Tax (Fish/Hunt)	\$106.3	\$8.8	\$25.0	\$140.0
Total	\$259.0	\$230.2	\$521.4	\$1,010.6

Source: IMPLAN Group, LLC

Table 8: Federal Tax Impact from Construction: Eklutna Casino (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$1,660.9	\$148.1	\$423.7	\$2,232.7
Social Insurance Tax- Employer Contribution	\$1,126.9	\$115.4	\$335.1	\$1,577.4
TOPI: Excise Tax	\$4.8	\$7.0	\$15.0	\$26.8
TOPI: Custom Duty	\$5.4	\$7.8	\$16.7	\$29.9
OPI: Corporate Profit Tax	\$22.5	\$71.9	\$204.3	\$298.7
Personal Tax: Income Tax	\$2,582.8	\$213.9	\$606.1	\$3,402.7
Personal Tax: Estate and Gift Tax	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$5,403.3	\$564.1	\$1,600.9	\$7,568.3

Source: IMPLAN Group, LLC

Social and Community Impacts

Vital to having local governments and citizens get on board with a casino opening in their region is an examination of the social and community impacts of the proposed casino development.

Increased local services and costs resulting from casino gaming operations generally fall into three categories: those arising from population and development growth, those arising from the impacts of increased visitation and traffic, and social impacts resulting from problem gambling.

Population and Employment Related Impacts

The Eklutna casino in Chugiak is expected to produce \$83.8 million in annual gross revenue and directly employ 228 people by the development's assumed second full year of operations.

For the purposes of this section, the greater Anchorage area, the host of the Eklutna casino gaming development, as well as the city of Chugiak will be used as the primary study areas of impacts as pertaining to employment and increased population.

Employment Impact

As noted, the proposed Eklutna casino gaming development is projected to require 228 jobs. These jobs are expected to be filled by a combination of local unemployed workers, local out-bound commuters, residents of neighboring cities, and new residents.

Unemployment in area stood at approximately 5,000 workers in 2023. While the 2020 recession adversely impacted unemployment in the area, pre-2020 we observed declines in unemployment levels in the market area. Additionally, 2021 and 2022 data reveal that unemployment continued its downward trajectory following the 2020 recession.

As the following table illustrates, the existing workforce in the area is estimated to fill nearly 88.0% of the new jobs created by a casino development, and the remaining 12% of workers are estimated to represent new residents moving into the area. Based on our quantitative analysis, we believe the subject development will have a neutral-to-positive on employment in the local area. The facility's operations will create jobs for the unemployed and bring more workers to the area

while employing a low percentage (1.1%) of Anchorage’s existing workforce—allowing other local employers to maintain their ability to attract workers.

Table 9: Eklutna Casino Project Source of Workforce

Source of Employment	Proposed Project	Metric Applied	Source
A. Number of New Employees	228	Total Jobs	Marnell Companies
B. Number of Commuters from outside Anchorage	1	0.5% based on Anchorage Workforce Commuting Patterns	US Census
C. Unemployed back to work	39	1.1% of a future estimate of 3,511 Unemployed Workers	US Census
D. Former Commuters Staying within Anchorage	162	3.0% of 5,384 Anchorage workers who currently commute outside the area	US Census
E. Total from Existing Area Workforce	201	B+C+D	
F. Estimated Total New Workers Needed	27	A-E	
% of New Employees	11.7%	F/A	

Household and Population Impact

Assuming approximately 1.1 casino workers per household, the total number of new households to Anchorage is estimated at 24. On average, households in Anchorage comprise 2.57 persons, bringing the total population increase to Anchorage to 63 people, or 0.02% of the projected 2027 Anchorage population

Table 10: Eklutna casino Development Impact on Households and Population

New employees moving to area	27
# of jobs per household	1.1
Number of new Households	24
Anchorage Avg. Household Size	2.57
New Area Population	63
% Increase of 2027 Total Area Pop	0.02%

Source: The Innovation Group

School Impact

Based on the number of new households and Alaska household metrics, the increase to school enrollment is estimated to be approximately 13 children in Anchorage. These represent an increase of 0.03% over projected 2027 enrollment.

Table 11: Eklutna Casino Impact on School Enrollment

	Metric	Anchorage
Number of New Households		24
Number of Households That Will Have Children*	32.1%	8
<i>Number of Children per Household**</i>	2.1	
Total Number of Children		17
Projected School Age Children	80%	13
2027 Projected School Enrollment		48,779
2027 Projected School Enrollment with Eklutna Casino		48,792
<i>% Change</i>		<i>0.03%</i>

Source: ESRI, US Census Bureau ACS

*Anchorage ACS 2018-22 estimated percent of households with presence of children under the age of 18

**Anchorage ACS 2018-22 average number of children per household with children under the age of 18

Housing Impact

The additional households that are projected to relocate in and around the Anchorage area will have a small, but positive impact on the housing market. New households will support the current and future development of housing in the community.

Currently, the number of housing units is nearly 10% greater than the number of households in Anchorage. This denotes an adequate number of units available for residents. Five-year projections estimate that the growth of housing units will outpace growth of families and households in the market. According to ESRI data, the number of housing units is projected to grow by about 240 units by 2027 in Anchorage. This will be sufficient to absorb the households estimated to relocate.

Table 12: Anchorage Population and Housing Trends

	2010 Census	2024 ESRI	2027 Projection
Population	291,826	287,385	284,766
<i>Percent Change</i>		-0.11%	-0.30%
Households	107,332	109,102	109,221
<i>Percent Change</i>		0.12%	0.04%
Families	70,544	68,078	67,811
<i>Percent Change</i>		-0.25%	-0.13%
Housing Units	113,032	119,622	119,861
<i>Percent Change</i>		0.41%	0.07%

Source: ArcGIS/ESRI

Anchorage had a vacancy rate for housing units of nearly 9% in 2024, which translated to roughly 10,500 units. If the same percentage is applied to the number of housing units projected for 2027, then there will be approximately 10,550 available housing units to accommodate the incoming households. Consequently, there is a substantial and sufficient housing supply to accommodate the 24 households estimated to relocate to Anchorage.

Conclusion

Overall, the proposed casino development in Anchorage is projected to increase local population by approximately 0.02 percent and increase school enrollment by approximately 0.03 percent.

Municipal Services Impact

Host communities should expect impacts similar in kind to other commercial development of similar scope and visitor potential. The projected increase in visitor population should be expected to lead to increases in public safety services and judicial system caseload.

Evidence suggests that on-going impacts from gaming developments to local communities are highly manageable and are typically offset by the new local tax dollars generated by the development. Of most direct consequence to casino development are police, fire, and EMS. These will largely fall into the purview of the local police and fire departments serving Anchorage.

Police

The Anchorage Police Department (APD) serves the entire Anchorage Municipality with its patrol area encompassing the core "Anchorage bowl", the Seward Highway corridor from Potter Creek south to McHugh Creek, the Glenn Highway corridor north of the Anchorage bowl to the municipality's border with the Matanuska-Susitna Municipality, and includes the communities of Eagle River, Chugiak, and Eklutna. APD is the largest municipal police force in Alaska serving the state's largest city. The department employs 610 people who hold both sworn and non-sworn positions.

According to the 2024 Approved General Government Operating Budget Report, the approved 2024 budget was increased by about 3.5% to about \$140 million from the revised 2023 budget of about \$135 million. Roughly 78.4% of the approved 2024 budget funds the police department's salaries and benefits. From 2022 to 2023, the department's budget increased by about 6% from roughly \$127 million to about \$135 million. In 2023, the police department's salaries and benefits accounted for 76.4% of the total budget.

In 2023, the Anchorage Municipality saw almost 40,000 trial court case filings with a judicial budget of nearly \$10 million, divided between the Equal Rights Commission, Equity & Justice, and Municipal Attorney. The Municipal Attorney budget was the largest segment at nearly \$8.6 million. Although the Anchorage Correctional Complex is located within the Anchorage Municipality, the Alaska Department of Corrections (DOC) is responsible for managing the state's correctional facilities. The Alaska DOC had a budget of nearly \$423 million for fiscal year 2023, and during that same period there were almost 28,000 bookings.

Fire

The Anchorage Fire Department (AFD) provides fire protection and emergency medical services to Anchorage. The AFD serves numerous areas including the incorporated areas of downtown Anchorage, Bird, Bootleggers Cove, Eagle River, Fairview, Indian, Mountain View, Muldoon, and Spenard, AFD is assisted by two volunteer fire departments operating in the outlying areas of the Anchorage Municipality. According to the 2024 Approved General Government Operating Budget, there are 15 fire stations with 14 fire engines, 13 medic units, 5 fire trucks, and 5 tenders.

The entire fire department employs just over 400 full-time positions. AFD’s approved budget for 2024 is approximately \$115 million, increasing 2.3% from the previous year. Salaries and benefits for 76.4% of the 2024 budget. From 2022 to 2023, the AFD budget decreased by about 2.2% from roughly \$115 million to about \$112 million.

Emergency Medical Services (EMS)

The Anchorage Fire Department also provides EMS to Anchorage with a mutual aid agreement between the Anchorage Fire Department, Ted Stevens International Airport Police and Fire Department, Joint Base Elmendorf-Richardson, Girdwood Fire Department, and Chugiak Volunteer Fire and Rescue Department. In 2023, EMS calls accounted for nearly 71% of the AFD’s total service calls, averaging about 73 EMS calls per day throughout the year.

Comparative Analysis

Research in other jurisdictions show that impacts to local communities are manageable and are typically offset by the new local tax dollars generated by the development. Based on casino evaluations performed by Purdue University and other research institutions on behalf of the Indiana Gaming Commission, statewide average actual costs borne by host communities are approximately 0.3% of gaming revenues. A study of the fiscal impact of Belterra on Switzerland County, Indiana in 2005 concluded, “...the added property, wagering and admissions taxes, and the incentive payments, exceeded the costs imposed by the riverboat. This allowed the county to increase appropriations—to cover added riverboat costs and to provide more public services—while charging Switzerland taxpayers less”.²

The introduction of a casino can lead to an increase in traffic patrol requirements and in the number of calls for police service. Arrests or citations related to increased visitation to the local area will create increased caseloads for the local judiciary. Even calls not resulting in arrest or citation can result in a need for increased police staffing.

A large, well-equipped fire department usually does not have to increase fire personnel in order to respond to incidents at a new casino. However, rural communities which do not have the types of equipment needed to respond to incidents at buildings beyond a certain height (e.g. ladder truck) often invest in new equipment. In general, rural municipalities have more limited service-infrastructure to handle large-scale developments and increased visitation than large cities, where impacts are marginal relative to resources.

² *Five-Year License Renewal: Belterra Resort Indiana, LLC*, performed by Indiana University-Purdue University Indianapolis on behalf of the Indiana Gaming Commission, October 2005, page 36.

Average Incident Rates

Incident rates from the data above were utilized in order to establish an average incident rate that will be applied to the proposed Eklutna development in order to project the number of calls and charges that can be expected with the addition of a casino to the community.

Table 13: Average Criminal Incident Rate

	Estimated Calls	Estimated Arrests
Yakama Nation Legends Casino - Toppenish, WA 2021	0.026%	0.006%
Northern Quest Resort & Casino - Airway Heights, WA 2021	0.025%	0.006%
Western US Tribal Gaming Facility 2019	0.044%	0.010%
Pacific Northwestern US Tribal Gaming Facility 2019	0.008%	0.002%
Average	0.026%	0.006%

In 2023, there were over 353,000 calls for service reported to Anchorage Police, excluding traffic stops. It is estimated that the proposed Eklutna casino will have approximately 199 police calls in 2027. These represent an increase of roughly 0.1% over projected 2027 volumes.

Table 14: Eklutna Casino Police Arrests Estimate

2027 Projected Casino Attendance	766,900
Average arrest rate	0.026%
Projected Service Calls	199
2027 Projected Calls for Service (Excluding Traffic Stops)	387,100
2027 Projected Service Calls with Eklutna Casino	387,299
% Change	0.1%

For additional information on criminal incidents and qualitative data from other jurisdictions, see Appendix B.

Fire Incident Rates

Incident rates from the data above was utilized in order to establish an incident rate that is applicable to the proposed Eklutna facility to project the number of calls the local fire departments can expect.

Table 15: Average Fire Department Call Rate Estimate

	Estimated Fire Incident Rate
Western US Tribal Gaming Facility	0.005%
Pacific Northwestern US Tribal Gaming Facility	0.007%
Average	0.006%

Source: The Innovation Group

As shown in the following table, it is estimated that the proposed casinos will result in an increase of 46 service calls in Anchorage. This is an increase of approximately 0.1% over the projected 2027 call volume.

Table 16: Eklutna Casino Fire/EMS Service Call Estimates

2027 Projected Casino Attendance	766,900
<i>Average service call rate</i>	<i>0.006%</i>
Projected Fire Service/EMS Calls	46
2027 Projected Fire/EMS Service Calls	47,245
2027 Projected Fire/EMS Service Calls with Eklutna Casino	47,291
<i>% Change</i>	<i>0.10%</i>

Conclusion and Implications

Fiscal Impacts and Municipal Services

Impacts arising from population and development growth would be effectively diluted by the size of Anchorage’s existing labor force, housing stock, and school capacity. Impacts arising from increased visitation—such as police, fire and EMS calls—are detailed below.

The gains in tax revenues that will accrue to the local governments as a result of increased economic activity generated by the Eklutna casino development and its employees, as detailed previously, will mitigate increases in municipal services expenses.

Estimated Municipal Expenses

The following table shows the estimated expenses attributable to the subject development from police, courts and public attorneys (both defending and prosecuting), corrections, and fire/EMS services, based on the Comparative Analysis section above and details below, with percent change estimates from that analysis applied to future budget estimates.

Table 17: Anchorage Municipality Police, Courts, and Fire/EMS Expense Increase – 2027

	Annual Budget		
Police	\$154,256,582	<i>Incremental %</i>	0.1%
		<i>Incremental \$</i>	\$79,457
Courts & Public Attorneys	\$11,543,410	<i>Incremental %</i>	0.5%
		<i>Incremental \$</i>	\$46,628
Corrections*	\$56,350,800	<i>Incremental %</i>	0.5%
		<i>Incremental \$</i>	\$284,718
Fire/EMS	\$125,520,339	<i>Incremental %</i>	0.1%
		<i>Incremental \$</i>	\$122,646
Total Increase in Municipal Services			\$533,449

Sources: City of Anchorage, The Innovation Group

*Statewide Alaska Corrections Budget and Incremental cost estimates based on data availability

To estimate court and attorney costs for the Anchorage Municipality, we calculated the maximum costs to the Anchorage judicial system under the highly conservative assumption that all 46 arrests estimated at the proposed Eklutna casino would not have pleaded out. Nearly 10,000 trial superior court cases were filed in the Anchorage Municipality in 2023; 46 cases would represent just 0.5%. The Municipality would likely be able to absorb that de minimis increase without incurring additional costs. However, to err on the side of caution, the attributable cost is estimated to be approximately over \$46,600.

Table 18: Anchorage Municipality Judicial Costs

	2023 Budget
Equal Rights Commission	\$836,568
Equity and Justice	\$437,953
Municipal Attorney	\$8,599,366
Total	\$9,873,887
<i>Annual Trial Superior Court Case Filings in Anchorage 2023</i>	<i>9,741</i>
<i>Annual Cases at Eklutna Casino</i>	<i>46</i>
<i>% at Eklutna Casino</i>	<i>0.5%</i>
<i>Cost</i>	<i>\$46,628</i>

Sources: City of Anchorage, The Innovation Group

While the Anchorage Municipality is home to the Anchorage Correctional Complex, the Alaska Department of Corrections is responsible for managing the state’s correctional facilities. Within the state budget for the fiscal year of 2023, a total of about \$56.4 million was allocated to the two correctional facilities in the Anchorage Municipality: Anchorage Correctional Complex and Hiland Mountain Correctional Center. With a total of approximately 9,000 bookings per year, the Anchorage Municipality spends an average of approximately \$6,000 per booking. The 46 projected bookings from the Eklutna casino only represent a 0.5% increase in bookings, so it is likely that the Alaska Department of Corrections could absorb this impact with little if any additional spending as a significant portion of their budget goes to fixed costs that would not

increase due to additional bookings. However, to be conservative, the attributable cost is estimated to be roughly \$285,000.

Table 19: Alaska Correctional Costs

FY 2023 Alaska Corrections Budget	\$56,350,800
FY2023 Bookings by AK Dept. of Corrections	9,107
Annual Projected Bookings from Eklutna Casino	46
Cost	\$284,718
% Change	0.5%

Source: Alaska Department of Corrections, The Innovation Group

Problem Gambling

Since Alaska does not currently offer any state-funded problem gambling services, we do expect a fiscal impact from the creation of prevention and treatment programs. The size and scope of these services will depend on the prevalence of problem gambling in the state. Since gambling is already in Alaska via Charitable Gaming, it is reasonable to assume a problem gambling population currently exists. We look to other markets and research studies to estimate the current number of problem gamblers in Alaska.

One of the most frequently cited studies on prevalence rates is *Estimating the Prevalence of Disordered Gambling Behavior in the United States and Canada: A Meta-analysis by the Harvard Medical School Division on Addictions*. The Harvard Medical School study analyzed 152 distinct previous prevalence studies and determined that 2.9% of the adult population could be considered problem or pathological gamblers. The *2021 Survey of Problem Gambling Services in the United States*³ conducted by the Association of Problem Gambling Service Administrators and the National Council on Problem Gambling estimated 2.2% of Alaska adults have a gambling problem. Combining these two studies, we estimate there are currently around 14,000 problem gamblers in Alaska, of which about 5,500 reside in Anchorage. Studies on problem gambling comorbidities show high rates of alcohol use disorder among problem gamblers⁴. Given Alaska’s prevalence of binge drinking⁵ is near the upper end of the national distribution, we would forecast the likely population of Alaska problem gamblers, after casinos are introduced, to be higher than the national average. Therefore, when assessing mitigation costs, we analyze a range from 2.2% (current estimates) to 5%, which we believe will be comfortably higher than the actual prevalence of problem gambling.

³ https://naadgs.org/wp-content/uploads/2022/06/NAADGS_2021_Survey_of_Publicly_Funded_Problem_Gambling_Services_in_the_United_States_v2.pdf

⁴ <https://pubmed.ncbi.nlm.nih.gov/15889941/>

⁵ Alaska has the 10th highest binge drinking prevalence in the country according to data obtained from the CDC <https://www.cdc.gov/alcohol/data-stats.htm>

To estimate mitigation costs for Alaska, we return to the *2021 Survey of Problem Gambling Services in the United States*, which includes data on state-funded problem gambling programs. We used three approaches from this report to triangulate the costs of a mitigation program for the state. First, we consider the distribution of per capita problem gambling services spend across the states. Second, we look at the distribution of total problem gambling services spend by state. Third, the study regresses problem gambling services spend by state on estimated number of problem gamblers by state. We use Alaska’s estimated number of problem gamblers to predict spend. These three approaches, described in more detail below, yield a range of about \$1 million to \$1.25 million in problem gambling services spend.

Competitive Effects Analysis

Alaska does not offer any Class III gaming; however, there are two approved Class II Indian Gaming facilities that operate under provisions approved by the National Indian Gaming Commission (NIGC). They are Klawock Bingo in Klawock and M.I.C. Gaming Hall in Metlakatla. They are located in the southeastern most portions of Alaska approximately 19 hours south of Juneau that includes both driving and a ferry ride. In short, these facilities are located multiple days travel away from the proposed Eklutna facility. Based on the analysis completed by The Innovation Group, we do not believe there will be any impact on these two existing facilities.

Charitable gaming, in the forms of pull-tabs, raffle/lottery, and bingo make up nearly 99% of all charitable gaming offered in Alaska. In 2019, the most recent available report not impacted by COVID, the charitable gaming adjusted gross income is estimated at over \$92 million. In 2020, as reported by the Alaska Department of Revenue, charitable gaming revenue decreased to under \$68 million, a decrease of about 30%. However, as with other gaming jurisdictions around the county, we believe that charitable gaming revenue has rebounded and even surpassed that of pre pandemic levels.

The Innovation Group used a gravity model to assist in realizing the impact of the proposed Class II Eklutna casino on the existing pull tab market. Pull tabs are offered at a wide range of locations across the state, including dozens in the greater Anchorage area. As a result, we utilized several of the more prominent locations in the area as a proxy within the model to represent the greater Anchorage pull tab market. We first calibrated the model to map the estimated current Anchorage pull tab market, then added the proposed Eklutna facility to the model to assess the impact it would have on that market.

The following table quantifies the impacts on charitable gaming from the proposed Eklutna casino development.

Table 20: Impact on Charitable Gaming from Casino

	2019	2027	Impact
Adjusted Gross Income (MM)	\$92.5	\$112.7	-\$24.3
Net Proceeds	-	21.8%	-21.6%

Source: AK Dept of Revenue; The Innovation Group

INTRODUCTION

Acorn Environmental (Acorn or Client) commissioned The Innovation Group for an Economic Impact Analysis and Community Impact Analysis of the proposed Native Village of Eklutna casino in Chugiak, Alaska in support of environmental documentation being prepared in compliance with the National Environmental Policy Act (NEPA). The Project is being pursued by the Native Village of Eklutna (Eklutna) and is expected to open with the first year of operation being 2027. Impacts are expressed in Year Two stabilized operations or 2028. Construction impacts are expressed in one calendar year prior to the opening on the casino, 2026.

The report summarizes the economic and community impacts—including job creation, increases in labor income, and positive revenue impacts for other local and state businesses, among others—that could be expected from the planned development, as well as the projected impacts on other tribal gaming facilities in the area.

Within the report we detail the Economic Impact Analysis, summarizing both our methodology and the corresponding results, followed by the Social and Community Impact Analysis and Competitive Effects Analysis.

ECONOMIC IMPACT ANALYSIS

The economic benefits—the revenues, jobs, and earnings—that accrue from the annual operations of an enterprise are termed *ongoing* impacts. The construction phase of a project is considered a *one-time* benefit to an area. This refers to the fact that these dollars will be introduced into the economy only during construction; construction impacts are expressed in single-year equivalence to be consistent in presentation with ongoing annual impacts.

The economic impact of an industry consists of three layers of impacts:

1. Direct effects
2. Indirect effects
3. Induced effects

The **direct effect** is the economic activity that occurs within the industry itself. The direct effect for casino operations represents the expenditures made by the facility in the form of employee compensation and purchases of goods and services (direct expenditures), which ultimately derive from patron spending on the casino floor, and patron spending on non-gaming amenities is an additional direct effect.

Indirect effects are the impact of the direct expenditures on other business sectors: for example, the advertising firm who handles a casino's local media marketing. Indirect effects reflect the economic spin-off that is made possible by the direct purchases of a casino. Firms providing goods and services to a casino have incomes partially attributable to the casino.

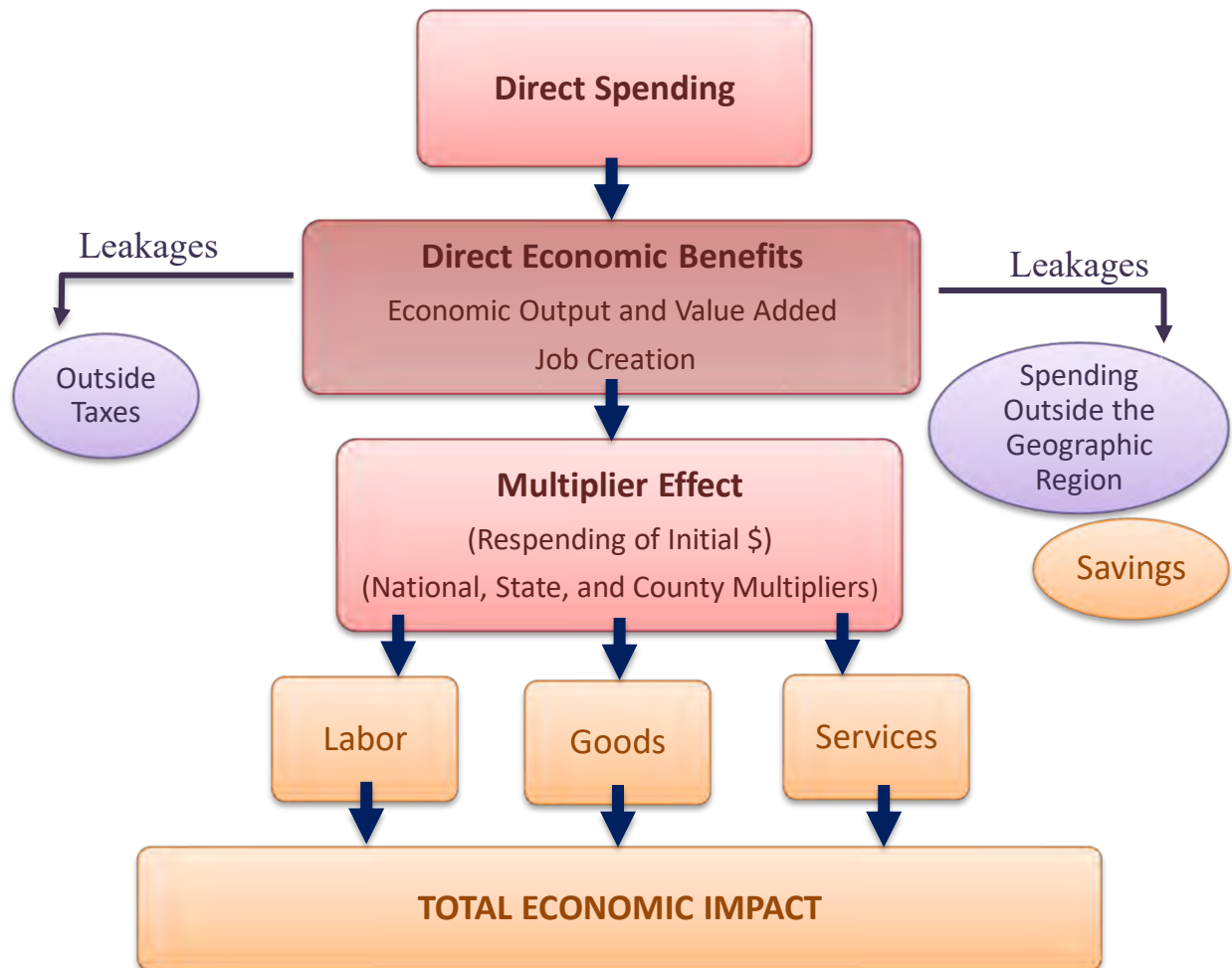
Finally, the **induced effects** result from the spending of labor income: for example, casino employees using their income to purchase consumer goods locally. As household incomes are affected by direct employment and spending, this money is recirculated through the household spending patterns causing further local economic activity.

The **total** economic impact of an industry is the sum of the three components.

Determining the direct economic impact is a critical first step in conducting a valid economic impact analysis. Once the direct expenditures are identified, the indirect and induced effects are calculated using multipliers derived from an input-output model⁶ of the economy. The IMPLAN input-output model identifies the relationships between various industries. The model is then used to estimate the effects of expenditures by one industry on other industries so that the total impact can be determined. Industry multipliers are developed based on U.S. Census data. IMPLAN accounts closely follow the accounting conventions used in the "Input-Output Study of the U.S. Economy" by the Bureau of Economic Analysis.

⁶ IMPLAN Online software and data were utilized for this study.

The following flow-chart shows how the economic impact model operates.



Multipliers

As shown in the following table, the standard data from IMPLAN for Sector 503-Gambling Industries (Except Casino Hotels) at the state level showed Other Property Income (OPI) at approximately 41% of total Output per Worker. Based on our experience analyzing the economic impacts of gaming within states that have existing casino resort operations and our knowledge of casino industry profitability, The Innovation Group believes this is an appropriate OPI to total Output per Worker ratio. We believe the Alaska state data within IMPLAN will provides realistic estimate of casino profitability and the corresponding economic impacts that will flow through the state's economy due to the introduction of gaming.

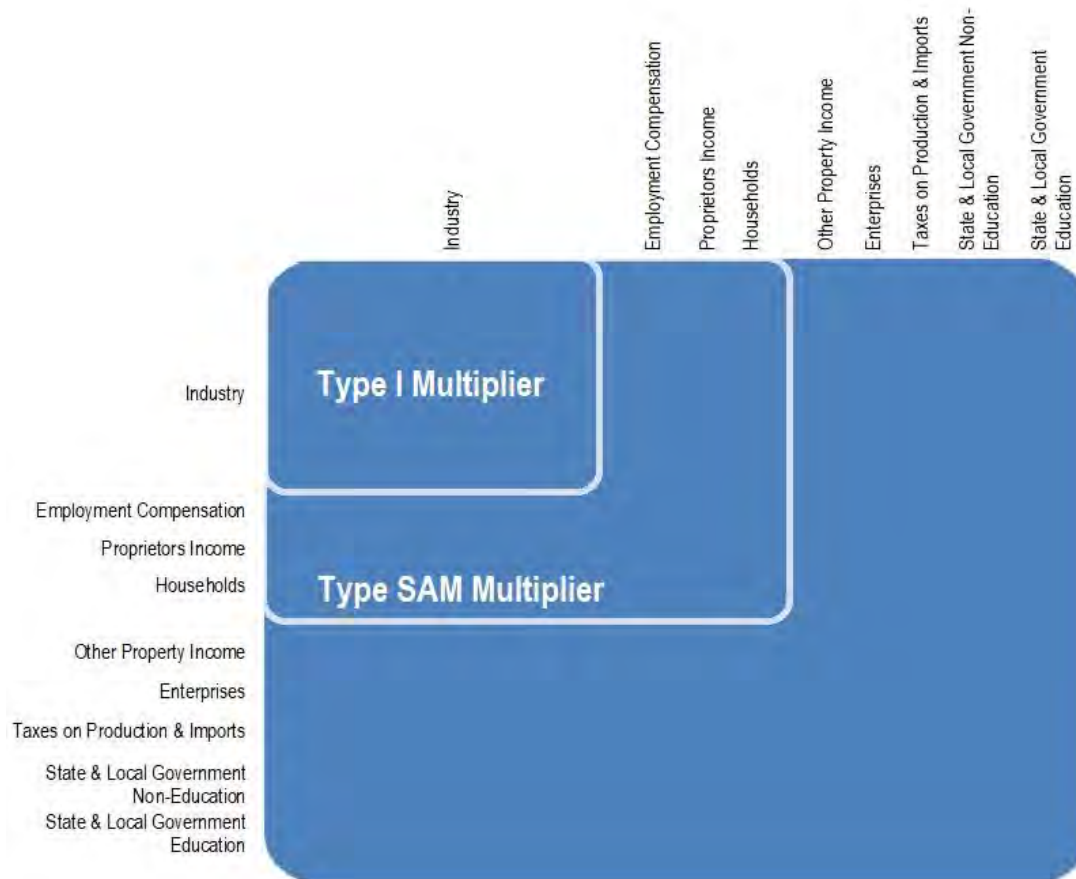
Table 21: IMPLAN Industry Sector 503 – Alaska State Data

Industry Ratio	Value (000's)	%
Employment Compensation (EC)	\$71,395	22%
Proprietor Income (PI)	\$7,280	2%
Other Property Income (OPI)	\$133,174	41%
Tax on Production & Imports (TOPI)	\$15,012	5%
<i>Value Added</i>	<i>\$226,860</i>	<i>69%</i>
Intermediate Expenditures (IE)	\$101,676	31%
Output per worker	\$328,536	

Source: IMPLAN Group, LLC, IMPLAN System (data and software); The Innovation Group

Compared to other industries with lower profitability levels, the gaming industry’s multipliers are lower since more of the output is shifted away from Intermediate Expenditures into Other Property Income (OPI). Multipliers are not applied to OPI in an economic impact analysis since it does not stimulate any additional impacts that can be attributed to the study area. For example, corporate profits from a casino operation may accrue to a company based in another state, effectively a leakage from the model. In other words, by generating higher OPI, more of the Output is effectively leaked out of state, and the multiplier effect is reduced. The table below illustrates.

Figure 1: IMPLAN Modeling Components



The following table shows the output multipliers for the Alaska state model for industry sector 503, Gambling Industries (Except Casino Hotels). To illustrate, an increase in direct effect of \$1,000,000 would produce a total effect of \$1,480,000 in the model.

Table 22: Output Multipliers for IMPLAN Industry Sector 503 – Alaska State

Multiplier	Standard Model
Type I	0.27
Type II	0.22
Total (SAM)	0.48

Source: IMPLAN Group, LLC, IMPLAN System (data and software); The Innovation Group

The Analysis-by-Parts method results in a much more conservative and we believe realistic estimate of the indirect and induced (or multiplier) effects of the operation of the casino component. The inputs into the IMPLAN casino model consist solely of the proforma estimates of employee compensation and purchases by the casino of goods and services. Operating profit and gaming taxes are excluded from the multiplier effect, although they are included in the displays of direct effects.

A Note on Substitution

Casino development frequently elicits concern that a substitution of consumer spending (the substitution effect) will negatively impact local businesses, especially smaller “mom and pop” retail, restaurant, and other entertainment industries. Intuitively it seems to be logical that spending at a casino would be diverted from other consumer activities such as going to a movie or taking a trip to the beach. However, numerous empirical studies have failed to find any conclusive evidence of significant economic substitution after the introduction of new casinos, nor is there any conclusive evidence as to the amount of spending that is substituted or the industry that it would have otherwise been spent in.

It is likely that countervailing positive effects dilute or outweigh any substitution that occurs. First, there is the increased household income in the area from casino employment. Secondly, there is a substantial body of research and case studies demonstrating the positive impacts that casinos have on surrounding local businesses. A review of studies of casino impacts on local business shows that casinos can stimulate local economies, resulting in communitywide growth, including in the local food and beverage business and retail businesses. Casino visitors stop at local retail outlets and restaurants in addition to some overnight casino guests patronizing local non-casino hotels. More information on local business impacts is contained in Appendix C.

It was determined after careful consideration that any substitution effects that may occur in the state as a result of the greater Anchorage area gaming operations would not be modeled in the economic impact analysis.

Economic Impact Modeling

The IMPLAN tools utilized to model direct effects vary according to the type of data collected for each input segment. There are six types of economic activity changes that IMPLAN is designed

to model for: industry, commodity, labor income, household income, industry spending pattern, and institutional (government) spending patterns. The most commonly used activity is an industry change, as the business generating a change in revenue, labor, or employment is often known and attributable to a specific industry sector.

The IMPLAN sectoring scheme is based on the 6-digit North American Industry Classification System (NAICS), developed under the auspices of the Office of Management and Budget (OMB), which classifies business establishments based on the activities they are primarily engaged in or the commodities they create. IMPLAN's current sectoring scheme aggregates the 2017 version of the NAICS classification scheme down to just 546 industry sectors. When an industry and the commodity produced by the industry have the same name, the commodity is considered the primary product of that industry and will share the same sector code. Other commodities produced by that industry are considered secondary products of that industry. Therefore, it is possible for more than one industry to produce a specific good or service.

When using the Industry Change function, the direct effect values are entered into IMPLAN using the appropriate sector and IMPLAN calculates the multiplier effects resulting from that direct spending. A commodity change will distribute the total demand or sales for the good or service as an industry change across all producing industries or institutions, based on their regional market share distribution of that commodity. For construction impacts as well as impacts from architectural and engineering, the Industry Change function was most appropriate for modeling the costs associated with land improvements, building, and design related costs. Costs associated with purchases of Furniture, Fixtures and Equipment (FF&E) and gaming machines were modeled using the Commodity Change function. The Industry Change function was also utilized for casino-related amenity operations including hotel, food and beverage, meetings and entertainment, retail/other, and museum/cultural center revenues.

Analysis-by-Parts for Gaming-Related Operating Impacts

The Analysis-by-Parts (ABP) differs from the traditional Industry Change Activity, as it separates out the multiplier effects into individual impact components, Intermediate Expenditure (indirect impacts from Type I multipliers) and Labor Income (induced impacts from Type II multipliers).⁷ This allows for more flexibility and customization capabilities in the analysis to model actual business operations.

For the Labor Income (LI) component we used a Labor Income Change activity to analyze the impact of the payroll of casino operations necessary to meet the demand or production level. The direct input for Labor Income in the casino analysis consisted of Employee Compensation from our operating pro forma models.

⁷ Economic impact multipliers consist of Type I, which measures only business-to-business purchases (indirect). Type II multipliers in the Bureau of Economic Analysis method measure the effects of local Household spending (induced). SAM (social accounting matrix) multipliers in the IMPLAN systems measure the combined indirect and induced effects.

For Intermediate Expenditures (IE), we import an Industry Spending Pattern to specify the goods and services of industry purchases needed for the sector 503 - Gambling industries (except casino hotels) in order to satisfy projected casino revenues. The purchase of these goods and services from local sources actually represents the first round of indirect purchases by the casino industry. The coefficients listed in an Industry Spending Pattern represent the amount spent on each commodity to produce one dollar of the industry's output, while the sum of all commodity coefficients equals total intermediate expenditures used by that industry sector.

Since the ABP technique shifts the direct inputs to indirect and induced impact results, the direct effects of these components are imputed using proforma operating statements, and in this instance was provided by Marnell Companies. The Analysis-by-Parts method results in a much more conservative and we believe realistic estimate of the indirect and induced (or multiplier) effects of the operation of the casino component. The inputs into the IMPLAN casino model consist solely of the proforma estimates of employee compensation and purchases by the casino of goods and services. Operating profit and gaming taxes are excluded from the multiplier effect, although they are included in the displays of direct effects.

Interpreting Results

The IMPLAN analysis expresses impacts (direct, indirect, and induced) for the following four economic variables:

Employment is measured in IMPLAN and by the U.S. Census as headcount, in other words the number of full and part-time workers supported by an economic activity.

Labor Income (LI) is compensation to all workers both employees and owners in terms of wages and salaries as well as benefits and payroll taxes. Profits from self-employed businesses can also be included in this category as compensation to the owner. These are known as employment compensation (EC) and proprietor income (PI) in IMPLAN. $LI = EC + PI$

Value-Added (VA) measures the industry or event's contribution to Gross Domestic Product (GDP). It consists of labor income (as described above), taxes on production and imports (TOPI), and other property income (OPI, such as corporate profits, rent payments, and royalties). It is the difference between a business or industry's total sales and the cost of all input materials or intermediate expenditures. $VA = LI + TOPI + OPI$

Output is the total value of industry production; it consists of value-added plus intermediate expenditures (IE). Output is frequently the total price paid by consumers for a good or service. $Output = VA + IE$

Value-Added is the most appropriate measure of economic impact because it excludes intermediate inputs, which are the goods and services (including energy, raw materials, semi-finished goods, and services purchased from all sources) used in the production process to produce *other* goods or services rather than for *final* consumption. For example, the paper stock used in a magazine publication is an intermediate input whereas paper stock sold in an office-supply store

is the final product sold to the consumer. The value of producing the magazine’s paper stock is accounted for in measures of GDP within the Paper Manufacturing sector, not in the Publishing sector.

Ongoing Operations

The ongoing operations of the gaming components in Chugiak and the greater Anchorage area will result in ongoing economic benefits that will accrue annually to the state of Alaska. Direct inputs for the casino development were derived from The Innovation Group’s previously completed gaming market assessment and pro forma analysis as provided by Marnell Companies of the casino property.

Proposed development on the project site includes a 58,000 square foot (sf) casino that will hold between between 700 and 1,000 Class II gaming devices, dependent of floor layout plan, and 438 paved surface parking spaces. The building would consist of one-story and would include approximately 33,000 square feet of gaming floor and approximately 24,000 square feet of support and circulation space. The casino would be open for up to 24 hours a day, 7 days a week, and would employ approximately 225 staff members (inclusive of food and beverage operations).

Operating Inputs

Direct effect inputs for casino operations account for the workers employed at the facility and the compensation they earn as well as direct spending (less any promotional rewards or benefits received) by the gaming operations. Staffing and employment compensation estimates were based on Marnell Companies (Marnell) operating pro forma model and input into the IMPLAN software. Marnell, as a long stand company with a great reputation in the industry as an architect, developer, interior designer, construction supervisor, and property manager, used the top line revenue inputs provided by The Innovation Group to estimate proper staffing of this size and magnitude. The staffing model has been calibrated to actual operating data from existing casinos and is on a Headcount basis.

The following table shows the total inputs utilized in the IMPLAN modeling for the Eklutna casino gaming development. Please note, relevant values for the assumed second full year of operations were used as input values for the direct effect inputs.

Table 23: Direct Effect Inputs – Ongoing Operations (000’s)

Industry Change	Revenue (MM)	Employment	Employee Compensation (MM)
495 Gambling industries (except casino hotels)	\$77,524	137	\$8,847
501 Full-service restaurants	\$7,272	91	\$3,316
Total	\$84,796	228	\$12,163

Source: IMPLAN Group, LLC, IMPLAN System (data and software); The Innovation Group.

Annual Economic Impacts from Operations

The following section presents the ongoing economic impacts resulting from the Eklutna casino gaming operating activities. These impacts occur annually and can be thought of as long-term benefits both locally and within the state as a whole.

Based on the operating data forecasted by The Innovation Group, the operations of the potential Eklutna gaming development are estimated to directly support 228 local workers annually, with annual labor income equaling \$12.2 million and total added value to the economy of \$48.1 million. These direct impacts drive a further \$19.6 million in added value to the economy and 190 jobs from indirect and induced effects.

In total, the local region is estimated to benefit from annual employment impacts of 419 workers, \$23.5 million in labor income and \$67.6 million in total value added, as shown in the table below.

Table 24: Eklutna Casino Local Ongoing Impacts

Impact Type	Employment	Labor Income (MM)	Value Added (MM)	Output (MM)
Direct Effect	228	\$12.2	\$48.1	\$84.8
Indirect Effect	116	\$6.8	\$11.2	\$21.6
Induced Effect	74	\$4.6	\$8.3	\$13.8
Total	419	\$23.5	\$67.6	\$120.2

Source: IMPLAN Group, LLC

Fiscal Impacts

Fiscal impacts from ongoing operations were primarily estimated utilizing IMPLAN software. Fiscal impacts resulting from IMPLAN include business taxes, payroll taxes, property taxes, and other relevant taxes both locally and statewide. Please note, we have excluded direct sales tax, property taxes, and corporate tax at the local, state, and federal level estimated by IMPLAN under the assumption that the Eklutna is exempt from these taxes. Based on the forecasted operations of the gaming development, IMPLAN estimates that \$446,000 of tax revenue would accrue to local governments, while over \$2.4 million of tax revenue would accrue to the state government, and an additional \$5.4 million would accrue to the federal government. It is important to note that the fiscal impacts estimated by IMPLAN and illustrated in the tables below exclude any gaming compact contributions generated for Alaska and includes taxes from direct, indirect, and induced effects.

Table 25: Local Tax Impact: Eklutna Casino Ongoing Operations (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$0.0	\$0.0	\$0.0	\$0.0
Social Insurance Tax- Employer Contribution	\$0.0	\$0.0	\$0.0	\$0.0
TOPI: Sales Tax	\$0.0	\$37.0	\$43.9	\$80.9
TOPI: Property Tax	\$0.0	\$149.5	\$177.5	\$327.0
TOPI: Motor Vehicle License	\$5.2	\$0.5	\$0.6	\$6.4
TOPI: Severance Tax	\$1.3	\$0.1	\$0.2	\$1.6
TOPI: Other Taxes	\$13.6	\$1.4	\$1.7	\$16.7
TOPI: Special Assessments	\$6.2	\$0.6	\$0.8	\$7.6
OPI: Corporate Profits Tax	\$0.0	\$0.0	\$0.0	\$0.0
Personal Tax: Motor Vehicle License	\$2.2	\$1.1	\$0.8	\$4.1
Personal Tax: Property Taxes	\$0.9	\$0.5	\$0.3	\$1.7
Personal Tax: Other Tax (Fish/Hunt)	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$29.4	\$190.7	\$225.7	\$445.9

Source: IMPLAN Group, LLC

Table 26: State Tax Impact: Eklutna Casino Ongoing Operations (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$1.1	\$0.4	\$0.3	\$1.8
Social Insurance Tax- Employer Contribution	\$1.1	\$0.4	\$0.3	\$1.8
TOPI: Sales Tax	\$0.0	\$25.5	\$30.3	\$55.8
TOPI: Property Tax	\$0.0	\$11.7	\$13.9	\$25.7
TOPI: Motor Vehicle License	\$9.3	\$1.0	\$1.1	\$11.4
TOPI: Severance Tax	\$1,740.9	\$180.0	\$213.8	\$2,134.6
TOPI: Other Taxes	\$58.3	\$6.0	\$7.2	\$71.5
TOPI: Special Assessments	\$0.0	\$0.0	\$0.0	\$0.0
OPI: Corporate Profits Tax	\$0.0	\$64.0	\$53.5	\$117.5
Personal Tax: Motor Vehicle License	\$4.0	\$2.0	\$1.3	\$7.3
Personal Tax: Property Taxes	\$0.1	\$0.0	\$0.0	\$0.1
Personal Tax: Other Tax (Fish/Hunt)	\$45.8	\$24.4	\$16.2	\$86.4
Total	\$1,814.7	\$291.1	\$321.7	\$2,427.5

Source: IMPLAN Group, LLC

Table 27: Federal Tax Impact: Eklutna Casino Ongoing Operations (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$843.0	\$404.0	\$275.0	\$1,522.0
Social Insurance Tax- Employer Contribution	\$752.0	\$306.9	\$217.4	\$1,276.3
TOPI: Excise Tax	\$79.2	\$8.2	\$9.7	\$97.1
TOPI: Custom Duty	\$88.2	\$9.1	\$10.8	\$108.2
OPI: Corporate Profit Tax	\$0.0	\$158.7	\$132.5	\$291.2
Personal Tax: Income Tax	\$1,111.6	\$591.9	\$393.5	\$2,097.0
Personal Tax: Estate and Gift Tax	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$2,874.1	\$1,478.8	\$1,039.0	\$5,391.9

Source: IMPLAN Group, LLC

Construction

Construction of the proposed development would bring one-time (non-recurring) benefits to Alaska. Construction impacts are expressed on a single-year basis. Therefore, the employment figures, for example, represent person-year equivalents; for a construction period of two years, the actual number of workers onsite would be half the person-year equivalent.

The impact of construction only relates to expenditures made directly by the development company to design, build and outfit the physical structure. For construction, architectural, and engineering impacts, the Industry Change function was employed using sectors 55-Construction of New Commercial Structures, Including Farm Structures and 457-Architectural, Engineering, and Related Services. Costs associated with purchases of Furniture, Fixtures and Equipment (FF&E) and machine purchases were modeled using the Commodity Change function sectors 3393-Wholesale Trade Services-Professional and Commercial Equipment and Supplies and 3391-All Other Miscellaneous Manufactured Products.

Construction Inputs

Based on high-level construction capital costs estimated by Marnell Companies, the following table outlines the final inputs used to calculate the economic impact by sector. The cost of gaming machines was separated out from the other FF&E. IMPLAN estimates what percentage of the purchases, including gaming machines, will originate from within the study area based on its Social Accounting Matrix (SAM).

Table 28: Eklutna Casino Estimated Construction Cost Inputs

Component	
<i>Industry Change</i>	
55 Construction of New Commercial Structures	\$42,000,000
457 Architectural, engineering, and related services	\$2,500,000
<i>Commodity Change</i>	
3393 Wholesale trade services	\$9,500,000
3391 All other miscellaneous manufactured products	\$17,500,000
Total Direct	\$71,500,000

Source: IMPLAN Group, LLC; Marnell Companies

Economic Impacts from Construction

Based on the construction capital costs estimated by The Innovation Group, the IMPLAN model estimates that construction of the Eklutna casino development will directly support 336 workers locally, with labor income equaling \$27.6 million and total added value to the economy of \$28.4 million. These direct impacts drive a further \$17.5 million in added value to the economy and 150 jobs from indirect and induced effects.

In total, the local region is estimated to have benefited from a one-time, single-year equivalent employment impact of 487 workers, \$37.2 million in labor income and \$45.8 million in total value added, as shown in the table below.

Table 29: Eklutna Casino Local Construction Impacts

Impact Type	Employment	Labor Income (MM)	Value Added (MM)	Output (MM)
Direct Effect	336	\$27.6	\$28.4	\$46.7
Indirect Effect	35	\$2.5	\$4.7	\$9.4
Induced Effect	115	\$7.1	\$12.8	\$21.2
Total	487	\$37.2	\$45.8	\$77.3

Source: IMPLAN Group, LLC
 Note: Single Year Equivalents

Fiscal Impacts

Fiscal impacts resulting from IMPLAN include business taxes (including sales taxes), payroll taxes, property taxes, and other relevant taxes both locally and statewide as well as at the federal level. Based on the construction cost estimates for the Eklutna casino gaming development, the one-time fiscal impacts from construction would contribute \$628,400, \$1.0 million, and \$7.6 million to the local, state, and federal governments, respectively.

Table 30: Local Tax Impact from Construction: Eklutna Casino (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$0.0	\$0.0	\$0.0	\$0.0
Social Insurance Tax- Employer Contribution	\$0.0	\$0.0	\$0.0	\$0.0
TOPI: Sales Tax	\$21.8	\$31.6	\$67.8	\$121.1
TOPI: Property Tax	\$88.0	\$127.6	\$274.0	\$489.5
TOPI: Motor Vehicle License	\$0.3	\$0.5	\$1.0	\$1.8
TOPI: Severance Tax	\$0.1	\$0.1	\$0.2	\$0.4
TOPI: Other Taxes	\$0.8	\$1.2	\$2.6	\$4.6
TOPI: Special Assessments	\$0.4	\$0.5	\$1.2	\$2.1
OPI: Corporate Profits Tax	\$0.0	\$0.0	\$0.0	\$0.0
Personal Tax: Motor Vehicle License	\$4.7	\$0.4	\$1.2	\$6.3
Personal Tax: Property Taxes	\$1.9	\$0.2	\$0.5	\$2.6
Personal Tax: Other Tax (Fish/Hunt)	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$118.0	\$162.1	\$348.4	\$628.4

Source: IMPLAN Group, LLC

Table 31: State Tax Impact from Construction: Eklutna Casino (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$1.6	\$0.2	\$0.5	\$2.2
Social Insurance Tax- Employer Contribution	\$1.6	\$0.2	\$0.5	\$2.3
TOPI: Sales Tax	\$15.0	\$21.8	\$46.7	\$83.5
TOPI: Property Tax	\$6.9	\$10.0	\$21.5	\$38.5
TOPI: Motor Vehicle License	\$0.6	\$0.8	\$1.8	\$3.1
TOPI: Severance Tax	\$105.9	\$153.6	\$329.9	\$589.5
TOPI: Other Taxes	\$3.5	\$5.1	\$11.0	\$19.7
TOPI: Special Assessments	\$0.0	\$0.0	\$0.0	\$0.0
OPI: Corporate Profits Tax	\$9.1	\$29.0	\$82.5	\$120.5
Personal Tax: Motor Vehicle License	\$8.3	\$0.7	\$2.1	\$11.1
Personal Tax: Property Taxes	\$0.2	\$0.0	\$0.0	\$0.2
Personal Tax: Other Tax (Fish/Hunt)	\$106.3	\$8.8	\$25.0	\$140.0
Total	\$259.0	\$230.2	\$521.4	\$1,010.6

Source: IMPLAN Group, LLC

Table 32: Federal Tax Impact from Construction: Eklutna Casino (000's)

Description	Direct	Indirect	Induced	Total
Social Insurance Tax- Employee Contribution	\$1,660.9	\$148.1	\$423.7	\$2,232.7
Social Insurance Tax- Employer Contribution	\$1,126.9	\$115.4	\$335.1	\$1,577.4
TOPI: Excise Tax	\$4.8	\$7.0	\$15.0	\$26.8
TOPI: Custom Duty	\$5.4	\$7.8	\$16.7	\$29.9
OPI: Corporate Profit Tax	\$22.5	\$71.9	\$204.3	\$298.7
Personal Tax: Income Tax	\$2,582.8	\$213.9	\$606.1	\$3,402.7
Personal Tax: Estate and Gift Tax	\$0.0	\$0.0	\$0.0	\$0.0
Total	\$5,403.3	\$564.1	\$1,600.9	\$7,568.3

Source: IMPLAN Group, LLC

SOCIAL AND COMMUNITY IMPACT ANALYSIS

This section assesses the social and community impacts of the proposed casino development in the Native Village of Eklutna.

Increased local services and costs due to casino gaming operations generally fall into three categories: those arising from population and development growth, those arising from the impacts of increased visitation and traffic, and social impacts resulting from problem gambling.

The analysis draws upon social science research as well as data analysis conducted by the Innovation Group. Although casinos are perceived to be different in kind from other commercial developments of comparable size and visitor base, inordinate negative impacts from casino development have not materialized, even in small communities with limited infrastructure and resources. In fact, experience over the past two decades has demonstrated that mitigation payments designed in anticipation of drastic impacts have often exceeded the actual need of the communities.

The perception that casinos breed crime is not supported by the evidence. While the *number* of reported crimes can increase, as with any commercial development that attracts visitors, casino gaming has not been shown to lead to an increase in crime *rates*.

Host communities should expect impacts similar in kind to other commercial developments of similar scope and visitor potential. The projected increase in visitor population should be expected to lead to increases in public safety services and judicial system caseload. The one significant difference in kind relates to the association between problem gambling and other social pathologies as discussed in Appendix A.

In summary, evidence suggests that on-going impacts to local communities are highly manageable, typically requiring only a small fraction of gaming revenues to address fully.

Section One: Population and Employment Related Impacts

The proposed Eklutna casino is expected to draw 816,000 annual gaming visits⁸ in 2027 and directly employ 228 people.

For the purposes of this section, the Anchorage Municipality will be used as the primary study area of impacts as it pertains to employment and increased population.

Employment Impact

As noted, the proposed Eklutna development is projected to require 228 jobs⁹ in 2027, the first full year of operations. These jobs are expected to be filled by a combination of local unemployed workers, local out-bound commuters, residents of neighboring cities, and new residents.

Unemployment in Anchorage stands at a little over 5,000 workers in 2023. While the 2020 recession adversely impacted unemployment in the Municipality, unemployment has declined below pre-covid levels as of 2023. Based on the historical growth rate, we have forecasted 3,511 unemployed Anchorage residents in 2027.

Table 33: Anchorage, AK Average Annual Employment Statistics

Year	Civilian labor force	Employment	Unemployment	Unemployment Rate
2010	157,743	146,993	10,750	6.8%
2011	158,382	148,731	9,651	6.1%
2012	157,748	149,177	8,571	5.4%
2013	157,637	149,423	8,214	5.2%
2014	157,650	149,591	8,059	5.1%
2015	156,476	148,856	7,620	4.9%
2016	155,471	147,254	8,217	5.3%
2017	154,046	145,674	8,372	5.4%
2018	151,067	143,566	7,501	5.0%
2019	149,118	142,485	6,633	4.4%
2020	148,392	137,421	10,971	7.4%
2021	149,263	140,525	8,738	5.9%
2022	149,832	144,596	5,236	3.5%
2023	150,823	145,654	5,169	3.4%

Source: Bureau of Labor Statistics, LAUS series, Not-seasonally adjusted; The Innovation Group

According to data from the US Census Bureau, over 96% of the 145,508 workers residing in Anchorage also work there, meaning nearly 4% commute out to other locations or work remotely

⁸ Visits, not individual patrons. Casino customers typically make more than one visit per year.

⁹ Headcount, including full and part-time workers.

for businesses located elsewhere. The percentage of jobs in Anchorage held by residents outside of Anchorage is 0.5%.

Table 34: Anchorage Commuting Patterns

A. Workers residing in Anchorage	145,508
B. Number of Jobs in Anchorage	140,800
C. Live & work in Anchorage	140,124
% who live & work in Anchorage	96.3%
% of A who commute OUT	3.7%
% of B who commute IN	0.5%

Source: US Census, Anchorage Economic Development Corporation (2022)

As the following tables illustrate, the existing workforce in the three locations is estimated to fill about 88% of the new jobs created by a casino development, and the remaining approximately 12% of workers are estimated to represent new residents moving into the area. Much of the analysis is based on comparable study data, such as the estimate 1.1% of future estimated employees going back to work. This estimate was based on observations of actual sources of employees at operating casino—we analyzed the percentage of employees at comparable casinos that came from their respective local unemployed populations and utilized an iterative process to determine a reasonable estimate for the subject casino. The subject casino is estimated to get 39 of its 201 workers (or 19.4% of its total workforce) from the local unemployed population. This 19.4% figure is in-line with The Innovation Group’s observations of actual casinos, with previously unemployed workers accounting for 15.0% to 20.0% of their respective workforces, on average. Additionally, there will be a number of employees that currently live in the Anchorage area that commute to areas confined by Anchorage but would then choose to change employment and not have to commute outside the area.

Table 35: Eklutna Casino Project Source of Workforce

Source of Employment	Proposed Project	Metric Applied	Source
A. Number of New Employees	228	Total Jobs	Marnell Companies
B. Number of Commuters from Outside Anchorage	1	0.5% based on Anchorage Workforce Commuting Patterns	US Census
C. Unemployed Back to Work	39	1.1% of a future estimate of 3,511 Unemployed Workers	US Census
D. Former Commuters Staying within Anchorage	162	3.0% of 5,384 Anchorage workers who currently commute outside the area	US Census
E. Total from Existing Area Workforce	201	B+C+D	
F. Estimated Total New Workers Needed	27	A-E	
% of New Employees	11.7%	F/A	

These estimates were based on previous Innovation Group research and results from an analysis of Plainridge casino in Plainville, Massachusetts. The survey of Plainridge employees demonstrates that casino employment is comprised mainly of workers already residing within commuting distance: a mixture of previously employed local residents looking for a better opportunity or work closer to home, along with previously unemployed local residents. The percentage of workers who moved to take the position with Plainridge was a small percentage of the staff. Furthermore, most casino workers had not had prior casino work experience.

Table 36: Plainridge Casino Source of Workforce

	# of Responses	Percentage
<i>Prior Employment status:</i>		
Unemployed	162	15.5%
Employed Part-time	363	34.7%
<i>Underemployed</i>	189	18.1%
Employed Full-time	522	49.9%
Total	1,047	100.0%
<i>Reason for taking the position</i>		
Job closer to home	305	29.1%
<i>Other results</i>		
No prior casino experience	902	86.2%
Moved to take the position	75	7.2%

New Employee Survey at Plainridge Park Casino: Analysis of First Two Years of Data Collection
 University of Massachusetts Donahue Institute, Economic and Public Policy Research Group, May 10, 2017

Other studies show similar impacts on employment. The Rappaport Institute for Greater Boston and the John F. Kennedy School of Economics at Harvard University (Baxandall and Sacerdote 2005) in a national, county-level study of Native American casinos found a slight decrease in unemployment rates after casinos opened. The analysis included all California casinos in existence in the 1990s. From their total sample of 156 casino counties, the Rappaport study isolated out 57 counties with large casinos and relatively low population and nine counties with both large casinos and large populations to see if there were statistical differences in terms of community impacts. The authors compared the county unemployment rate averaged for the year before and after a casino opens in a county, and then subtracted that number from the average state change in unemployment to isolate the county-specific effect. The following table shows their results:

Table 37: Rappaport Study Employment Results

	All Casino- Counties ¹	Counties with Large- Capacity Casinos ²	Populous Casino Counties ³
Population Growth (%)	+5*	+8.6	+8.1*
Total Employment (%)	+6.7*	+14.9*	+5.7
Unemployment (%)	-0.3	-1.2*	+0.5

*Statistically significant results at 99% confidence interval.

1. Reports how adjusted outcomes in 156 counties that introduced Indian-run casinos during the 1990s differed from the other 2,959 that did not.

2. The effect for 21 counties in the top 10th percentile in terms of number of slot machines (over 1,760).

3. The effect for the 57 casino counties in the top population quartile (over 55,000 residents).

The Rappaport study also highlighted results for three counties in southern California: Riverside, San Bernardino, and San Diego. In all three counties, the unemployment decreased relative to the state average. For example, before casino development, Riverside County had a slightly lower unemployment rate than the state average (by 0.3%). After casino development, the county’s unemployment rate was 1.7% lower than the state average, a relative decrease of 1.4 percentage points. San Bernardino had a relative decrease of 0.5 points and San Diego 0.4.

Table 38: Rappaport Study California County Results for Employment (%)

	Relative Unemployment % (County - State Average) Before	Relative Unemployment % (County - State Average) After	Change in Relative % Unemployment (Before - After)
Riverside, CA	-0.3	-1.7	-1.4
San Bernardino, CA	-2.2	-2.7	-0.5
San Diego, CA	-4.1	-4.5	-0.4

Household and Population Impact

Assuming approximately 1.1 casino workers per household, the total number of new households to Anchorage is estimated at 24. On average, households in Anchorage comprise 2.57 persons, bringing the total population increase to Anchorage to 63 people, or 0.02% of the projected 2027 Anchorage population.

Table 39: Eklutna Casino Impact on Households and Population

New employees moving to area	27
# of jobs per household	1.1
Number of new Households	24
Anchorage Avg. Household Size	2.57
New Area Population	63
% Increase of 2027 Total Area Pop	0.02%

School Impact

Based on the number of new households and Alaska household metrics, the increase to school enrollment is estimated to be approximately 13 children in Anchorage. These represent an increase of 0.03% over projected 2027 enrollment.

Table 40: Eklutna Casino Impact on School Enrollment

	Metric	Anchorage
Number of New Households		24
Number of Households That Will Have Children*	32.1%	8
<i>Number of Children per Household**</i>	<i>2.1</i>	
Total Number of Children		17
Projected School Age Children	80%	13
2027 Projected School Enrollment		48,779
2027 Projected School Enrollment with Eklutna Casino		48,792
<i>% Change</i>		<i>0.03%</i>

Source: ESRI, US Census Bureau ACS

*Anchorage ACS 2018-22 estimated percent of households with presence of children under the age of 18

**Anchorage ACS 2018-22 average number of children per household with children under the age of 18

School enrollment trends for the districts serving Anchorage can be found in the table below.

Table 41: Anchorage Annual School Enrollment

	2015-16	2016-17	2017-18	2018-19	2019-20	2020-21	2021-22	2022-23	CAGR
All Students (Public and AE)	48,827	48,789	48,098	46,689	46,701	41,856	43,054	43,727	-1.6%
Pre-Kindergarten	767	825	878	749	987	696	385	438	-7.7%
Kindergarten	3,929	3,827	3,786	3,641	3,595	2,869	3,364	3,285	-2.5%
Grades 1-8	29,673	29,732	29,415	28,407	28,643	25,457	26,271	26,852	-1.4%
Grades 9-12	14,458	14,405	14,019	13,446	13,476	12,834	13,034	13,152	-1.3%

Source: National Center for Educational Statistics (NCES)

Housing Impact

The additional households that are projected to relocate in and around the Anchorage area will have a small impact on the housing market. New households will support the current and future development of housing in the community.

Currently, the number of housing units is nearly 10% greater than the number of households in Anchorage. This denotes an adequate number of units available for residents. Five-year projections estimate that the growth of housing units will outpace growth of families and households in the market. According to ESRI data, the number of housing units is projected to grow by about 240 units by 2027 in Anchorage. This will be sufficient to absorb the households estimated to relocate.

Table 42: Anchorage Population and Housing Trends

	2010 Census	2024 ESRI	2027 Projection
Population	291,826	287,385	284,766
<i>Percent Change</i>		-0.11%	-0.30%
Households	107,332	109,102	109,221
<i>Percent Change</i>		0.12%	0.04%
Families	70,544	68,078	67,811
<i>Percent Change</i>		-0.25%	-0.13%
Housing Units	113,032	119,622	119,861
<i>Percent Change</i>		0.41%	0.07%

Source: ArcGIS/ESRI

As can be seen in the following table, Anchorage had a vacancy rate for housing units of nearly 9% in 2024, which translated to roughly 10,500 units. If the same percentage is applied to the number of housing units projected for 2027, then there will be approximately 10,550 available housing units to accommodate the incoming households. Consequently, there is a substantial and sufficient housing supply to accommodate the 24 households estimated to relocate to Anchorage.

The Anchorage Municipality has higher than average rates of renter-occupied housing compared to both the Alaska and national averages. The new jobs created at the proposed casino developments will provide stable employment for existing residents that can support homeownership, increasing homeownership figures throughout the local area. The addition of new families has the potential to support current and planned housing development throughout the city without overwhelming existing infrastructure.

Table 43: 2024 Occupied Housing Units by Tenure

	Anchorage	Alaska	United States
Owner-Occupied	55.7%	53.1%	57.9%
Renter-Occupied	35.5%	29.0%	32.1%
Vacant	8.8%	17.9%	10.0%

Source: ArcGIS/ESRI

Conclusion

Overall, the proposed casino development in Anchorage is projected to increase local population by approximately 0.02 percent and increase school enrollment by approximately 0.03 percent.

Section Two: Municipal Services Impact

Host communities should expect impacts similar in kind to other commercial developments of similar scope and visitor potential. The projected increase in visitor population should be expected to lead to increases in public safety services and judicial system caseload.

Evidence suggests that on-going impacts to local communities are highly manageable, typically requiring only a small fraction of gaming revenues to address fully. Of most direct consequence to casino development are police, fire, and EMS. These will largely fall into the purview of the local police and fire department serving Anchorage.

Police

The Anchorage Police Department (APD) serves the entire Anchorage Municipality with its patrol area encompassing the core "Anchorage bowl", the Seward Highway corridor from Potter Creek south to McHugh Creek, the Glenn Highway corridor north of the Anchorage bowl to the municipality's border with the Matanuska-Susitna Municipality, and includes the communities of Eagle River, Chugiak, and Eklutna. APD is the largest municipal police force in Alaska serving the state's largest city. The department employs 610 people who hold both sworn and non-sworn positions.

According to the 2024 Approved General Government Operating Budget Report, the approved 2024 budget was increased by about 3.5% to about \$140 million from the revised 2023 budget of about \$135 million. Roughly 78.4% of the approved 2024 budget funds the police department's salaries and benefits. From 2022 to 2023, the department's budget increased by about 6% from roughly \$127 million to about \$135 million. In 2023, the police department's salaries and benefits accounted for 76.4% of the total budget.

In 2023, the Anchorage Municipality saw almost 40,000 trial court case filings with a judicial budget of nearly \$10 million, divided between the Equal Rights Commission, Equity & Justice, and Municipal Attorney. The Municipal Attorney budget was the largest segment at nearly \$8.6 million. Although the Anchorage Correctional Complex is located within the Anchorage Municipality, the Alaska Department of Corrections (DOC) is responsible for managing the state's correctional facilities. The Alaska DOC had a budget of nearly \$423 million for fiscal year 2023, and during that same period there were almost 28,000 bookings.

Fire

The Anchorage Fire Department (AFD) provides fire protection and emergency medical services to Anchorage. The AFD serves numerous areas including the incorporated areas of downtown Anchorage, Bird, Bootleggers Cove, Eagle River, Fairview, Indian, Mountain View, Muldoon, and Spenard, AFD is assisted by two volunteer fire departments operating in the outlying areas of the Anchorage Municipality, specifically speaking about the Chugiak Volunteer Fire and Rescue Company (CVFRD) and the Girdwood Fire Department. According to the 2024 Approved General Government Operating Budget, there are 15 fire stations with 14 fire engines, 13 medic units, 5 fire trucks, and 5 tenders. The entire fire department employs just over 400 full-time positions. AFD's approved budget for 2024 is approximately \$115 million, increasing 2.3% from the previous year. Salaries and benefits for 76.4% of the 2024 budget. From 2022 to 2023, the AFD

budget decreased by about 2.2% from roughly \$115 million to about \$112 million. The figures are inclusive of the CVFRD budget that consists of over 100 members and responds to more than 1,000 emergency calls per year, of which most are medically related. Of note, Station 34 is the nearest fire station to the Project Site and is located approximately 0.2 miles to the north and is serviced by the CVFRD.

Emergency Medical Services (EMS)

The Anchorage Fire Department also provides EMS to Anchorage with a mutual aid agreement between the Anchorage Fire Department, Ted Stevens International Airport Police and Fire Department, Joint Base Elmendorf-Richardson, Girdwood Fire Department, and Chugiak Volunteer Fire and Rescue Department. In 2023, EMS calls accounted for nearly 71% of the AFD's total service calls, averaging about 73 EMS calls per day throughout the year.

Comparative Analysis

Research in other jurisdictions shows that impacts to local communities are manageable, typically requiring only a small fraction of gaming revenues to address fully. Based on casino evaluations performed by Purdue University and other research institutions on behalf of the Indiana Gaming Commission, statewide average actual costs borne by host communities are approximately 0.3% of gaming revenues. A study of the fiscal impact of Belterra on Switzerland County, Indiana in 2005 concluded, "...the added property, wagering and admissions taxes, and the incentive payments, exceeded the costs imposed by the riverboat. This allowed the county to increase appropriations—to cover added riverboat costs and to provide more public services—while charging Switzerland taxpayers less".¹⁰

The introduction of a casino can lead to an increase in traffic patrol requirements and in the number of calls for police service. Arrests or citations related to increased visitation to the local area will create increased caseloads for the local judiciary. Even calls not resulting in arrest or citation can result in a need for increased police staffing.

A large, well-equipped fire department usually does not have to increase fire personnel in order to respond to incidents at a new casino. However, rural communities, which do not have the types of equipment needed to respond to incidents at buildings beyond a certain height (e.g. ladder truck), often invest in new equipment. In general, rural municipalities have more limited service-infrastructure to handle large-scale developments and increased visitation than large cities, where impacts are marginal relative to resources.

Criminal Incident Rates

The following section provides data for the number of calls police respond to and the numbers of arrests made at a sampling of casinos throughout the western United States. The analysis includes

¹⁰ *Five-Year License Renewal: Belterra Resort Indiana, LLC*, performed by Indiana University-Purdue University Indianapolis on behalf of the Indiana Gaming Commission, October 2005, page 36.

several individual properties. Please note, due to the confidential nature of some of the data included in this section, some of the property names have been given generic labels.

Yakama Nation Legends Casino – Toppenish, Washington

Located in Toppenish, Washington, the Yakama Nation Legends Casino is an approximately 25-minute drive time from Yakima, Washington. The facility is roughly 3 hours northeast of Portland and two and a half hours southeast of Seattle. The following table provides attendance and service call figures utilized to calculate the incident rates for calls made to local police.

Table 44: Toppenish, WA Service Call Rates

Casino Attendance 2021	1,291,575
2021 Casino Service Calls	334
Incident Rate for Service Calls	0.026%

Source: Toppenish Police Department, Washington State Gambling Commission, The Innovation Group

Northern Quest Resort & Casino – Airway Heights, Washington

The Northern Quest Resort and Casino is located just outside of Spokane, Washington—roughly 15 minutes to the west. The following table provides attendance and service call figures utilized to calculate the incident rates for calls made to local police.

Table 45: Airway Heights, WA Service Call Rates

Casino Attendance 2021	1,691,348
2021 Casino Service Calls	422
Incident Rate for Service Calls	0.025%

Source: Airway Heights Police Department, Washington State Gambling Commission, The Innovation Group

Western US Tribal Gaming Facility

This western US tribal gaming facility received over 2 million visits in 2019. The following table provides attendance and service call figures utilized to calculate the incident rates for calls made to local police.

Table 46: Western US Tribal Gaming Facility Service Call Rates

Casino Attendance 2019	2,043,000
Average Casino Service Calls	895
Incident Rate for Service Calls	0.044%

Source: Proprietary and confidential data, The Innovation Group

Pacific Northwestern US Tribal Gaming Facility

This Pacific Northwestern US tribal gaming facility received nearly 2.5 million visits in 2019. The following table provides attendance and average annual service call figures utilized to calculate the incident rates for calls made to local police.

Table 47: Pacific Northwestern Tribal Gaming Facility Service Call Rates

Casino Attendance 2019	2,485,159
Average Casino Service Calls	191
Incident Rate for Service Calls	0.008%

Source: Proprietary and confidential data, The Innovation Group

Average Incident Rates

Incident rates from the data above were utilized in order to establish an average incident rate that will be applied to the proposed Eklutna development in order to project the number of calls and charges that can be expected with the addition of a casino to the community.

Table 48: Average Criminal Incident Rate

	Estimated Calls	Estimated Arrests
Yakama Nation Legends Casino - Toppenish, WA 2021	0.026%	0.006%
Northern Quest Resort & Casino - Airway Heights, WA 2021	0.025%	0.006%
Western US Tribal Gaming Facility 2019	0.044%	0.010%
Pacific Northwestern US Tribal Gaming Facility 2019	0.008%	0.002%
Average	0.026%	0.006%

In 2023, there were over 353,000 calls for service reported to Anchorage Police, excluding traffic stops. It is estimated that the proposed Eklutna casino will have approximately 199 police calls in 2027. These represent an increase of roughly 0.1% over projected 2027 volumes.

Table 49: Eklutna Casino Police Arrests Estimate

2027 Projected Casino Attendance	766,900
Service Call Rate	0.026%
Projected Service Calls	199
2027 Projected Calls for Service (Excluding Traffic Stops)	387,100
2027 Projected Service Calls with Eklutna Casino	387,299
% Change	0.05%

For additional information on criminal incidents and qualitative data from other jurisdictions, see appendix.

Fire Incident Rates

The following section provides data for the number of calls local fire departments respond to for two casinos in the western United States. Most calls made to the area around the casino are calls for EMS service or traffic accidents. There are a very small number of calls that are labeled as fire and usually involve a false alarm.

Western US Tribal Gaming Facility

The following table provides attendance and service call figures utilized to calculate the incident rates for calls made to the local fire department.

Table 50: Western US Tribal Gaming Facility Fire Call Rate

Casino Attendance 2019	2,043,000
Average Casino Service Calls	97
Incident Rate for Service Calls	0.005%

Source: Proprietary and confidential data, The Innovation Group

Pacific Northwestern US Tribal Gaming Facility

This pacific northwestern US tribal gaming facility received nearly 2.5 million visits in 2019. The following table provides attendance and average annual service call figures utilized to calculate the incident rates for calls made to local fire department and EMS services.

Table 51: Pacific Northwestern Tribal Gaming Facility Service Fire Call Rate

Casino Attendance 2019	2,485,159
Average Casino Service Calls	167
Incident Rate for Service Calls	0.007%

Source: Proprietary and confidential data, The Innovation Group

Average Fire Incident Rates

Incident rates from the data above was utilized in order to establish an incident rate that is applicable to the proposed Eklutna facility to project the number of calls the local fire departments can expect.

Table 52: Average Fire Department Call Rate Estimate

	Estimated Fire Incident Rate
Western US Tribal Gaming Facility	0.005%
Pacific Northwestern US Tribal Gaming Facility	0.007%
Average	0.006%

Source: The Innovation Group

As shown in the following table, it is estimated that the proposed casinos will result in an increase of 46 service calls in Anchorage. This is an increase of approximately 0.1% over the projected 2027 call volume.

Table 53: Eklutna Casino Fire/EMS Service Call Estimates

2027 Projected Casino Attendance	766,900
Average service call rate	0.006%
Projected Fire Service/EMS Calls	46
2027 Projected Fire/EMS Service Calls	47,245
2027 Projected Fire/EMS Service Calls with Eklutna Casino	47,291
% Change	0.1%

The following provides a breakdown of calls to the Anchorage Fire Department.

Table 54: Anchorage Fire Department Calls 2021-2023 (Includes EMS)

Type	2021	2022	2023
Total	37,652	41,000	42,504
EMS Incidents by Type	26,557	28,326	29,798
Medical	25,304	26,907	28,271
Vehicle Accidents	1,210	1,331	1,427
Rescue (All Types)	43	88	100
Non-Emergent Incidents by Type	9,889	10,928	11,173
Good Intent	4,867	4,771	5,263
Service Calls	2,374	3,171	3,079
False Alarms	2,648	2,986	2,831
Fire Incidents by Type	1,206	1,746	1,533
Structure Fires	279	383	400
Vehicle Fires	122	196	162
Wildland/Outdoor	98	218	115
Trash/Unauthorized	707	949	856

Total Population	288,330	288,015	287,700
<i>Incidents per 100 residents</i>	<i>13.1</i>	<i>14.2</i>	<i>14.8</i>

Conclusion and Implications

Fiscal Impacts and Municipal Services

Impacts arising from population and development growth would be effectively diluted by the size of Anchorage’s existing labor force, housing stock, and school capacity. Impacts arising from increased visitation—such as police, fire and EMS calls—are less than significant but detailed below. Moreover, the gains in tax revenues that will accrue to the local governments due to increased economic activity generated by the casino and its employees will mitigate increases in municipal services expenses. In sum, the casino will have a significant positive impact on Anchorage’s governmental services.

Estimated Municipal Expenses

The following table shows the estimated expenses attributable to the subject development from police, courts and public attorneys (both defending and prosecuting), corrections, and fire/EMS services, based on the Comparative Analysis section above and details below, with percent change estimates from that analysis applied to future budget estimates.

Table 55: Anchorage Municipality Police, Courts, and Fire/EMS Expense Increase – 2027

	Annual Budget		
Police	\$154,256,582	<i>Incremental %</i>	<i>0.1%</i>
		<i>Incremental \$</i>	<i>\$79,457</i>
Courts & Public Attorneys	\$11,543,410	<i>Incremental %</i>	<i>0.5%</i>
		<i>Incremental \$</i>	<i>\$46,628</i>
Corrections*	\$56,350,800	<i>Incremental %</i>	<i>0.5%</i>
		<i>Incremental \$</i>	<i>\$284,718</i>
Fire/EMS	\$125,520,339	<i>Incremental %</i>	<i>0.1%</i>
		<i>Incremental \$</i>	<i>\$122,646</i>
Total Increase in Municipal Services			\$533,449

Sources: City of Anchorage, The Innovation Group

*Budget for correctional facilities within Anchorage Municipality from the Statewide Alaska Corrections Budget

To estimate court and attorney costs for the Anchorage Municipality, we calculated the maximum costs to the Anchorage judicial system under the highly conservative assumption that all 46 arrests estimated at the proposed Eklutna casino would not have pleaded out. The 46 service calls were calculated by multiplying the total visits to the property (approximately 767,000) by the comparable sets average service call rate (0.006%). Nearly 10,000 trial superior court cases were filed in the Anchorage Municipality in 2023; 46 cases would represent just 0.5%. The Municipality would likely be able to absorb that de minimis increase without incurring additional costs.

However, to err on the side of caution, the attributable cost is estimated to be approximately over \$46,600.

Table 56: Anchorage Municipality Judicial Costs

	2023 Budget
Equal Rights Commission	\$836,568
Equity and Justice	\$437,953
Municipal Attorney	\$8,599,366
Total	\$9,873,887
<i>Annual Trial Superior Court Case Filings in Anchorage 2023</i>	9,741
<i>Annual Cases at Eklutna Casino</i>	46
<i>% at Eklutna Casino</i>	0.5%
<i>Cost</i>	\$46,628

Sources: City of Anchorage, The Innovation Group

While the Anchorage Municipality is home to the Anchorage Correctional Complex, the Alaska Department of Corrections is responsible for managing the state’s correctional facilities. Within the state budget for the fiscal year of 2023, a total of about \$56.4 million was allocated to the two correctional facilities in the Anchorage Municipality: Anchorage Correctional Complex and Hiland Mountain Correctional Center. With a total of approximately 9,000 bookings per year, the Anchorage Municipality spends an average of approximately \$6,000 per booking. The 46 projected bookings from the Eklutna casino only represent a 0.5% increase in bookings, so it is likely that the Alaska Department of Corrections could absorb this impact with little if any additional spending as a significant portion of their budget goes to fixed costs that would not increase due to additional bookings. However, to be conservative, the attributable cost is estimated to be roughly \$285,000.

Table 57: Anchorage Correctional Costs

FY 2023 Anchorage Municipality Corrections Budget	\$56,350,800
FY2023 Anchorage Municipality Bookings	9,107
Annual Projected Bookings from Eklutna Casino	46
Cost	\$284,718
<i>% Change</i>	0.5%

Source: Alaska Department of Corrections, The Innovation Group

Problem Gambling

Since Alaska does not currently offer any state-funded problem gambling services, we do expect a fiscal impact from the creation of prevention and treatment programs. The size and scope of these services will depend on the prevalence of problem gambling in the state. Since gambling is already in Alaska via Charitable Gaming, it is reasonable to assume a problem gambling population

currently exists. We look to other markets and research studies to estimate the current number of problem gamblers in Alaska.

One of the most frequently cited studies on prevalence rates is *Estimating the Prevalence of Disordered Gambling Behavior in the United States and Canada: A Meta-analysis by the Harvard Medical School Division on Addictions*. The Harvard Medical School study analyzed 152 distinct previous prevalence studies and determined that 2.9% of the adult population could be considered problem or pathological gamblers. The *2021 Survey of Problem Gambling Services in the United States*¹¹ conducted by the Association of Problem Gambling Service Administrators and the National Council on Problem Gambling estimated 2.2% of Alaska adults have a gambling problem. Combining these two studies, we estimate there are currently around 14,000 problem gamblers in Alaska, of which about 5,500 reside in Anchorage. Studies on problem gambling comorbidities show high rates of alcohol use disorder among problem gamblers¹². Given Alaska's prevalence of binge drinking¹³ is near the upper end of the national distribution, we would forecast the likely population of Alaska problem gamblers, after casinos are introduced, to be higher than the national average. Therefore, when assessing mitigation costs, we analyze a range from 2.2% (current estimates) to 5%, which we believe will be comfortably higher than the actual prevalence of problem gambling.

To estimate mitigation costs for Alaska, we return to the *2021 Survey of Problem Gambling Services in the United States*, which includes data on state-funded problem gambling programs. We used three approaches from this report to triangulate the costs of a mitigation program for the state. First, we consider the distribution of per capita problem gambling services spend across the states. Second, we look at the distribution of total problem gambling services spend by state. Third, the study regresses problem gambling services spend by state on estimated number of problem gamblers by state. We use Alaska's estimated number of problem gamblers to predict spend. These three approaches, described in more detail below, yield a range of about \$1 million to \$1.25 million in problem gambling services spend.

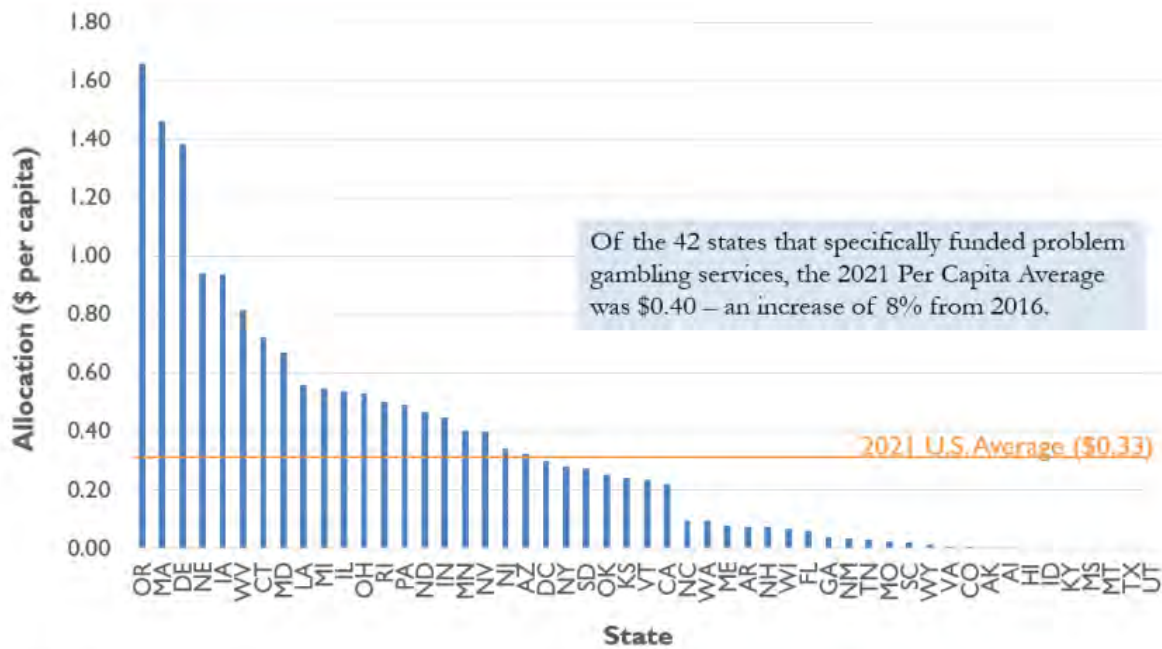
The bar chart below shows per capita funding for problem gambling services. Among the states with state-funded problem gambling programs, average per capita funding is \$0.40, an increase of 8% from 2016. Oregon spends the most per capita at over \$1.60. Assuming Alaska matched Oregon's per capita spend, Alaska would spend approximately \$1.17 million annually on problem gambling prevention and mitigation.

¹¹ https://naadgs.org/wp-content/uploads/2022/06/NAADGS_2021_Survey_of_Publicly_Funded_Problem_Gambling_Services_in_the_United_States_v2.pdf

¹² <https://pubmed.ncbi.nlm.nih.gov/15889941/>

¹³ Alaska has the 10th highest binge drinking prevalence in the country according to data obtained from the CDC <https://www.cdc.gov/alcohol/data-stats.htm>

Figure 2: 2021 Per Capita Allocation for Problem Gambling Services by U.S. State



Note: Includes only funds line itemed for problem gambling services and passing through a state agency, including the District of Columbia. Per Capita Average is based on the 2020 Census state population, including minors. Some states had problem gambling service allocations at the level depicted on this graph, however, had expended fewer than 10% of the funds from that allocation in 2021 (AR, DC, WY) while others expended approximately 70% of the allocation in 2021 (NJ & MI)

Source: 2021 Survey of Problem Gambling Services in the United States

Next, we analyzed total spend on problem gambling services by state. That chart below lists total spend by state (note: nine states do not have any funding for problem gambling services, three more than in 2016). If Alaska matched the median level of funding, it would spend \$1.25 million on problem gambling services.

Table 58: Total Spend on Problem Gambling Services by State (Fiscal Year 2021)

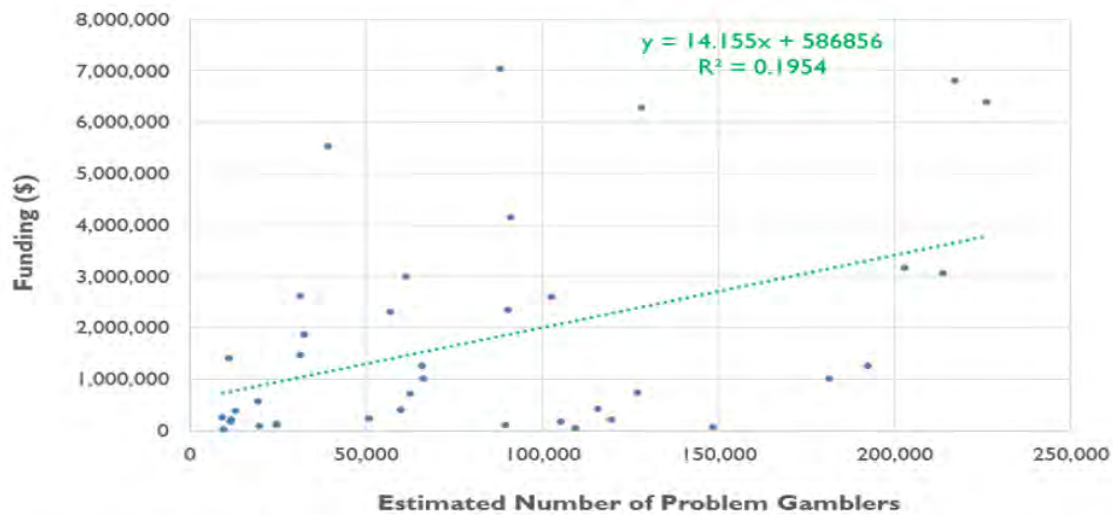
State	Total Spend on Problem Gambling Services
Arizona	\$2,344,300
Arkansas	\$220,000
California	\$8,681,501
Colorado	\$30,000
Connecticut	\$2,609,799
Delaware	\$1,389,842
Florida	\$1,250,000
Georgia	\$400,000
Illinois	\$6,800,000
Indiana	\$3,047,034
Iowa	\$2,992,114
Kansas	\$712,316
Louisiana	\$2,583,873
Maine	\$106,000
Maryland	\$4,132,375
Massachusetts	\$10,200,000
Michigan	\$5,515,300
Minnesota	\$2,301,271
Missouri	\$153,606
Nebraska	\$1,850,000
Nevada	\$1,252,693
New Hampshire	\$100,000
New Jersey	\$3,150,000
New Mexico	\$70,000
New York	\$5,557,398
North Carolina	\$1,000,000
North Dakota	\$360,000
Ohio	\$6,265,840
Oklahoma	\$1,000,000
Oregon	\$7,034,955
Pennsylvania	\$6,369,000
Rhode Island	\$549,617
South Carolina	\$100,000
South Dakota	\$244,000
Tennessee	\$200,000
Vermont	\$149,800
Virginia	\$55,000
Washington	\$730,000
West Virginia	\$1,453,840
Wisconsin	\$396,000
Wyoming	\$7,188
Median	\$1,250,000

Source: 2021 Survey of Problem Gambling Services in the United States

Finally, we use a regression of problem gambling services funding on a state’s estimated number of problem gamblers to predict Alaska’s problem gambling services spend. The best fit line shown

below has an intercept of about 600,000, suggesting baseline spend regardless of problem gamer population size is \$600,000. The slope of the line is approximately 14, indicating that for every one problem gambler, a state’s problem gambling budget increases by \$14 above the baseline \$600,000. At what we believe is a substantial overestimate of 5% of Alaskan adult population being problem gamblers (solely for the purposes of developing a conservative budget estimate), the regression would indicate a state spend of just under \$1 million on problem gambling mitigation.

Figure 3: Relationship between a State’s Estimated Number of Adult Problem Gamblers and Problem Gambling Service Funding



▶ Linear least squares regression of Funding vs Estimated Number of Problem Gamblers, p-value <.01. (3 outliers removed. N=39)

Source: 2021 Survey of Problem Gambling Services in the United States

According to the most recent data available, Alaska does not currently have any state funded problem gambling services. Therefore, we used the three approaches described above to calculate three estimates of the funds needed to support a problem gambling mitigation program in Alaska. This resulted in estimates of approximately \$1.17 million, \$1.25 million, and \$1 million, indicating that the state of Alaska would need to spend between roughly \$1 million and about \$1.25 million on problem gambling services.

More information on problem gambling research is contained in Appendix A.

COMPETITIVE EFFECTS ANALYSIS

Based on a previously conducted analysis completed by The Innovation Group, we identified one possible category of substitution effects would arise from development of the Eklutna project:

1. The casino's impact on charitable gaming in the region.

Impacts on Charitable Gaming

Alaska Charitable Gaming

Currently charitable gaming is the only legal form of gambling in Alaska. Despite several attempts to legalize casinos and lotteries, Alaska's gaming laws remain among the strictest in the country.

Dating back more than 30 years there has been attempts to legalize different forms of gaming and lotteries across Alaska. In the early days, as early as 1990, a referendum was voted down to legalize gaming and establish an Alaska Gambling Board by nearly a 2 to 1 vote. As time progressed into the late 1990's and early 2000's several other pieces of legislature were either passed to limit gaming in the state such as a 1995 ban on cruise ships operating casino gaming in Alaska waters or rejected to stop the introduction of gambling in the state such as the 2004 rejections of a proposal to establish a gaming commission or the 2005 and 2006 rejections of attempts to place an Alaska Video Lottery Law on the ballots.

A more recent examination of the gaming landscape in Alaska includes charitable gaming establishments that offer pull tabs (sing play card with breakopen tabs where a winning "card" displays a certain symbol or number), bingo, and raffles but there has been some recent push to expand gaming within the state but to no avail.

However, in the last couple years there has been measure looking to allow other forms of gambling such as electronic pull tabs and the addition of some type of gambling aboard Alaska Marine Highway system ships. House Bill 200 and Senate Bill 146 from 2024 both address the introduction of electronic pull tabs to the state while House Bill 197 address gambling on the state ferries.

Authorized in "AS 05.15 The Alaska Gaming Reform Act (Act) and 15 AAC 160 Authorized Games of Skill and Chance," charitable gaming gives "permittees" the opportunity to "generate net proceeds for political, educational, civic, public, charitable, patriotic, and religious uses in the state."

Permitted games include: "Bingo, raffles and lotteries, pull-tab games, authorized classics (for example, ice classics, cabbage classics), fish and moose derbies, Calcutta pools, and contests of skill." Pull-tabs account for approximately two-thirds of proceeds to charities, followed by raffle/lottery at 22% and bingo at 10%.

There are four categories of operations:

1. Permittee (Self-directed): A municipality or qualified organization that holds a permit to conduct gaming activities.
2. Multiple-Beneficiary Permittee (MBP): Two to six municipalities or qualified organizations that jointly obtain a permit in order to aggregate prize limits.
3. Vendor: A for-profit business that sells pull-tabs on behalf of a permittee, from a location licensed by the Alcoholic Beverage Control Board.
4. Operator: A for-profit business licensed to conduct gaming activities on behalf of a permittee.

Qualified organizations include “civic or service organizations, religious, charitable, fraternal, veterans, labor, political and education organizations, police and fire departments, associations and derbies, and nonprofit trade associations.”

Adjusted Gross Income (AGI) is the amount retained by the operation after prizes and federal excises taxes are deducted from Gross Sales. Net Proceeds is the amount retained by permittees after expenses and state taxes are deducted from AGI.

Please note The Innovation Group included 2019 as reference data as the most recent available data from the Alaska Department of Revenue – Tax Division and the Annual Charitable Gaming Report is from 2020 and has the corresponding COVID impact. 2019 is a more realistic representation of charitable gaming data under normal market conditions.

Table 59: Alaska Charitable Gaming 2019

	# of Licenses	Gross Sales	Federal Excise Tax	Cost of Prizes	Adjusted Gross Income (AGI)	Total Expenses	Net Proceeds
Permittees	1,101	\$149,711,684	\$165,580	\$109,967,009	\$39,579,096	\$18,893,603	\$20,685,492
MBPs	13	\$77,269,375	\$11,405	\$60,625,212	\$16,632,757	\$11,834,354	\$4,798,404
Vendors	165	\$40,732,412	\$17,115	\$32,439,528	\$8,275,769	\$4,055,567	\$4,220,202
Operators	24	\$117,512,615	\$174,116	\$89,321,211	\$28,017,287	\$21,100,511	\$6,916,776
Total	1,303	\$385,226,086	\$368,216	\$292,352,960	\$92,504,909	\$55,884,035	\$36,620,874

Source: Alaska Department of Revenue

The percentage of AGI allowed for expenses varies by and game type. Expenses may not exceed 70% of pull-tab AGI and 90% of bingo and raffle AGI. The expense ratio results also vary by category of operation, with Operators having the highest expense ratios. All (100%) Net Proceeds must be remitted to a permittee. Vendors only operate pull-tabs.

Table 60: Pull-Tab Proceeds 2019

	Gross Sales	Federal Excise Tax	Cost of Prizes	Adjusted Gross Income (AGI)	Total Expense %	Net Proceeds
MBP	\$52,292,715	\$11,405	\$42,488,298	\$9,793,012	65%	\$3,449,249
Operator	\$92,927,334	\$173,003	\$73,263,557	\$19,490,774	70%	\$5,876,503
Self-directed	\$116,863,920	\$156,084	\$92,523,851	\$24,183,986	56%	\$10,539,978
Vendor	\$40,732,412	\$17,115	\$32,439,528	\$8,275,769	49%	\$4,220,202
Total	\$302,816,381	\$357,607	\$240,715,234	\$61,743,541	61%	\$24,085,932

Source: Alaska Department of Revenue

Bingo accounts for 17% of sales but because of higher operating expenses only 10% of proceeds.

Table 61: Bingo Proceeds 2019

	Gross Sales	Federal Excise Tax	Cost of Prizes	Adjusted Gross Income (AGI)	Total Expense %	Net Proceeds
MBP	\$24,926,512	\$0	\$18,101,054	\$6,825,459	80%	\$1,345,927
Operator	\$22,352,232	\$0	\$14,721,032	\$7,631,200	88%	\$900,586
Self-directed	\$17,148,443	\$2,892	\$12,171,400	\$4,974,151	69%	\$1,527,109
Total	\$64,427,187	\$2,892	\$44,993,486	\$19,430,810	81%	\$3,773,622

Source: Alaska Department of Revenue

Raffles are dominated by Self-directed Permittees and have little overhead, meaning that nearly 85% of AGI is retained as Net Proceeds.

Table 62: Raffle Proceeds 2019

	Gross Sales	Federal Excise Tax	Cost of Prizes	Adjusted Gross Income (AGI)	Total Expense %	Net Proceeds
MBP	\$50,148	\$0	\$35,861	\$14,287	77%	\$3,228
Operator	\$1,050,244	\$1,113	\$539,087	\$510,044	81%	\$97,669
Self-directed	\$13,703,658	\$6,604	\$4,369,745	\$9,327,308	12%	\$8,214,956
Total	\$14,804,050	\$7,717	\$4,944,693	\$9,851,639	16%	\$8,315,853

Source: Alaska Department of Revenue

Annual win (or AGI) per adult is nearly \$170, which is significantly lower than casinos typically penetrate but very high compared to charitable gaming in other states, where other gambling options are more accessible.

Alaska Casino Gaming

Alaska does not offer any Class III gaming; however, there are two approved Class II Indian Gaming facilities that operate under provisions approved by the National Indian Gaming Commission (NIGC). They are Klawock Bingo in Klawock and M.I.C. Gaming Hall in Metlakatla.

They are located in the southeastern most portions of Alaska approximately 19 hours south of Juneau that includes both driving and a ferry ride. In short, these facilities are located multiple days travel away from the proposed Eklutna facility. Based on the analysis completed by The Innovation Group, we do not believe there will be any impact on these two existing facilities.

Charitable Gaming Impact

Charitable gaming, in the forms of pull-tabs, raffle/lottery, and bingo make up nearly 99% of all charitable gaming offered in Alaska. In 2019, the most recent available report not impacted by COVID, the charitable gaming adjusted gross income is estimated at over \$92 million.

In 2020, as reported by the Alaska Department of Revenue, charitable gaming revenue decreased to under \$68 million, a decrease of about 30%. However, as with other gaming jurisdictions around the county, we believe that charitable gaming revenue has rebounded and even surpassed that of pre pandemic levels.

The gravity model was initially calibrated using charitable gaming data from the Department of Revenue. As discussed previously in this section, Adjusted Gross Income (AGI, which is equivalent to gaming revenue in the gaming industry) in 2019 was \$92.5 million, a majority of which we estimate originated from the defined gravity model market area. The Innovation Group then estimated the amount of charitable gaming in each market area adjusted to account for population difference and average annual household income.

Based on Net Proceeds data by physical location (that is, where the spending on charitable gaming occurred as opposed to which Permittee the Net Proceed was distributed), we estimate that 64% of Net Proceeds was derived by spending at facilities in the gravity model area. However, the MBPs and Operators, which predominantly operate in the Anchorage-Wasilla/Mat-Su Valley region and Fairbanks region, have higher expense margins, which means that the AGI generated in the gravity model area is substantially higher than 64%.

As previously noted, The Innovation Group used a gravity model to assist in realizing the impact of the proposed Class II Eklutna casino on the existing pull tab market. Pull tabs are offered at a wide range of locations across the state, including dozens in the greater Anchorage area. As a result, we utilized several of the more prominent locations in the area as a proxy within the model to represent the greater Anchorage pull tab market. We first calibrated the model to map the estimated current Anchorage pull tab market, then added the proposed Eklutna facility to the model to assess the impact it would have on that market.

The locations of each permit as the holder of the permit and location of the pull tabs are somewhat undefined as described by the Alaska Department of Revenue; however, we estimated the greater anchorage revenue in relation to the rest of the state by analyzing population distribution then applying a premium given the tourist aspect of Anchorage relative to other parts of the state. Based on our estimation we believe between 70% and 80% of the pull tab revenue takes place in the greater Anchorage area. With that being said, due to the unclear nature of the exact pull tab locations and an understanding of their exact market dynamics, The Innovation Group assessed the impact on a state level with an understanding that a vast majority of the impact would take place in the greater Anchorage area.

However, we would like to note that the impact on organizations that host charitable gaming on one off or semi-frequent basis, such as fire stations, elderly homes, or fraternal organizations would be minimally impacted but the standalone pull tab facilities would shoulder much of the impact.

The following table quantifies the impacts on charitable gaming from the proposed Eklutna casino development.

Table 63: Impact on Charitable Gaming from Casino

	2019	2027	Impact
Adjusted Gross Income (MM)	\$92.5	\$112.7	-\$24.3
Growth/Impact	-	21.8%	-21.6%

Source: AK Dept of Revenue; The Innovation Group

APPENDIX A: PROBLEM GAMBLING MITIGATION

Definition and Prevalence

A majority of Americans, about 86%, report having gambled at least once in their lifetime¹⁴. Most people gamble for recreational purposes without the behavior becoming a problem. Studies, however, estimate that 0.4%-1.6% of the United States population can be classified as pathological gamblers.^{15,16} Pathological gambling has been commonly associated with relationship problems, employment issues, and significant financial difficulties.

The American Psychiatric Association (2004) defines a pathological gambler as a person who features a continuous loss of control over gambling. Furthermore, this gambler illustrates a progression, in gambling frequency and amounts wagered, in the preoccupation with gambling and in obtaining monies with which to gamble. However, problem gambling is a more loosely defined term and is commonly associated with gaming-related difficulties that are considered less serious than those of a pathological gambler. For the sake of this report, we will utilize the definition by noted researchers Cox, Rosenthal and Volberg which defines problem gambling as a pattern of gambling behavior that compromise, disrupt or damage personal, family or vocational pursuits.¹⁷

The National Research Council¹⁸ utilizes a three-level metric. Level 1 gambling is considered social and or recreational gambling with no appreciable harmful effects. Level 2 gambling is synonymous with problem gambling. Level 3 gambling is synonymous with pathological gambling. Problem gambling is an urge to gamble despite harmful negative consequences or a desire to stop. It is often defined by whether harm is experienced by the gambler or others, such as the gamblers family, significant other, spouse, friends, or coworkers. A problem gambler may or may not be a pathological gambler. Pathological or compulsive gambling is defined as a mental disorder characterized by a continuous or periodic loss of control over gambling, a preoccupation with gambling and with obtaining money with which to gamble, irrational thinking, and a continuation of the behavior despite adverse consequences.

Prevalence rates to determine adult problem gambling rates are measured by administering a survey (often a variation of the South Oaks Gambling Screen or a modified DSM-IV questionnaire) to a statistically valid sample of the adult population of the jurisdiction being

¹⁴ James KC, Bible WA, Dobson JC, Lanni JT, Leone RC, Loescher RW, et al. *National gambling impact study commission final report*. National Gambling Impact Study Commission. 1999.

¹⁵ Shaffer HJ, Hall MN, Vander Bilt J. "Estimating the prevalence of disordered gambling behavior in America and Canada: a research synthesis." *Am J Public Health*. 1999

¹⁶ Petry NM, Stinson FS, Grant BF. "Comorbidity of DSM-IV pathological gambling and other psychiatric disorders: results from the national epidemiologic survey on alcohol and related conditions." *J Clin Psychiatry*. 2005

¹⁷ Cox, S., H. R. Lesieur, R. J. Rosenthal & R. A. Volberg. 1997. *Problem and Pathological Gambling in America: The National Picture*. Columbia, MD: National Council on Problem Gambling.

¹⁸ National Research Council, pp. 20-21.

measured. Adolescent rates are measured in a similar manner. Such a method and analysis of data that accompanies the process is referred to as a general population prevalence study.

Jurisdictions, both domestically and internationally, have conducted studies to estimate the percentage of the population that could be classified as having some level of problem gambling behavior. These studies, commonly referred to as prevalence studies, are designed to reflect the scope and severity of problem gambling behavior.¹⁹

One of the most frequently cited studies on prevalence rates is *Estimating the Prevalence of Disordered Gambling Behavior in the United States and Canada: A Meta-analysis by the Harvard Medical School Division on Addictions*. The meta-analysis method of estimating prevalence rates has been used in related addiction fields of drug prevention and patterns of alcohol use and alcohol treatment. It is considered a more cost-effective method than a national study since it makes use of existing research already conducted in a field.

The Harvard Medical School study, believed to be the first to use meta-analysis measurements for problem gambling prevalence rates, analyzed 152 distinct previous prevalence studies available for review by June 15, 1997. The study determined that 2.0 percent of the adult population could be considered as Level 2 of disordered gambling (often referred to as problem gambling) and 0.9 percent of Level 3 or disordered gambling (also referred to as pathological gambling) during the past year. The vast majority of adults in the general population, then, do not experience gambling-related problems of any clinical significance.

The meta-analysis raw data was given to the Committee on the Social and Economic Impact of Pathological Gambling of the National Research Council (NRC) in its analysis for the National Gaming Impact Study Commission. After an extensive review, the NRC agreed with the above rates of problem gambling and used the numbers in its own analysis of problem gambling in its final report.

The introduction of casino gambling has the potential of negative social impacts. These potential impacts can be controlled and minimized through proper planning, awareness campaigns, and prevention and treatment programs applied in a coordinated manner by all relevant stakeholders. By utilizing some of the many proven prevention and treatment programs, the potential social impact of the advent of gaming can be minimized. Allocating funds to problem gambling services can help mitigate problem gambling and promote responsible gambling.

As an example, by devoting more resources to prevention and treatment, Connecticut was able to cut prevalence rates despite further gaming development. In 1996, Connecticut had only a single clinic, but by the time of an updated study in 2008, the state had 17 clinics.²⁰ Prevalence rates

¹⁹ *Estimating the Prevalence of Disordered Gambling Behavior in the United States and Canada: A Meta-analysis*, Harvard Medical School Division on Addictions, 1997.

²⁰ Spectrum Gaming Group, *Gambling in Connecticut: Analyzing the Economic and Social Impacts*, prepared for the State of Connecticut, Division of Special Revenue, June 2009.

declined substantially during that period, despite the opening of Mohegan Sun late in 1996 and further expansion at Foxwoods, including the opening of Grand Pequot Tower hotel in 1997.

Table 64: Connecticut Prevalence Rates

	2008 Survey	1997 Study
Problem Gamblers	0.90%	2.20%
Probable Pathological Gamblers	0.70%	0.60%
Total Disordered Gamblers	1.60%	2.80%

Source: Spectrum Gaming Group.

Responsible Gaming and Harm Minimization

Responsible gambling/gaming programs take several forms in an effort to combat and prevent gambling-related harms. Instances of problem gambling manifest in two categories of harm: (1) personal harm, including effects on health, well-being, and relationships, and/or (2) economic harm. Research on responsible gaming falls short of the levels of scientific analysis necessary to develop responsible gaming “best practices.” While various publications have attempted to synthesize existing research on common responsible gaming and harm minimization practices, the field of research often lacks peer-reviewed scientific analyses.

In their current form, the most common responsible gaming practices reflected in the field of research are self-exclusion programs, gambling help lines, tracking behavioral characteristics, setting gambling limits, providing responsible gaming-oriented game features, and employee training. Each of these strategies will be discussed below.

As a condition of licensing, commercial casino states may mandate that casinos prepare and submit for approval a wide-ranging plan for addressing responsible gaming issues. Required elements of the plan often include employee training and public awareness efforts along with other policies that various states have addressed specifically through standalone statutes, or regulations, that address only a single subject. The required elements of these plans vary by state.

In Maryland, for example, a responsible gambling program must consist of mechanisms that both mitigate the effects of problem gambling in the State and maximize the access of individuals with a gambling problem to problem gambling resources.²¹

Massachusetts makes the issuance of gaming licenses contingent upon the submission of a plan to “address lottery mitigation, compulsive gambling problems, workforce development and community development [,] and host and surrounding community impact and mitigation issues.”²² The State intends for these requirements to advance its objective of providing a gaming

²¹ Maryland responsible gaming plan statute. COMAR 36.01.03.07(B).

²² Massachusetts responsible gaming statute. M.G.L. Ch. 23K, § 15(6).

environment that is safe and productive for all stakeholders. In furtherance of this objective, Massachusetts prompts gaming licensees to develop plans that train employees to identify patrons exhibiting problems with gambling, and prevention programs for vulnerable populations.²³

Other states, such as Ohio, connect their responsible gaming plans to other mitigation mechanisms, such as voluntary exclusion programs, to better protect vulnerable groups.²⁴ Overall, the development of responsible gaming plans serves to establish concrete frameworks to better promote safe gaming.

Self-Exclusion Programs

Voluntary self-exclusion programs, typically operated by casinos and online gambling sites or gaming regulators, give individuals the ability to exclude themselves from gambling activities. Many states require that patrons have the ability to authorize a casino to refuse their right to gamble and to expel them if they are found gambling or, in some cases, otherwise found on the premises. Program management models vary; in some cases, they are run by the state or a state-appointed group, in others they are managed directly by licensees. State statutes vary in the length of the self-exclusion periods available – typically ranging from a six month ban to lifetime restriction – and in the procedures for reversing self-exclusion. In some states, third parties also have the ability to voluntarily exclude patrons exhibiting problem gambling behavior. Many state laws specify that, in addition to banning play, the casino must also eliminate direct promotional outreach to these individuals as well as exclude them from complimentary offerings (“comps”) or access to credit. Such programs illustrate efforts to mitigate the potential social harms of expanded gaming in a state, including mental health issues, relationship concerns, and financial and work problems resulting from problem gambling.²⁵ As one of the most investigated responsible gaming strategies, self-exclusion programs benefit from a robust body of research conducted around the world.

Generally, the research on the effectiveness of self-exclusion programs concludes that this method is a safe and, for some gamblers, effective form of intervention against problem gambling. As one study suggests, self-exclusion may have similar outcomes to counseling and may reduce harm in the short-term. Additional research has indicated that self-excluded persons also engage in treatment, self-help groups, or other forms of support experience more positive outcome than those who do not. This research suggests that self-exclusion programs that serve as a gateway to treatment are most successful for individuals harmed by problem gambling. Research has also indicated that problem gamblers appear to be more receptive to self-exclusion mitigation strategies when compared to self-led efforts to seek professional help.²⁶ Ultimately, self-exclusion has

²³ M.G.L., Ch. 23K, § 18(6)

²⁴ See e.g., Ohio Regulation 3772-12-06.

²⁵ Nerilee Hing, Barry Tolchard, Elaine Nuske & Louise Holdsworth, *A Process Evaluation of a Self-Exclusion Program: A Qualitative Investigation from the Perspective of Excluders and Non-Excluders*, 12 INTERNATIONAL JOURNAL OF MENTAL HEALTH AND ADDICTION 509, 510 (2014), [10.1007/s11469-014-9482-5](https://doi.org/10.1007/s11469-014-9482-5).

²⁶ Hing, *supra* note 5, at 510.

transitioned from a “punitive” enforcement model to one that aims to provide individual assistance in order to connect vulnerable persons with counseling and other support services.

The framework for self-exclusion programs varies from state to state, but many states mandate that patrons have the ability to refuse their right to gamble and to expel them from the premises.²⁷ In Kansas, for example, the voluntary exclusion statutes require that each self-exclusion applicant “refrain from visiting gaming facilities, pari-mutuel licensee locations, and fair association race meets.”²⁸ Kansas’ statutes also enable the gaming commission to “prohibit the applicant from entering the premises of all gaming facilities.”

Similarly, Massachusetts enables a person to be placed on a self-exclusion list by “acknowledging that the person is a problem gambler and by agreeing that, during any period of voluntary exclusion, the person shall not collect any winnings or recover any losses.”²⁹ Massachusetts also prohibits gaming establishments from marketing “to persons on any excluded persons list,” and requires gaming establishments to deny access to complimentary credits. Ultimately, Massachusetts identifies voluntary self-exclusion as “one means to help address problem gambling behavior or deter an individual with family, religious, or other personal concerns from entering . . . a gaming establishment.”³⁰

Various challenges interfere with the effectiveness of self-exclusion. First, the number of gambling facilities within a jurisdiction may make the enforcement of self-exclusion impractical; if alternative facilities can be easily accessed, the effectiveness of self-exclusion may be compromised. Notably, statutorily required training may not sufficiently prepare officials responsible for self-exclusion enforcement.³¹ The diversity of socioeconomic and psychological conditions among voluntary self-excluders may require responsive enforcement mechanisms. Furthermore, the need to apply for placement on a self-exclusion list within a gaming facility may compromise the integrity of the process, thereby deterring potential self-excluders from participating.

Individual compliance poses another well-documented challenge to the effectiveness of self-exclusion programs. For example, one study determined that more than half of the participants for whom self-exclusion was still in effect had returned to a casino or breached their contracts by the six-month follow-up interview. Additionally, a study of self-excluded individuals in Missouri found similar breaches, indicating that the benefits of the program were attributable more to the act of enrollment than to enforcement. This research has led to the frequent conclusion that responsibility for self-exclusion lies with both the gaming industry and the self-excluding individual.

²⁷ Regulatory Management Counselors, *Comparative Governance and Regulatory Structure of Gaming Regulations Related to Expanded Legalized Gaming Activities in the Commonwealth of Virginia* (Aug. 5, 2019), at 160 (hereinafter *Comparative Governance Report*).

²⁸ *Id.* at 161.

²⁹ *Id.* at 169.

³⁰ *Id.* at 171.

³¹ Hing, *supra* note 5, at 511.

In conclusion, voluntary self-exclusion programs may reduce the urge to gamble and increase the perception of control over personal behavior.³² While self-exclusion alone cannot substitute for dedicated treatment, it provides an external control mechanism that may limit problem gambling and encourage voluntary excluders to seek professional help.

Tracking Behavioral Characteristics

In an effort to predict the likelihood that a patron will experience harm from gambling and to introduce preventative interventions before the onset of such problems, gaming jurisdictions have implemented systems to track player behavioral characteristics. These behavioral tracking systems are based on algorithms of play. Implementation strategies vary with the form of gaming: whereas in online gaming environments tracking procedures benefit from access to all player transaction information, in brick-and-mortar environments, the strategy is often designed around player tracking systems (e.g., Players Clubs) that depend upon an individual patron's participation.

Research on the effectiveness of tracking frameworks has produced informative findings. Based on analysis of player habits, studies have suggested that efforts to promote responsible gaming should be tailored to each type of gambling offered at a gaming location, rather than adhering to a general mitigation program. By studying behaviors and thoughts patrons use to control the amount they gamble, such as attempts to set a budget or to seek help, research has identified characteristics that could be used to develop prevention and early intervention programs for problem gamblers. Research dedicated to tracking the behavioral characteristics of online gamblers has determined that patrons who engaged in more than two types of gambling within their first month of play, with high variability of wagers, were more likely to benefit from responsible gaming programs.

The study of behavioral characteristics remains a highly-variable task. Given the limitations inherent in the use of personalized player data, there remains a lack of definitive evidence of any behavioral algorithm that can accurately predict patterns of gambling disorder.

Setting Gambling Limits

The ability to set gambling limits, a process also known as pre-commitment, allows gamblers to predetermine the amount of time or money they are permitted to devote to gambling activities before play begins. Depending on the gaming venue or website, spending limits can include deposit, play, loss, win, bet, and time limits.

Research on the effectiveness of pre-determined gambling limits has demonstrated mixed outcomes and has illustrated positive and negative results of this mitigation technique. Studies have indicated that requiring individuals to set such limits may reduce overall money spent on gambling, but evidence is still lacking to suggest that this spending reduction occurred in individuals who were experiencing gambling-related harms, or that gambling-related harm was reduced. Furthermore, research has indicated that voluntary money limit setting was more effective

³² Robert Ladouceur, Caroline Sylvain & Patrick Gosselin, *Self-Exclusion Program: A Longitudinal Evaluation Study*, 23 J. GAMBLING STUDIES 85, 85 (2007), [10.1007/s10899-006-9032-6](https://doi.org/10.1007/s10899-006-9032-6).

than time limits in reducing problem gambling behavior. While self-limiting has been found to reduce the variety of games played and the number of bets placed, gambling limits have not been found to reduce the amount wagered per bet. Additionally, research has indicated that pre-commitment may have little effect on decreasing gambling expenditures, especially among those who are intent on continued gambling and who are likely to find methods of circumventing gambling limits.

Finally, the emergence of GameSense, a program that employs in-house responsible gaming information centers or advisors, and other limit-setting programs like PlayMyWay, signal that the future direction of gambling mitigation plans is likely to employ gambling limits. Further research will be required to produce evidence that supports the effectiveness of pre-commitment initiatives.

Responsible Gaming-Oriented Game Features

This harm minimization technique involves the modification to the structure or operation of games to assist patrons in making informed choices about their gambling activity, and to encourage responsible gaming behavior. While research on this mitigation strategy is often focused on the use of warning messages, select studies have explored the use of additional modifications, such as slowing down the rate of play, posting clocks around gambling facilities, and offering “play money” modes.

A threshold study evaluating the effectiveness of five game features (messages, bank meters, clocks, demo mode, and charity donations) found that most participants were aware of at least one feature, but that only a small portion actually utilized the features. Further research concluded that, when compared to warning messages that appear on the periphery of a screen, messages that appear in the middle of a screen are more frequently recalled and considered more useful. Patrons in one study also identified a cash display as helpful to controlling gambling activities.

The research on responsible gaming-oriented game features has provided varying insights on the effectiveness of such features. While evidence confirming the efficacy of responsible game features is mixed, little research has shown that game features reduce gambling-related harm in a real-world setting.

Employee Training

Training of gaming facility employees in responsible gaming is a nearly universal practice. Some states require that this training include instruction on the complex question of how to identify problem gamblers on the gaming floor. Other states provide for in-depth education on the nature and symptoms of problem gambling.³³ With this training, employees of gambling facilities can better serve patrons who may be identified as problem gamblers by providing information about problem gambling programs. Delaware, for example, requires that the rules for state lottery games provide “procedures for the display and presentation of messages concerning responsible gaming and the regulations, procedures and training for identification of and assistance to compulsive gamblers.”³⁴

While few studies exist that explore the effectiveness of employee training programs, research has determined that there is considerable disparity in employee ability to accurately identify problem gambling behavior among patrons. Studies indicate that employee training can improve employee knowledge of responsible gambling, however, there is limited evidence that this enhanced understanding enables employees to more accurately identify patrons with a gambling disorder.

Additional obstacles to the effectiveness of employee training are found in the difficulty, awkwardness, and uncertainty present in the act of confronting a patron. Studies have indicated that gaming facility employees often experience difficulty when approaching patrons due to uncertain estimations of a patron’s potential problems or in an attempt to avoid causing a patron embarrassment.

Ultimately, the spectrum of harm from problem gambling manifests differently from state to state. As a result, the role of employee training may vary with the extent of a state’s understanding of the gambling problems its residents face.

Public Health

By understanding gambling and its potential impacts on public health, policymakers and health practitioners alike can work to minimize gambling’s negative impacts, while promoting its potential benefits. Today, public health perspectives are not limited to the biological and behavioral dimensions of gambling. Rather, a contemporary public health perspective can also target the social and economic determinants of gambling, such as income, employment, and poverty. Four principles have emerged as the basis for a public health framework on gambling: (1) scientific research is the foundation of public health knowledge, (2) public health knowledge is derived from population-based observations, (3) health initiatives are proactive (i.e., health promotion and prevention are primary, while treatment is secondary), and (4) public health is balanced and considers both the costs and benefits of gambling. This framework can stimulate a

³³ Mississippi employee training: MGC Regs. Title 13, Part 3, Rule 10.6

³⁴ Delaware employee training: 19 Del. C. § 4805(a)(29).

better understanding of gambling, further elucidate the determinants of problem gambling, and indicate a range of intervention strategies.

Throughout the past decade, publicly-funded problem gambling services have received increased support in the United States. The total number of states that reported publicly-funded problem gambling services increased from 37 in 2010 to 40 in 2016, and the total amount of public funding allocated to problem gambling services increased from \$60.6 million in 2013 to \$73.0 million in 2016. Among the states that provided funding, the most commonly supported services were problem gambling awareness programs, counselor training, helplines, and problem gambling treatment. Despite the continued growth of problem gambling efforts throughout the United States, in 2016, about one quarter of one percent of people who needed problem gambling treatment received publicly-funded care from a gambling treatment specialist.

Public Education and Informed Choice

Across gaming jurisdictions worldwide, governments and gaming providers have recognized the importance of providing patrons sufficient information to make informed decisions about their gambling. While individuals retain the ultimate responsibility over their gambling choices and level of participation, optimal decision-making depends significantly on the availability of reliable and comprehensive information. This concept of the “informed decision” is pervasive in systems of law and economics and remains an essential component of effective problem gambling mitigation efforts.

Several environmental factors may influence gambling behavior simultaneously, making it difficult to determine the local impact of any one factor. Advertising to promote problem gambling awareness, for example, has attempted to influence gambling behavior and reduce gambling-related harm. Various studies have concluded that the impact of advertising is not likely to be overt, and it may be difficult to measure the impact of advertising efforts to promote problem gambling awareness.

States may require that casinos post signs and/or offer brochures identifying the risks of gambling, signs of gambling disorder, the odds of casino games and/or toll-free phone numbers and other resources for assistance. Common practices among the states include requirements that gambling facilities ensure their advertisements display problem gambling help-line phone numbers. Additionally, some states, like Maryland, require that radio, television, and video advertisements contain a gambling assistance message.³⁵

Some states provide regulations that specifically address risk-related advertisements for internet and mobile gaming. Delaware, for example, mandates that internet lottery websites include advertisements for and links to information for treatment, education, and assistance of compulsive

³⁵ Maryland advertising requirements. COMAR 36.03.06.03(B)(5).

gamblers and their families.³⁶ Similarly, West Virginia requires online sportsbooks and mobile gambling applications to display links to responsible gaming resources.³⁷

Gaming jurisdictions have acknowledged that different messaging approaches may work better for different groups. One Canadian study prospectively detailed the most effective messaging approach for different styles of gaming. For casual gamblers (new and occasional gamblers), programs that enhance gambling literacy, including key safeguards and main risk factors, are essential. Frequent gamblers (i.e., those that gamble at least once per month, but not weekly) need a deeper understanding of how gambling works, including information on house edge, randomness, and independence of events. Finally, the study concluded that intensive gamblers (i.e., those who gamble weekly or more often) need to be informed of their play activity, offered self-assessment tools that draw attention to the consequences of their gaming habits, and made aware of the options available for help in addressing gambling-related problems.

Additional Mitigation Strategies

In addition to the main mitigation techniques discussed above, various jurisdictions also employ additional strategies to promote healthy gambling practices. These strategies include restrictions on alcohol, treatment and research funding, and casino credit restrictions along with bet limits.

Restrictions on Alcohol

Several states require casinos to limit alcoholic beverage service on the gaming floor, or to limit access to gambling services for patrons who are visibly intoxicated. The extent of restrictions on the sale of alcoholic beverages varies across different states. Some states, like Michigan and Kansas do not impose any restriction on alcohol service in gaming facilities. Other states, however, like Massachusetts and Maryland limit the time and place of alcohol sales.

Many states that restrict alcohol service mandate that gambling facilities refuse to sell or serve alcohol to patrons that appear intoxicated, or are younger than 21-years old.³⁸ Maryland, for example, requires that video lottery licensees prevent intoxicated individuals from playing video lottery or table games and prohibit intoxicated individuals from entering areas where such games are located. Maryland further restricts alcohol service by prohibiting licensed operators from providing complimentary alcoholic beverages.³⁹

As a further restriction on alcohol service in gambling facilities, Massachusetts requires gambling facilities to obtain a gaming beverage license in order to serve alcohol on the premises of such a facility.⁴⁰ The sale of alcohol must adhere to the conditions of the issued gaming beverage license, which may be imposed on such license “in the interest of the integrity of gaming and/or public

³⁶ Delaware advertising requirements. 29 Del. C. § 4826.

³⁷ West Virginia advertising requirements. WV CSR § 179-9-13.4.

³⁸ See e.g., 4 Del. C § 706; Md. Code Ann., State Govt. Law, § 9-1A-24(c)(1); 205 CMR 136.02.

³⁹ COMAR 36.03.10.09(A)(2)

⁴⁰ M.G.L. Ch. 23K, § 26.

health, welfare, or safety.”⁴¹ Massachusetts further requires that gaming licensees promulgate a system of internal controls to monitor the sale of alcohol. At minimum, such a system must include procedures to (1) ensure proper training of employees involved in the service of alcoholic beverages, (2) prevent serving alcoholic beverages to underage or visibly intoxicated individuals, (3) ensure that visibly intoxicated or impaired patrons are not permitted to play slot machines or table games, and (4) ensure that alcohol is properly secured and stored.⁴² In addition, Massachusetts prohibits the sale of alcohol between 2:00AM and 4:00AM to patrons who are not in the gaming area and not actively engaged in gambling.⁴³

Restrictions on the sale of alcohol play a significant role in the gambling regulations of several states. While the extent of such restrictions may vary, the motivation to promote public health and welfare remains widely relevant.

Treatment and Research Funding

States may implement financial commitments to support treatment for problem gamblers, education services concerning problem gambling, and research to advance responsible gaming and prevent problem gambling. Most states that implement such commitments earmark certain state revenues from gaming for these programs.

Pursuant to advancing public health efforts, Massachusetts assesses an annual fee in proportion to the number of gaming positions at each gaming establishment. This fee is meant to cover the costs of public health services and programs dedicated to addressing problems associated with compulsive gambling.⁴⁴ Monies within the Fund may be expended to assist social service programs that address gambling prevention, substance abuse services, and educational campaigns to mitigate the potential addictive nature of gambling.⁴⁵ Massachusetts also imposes upon each gaming licensee a requirement to provide on-site space for independent substance abuse, compulsive gambling, and mental health counseling services.⁴⁶

Efforts in other states pursue a more targeted approach, focusing treatment funding specifically on problem gambling, rather than on addictive behavior in general. Kansas, for example, established the Problem Gambling and Addictions Grant Fund to provide assistance for the treatment of “persons diagnosed as suffering from pathological gambling.”⁴⁷

The scope of research efforts varies from state to state. Massachusetts has established an annual research agenda to study the social and economic effects of gaming in the State and to obtain

⁴¹ 205 CMR 136.02.

⁴² Massachusetts alcohol service restriction: 205 CMR 138.12.

⁴³ Massachusetts alcohol service restriction: 205 CMR 136.07(7)(i).

⁴⁴ Massachusetts research statutes. M.G.L., Ch. 23K, § 56(e).

⁴⁵ M.G.L., Ch. 23K, § 58.

⁴⁶ M.G.L., Ch. 23K, § 21.

⁴⁷ Kansas problem gambling treatment statutes. K.S.A. §79-4805(c)(1).

scientific information relative to neuroscience, psychology, sociology, epidemiology, and etiology of gambling.⁴⁸ Similarly, Michigan reserves a significant portion of the monies within its Compulsive Gambling Prevention Fund for, among other things, “research, and evaluation of pathological gamblers and their families.”⁴⁹

The majority of states have implemented treatment and research funding provisions to make gaming as healthy for participating individuals, and the environment around them, as possible.

Casino Credit Restrictions and Bet Limits

Some state laws aim to protect patrons from betting more than they can afford to lose by banning casinos from offering credit advances and limiting bet amounts. Methods to limit credit advances include both patron-driven efforts, such as voluntarily placing one’s name on a credit exclusion list, and facility efforts, including policies and procedures that limit those patrons to whom a gambling facility may issue credit.

Generally, the procedures established by states aim to ensure that a gaming facility does not extend credit to patrons beyond an amount that those patrons lack a reasonable ability to repay. Regulations may range from broad mandates to gaming operators to exercise caution and good judgment in extending credit⁵⁰, to more specific rules that identify groups to whom credit should be limited. As an example of targeted restrictions, Massachusetts requires that a gaming licensee’s policies prevent the extension of credit to patrons who self-identify as problem gamblers, place themselves on a voluntary credit suspension list, or are on public assistance.⁵¹

While the use of credit restrictions as a mitigation tool may vary across states, the desired effect of such restrictions and limitations remains similar. The promotion of safe gambling habits through credit restrictions and bet limits emerges as a primary goal of many states.

⁴⁸ Massachusetts research statutes. M.G.L., 23K, § 71.

⁴⁹ Michigan problem gambling research statutes. MCL 432.253.

⁵⁰ Delaware credit restrictions. 10 Del. Admin. Code 204-6.1.10.

⁵¹ Massachusetts credit restrictions. 205 CMR 138.43(1)(d).

APPENDIX B: CASINOS AND CRIME

The social and community impacts of gaming development have been extensively studied. In many areas research findings have been inconclusive and thus considerable resources continue to be devoted to researching possible negative impacts given the unique nature of gaming compared to other commercial enterprises.

A number of broad studies of the social and economic impact of casinos have been conducted in the United States. In the late 1990s, prompted by the expansion of casinos throughout the United States, mainly in the form of riverboat casinos, Native American casinos, and racetrack slot parlors, Congress set up the National Gambling Impact Study Commission (NGISC). Its findings were released in 1999.

The Commission retained the National Research Council (NRC) to review the existing research on the socio-economic impacts of casino development. The NRC concluded that the existing research on the subject was inadequate:

The NRC project involved a review of all existing and relevant studies by representatives of a variety of scientific fields. In the end, NRC recommended that further study be initiated. Study of the benefits and costs of gambling “is still in its infancy.” Lamenting past studies that utilized “methods so inadequate as to invalidate their conclusions,” the absence of “systematic data,” the substitution of “assumptions for the missing data,” the lack of testing of assumptions, “haphazard” applications of estimations in one study by another, the lack of clear identification of the costs and benefits to be studied, and many other problems, NRC concluded the situation demands a “need for more objective and extensive analysis of the economic impact that gambling has on the economy.”⁵²

The Commission then retained the National Opinion Research Center (NORC) to undertake said “objective and extensive analysis” concerning impacts. The NORC came to the following conclusion:

First, the casino effect is not statistically significant for any of the bankruptcy or crime outcome measures..... This is not to say that there is no casino-related crime or the like; rather, these effects are either small enough as not to be noticeable in the general wash of the statistics, or whatever problems that are created along these lines when a casino is built may be countered by other effects.⁵³

Despite the NGISC’s authoritative findings, some researchers continue to claim that casinos cause crime.⁵⁴ However, there are three major flaws in much of this research:

⁵² National Gambling Impact Study, Chapter 7. 1999. Gambling’s Impact on People and Places.

⁵³ The National Gambling Impact Study Commission, “National Gambling Impact Study” (1999).

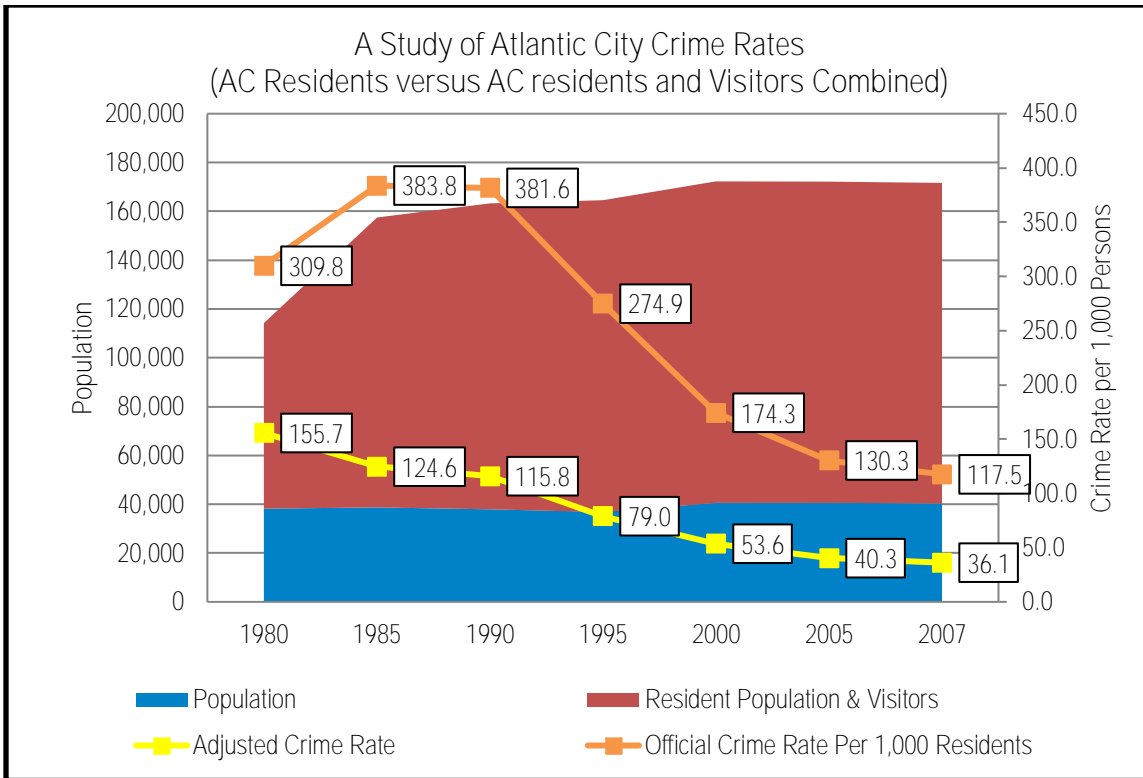
⁵⁴ See Grinols and NBER discussion below.

1. Much of the research that attributes an increase in crime to casinos has ignored the temporary population increases brought about by casino visitation. When crime rates are calculated not accounting for the influx of visitors, there appears to be an increase in crime. While this may be true in absolute terms, it radically overestimates the increase in likelihood of residents being victims of crime.
2. Further to #1, some research applies crimes such as on-site thefts of casino visitors to the local population, leading to an invalid increase in the local crime rate.
3. The crimes rates are not studied over a sufficient period of time and therefore temporary increases or long term trends attributable to more primary causal factors are not always recognized or are misinterpreted.

One of the earliest examples of flawed research is related to Atlantic City. The *number* of crimes tripled after casinos opened in 1978, and some researchers applied the increase to the local resident population, which in the resulting invalid calculation resulted in a tripling of the crime *rate*. However, most of the increase related to thefts within the casinos, which did not impact the local population. A valid calculation of the crime rate has to include the visitation base.

In fact, there has been a *decreased* chance of being a victim of crime since casinos were developed in Atlantic City. Factors likely include an increase in casino employment and law enforcement resources, safer infrastructure with well-lit garages, and an increase in general tourism activity. According to more recent data supplemented to the study completed by Margolis et al,⁵⁵ this decline in crime rates per 1,000 residents continued through 2007 to a rate of 36.1 per thousand residents. The chart below illustrates the crime rate trends from 1980 to 2007.

⁵⁵ Margolis, J. & Altheimer & Gray. (December 1997). "Casinos and crime: An analysis of the evidence." American Gaming Association. <http://www.americangaming.org/assets/files/studies/Crime.pdf> . The Innovation Group.



The Rappaport Institute for Greater Boston and the John F. Kennedy School of Economics at Harvard University (Baxandall and Sacerdote 2005) in a national, county-level study of Native American casinos found a slight decrease in crime rates after casinos opened. The analysis included all California casinos in existence in the 1990s. From their total sample of 156 casino counties, the Rappaport study isolated out 57 counties with large casinos and relatively low population and nine counties with both large casinos and large populations to see if there were statistical differences in terms of community impacts. The following table shows their results:

Table 65: Rappaport Study Results

	All Casino- Counties ¹	Counties with Large- Capacity Casinos ²	Populous Casino Counties ³
Population Growth (%)	+5*	8.6	+8.1*
Total Employment (%)	+6.7*	+14.9*	5.7
Unemployment (%)	-0.3	-1.2*	0.5
House Prices	\$5,869	\$8,924	\$7,083
Crime (Per 1,000 People)	-3	-6	-1

*Statistically significant results at 99% confidence interval.

1. Reports how adjusted outcomes in 156 counties that introduced Indian-run casinos during the 1990s differed from the other 2,959 that did not.

2. The effect for 21 counties in the top 10th percentile in terms of number of slot machines (over 1,760).

3. The effect for the 57 casino counties in the top population quartile (over 55,000 residents).

The Rappaport study concluded:

Our analysis shows that while total crime can be expected to increase when casinos open, the increase is due to increased population, not to a casino-created crime wave. Looking at FBI indexed crimes per resident in all [156] counties; we find that introducing a casino is associated with a decrease of 3 reported crimes per 1,000 people. The introduction of a casino, however, had no statistically significant effect on per-capita crime rates in either large-population casino counties or in large-casino counties. The per-capita crime rate in the 9 large-population counties that also hosted large-capacity casinos dropped 9 crimes per 1,000 residents, however.⁵⁶

It is worth noting that the study included two of the largest casinos in the world, Foxwoods and Mohegan Sun. In Ledyard, Connecticut (which hosts the Foxwoods casino), crimes outside the casino increased from 214 in 1991 to 364 in 1998, but in subsequent years, State Police data show that off-casino crimes in Ledyard fell below pre-casino levels. In Montville, Connecticut (host to Mohegan Sun), as with Ledyard, the number of crimes reported “remained relatively constant,” which the authors conclude is “surprising since the sheer increase in activity around these towns might have led to greater crime.”⁵⁷

The study also highlighted results for three counties in southern California: Riverside, San Bernardino, and San Diego. In all three counties, crime decreased relative to the state average. For example, before casino development, Riverside County suffered 22 more crimes per 1,000 residents than the state average. After casino development, the county had just 6 more crimes per 1,000 residents than the state average, a relative decrease of 16 crimes per thousand residents. San Bernardino had a relative decrease of 10 crimes per thousand, and San Diego 9.

Table 66: Rappaport Study California County Results for Crime

	Relative Crime (Before)	Relative Crime (After)	Change in Relative Crime (After - Before)
Riverside, CA	0.022	0.006	-0.016
San Bernardino, CA	0.016	0.006	-0.01
San Diego, CA	0.008	-0.001	-0.009

⁵⁶ IBID. As summarized in their 2008 report, “Betting on the Future: The Economic Impact of Legalized Gambling.”

⁵⁷ Baxandall, P. & B. Sacerdote (January 2005). *The Casino Gamble in Massachusetts: Full Report and Appendices*. Rappaport Institute for Greater Boston, John F. Kennedy School of Economics, Harvard University. Page 14.

In other western jurisdictions, the Montana legislature in 1997 commissioned a study on the video gaming industry. The resulting analysis found no impact on crime rates in Montana:

While gambling may have caused an increase of certain types of crime, Montana's overall crime rate increase is not any higher than the increases in matched cities with little or no legal gambling. In fact, in almost three-quarters of the specific comparisons carried out, crime rates rose more (or decreased less) in the matched cities than in the Montana cities.

Each of the seven largest Montana cities was matched with an out-of-state city in the region with similar population size, similar population growth rate, similar racial composition, but with little or no legal gambling. The percentage change in crime rates for three indices of crime (total serious crime, property crime, and violent crime) was computed for three time periods... between 1984 and 1994. [The data] illustrate the lack of a systematic pattern in crime rate changes between Montana cities and those in states with little or no gambling. For example, the violent crime rate grew faster in Cheyenne, Wyo., than in Great Falls between 1984 and 1994, yet the index of property crime decreased in Cheyenne while it increased in Great Falls during the same period.⁵⁸

In summary, there is no evidence from gross level data that the advent of casinos has a measurable impact on local crime rates in general, whether in Eastern, Midwestern, or Western jurisdictions. It is highly likely any crimes associated with casinos are either offset by economic benefits or that the level of crime is so small as to be overwhelmed by other factors such as economic trends.

Primary Research from Select Casino Jurisdictions

The figures from the casinos used in the Comparative Analysis Criminal Incidents section, provide a general picture of criminal activity at a casino. Other communities have found lower and higher levels of incidents. For example, figures from the Kenner Police Department note an average of 9 criminal incidents at the Treasure Chest Casino from 2012 to 2014. Attendance at the Treasure Chest Casino in Kenner is over one million annually.

A recent article in The Enterprise provided additional qualitative data from the casinos in this analysis. An officer from the Pittsburgh Police department compared the number of calls to games at the local baseball and football stadiums, "Nothing different than when there's a ball game," Luczak said. "I wouldn't say there's much change."⁵⁹

Des Plaines Police Deputy Chief Nick Treantafeles had similar sentiments, "It's just like any place that serves alcohol," he said. "You get drunk and disorderly, but their security handles 98 percent of the issues there. We might get called for a fight that gets out of hand. ... It hasn't put a damper on the services we offer the rest of the community."⁶⁰

⁵⁸ Montana Gambling Commission Study, 1998, Chapter 8.

⁵⁹ <http://www.enterpriseneews.com/article/20150517/NEWS/150516955/12741/NEWS/?Start=1>

⁶⁰ <http://www.enterpriseneews.com/article/20150517/NEWS/150516955/12741/NEWS/?Start=1>

While specific increase in police staffing varies from community to community, many communities found no need to increase police staffing, as shown below in the examples from Indiana. The Center for Urban Policy and the Environment at Indiana University-Purdue University has prepared 5-year evaluations of riverboat licensees for the Indiana Gaming Commission which contain sections on community impacts. The following bullet points include summaries and excerpts from these reports with respect to police and fire protection.

Casino Aztar:

- The Evansville Police Department reports no increases in crime since the riverboat opening. They do report a drop in crime in 1999 when compared to the previous year.
- “No new police officers or firefighters were added. Traffic control has not been a problem...”

Majestic Star:

- The community purchased 12 police cars with Year 1 incentive payments.
- Gary’s Chief of Police reports no additional criminal activity surrounding the riverboat.

Horseshoe Hammond (formerly Empress Casino Hammond):

- The Hammond Police Department reports crime has fallen in most categories when compared to before the boat opened.

Hollywood (formerly Argosy):

- According to the Lawrenceburg Police Department, casino-related arrests for public intoxication, DWI, and minor theft, as well as traffic accidents in the area have increased slightly each year from 1997 to 2000.
- Lawrenceburg has added two police officers since the boat opened to deal with the increased caseload.

Ameristar (formerly Harrah’s East Chicago):

- According to East Chicago’s police department, no additional criminal activity can be attributed to the riverboat’s presence.
- “Crime in East Chicago has decreased substantially over this time period due to increased cooperation with federal agencies, community policing and increased staffing.”

Blue Chip Casino:

- According to Michigan City’s chief of police, no additional criminal activity can be attributed to Blue Chip’s presence.

On the issue of crime, Jeremy Margolis, who had served as Assistant U.S. Attorney in Chicago, Illinois Inspector General, and Director of the Illinois State Police, found in a 1997 study⁶¹ that the chance of being victim of a crime decreases after casino development. Factors include an increase in employment brought by casinos, increased law enforcement resources, safer infrastructure with well-lit garages, and an increase in general tourism activity.

In testimony before the Pennsylvania Gaming Control Board (PGCB) in 2006, Margolis was asked to give an update of his seminal study. Margolis concluded, based on examining updated crime data from the F.B.I. as well as interviews with the Executive Director of the Illinois Crime Commission, the Illinois State Police, and the Illinois Gaming Board, that the situation is “really unchanged except for the maturation of the industry, the maturation of the regulatory process has probably settled things down more than it had settled when I completed my study in 1997. It’s just not an issue.”⁶²

⁶¹ Margolis, J. (December 1997). “Casinos and crime: An analysis of the evidence.” American Gaming Association.

⁶² PGCG hearing transcript, September 7, 2006, pages 22-23.

APPENDIX C: IMPACT ON LOCAL BUSINESS

Research Results

There is a substantial body of research and case studies demonstrating the impacts that casinos have on surrounding local businesses. There are several important reasons that local businesses benefit from the development of a casino:

- Casino visitors stopping at local retail outlets and restaurants.
- Long-distance patrons staying at area hotels; even in markets with casino hotels, non-casino hotels enjoy boosts in occupancy.
- Casino expenditures on local goods and services put more money into the local economy.

A review of studies of casino impacts on local business shows that casinos can stimulate local economies, resulting in communitywide growth, including in the local food and beverage business and retail businesses. There is little evidence of significant economic substitution after the introduction of new casinos, particularly for casinos in urban areas.

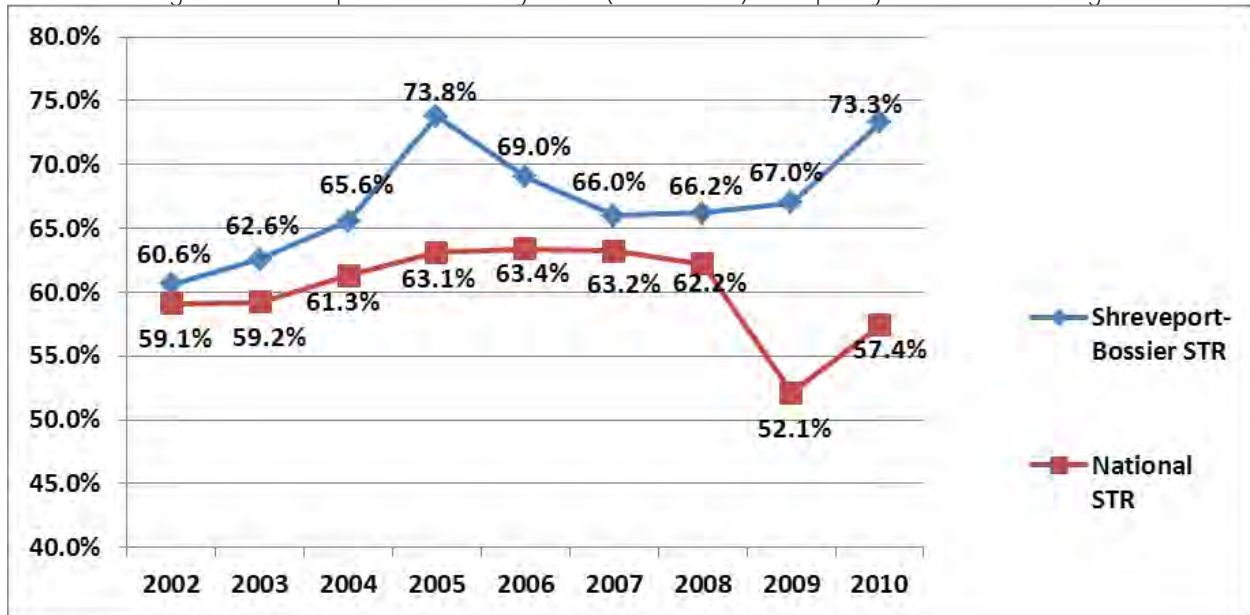
Primary Research

Casino development increases room demand at non-casino hotels even when casino hotels are built. For example, in Shreveport-Bossier City, Louisiana, hotel occupancy rates averaged approximately 60% before casinos were developed beginning in 1994, which is a standard level of occupancy for a small city market without casinos. The Shreveport-Bossier City casino industry was fully developed by 2003 with six casinos featuring 9,500 gaming positions, by which time hotel occupancy in non-casino hotels had risen to 63%, climbing to 74% by 2005.⁶³

The casino industry also helped non-casino hotels in Shreveport-Bossier City weather the impacts of the Great Recession much better than the national hotel market, with hotel occupancy dropping to no lower than 66% compared to the national trough of 52%.

⁶³ Shreveport-Bossier Convention and Tourism Bureau 2011 Lodging Report.

Figure 4: Shreveport-Bossier City Hotel (non-casino) Occupancy v. National Average



Source: Shreveport-Bossier Convention and Tourism Bureau 2011 Lodging Report; Smith Travel Research

Such a boost to non-casino hotel demand results from the overall increased visitation to the area and the overflow from peak periods when casino hotels are fully booked. On the Mississippi Gulf Coast, gaming began in late 1992 and by the early 2000's there were 11 casinos, all but one of which had associated hotels. During this period, occupancy rates in non-casino hotels remained steady at 55% despite a 143% increase in total rooms, including a 60% increase in non-casino hotel rooms.⁶⁴

The overflow effect has been experienced in numerous jurisdictions beyond the Mississippi and Shreveport examples presented above. In fact, third-party developers frequently build new hotels in the vicinity of a casino to take advantage of that overflow, even in remote areas with no other organic sources of demand. For example, an Americas Best Value Inn, a Best Western and a Days Inn were developed next to the Coshatta Casino Resort in Kinder, Louisiana even though the remote casino property has over 950 rooms of its own.

Gaming development on the Mississippi Gulf Coast also boosted retail and restaurant development by local business owners taking advantage of the increased visitation to the area. As the following table shows, between 1991 and 1997 the number of retail and eating and drinking establishments increased in the two counties that host casinos. The increases were an astounding 77 percent for drinking and dining establishments in Hancock County while retail establishments in both counties also increased over this period.

⁶⁴ Source: Mississippi Gaming Commission.

Table 67: Change in Retail and Eating and Drinking Establishments 1991-1997

County	Retail	Eating and drinking places
Harrison County, MS	14.7%	4.2%
Hancock County, MS	6.6%	77.1%

Source: US Census County Business Patterns

On the West Coast, three separate data sources indicate that substantial hotel development at tribal casinos on the outskirts of San Diego (and not within the City) has not negatively impacted local hotels. The Transient Occupancy Tax (TOT) collected by the City of San Diego has grown substantially since recovering from the 2009-10 recession effects, despite the operation of several large rural casino hotels, including an 1,100-room hotel at Harrah’s Rincon, that do not pay the TOT.

Table 68: City of San Diego, CA Transient Occupancy Tax Collections (MMs)

2008	2009	2010	2011	2012	2013	2014	2015	2016	2017
\$160.24	\$136.32	\$128.11	\$139.77	\$150.82	\$157.03	\$170.17	\$186.24	\$202.80	\$221.10

Source: San Diego Tourism Authority

While some of the TOT increase may be related to collection of the TOT at AirBnBs beginning in 2015, hotel occupancy data—which do not include AirBnBs—also increased in the three years from 2014 to 2016. HVS reports that hotel demand and occupancy in the San Diego market “increased steadily from 2010 through 2018, resulting in peak occupancy levels above 78% by year-end 2018.” RevPAR (revenue per available room, a measure of hotel performance) has also seen steady increases since the recession, rising above \$130 for the first time in 2018.⁶⁵

This HVS report is corroborated by STR data for the San Diego market, data that excludes casino properties, as shown in the following table.

⁶⁵ http://hvi.hvs.com/market/united-states/San_Diego; accessed June 2, 2022.

Table 69: San Diego pre-Covid Hotel Trends

	Occupancy %	RevPAR \$	Room Revenue (\$MMs)
2012	73.3	100.27	1,377
2013	74.0	103.22	1,414
2014	76.6	112.00	1,534
2015	78.5	122.23	1,674
2016	79.3	126.62	1,768
2017	79.2	130.47	1,844
2018	80.9	138.44	1,951
2019	78.7	134.84	1,937

Source: STR; The Innovation Group

Secondary Research

Casino development sometimes elicits concern, which research has dispelled, that substitution of consumer spending (the substitution effect) will impact local businesses, especially smaller “mom and pop” retail, restaurant and entertainment businesses. This argument has its origins in long-ago controversies regarding Atlantic City. Clyde Barrow, Director for the Center of Policy Analysis at the University of Massachusetts Dartmouth, traces the Atlantic City “myth” to a misinformation campaign by the Atlantic City Restaurant and Tavern Association “to win more concessions for its members from the city’s casino hotels.”⁶⁶

Research by Kathryn Hashimoto and George Fenich found that contrary to a negative impact, casinos in Atlantic City actually reversed a downward trend:

The number of eating and drinking establishments in Atlantic County was actually declining in the years *prior* to the opening of the first casinos. However, this decline was actually reversed after the first casinos opened, when the number of non-casino eating and drinking places increased from 415 in 1978 to 569 in 1994 (37 percent). Moreover, in the 11 years since the Hashimoto and Fenich study, the number of non-casino eating and drinking places in Atlantic County has continued to increase to 625 (9.8 percent) in 2004 with 9,020 employees (36 percent).⁶⁷

In a review of available literature, the research division of the Federal Reserve Bank of St. Louis concluded in a 2003 report that the results are “mixed” regarding the impacts of casinos on other local businesses. The report references one study that “found that the growth in retail sales tax collections from various industries slowed after the introduction of casino gambling.” However,

⁶⁶ Barrow, Clyde and Mathew Hirshy. “The Persistence of Pseudo-Facts in the U.S. Casino Debate: The Case of Massachusetts” *Gaming Law Review and Economics* Volume 12, Number 4, 2008.

⁶⁷ *Ibid.*

another referenced study from Indiana showed that casino development retained spending by patrons “who would have, without the casino, spent their money outside of the local area.”⁶⁸

Furthermore, there is substantial economic research from throughout the country contradicting the substitution effect. Hashimoto and Fenich’s 1997 research shows that “in jurisdictions from the seashore to the riverfront to rural areas, north and south, east and west, local restaurants tended to thrive after a casino opened nearby.” Furthermore, Hashimoto and Fenich conclude: “When casinos are developed, all aspects of the local food and beverage business increase: the number of establishments increases, the number of people employed increases and payroll increases at an even greater rate than the first two.”⁶⁹

Research conducted in 1996 by Nancy Reeves and Associates for the Mille Lacs Band of Ojibwe, entitled “The Economic Impact of Grand Casino Mille Lacs and Grand Casino Hinckley on Their Surrounding Areas” concluded that:

At least 15 businesses have either opened, expanded, or re-opened since the opening of Grand Casino Mille Lacs. Included are 4 hotels/motels and resorts, 8 restaurants and fast food establishments, 2 gas stations and a go-kart track. Together, these businesses have added an estimated 142 jobs in the area.

With the opening of Grand Casino Hinckley in 1992, the hospitality business in Hinckley was transformed from a rest stop for travelers to a tourist destination. In addition to the casino complex, with its 1,275 jobs, Hinckley has added 11 new businesses and expanded 4 more since 1992, adding 87 new jobs. As is the case in the Mille Lacs area, Hinckley is now a year round destination because of the casino. Also similar to the Mille Lacs situation, the main street businesses in Hinckley have seen increases in customer spending attributed primarily to casino employees living in the area.

The Center for Policy Analysis University of Massachusetts Dartmouth came to similar conclusions analyzing a number of gaming jurisdictions throughout the country. The number of restaurants and retail sales excluding those from casinos increased in Bossier City, Louisiana; Biloxi/Gulfport, Mississippi; Connecticut; Gilpin County, Colorado, and; Tunica County, Mississippi.

There was a net increase of eight restaurants in Bossier City, Louisiana following the introduction of riverboat casinos. The city’s taxable restaurant sales, excluding restaurants in the hotels and casinos, increased by 5 percent in 1994 and by 7 percent in 1995 *after* the introduction of riverboat casinos. In Biloxi/Gulfport, Mississippi, the rate of non-casino

⁶⁸ Thomas A. Garrett, Senior Economist, Federal Reserve Bank of St. Louis, *Casino Gambling in America and Its Economic Impacts*, August 2003.

⁶⁹ George Fenich and Kathryn Hashimoto, “The Effects of Casinos on Local Restaurant Business,” paper presented at the International Conference on Gambling and Risk-Taking, Montreal, 1997.

retail sales growth increased from an average of 3 percent annually (1990-1992) in the years prior to riverboat gambling to 12 percent annually in the years after riverboat gaming began in the locality.

...the number of restaurants in the area surrounding Foxwoods and Mohegan Sun increased from 472 to 506 following the casino's opening, while restaurant employment increased from 5,911 to 6,628 during the same period.... In Gilpin County Colorado, the number of restaurants increased from 31 to 40 after the introduction of casino gaming. In Tunica County, Mississippi, the number of restaurants increased by 13 percent and restaurant employment grew by 9 percent after the introduction of casino gaming in the county.⁷⁰

Similar conclusions have been reached in other studies:

- Even after accounting for substitution effect, economists at the University of Missouri and Washington University concluded that casino gambling in Missouri had a net positive annual impact on Missouri output of \$759 million, corresponding to a continuing higher level of employment of 17,932 jobs generating \$508 million more in personal income.⁷¹
- A multijurisdictional analysis of retail spending found that in Biloxi/Gulfport, Miss., annual retail sales growth rates increased an average of 3 percent per year from 1990 to 1992, the year when casinos were introduced. Between 1993 and 1995, retail sales jumped 13 percent. In Will County, Ill., retail sales growth trailed statewide trends until 1992, when riverboat casinos were introduced in the local economy. But each year between 1992 and 1995, retail sales growth in Will County exceeded the state rate. In Shreveport/Bossier City, La., retail sales increased by more than 10 percent during 1994, the year that riverboat casinos opened, as the region enjoyed the highest retail sales increase in more than a decade.⁷²

In summary, there is a wealth of evidence contradicting the proposition that gaming substitutes for other expenditures. The positive spillover effect on local hotels for one is unequivocally demonstrated in numerous jurisdictions, even in markets where casinos operate hotels for their gaming customers.

⁷⁰ Ibid.

⁷¹ Charles Leven et al., "Casino Gambling and State Economic Development," paper presented at the Regional Science Association, 37th European Congress, Rome, Aug. 26-29, 1997.

⁷² Arthur Andersen, *Economic Impacts of Casino Gaming in the United States, Volume 2: Micro Study* (Washington, D.C.: American Gaming Association, May 1997).

DISCLAIMER

Certain information included in this report contains forward-looking estimates, projections and/or statements. The Innovation Group has based these projections, estimates and/or statements on our current expectations about future events. These forward-looking items include statements that reflect our existing beliefs and knowledge regarding the operating environment, existing trends, existing plans, objectives, goals, expectations, anticipations, results of operations, future performance and business plans.

Further, statements that include the words "may," "could," "should," "would," "believe," "expect," "anticipate," "estimate," "intend," "plan," "project," or other words or expressions of similar meaning have been utilized. These statements reflect our judgment on the date they are made and we undertake no duty to update such statements in the future.

Although we believe that the expectations in these reports are reasonable, any or all of the estimates or projections in this report may prove to be incorrect. To the extent possible, we have attempted to verify and confirm estimates and assumptions used in this analysis. However, some assumptions inevitably will not materialize as a result of inaccurate assumptions or as a consequence of known or unknown risks and uncertainties and unanticipated events and circumstances, which may occur. Consequently, actual results achieved during the period covered by our analysis will vary from our estimates and the variations may be material. As such, The Innovation Group accepts no liability in relation to the estimates provided herein.

Appendix E

Expanded Regulatory and
Environmental Setting

Expanded Regulatory and Environmental Setting

INTRODUCTION

This document summarizes the framework of laws and regulations relevant to the Project Site and actions outlined in the Environmental Assessment (EA). Sections are organized by resource category, and while most regulations discussed within the document are described here, this list is not comprehensive and is limited to the primary regulations relevant to the analysis within the EA. Additionally, because the Project Site currently consists of Native allotment land, it is not generally subject to State or local laws or regulations.

LAND RESOURCES – EA SECTION 3.2

Regulatory Setting

Federal

Clean Water Act

The Clean Water Act (CWA) prohibits sediment and erosion discharge into navigable waters of the United States and establishes water quality goals. The Water Resources Board requires a Construction General Permit if a project disturbs one or more acres of soil. A site-specific Stormwater Pollution Prevention Plan (SWPPP) is required under this permit. For more information on the CWA and the Water Resources Board, see **Water Resources – Section 3.3 of the EA** below.

Surface Mining Control and Reclamation Act (SMCRA) of 1977

The SMCRA regulates the environmental effects of coal mining in the United States. It is the first federal environmental statute to regulate a specific industry rather than a specific type of pollution. The Act prohibits surface coal mining within the boundaries of any unit of the National Park System. There is a program for regulating active coal mines and one for reclaiming abandoned mine lands.

State and Local

Seismic Hazards Mapping Act

The Seismic Hazards Mapping Act was enacted in 1990 to protect the public from the effects of strong ground shaking, liquefaction, landslides, ground failure, or other hazards caused by earthquakes. This act requires a state geologist to delineate various seismic hazard zones and requires cities, counties, and other local permitting agencies to regulate certain development projects within the portions of those zones where they have jurisdiction. Before a development permit is granted by a city, county or other local permitting agency for a site within a seismic hazard zone, a geotechnical investigation must be conducted, and appropriate mitigation measures must be incorporated into the project's design.

Department of Natural Resources, Division of Agriculture Soil Conservation Program and Plant Materials Center

Provides plant science expertise and technical assistance to government agencies, contractors, land users, and the general public for the protection of soil resources. Provides professional advice and guidance with Storm Water Pollution Prevention Plans (SWPPP), site specific erosion and sediment control measures, and soils analysis interpretation (ADNR, 2024b).

Department of Natural Resources, Geological and Geophysical Surveys

Provides information related to oil and mineral discoveries. Investing in geologic information is necessary for the discovery and commercialization of the state's undiscovered oil, gas, coal, and mineral resources, and in protecting Alaskans from geologic hazards. It is an investment in future mines, oil and gas production, State revenue, jobs, and a sound economy (ADNR, 2024c).

Municipality of Anchorage Code

Contains the municipalities regulations including those related to sediment and erosion control, seismic design standards, and setbacks to surface waters.

Chugiak-Eagle River Comprehensive Plan

The update is for the 1993 comprehensive plan. The Municipal code requires that a comprehensive plan be reviewed and revised every 20 years. It also requires a re-evaluation of the major trends and policies of a comprehensive plan 10 years from the time of its initial adoption. The plan provides population, housing and employment projections for 2025, updated goals and strategies for development, land use development, and public facilities and services (Municipality of Anchorage, 2006). It also identifies measures to prevent flooding, minimize erosion, assure safety, and prevent eroded material from entering waterways.

Alaska Department of Environmental Conservation CGP

The Construction General Permit authorizes stormwater discharges from large and small construction-related activities resulting in a land disturbance of greater than or equal to one acre where those discharges enter waters of the United States through a stormwater conveyance system or a municipal separate storm sewer system. The 2021 CGP became effective on February 1, 2021 and expires on January 31, 2026 (ADEC, 2024b).

Environmental Setting

Geological Setting

The region's geologic history has been influenced by tectonic activities associated with the Pacific and North American plates. These tectonic activities continue to fuel earthquakes along the Aleutian Trench and associated fault systems. The local geology is characterized as an unconsolidated landform derived from glacial and periglacial deposits that consist of end, lateral, and ground moraine, and outwash. These glacial deposits are prevalent throughout the area, contributing to the region's well-drained soils and diverse geomorphological features. As the glaciers retreated, they also carved out surrounding valleys and fjords, sculpting the regional terrain. Locally, the Elmendorf Moraine is composed of glacial till, which includes a mix of clay, sand, gravel, and boulders that were deposited by retreating glaciers. The moraine marks the furthest advance of the glaciers during the last glacial maximum and lies just to the east of the project site at the base of the Chugach Mountains.

Topography

The Project Site is relatively flat land with sloping topography towards Peters Creek. Elevations on-site range from approximately 73 to 88 feet amsl, with the majority of the site sloping gradually to the north. There is a bench cutting across the northwest corner of the property that drops about 8 feet and then flattens out toward the bank of Peters Creek. The highest portion of the site is the southeast corner at an elevation of 88 feet amsl and the lowest portion of the site is the northwest corner at an elevation of 73 feet amsl. Typical slopes across the site are around 1 to 3 percent. Peters Creek runs along the east side of the site.

Soil Types and Characteristics

The Project Site contains three soil types: Cryorthents and Urban Land, 5 to 20 percent slopes; Kashwitna-Kichatna complex, 0 to 3 percent slopes; and Moose River-Niklason complex, occasionally flooded, 0 to 3 percent slopes (NRCS, 2024). The hydrologic soil group is a classification based on the runoff potential of the soils when thoroughly wet, which is defined by the Natural Resource Conservation Service (NRCS) as being under the conditions of maximum yearly wetness (NRCS, 2007). Soils are grouped into four classes that grade from A to D, with A being coarse-grained soils with high infiltration and low runoff potential and D being mostly fine-grained clays with extremely slow infiltration and high runoff potential. The soils on the Project Site have a hydrologic rating of B, indicating the soils have a moderately low runoff potential when wet and a moderate infiltration rate. Soils with this rating are typically moderately deep to deep, moderately well to well drained, and have moderately fine to moderately coarse textures (NRCS, 2024).

Saturated hydraulic conductivity [Ksat] is a quantitative measurement for the movement of water through saturated soil or the ease with which pores in a saturated soil transmit water. Ksat is a factor in determining the hydrologic soil group and is often used in the design of water and wastewater disposal features such as percolation ponds and septic systems. Ksat measures transport only in a vertical direction under completely saturated conditions.

Soil erosion is the wearing and removal of soil materials from the ground surface and the transportation of these soil materials resulting in deposition elsewhere. Mechanisms of soil erosion include stormwater runoff and wind as well as human activities. Factors that influence erosion include physical properties of the soil, topography, annual rainfall, and peak intensity. Soils on the Project Site transmit water at moderately high to high rates, this indicates that the Project Site has low to medium surface runoff potential. The Project Site has a depth to water table of above 80 inches in the majority of the project site and a depth of 18 to 30 inches in the portion containing the Moose River-Niklason complex soils.

Corrosivity pertains to a soil-induced electrochemical or chemical reaction that corrodes concrete or steel. The soils on the Project Site have no rating or a moderate risk of corrosion to concrete and corrosion to steel (NRCS, 2024). Expansive soils may increase in volume when water is absorbed and may shrink when dried, as expansive soils are largely comprised of clays. The property of expansion is measured using linear extensibility. Expansive soils are of concern because they can cause building foundations to rise during the rainy season and fall during the dry season, causing structural distortion. The soils on the Project Site have mapped low linear extensibility ratings and therefore are not considered to be expansive soils.

Liquefaction occurs when loose, saturated, uniformly graded, fine-grained sand, and relatively cohesionless soil deposits temporarily lose strength from seismic shaking. The primary factors controlling the onset of liquefaction include intensity and duration of strong ground motion, characteristics of subsurface soil, on-site stress conditions, and the depth to groundwater.

The Project site was found to be in Zone 3 seismic zone, which has a moderate ground failure susceptibility (Municipality of Anchorage, 2022).

WATER RESOURCES – EA SECTION 3.3

Regulatory Setting

Federal

Executive Order 11988

Executive Order (EO) 11988 requires federal agencies to avoid to the extent possible the long and short-term adverse impacts associated with the occupancy and modification of floodplains and to avoid direct and indirect support of floodplain development wherever there is a practicable alternative. Specifically, EO 11988 states that agencies shall first determine whether the proposed action will occur in a floodplain. EO 11988 defines a floodplain as an area that has a one percent or greater chance of flooding in any given year. Second, if an agency proposes to allow an action to be located in a floodplain, the agency shall consider alternatives to avoid adverse effects and incompatible development in the floodplains. If the only practicable alternative action requires siting in a floodplain, the agency shall minimize potential harm to or within the floodplain.

Clean Water Act

CWA (33 U.S. Code [USC] § 1251-1376), as amended by the Water Quality Act of 1987, is the major federal legislation governing water quality. The objective of the CWA is “to restore and maintain the chemical, physical, and biological integrity of the Nation’s waters.” The U.S. Environmental Protection Agency (USEPA) is delegated as the administrative agency under the CWA. Relevant sections of the CWA are as follows.

- Sections 303 and 304 provide for water quality standards, criteria, and guidelines. Section 303(d) requires states to identify impaired off-Reservation water bodies, rank these impaired bodies based on severity of contamination and uses for the waters, and develop water quality management strategies, usually in the form of total maximum daily loads for the contaminant(s) of concern.
- Section 401 (Water Quality Certification) requires an applicant for any federal permit that proposes an activity that may result in a discharge to Waters of the U.S., to obtain certification from the USEPA for tribal land activities, or the state for off-Reservation activities, that the discharge will comply with other provisions of the CWA.
- Section 402 establishes the National Pollutant Discharge Elimination System (NPDES), a permitting system for the discharge of any pollutant (except for dredged or fill material) into Waters of the U.S. Each NPDES permit contains limits on concentrations of pollutants discharged to surface waters to prevent degradation of water quality and protect beneficial uses.

The Federal Antidegradation Policy was adopted as part of the 1972 amendments to the CWA. Federal policy (Code of Federal Regulations [CFR], Title 40, Part 131.12) specifies that each state must develop, adopt, and retain an anti-degradation policy to protect the minimum level of off-Reservation surface water quality necessary to support existing uses. Each state must also develop procedures to implement the anti-degradation policy through water quality management processes. Each state anti-degradation policy must include implementation methods consistent with the provisions outlined in 40 CFR § 131.12.

General NPDES Permit for Construction

In 1990, an amendment to the CWA directed the NPDES permitting program to address non-point source pollution from construction activities. Construction activities include clearing, grading, excavation, stockpiling, and reconstructing existing facilities involving removal and replacement of existing foundations or other hardscapes. Construction projects disturbing one or more acres of soil must be covered under the NPDES Construction General Permit process. For tribal projects on land held in trust by the federal government, the Tribe proposing the project must apply for coverage under the USEPA's NPDES Construction General Permit. Project proponents are required to submit to the USEPA a complete Notice of Intent (NOI) to comply with the permit. A complete NOI package consists of an NOI form, site map, and fee. The USEPA's NPDES Construction General Permit also requires the development and implementation of a SWPPP. The SWPPP contains a site map showing the construction site perimeter, existing and proposed buildings, lots and roadways, stormwater collection and discharge points, general topography both before and after construction, and drainage patterns across the site. The SWPPP must list Best Management Practices (BMP) that will be implemented during construction and operation to address stormwater runoff rates and quality. SWPPP BMPs include the following categories:

- Site planning considerations, such as preservation of existing vegetation;
- Vegetation stabilization through methods such as seeding and planting;
- Physical stabilization through use of dust control and stabilization measures;
- Diversion of runoff by utilizing earth dikes and temporary drains and swales;
- Velocity reduction through measures such as slope roughening/terracing; and
- Sediment trapping/filtering through use of silt fences, straw bales and sandbag filters, and sediment traps and basins.

Safe Drinking Water Act

Under the mandate of the Safe Drinking Water Act, the USEPA sets legally enforceable National Primary Drinking Water Regulations (primary standards) that apply to public water systems. These standards are established to protect human health by limiting the levels of contaminants in drinking water. The USEPA also defines National Secondary Drinking Water Regulations (secondary standards) for contaminants that cause cosmetic and aesthetic effects, but not for health effects. The USEPA recommends that these secondary standards be met but does not require systems to comply with them.

The USEPA does not oversee the construction and permitting of groundwater wells, but requires that public health standards, such as an effectively installed sanitary seal, are in place, and recommends that water systems be installed to meet California Department of Public Health Standards. The USEPA will also primarily establish monitoring and operational requirements, which will typically be specific to the project area.

Both primary and secondary drinking water standards are expressed as either Maximum Contaminant Levels, which define the highest level of a contaminant allowed in drinking water, or Maximum Contaminant Level Goals, which define the level of a contaminant below which there is no known or expected risk to health. Monitoring requirements typically include total coliform, nitrate, inorganic chemicals, volatile organic chemicals, non-volatile synthetic organic chemicals, secondary drinking water standard constituents, and general chemistry (including alkalinity, hardness, and minerals). The frequency of sampling varies and may be reduced over time.

Federal Emergency Management Agency

The Disaster Relief Act of 1974 as amended by the Robert T. Stafford Disaster Relief and Emergency Assistance Act of 1988 created the Federal Emergency Management Agency (FEMA), which is responsible for determining flood elevations and floodplain boundaries based on U.S. Army Corps of Engineers (USACE) studies. FEMA is also responsible for distributing Flood Insurance Rate Maps, which are used in the National Flood Insurance Program. These maps identify the locations of special flood hazard areas, including 100-year floodplains.

State and Local

Alaska State Statutes

The Alaska State Statutes contain several regulations relating to waters within the state and within three miles of shore. This includes protections of riparian areas, such as allowable uses. Alaska Statutes also protect drinking water sources by prohibiting actions that would impact groundwater wells. The statutes also contain an anti-degradation policy that identifies water quality maintenance actions.

Alaska Water Use Act (AS 46.15)

The Alaska Water Use Act governs the allocation and use of all surface and groundwater in the state. It designates water as a public resource belonging to Alaska's residents. The Act requires individuals and entities to obtain permits from the Alaska Department of Natural Resources (DNR) for any significant water use.

Municipality of Anchorage Municipal Code

Title 15 of the Municipality of Anchorage Municipal Code identifies the Municipality's environmental protection regulations, including watershed regulations and building codes. Title 21 regulates planning and zoning and provides standards for stormwater management planning and water pollution control, design standards for setbacks to surface waters and encroachments into such setbacks.

Clean Water Act Section 401

The state CGP applies to construction related activities within the state that disturb one or more acres of land, including larger projects that cumulatively disturb more than one acre of land. Project proponents must submit a Notice of Intent to obtain coverage under an Alaska Pollutant Discharge Elimination System CGP and must implement stormwater BMPs protective of water quality throughout the life of construction activities. A CGP SWPPP is also required as part of coverage under the CGP.

AIR QUALITY – EA SECTION 3.4

Regulatory Setting

Federal

Clean Air Act of 1970

The Clean Air Act (CAA; 42 USC Chapter 85) is the federal legislation for the protection of air quality. The CAA gives the USEPA authority to regulate air quality by promulgating standards and levels for air quality and enforcing those standards and levels on federal, state, and tribal land.

The CAA requires the USEPA to regulate hazardous air pollutants, which are those pollutants that are known or suspected to cause cancer or other serious health effects, such as reproductive effects or birth defects, or adverse environmental effects. Certain air pollutants, either directly or in reaction with other pollutants, have been recognized to cause notable health problems and consequential damage to the environment due to elevated concentrations in the atmosphere. Such pollutants have been identified and regulated as part of the overall endeavor to prevent further deterioration and facilitate improvement in air quality. The Federal CAA of 1970, as amended, establishes air quality standards for several critical air pollutants (CAPs): ozone (O₃), carbon monoxide (CO), particulate matter (PM), nitrogen dioxide (NO₂), sulfur dioxide (SO₂), and lead (Pb). These pollutants are termed “criteria” pollutants because the USEPA has established specific concentration threshold criteria based upon specific medical evidence of health effects or visibility reduction, soiling, nuisance, and other forms of damage. These National Ambient Air Quality Standards (NAAQS) are divided into primary standards and secondary standards. Primary standards are designed to protect the public health and secondary standards are intended to protect the public welfare from effects such as visibility reduction, soiling, nuisance, and other forms of damage. NAAQS and California Ambient air quality standards (CAAQS) are presented in **Table 1**. Areas are designated attainment, nonattainment, or maintenance by the USEPA depending on whether the area is below or exceed the established NAAQS. Nonattainment areas must take steps towards attainment within a specific period of time. Once an area reaches attainment for particular criteria pollutant, then the area is re-designated attainment or maintenance. The CAA places most of the responsibility on states to achieve compliance with the NAAQS. States, municipal statistical areas, and counties that contain areas of nonattainment are required to develop a State Implementation Plan (SIP), which outlines policies and procedures designed to bring the state into compliance with the NAAQS.

Ozone

Photochemical reactions involving reactive organic gases (ROG)/volatile organic compounds (VOC) and nitrogen oxides (NOX) resulting from the incomplete combustion of fossil fuels are the largest source of ground-level O₃. Because photochemical reaction rates depend on the intensity of ultraviolet light and air temperature, O₃ is primarily a summer air pollution problem. As a photochemical pollutant, O₃ is formed only during daylight hours under appropriate conditions. However, it is destroyed throughout the day and night. O₃ is considered a regional pollutant as the reactions forming it take place over time and are often most noticeable downwind from the sources of the emissions.

Particulate Matter 2.5

Particle pollution is a mixture of microscopic solids and liquid droplets suspended in air. This pollution, also known as PM_{2.5}, is made up of a number of components, including acids (such as nitrates and sulfates), organic chemicals, metals, soil or dust particles, and allergens (such as fragments of pollen or mold spores). The size of particles is directly linked to their potential for causing health problems. Particles smaller than 2.5 µm pose the greatest problems because they can be inhaled deep into the lungs. Exposure to such particles can affect respiratory system function. Areas are designated attainment, nonattainment, or maintenance by the USEPA depending on whether the area is below or exceed the established NAAQS. Nonattainment areas must take steps towards attainment within a specific period of time. Once an area reaches attainment for particular criteria pollutant, then the area is re-designated attainment or maintenance. The CAA places most of the responsibility on states to achieve compliance with the NAAQS. States, municipal statistical areas, and counties that contain areas of nonattainment are required to develop a State Implementation Plan (SIP), which outlines policies and procedures designed to bring the state into compliance with the NAAQS.

Table 1: Ambient Air Quality Standards

Pollutant	Averaging Time	Standard (parts per million)		Standard (mcg per cubic meter)		Violation Criteria	
		Alaska AAQS	NAAQS	Alaska AAQS	NAAQS	Alaska AAQS	NAAQS
O ₃	8 hours	0.070	0.070	N/A	137	If 3-year average of the annual 4th high daily maximum exceeds standard	If exceeded on more than 3 days in 3 years
CO	8 hours	N/A	9	10,000	10,000	If exceeded on more than 1 day per year	If exceeded on more than 1 day per year
	1 hour	N/A	35	40,000	40,000	If exceeded on more than 1 day per year	If exceeded on more than 1 day per year
NO ₂	Annual average	N/A	0.053	100	100	If exceeded	If exceeded
	1 hour	N/A	0.100	188	188	If exceeded	N/A
SO ₂	Annual arithmetic mean	N/A	0.030	80	79	If exceeded	If exceeded
	24 hours	N/A	0.14	365	N/A	If exceeded on more than 1 day per year	If exceeded on more than 1 day per year
	1 hour (primary)	N/A	0.075	196	196	If 3-year average of the annual 99th-percentile exceeds standard	If 3-year average of the annual 99th-percentile exceeds standard
	3 hours (secondary)	N/A	0.5	1,300	N/A	If exceeded on more than 1 day per year	If exceeded on more than 1 day per year
PM ₁₀	24 hours	N/A	N/A	150	150	If exceeded on more than 1 day per year	If exceeded on more than 1 day per year
PM _{2.5}	Annual arithmetic mean (primary)	N/A	N/A	12	12	If 3-year average exceeds standards	If exceeded
	Annual arithmetic mean (secondary)	N/A	N/A	N/A	15	N/A	If exceeded
	24 hours	N/A	N/A	35	35	If 3-year average exceeds standards	If exceeded on more than 1 day per year
Lead	Rolling 3-month Avg.	N/A	N/A	0.15	0.15	If 3-month average exceeds standards	If exceeded
Ammonia	8 hours	N/A	No Federal Standard	2100	No Federal Standard	If exceeded on more than 1 day per year	N/A

Source: ADEC, 2022; USEPA, 2024

Carbon Monoxide

CO is not readily dispersed throughout the atmosphere; therefore, it is considered a localized air quality issue as it is close to the emission source. CO emissions generally cause an acute (short-term) health threat. CO is a pollutant of concern at major signalized intersections (greater than 100,000 vehicles per day) that exhibit prolonged vehicle idling times.

Hazardous Air Pollutants

In addition to the above-listed CAPs, Hazardous Air Pollutants (HAP) are a group of chemical pollutants which can cause adverse effects to human health and/or the environment. Haps are also known as toxic air pollutants or air toxics.

HAPs are a list of over 188 airborne chemicals developed by the USEPA. Sources of HAPs include industrial processes, such as petroleum refining and chrome plating operations; commercial operations, such as gasoline stations and dry cleaners; cigarette smoke; and motor vehicle exhaust. Cars and trucks release at least 40 different HAPs. The most important, in terms of health risk, are diesel particulate matter (DPM), benzene, formaldehyde, 1,3-butadiene, and acetaldehyde. Health effects of HAPs can include cancer, birth defects, and neurological damage.

HAPs are less pervasive in the urban atmosphere than CAPs but are linked to short-term (acute) or long-term (chronic or carcinogenic) adverse human health effects. The majority of the estimated health risk from HAPs can be attributed to relatively few compounds. The most important HAPs are found in DPM. Diesel engines emit a complex mixture of air pollutants, composed of gaseous and solid material. Diesel exhaust contains a variety of harmful gases and over 40 other cancer-causing substances, and the visible emissions in diesel exhaust are PM that includes carbon particles or “soot.” Exposure to DPM is a health hazard, particularly to children whose lungs are still developing and the elderly who may have other serious health problems.

Federal General Conformity

Under the General Conformity Rule, updated in 2010, the lead agency with respect to a federal action is required to demonstrate that the proposed federal action conforms to the applicable SIP before the action is taken. There are two phases to a demonstration of general conformity.

- The Conformity Review process, which entails an initial review of the federal action to assess whether a full conformity determination is necessary
- The Conformity Determination process, which requires that a proposed federal action be demonstrated to conform to the applicable SIP

The Conformity Review requires the lead agency to compare estimated emissions to the applicable general conformity levels (40 CFR 93.153 [b][1] and [2]), which these can be seen in **Table 2** and **Table 3**. If the emission estimates from step one is below the applicable threshold(s), then a general conformity determination is not necessary and the full Conformity Determination is not required. If emission estimates are greater than the applicable threshold(s), the lead agency must conduct a Conformity Determination.

Federal Class I Areas

Title 1, Part C of the CAA was established in part to preserve, protect, and enhance the air quality in national parks, national wilderness areas, national monuments, national seashores, and other areas of special national or regional natural, recreational, scenic, or historic value. The CAA designates all international parks, national wilderness areas, and memorial parks larger than 5,000 acres and national parks larger than 6,000 acres as “Class I areas.” The CAA prevents significant deterioration of air quality in Class I areas under the Prevention of Significant Deterioration (PSD) Program. The PSD Program protects Class I areas by allowing only a small increment of air quality deterioration in these areas by requiring assessment of potential impacts on air quality related values of Class I areas. Any major source of emissions within 100 kilometers (62.1 miles) from a federal Class I area is required to conduct a pre-construction review of air quality impacts on the area(s). A “major source” for the PSD Program is defined as a facility that will emit (from direct stationary sources) 250 tons per year (tpy) of regulated pollutant. For certain industries, these requirements apply to facilities that emit (through direct stationary sources) 100 tpy or more of a regulated pollutant. Mobile sources (e.g., vehicle emissions) are by definition not stationary sources and are therefore not subject to the PSD program.

Table 2: 40 CFR 93.153 [b][1] Emission Rates for Nonattainment Areas (NAAs)

Pollutant	Tons per Year
Ozone (VOC's or NOX)	
Serious NAA's	50
Severe NAA's	25
Extreme NAA's	10
Other ozone NAA's outside ozone transport region	100
Other ozone NAA's inside an ozone transport region	
VOC	50
NOx	100
Carbon Monoxide: all maintenance areas	100
SO ₂ or NO ₂ : All NAAs	100
PM ₁₀ :	
Moderate NAA's	100
Serious NAAs	70
PM _{2.5} (direct emissions, SO ₂ , NOx, VOC, and Ammonia)	
Moderate NAA's	100
Serious NAAs	70
PD: all NAA's	25

Table 3: 40 CFR 93.153 [b][2] Emission Rates for Maintenance Areas

Pollutant	Tons per Year
Ozone (NOX), SO ₂ or NO ₂ :	
All maintenance areas	100
Ozone (VOC's)	
Maintenance areas inside an ozone transport region	50
Maintenance areas outside an ozone transport region	100
Carbon monoxide: All maintenance areas	100
PM ₁₀ : All maintenance areas	100
PM _{2.5} (direct emissions, SO ₂ , NOx, VOC, and Ammonia)	100
All maintenance areas	100
Pb: All maintenance areas	25

Tribal New Source Review

The Tribal Minor New Source Review (NSR) permitting program was established by the USEPA under the CAA. The minor NSR program applies to both new minor sources and minor modifications to both major and minor projects in attainment and nonattainment areas. NSR programs must comply with the standards and control strategies of the Tribal Implementation Plan (TIP) or SIP. If there is not an applicable SIP or TIP, the USEPA issues permits and implements the program.

A General Permit under the minor NSR program would be required on tribal land if stationary source allowable emissions of regulated pollutants would exceed the thresholds presented in 40 CFR 49.153, Table 1 (presented in **Table 4**). This General Permit serves as a preconstruction permit containing limitations and other restrictions specifying the construction, modification, and operation of a minor source. The applicability of Tribal NSR is made on a source’s potential to emit (PTE). For emergency generators, the USEPA has determined that 500 hours per year should be assumed as a reasonable and realistic "worst-case" estimate on a PTE basis (ADEC, 2024).

Table 4: Tribal Minor New Source Review Thresholds

Pollutant	Emissions Thresholds for Nonattainment Areas (tpy)	Emissions Thresholds for Attainment Areas (tpy)
NOx	5	10
ROG	2	5
PM	5	10
PM ₁₀	1	5
PM _{2.5}	0.6	3
CO	5	10
SO ₂	5	10
Pb	0.1	0.1

Source: 40 CFR 49.153.

Climate Change

Secretarial Order (SO) 3399

On February 19, 2021, Secretary of the Interior Deb Haaland issued Secretarial Order (SO) 3399 to prioritize action on climate change throughout the Department and to restore transparency and integrity in the Department’s decision-making processes. SO 3399 specifies that when considering the impact of GHG emissions from a proposed action, Bureaus/Offices should use appropriate tools, methodologies, and resources available to quantify GHG emissions and compare GHG quantities across alternatives. SO 3399 acknowledges that identifying the interactions between climate change and the environmental impacts of a proposed action in NEPA documents can help decision makers identify opportunities to reduce GHG emissions, improve environmental outcomes, and contribute to protecting communities from the climate crisis.

NEPA Guidance on Consideration of GHG Emissions and Climate Change (2023)

On January 9, 2023, the Council on Environmental Quality issued National Environmental Policy Act Guidance on Consideration of Greenhouse Gas Emissions and Climate Change (88 Fed. Reg. 1196). This interim guidance directs agencies to consider the potential effects of a proposed action on climate change and the effects of climate change on a proposed action and its environmental impacts. CEQ recommends that agencies quantify a proposed action’s projected GHG emissions for the expected lifetime of the action and provide additional context for GHG emissions, including the use of the best available social cost of GHG (SC–GHG) estimates, to translate climate impacts into the more accessible metric of dollars. This guidance does not propose a specific, quantitative threshold of significance; however, it states that agencies should consider the potential for mitigation measures to reduce or mitigate GHG emissions and climate change effects when those measures are reasonable and consistent with achieving the purpose and need for the proposed action.

CEQ recommends that agencies explain how the proposed action and alternatives would help meet or detract from achieving relevant climate action goals and commitments, including federal goals, international agreements, state or regional goals, Tribal goals, agency-specific goals, or others as appropriate.

State and Local

Alaska Department of Environmental Conservation (ADEC)

ADEC is a state agency responsible for protecting Alaska's environment and public health through a range of regulatory and enforcement activities. Established in 1970, ADEC's primary mission is to ensure that Alaska's natural resources, including air, water, and land, are managed in a way that maintains their health and sustainability for current and future generations. The ADEC Division of Air quality manages air quality in the State through a comprehensive program that includes monitoring, regulation, and enforcement. The department operates a network of air quality monitoring stations to measure pollutants like particulate matter, ozone, nitrogen dioxide, sulfur dioxide, and carbon monoxide. ADEC develops and implements the SIP, ensuring compliance with NAAQS established by the USEPA. It also issues permits for industrial and other pollution sources, sets emission limits, and enforces regulations to maintain air quality.

Alaska Administrative Code (AAC) Title 18, Chapter 50

AAC Title 18, Chapter 50, governs the State's air quality control regulations. This chapter establishes standards, permitting requirements, and enforcement mechanisms to manage and mitigate air pollution in Alaska. It covers a broad range of topics, including the classification of air quality areas, the establishment of emission limits for various pollutants, and the procedures for obtaining necessary permits for activities that might affect air quality. The regulations in this chapter also address compliance monitoring, reporting requirements, and the roles and responsibilities of state agencies in enforcing air quality standards. Additionally, Chapter 50 outlines specific measures to protect air quality in areas designated as nonattainment for certain pollutants, ensuring that industrial, commercial, and residential activities do not compromise the state's air quality objectives. Alaska's ambient air quality standards are summarized in **Table 1** above.

Alaska State Statutes 46.03 and 46.14

Title 46 of the Alaska State Statutes, which addresses Water, Air, Energy, and Environmental Conservation, includes several chapters dedicated to air quality management and control. Chapter 3 establishes the regulatory framework for environmental protection in Alaska, focusing on air and water quality. It grants the Alaska DEC the authority to set and enforce air quality standards, issue permits for pollution sources, and ensure regulatory compliance. The chapter details the DEC's responsibilities for monitoring air quality, inspecting facilities, and implementing pollution control measures. Chapter 14 further specifies the state's air quality management program, emphasizing the importance of clean air for public health and the environment. It empowers the DEC to develop and enforce air quality regulations, issue permits, and manage both major and minor pollution sources, while also outlining procedures for compliance, monitoring, and enforcement.

Alaska State Implementation Plan (SIP)

The Alaska SIP is a comprehensive strategy required under the CAA to ensure that the state meets and maintains federal air quality standards. It outlines the measures and regulations Alaska will use to control emissions from various sources, including industrial facilities, transportation, and residential areas.

The SIP includes detailed plans for monitoring air quality, setting emission limits, and enforcing compliance through state and local agencies, such as the Alaska DEC. It also emphasizes public involvement by providing opportunities for stakeholder input on air quality plans and regulations. For areas not meeting federal standards, the SIP provides strategies to achieve compliance and maintain air quality over time.

State of Alaska Priority Sustainable Energy Action Plan

The State of Alaska Priority Sustainable Energy Action Plan is a strategic framework aimed at guiding the state towards a sustainable and resilient energy future. It emphasizes the development and integration of renewable energy sources like wind, solar, hydroelectric, and biomass to reduce reliance on fossil fuels and leverage Alaska's natural resources. The plan prioritizes energy efficiency and conservation across residential, commercial, and public sectors by promoting energy-efficient technologies and practices. It also focuses on upgrading and modernizing energy infrastructure to enhance reliability and resilience, especially in remote areas. Economic and community development is a key aspect, with the plan aiming to create jobs, support local entrepreneurship, and provide affordable energy solutions. To facilitate these goals, the plan outlines the need for supportive policies and streamlined regulations, ensuring collaborative efforts among state agencies and stakeholders.

BIOLOGICAL RESOURCES – EA SECTION 3.5

Regulatory Setting

Federal

Federal Endangered Species Act

The Federal Endangered Species Act (FESA) protects species that are at risk of extinction and provides for the conservation of the ecosystems on which they depend. The U.S. Fish & Wildlife Service (USFWS) and the National Oceanic and Atmosphere Administration, Fisheries Service (NOAA Fisheries) share responsibility for implementing FESA. Generally, USFWS manages terrestrial and freshwater species, while NOAA Fisheries is responsible for marine and anadromous species. Section 9 (§ 1538) prohibits the "take" of a listed species by anyone, including private individuals and state and local agencies. Threatened and endangered species on the federal list (50 CFR Sections 17.11 and 17.12) are protected from take, which is defined as direct or indirect harm. If "take" of a listed species is incidental to an otherwise lawful activity, this triggers the need for consultation under Section 7 of the FESA for federal agencies.

Pursuant to the requirements of the FESA, a federal agency reviewing a proposed project within its jurisdiction must determine whether any federally listed species may be present on a proposed project site and whether a proposed project will have a potentially significant impact upon such species. Under the FESA, habitat loss is considered to be an impact to the species. In addition, the agency is required to determine whether a project is likely to jeopardize the continued existence of any species that is proposed for listing under the FESA or to result in the destruction or adverse modification of critical habitat proposed to be designated for such species (16 USC Section 1536[3], [4]).

Migratory Bird Treaty Act

Migratory birds are protected under the federal Migratory Bird Treaty Act (MBTA) of 1918 (16 USC 703-711). The MBTA makes it unlawful to take, possess, buy, sell, purchase, or barter any migratory bird listed under 50 CFR 10, including feathers or other parts, nests, eggs, or products, except as allowed by

implementing regulations (50 CFR 21). The direct injury or death of a migratory bird due to construction activities or other construction-related disturbance that causes nest abandonment, nestling abandonment, or forced fledging would be considered take under federal law. As such, project-related disturbances must be reduced or eliminated during the nesting season. The general nesting season extends from February 15 to July 15 for southeast Alaska in forest/woodland areas (USFWS, 2024c).

Bald and Golden Eagle Protection Act

The Bald and Golden Eagle Protection Act was originally enacted in 1940 to protect bald eagles and was later amended to include golden eagles (16 USC Subsection 668-668). This act prohibits take, possession, and commerce of bald and golden eagles and associated parts, feathers, nests, or eggs with limited exceptions. The definition of take is the same as the definition under the FESA.

Clean Water Act - Sections 404 and 401

Any project that involves discharge of dredged or fill material into jurisdictional Waters of the U.S. must first obtain authorization from the USACE, under Section 404 of the CWA. Projects requiring a 404 permit under the CWA also require a Section 401 certification from the USEPA for tribal land. The agency also administers the NPDES general permits for construction activities disturbing one acre or more. Effective September 8, 2023, the USEPA and the USACE have issued a new final rule in the Code of Federal Regulations to conform the definition of ‘waters of the United States’ to the 2023 Supreme Court’s May 25, 2023 decision in Sackett vs. EPA. Under the new final rule, tributaries and wetlands must have a continuous surface connection to navigable waterways to be considered jurisdictional under the Clean Water Act. Only those relatively permanent, standing, or continuously flowing bodies of water meet the current definition. In certain states where litigation regarding this definition is ongoing, the pre-2015 definition of waters of the U.S. is in effect. Alaska is one of these states and currently operates under the pre- 2015 regulatory regime (USACE, 2023).

Magnuson-Stevens Act and Sustainable Fisheries Act

The Magnuson–Stevens Fishery Conservation and Management Act (Magnuson-Stevens Act) is the primary law that governs marine fisheries management in U.S. federal waters. First passed in 1976, the Magnuson-Stevens Act fosters the long-term biological and economic sustainability of marine fisheries. Its objectives include: preventing overfishing; rebuilding overfished stocks; increasing long-term economic and social benefits; ensuring a safe and sustainable supply of seafood; and protecting habitat that fish need to spawn, breed, feed, and grow to maturity.

The Sustainable Fisheries Act of 1996 (Public Law 104-297) amended the Magnuson-Stevens Act to establish new requirements for fishery management councils to identify and describe Essential Fish Habitat (EFH) and to protect, conserve, and enhance EFH for the benefit of fisheries. EFH is defined as those waters and substrate necessary to fish for spawning, breeding, feeding, or growth to maturity. The Sustainable Fisheries Act also established a federal EFH consultation process that advises federal agencies to avoid, minimize, mitigate, or otherwise offset adverse effects on EFH. Consultation is required if a federal agency has authorized, funded, or undertaken part or all of a proposed activity and the action will adversely affect EFH. An adverse effect includes direct or indirect physical, chemical, or biological alternations to waters or substrate, species and their habitat, quality and/or quantity of EFH, or other ecosystem components. If a federal agency determines that an action will not adversely affect EFH, and NOAA Fisheries agrees, no consultation is required. Fishery management councils can designate Habitat Areas of Particular Concern, specific areas within EFH that have extremely important ecological functions and/or are especially vulnerable to degradation.

State and Local

Alaska Department of Fish and Game, Subsistence Regulations

The state manages subsistence use of fish and wildlife on State public lands and privately-held Tribal lands (including Alaska Native Corporation lands and Native allotments). Alaska State law (16.05.094) directs the Division of Subsistence to compile data and conduct studies on all aspects of subsistence hunting and fishing from residents of the State, evaluate impacts, make recommendations, and quantify amounts and nutritional value of the resource.

Alaska Forest Resources and Practices Act (FRPA)

This act governs how timber harvesting, reforestation, and timber access occur on state, private, and municipal land. Timber harvesting does not require a permit from ADF&G Habitat Section, but any activities related to commercial or personal use timber harvest that occur in fish-bearing waterbodies will require a fish habitat permit. The FRPA is designed to protect fish habitat and water quality and ensure prompt reforestation of forestland while providing for a healthy timber industry. The ADNR division of forestry is responsible for oversight of timber harvest under the FRPA (ADFG, 2024).

Alaska Wetland Program Plan

Under the Alaska Department of Environmental Conservation, this plan establishes a strategic statewide plan for assessing the state's wetlands, compiles science-based information to identify wetland functions and values, and develops a framework for identifying, evaluating, and implementing efficiencies in wetland regulatory programs within the state (ADEC, 2015).

Alaska Statute 16.20.190 Determining Endangered Species

Under the Alaska Department of Fish and Game. A species or subspecies of fish and wildlife is considered endangered when the commissioner of fish and game determines its numbers have decreased to such an extent as to indicate its continued existence is threatened. The following must be considered:

- Destruction, drastic modification, or severe curtailment of its habitat
- Overutilization for commercial or sporting purposes
- Effect on it of disease or predation
- Other natural or man-made factors affecting its continued existence.

Once a determination is made, the commissioner must publish a list of the species or subspecies of fish and wildlife that are endangered. Every two years reviews must be made to determine any changes in the status (AS 16.20.190).

State Wildlife Action Plan (SWAP)

The United States Congress created the State Wildlife Grants (SWG) program in 2000. The program provides critical funding to every state and territory to plan and implement proactive conservation actions to prevent the nation's fish and wildlife from becoming endangered. Funding is provided to the states through the USFWS. Using funding, each state and territory develops a SWAP.

These plans conserve priority fish and wildlife species and their habitats, by identifying species with important conservation needs and offer a set of actions to address key threats. The plans also provide a voluntary, non-regulatory alternative to the federal listing process. The plan fulfills the 10-year revision requirement of the grant. This plan is an update to the original 2006 plan (ADFG, 2015).

CULTURAL AND PALEONTOLOGICAL RESOURCES – EA SECTION 3.6

Regulatory Setting

Federal

National Historic Preservation Act

Section 106 of the National Historic Preservation Act (NHPA), as amended, and its implementing regulations found in 36 CFR Part 800 require federal agencies to identify cultural resources that may be affected by actions involving federal lands, funds, or permitting. The BIA must comply with Section 106 for the proposed business lease approval. The significance of the resources must be evaluated using established criteria outlined in 36 CFR 60.4, as described below. If a resource is determined to be a historic property, Section 106 of the NHPA requires that effects of the federal undertaking on the resource be determined. A historic property is defined as:

...any prehistoric or historic district, site, building, structure or object included in, or eligible for inclusion in the National Register of Historic Places, including artifacts, records, and material remains related to such a property... (NHPA Sec. 301[5])

Section 106 of the NHPA prescribes specific criteria for determining whether a project would adversely affect a historic property, as defined in 36 CFR 800.5. An impact is considered adverse when prehistoric or historic archaeological sites, structures, or objects that are listed on or eligible for listing in the National Register of Historic Places (NRHP) are subjected to the following:

- Physical destruction of or damage to all or part of the property;
- Alteration of a property;
- Removal of the property from its historic location;
- Change of the character of the property's use or of physical features within the property's setting that contribute to its historic significance;
- Introduction of visual, atmospheric, or audible elements that diminish the integrity of the property's significant historic features;
- Neglect of a property that causes its deterioration; and
- Transfer, lease, or sale of the property out of federal control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property's historic significance.

If the historic property will be adversely affected by the undertaking, then prudent and feasible measures to resolve adverse impacts must be taken. The State Historic Preservation Office must be provided an opportunity to review and comment on these measures prior to project implementation.

National Register of Historic Places

The eligibility of a resource for listing in the NRHP is determined by evaluating the resource using criteria defined in 36 CFR § 60.4 as follows.

The quality of significance in American history, architecture, archaeology, and culture is present in districts, sites, buildings, structures, and objects of state and local importance that possess integrity of location, design, setting, materials, workmanship, feeling, association, and:

- A) That are associated with events that have made a significant contribution to the broad patterns of our history;
- B) That are associated with the lives of persons significant in our past;
- C) That embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or that possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or
- D) That have yielded, or may be likely to yield, information important to prehistory or history.

Sites younger than 50 years, unless of exceptional importance, are not eligible for listing in the NRHP.

In addition to meeting at least one of the criteria listed above, the property must also retain enough integrity to enable it to convey its historic significance. The NRHP recognizes seven aspects or qualities that, in various combinations, define integrity. These seven elements of integrity are location, design, setting, materials, workmanship, feeling, and association. To retain integrity a property will always possess several, and usually most, of these aspects.

While most historic buildings and many historic archaeological properties are significant because of their association with important events, people, or styles (Criteria A, B, and C), the significance of most prehistoric and some historic-period archaeological properties is usually assessed under Criterion D. Criterion D stresses the importance of the information contained in an archaeological site rather than its intrinsic value as a surviving example of a type or its historical association with an important person or event. It places importance not on physical appearance but rather on information potential.

Native American Graves Protection and Repatriation Act

The Native American Graves Protection and Repatriation Act (NAGPRA), 25 USC 3001 et seq., provides a process for museums and federal agencies to return Native American cultural items – human remains, funerary objects, sacred objects, or objects of cultural patrimony – to lineal descendants, and culturally affiliated Indian tribes and Native Hawaiian organizations. NAGPRA includes provisions for unclaimed and culturally unidentifiable Native American cultural items, intentional and inadvertent discovery of Native American cultural items on federal and Tribal lands, and penalties for noncompliance and illegal trafficking.

Archaeological Resources Protection Act of 1979

The Archaeological Resources Protection Act of 1979 (ARPA; Public Law 96-95; 16 USC 470aa-mm) provides for the protection of archaeological resources and sites that are on public and Indian lands and fosters increased cooperation and exchange of information between governmental authorities, the professional archaeological community, and private individuals having collections of archaeological resources and data that were obtained before October 31, 1979. ARPA also provides penalties for noncompliance and illegal trafficking.

Paleontological Resources Preservation Act

Paleontological resources are defined as the traces or remains of prehistoric plants and animals. Such remains often appear as fossilized or petrified skeletal matter, imprints, or endocasts, and reside in sedimentary rock layers. Paleontological resources are considered important for their scientific and educational value. Fossil remains of vertebrates are considered significant. Invertebrate fossils are considered significant if they function as index fossils. Index fossils are those that appear in the fossil record for a relatively short and known period of time. This allows geologists to interpret the age range of the geological formations in which they are found. The Paleontological Resources Preservation subtitle of the Omnibus Public Land Management Act, 16 USC 470aaa to aaa-11 requires the U.S. Department of Agriculture (USDA) and the U.S. Department of the Interior to issue implementation regulations to provide for the preservation, management, and protection of paleontological resources on federal lands and ensure that these resources are available for current and future generations to enjoy as part of America's national heritage.

State

Alaska Department of Natural Resources, Office of History and Archaeology

They serve as Alaska's State Historic Preservation Office. The office administers programs authorized by the NHPA and the Alaska Historic Preservation Act. They work with local governments, the public, and not for profit organizations to identify, preserve, protect, and interpret the state's cultural, historic, and archaeological resources.

Alaska Historic Preservation Plan

The plan intends to guide the activities and priorities of agencies and organizations involved in preservation throughout the state. The plan establishes ways the preservation community in Alaska can work to achieve common goals. Currently, this plan is being updated for the next ten years, 2025 to 2034 (OHA, 2018).

Alaska Historic Preservations Act of 1971

The Act serves to preserve and protect the historic, prehistoric, and archeological resources of Alaska from loss, desecration, and destruction so that the scientific, historic, and cultural heritage embodied in these resources may pass undiminished to future generations.

Environmental Setting

Prehistoric and Archaeological Overview

The prehistory of south-central Alaska is understood as the geographic region of Beringia, which stretches from Pacific Russia on the west through Alaska, and to northwestern Canada on the east. The earliest known archaeological sites in Alaska date back to 14,150 years before present (B.P.) from the Tanana River Valley, approximately 200 miles north of the Project Site. Subsequent to the earliest Diuktai tradition, hunters of the American Paleoarctic left behind two distinct tool assemblages known as the Nenana and Denali complexes, which continue to fuel inquiry about the Alaska's earliest inhabitants. Denali complexes are lithic assemblages that represent a variant of the American Paleoarctic tradition found in the interior of Alaska, which are dated 12,500 to 9,000 years B.P. Archaeological sites along Cook Inlet include sites at Beluga Point, Fish Creek, and Hewitt Lake which have the earliest occupation dates in the region, with a human presence dating between 10,000 and 7,500 B.P.

The Northern Archaic period, 6,000 to 3,000 B.P. reflects subsistence strategies in upland areas. Individuals moved upriver from the coast between 3,000 to 1,500 B.P. during the Kachemak tradition. The distinct signature of the Dena'ina has been documented in the Upper Cook Inlet Basin around 1,000 B.P. and into the Late Prehistoric period.

Dena'ina Cultural Overview

Traditionally, the Dena'ina people occupied the region surrounding Cook Inlet and spoke the Athabaskan language. The Project Site lies within the K'enaht'ana territory, the indigenous people of Nuti (Knik Arm) who are members of the Eklutna Native Village. The Dena'ina are originally from the interior, but integrated marine resources into their lifestyle upon arriving to Cook Inlet. They are the most populous of the Alaska Athabaskans, and their settlement patterns are seasonally dynamic. Eklutna was a primary village on the east Knik Arm. Families would spend winters there and then move through the landscapes along the Knik Arm during warmer months. The Dena'ina use subsistence practices including gathering, fishing, and hunting. They used birch and spruce trees for food, firewood, and raw material for snowshoes, boxes, sleds etc. Peeling of bark on birch and spruce trees results in culturally modified trees. These practices are occasionally reflected in the archaeological record.

Prior to the 20th century, three societies existed among the Dena'ina people. The Village of Eklutna was in the Susitna society. Each society was distinguished by social customs and subsistence. White settlement accelerated when Anchorage was founded in 1915, with the primary focus on the Alaska Railroad. Before WWII, two military bases were established in Dena'ina land which had an influx of new people to the region. The Tribe has been included on all lists of "Alaska Native entities recognized and eligible to receive services" that have been published by the Department of the Interior, beginning with the first such list published in 1982. Most recently, NVE was included in the January 8, 2024 list of Alaska tribal entities published at 89 Fed. Reg. 944, 947.

Historic Overview

During the early contact period, Danish navigators in Russian service explored the waters between Kamchatka Peninsula and North America, where they spotted land on July 15 and 16, 1741. In 1743, the earliest phase of Russian occupation was in Alaska, in which fur-trading became popular. James Cook enticed others to enter the fur-trade. His voyage spent weeks charting the coastline, interacting with the native people, and documenting the natural environment.

Russian presence in Alaska continued to grow throughout the late 18th century, with settlements spread throughout Alaska, which left a lasting legacy on the cultural and geopolitical landscape of the region. There weren't any settlements on the Knik Arm, but a trading relationship was established with the Dena'ina in the 1790s. By the mid-19th century, Russian influence in Cook Inlet began to lessen due to depletion of resources and increases competition with American and British traders. Alaska was transferred to the United States in 1867 via the Treaty of Cession, which marked the end of Russian claims in the area. The formal transfer of Alaska occurred on October 18, 1867. After the purchase, Alaska was under military rule, which lasted for a decade. The Klondike Gold Rush of 1896 increased the population and economic activity. Other economic resources besides gold included coal, fur trading, fishing and timber. The Alaska Railroad was completed in 1923 and changed the Cook Inlet region, allowing other industries more accessibility. Two military bases near Anchorage were established due to Europe at war in 1940 for use in the defense of North America. In 2010, those bases were consolidated and are now operated as Joint Base Elmendorf-Richardson.

The Glenn Highway was established as a strategic transportation route to improve military logistics and connectivity between Anchorage and the interior regions of the State. By 1945, the majority of the road was completed and fostered post-war economic development and settlement in the region. In 1971, Eklutna, Inc. was established as part of the Alaska Native Claims Settlement Act. The native corporation represents one of 12 regional Alaska native corporations that were created to settle indigenous land claims and promote economic development. Eklutna, Inc. was granted title to approximately 90,000 acres of land in which the corporation was able to diversify their business practices while preserving cultural heritage and supporting the local native community.

Paleontological Resources

Paleontological resources are the fossilized remains of plants and animals, including vertebrates, invertebrates, and fossils of microscopic plants and animals (microfossils). The age and abundance of fossils depends on the location, topographic setting, and particular geologic formation in which they are found.

SOCIOECONOMIC CONDITIONS – EA SECTION 3.7

Regulatory Setting

Executive Order 12898

Executive Order 12898, *Federal Actions to Address Environmental Justice in Minority and Low-Income Populations*, as amended, directs federal agencies to develop an Environmental Justice Strategy that identifies and addresses disproportionately high and adverse human health or environmental effects of their programs, policies, and activities on minority populations and low-income populations. The CEQ has oversight responsibility of the federal government’s compliance with EO 12898 and NEPA. The CEQ, in consultation with the USEPA and other agencies, has developed guidance to assist federal agencies with their NEPA procedures so that environmental justice concerns are effectively identified and addressed.

The document *Final Guidance for Incorporating Environmental Justice Concerns in EPA’s NEPA Compliance Analyses* provides the following direction on how to analyze the impacts of actions on low-income and minority populations:

Under NEPA, the identification of a disproportionately high and adverse human health or environmental effect on a low-income population, minority population, or Indian tribe does not preclude a proposed agency action from going forward, nor does it necessarily compel a conclusion that a proposed action is environmentally unsatisfactory. Rather, the identification of such an effect should heighten agency attention to alternatives (including alternative sites), mitigation strategies, monitoring needs, and preferences expressed by the affected community or population. (USEPA, 1998)

As previously stated, according to guidance from the CEQ (1997) and USEPA (1998), agencies should consider the composition of the affected area, to determine whether minority populations, low-income populations, or Indian tribes are present in the area affected by a proposed action and, if so, whether there may be disproportionately high and adverse environmental effects to those populations. Communities may be considered “minority” under the executive order if one of the following characteristics apply.

- The cumulative percentage of minorities within a census tract is greater than 50 percent (primary method of analysis); or
- The cumulative percentage of minorities within a census tract is less than 50 percent, but the percentage of minorities is meaningfully greater than the minority population percentage in the general population or other appropriate unit of geographic analysis (secondary method of analysis).

According to USEPA, either the county or the state can be used when considering the scope of the “general population.” A definition of “meaningfully greater” is not given by the CEQ or USEPA, although the latter has noted that any affected area that has a percentage of minorities above the state’s percentage is a potential minority community and any affected area with a minority percentage double that of the state’s is a definite minority community under EO 12898. Communities may be considered “low-income” under the EO if one of the following characteristics applies.

- The median household income for a census tract is below the poverty line (primary method of analysis); or
- Other indications are present that indicate a low-income community is present within the census tract (secondary method of analysis).

Executive Order 14096

EO 14096, issued in April of 2023, amends certain provisions of EO 12898, and includes the following:

- Provides a broader definition of potentially disadvantaged communities.
- Explicitly expands definition of potentially disadvantaged communities to include persons with a Tribal affiliation and disabled persons;
- Requires Federal Agencies to fulfill environmental justice reporting requirements and prepare strategic plans; and
- Describes additional reporting and notification requirements related to toxic spills.

Environmental Setting

Environmental Justice Screening Tools

The U.S. Federal Government has several tools that can be used to access high-resolution environmental and demographic information for locations in the U.S. and compare their selected locations to the rest of the state, USEPA region, or the nation. These tools can help identify areas with people of color and/or low-income populations, potential environmental quality issues, or a combination of environmental and demographic indicators that are greater than usual.

TRANSPORTATION/CIRCULATION – EA SECTION 3.8

Regulatory Setting

Federal

Department of Transportation (DOT)

The mission of the DOT is to ensure a fast, safe, efficient, accessible and convenient transportation system that meets national interests and enhances quality of life.

Organizations within the DOT include the Federal Highway Administration (FHWA), the Federal Aviation Administration, the National Highway Traffic Safety Administration, the Federal Transit Administration, the Federal Railroad Administration, and the Maritime Administration. The FHWA supports State and local governments in the design, construction, and maintenance of the Nation's highway system (Federal Aid Highway Program) and various federally and tribal owned lands (Federal Lands Highway Program). Alaska Route 1 is a federal highway within the vicinity of the Project Site.

State and Local

Alaska Department of Transportation and Public Facilities (DOT&PF)

The DOT&PF is the principal agency in the state for the planning, construction, maintenance, and operation of the highway system. The DOT&PF is charged with carrying out a highway program that provides for a common defense to the United States and the state, a network of highways linking together cities and communities throughout the state (thereby contributing to the development of commerce and industry in the state and aiding the extraction and utilization of its resources), and otherwise improve the economic and general welfare of the people of the state.

Anchorage Metropolitan Area Transportation Solutions (AMATS)

AMATS is the designated metropolitan planning organization (MPO) that has overseen planning and programming of the Federal Highway Trust Fund dollars designated for the Anchorage Bowl, Chugiak, and Eagle River since 1976. The 2050 Metropolitan Transportation Plan (MTP) was released by AMATS in February 2024. The MTP is the primary tool used by AMATS to plan for long-range transportation needs and recommend solutions based on anticipated funding availability over a minimum 20-year horizon. The MTP includes the whole transportation system: streets, sidewalks and pathways, trails, public transit, freeways, highways, and freight mobility. It is also required to address congestion management for a multimodal system and air quality standards and be based on land uses described in the current Comprehensive Plan and Land Use Plans for Anchorage and Chugiak-Eagle River.

LAND USE – EA SECTION 3.9

Regulatory Setting

Federal

Farmland Protection Policy Act

The Farmland Protection Policy Act (FPPA) is intended to minimize the impact federal programs have on the unnecessary and irreversible conversion of farmland to nonagricultural uses. It assures that federal programs are administered in a matter that is compatible with state and local units of government, and private programs and policies to protect farmland (7 U.S.C. § 4201). The Natural Resource Conservation Service (NRCS) is responsible for the implementation of the FPPA and categorizes farmland in a number of ways. These categories include prime farmland, farmland of statewide importance, and unique farmland. Prime farmland is considered to have the best possible features to sustain long-term productivity. Farmland of statewide importance includes farmland similar to prime farmland, but with minor shortcomings, such as greater slopes or less ability to store soil moisture. Unique farmland is characterized by inferior soils and, depending on climate, generally needs irrigation.

The NRCS fulfills the directives of the Soil and Water Conservation Act (16 USC § 2001-2009) by identifying significant areas of concern for the protection of national resources. NRCS uses a land evaluation and site assessment system to establish a Farmland Conversion Impact Rating (FCIR) score. The FCIR is completed on form AD-1006. The FCIR form has two components: land evaluation, which rates soil quality up to 100 points, and the site assessment, which measures other factors that affect the property's viability up to 160 points.

The total FCIR score is used as an indicator for the project's sponsor to consider alternative sites if the potential adverse impacts on the farmland exceed the allowable level; however, the FPPA does not require federal agencies to alter projects to avoid or minimize farmland conversion. Sites receiving a combined score of less than 160 (out of 260 possible points) do not require further evaluation. For sites with a combined score greater than 160 points, at least two other alternatives are required to be considered and the alternative with the lowest number of points selected unless there are other overriding considerations.

Federal Aviation Regulation

In accordance with 14 CFR 77, which provides requirements, standards, and processes for determining obstructions to air navigation, the Federal Aviation Administration's (FAA's) primary objective is to promote air safety and the efficient use of the navigable airspace. In furthering this mission, the FAA conducts aeronautical studies based on information provided on FAA Form 7460-1, Notice of Proposed Construction or Alteration, by proponents of construction or development in the vicinity of airports. Developers must file Form 7460-1 with the FAA at least 45 days prior to construction if any of the following parameters are met:

- Proposed structure(s) will exceed 200 feet above ground level;
- Proposed structure(s) will be in proximity to an airport and will exceed the slope ratio;
- Proposed structure(s) involves construction of a traverseway (i.e., highway, railroad, waterway, etc.) and once adjusted upward with the appropriate vertical distance would exceed a standard of 77.9(a) or (b);
- Proposed structure(s) will emit frequencies, and do/does not meet the conditions of the FAA Colocation Policy;
- Proposed structure(s) will be in an instrument approach area and might exceed part 77 Subpart C;
- Proposed structure(s) will be in proximity to a navigation facility and may impact the assurance of navigation signal reception;
- Proposed structure(s) will be on an airport or heliport; or
- Filing has been requested by the FAA.

State and Local

Alaska Department of Transportation and Public Facilities

Alaska's Department of Transportation and Public Services (DOT&PF) is responsible for the operation and maintenance of many state-owned airports and ensure these facilities meet safety, security, and operational standards. The DOT&PF develop and implement aviation plans that guide the growth and development of airports, including updates to the Alaska Aviation System Plan (AASP) which outlines priorities and infrastructure needs. The department ensures that airport operations comply with Federal Aviation Administration (FAA) regulations and standards and coordinate with the FAA on safety inspections, certification, and funding.

The DOT&PF works to enhance airport safety and security measures. This includes maintaining infrastructure, implementing safety programs, and ensuring compliance with federal security requirements. The department enforces airport zoning regulations to manage land use around airports. These regulations are designed to prevent incompatible land uses that could affect airport operations or safety. ADOT&PF collaborates with local governments and planning agencies to integrate airport needs into broader land use and development plans and work to ensure that local zoning ordinances are compatible with airport operations.

Alaska Department of Natural Resources

The Department of Natural Resources manages agricultural land through various programs, including leasing state-owned lands for farming and ranching. They oversee the allocation and use of these lands to ensure they are utilized effectively for agricultural purposes. The department supports agricultural development through initiatives that promote farming and ranching activities. They work to enhance the productivity of agricultural lands and may provide resources or assistance to farmers and ranchers. The Department of Natural Resources develops land use plans that identify suitable areas for agricultural development. These plans help to guide the location and expansion of agricultural activities while considering environmental and community impacts. Furthermore, they ensure that agricultural practices comply with state regulations and environmental standards. This includes monitoring and managing issues related to soil conservation, water use, and land reclamation.

Anchorage Municipal Code

The Anchorage Municipal Code designates land use through a combination of zoning regulations, land use plans, and specific ordinances. These regulations are intended to guide development, ensure compatibility between land uses, and protect community interests. The Code divides Anchorage into various zoning districts, each with specific land use regulations. These districts are designed to control the type and intensity of land uses in different areas of the city. Common zoning categories include residential, commercial, industrial, and mixed-use districts. Each zoning district specifies permitted and conditional uses. Permitted uses are those allowed by right within the district, while conditional uses require additional review and approval, often through a public hearing process. Chapter 10 of the Municipal Code provides the standards and regulations for zoning and development in the Chugiak-Eagle River area where the Project Site is located.

Anchorage 2040 Land Use Plan

The Anchorage 2040 Land Use Plan is a comprehensive framework designed to guide the growth and development in Anchorage, Alaska. It addresses land use by outlining strategies to accommodate the city's projected population increase while preserving its unique character and natural environment. The plan emphasizes sustainable development, promoting a mix of residential, commercial, and recreational spaces to foster vibrant, walkable neighborhoods. It incorporates guidelines for efficient land use, such as encouraging infill development and revitalizing underutilized areas, to reduce urban sprawl. Additionally, the plan addresses infrastructure needs, transportation connectivity, and environmental conservation, aiming to create a balanced and resilient urban ecosystem that aims to foster a resilient urban environment that supports both community well-being and economic growth.

Municipality of Anchorage Ordinance No. 2020-137

The Municipality of Anchorage Ordinance No. 2020-137, enacted in December 2020, includes key provisions that directly influence the Eklutna area through the adjustments outlined in sections 1.70.010 and 1.70.020 of the Anchorage Municipal Code.

Section 1.70.010 establishes the framework for updating land use policies to better align with the city's evolving housing needs, which includes addressing development challenges in the Eklutna region. Section 1.70.020 specifically addresses the designation and utilization of land for various purposes, paving the way for increased residential density and infrastructure improvements in Eklutna. These sections aim to facilitate more effective and sustainable land use in Eklutna, promoting development that supports both affordable housing goals and broader regional growth objectives. By revising these regulatory frameworks, Ordinance No. 2020-137 helps integrate Eklutna into the city's comprehensive growth strategy, ensuring that the area can contribute to and benefit from Anchorage's future development.

Chugiak River Comprehensive Update 2006

The Chugiak River Comprehensive Plan is a strategic document designed to guide the development and preservation of the Chugiak River area, focusing on sustainable land use practices. This plan outlines a vision for balancing growth with environmental stewardship, emphasizing the need to protect natural resources while accommodating community needs.

Birchwood Community Council Bylaws

The Birchwood Community Council Bylaws serve as the governing framework for the Birchwood Community Council, outlining the structure, responsibilities, and operational procedures of the council. These bylaws establish the council's mission to represent and advocate for the interests of the Birchwood community, ensuring effective local governance and community engagement.

Birchwood Airport Master Plan

The Birchwood Airport Master Plan outlines how land adjacent to the airport will be utilized to support aviation operations while balancing the needs of the community and environmental considerations. The plan addresses how different land uses—such as commercial, industrial, and recreational—can coexist with airport activities, ensuring that growth in these areas does not negatively impact airport functions or nearby residential neighborhoods.

Environmental Setting

Surrounding Land Uses and Zoning

The Project Site is located within the unincorporated community of Chugiak within the municipality of Anchorage and is part of the Birchwood Community Council. The Project Site is zoned and designated as light-industrial and low-density residential in the Anchorage Municipal Code. The generalized zoning district for light industrial is intended for private and public industrial uses including manufacturing, storage, wholesale, and distribution operation, as well as commercial uses that support and/or are compatible with industrial uses (Municipality of Anchorage, 2017). The generalized zoning district for low density residential is intended for residential development with up to one dwelling per acre, while protecting physical and environmental features (Municipality of Anchorage, 2017). The Project Site is included in the Chugiak-Eagle River Comprehensive Plan Update revised December 2006 (Comprehensive Plan). The Project Site was not included in the Anchorage 2040 Land Use Plan. The Project Site is within the boundaries of the Birchwood Community Council, a non-profit, voluntary, self-governing association. The Comprehensive Plan identifies vacant areas as unsuitable, marginally suitable, or suitable for development, with the Project Site categorized as suitable for development. The goal of economic development in the Comprehensive Plan is to promote economic growth that utilizes the area's resources while supporting urban, suburban, and rural lifestyles and providing employment opportunities and a variety of goods and services (Comprehensive Plan, 2006).

The Project Site is bordered by Birchwood Spur Road to the west, the Alaska Railroad to the north, residential properties to the east, and commercial properties to the south. Land zoned to the west of the Project Site is zoned for heavy industrial use and contains warehouses and commercial developments. The Alaska Railroad borders the northern portion of the Project Site. Land to the north is zoned for light industrial uses and contains vacant land. Birchwood Airport is located to the northwest of the Project Site and discussed further below. Land to the east of the Project Site is zoned for low density residential and contains residential and vacant land. Land to the southwest is zoned for light industrial and contains commercial developments, and land to the southeast is zoned for low density residential and contains single family homes.

The nearest airport is Birchwood Airport located approximately 750-feet northwest of the Project Site. The airport is within the jurisdiction of the Anchorage Department of Transportation, which manages municipal airports and transportation infrastructure in the area. The Project Site is within an area of height restriction as delineated by the airport height zoning map prepared February 8, 1984 by the State's Department of Transportation and Public Facilities and adopted for the Municipality of Anchorage designations. The Municipality establishes airport height limitations to be no greater than thirty-five feet above ground elevation as outlined in Title 21, Chapter 21.65.

The Project Site is bordered to the north by the Alaska Railroad, which runs in an east-west direction. The Alaska Railroad is a state-owned railroad system that provides freight and passenger services. The railroad specializes in freight services related to lumber, heavy machinery, rebar, pipe, and hazardous material (Alaska Railroad, 2020). A 1.68-acre easement for the railroad occurs along the northern portion of the Project Site. This easement allows railroad personnel to access the area for construction, operation, and maintenance activities.

Agriculture

The U.S. Department of Agriculture (USDA) conducts a state-by-state census of agriculture every five years. The National Agriculture Statistical Service collects census data from a list of all known potential agriculture operators. The census reports on various statistics relating to crop yields, farm acreage, and farm economics. According to the 2022 Census of Agriculture, a total of 39,532 acres in the Anchorage area are used for farming purposes (USDA, 2022).

PUBLIC SERVICES AND UTILITIES – EA SECTION 3.10

Regulatory Setting

Federal

Safe Drinking Water Act and Clean Water Act

See **Water Resources – Section 3.3 of the EA** above.

Public Law 280

Public Law 280 was enacted in 1953 to grant certain states criminal jurisdiction over Indians on reservations in addition to permitting civil litigation under tribal or federal court jurisdiction to be handled by state courts.

The states mandated to assume criminal and civil jurisdiction over federal Indian lands are Alaska, California, Minnesota, Nebraska, Oregon, and Wisconsin, although certain tribal lands are exempt, including Metlakatla Indian Community on the Annette Island Reserve, Red Lake Reservation, and Warm Springs Reservation. In addition to these states, other states elected to assume full or partial responsibility, including Arizona, Florida, Idaho, Iowa, Montana, Nevada, North Dakota and Utah. The federal government relinquished all special criminal jurisdictions over Indian offenders and victims in these states. However, Public Law 280 does not grant states the following regulatory powers over lands held in federal trust or tribes:

- Federally guaranteed fishing, tribal hunting, and trapping rights;
- Fundamental tribal governmental functions, such as domestic relations and tribal enrollment; and
- Authority to impose state taxes.

Due to the one-sided process that imposed state jurisdiction on tribes and the failure to recognize tribal sovereignty and tribal self-determination, Public Law 280 was opposed by Indian Nations from its enactment. Subsequent acts of Congress, court decisions, and state actions to retrocede (or give back) jurisdiction back to the federal government have mitigated some of the effects of the 1953 law and strengthened tribes' jurisdiction over civil and criminal matters on their reservations.

State and Local

Alaska State Regulations

Alaska State Regulations contains several regulations relating to utilities and public services. AS 42 houses regulations related to utilities, including cost for service, prohibition against acts damaging utilities, relocation of utilities to accommodate development, and establishment of utility boards. AS 13 AAC 85 houses regulations related to law enforcement, including basic standards for police officers and training requirements. Several state regulations are also applicable to emergency medical services, including emergency care service requirements, personnel training and facility licensing requirements, medical transport standards, and others.

Municipality of Anchorage Municipal Code

The Municipality of Anchorage Municipal Code contains several regulations relating to utilities and public services. Title 26 houses utility-related codes, including regulations related to utility costs, dedication of easements, inspections, construction coordination, and general rules and provisions. Title 29 specifically identifies regulations related to public schools. Additionally, Title 16 identifies regulations related to emergency response, including designation of emergency response vehicles and responsibilities of municipal emergency medical service personnel (Chapter 16.95). Several regulations exist related to law enforcement personnel, including fees for police services and general authorities of emergency response personnel.

NOISE – EA SECTION 3.11

Regulatory Setting

Federal Highway Administration Construction Noise Abatement Criteria

The Federal Highway Administration (FHWA) Construction Noise Handbook (2006) provides guidance with respect to the development of construction noise level thresholds. Based on that guidance and estimated ambient noise levels in the Project Site vicinity, the criteria in **Table 5** were developed for use in evaluating the significance of construction noise impacts.

Table 5: Federal Construction Noise Thresholds

Noise Receptor Locations and Land Uses	Daytime (7 am – 6 pm)	Nighttime (10 pm – 7 am)
Noise-Sensitive Locations (residences, institutions, hotels, etc.)	90 L _{max}	80 L _{max}
Commercial Areas (businesses, offices, stores, etc.)	None	None
Industrial Areas (factories, plants, etc.)	None	None

Source: FHWA, 2006

Federal Noise Abatement Criteria

23 CFR 772 establishes Noise Abatement Criteria (NAC) for various land uses that have been categorized based upon activity. The FHWA NAC is based on noise generated from peak traffic hour noise levels, and land uses are categorized based on their sensitivity to noise as indicated in **Table 6**. Sensitive receptors with the potential to be impacted by the project alternatives primarily consist of residential land uses; thus, the Category B noise standard (67 dBA L_{eq}) would apply to those uses.

Vibration Standards

The effects of groundborne vibrations typically cause only a nuisance to people, but at extreme vibration levels, damage to buildings may occur. Although groundborne vibration can be felt outdoors, it is typically an annoyance only indoors, where the associated effects of the building shaking can be notable. Groundborne noise is an effect of groundborne vibration and only exists indoors since it is produced from noise radiated from the motion of the walls and floors of a room and may consist of the rattling of windows or dishes on shelves.

Peak particle velocity (PPV) is often used to measure vibration. PPV is the maximum instantaneous peak (inches per second) of the vibration signal. The PPV levels are used to estimate L_v or VdB levels (vibration decibels with a reference velocity of one micro-inch per second). Scientific studies have shown that human responses to vibration vary by the source of vibration, which is either continuous or transient. Continuous sources of vibration include construction while transient sources include truck movements. Generally, the thresholds of perception and annoyance are higher for transient sources than for continuous sources. **Table 7** summarizes the Federal Transit Administration's (FTA) guideline vibration damage criteria for various structural categories. As shown therein, buildings extremely susceptible to vibration damage could be damaged if vibration levels exceed 90 VdB. Additionally, although humans have a perceptibility threshold of 65 VdB, the threshold for irritation does not occur until approximately 80 VdB (FTA, 2018).

Table 6: Federal Noise Abatement Criteria Hourly A-Weighted Sound Level Decibels¹

Activity Category	Activity Criteria Leq (h), dBA	Evaluation Location	Activity Category Description
A	57	Exterior	Lands on which serenity and quiet are of extraordinary significance and serve an important public need and where the preservation of those qualities is essential if the area is to continue to serve its intended purpose.
B	67	Exterior	Residential.
C	67	Exterior	Active sport areas, amphitheaters, auditoriums, campgrounds, cemeteries, daycare centers, hospitals, libraries, medical facilities, parks, picnic areas, places of worship, playgrounds, public meeting rooms, public or nonprofit institutional structures, radio studios, recording studios, recreation areas, Section 4(f) sites, schools, television studios, trails and trail crossings.
D	52	Interior	Auditoriums, daycare centers, hospitals, libraries, medical facilities, places of worship, public meeting rooms, public or non-profit institutional structures, radio studios, recording studios, schools, and television studios.
E ¹	72	Exterior	Hotels, motels, offices, restaurants/bars, and other developed lands, properties or activities not included in A–D or F.
F	–	–	Agriculture, airports, bus yards, emergency services, industrial, logging, maintenance facilities, shipyards, utilities (water resources, water treatment, electricity), and warehousing.
G	–	–	Undeveloped lands that are not permitted.

Source: 23 CFR 772

¹ Includes undeveloped lands permitted for this activity category.

Table 7: Construction Vibration Damage Criteria

Building Category	Approximate PPV (in/sec)	Approximate Lv (VdB)
Reinforced-concrete, steel, or timber (no plaster)	0.5	102
Engineered concrete and masonry (no plaster)	0.3	98
Non-engineered timber and masonry buildings	0.2	94
Buildings extremely susceptible to vibration damage	0.12	90

Source: FTA, 2018

State and Local

Anchorage Municipal Noise Ordinance

The primary purpose of the Chapter 15.70 Noise Control ordinance is to protect public health and welfare by regulating and minimizing excessive noise and vibration within the municipality. It establishes detailed noise level limits for various settings (residential, commercial, industrial) and times of the day to prevent noise disturbances and protect public health and quality of life. It also includes provisions for different sources of noise such as construction sites, motor vehicles, and public activities, outlining permissible noise levels and operational restrictions during specified times. Examples of key sections are provided below:

- 15.70.060 Prohibited acts and conditions.
 - 3. Construction. No person shall operate or cause the operation of any tools or equipment used in construction, drilling, repair, alteration, or demolition.
 - a) So that the sound therefrom creates a noise disturbance across a residential real property boundary or within a noise-sensitive zone between the hours of 10:00 pm and 6:00 am during the construction season, between the hours of 10:00 pm and 7:00 am during other months, or at any time on Sundays or state holidays;
 - b) So that the sound level therefrom exceeds an Leq of 80 dB(A) at or within a residential real property boundary or within a noises sensitive zone during any one hour of the daily period from 6:00 am to 10:00 pm during the construction season or from 7:00 am to 10:00 pm during other months; or
 - c) So that the sound level therefrom exceeds an Leq of 80 dB(A) at or within a commercial or industrial real property boundary during any one hour of the daily period from 6:00 am to 10:00 pm during the construction season or from 7:00 am to 10:00 pm during other months.
 - 7. Loading and unloading. No person shall load, unload, open, close or otherwise handle boxes, crates, containers, building materials, garbage cans or similar objects between the hours of 10:00 pm and 7:00 am the following day in such a manner as to cause a noise disturbance across a residential real property boundary or within a noise-sensitive zone.
 - 11. Noise-sensitive zones. No person shall create or cause the creation of any sound within any noise-sensitive zone designated pursuant to Section 15.70.040.A.7 so as to disrupt the activities normally conducted within the zone, provided that conspicuous signs are displayed indicating the presence of the zone. No person shall create or cause the creation of any sound within any noise-sensitive zone designated pursuant to Section 15.70.040.A.7 containing a hospital, nursing home or similar activity so as to interfere with the functions of such activity or disturb or annoy the patients in the activity, provided that conspicuous signs are displayed indicating the presence of the zone.
 - 19. Vibration. No person shall operate or cause the operation of any device that produces low-frequency, inaudible sound that creates vibration above the vibration perception threshold of any individual within a residential real property boundary or within a noise-sensitive zone between the hours of 10:00 pm and 7:00 am the following day.
- 15.70.080 Property line noise emission standards.
 - A. Except as otherwise provided in subsections B and C of this section, no person shall operate or cause to be operated on private property any source of sound in such a manner as to create a sound level that exceeds limits set forth for the receiving land use category in **Table 8** when measured at or within the property boundary of the receiving land use.
 - C. The provisions of subsections A and B of this section shall not apply to the unamplified human voice and activities covered by Sections 15.70.060.B.1, pertaining to aircraft and airport operations, 15.70.060.B.3, pertaining to construction, 15.70.060.B.4, pertaining to domestic power tools, 15.70.060.B.5, pertaining to emergency signaling devices, 15.70.060.B.6, pertaining to explosives, firearms and similar devices, 15.70.060.B.9, pertaining to motorboats, 15.70.060.B.14, pertaining to public service utilities, 15.70.060.B.16, pertaining to stationary nonemergency signaling devices, 15.70.090.A, pertaining to motor vehicles operating on public rights-of-way, 15.70.090.D, pertaining to refuse collection vehicles, and 15.70.090.E, pertaining to snow removal vehicles on public rights-of-way.

Table 8: Sound Levels by Receiving Land Use

Receiving Land Use Category	Time	Sound Level Limit (dB(A))
Residential area	7:00 am--10:00 pm	60
	10:00 pm-- 7:00 am	50
Commercial area	7:00 am--10:00 pm	70
	10:00 pm-- 7:00 am	60
Industrial area	At all times	80

Environmental Setting

Acoustical Background and Terminology

Acoustics is the science of sound. Sound may be thought of as mechanical energy of a vibrating object transmitted by pressure waves through a medium to human (or animal) ears. If the pressure variations occur frequently enough (at least 20 times per second), then they can be heard and are called sound. The number of pressure variations per second is called the frequency of sound and is expressed as cycles per second or Hertz (Hz).

Noise is a subjective reaction to different types of sounds. Noise is typically defined as (airborne) sound that is loud, unpleasant, unexpected or undesired, and may therefore be classified as a more specific group of sounds. Perceptions of sound and noise are highly subjective from person to person.

Measuring sound directly in terms of pressure would require a very large and awkward range of numbers. To avoid this, the decibel scale was devised. The decibel scale uses the hearing threshold (20 micropascals), as a point of reference, defined as 0 dB. Other sound pressures are then compared to this reference pressure, and the logarithm is taken to keep the numbers in a practical range. The decibel scale allows a million-fold increase in pressure to be expressed as 120 dB, and changes in levels (dB) correspond closely to human perception of relative loudness.

The perceived loudness of sounds is dependent upon many factors, including sound pressure level and frequency content. However, within the usual range of environmental noise levels, perception of loudness is relatively predictable, and can be approximated by A-weighted sound levels. There is a strong correlation between A-weighted sound levels (expressed as dBA) and the way the human ear perceives sound. For this reason, the A-weighted sound level has become the standard tool of environmental noise assessment.

The decibel scale is logarithmic, not linear. In other words, two sound levels 10-dB apart differ in acoustic energy by a factor of 10. When the standard logarithmic decibel is A-weighted, an increase of 10-dBA is generally perceived as a doubling in loudness. For example, a 70-dBA sound is half as loud as an 80-dBA sound, and twice as loud as a 60-dBA sound.

Community noise is commonly described in terms of the ambient noise level, which is defined as the all-encompassing noise level associated with a given environment. A common statistical tool is the average, or equivalent, sound level (L_{eq}), which corresponds to a steady-state A-weighted sound level containing the same total energy as a time varying signal over a given time period (usually one hour). The L_{eq} is the foundation of the composite noise descriptor, L_{dn} , and shows very good correlation with community response to noise.

The day/night average level (DNL or L_{dn}) is based upon the average noise level over a 24-hour day, with a +10-decibel weighing applied to noise occurring during nighttime (10:00 pm to 7:00 am) hours. The nighttime penalty is based upon the assumption that people react to nighttime noise exposures as though they were twice as loud as daytime exposures. Because L_{dn} represents a 24-hour average, it tends to disguise short-term variations in the noise environment. **Table 9** lists several examples of the noise levels associated with common situations.

Table 9: Typical Noise Levels

Common Outdoor Activities	Noise Level (dBA)	Common Indoor Activities
	110	Rock Band
Jet Fly-over at 300 m (1,000 ft.)	100	
Gas Lawn Mower at 1 m (3 ft.)	90	
Diesel Truck at 15 m (50 ft.), at 80 km/hr. (50 mph)	80	Food Blender at 1 m (3 ft.) Garbage Disposal at 1 m (3 ft.)
Noisy Urban Area, Daytime Gas Lawn Mower, 30 m (100 ft.)	70	Vacuum Cleaner at 3 m (10 ft.)
Commercial Area Heavy Traffic at 90 m (300 ft.)	60	Normal Speech at 1 m (3 ft.)
Quiet Urban Daytime	50	Large Business Office Dishwasher in Next Room
Quiet Urban Nighttime	40	Theater, Large Conference Room (Background)
Quiet Suburban Nighttime	30	Library
Quiet Rural Nighttime	20	Bedroom at Night, Concert Hall (Background)
	10	Broadcast/Recording Studio
Lowest Threshold of Human Hearing	0	Lowest Threshold of Human Hearing

Source: Caltrans, Technical Noise Supplement, Traffic Noise Analysis Protocol. September 2013

The effects of noise on people can be placed into three categories:

- Subjective effects of annoyance, nuisance, dissatisfaction
- Interference with activities such as speech, sleep, and learning
- Physiological effects such as hearing loss or sudden startling

Environmental noise typically produces effects in the first two categories. Workers in industrial plants can experience noise in the last category. There is no completely satisfactory way to measure the subjective effects of noise or the corresponding reactions of annoyance and dissatisfaction. A wide variation in individual thresholds of annoyance exists and different tolerances to noise tend to develop based on an individual's past experiences with noise. Thus, an important way of predicting a human reaction to a new noise environment is the way it compares to the existing environment to which one has adapted: the so-called ambient noise level. In general, the more a new noise exceeds the previously existing ambient noise level, the less acceptable the new noise will be judged by those hearing it. With regards to increases in A-weighted noise level, the following relationships occur:

- Except in carefully controlled laboratory experiments, a change of 1-dBA cannot be perceived;
- Outside of the laboratory, a 3-dBA change is considered a just-perceivable difference;
- A change in level of at least 5-dBA is required before any noticeable change in human response would be expected; and

- A 10-dBA change is subjectively heard as approximately a doubling in loudness and can cause an adverse response.

Stationary point sources of noise – including stationary mobile sources such as idling vehicles – attenuate (lessen) at a rate of approximately 6-dB per doubling of distance from the source, depending on environmental conditions (i.e. atmospheric conditions and either vegetative or manufactured noise barriers, etc.). Widely distributed noises, such as a large industrial facility spread over many acres or a street with moving vehicles, would typically attenuate at a lower rate.

Vibration Background and Terminology

Vibration is like noise in that it involves a source, a transmission path, and a receiver. While vibration is related to noise, it differs in that noise is generally considered to be pressure waves transmitted through air, whereas vibration usually consists of the excitation of a structure or surface. As with noise, vibration consists of an amplitude and frequency. A person’s perception to the vibration will depend on their individual sensitivity to vibration, as well as the amplitude and frequency of the source and the response of the system which is vibrating. Vibration can be measured in terms of acceleration, velocity, or displacement. A common practice is to monitor vibration measures in terms of peak particle velocities in inches per second. Standards pertaining to perception as well as damage to structures have been developed for vibration levels defined in terms of peak particle velocities. Human and structural response to different vibration levels is influenced by a number of factors, including ground type, distance between source and receptor, duration, and the number of perceived vibration events. Table 10 shows the vibration levels which would normally be required to result in damage to structures. The vibration levels are presented in terms of peak particle velocity in inches per second. The threshold for architectural damage to structures is 0.20 in/sec p.p.v. A threshold of 0.20 in/sec p.p.v. is considered to be a reasonable threshold for short-term construction projects.

Table 10: Effects of Vibration on People and Buildings

Peak Particle Velocity (mm/second)	Peak Particle Velocity (in/second)	Human Reaction	Effect on Buildings
0.15 – 0.20	0.006 – 0.019	Threshold of perception; possibility of intrusion	Vibrations unlikely to cause damage of any type
2.0	0.08	Vibrations readily perceptible	Recommended upper level of the vibration to which ruins and ancient monuments should be subjected
2.5	0.10	Level at which continuous vibrations begin to annoy people	Virtually no risk of “architectural” damage to normal buildings
5.0	0.20	Vibrations annoying to people in buildings (this agrees with the levels established for people standing on bridges and subjected to relative short periods of vibrations)	Threshold at which there is a risk of “architectural” damage to normal dwelling-houses with plastered walls and ceilings. Special types of finish such as lining of walls, flexible ceiling treatment, etc., would minimize “architectural” damage
10 - 15	0.4 – 0.6	Vibrations considered unpleasant by people subjected to continuous vibrations and unacceptable to some people walking on bridges	Vibrations at a greater level than normally expected from traffic, but would cause “architectural” damage and possibly minor structural damage

Source: Transportation Related Earthborne Vibrations. Caltrans. TAV-02-01-R9601. February 20, 2002

Sensitive Receptors

Some land uses are considered more sensitive to noise than others due to the amount of noise exposure (in terms of both exposure duration and insulation from noise) and the types of activities typically involved. Residences, schools, libraries, hospitals, and passive recreation areas generally are more sensitive to noise than commercial or industrial land uses. Sensitive noise receptors may also include threatened or endangered noise-sensitive biological species. Noise sensitive land uses are typically given special attention in order to achieve protection from excessive noise.

HAZARDOUS MATERIALS AND HAZARDS – EA SECTION

3.12

Regulatory Setting

Federal

Resource Conservation and Recovery Act

The Resource Conservation and Recovery Act (RCRA) regulates the land disposal of hazardous materials from cradle-to-grave. This means establishing a regulatory framework for the generation, transport, treatment, storage and disposal of hazardous waste. Specifically, Subtitle D of RCRA pertains to non-hazardous solid waste and Subtitle C focuses on hazardous solid waste. A solid waste can consist of solids, liquids and gases, but these must be discarded in order to be considered waste. Additionally, the USEPA has developed regulations to set minimum national technical standards for how disposal facilities should be designed and operated. States issue permits to ensure compliance with USEPA and state regulations. The regulated community is comprised of a diverse group that must comprehend and adhere to RCRA regulations. These groups can consist of hazardous waste generators, government agencies, small businesses, and gas stations with underground petroleum tanks.

Food, Drug, and Cosmetic Act

Under the federal Food, Drug, and Cosmetic Act, the USEPA sets maximum residue limits, or tolerances, for pesticides residues on food. When the USEPA sets a tolerance level for a food, this is the level deemed safe. In defining safe, this means that, “reasonable certainty that no harm will result from aggregate exposure to the pesticide residue.” When determining a safety finding for a tolerance level, the USEPA considers the toxicity of the pesticide and its break-down products, aggregate exposure to the pesticide in foods and from other sources of exposure if applicable, and any special risks specific to infants and children. If a tolerance is not set for a pesticide residue, a food containing that pesticide residue will be subject to government seizure if deemed appropriate. However, once a tolerance has been established for a pesticide residue, then residue levels below the tolerance will not trigger enforcement actions. If the residue level is detected above that tolerance, then the commodity will be subject to seizure. Some pesticides do not have a set tolerance level as the USEPA may grant exemptions in the cases where the pesticide residue does not pose, under foreseeable situations, a significant dietary risk.

Insecticide, Fungicide, and Rodenticide Act

The federal Insecticide, Fungicide, and Rodenticide Act (FIFRA) addresses the sale, distribution, and labeling of pesticides, as well as the certification and training of pesticide applicators. FIFRA establishes recordkeeping and reporting requirements on certified applicators of restricted use pesticides.

Furthermore, FIFRA imposes storage, disposal, and transportation requirements on registrants and applicants for the registration of pesticides. Pesticide use is regulated through requirements to apply pesticides in a manner consistent with the label. The labeling requirement includes directions for use, warnings, and cautions along with the uses for which the pesticide is registered (e.g., pests and appropriate applications). This includes the specific conditions for the application, mixture, and storage of the pesticide. Additionally, the label must specify a time period for re-entry into an area after the pesticide has been applied, and when crops may be harvested after the application of the pesticide. If a pesticide is used in a manner contrary to specifics on its label, then the use constitutes a violation of the FIFRA.

Hazardous Communication Standard

The Occupational Safety and Health Administration helps ensure employee safety by regulating the handling and use of chemicals in the workplace. For instance, it administers the Hazard Communication Standard (HCS). The HCS ensures safety in the workplace concerning chemicals through requiring information to be provided and understood by workers about the identity and hazards associated with chemicals they may work with. This also requires that chemical manufactures and importers evaluate the hazards associated with the chemicals they create or import, and that these chemicals have proper labels and material safety data sheets concerning their hazards to others (e.g., customers). Downstream of the production, employers who utilize these hazardous chemicals in their workplaces are obligated to have labels and safety data sheets for their workers and to train them on the proper handling of these chemicals.

Hazardous Substances Act

The Consumer Product Safety Commission has a limited role in regulating hazardous substances; it primarily deals with the labeling of consumer products through the federal Hazardous Substances Act (HSA). HSA only requires products that may at some point be in the presence of people's dwellings to be labeled, including during purchase, storage, or use. These labels must alert consumers of the potential hazards that the product may pose. However, in order for a product to be required for labelling, the product must be toxic, corrosive, flammable/combustible, an irritant, a strong sensitizer, or have the ability to generate pressure through decomposition, heat, or other means. Furthermore, the product must possess the ability to cause severe personal injury or substantial illness during or as a result of any customary or reasonably predictable handling or use, including reasonably foreseeable ingestion by children.

Toxic Substances Control Act

The federal Toxic Substances Control Act (TSCA), as amended by the Frank R. Lautenberg Chemical Safety for the 21st Century Act, permits the USEPA to evaluate the potential risk from novel and existing chemicals and address unacceptable risks chemicals may have on human health and the environment. The USEPA oversees the production, importation, use, and disposal of certain chemicals. This includes the USEPA having the authority to require record keeping, reporting, and test requirements and restrictions associated with certain chemical substances and/or mixtures. However, certain groups of chemicals are excluded from TSCA consideration, including—but not limited to—food, drugs, cosmetics and pesticides. Examples of chemicals included in TSCA consideration are lead paint, asbestos, mercury, formaldehyde, and polychlorinated biphenyls.

Emergency Planning and Community Right-to-Know Act

The federal Emergency Planning and Community Right-to-Know Act (EPCRA) is designed to assist local communities protect public health, safety, and the environment from chemical hazards. The Community Right-to-Know provisions help increase the public's knowledge and access to information on chemicals at individual facilities, their uses, and releases into the environment. The EPCRA also requires industry to report on the storage, usage, and releases of hazardous substances to federal, state, and local governments, and states and communities can use the information gained to improve chemical safety and protect public health and the environment.

National Fire Protection Association Codes and Standards

The National Fire Protection Association (NFPA) publishes more than 300 consensus codes and standards intended to minimize the possibility and effects of fire and other risks, including, but not limited to:

- NFPA 13 Standard for the Installation of Sprinkler Systems
- NFPA 72 National Fire Alarm and Signaling Code
- NFPA 88A Standard for Parking Structures
- NFPA 1660 Standard for Emergency, Continuity, and Crisis Management: Preparedness, Response, and Recovery
- NFPA 1140 Standard for Wildland Fire Protection

State and Local

Alaska Statutes

The Alaska Statutes, particularly Title 41, Chapter 15, address various aspects of wildfire prevention, management, and response with the following being example key provisions:

- **Protection of Forested Land (AS 41.15.010 through 41.15.170):** This section outlines the state's efforts to protect forested lands from wildfires and other destructive forces. It mandates measures to prevent, manage, and suppress wildfires and establishes the responsibilities of landowners in these efforts.
- **Fire Suppression Fund (AS 41.15.200 through 41.15.240):** This section creates a special fund within the state treasury, designated specifically for fire suppression activities. The fund is used by the Department of Natural Resources to cover the costs associated with wildfire suppression.
- **Permits and Fire Seasons (AS 41.15.050 and AS 41.15.060):** These statutes establish the annual fire season from April 1 to August 31, during which additional precautions are required. Permits are necessary for activities that involve fire or that increase fire risk during this period.
- **Enforcement Authority (AS 41.15.950):** This statute grants enforcement authority to designated peace officers to ensure compliance with wildfire regulations. These officers have the power to arrest individuals who violate wildfire laws and issue citations.

In addition, the Alaska Statutes regulate hazardous materials, focusing on handling, reporting, and response to ensure public and environmental safety with the following being examples of key statutes:

- **Reporting Requirements (AS 29.35.500 through 590):** This section mandates businesses and government agencies handling hazardous chemicals, materials, or wastes to submit regular inventories and reports. It includes specific reporting thresholds for various hazardous substances, such as compressed gases, flammable solids, and extremely hazardous substances.

- **Municipal Responsibilities (AS 29.35.530):** Municipalities that establish hazardous material programs have the authority to enforce reporting requirements and coordinate emergency responses. They can also impose penalties for non-compliance.
- **Placarding (AS 18.70.310):** Businesses handling hazardous materials are required to post warning placards that follow the National Fire Protection Association (NFPA) standards. These placards provide essential information to the public and emergency responders.
- **Title 46: Water, Air, Energy, and Environmental Conservation:** Establishes the legal framework for managing hazardous materials and responding to environmental hazards, such as oil spills and other dangerous substances. Specifically:
 - **AS 46.04** addresses oil and hazardous substance pollution control, including the establishment of contingency plans and the creation of response mechanisms for hazardous material incidents.
 - **AS 46.08** creates the Oil and Hazardous Substance Release Response Fund, which finances the state's efforts in spill prevention, preparedness, and response, including the coordination of HAZMAT response teams. Furthermore, HAZMAT Response Programs fall primarily under Title 46. Alaska has established several statewide programs to manage hazardous material spills and emergencies, such as the Statewide Hazardous Materials Response Team that is crucial for coordinating responses during significant hazmat incidents.

Alaska Interagency Fire Management Plan

Alaska's fire management strategy is coordinated through the Alaska Interagency Fire Management Plan, which provides a framework for wildfire suppression and management across the state's 375 million acres. The plan divides Alaska into four fire protection categories—Critical, Full, Modified, and Limited—to prioritize fire suppression efforts based on the value of resources at risk and fire management objectives. Critical Protection areas, such as population centers and critical infrastructure, receive the highest priority for fire suppression, while Limited Protection zones, which often encompass remote or wilderness areas, allow fires to burn naturally unless they threaten specific resources. The Project Site is located within a Critical Protection zone, which prioritizes aggressive fire suppression efforts due to the presence of population centers, key infrastructure, and high-value resources. This designation ensures that any wildfire in the vicinity of the project site will be met with the highest level of suppression response to protect life, property, and essential services. (Grabinski, Z. & H. R. McFarland, 2019).

The implementation of this plan involves close collaboration among several federal and state agencies, including the Bureau of Land Management Alaska Fire Service, the Alaska Division of Forestry, and the U.S. Forest Service. These agencies coordinate fire suppression efforts under the Alaska Master Cooperative Wildland Fire Management and Stafford Act Agreement to ensure efficient resource allocation and response across the state. Additionally, the Alaska Wildland Fire Coordinating Group oversees statewide interagency fire management planning, while the Alaska Interagency Coordination Center serves as the central hub for all fire management and suppression activities in Alaska (Grabinski, Z. & H. R. McFarland, 2019).

VISUAL RESOURCES – EA SECTION 3.13

Regulatory Setting

Federal

Wild and Scenic Rivers Act

The Wild and Scenic Rivers Act of 1968 is a federal law that was established to protect selected rivers in the United States that have outstandingly remarkable scenic, recreational, geologic, fish and wildlife, historic, cultural, or other similar values. The Act preserves the unique character of these rivers while also acknowledging their potential for appropriate use and development. It encourages river management that crosses political boundaries and promotes public participation in developing goals for river protection. The National Wild and Scenic Rivers System was created by the Wild and Scenic Rivers Act. River units designated as part of the system are classified and administered three types based on the condition of the river, the amount of development in the river or on the shorelines, and the degree of accessibility by road or trail at the time of designation:

- Wild River Areas: These rivers or sections of rivers are free of impoundments and generally inaccessible except by trail, with watersheds or shorelines essentially primitive and waters unpolluted. These represent vestiges of primitive America.
- Scenic River Areas: These rivers or sections of rivers are free of impoundments, with shorelines or watersheds still largely primitive and shorelines largely undeveloped, but accessible in places by roads.
- Recreational River Areas: These rivers or sections of rivers are readily accessible by road or railroad, may have some development along their shorelines, and may have undergone some impoundment or diversion in the past.

Typically, rivers are added to the system by an act of Congress, but they may also be added by state nomination with the approval of the Secretary of the Interior. Congress initially designated 789 miles of eight rivers as part of the system. Today there are 208 river units with 12,708.8 miles in 40 states and Puerto Rico, administered by federal agencies or by state, local, or tribal governments. Federal agencies are typically the National Park Service, the Bureau of Land Management, the Forest Service, or the Fish and Wildlife Service.

State and Local

Department of Transportation and Public Facilities

The DOT&PF manages scenic and visual resources through a combination of policies, planning processes, and guidelines designed to preserve and enhance the natural beauty of Alaska while accommodating transportation needs. Alaska DOT&PF designates and manages scenic byways to highlight and protect visually significant routes. These programs aim to enhance travelers' experiences while ensuring that transportation infrastructure integrates well with the natural environment.

Anchorage Municipal Code

The Anchorage Municipal Code addressing visual resources by providing guidelines and policies aimed at preserving and enhancing the city's aesthetic and scenic qualities. These policies focus on protecting significant views, maintaining the character of neighborhoods, and ensuring new developments are visually harmonious with their surroundings.

The Municipal Code includes zoning ordinances that regulate the use and appearance of properties within different zones. Some areas of Anchorage are designated for their scenic value, and the AMC includes provisions to protect these views. For example, there may be restrictions on building heights or types of development that could obstruct important scenic vistas.

Additionally, to prevent visual clutter and maintain aesthetic quality, the code has guidelines for signage, including size, placement, and illumination. These regulations are designed to ensure that signs are compatible with their surroundings and do not detract from the visual environment. There are also provisions for the preservation of historic and culturally significant sites, which can include visual and scenic aspects. These regulations help protect landmarks and areas of historical value from inappropriate alterations that could impact their visual integrity.

Chapter 21.12 SIGNS

21.12.070 - Signs in the commercial, industrial, downtown (DT), turnagain arm (TA), transition (TR), and airport (A) districts. The purpose of this chapter is to promote the public health, safety, and welfare by establishing standards and criteria for the construction, installation, maintenance, and operation of signs in the municipality, which are subject to the provisions of this chapter. Some excerpts are included below.

A. *Applicability.* Signs in the commercial and industrial districts, as well as the downtown (DT), transition (TR), and airport (A) districts shall conform to the standards set forth in this section. The standards of this section apply only to the nonresidential parcels in the TA district.

B. *Basic standards for building signs.*

1. *Maximum area of permanent building signs.* Building signs shall conform to the maximum area limitations set forth in Table 21.12-5.

2. *Placement of building signs.*

a. The building signs permitted in Table 21.12-5 may be placed on the wall, awnings, canopies, parapets, or be a projecting sign in compliance with subsection 2.b. below.

b. Projecting signs shall be limited to occupants that have a minimum of ten feet of occupant building frontage provided that:

i. All projecting signs shall have a minimum clearance of eight feet from the ground to the bottom of the sign. A projecting sign may be a minimum of six feet from the ground when it is located above a landscaped area or other area that does not permit pedestrian traffic beneath the sign.

ii. Projecting signs shall be placed on the building so that the signs are intended to be viewed by the pedestrians on the abutting street or pedestrian way.

c. Except in a commercial development as described in subsection 1.d. below, no more than two freestanding signs are allowed along any one frontage. Freestanding signs must be separated by a minimum distance of 150 feet. On corner lots, the property frontage on both streets shall be considered when measuring the separation.

d. If a commercial development comprises more than a single platted lot, and one or more of the platted lots or commercial tract lots of the commercial development has frontage on a classified street of class III or greater, then each platted lot or commercial tract lot having a building and frontage of 100 feet or more on the class III or greater street shall be permitted a freestanding, monument style sign, subject to the limitations of this subsection. In addition, if a building central to the commercial development has frontage of less than 100 feet on the class III or greater street, then one freestanding, monument style sign adjacent to each entrance to the property from the class III or greater street shall be permitted subject to the limitations of this subsection. The height limitation shall be eight feet, excluding the base; the total area of each sign shall not exceed 120 square feet; pole signs are prohibited; and each sign shall comply with all other requirements of this chapter. Signs permitted by this subsection are in addition to all other signage within the commercial development permitted by this chapter including freestanding signs along frontage on classified streets of less than class III and building signs.

G. *Menu boards and ordering screens.* In addition to other allowed building or freestanding signs, restaurants with accessory drive-through service and food and beverage kiosks may have menu boards, ordering screens, and clearance bars.

H. *Unified sign plan.* To recognize and accommodate irregular site shapes (which are typically characterized by narrow lot frontages resulting in some buildings with large setbacks and limited visibility to a public street) multiple contiguous lots and/or tracts may be considered as a single site for the purposes of determining the size, number, and placement of freestanding signs permitted pursuant to this section. Solely for the purposes of this section:

1. The number and area of the freestanding signs permitted, pursuant to Table 21.12-6, shall be determined based on size and frontage of the multiple properties being considered as a single parcel.
2. After a unified sign plan has been approved by the municipality and a permit has been issued, the sign rights or limitations shall be recorded with the state district recorder's office as a deed restriction.
3. Revocation of a unified sign plan must be approved by all participants in the plan; all signs on the affected properties must be in compliance with this code before the plan can be revoked.
4. This subsection shall not be interpreted as authorizing the erection or maintenance of any sign or display within 660 feet of the nearest edge of the right-of-way and visible from the main-traveled way of an interstate, primary, or secondary highway, or the erection or maintenance of any sign or display beyond 660 feet of the nearest edge of the right-of-way of the main traveled way of an interstate, primary, or secondary highway with the purpose of the message displayed being read from that travel way, in a manner that would conflict with the provisions of AS 19.25.075—19.25.180.

I. *Display of commercial flags.* In the districts listed in Table 21.12-5, a maximum of three flagpoles may be erected on any parcel provided that:

1. A maximum of three commercial flags may be displayed simultaneously.
2. The maximum length of the flag pole shall be 30 feet.

3. The total maximum size of all commercial flags displayed shall not exceed 120 square feet. Subject to the total maximum size of commercial flags, a commercial or non-commercial organization may display alongside a national or governmental flag, one organizational flag not larger than the national or governmental flag.

4. The corporate or commercial flag may only display the name, trademark, or logo of the business on the parcel and such flag may not be used for other business or advertising purposes.

J. *Temporary signs.* Temporary signs in nonresidential districts are permitted pursuant to Table 21.12-7.

Anchorage 2040 Land Use Plan

The Anchorage 2040 Land Use Plan includes guidelines that aim to protect these visual assets from the impacts of urbanization. It promotes land use practices that ensure new developments are sensitive to the surrounding viewsheds, such as maintaining scenic vistas and implementing design standards that complement the natural environment. The Anchorage 2040 Land Use Plan seeks to preserve the city of Anchorage and adjacent neighborhood's visual character while accommodating growth.

Chugiak-Eagle Creek Comprehensive Plan Update 2006

The Chugiak River Comprehensive Plan incorporates guidelines aimed at protecting and showcasing its natural landscapes. It proposes land use practices and development standards that maintain the visual integrity of key natural features, such as river vistas, forested areas, and mountainous backdrops. The plan encourages design approaches that blend with the natural environment, such as maintaining visual buffers and restricting intrusive structures.

TIMBER HARVESTING - EA SECTION 3.14

Regulatory Setting

Federal

25 CFR Part 163- General Forestry Regulations

Regulations regarding Indian Forest Land and the management activities. The regulations have forest management planning and sustained yield management, harvesting restrictions, and enterprise operations.

Forest and Rangeland Renewable Resources Planning Act of 1974 (16 USC 1600 to 1614)

This Act was passed in response to the growing tension between the timber industry and environmentalists in the late 1960s and the early 1970s. It authorizes long-range planning by the U.S. Forest Service to protect, develop, and enhance the productivity and other values of forest resources.

Indian Forest Management Handbook

The handbook, 53 IAM 4, provides information, procedures, and processes to prepare and administer the harvesting of Indian trust forest products through timber cutting permits or free-use harvesting without a permit when volumes and values are sufficiently small where formal contracts are deemed unnecessary (BIA, 2015).

Harvest of Forest Products Manual

Documents the BIA's policy, responsibilities, and standards for the authorized harvest of forest products on Indian land. It is an updated manual that combines the separate policy chapters on contract sales of forest products (53 IAM 3) and Permit of Sales of Forest Products (53 IAM 4) [BIA, 2018].

State and local

Forest Resources Practices Act

This Act (AS 41.17) governs how timber harvesting, reforestation, and timber access occur on state, private, and municipal lands. The Forest Resources Practices Act (FRPA) was adopted in 1978, with major revisions in 1990. FRPA standards are tailored to three regions, consisting of coastal forest (Region I), southcentral transitional forest (Region II), and interior boreal forest (Region III). The Project Site is located in Region II (ADNR, 2015). The FRPA is designed to protect fish habitat and water quality and ensure prompt reforestation of forest land while providing for a healthy timber harvest industry (ADNR, 2024d).

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Appendix F

Air Quality Modeling Output Data

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 1a
Alternative A - Vehicle Miles Traveled per Year

Market Areas	Trip Generation (average daily trips) ²	Trip Distribution	Average Distance (miles) ¹	Alternative A	
				Trips/Year	VMT/Year (miles)
Anchorage City Center	8736	100%	20	3,188,640	63,772,800
Water Deliveries ³			8	793	6,344
				3,189,433	63,779,144

¹ Trip lengths based on weighted average of distance to population centers.

Sources: Market Demand Analysis Casino - Anchorage Alaska, July 2015

² The proposed project is expected to generate 8,010 total weekday trips and 10,550 Saturday trips.

Sources: Traffic Impact Analysis Report Eklutna Village Native Casino, October, 2024

³ Water demand from the Water and Wastewater Study, November 2024. Assumed water truck capacity of 4,000 gallons.

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 2a
2026 Mobile Operations Criteria Pollutant and GHG Emissions

	Alternative A
vmt/yr	63,779,144
Criteria Pollutant Emissions (tpy)	
NO _x	31.57
VOC	10.98
SO ₂	0.12
CO	339.21
PM _{2.5}	1.05
PM ₁₀	3.67
Greenhouse Gas¹	
CO ₂	29749.5
CH ₄	2.7
N ₂ O	1.6
CO ₂ e	30279.3

¹ GHG emissions shown in metric tonnes.
Source: MOVES4

Table 2b
2040 Mobile Operations Criteria Pollutant and GHG Emissions

	Alternative A
vmt/yr	63,779,144
Criteria Pollutant Emissions (tpy)	
NO _x	10.86
VOC	5.75
SO ₂	0.09
CO	150.48
PM _{2.5}	0.64
PM ₁₀	3.21
Greenhouse Gas¹	
CO ₂	21759.32
CH ₄	1.82
N ₂ O	1.35
CO ₂ e	22209.32

¹ GHG emissions shown in metric tonnes.
Source: MOVES4

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 3a
 2026 Operational Mobile Annual Average
 Emission Factors

Criteria Pollutant	grams per mile
NOx	0.43
VOC	0.12
SO ₂	0.002
CO	4.26
PM _{2.5}	0.01
PM ₁₀	0.05
Greenhouse Gases	
CO ₂	460.39
CH ₄	0.04
N ₂ O	0.02
CO ₂ e	468.08

Source: MOVES4

Table 3b
 2026 Operational Start Annual Average
 Emission Factors

Criteria Pollutant	grams per start
NOx	0.45
VOC	0.68
SO ₂	0.000
CO	11.30
PM _{2.5}	0.02
PM ₁₀	0.02
Greenhouse Gases	
CO ₂	121.06
CH ₄	0.10
N ₂ O	0.03
CO ₂ e	133.50

Source: MOVES4

Table 3c
 2040 Operational Mobile Annual Average
 Emission Factors

Criteria Pollutant	grams per mile
NOx	0.14
VOC	0.06
SO ₂	0.001
CO	1.83
PM _{2.5}	0.01
PM ₁₀	0.04
Greenhouse Gas	
CO ₂	337.24
CH ₄	0.03
N ₂ O	0.020
CO ₂ e	343.86

Source: MOVES4

Table 3d
 2040 Operational Start Annual Average
 Emission Factors

Criteria Pollutant	grams per start
NOx	0.29
VOC	0.43
SO ₂	0.000
CO	6.13
PM _{2.5}	0.01
PM ₁₀	0.02
Greenhouse Gas	
CO ₂	78.46
CH ₄	0.05
N ₂ O	0.02
CO ₂ e	87.16

Source: MOVES4

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 4
 Fugitive Dust Emissions from Construction

	Alternatives A and B
Construction Area (acres)	6.42
Duration of Construction (months)	12
On-site cut/fill (1,000 cubic yards)	57.1
PM ₁₀ Emission Factor (tons PM ₁₀ /acre-month)	0.011
On-site cut/fill PM10 Emission Factor (tons PM10/1,000 cubic yards)	0.059
Total PM10 Emissions (tons)	4.22
Total PM_{2.5} Emissions (tons)	2.11

Source: Emission factors from WRAP's Fugitive Dust Handbook (Level 2).

Note: On-site cut/fill estimated based on Grading and Drainage Study, July 2024. PM2.5 conservatively estimated to be half of PM10 emissions.

"10,350 cubic yards of unusable excavation that will need to be hauled off-site (topsoil). Net volume of 11,600 CY of unusable excavation (poor silty material). Net fill of 29,750 cy of materials required."

Eklutna Native Village Gaming Hall Project

Air Quality Emissions Calculations

Table 5
Alternatives A & B - Construction Emissions

Construction Equipment ¹	Horsepower	Load Factor	Hours in Use ² (hours/day)	Emission Factors (g/bhp/hr) ⁶						Emission (tons/year)					
				CO	VOC	NOx	SO ₂	PM ₁₀	PM _{2.5}	CO	VOC	NOx	SO ₂	PM ₁₀	PM _{2.5}
Demolition															
2 Rubber Tired Dozers	367	0.4	8	2.9	0.371	3.51	0.005	0.154	0.142	0.08	0.01	0.09	0.00	0.00	0.00
3 Excavators	36	0.38	8	4.21	0.402	3.45	0.005	0.107	0.098	0.02	0.00	0.01	0.00	0.00	0.00
1 Concrete/Industrial Saw	33	0.73	8	4.35	0.439	3.63	0.007	0.101	0.093	0.01	0.00	0.01	0.00	0.00	0.00
Employee Trips (miles) ³		3,000		4.24	0.12	0.21	0.00	0.04	0.01	0.01	0.00	0.00	0.00	0.00	0.00
Hauling Trips (miles) ⁴		200		3.65	0.26	1.24	0.00	0.11	0.04	0.00	0.00	0.00	0.00	0.00	0.00
Site Preparation															
3 Rubber Tired Dozers	367	0.4	8	2.9	0.371	3.51	0.005	0.154	0.142	0.17	0.02	0.20	0.00	0.01	0.01
4 Tractors/Loaders/Backhoes	84	0.37	8	3.48	0.196	2.01	0.005	0.077	0.071	0.06	0.00	0.03	0.00	0.00	0.00
Employee Trips (miles) ³		7,200		4.24	0.12	0.21	0.00	0.04	0.01	0.03	0.00	0.00	0.00	0.00	0.00
Hauling Trips (miles) ⁸		728		3.65	0.26	1.24	0.00	0.11	0.04	0.00	0.00	0.00	0.00	0.00	0.00
Site Grading															
1 Grader	148	0.41	8	3.42	0.34	2.86	0.01	0.16	0.15	0.05	0.01	0.05	0.00	0.00	0.00
1 Excavator	36	0.38	8	4.21	0.40	3.45	0.01	0.11	0.10	0.02	0.00	0.01	0.00	0.00	0.00
3 Tractors/Loaders/Backhoes	84	0.37	8	3.48	0.20	2.01	0.01	0.08	0.07	0.09	0.00	0.05	0.00	0.00	0.00
1 Rubber Tired Dozer	367	0.4	8	2.90	0.37	3.51	0.01	0.15	0.14	0.11	0.01	0.14	0.00	0.01	0.01
Employee Trips (miles) ³		12,000		4.24	0.12	0.21	0.00	0.04	0.01	0.06	0.00	0.00	0.00	0.00	0.00
Hauling Trips (miles) ⁴		68,000		3.65	0.26	1.24	0.00	0.11	0.04	0.27	0.02	0.09	0.00	0.01	0.00
Fugitive Dust														4.22	2.11
Construction															
3 Forklift	82	0.20	8	3.60	0.27	2.55	0.01	0.14	0.12	0.28	0.02	0.20	0.00	0.01	0.01
1 Generator Set	14	0.74	8	2.87	0.54	4.35	0.01	0.18	0.16	0.05	0.01	0.07	0.00	0.00	0.00
1 Crane	367	0.29	7	1.66	0.20	1.95	0.01	0.08	0.07	0.25	0.03	0.29	0.00	0.01	0.01
1 Welder	46	0.45	8	4.52	0.50	3.68	0.01	0.11	0.10	0.15	0.02	0.12	0.00	0.00	0.00
3 Tractor/Loaders/Backhoes	84	0.37	7	3.48	0.20	2.01	0.01	0.08	0.07	0.45	0.03	0.26	0.00	0.01	0.01
Employee Trips (miles) ³		96,000		4.24	0.12	0.21	0.00	0.04	0.01	0.45	0.01	0.02	0.00	0.00	0.00
Vendor Trips (miles) ⁵		40,000		4.11	0.13	0.24	0.00	0.05	0.01	0.18	0.01	0.01	0.00	0.00	0.00
Hauling Trips (miles) ⁴		68,000		3.65	0.26	1.24	0.00	0.11	0.04	0.27	0.02	0.09	0.00	0.01	0.00
Paving															
2 Pavers	81	0.42	8	3.45	0.25	2.65	0.01	0.14	0.13	0.03	0.00	0.02	0.00	0.00	0.00
2 Paving Equipment	89	0.36	8	3.42	0.20	2.22	0.01	0.09	0.08	0.03	0.00	0.02	0.00	0.00	0.00
2 Rollers	36	0.38	8	4.11	0.57	3.68	0.01	0.17	0.15	0.01	0.00	0.01	0.00	0.00	0.00
Employee Trips (miles) ³		6,000		4.24	0.12	0.21	0.00	0.04	0.01	0.03	0.00	0.00	0.00	0.00	0.00
Architectural Coating															
1 Air Compressor	37	0.48	6	4.85	0.545	3.76	0.007	0.117	0.107	0.01	0.00	0.01	0.00	0.00	0.00
Employee Trips (miles) ³		2,000		4.24	0.12	0.21	0.00	0.04	0.01	0.01	0.00	0.00	0.00	0.00	0.00
Coating ⁷		0.0116									0.75				
Total Project Construction Emissions										3.17	0.98	1.82	0.00	4.31	2.18

Sources:

¹ Construction equipment list and quantity from CalEEMod. Emission factors for equipment from CalEEMod (grams/mile).

² Hours per normal work day from CalEEMod.

³ Based on 20 mile trip length. Worker trip numbers from CalEEMod. On-Road Emission Factors from MOVES4 for Passenger Truck (31). (ex. # worker trips * 2 one-way trips * construction phase length * 20 miles)

⁴ Based on 20 mile trip length. Hauling trip numbers from CalEEMod. On Road Emission Factors from MOVES4 for Single-Unit Short Haul Truck (52)

⁵ Based on 20 mile trip length. Vendor trip numbers from CalEEMod. On Road Emission Factors from MOVES4 for Light Commercial Truck (32)

⁶ Off-Road Emission factors provided from California Air Resources Board OFFROAD2011 emission factors, as sourced from CalEEMod Default Data Tables; On-Road Emission Factors from MOVES4

⁷ Paved area from CalEEMod.

⁸ Hauling trips for tree removal. Based on 7 mile trip length. 171 cords of trees observed on Project Site. Assumed 3.375 cords/haul trip (16-CY truck capacity).

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 6
 Alternatives A & B - Construction GHG Emissions

Construction Equipment ¹	Horsepower	Load Factor	Hours in Use ² (hours/day)	Emission Factors		Emission (metric tons) CO ₂ e
				(g/bhp/hr) ⁶		
				CO ₂	CH ₄	
Demolition						
2 Rubber Tired Dozers	367	0.4	8	532.00	0.02	12.51
3 Excavators	36	0.38	8	587.00	0.02	1.93
1 Concrete/Industrial Saw	33	0.73	8	575.00	0.02	1.11
Employee Trips (total miles) ³	3,000			389.25	0.02	392.16
Hauling Trips (total miles) ⁴	200			930.98	0.20	963.38
Site Preparation						
3 Rubber Tired Dozers	367	0.4	8	532.00	0.02	28.14
4 Tractors/Loaders/Backhoes	84	0.37	8	530.00	0.02	7.91
Employee Trips (total miles) ³	7,200			389.25	0.02	392.16
Hauling Trips (total miles) ⁷	728			930.98	0.20	963.38
Site Grading						
1 Grader	148	0.41	8	531.00	0.02	7.74
1 Excavator	36	0.38	8	587.00	0.02	1.93
3 Tractors/Loaders/Backhoes	84	0.37	8	530.00	0.02	11.87
1 Rubber Tired Dozer	367	0.4	8	532.00	0.02	18.76
Employee Trips (total miles) ³	12,000			389.25	0.02	392.16
Hauling Trips (total miles) ⁴	68,000			930.98	0.20	963.38
Construction						
3 Forklifts	82	0.2	8	527.00	0.02	37.37
1 Generator Set	14	0.74	8	568.00	0.02	8.48
1 Crane	367	0.29	7	528.00	0.02	70.88
1 Welder	46	0.45	8	568.00	0.02	16.95
3 Tractors/Loaders/Backhoes	84	0.37	7	530.00	0.02	62.33
Employee Trips (total miles) ³	96,000			389.25	0.02	392.16
Vendor Trips (total miles) ⁵	40,000			429.87	0.02	433.68
Hauling Trips (total miles) ⁴	68,000			930.98	0.20	963.38
Paving						
2 Pavers	81	0.42	8	527.00	0.02	4.31
2 Paving Equipment	89	0.36	8	528.00	0.02	4.06
2 Rollers	36	0.38	8	587.00	0.02	1.93
Employee Trips (total miles) ⁴	6,000			389.25	0.02	392.16
Architectural Coating						
1 Air Compressor	37	0.48	6	567.00	0.02	0.91
Employee Trips (total miles) ³	2,000			389.25	0.02	392.16
Construction GHG Emissions						497

Source:

¹ Construction equipment list and quantity from CalEEMod. Emission factors for equipment from CalEEMod (grams/mile).

² Hours per normal work day

³ Based on 20 mile trip length. Worker trip numbers from CalEEMod. On-Road Emission Factors from MOVES4 for Passenger Truck (31). (ex. # worker trips * 2 one-way trips * construction phase length * 20 miles)

⁴ Based on 20 mile trip length. Hauling trip numbers from CalEEMod. On Road Emission Factors from MOVES4 for Single-Unit Short Haul Truck (52)

⁵ Based on 20 mile trip length. Vendor trip numbers from CalEEMod. On Road Emission Factors from MOVES4 for Light Commercial Truck (32)

⁶ Off-Road Emission factors provided from California Air Resources Board OFFROAD2011 emission factors, as sourced from CalEEMod Default Data Tables; On-Road Emission Factors from MOVES4

⁸ Hauling trips associated with tree removal. Based on 7 mile trip length. 171 cords of trees observed on Project Site. Assumed 3.375 cords/haul trip (16-CY truck capacity).

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 7a
 Alternative A

Pollutant/GHG	MMscf/year	Emission Factors (lb/MMscf)	Conversion factor (lb/tons)	Emissions (tons)
VOC	2.33	5.5	0.0005	0.01
NO _x	2.33	0.64	0.0005	0.00
CO	2.33	84	0.0005	0.10
SO ₂	2.33	0.6	0.0005	0.00
PM ₁₀	2.33	7.6	0.0005	0.01
PM _{2.5}	2.33	7.6	0.0005	0.01
Greenhouse Gas			lb/MT	MT
CO ₂	2.33	120,000	0.00045	126

Stationary Sources include boilers, stoves, heating units, and other equipment.

Source: AP 42, Tables 1.4-1 and 1.4-2 (EPA, 1998), USEIA, 2022.

Eklutna Native Village Gaming Hall Project
Air Quality Emissions Calculations

Table 8a Indirect GHG Emissions
 Alternative A

Sources	Emission Factors			Use	Emissions (MT of CO ₂ e)
	CO ₂	CH ₄	N ₂ O		
	lbs of/MWh			MWh	
Electricity	995.8	0.107	0.015	1,256	571.22
	MT of CO ₂ e/MT of Solid Waste			MT of Solid Waste	
Solid Waste	0.503			52.92	26.62
	MT of CO ₂ e/Million Gallons			Million Gallons	
Water/Wastewater	6.428			0.00	0.00
Total					598

Sources: Electricity based on U.S. Energy Information Administration, 2018 Commercial Buildings Energy Consumption Survey.
 Solid Waste value from CalEEMod for Quality Restaurant land use

Potential To Emit Calculator for Boilers and Emergency Engines

7/1/2016

This spreadsheet helps estimate a facility's potential to emit. It is provided for the convenience of the permitted community. Emission factor sources are subject to revision or correction. It is the permittee's responsibility to determine their emissions. The permittee should consult with the reviewing authority to determine the appropriateness of this calculator for its source.

If you have one or more of the following units that are exempt from the Indian Country Minor NSR Program, please contact your EPA Regional office before you use this calculator to determine whether you need to obtain a minor NSR permit: Internal combustion engines used for landscaping purposes; Emergency generators, designed solely for the purpose of providing electrical power during power outages; in nonattainment areas classified as Serious or lower, the total maximum manufacturer's site-rated hp of all units shall be below 500; in attainment areas, the total maximum manufacturer's site-rated hp of all units shall be below 1,000; Stationary internal combustion engines with a manufacturer's site-rated hp of less than 5; and Furnaces or boilers used for space heating that use only gaseous fuel, with a total maximum heat input (i.e., from all units combined) of in nonattainment areas classified as Serious or lower, 5 MMBtu/hr or less; in nonattainment areas classified as Severe or Extreme, 2 MMBtu/hr or less; and in attainment areas, 10 MMBtu/hr or less.

Directions: Enter the facility's information below.

Write the letter "Y" or "N" next to each fuel type to indicate that the facility does or does not burn that type of fuel.

For **Total Boiler Heat Input** - only add together the heat input of those units that are rated at least 10.0 MMBtu/hr or larger, unless in a severe or extreme ozone nonattainment area. Include all units 2.0 MMBtu/hr and greater in severe or extreme ozone nonattainment areas.

For **Total Small Boilers and Heaters Input** - only add together the heat input of those boilers and heaters that are rated at less than 10.0 MMBtu/hr, unless in a severe or extreme ozone nonattainment area. Include all units less than 2.0 MMBtu/hr in severe or extreme ozone nonattainment areas.

For **Emergency Generators/Engines** - this calculator does not calculate emissions from non-emergency (prime) engines. If you have non-emergency engines please contact your reviewing authority

Facility Profile*

Total Boiler Heat Input -	0	(MMBtu/hr)	Total of boilers 10.0 MMBtu/hr and greater, unless in extreme ozone nonattainment area then include 2.0 MMBtu/hr and greater.
Fuels Used			
Natural Gas-	Y	(Y or N)	
LPG	N	(Y or N)	Sulfur %
Liquid Fuel (distillate, diesel, etc.)	N	(Y or N)	0.0015 Default = 0.0015
Total Small Boilers and Heaters Input -	0	(MMBtu/hr)	Total of boilers less than 10.0 MMBtu/hr, unless in extreme ozone nonattainment area then less than 2.0 MMBtu/hr.
Fuels Used			
Natural Gas	N	(Y or N)	
LPG	N	(Y or N)	Sulfur %
Liquid Fuel (distillate, diesel, etc.)	N	(Y or N)	0.0015 Default = 0.0015
Emergency Generator/Engines -	680	(hp)	
Fuels Used			
Diesel-	Y	(Y or N)	
Gasoline	N	(Y or N)	Sulfur %
Natural Gas/LPG	N	(Y or N)	0.0015 Default = 0.0015

Converter

Insert Value:			
Convert MMscf/yr to MMBtu/yr	345	MMscf	
			351900 MMBtu/yr
			40,17123 MMBtu/hr

Potential To Emit Calculator for Boilers

7/1/2016

Emissions from Emergency Generator/Engine - Criteria Pollutants

Engine Size: 680 hp

Purple values are pulled from other worksheet

Diesel Used: Y

Blue values are results

Worst Case PTE (ton/yr)

Pollutant									
PM	PM ₁₀	PM _{2.5}	SO ₂	NO _x	CO	VOC	CO ₂	Single HAP	Combined HAP
0.12	0.12	0.12	0.00	4.08	0.94	0.12	195.50	0.20	0.65

Engine Type: Diesel Engine (<= 600 hp) Used: N

	Pollutant									
	PM ²	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single Hap	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	2.20E-03	2.20E-03	2.20E-03	2.05E-03	3.10E-02	6.68E-03	2.47E-03	1.15	0.00118	0.0038
PTE (ton/yr)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Note:

- Emission factors are from Chapter 3.3, Table 3.3-1 (updated 10/96).
- Assume PM and PM_{2.5} emissions are equal to PM₁₀ emissions.
- Assume TOC (total organic compounds) emissions equal to VOC emissions.
- Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Engine Type: Diesel (> 600 hp) Used: Y Sulfur Content: 0.00 %

	Pollutant									
	PM	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single HAP	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	0.0007	0.0007	0.0007	1.21E-05	0.024	5.50E-03	7.05E-04	1.15	0.00118	0.0038
Limited PTE (ton/yr)	0.12	0.12	0.12	0.00	4.08	0.94	0.12	195.50	0.20	0.65

Note:

- Emission factors are from Chapter 3.4, Tables 3.4-1 and 3.4-2 for Large Stationary Diesel and Dual Fuel Engines (updated 10/96).
- Assume PM_{2.5} emissions are equal to PM₁₀ emissions.
- Assume TOC (total organic compounds) emissions equal to VOC emissions.
- Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Engine Type: Gasoline Used: N

	Pollutant									
	PM ²	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single Hap	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	7.21E-04	7.21E-04	7.21E-04	5.91E-04	0.011	6.96E-03	2.05E-02	1.08E+00	1.18E-03	3.80E-03
PTE (ton/yr)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Note:

- Emission factors are from Chapter 3.3, Table 3.3-1 (updated 10/96).
- Assume PM and PM_{2.5} emissions are equal to PM₁₀ emissions.
- Assume TOC (total organic compounds) emissions equal to VOC emissions.
- Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Engine Type: Natural Gas/LPG Used: N

	Pollutant									
	PM ²	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single HAP	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	1.67E-04	1.67E-04	1.67E-04	5.06E-06	1.90E-02	3.20E-02	2.55E-04	0.946	0.00045408	0.0006192
PTE (ton/yr)	FALSE	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Note:

- Emission factors are from Chapter 3.3, Table 3.3-1 (updated 10/96).
- Assume PM and PM_{2.5} emissions are equal to PM₁₀ emissions.
- Assume TOC (total organic compounds) emissions equal to VOC emissions.
- Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Potential To Emit Calculator for Boilers and Emergency Engines

7/1/2016

This spreadsheet helps estimate a facility's potential to emit. It is provided for the convenience of the permitted community. Emission factor sources are subject to revision or correction. It is the permittee's responsibility to determine their emissions. The permittee should consult with the reviewing authority to determine the appropriateness of this calculator for its source.

If you have one or more of the following units that are exempt from the Indian Country Minor NSR Program, please contact your EPA Regional office before you use this calculator to determine whether you need to obtain a minor NSR permit: Internal combustion engines used for landscaping purposes; Emergency generators, designed solely for the purpose of providing electrical power during power outages; in nonattainment areas classified as Serious or lower, the total maximum manufacturer's site-rated hp of all units shall be below 500; in attainment areas, the total maximum manufacturer's site-rated hp of all units shall be below 1,000; Stationary internal combustion engines with a manufacturer's site-rated hp of less than 5; and Furnaces or boilers used for space heating that use only gaseous fuel, with a total maximum heat input (i.e., from all units combined) of in nonattainment areas classified as Serious or lower, 5 MMBtu/hr or less; in nonattainment areas classified as Severe or Extreme, 2 MMBtu/hr or less; and in attainment areas, 10 MMBtu/hr or less.

Directions: Enter the facility's information below.

Write the letter "Y" or "N" next to each fuel type to indicate that the facility does or does not burn that type of fuel.

For **Total Boiler Heat Input** - only add together the heat input of those units that are rated at least 10.0 MMBtu/hr or larger, unless in a severe or extreme ozone nonattainment area. Include all units 2.0 MMBtu/hr and greater in severe or extreme ozone nonattainment areas.

For **Total Small Boilers and Heaters Input** - only add together the heat input of those boilers and heaters that are rated at less than 10.0 MMBtu/hr, unless in a severe or extreme ozone nonattainment area. Include all units less than 2.0 MMBtu/hr in severe or extreme ozone nonattainment areas.

For **Emergency Generators/Engines** - this calculator does not calculate emissions from non-emergency (prime) engines. If you have non-emergency engines please contact your reviewing authority

Facility Profile*				Converter				
Total Boiler Heat Input -	0	(MMBtu/hr)	Total of boilers 10.0 MMBtu/hr and greater, unless in extreme ozone nonattainment area then include 2.0 MMBtu/hr and greater.	Insert Value:				
Fuels Used				Convert MMscf/yr to MMBtu/yr	345	MMscf	351900	MMBtu/yr
Natural Gas-	Y	(Y or N)						
LPG	N	(Y or N)	Sulfur %					
Liquid Fuel (distillate, diesel, etc.)	N	(Y or N)	0.0015	Default = 0.0015			40,17123	MMBtu/hr
Total Small Boilers and Heaters Input -	0	(MMBtu/hr)	Total of boilers less than 10.0 MMBtu/hr, unless in extreme ozone nonattainment area then less than 2.0 MMBtu/hr.					
Fuels Used								
Natural Gas	N	(Y or N)						
LPG	N	(Y or N)	Sulfur %					
Liquid Fuel (distillate, diesel, etc.)	N	(Y or N)	0.0015	Default = 0.0015				
Emergency Generator/Engines -	1220	(hp)						
Fuels Used			Sulfur %					
Diesel-	Y	(Y or N)	0.0015	Default = 0.0015				
Gasoline	N	(Y or N)						
Natural Gas/LPG	N	(Y or N)						

Potential To Emit Calculator for Boilers

7/1/2016

Emissions from Emergency Generator/Engine - Criteria Pollutants

Engine Size: 1220 hp

Purple values are pulled from other worksheet

Diesel Used: Y

Blue values are results

Worst Case PTE (ton/yr)

Pollutant									
PM	PM ₁₀	PM _{2.5}	SO ₂	NO _x	CO	VOC	CO ₂	Single HAP	Combined HAP
0.21	0.21	0.21	0.00	7.32	1.68	0.22	350.75	0.36	1.16

Engine Type: Diesel Engine (<= 600 hp) Used: N

	Pollutant									
	PM ²	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single Hap	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	2.20E-03	2.20E-03	2.20E-03	2.05E-03	3.10E-02	6.68E-03	2.47E-03	1.15	0.00118	0.0038
PTE (ton/yr)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Note:

1. Emission factors are from Chapter 3.3, Table 3.3-1 (updated 10/96).
2. Assume PM and PM_{2.5} emissions are equal to PM₁₀ emissions.
3. Assume TOC (total organic compounds) emissions equal to VOC emissions.
4. Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Engine Type: Diesel (> 600 hp) Used: Y Sulfur Content: 0.00 %

	Pollutant									
	PM	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single HAP	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	0.0007	0.0007	0.0007	1.21E-05	0.024	5.50E-03	7.05E-04	1.15	0.00118	0.0038
Limited PTE (ton/yr)	0.21	0.21	0.21	0.00	7.32	1.68	0.22	350.75	0.36	1.16

Note:

1. Emission factors are from Chapter 3.4, Tables 3.4-1 and 3.4-2 for Large Stationary Diesel and Dual Fuel Engines (updated 10/96).
2. Assume PM_{2.5} emissions are equal to PM₁₀ emissions.
3. Assume TOC (total organic compounds) emissions equal to VOC emissions.
4. Assume 500 hours/yr of operation for an emergency engine

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Engine Type: Gasoline Used: N

	Pollutant									
	PM ²	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single Hap	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	7.21E-04	7.21E-04	7.21E-04	5.91E-04	0.011	6.96E-03	2.05E-02	1.08E+00	1.18E-03	3.80E-03
PTE (ton/yr)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Note:

1. Emission factors are from Chapter 3.3, Table 3.3-1 (updated 10/96).
2. Assume PM and PM_{2.5} emissions are equal to PM₁₀ emissions.
3. Assume TOC (total organic compounds) emissions equal to VOC emissions.
4. Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Engine Type: Natural Gas/LPG Used: N

	Pollutant									
	PM ²	PM ₁₀	PM _{2.5} ²	SO ₂	NO _x	CO	VOC ³	CO ₂	Single HAP	Combined HAP
Emission Factor ¹ (lbs/hp-hr)	1.67E-04	1.67E-04	1.67E-04	5.06E-06	1.90E-02	3.20E-02	2.55E-04	0.946	0.00045408	0.0006192
PTE (ton/yr)	FALSE	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00

Note:

1. Emission factors are from Chapter 3.3, Table 3.3-1 (updated 10/96).
2. Assume PM and PM_{2.5} emissions are equal to PM₁₀ emissions.
3. Assume TOC (total organic compounds) emissions equal to VOC emissions.
4. Assume 500 hours/yr of operation for an emergency engine.

Methodology

PTE (ton/yr) = Engine Capacity (hp) x EF (lb/hp-hr) x 8760 hr x 1 ton/2000 lb

Eklutna Native Village Gaming Hall Project
Social Cost of Greenhouse Gas Calculations

GHG/Cost per metric ton	Alternative A		
		Tons	Cost
Construction (2025-2026)			
CO ₂ e	\$56	692	\$38,752
Operation (2026)			
CO ₂ e	\$56	31,416	\$1,759,296
Operation (2040)			
CO ₂ e	\$73	23,346	\$1,704,258
Lifetime			
CO ₂ e		701,072	\$51,166,492

Costs from IWG, 2021 (3% discount rate)

Appendix G

Biological Assessment and ESA Section 7
Consultation Materials

Expedited Request for Informal Consultation with NMFS/NOAA



United States Department of the Interior

BUREAU OF INDIAN AFFAIRS

Alaska Regional Office
3601 C Street, Suite 1200
Anchorage, Alaska 99503

IN REPLY REFER TO
E620

Ms. Sierra Franks
Protected Species Biologist, Section 7 Coordinator
National Marine Fisheries Service, Alaska Regional Office
P.O. Box 21668
709 West 9th Street, Room 323
Juneau, AK 99802

Dear Ms. Franks:

The Bureau of Indian Affairs (BIA) received and, is reviewing a request for approvals required for implementation of the proposed Eklutna Native Village (Tribe) Gaming Facility Project (Proposed Action), as described below. The BIA requests an expedited informal consultation under section 7(a)(2) of the Endangered Species Act (ESA) for the Proposed Action. The BIA determined the Proposed Action may affect, but is not likely to adversely affect, federally listed fish species. Our supporting analysis is provided below and, in greater detail the Biological Assessment (**Attachment 1**). We request your written concurrence with our determinations.

The BIA determined the Proposed Action will have no effect on federally listed: Whales, pinnipeds, reptiles, marine invertebrates, Critical Habitat, or Essential Fish Habitat (EFH). The Action Area does not contain suitable habitat for these species, nor any designated Critical Habitat or Essential Fish Habitat. Whales, pinnipeds, reptiles, and marine invertebrates will not be discussed further in this consultation.

Description of the Action Area

The Project Area is defined as: 6.37-acre property to be developed under the Proposed Action; and includes all areas that may be directly or indirectly impacted by the Proposed Action (see **Figures 1-3 of Attachment 1**). The Action Area includes the Project Area, as well as a portion of Peters Creek, adjacent to-but outside the Project Area, and a downstream portion of Peters Creek. Over a half-mile of vegetated creek exists between the Project Area and its discharge point at the Knik Arm. Knik Arm and waters draining into the ocean are not evaluated.

The approximately 6.37-acre Project Area is located in the unincorporated community of Chugiak, within the municipality of Anchorage, Alaska. The Project Area consists of three parcels: (Assessor's Parcel Numbers 05108101000, 05108102000, and 05108115000) currently owned by members of the Eklutna Native Village, as Native allotment land under the Alaskan Native Allotment Act (ANAA). The Project Area is located off Birchwood Spur Road. The Alaska Railroad runs along the northern boundary of the Project Area, and the Birchwood Airport lies to the west. Other surrounding land uses include rural residential and industrial.

The Action Area is located within Section 5 of Township 15 North, Range 1 West, within the Seward Meridian, in the Anchorage B-7 NW United States Geological Survey (USGS) 7.5' quadrangle map. Elevations on the Action Area range between approximately 73 feet to 88 feet above mean sea level (amsl), with the majority of the site sloping gradually to the north. There is a bench cutting across the northwest corner of the property which drops approximately 8 feet, then flattens out towards the adjacent bank of Peters Creek.

Description of the Proposed Action

The Proposed Action involves the development of a Class II Gaming Facility by the Tribe; on a 6.37-acre portion of a restricted fee Native Allotment owned by members of the Tribe (Action Area). The Proposed Action consists of the following components: 1) BIA approval of a business lease by tribal members who own the restricted fee Native Allotment to the tribal government for operation of a proposed gaming facility within the Action Area; 2) development by the Tribe of a gaming facility and associated infrastructure on the Action Area; and 3) potential NIGC approval of a Gaming Management Agreement for operation of the gaming facility. Construction is estimated to commence in March of 2025 and would continue for a period of approximately 13 - 15 months.

A Site Plan is included as **Figure 4 of Attachment 1**. The Proposed Action includes a 58,000 square foot (sf) building, with paved surface parking and supporting infrastructure. The gaming facility consists of one-story (approximately 40 feet high) and approximately 32,000 square feet of gaming floor, and approximately 26,000 square feet of support and circulation space. Proposed loading docks to accommodate deliveries will be located behind the building. The gaming facility is proposed to be open for up to 24 hours a day, 7 days a week.

Access to the Action Area is currently provided via a driveway and dedicated right-of-way along the southern boundary of the property connecting to Birchwood Spur Road. The driveway will be improved and paved to serve as the primary access for vehicles ingress and egress to the Project Area (Figure 4). Development will include 443 paved surface parking spaces to accommodate employees and patrons. Exterior signage will be compatible with the building architecture, as well as the natural characteristics of the site. Exterior lighting will be strategically positioned around the building to minimize off-site glare. Downcast lighting and LED bulbs will be used in the landscaped and parking areas to minimize off-site scatter. The parking lot and areas surrounding the building will be enhanced by landscaping which incorporate plants native to the region.

There are no municipal connections for potable water supply or wastewater treatment within the Project Area. Water will be supplied via a proposed groundwater well, and a proposed on-site wastewater treatment and disposal facility. The Proposed Action will have an average water demand and wastewater generation rate of 8,681 gpd. Wastewater will be treated and disposed of through the proposed on-site septic facilities (appropriately sized). Septic facilities will include: On-site grease interceptor tank, 10,000 – 15,000-gallon septic tank, a treatment tank, an advanced secondary treatment system, and a septic drainage field that will be installed below the paved parking areas, within the Project Area.

While there are no surface waters within the Project Area, Peters Creek is adjacent to - but outside the eastern border. All development activities will be set back a minimum of 25 feet from Peters Creek, and no discharges of stormwater or wastewater to the Creek will occur. The proposed septic drain field will be located more than 200 feet from Peters Creek (see Figure 4 of **Attachment 1**).

A Grading and drainage study has been prepared and is an attachment to the Biological Assessment (**Attachment 1**). Development will require retaining walls on the north and east sides of the Action Area, and the northeast corner will be raised by three to seven feet above the existing grade due, to the steep drop off towards Peters Creek.

Following construction, impervious areas would total approximately 6.37 acres. Trenching and excavation are proposed to create stormwater detention and associated drainage infrastructure to provide volume control, treatment, and rate control. Stormwater would be collected and treated consistent with the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0 (Municipality of Anchorage, 2017). Vegetated swales would be established in the parking areas to filter runoff and, storm drainage catch basins would be located at low points on the Project Area to direct runoff to buried infiltration beds to provide volume control, treatment, and rate control (See **Attachment 1, Attachment A**). The stormwater collection and treatment system would be designed to accommodate a 24-hour, 100-year flood event with off-site runoff rates modeled to be equal to or less than existing rates.

Project-Specific Best Management Practices

The following best management practices (BMPs) related to biological resources have been included as part of the Proposed Action:

- Proposed facilities would be constructed generally consistent with the AMC and in accordance with the IBC, (including electrical, mechanical, plumbing, fire protection, and seismic standards).
- Construction and design would follow standard engineering practices related to grading and soil suitability.
- Paving and ground disturbance would be set back from Peters Creek by at least 25 feet, in accordance with the AMC (AMC 21.45.210).
- Proposed storm drainage catch basins would be located at low points on the Action Area to direct runoff to buried infiltration beds.
- Stormwater collection and treatment system would be designed to accommodate a 24-hour 100-year flood event with off-site runoff rates modeled to be equal to or less than existing rates.
- Stormwater collection and treatment facilities shall be designed with excess capacity to offset lost floodplain capacity from development within the 500-year floodplain. This capacity shall be in excess of modeled sizing, compliant with the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0. Coverage under the National Pollutant Discharge Elimination System (NPDES) Construction General Permit (CGP) shall be obtained from the U.S. Environmental Protection Agency (USEPA) for construction site runoff during the construction phase, in compliance with the Clean Water Act (CWA). Conditions of the NPDES General Construction Permit shall be adhered to.
- A Stormwater Pollution Prevention Plan (SWPPP) shall be prepared, implemented, and maintained throughout the construction phase, consistent with CGP requirements. The SWPPP shall include BMPs to minimize stormwater effects to water quality during construction. These will include, but not be limited to, the following:
 - Temporary erosion control measures (such as silt fences, fiber rolls, staked straw bales, temporary re-vegetation, rock bag dams, erosion control blankets, and sediment traps) shall be employed, as needed for disturbed areas.
 - Construction activities shall be scheduled to minimize land disturbance during peak runoff periods, to the extent feasible.
 - Disturbed areas shall be paved, re-vegetated, and/or stabilized following construction.

- A spill prevention and countermeasure plan shall be developed identifying proper storage, collection, and disposal measures for potential pollutants (such as fuel, fertilizers, pesticides, etc.) used on-site.
- Petroleum products shall be stored, handled, used and disposed of properly in accordance with provisions of the CWA (33 USC §§ 1251 to 1387).
- Construction materials shall be stored, covered, and isolated to prevent runoff loss and contamination of surface and groundwater.
- Fuel and vehicle maintenance areas shall be limited to the Action Area.
- To minimize dust generation during construction, soil shall be wetted down with water prior to ground disturbance as needed.
- Trash storage areas for receptacles will be designed to minimize stormwater runoff contact with disposed solid waste. Receptacles shall contain lids and shall be placed on impervious pavement. Receptacles along with signs encouraging use of trash receptacles will be placed in common areas to reduce littering.

Federally Listed Species and Critical Habitat/EFH

The National Marine Fisheries Service “NMFS” website was reviewed to determine appropriate species under NMFS jurisdiction that warrant evaluation. As discussed above and in **Attachment 1**, the BIA determined the Proposed Action will have no effect on federally listed: Whales, pinnipeds, reptiles, and marine invertebrates, as there is no suitable habitats within the Action Area. Therefore, whales, pinnipeds, reptiles, and marine invertebrates will not be discussed further in this consultation. A list of fish species provided by NMFS that may occur within the Alaska Region geographic area is included below.

Fish:

- Green Sturgeon (Southern DPS)(*Acipenser medirostris*): threatened
- Hood Canal Summer-run Chum Salmon (*Oncorhynchus keta*): threatened
- Lower Columbia River Coho Salmon (*Oncorhynchus kisutch*): threatened
- Lower Columbia River Steelhead (*Oncorhynchus mykiss*): threatened
- Middle Columbia River Steelhead (*Oncorhynchus mykiss*): threatened
- Snake River Basin Steelhead (*Oncorhynchus mykiss*): threatened
- Upper Columbia River Steelhead (*Oncorhynchus mykiss*): threatened
- Upper Willamette River Steelhead (*Oncorhynchus mykiss*): threatened
- Snake River Sockeye Salmon (*Oncorhynchus nerka*): endangered
- Lower Columbia River Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Puget Sound Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Snake River Fall Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Snake River Spring/Summer-run Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Upper Columbia River Spring Chinook Salmon (*Oncorhynchus tshawytscha*): endangered
- Upper Willamette River Chinook Salmon (*Oncorhynchus tshawytscha*): threatened

Federally listed fish species identified on the NMFS list are anadromous species that are reared in freshwater and spend their adult life in saltwater. Adults return to their natal freshwaters to spawn. Federally listed anadromous species do not spawn in the Alaska Region, but they may occur in Alaskan waters during the marine stages of their life cycle. Threats to these species often consist of loss of habitat, especially spawning habitat, degradation of habitat, overfishing or bycatch, and competition with invasive species.

There is no habitat for anadromous fish species in the Project Area. Peters Creek however, is hydrologically connected to the Project Area and located within 25 feet of construction activities. It is

therefore considered within the Action Area. Peters Creek is listed in the Alaska Department of Fish and Game (ADFG) – Fish Distribution Database (FDD) as an anadromous stream (#247-50-10160) showing the occurrence of rearing Coho salmon (*Oncorhynchus kisutch*), and presence of Chinook (King) salmon (*Oncorhynchus tshawytscha*) and pink salmon (*Oncorhynchus gorbuscha*). Fishing reports show Chinook salmon, coho salmon, and steelhead have occurred inland of the Knik Arm, and that Coho salmon have been caught in Peters Creek (FishBrain, 2024).

Fish species included on the NMFS list are largely specific evolutionary significant units (ESUs). ESU's are, by definition, taxonomic identifications below the species level; however, these ESUs are considered to be a "distinct population segment" and, thus a "species" under the Endangered Species Act.

While Peters Creek is known to provide habitat for certain species of anadromous fishes, the federally listed ESU species identified do not spawn in Alaskan waters, they are unlikely to occur within the inland freshwaters of Alaska. While the ADFG database and fishing reports do not specify whether the species observed in Peters Creek were the federally listed ESU species identified, it is conservatively assumed some individuals of these species may be infrequent visitors to inland waters in close proximity to marine waters. While Peters Creek is not considered spawning habitat for federally listed species, impacts to individuals that may transiently occur within Peters Creek were assessed.

Effects of the Action

Construction

Paving and ground disturbance would be set back a minimum of 25 feet from Peters Creek, consistent with the setbacks in the Anchorage Municipal Code (AMC 21.45.210). **Alternative A** has been designed to have stable slopes following construction, including the placement of a retaining wall along the Peters Creek setback to stabilize an area with steep slopes that would otherwise be prone to erosion. As part of the Proposed Action, conditions of the NPDES Construction General Permit would be adhered to. Additionally, a Stormwater Pollution Prevention Plan (SWPPP) would be prepared, implemented, and maintained throughout the construction phase. The SWPPP would include BMPs discussed above to minimize on- and off-site stormwater effects to water quality during construction and ensure that water quality thresholds designated by the USEPA to protect the environment are not exceeded. With consideration of project BMPs, construction activities associated with the Proposed Action may affect, but is not likely to adversely affect, federally listed fish species.

Operations

The operation of the Proposed Action will result in an increase in impervious surfaces within the Project Area and, will result in stormwater runoff over these surfaces, including building footprints and parking stalls. Stormwater runoff over these surfaces has the potential to carry pollutants into surface water resources. Specifically, recent research has found 6PPD-quinone, a contaminant found in runoff from roadways, to be a major contributor to pre-spawning mortality in Coho salmon (Tian et al. 2021), suggesting that other fishes may similarly be impacted.

The Proposed Action will utilize the design guidelines found in the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0 (Municipality of Anchorage, 2017). Per the design criteria, the Proposed Action would be considered a large project and would be subject to water quality treatments, including bioretention, infiltration basins, vegetated swales, chamber systems, filter strips, and others. Conceptual stormwater designs include a mixture of vegetated swales and infiltration basins. Research suggests that methods such as vegetated swales are effective at sequestering 6PPD-quinone, which otherwise has a tendency to float. Other BMPs identified with potential to reduce 6PPD-quinone concentration include particle filtering to capture larger tire debris, flow control BMPs, and

source control BMPs (McIntyre et al., 2023; Ecology, 2022). As stormwater would be treated within the Project Area prior to discharge, and may be wholly infiltrated within the Project Area, no appreciable levels of 6PPD-quinone or other pollutants would enter Peters Creek from the Project Area.

An on-site septic system would be constructed to replace the existing septic and leech field system. The septic system would be registered with the USEPA under the Underground Injection Control (UIC) program as a Class V injection well and designed and installed consistent with the ADEC standards (ADEC 18 AAC 72 Wastewater Disposal, 18 AAC 72.530(e)(2)). By proper design and installation of the on-site wastewater system consistent with federal, tribal, and local standards and requirements, impacts to water quality from treatment and discharge of wastewater will be less than significant.

In consideration of project BMPs, the Proposed Action may affect, but is not likely to adversely affect federally listed fish.

Critical Habitat and EFH

There are no NMFS designated or proposed Critical Habitat within the Action Area. The nearest Critical Habitat is designated for Cook Inlet DPS beluga whale within the Knik Arm. The Knik Arm is over 0.5 miles of vegetated streambank downstream of the Project Area and is considered outside of the Action Area. The Proposed Action will have no effect on NMFS Critical Habitat.

Similarly, there are no NMFS EFH within the Action Area. The nearest EFH is in the Knik Arm for Alaska plaice, pink salmon, chum salmon, Chinook salmon, sockeye salmon, coho salmon, dover sole, yellowfin sole, and northern rockeye sole. The EFH is located out of the Action Area and over a half-mile downstream of the Project Area. The Proposed Action would not result in indirect water quality effects to aquatic habitat in Peters Creek, which is hydrologically connected to Knik Arm. Therefore, the Proposed Action would have no effect on NMFS EFH.

Conclusion

Based on the analysis that all effects of the Proposed Action will be insignificant and/or discountable, we have determined the Proposed Action is not likely to adversely affect any listed species or Critical Habitat under NMFS jurisdiction. We used the best scientific and commercial data available to complete this analysis. We request your written concurrence with this determination.

Please direct any questions regarding this letter to Harrilene Yazzie, Regional Environmental Scientist, via email at Harrilene.Yazzie@bia.gov, or by phone at 907-271-4004.

Sincerely,

For Jolene John
Regional Director

Enclosure

Biological Assessment

- Grading and Drainage Study
- Attachment A
- References

From: Sean McDermott - NOAA Federal <sean.mcdermott@noaa.gov>
Sent: Tuesday, December 17, 2024 7:14 AM
To: Sierra Franks - NOAA Federal <sierra.franks@noaa.gov>
Cc: Yazzie, Harrilene J <Harrilene.Yazzie@bia.gov>; akr.prd.section7@noaa.gov <akr.prd.section7@noaa.gov>; Chen, Glenn <Glenn.Chen@bia.gov>; Polacca, Lynn <Lynn.Polacca@bia.gov>; Andrews, Cyril J. <Cyril.Andrews@bia.gov>
Subject: [EXTERNAL] Re: Eklutna Native Village Proposed Class II Casino

Thanks for looping me in, Sierra.

Harrilene - I read through the section about fish resources and essential fish habitat (EFH). To clarify, EFH for Pacific salmon includes the freshwater habitat they use for spawning and rearing. While there is designated EFH in Peter's Creek, which was identified as being proximate to the project area, the project does not appear to impact the Creek. I agree with the final conclusion of no effect EFH. No further consultation is required.

-Sean

On Mon, Dec 16, 2024 at 1:32 PM Sierra Franks - NOAA Federal <sierra.franks@noaa.gov> wrote:

Hi Harrilene,

Thanks for sending this over. In reading through the cover letter it is not clear what you are requesting ESA Section 7 consultation on? Our division consults on ESA listed marine mammals and the proposed sunflower sea star, which it seems you are stating "no effect" from your project.

I am looping in our habitat conservation division (Sean cc'd here) that works with Essential Fish Habitat consultation, but we don't have any ESA listed fish species currently in Alaska. Please let us know if we can assist any further with this request that I may be missing from this initial one.

Thanks

Sierra E. Franks (*she/her*)

Branch Chief &

Regional Endangered Species Act Section 7 Coordinator

Alaska Region, Protected Resource Division, Anchorage

Phone: 907-531-6610

<https://www.fisheries.noaa.gov/about/alaska-regional-office>

On Thu, Dec 12, 2024 at 1:01 PM 'Yazzie, Harrilene J' via _NMFS AKR PRD Section 7 <akr.prd.section7@noaa.gov> wrote:

Good afternoon,

The BIA Alaska Region is submitting a request for an expediated consultation under section 7(a)(2) of the Endangered Species Act for the proposed class II casino proposed by the Eklutna Native Village.

The BIA determined that the Proposed Action may affect, but not likely to adversely affect, a federally listed fish species. This email includes a letter address to Ms. Sierra Franks requesting the expediated consultation and the Biological Assessment (Attachment 1). Should you have any questions, please do not hesitate to contact me directly via email at Harrilene.Yazzie@bia.gov or via cell at 907-280-8122.

Respectfully,
Harrilene

Harrilene Yazzie

Division of Environmental & Cultural Resources Management
Bureau of Indian Affairs - Alaska Region
3601 C Street, Suite 1100
Anchorage, AK 99503

Work: 907-271-4004
Work Cell: 907-280-8122

<https://www.bia.gov/regional-offices/alaska>

Biological Assessment

BIOLOGICAL ASSESSMENT



Eklutna Native Village Gaming Facility Project

Chugiak, AK | December 2024

Prepared For:

Bureau of Indian Affairs
Alaska Regional Office
3601 C Street Suite 1200
Anchorage, AK 99503



BIOLOGICAL ASSESSMENT

Eklutna Native Village Gaming Facility Project

Chugiak, AK | December 2024

Prepared For:

Bureau of Indian Affairs
Alaska Regional Office
3601 C Street Suite 1200
Anchorage, AK 99503



Prepared By:

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LIST OF ATTACHMENTS

- Attachment A Grading and Drainage Report
- Attachment B NMFS Species List
- Attachment C Species Observed
- Attachment D Site Photographs

Section 1 | Introduction

This Biological Assessment (BA) has been prepared to address the potential effects of the proposed Eklutna Native Village Gaming Facility Project (Proposed Action) on federally listed species. The Proposed Action includes the development and operation of a gaming facility and associated infrastructure within an approximately 6.37-acre property located in the unincorporated community of Chugiak within the municipality of Anchorage, Alaska (Project Area).

This BA is intended to facilitate consultation with the National Oceanic and Atmospheric Administration's (NOAA) National Marine Fisheries Service (NMFS) and has been prepared in accordance with legal requirements set forth under Section 7 of the Endangered Species Act (ESA; 16 USC 1536 (c)) and the Magnuson-Stevens Fishery Conservation and Management Act (Section 305) concerning effects of the Proposed Action. The purpose of this BA is to evaluate the Proposed Action in sufficient detail to determine the extent to which it may affect federally listed species, critical habitat, and/or Essential Fish Habitat (EFH) under the jurisdiction of NMFS. For the purposes of this BA, federally listed species include species that are federally listed as endangered or threatened, species that are formally proposed for listing, and species that are listed as candidate species under the ESA.

To fulfill its purpose, this BA:

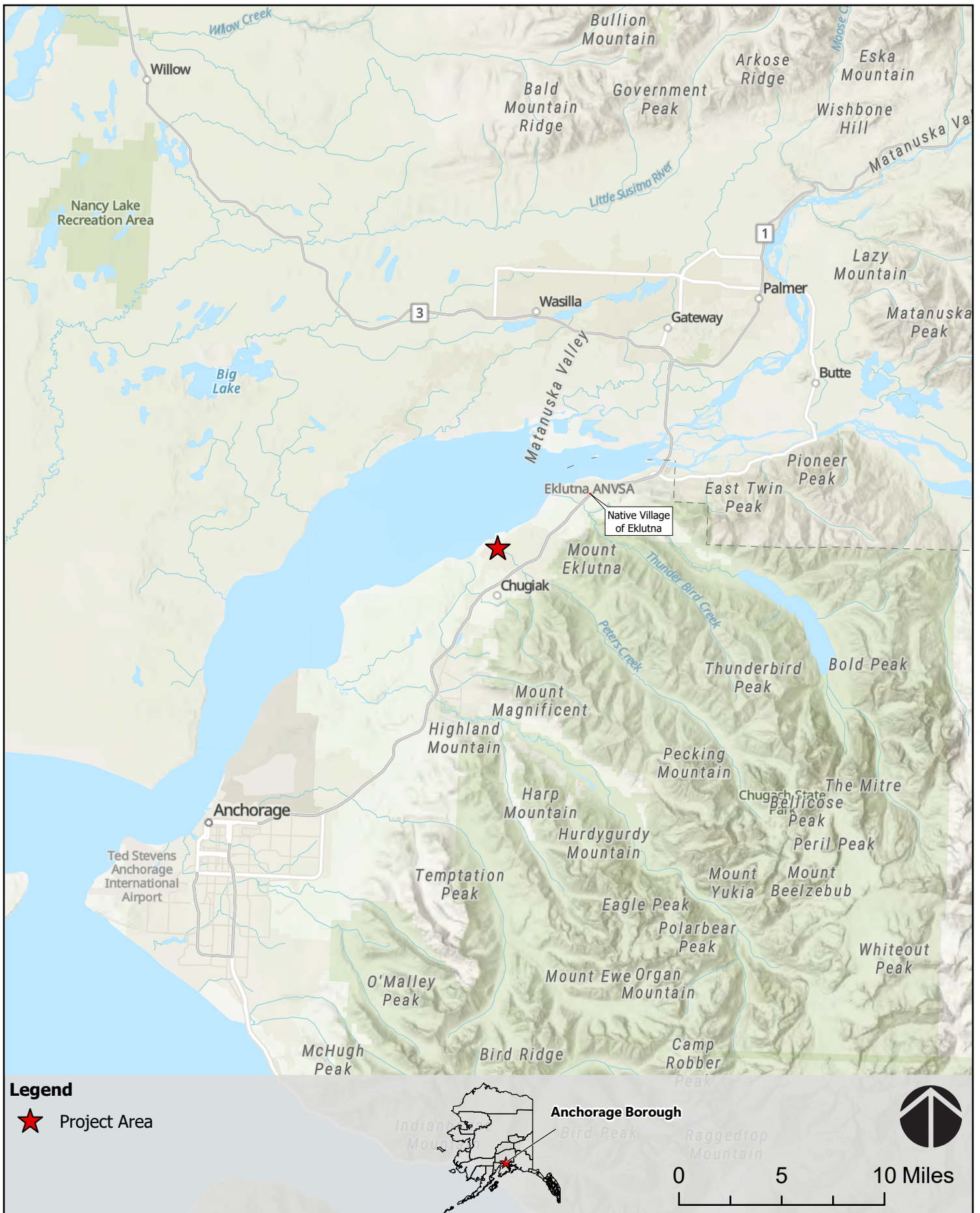
- Characterizes the habitat types present within the areas that may be impacted by the Proposed Action;
- Evaluates the potential for the occurrence of federally endangered, threatened, proposed, or candidate species under the ESA;
- Assesses the potential for the Proposed Action to adversely impact federally listed species, critical habitat, or EFH; and
- Recommends mitigation measures designed to avoid or minimize project-related impacts.

1.1 DESCRIPTION OF THE ACTION AREA

For the purpose of this report, the Project Area is defined as the 6.37-acre property that will be developed under the Proposed Action, and the Action Area includes all areas that may be directly or indirectly impacted by the Proposed Action. The Action Area includes the Project Area, as well as the portion of Peters Creek adjacent to the Project Area, and the downstream portion of Peters Creek. As over a half mile of vegetated creek exists between the Project Area and its discharge point at the Knik Arm, Knik Arm and waters into the ocean are not evaluated (**Figures 1, 2, and 3**).

1.2 PURPOSE OF THE PROPOSED ACTION

The federal Proposed Actions include the Bureau of Indian Affairs (BIA) approval of a business lease by members of the Tribe to the Eklutna Native Village tribal government for the development of a gaming facility on the Project Area and the potential approval of a timber harvest permit. The purpose and need for the Proposed Action is to allow the Native Allotment landowners to develop their land, putting it to its highest and best use, in order to generate much needed income and employment opportunities for the Native Allotment landowners and their community.



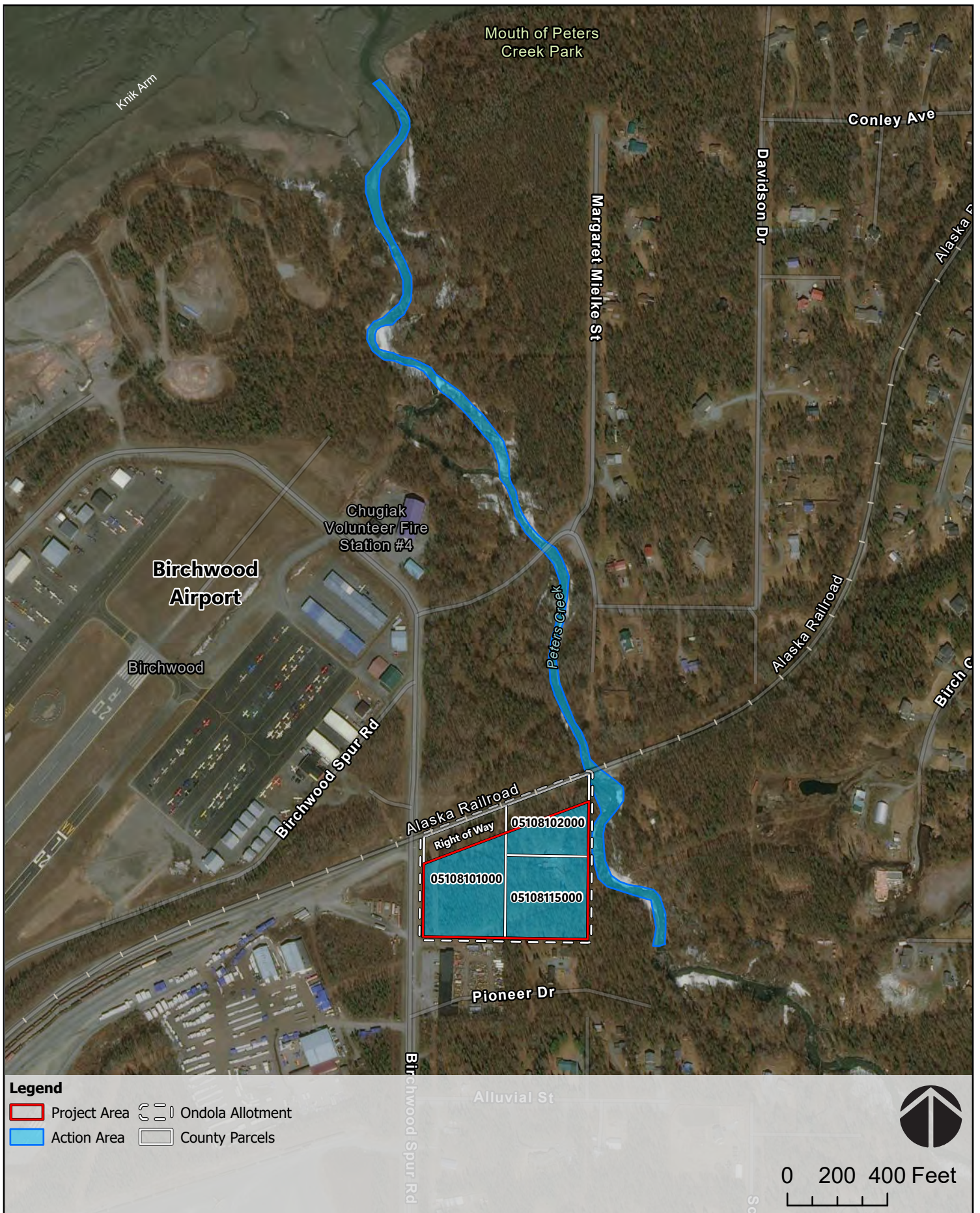
Kenai Peninsula Borough, Matanuska-Susitna Borough GIS, Municipality of Anchorage, State of Alaska, Esri, TomTom, Garmin, SafeGraph, FAO, METI/NASA, USGS, EPA, NPS, USFWS, Esri, USGS

FIGURE 1
REGIONAL LOCATION



Kenai Peninsula Borough, Matanuska-Susitna Borough GIS, Municipality of Anchorage, State of Alaska, Esri, TomTom, Garmin, SafeGraph, METI/NASA, USGS, EPA, NPS, USDA, USFWS

FIGURE 2
SITE AND VICINITY



Maxar, Microsoft, Kenai Peninsula Borough, Matanuska-Susitna Borough GIS, Municipality of Anchorage, State of Alaska, © OpenStreetMap, Microsoft, Esri, TomTom, Garmin, SafeGraph, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, USFWS

FIGURE 3
AERIAL PHOTOGRAPH

1.3 PROJECT LOCATION

The approximately 6.37-acre Project Area is in the unincorporated community of Chugiak within the municipality of Anchorage, Alaska. The Project Area consists of three parcels; Assessor's Parcel Numbers 05108101000, 05108102000, and 05108115000 that are currently owned by members of the Eklutna Native Village as Native allotment land under the Alaskan Native Allotment Act (ANAA). The Project Area is located off Birchwood Spur Road. The Alaska Railroad runs along the northern boundary of the Project Area, and the Birchwood Airport lies to the west. Other surrounding land uses include rural residential and industrial. The regional location is shown in **Figure 1**.

The Action Area is within Section 5 of Township 15 North, Range 1 West, Seward Base and Meridian, in the Anchorage B-7 NW U.S. Geological Service (USGS) quadrangle (**Figure 2**). An aerial image is provided in **Figure 3**. Elevations on the Action Area range from approximately 73 to 88 feet above mean sea level (amsl), with the majority of the site sloping gradually to the north. There is a bench cutting across the northwest corner of the property that drops about 8 feet then flattens out towards the adjacent bank of Peters Creek.

1.4 PROPOSED ACTION

Alternative A consists of the following components: 1) BIA approval of a business lease by members of the Tribe to the tribal government for operation of a proposed gaming facility within the Project Area; 2) development by the Tribe of a gaming facility and associated infrastructure on the Project Area; 3) the harvest and sale of timber resources on the Project Area; and 4) potential NIGC approval of a Gaming Management Agreement for operation of the gaming facility.

1.4.1 Development

A site plan is included as **Figure 4**. The Proposed Action includes a 58,000 square foot (sf) building with paved surface parking and supporting infrastructure. The gaming facility would consist of one-story (approximately 40 feet high) and would include approximately 32,000 square feet of gaming floor and approximately 26,000 square feet of support and circulation space. Proposed loading docks to accommodate deliveries would be located behind the building. The gaming facility would be open for up to 24 hours a day, 7 days a week.

Access to the Action Area is currently provided via a driveway and dedicated right-of-way along the southern boundary of the property that connects to Birchwood Spur Road. This driveway would be paved and improved to serve as the primary access for vehicles entering and exiting the Project Area (**Figure 4**). Development would include 443 paved surface parking spaces to accommodate employees and patrons.

Exterior signage would be compatible with the building architecture as well as the natural characteristics of the site. Exterior lighting would be strategically positioned around the building to minimize off-site glare. Downcast lighting and LED bulbs would be used in the landscaped and parking areas to minimize off-site scatter. The parking lot and areas around the building would be enhanced by landscaping that would incorporate plants native to the region.

There are no municipal connections for potable water supply or wastewater treatment within the Project Area. Therefore, water will be supplied via a proposed groundwater well, and an on-site wastewater treatment and disposal facility will be constructed.

1.4.2 Construction

Construction would involve grading, earthwork, paving, land clearing, and timber harvest. Construction would generally involve grubbing and clearing, grading and paving using heavy-duty and light-duty equipment, trenching for utilities, and construction of buildings. Construction equipment would consist of scrapers/earthmovers, wheeled or tracked bulldozers and loaders, dump trucks, and concrete trucks. Equipment and materials would be staged within the Action Area. Proposed facilities would be constructed generally consistent with applicable Tribal codes and in accordance with the International Building Code (IBC), including electrical, mechanical, plumbing, fire protection, and seismic standards. An indoor fire suppression system would be installed. Construction is estimated to commence in 2025 and would continue for a period of approximately 14 months. Paving and ground disturbance would be set back from Peters Creek by at least 25 feet, in accordance with the AMC (AMC 21.45.210).

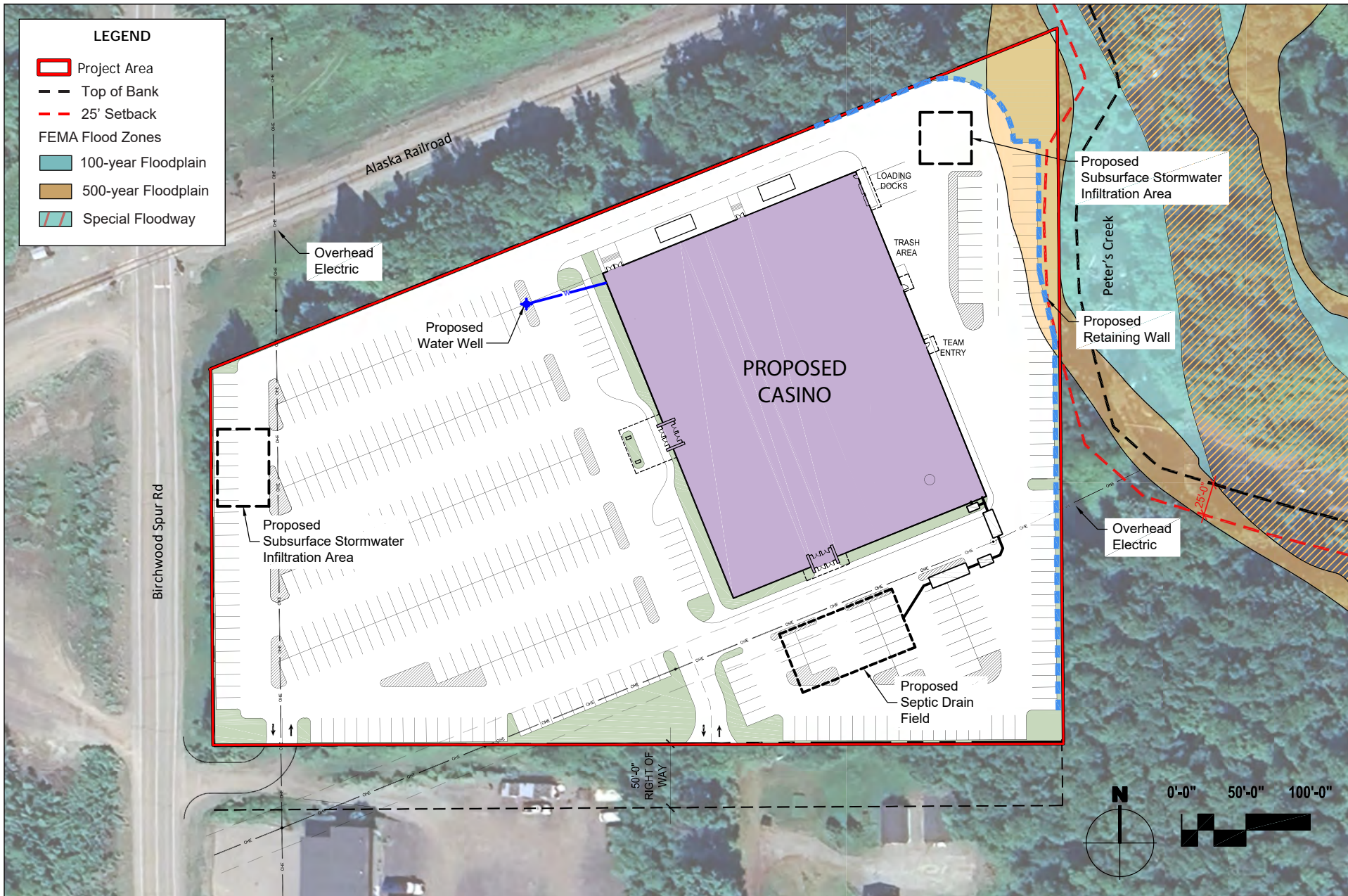
1.4.3 Timber Removal and Clearing

The Action Area consists largely of undeveloped forested land. Development would require timber removal and land clearing within the Action Area. Tree species within the Action Area are dominated by birch (*Betula* sp.), specifically, paper birch (*Betula papyrifera*). Other deciduous trees within the Action Area include alder (*Alnus* sp.) and poplar (*Populus* sp.). Spruce, including white spruce (*Picea glauca*) and red spruce (*Picea rubens*), comprise a minor component of the tree canopy. A few trees would be retained in the proposed parking and landscaping areas and along the perimeter of the Action Area. Merchantable trees would be donated to the Tribe to be utilized for construction of community facilities or sold as timber. Clearing activities would result in leftover vegetation and wood debris, such as stumps. The leftover vegetation and wood debris would be chipped on-site and utilized where feasible and as dictated by the quality of chips. A timber harvest permit may be required through the BIA.

1.4.4 Grading and Drainage

A grading and drainage study has been prepared and is included as **Attachment A**. Development would require retaining walls on the north and east sides of the Action Area, and the northeast corner would be raised by three to seven feet above the existing grade due to the steep drop off towards Peters Creek. The topmost foot of soil would be stripped across the Action Area to remove tree roots, organics, and topsoil/peat. This would equate to approximately 10,350 cubic yards (CY) of unusable material, which would be hauled off-site. Approximately 15,800 CY would be excavated. Of that, 4,200 CY would be reused across the Action Area to fill the lower areas. This would result in a net volume of 11,600 CY of unusable excavation. Approximately 20,500 CY of classified material would be imported for the pavement section.

Following construction, impervious areas would total approximately 6.37 acres. Trenching and excavation would occur to create stormwater detention and associated drainage infrastructure to provide volume control, treatment, and rate control. Stormwater would be collected and treated consistent with the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0 (Municipality of Anchorage, 2017). Vegetated swales would be established within the parking areas to filter runoff and storm drainage catch basins would be located at low points on the Project Area to direct runoff to buried infiltration beds to provide volume control, treatment, and rate control (**Appendix B**). The stormwater collection and treatment system would be designed to accommodate a 24-hour 100-year flood event with off-site runoff rates modeled to be equal to or less than existing rates.



Source: Marnell Companies, 6/5/2024, Acorn Environmental, 8/28/2024

FIGURE 4
 PROPOSED PROJECT AREA SITE PLAN

1.4.5 Best Management Practices

The following best management practices (BMPs) related to biological resources have been included as part of the Proposed Action:

- Proposed facilities would be constructed generally consistent with the AMC and in accordance with the IBC, including electrical, mechanical, plumbing, fire protection, and seismic standards.
- Construction and design would follow standard engineering practices related to grading and soil suitability.
- Paving and ground disturbance would be set back from Peters Creek by at least 25 feet, in accordance with the AMC (AMC 21.45.210).
- Proposed storm drainage catch basins would be located at low points on the Action Area to direct runoff to buried infiltration beds.
- Stormwater collection and treatment system would be designed to accommodate a 24-hour 100-year flood event with off-site runoff rates modeled to be equal to or less than existing rates.
- Stormwater collection and treatment facilities shall be designed with excess capacity to offset lost floodplain capacity from development within the 500-year floodplain. This capacity shall be in excess of modeled sizing compliant with the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0. Coverage under the National Pollutant Discharge Elimination System (NPDES) Construction General Permit (CGP) shall be obtained from the U.S. Environmental Protection Agency (USEPA) for construction site runoff during the construction phase in compliance with the Clean Water Act (CWA). Conditions of the NPDES General Construction Permit shall be adhered to. A Stormwater Pollution Prevention Plan (SWPPP) shall be prepared, implemented, and maintained throughout the construction phase consistent with CGP requirements. The SWPPP shall include BMPs to minimize stormwater effects to water quality during construction. These will include, but not be limited to, the following:
 - Temporary erosion control measures (such as silt fences, fiber rolls, staked straw bales, temporary re-vegetation, rock bag dams, erosion control blankets, and sediment traps) shall be employed as needed for disturbed areas.
 - Construction activities shall be scheduled to minimize land disturbance during peak runoff periods to the extent feasible.
 - Disturbed areas shall be paved, re-vegetated, and/or stabilized following construction.
 - A spill prevention and countermeasure plan shall be developed that identifies proper storage, collection, and disposal measures for potential pollutants (such as fuel, fertilizers, pesticides, etc.) used on-site.
 - Petroleum products shall be stored, handled, used, and disposed of properly in accordance with provisions of the CWA (33 USC §§ 1251 to 1387).
 - Construction materials shall be stored, covered, and isolated to prevent runoff loss and contamination of surface and groundwater.
 - Fuel and vehicle maintenance areas shall be limited to the Action Area.
 - To minimize dust generation during construction, soil shall be wetted down with water prior to ground disturbance as needed.
 - Trash storage areas for receptacles will be designed to minimize stormwater runoff contact with disposed solid waste. Receptacles shall contain lids and shall be placed on impervious pavement. Receptacles along with signs encouraging use of trash receptacles will be placed in common areas to reduce littering.

Section 2 | Study Methods

2.1 LITERATURE REVIEW

Prior to conducting biological resources surveys within the Action Area, the following sources were reviewed:

- U.S. Geological Survey (USGS) 7.5-minute Anchorage B-7 NW and NE topographic quadrangles;
- Color aerial photography of the Action Area and vicinity;
- U.S. Fish and Wildlife Service (USFWS) National Wetland Inventory Mapper (USFWS, 2024a; **Figure 5**);
- Federal Emergency Management Agency (FEMA) flood map (**Figure 6**);
- NOAA Critical Habitat and EFH mapper (NMFS, 2024a, b);
- NOAA Species Directory (NMFS, 2024c);
- NRCS Soil report (NRCS, 2024); and
- The USFWS Information for Planning and Consultation (USFWS, 2024b). No species were returned on the USFWS list.

The NMFS website was reviewed to determine the appropriate species under NMFS jurisdiction that warrant evaluation within this BA. The Action Area falls within the Alaska Region. **Attachment B** includes the list of species provided by NMFS that may occur within the Alaska Region geographic area. The following federally listed species were identified in the NMFS list included in **Attachment B** and are evaluated herein:

Whales:

- Bowhead Whale (*Balaena mysticetus*): endangered
- Sei Whale (*Balaenoptera borealis*): endangered
- Blue Whale (*Balaenoptera musculus*): endangered
- Fin Whale (*Balaenoptera physalus*): endangered
- Cook Inlet Distinct Population Segment (DPS) Beluga Whale (*Delphinapterus leucas*): endangered
- Western North Pacific DPS Gray Whale (*Eschrichtius robustus*): endangered
- North Pacific Right Whale (*Eubalaena japonica*): endangered
- Mexico DPS Humpback Whale (*Megaptera novaeangliae*): threatened
- Western North Pacific DPS Humpback Whale (*Megaptera novaeangliae*): endangered
- Sperm Whale (*Physeter macrocephalus*): endangered

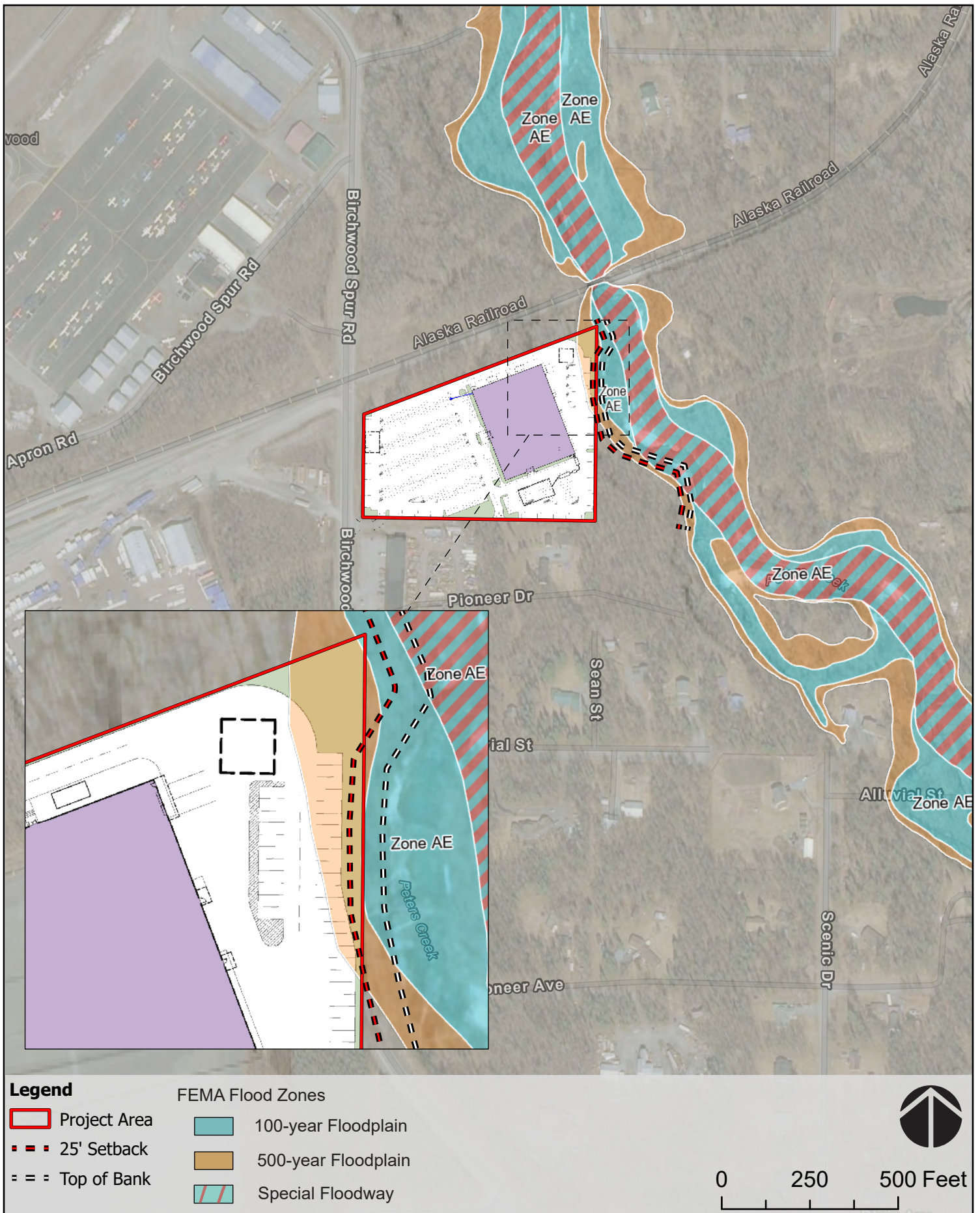
Pinnipeds:

- Arctic Ringed Seal (*Phoca hispida hispida*): threatened
- Beringia DPS, Bearded Seal (*Erignathus barbatus nauticus*): threatened
- Western DPS Steller Sea Lion (*Eumetopias jubatus*): endangered



Maxar, Microsoft, Kenai Peninsula Borough, Matanuska-Susitna Borough GIS, Municipality of Anchorage, State of Alaska, Esri, TomTom, Garmin, SafeGraph, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, USFWS, Airbus, USGS, NGA, NASA, CGIAR, NCEAS, NLS, OS, NMA, Geodatastyrelsen, GSA, GSI and the GIS User Community

FIGURE 5
NATIONAL WETLAND INVENTORY



Maxar, Microsoft, Airbus, USGS, NGA, NASA, CGIAR, NCEAS, NLS, OS, NMA, Geodatastyrelsen, GSA, GSI and the GIS User Community, Matanuska-Susitna Borough, Microsoft, Kenai Peninsula Borough, Matanuska-Susitna Borough GIS, Municipality of Anchorage, State of Alaska, © OpenStreetMap, Microsoft, Esri, TomTom, Garmin, SafeGraph, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, USFWS

FIGURE 6
FEMA FLOOD MAP

Reptiles:

- Loggerhead Sea Turtle (*Caretta caretta*): threatened
- Green Sea Turtle (*Chelonia mydas*): threatened
- Leatherback Sea Turtle (*Dermochelys coriacea*): endangered
- Olive Ridley Sea Turtle (*Lepidochelys olivacea*): threatened

Fish:

- Green Sturgeon (Southern DPS)(*Acipenser medirostris*): threatened
- Hood Canal Summer-run Chum Salmon (*Oncorhynchus keta*): threatened
- Lower Columbia River Coho Salmon (*Oncorhynchus kisutch*): threatened
- Lower Columbia River Steelhead (*Oncorhynchus mykiss*): threatened
- Middle Columbia River Steelhead (*Oncorhynchus mykiss*): threatened
- Snake River Basin Steelhead (*Oncorhynchus mykiss*): threatened
- Upper Columbia River Steelhead (*Oncorhynchus mykiss*): threatened
- Upper Willamette River Steelhead (*Oncorhynchus mykiss*): threatened
- Snake River Sockeye Salmon (*Oncorhynchus nerka*): endangered
- Lower Columbia River Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Puget Sound Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Snake River Fall Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Snake River Spring/Summer-run Chinook Salmon (*Oncorhynchus tshawytscha*): threatened
- Upper Columbia River Spring Chinook Salmon (*Oncorhynchus tshawytscha*): endangered
- Upper Willamette River Chinook Salmon (*Oncorhynchus tshawytscha*): threatened

Marine Invertebrates:

- Sunflower sea star (*Pycnopodia helianthoides*): proposed threatened

Species listed as Proposed Threatened are not subject to Section 7 consultation. Therefore, effects to sunflower sea star are not formally addressed within this report, however, it is noted that suitable habitat is not located within the Action Area for this species.

It is noted by NMFS that the fish listed above spawn on the west coast of the lower contiguous 48 states but may occur in Alaskan waters during the marine phase of their life cycles.

2.2 BIOLOGICAL RESOURCES SURVEY

A biological resources survey was conducted within the Project Area on July 16-18, 2024. The survey also covered the portion of Peters Creek adjacent to the Project Area and downstream, as walkable. Peters Creek was observed to be immediately adjacent to the Project Area at its closest point. The survey consisted of walking transects throughout the Project Area to identify plant and animal species, characterize habitat types and vegetation communities, and assess the potential for habitats to support federally listed species. Habitat requirements for each federally listed species were assessed and compared to the type and quality of habitats observed during the biological resources survey of the Project Area.

2.3 DEFINITION OF POTENTIAL EFFECT DETERMINATIONS

Five possible determinations exist regarding a project's effects on species listed under the ESA. These determinations are as follows:

- **No effect:** The appropriate conclusion when it is determined that a project would not affect a listed species or designated critical habitat.
- **May affect but is not likely to adversely affect:** The appropriate finding when effects on ESA-protected species are expected to be discountable, insignificant, or completely beneficial.
- **May affect and is likely to adversely affect:** The appropriate finding if any adverse effect to listed species may occur as a direct or indirect result of a project or its interrelated or interdependent actions and the effect is not discountable, insignificant, or beneficial. In the event the overall effect of the project is beneficial to a ESA-protected species but is also likely to cause some adverse effects, then the project "is likely to adversely affect" the listed species. If incidental take is anticipated to occur as a result of a project, an "is likely to adversely affect" determination should be made.
- **Is likely to jeopardize the continued existence of a listed species or result in the destruction or adverse modification of critical habitat:** The appropriate determination when the action agency or NMFS identify situations where a project is likely to jeopardize the species or adversely modify critical habitat. Jeopardy occurs when a project is likely to directly or indirectly appreciably reduce the likelihood of both the survival and recovery of a protected species in the wild by reducing their reproduction, numbers, or distribution. Destruction or adverse modification of critical habitat is a direct or indirect alteration that appreciably diminishes the value of critical habitat for the survival or recovery of a listed species. Such alterations include, but are not limited to, alterations adversely modifying any of those physical or biological features that were the basis for determining the habitat to be critical.
- **Is likely to jeopardize a proposed species or adversely modify proposed critical habitat:** The appropriate conclusion if a project is likely to jeopardize the continued existence of a proposed species or adversely modify proposed critical habitat.

Section 3 | Environmental Setting

3.1 HABITAT TYPES

Habitat types in the Action Area consist of boreal forest, ruderal/developed, and Peters Creek. These habitat types are described below and are shown in **Figure 7**. A list of species observed is included as **Attachment B**, and representative photos are provided in **Attachment C**.

3.1.1 Boreal Forest (5.13 acres)

Boreal forest within the Action Area was dominated by birch trees, specifically paper birch. Other deciduous trees within the boreal forest included alders and poplars. Spruce, including white spruce and red spruce, comprise a minor component of the tree canopy. The shrub layer is generally absent from the understory and was limited to young trees and scattered willows (*Salix* sp.). The forest floor contained a dense mixture of species that left little to no ground visibility. In some areas, ground cover was dominated by Devil's club (*Oplopanax horridus*) or cow parsley (*Anthriscus sylvestris*). Other commonly occurring species included lady fern (*Athyrium filix*), fireweed (*Chamaenerion angustifolium*), meadow horsetail (*Equisetum pratense*), prickly rose (*Rosa acicularis*), raspberry (*Rubus idaeus*), and claspleaf twisted stalk (*Streptopus amplexifolius*). Tracks from use of ATVs on the Action Area were observed.

3.1.2 Ruderal/Developed (1.24 acres)

Ruderal/developed areas included those portions of the Action Area where human use and activity have converted habitats from their natural condition. A dirt access drive was observed leading to a cleared area previously used as a residential site. Residential utilities (well, septic) were observed. Patches of this habitat were observed along the access drive where heavy machinery had been used to clear piled solid waste. Vegetation in these areas was generally absent. An electrical transmission line was also observed crossing the southern portion of the Action Area. Vegetation had been cleared along this row and was limited to ground cover cropped low. Finally, a cleared area is maintained along the northern boundary of the Action Area associated with the adjacent off-site railroad.

3.1.3 Peters Creek

Peters Creek runs along and just outside the eastern boundary of the Project Area. Peters Creek is identified as the Riverine feature on the NWI map provided as **Figure 5**. A sliver of the floodplain from Peters Creek overlaps with the Project Area, as shown in **Figure 6**. There is potential for runoff to enter Peters Creek from the Project Area. Therefore, Peters Creek adjacent to the Project Area to its terminal at Knik Arm is considered part of the Action Area. Peters Creek along the Project Area was observed during the survey. Flows were fast and relatively consistent across the width and length of the creek that was observed. Observed substrate was dominated by gravel to fist-sized cobble intermixed with larger, smooth river rock. Sandy substrate made up a minor component of the visible bed and bank. Water depth appeared relatively shallow where depth was visible. It is likely depth is relatively shallow across the width given the relatively consistent flow observed. The banks consisted of a steep drop across most of the observed area, with the exception of approximately 20 feet of bank with a more terraced, sloping topography. This is consistent with the described conditions along the totality of the creek's length, which is described as swift, shallow, and rocky (Alaska Outdoors Supersite, n.d.).



Kenai Peninsula Borough, Matanuska-Susitna Borough GIS, Municipality of Anchorage, State of Alaska, © OpenStreetMap, Microsoft, Esri, TomTom, Garmin, SafeGraph, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, USFWS

FIGURE 7
HABITAT TYPES

Section 4 | Results

This section provides an account of species identified in the NMFS list for the region as well as a discussion of the potential for each species to occur in the Action Area and the potential impacts.

4.1 EFFECTS ON FEDERALLY LISTED SPECIES

4.1.1 Whales

The NMFS list included as **Attachment B** identifies 10 federally listed whale species with the potential to occur within the Alaska Region. Peters Creek is the only surface water resource within the Action Area. As discussed above, Peters Creek is a shallow freshwater body and therefore would not provide habitat for whales. Thus, federally listed whales do not have the potential to occur within the Action Area, and the Proposed Action would have no effect on federally listed whales.

4.1.2 Pinnipeds

The NMFS list included as **Attachment B** identifies three federally listed pinniped species with the potential to occur within the Alaska Region. Peters Creek is the only surface water resource within the Action Area. As discussed above, Peters Creek is a shallow freshwater body and therefore would not provide habitat for pinnipeds. Thus, federally listed pinnipeds do not have the potential to occur within the Action Area, and the Proposed Action would have no effect on federally listed pinnipeds.

4.1.3 Reptiles

The NMFS list included as **Attachment B** identifies four federally listed reptile species with the potential to occur within the Alaska Region. All of these species are sea turtles. As discussed above, Peters Creek is the only surface water resource within the Action Area and is a shallow freshwater body. Peters Creek therefore would not provide habitat for sea turtles. Thus, federally listed reptiles do not have the potential to occur within the Action Area, and the Proposed Action would have no effect on federally listed reptiles.

4.1.4 Fish

Federally listed fish species identified on the NMFS list in **Attachment B** are anadromous species that are reared in freshwater and spend their adult life in saltwater. Adults return to their natal freshwaters to spawn. As noted in **Attachment B** and above, federally listed anadromous species do not spawn in the Alaska Region, but they may occur in Alaskan waters during the marine stages of their life cycle. Threats to these species often consist of loss of habitat, especially spawning habitat, degradation of habitat, overfishing or bycatch, and competition with invasive species.

There is no habitat for anadromous fish species in the Project Area. Peters Creek, however, is hydrologically connected to the Project Area and located within 25 feet of construction activities and is therefore considered within the Action Area. Peter's Creek is listed in the Alaska Department of Fish and Game (ADFG) – Fish Distribution Database (FDD) as an anadromous stream (#247-50-10160) showing the occurrence of rearing Coho salmon, and presence of Chinook (King) salmon and pink salmon (ADFG, 2024 a). Further, fishing reports show that Chinook salmon, coho salmon, and steelhead have occurred inland of the Knik Arm, and that coho salmon have been caught in Peters Creek (FishBrain, 2024).

Fish species included in **Attachment B** are largely specific evolutionary significant units (ESUs). ESUs are, by definition, taxonomic identifications below the species level; however, these ESUs are considered to be a "distinct population segment" and thus a "species" under the Endangered Species Act. While Peters Creek is known to provide habitat for certain species of anadromous fishes, the federally listed ESU species identified in **Attachment B** do not spawn in Alaskan waters and therefore are unlikely to occur within the inland freshwaters of Alaska.

While the ADFG database and fishing reports do not specify whether the species observed in Peters Creek were the federally listed ESU species identified in **Attachment B**, it is conservatively assumed that at least some individuals of these species may be infrequent visitors to inland waters in close proximity to marine waters. Therefore, while Peters Creek is not considered spawning habitat for federally listed species, impacts to individuals that may transiently occur within Peters Creek are assessed herein.

Potential Impacts

Construction

As part of the Proposed Action, a SWPPP would be required and would be reviewed and approved by the USEPA prior to construction. Adherence to the SWPPP and a list of SWPPP BMPs are discussed in **Section 1.4.1**. The SWPPP will ensure that water quality thresholds designated by the USEPA to protect the environment are not exceeded and thus will prevent significant adverse effects to Peters Creek during construction of the Proposed Action.

Operation

Operation of the Proposed Action would result in an increase in impervious surfaces within the Project Area and would result in stormwater runoff over these surfaces, including building footprints and parking stalls. Stormwater runoff over these surfaces has the potential to carry pollutants into surface water resources. Specifically, recent research has found 6PPD-quinone, a contaminant found in runoff from roadways, to be a major contributor to pre-spawning mortality in coho salmon (Tian et al. 2021), suggesting that other fishes may similarly be impacted. The effects of this contaminant on other species, including Chinook salmon and steelhead are still being studied, but some of the early studies indicate that first flush storm events are lethal to juvenile coho, steelhead, and Chinook salmon (French et al. 2022). However, the concentration at which 6PPD-quinone in stormwater may have toxic effects on those species is still being evaluated. Another common component of stormwater runoff is copper, which can impair the olfactory system of salmonids and hinder their predator avoidance behavior (Sandahl et al., 2007).

Research has shown that in addition to juvenile coho, both juvenile steelhead (*Oncorhynchus mykiss*) and juvenile Chinook salmon (*Oncorhynchus tshawytscha*) are also vulnerable to untreated stormwater containing 6PPD-quinone; however, the effects are not as immediate and instead of mortality occurring within a few hours of exposure (up to 4 hours for coho salmon), it can take one or two days for steelhead and Chinook salmon to display mortality (French et al., 2022). A recent study looked at the sensitivity of early life stage (~ three weeks post swim-up [newly feeding] coho salmon and Chinook salmon) and found that juvenile coho salmon are three orders of magnitude more sensitive to 6PPD-quinone than Chinook salmon (Lo et al., 2023). This study found only 7.1 percent survival for coho salmon exposed to the lowest concentration treatment group, while Chinook salmon showed 61.4 percent survival in the highest concentration treatment group (Lo et al., 2023).

While not showing the degree of sensitivity to stormwater runoff containing 6PPD-quinone as coho salmon, juvenile steelhead and Chinook do appear to exhibit direct mortality, although at higher concentrations and with longer response times (Tien et al., 2021; French et al., 2022; Lo et al., 2023; and Brinkman et al., 2022).

As discussed in **Section 1**, the Proposed Action would utilize the design guidelines as found in the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0 (Municipality of Anchorage, 2017). Per the design criteria, the Proposed Action would be considered a large project and would be subject to water quality treatments, including bioretention, infiltration basins, vegetated swales, chamber systems, filter strips, and others. Conceptual stormwater designs include a mixture of vegetated swales and infiltration basins. Research suggests that methods such as vegetated swales are effective at sequestering 6PPD-quinone, which otherwise has a tendency to float. Other BMPs identified with potential to reduce 6PPD-quinone concentration includes particle filtering to capture larger tire debris, flow control BMPs, and source control BMPs (McIntyre et al., 2023; Ecology, 2022).

As stormwater would be treated within the Project Area prior to discharge, and may be wholly infiltrated within the Project Area, no appreciable levels of 6PPD-quinone or other pollutants would enter Peters Creek from the Project Area. With consideration of project BMPs, the Proposed Action may affect, but is not likely to adversely affect federally listed fish.

Recommended Impact Avoidance, Minimization, or Mitigation Measures

None.

4.1.5 Marine Invertebrates

The NMFS list included as **Attachment B** identifies one federally listed marine invertebrate with the potential to occur within the Alaska Region. Peters Creek is the only surface water resource within the Action Area. As discussed above, Peters Creek is a shallow freshwater body and therefore would not provide habitat for marine invertebrates. Thus, federally listed marine invertebrates do not have the potential to occur within the Action Area, and the Proposed Action would have no effect on federally listed marine invertebrates.

4.2 EFFECTS ON CRITICAL HABITAT AND EFH

There is no NMFS designated or proposed critical habitat within the Action Area (NMFS, 2024a). The nearest critical habitat is designated for Cook Inlet DPS beluga whale within the Knik Arm, over a half mile downstream of the Project Area. As discussed in **Section 1.1**, the Knik Arm is over 0.5 miles of vegetated streambank downstream of the Project Area and is considered outside of the Action Area. Therefore, the Proposed Action would have no effect on NMFS critical habitat.

Similarly, there is no NMFS EFH within the Action Area (NMFS, 2024b). The nearest EFH is within the Knik Arm for Alaska plaice, pink salmon, chum salmon, Chinook salmon, sockeye salmon, coho salmon, dover sole, yellowfin sole, and northern rockeye sole. This EFH is located out of the Action Area and over a half mile downstream of the Project Area. As discussed above in Section 4.1.4, the Proposed Action would not result in indirect water quality effects to aquatic habitat in Peters Creek, which is hydrologically connected to Knik Arm. Therefore, the Proposed Action would have no effect on NMFS EFH.

Section 5 | Interrelated and Interdependent Effects

Interrelated and interdependent actions associated with the Proposed Action are those actions that have no independent utility and/or are dependent on the Proposed Action for their justification. Interrelated and interdependent actions that will likely occur as a result of the Proposed Action include the construction of access improvements to the Project Area.

Implementation of site access improvements would fall within roadside shoulders with ruderal vegetation and no surface waters, where significant biological resources do not occur and suitable habitat for species listed under the ESA would not occur. Additionally, these actions would be subject to the BMPs listed in Section 1.4.1, including erosion control, adherence to necessary permitting, and other standard measures protective of water quality. Therefore, interrelated and interdependent impacts to federally listed species would not occur.

Section 6 | Cumulative Effects

Cumulative effects resulting from the Proposed Action could occur if other developments along Peters Creek occurred and resulting in adverse impacts to federally listed species in combination with the Proposed Action. Property alongside Peters Creek from its discharge at Knik Arm upstream for approximately three miles is developed, predominantly with rural residential properties. These parcels contain minimal paved surfaces and a significant amount of tree canopy and permeable ground (**Figure 3**). There are no known future developments along Peters Creek in the vicinity of the Action Area. Therefore, cumulative impacts would not be incurred.

Section 7 | Conclusions and Determination

This Biological Assessment is respectfully submitted to NMFS for review and concurrence on the determinations presented herein and summarized below.

Federally listed Whales: The Proposed Action will have no effect on these species for the following reason:

- There is no potential for these species to occur within the Action Area.

Federally listed Pinnipeds: The Proposed Action will have no effect on these species for the following reason:

- There is no potential for these species to occur within the Action Area.

Federally listed Reptiles: The Proposed Action will have no effect on these species for the following reason:

- There is no potential for these species to occur within the Action Area.

Federally listed Fish: The Proposed Action will may affect but is not likely to adversely affect these species for the following reason:

- Potential for these species to occur is low and limited to transient individuals.
- Suitable habitat will not be converted.
- Indirect impacts to water quality would be avoided or minimized by construction and operational stormwater BMPs.

Federally listed Marine Invertebrates: The Proposed Action will have no effect on these species for the following reason:

- There is no potential for these species to occur within the Action Area.

NMFS Critical Habitat and EFH: The Proposed Action will have no effect on NMFS critical habitat or EFH for the following reason:

- There is no critical habitat or EFH within the Action Area.

Section 8 | References

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Section 9 | Qualifications of Preparers

9.1.1 Kelli Raymond, B.S.

Ms. Raymond holds a B.S. in Animal Biology with a focus on Wildlife Ecology. She has approximately 10 years of experience collecting field data and preparing environmental assessments. Ms. Raymond has worked in several states across the U.S. performing biological resources surveys, including plant surveys, wetland delineations, and wildlife utilization monitoring. She also has experience live handling numerous wildlife species, including fish, migratory birds, and big game. Ms. Raymond is experienced in the preparation of Biological Assessments and Section 7 consultation with both the USFWS and NMFS under the federal Endangered Species Act.

9.1.2 Kimberly Fuchs, B.S.

Ms. Fuchs holds a B.S. in Biology with a specialization in Marine Biology and a minor in Psychology. She also has an A.S. in Biology, A.S. in Math and Science, A.S. in Psychology, A.A. in Liberal Arts and A.A. in General Studies. Ms. Fuchs has over 20 years of experience in environmental research, field experience, permitting, and preparing environmental documents. She has worked in several states across the U.S., including Alaska, California, Texas, and Oklahoma performing biological field studies including wetland delineations, break up studies, bathymetry, marine mammal observations, fisheries, riparian assessments, and water quality data. She has also performed protected species observer/fisheries observer duties in the Bering Sea, Gulf of Alaska, and Beaufort Seas. Ms. Fuchs is experienced in the preparation of Biological Assessments, Biological Opinions, and Section 7 consultation with both the USFWS and NMFS under the Endangered Species Act.

Attachment A
Grading and Drainage Report

Attachment B
NMFS Species List

Hood Canal Summer-run Chum Salmon*	<i>Oncorhynchus keta</i>	Threatened	No
Lower Columbia River Coho Salmon*	<i>Oncorhynchus kisutch</i>	Threatened	No
Lower Columbia River Steelhead*	<i>Oncorhynchus mykiss</i>	Threatened	No
Middle Columbia River Steelhead*	<i>Oncorhynchus mykiss</i>	Threatened	No
Snake River Basin Steelhead*	<i>Oncorhynchus mykiss</i>	Threatened	No
Upper Columbia River Steelhead*	<i>Oncorhynchus mykiss</i>	Threatened	No
Upper Willamette River Steelhead*	<i>Oncorhynchus mykiss</i>	Threatened	No
Snake River Sockeye Salmon*	<i>Oncorhynchus nerka</i>	Endangered	No
Lower Columbia River Chinook Salmon*	<i>Oncorhynchus tshawytscha</i>	Threatened	No
Puget Sound Chinook Salmon*	<i>Oncorhynchus tshawytscha</i>	Threatened	No
Snake River Fall Chinook Salmon*	<i>Oncorhynchus tshawytscha</i>	Threatened	No
Snake River Spring/Summer-run Chinook Salmon*	<i>Oncorhynchus tshawytscha</i>	Threatened	No
Upper Columbia River Spring Chinook Salmon*	<i>Oncorhynchus tshawytscha</i>	Endangered	No
Upper Willamette River Chinook Salmon*	<i>Oncorhynchus tshawytscha</i>	Threatened	No

*These species spawn on the West Coast of the Lower 48, but may occur in Alaskan waters during the marine phase of their life cycles

Marine Invertebrates

Common Name	Scientific Name	Status	Critical Habitat in AK?
Sunflower sea star	<i>Pycnopodia helianthoides</i>	Proposed Threatened	No

More Information

- › [ESA Species Directory \(searchable\)](#)
- › [Section 7 Consultations in Alaska](#)
- › [Expedited Informal Consultation Process in Alaska](#)
- › [Endangered Species Directory](#)
- › [ESA Petition and Listing Process](#)
- › [Endangered Species Conservation](#)

Last updated by [Alaska Regional Office](#) on 01/10/2024

Attachment C
Species Observed

Species Observed During the Biological Survey

Common Name	Scientific Name
PLANTS	
Yarrow	<i>Achillea millefolium</i>
Gray alder	<i>Alnus incana</i>
Green alder	<i>Alnus viridis</i>
Eastern bluestar	<i>Amsonia tabernaemontana</i>
Cow parsley	<i>Anthriscus sylvestris</i>
Lady fern	<i>Athyrium filix</i>
Yellow birch	<i>Betula alleghaniensis</i>
Black birch	<i>Betula lenta</i>
Red birch	<i>Betula occidentalis</i>
Paper birch	<i>Betula papyrifera</i>
Canadian bluejoint	<i>Calamagrostis canadensis</i>
Fireweed	<i>Chamaenerion angustifolium</i>
Bunchberry dogwood	<i>Cornus canadensis</i>
Meadow horsetail	<i>Equisetum pratense</i>
European ash	<i>fraxinus excelsior</i>
Wintergreen	<i>Gaultheria procumbens</i>
Yellow avens	<i>Geum aleppicum</i>
Cudweed	<i>Gnaphalium spp.</i>
Oak fern	<i>Gymnocarpium dryopteris</i>
Cow parsnip	<i>Heracleum maximum</i>
Oxeye daisy	<i>Leucanthemum vulgare</i>
Devils club	<i>Oplopanax horridus</i>
White spruce	<i>Picea glauca</i>
Red spruce	<i>Picea rubens</i>
Balsam poplar	<i>Populus balsamifera</i>
Black poplar	<i>Populus nigra</i>
Alder buckthorn	<i>Rhamnus frangula</i>
Arctic lebox	<i>Rhinanthus minor ssp. groenlandicus</i>
Labrador tea	<i>Rhododendron groenlandicum</i>
Red currant	<i>Ribes rubrum</i>
Prickly rose	<i>Rosa acicularis</i>
Raspberry	<i>Rubus idaeus</i>
Bebbs willow	<i>Salix bebbiana</i>
Willow	<i>Salix sp.</i>
Interrupted clubmoss	<i>Spinulum annotinum</i>
Claspleaf twisted stalk	<i>Streptopus amplexifolius</i>
Dandelion	<i>Taraxacum</i>
White clover	<i>Trifolium repens</i>
Highbush cranberry	<i>Viburnum trilobum</i>

American vetch	<i>Vicia americana</i>
ANIMALS	
Moose	<i>Alces alces</i>
Dog	<i>Canis lupus familiaris</i>
Mosquito	<i>Culicidae sp.</i>
Dark-eyed junco	<i>Junco hyemalis</i>
American robin	<i>Turdus migratorius</i>
Black bear	<i>Ursus americanus</i>

Attachment D

Representative Photographs



Representative photo of ruderal/developed habitat with boreal forest in the background.



View of Peters Creek looking upstream from terraced streambank.



View of Peters Creek looking downstream from terraced streambank. Large woody debris observed.



View looking across Peters Creek showing eroded drop off.

Appendix H

Cultural Resources Inventory and
Evaluation

(CONFIDENTIAL – AVAILABLE UPON REQUEST)

SHPO Consultation Request



United States Department of the Interior

BUREAU OF INDIAN AFFAIRS

Alaska Regional Office
3601 C Street, Suite 1200
Anchorage, Alaska 99503

IN REPLY REFER TO
[DECRM/6200]

8/23/2024

Judith Bittner
State Historic Preservation Officer
Office of History & Archaeology
550 West 7th Avenue
Anchorage, AK 99501

Dear Ms. Bittner:

The Bureau of Indian Affairs, Alaska Region (BIA) received a request for the development of the Eklutna Native Village Gaming Facility Project on the Olga Ondola's Alaska Native Allotment (AA-55026). The survey and report were conducted by Mike Taggart, RPA, from Acorn Environmental, and submitted to the BIA Archeology department for assessment. The BIA Archeology Department has reviewed the report, and determined that it was sufficient, and support the conclusion that the Undertaking would have a No Historic Properties Affected [36 CFR §800.4(d)(1)].

Additionally, it has come to the attention of the BIA Archeology Department that the Eklutna Tribe has begun work on the project location and has cleared land and conducted some ground disturbance work for preparation of the proposed construction. BIA Forestry visited the property and gave the BIA Archeology department photographs and GIS data to show the extent of the disturbance (Attachment 2). While this work has begun prior to your Concurrence or a NEPA evaluation, however office suggests that there likely was no damage to cultural resources as the report was completed and submitted to our office prior to the work.

With our department determining that the Eklutna Native Village Gaming Facility Project on the Olga Ondola's Alaska Native Allotment (AA-55026) will result in a **No Historic Properties Affected**, and we request concurrence with your office. If you have any further questions or concerns, feel free to contact our office at joseph.sparaga@bia.gov, or at (907) 271-4003.

Sincerely,

Joseph Sparaga
Regional Archeologist

Appendix I

EJScreen Community Report

EJScreen Community Report

This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

Anchorage, AK

Tract: 02020000102

Population: 4,323

Area in square miles: 70.90



August 7, 2024
Project 1
11705.94
0 20 40 Miles
© 2024 EPA, U.S. Census Bureau, Esri, DeLorme, Garmin

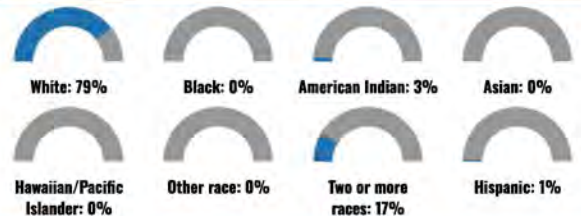
COMMUNITY INFORMATION



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	100%

BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

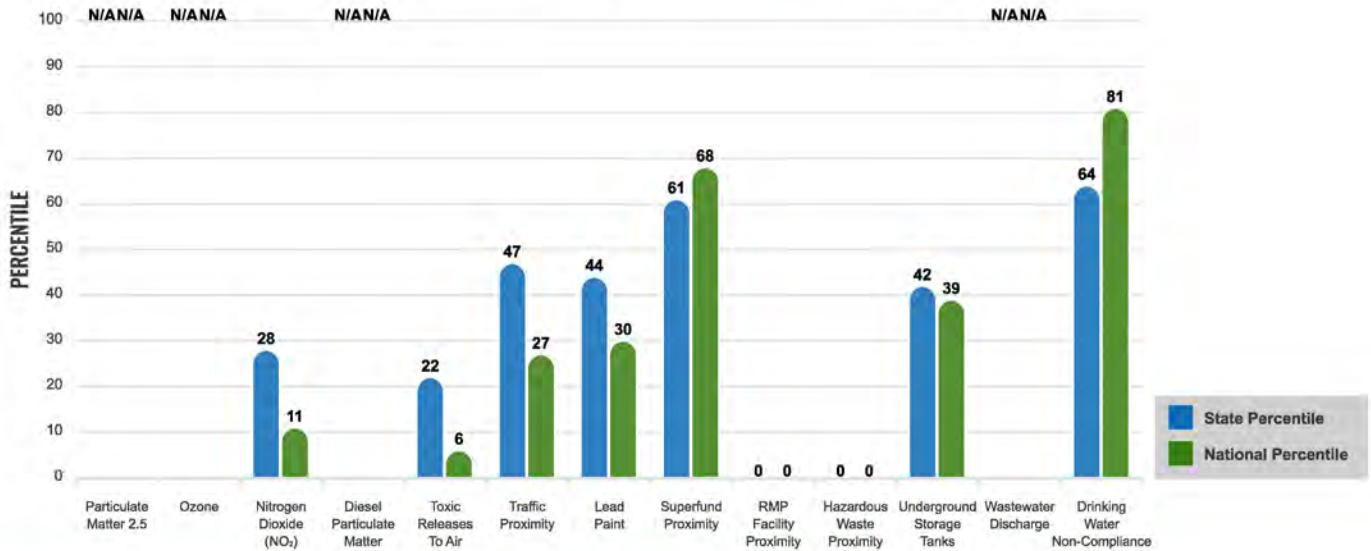
Environmental Justice & Supplemental Indexes

The environmental justice and supplemental indexes are a combination of environmental and socioeconomic information. There are thirteen EJ indexes and supplemental indexes in EJScreen reflecting the 13 environmental indicators. The indexes for a selected area are compared to those for all other locations in the state or nation. For more information and calculation details on the EJ and supplemental indexes, please visit the [EJScreen website](#).

EJ INDEXES

The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator.

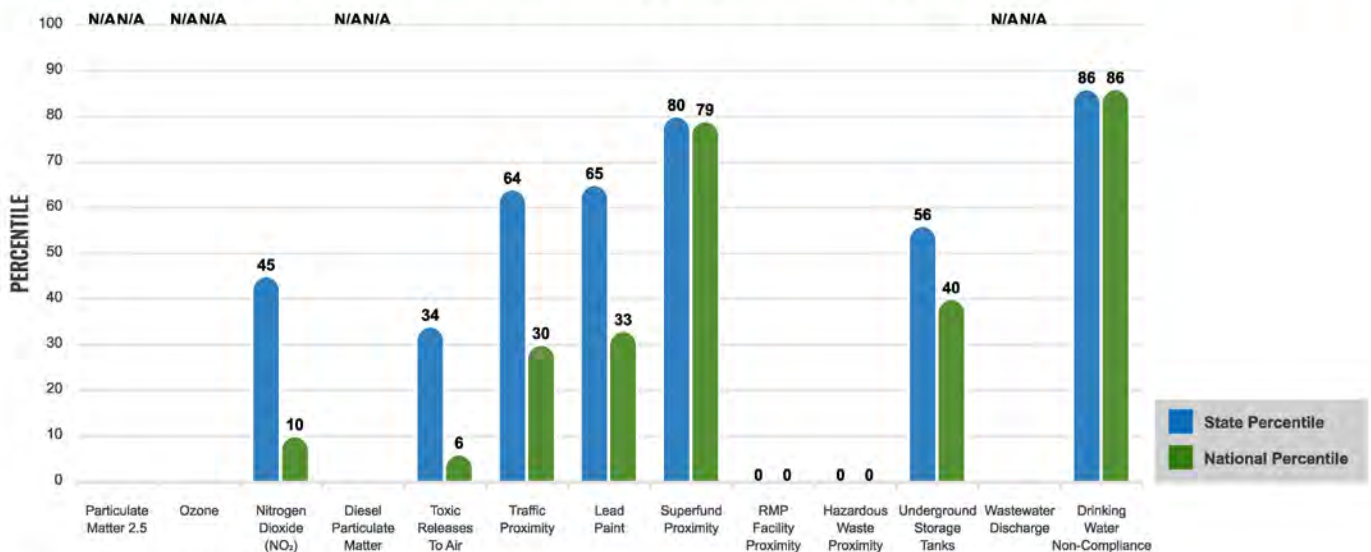
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020000102

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO_2) (ppbv)	3	5.6	34	7.8	6
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	3	76	26	4,600	6
Traffic Proximity (daily traffic count/distance to road)	250,000	590,000	55	1,700,000	28
Lead Paint (% Pre-1960 Housing)	0.084	0.087	69	0.3	33
Superfund Proximity (site count/km distance)	0.47	0.57	78	0.39	82
RMP Facility Proximity (facility count/km distance)	0	0.32	0	0.57	0
Hazardous Waste Proximity (facility count/km distance)	0	0.21	0	3.5	0
Underground Storage Tanks (count/ km^2)	0.21	2.7	48	3.6	37
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	14	5.4	92	2.2	97
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.71	N/A	N/A	1.34	27
Supplemental Demographic Index USA	1.45	N/A	N/A	1.64	44
Demographic Index State	0.93	1.65	18	N/A	N/A
Supplemental Demographic Index State	1.44	1.48	55	N/A	N/A
People of Color	21%	43%	20	40%	38
Low Income	16%	25%	31	30%	30
Unemployment Rate	3%	7%	31	6%	43
Limited English Speaking Households	0%	2%	0	5%	0
Less Than High School Education	5%	7%	51	11%	39
Under Age 5	4%	6%	32	5%	42
Over Age 64	18%	14%	76	18%	59

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	1
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	25
Air Pollution	0
Brownfields	1
Toxic Release Inventory	0

Other community features within defined area:

Schools	4
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	No
Impaired Waters	No

Selected location contains American Indian Reservation Lands*	Yes
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	Yes

Report for Tract: 02020000102

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	20%	19%	65	20%	60
Heart Disease	4.3	4.7	44	5.8	22
Asthma	9.5	10	26	10.3	28
Cancer	5.9	5.5	67	6.4	38
Persons with Disabilities	16.6%	13.2%	80	13.7%	72

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	3%	18%	20	12%	27
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	5%	12%	32	13%	31
Lack of Health Insurance	13%	12%	60	9%	80
Housing Burden	No	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	Yes	N/A	N/A	N/A	N/A

Report for Tract: 02020000102

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

Knik-Fairview, AK

Tract: 02170000601
 Population: 6,216
 Area in square miles: 65.53



August 7, 2024
 Project 1
 1:125,000
 © 2024 EPA, U.S. Census Bureau, and U.S. Department of Health and Human Services

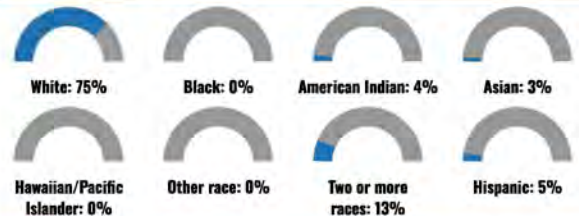
COMMUNITY INFORMATION



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	90%
Spanish	2%
German or other West Germanic	2%
Korean	1%
Tagalog (including Filipino)	2%
Other Asian and Pacific Island	2%
Arabic	1%
Other and Unspecified	1%
Total Non-English	10%

BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

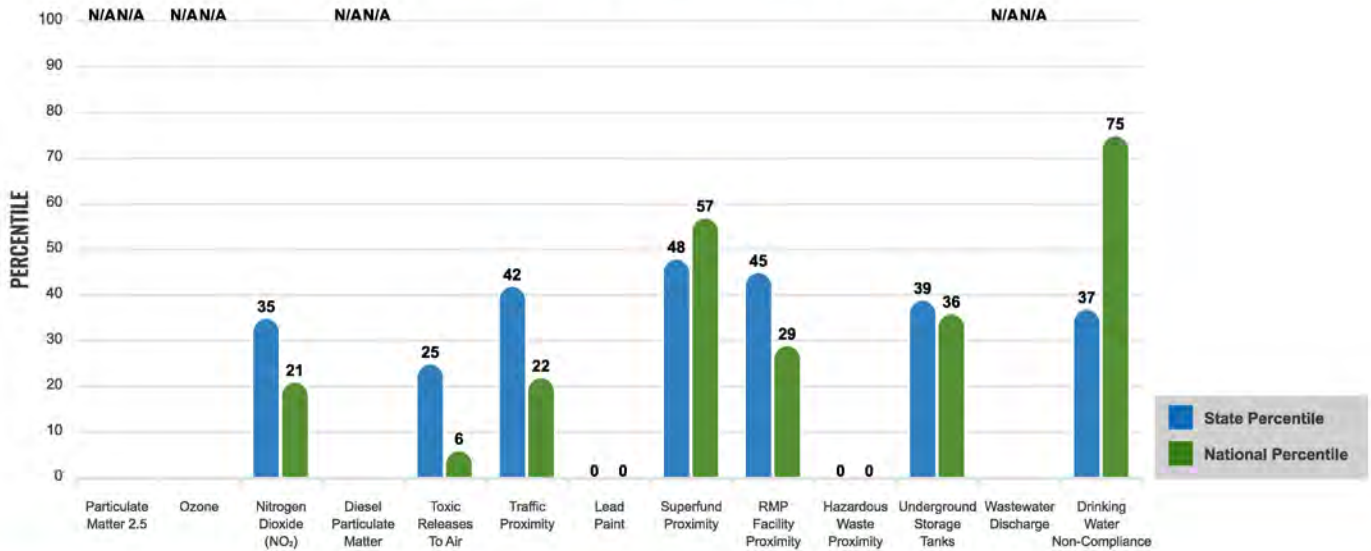
Environmental Justice & Supplemental Indexes

The environmental justice and supplemental indexes are a combination of environmental and socioeconomic information. There are thirteen EJ indexes and supplemental indexes in EJScreen reflecting the 13 environmental indicators. The indexes for a selected area are compared to those for all other locations in the state or nation. For more information and calculation details on the EJ and supplemental indexes, please visit the [EJScreen website](#).

EJ INDEXES

The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator.

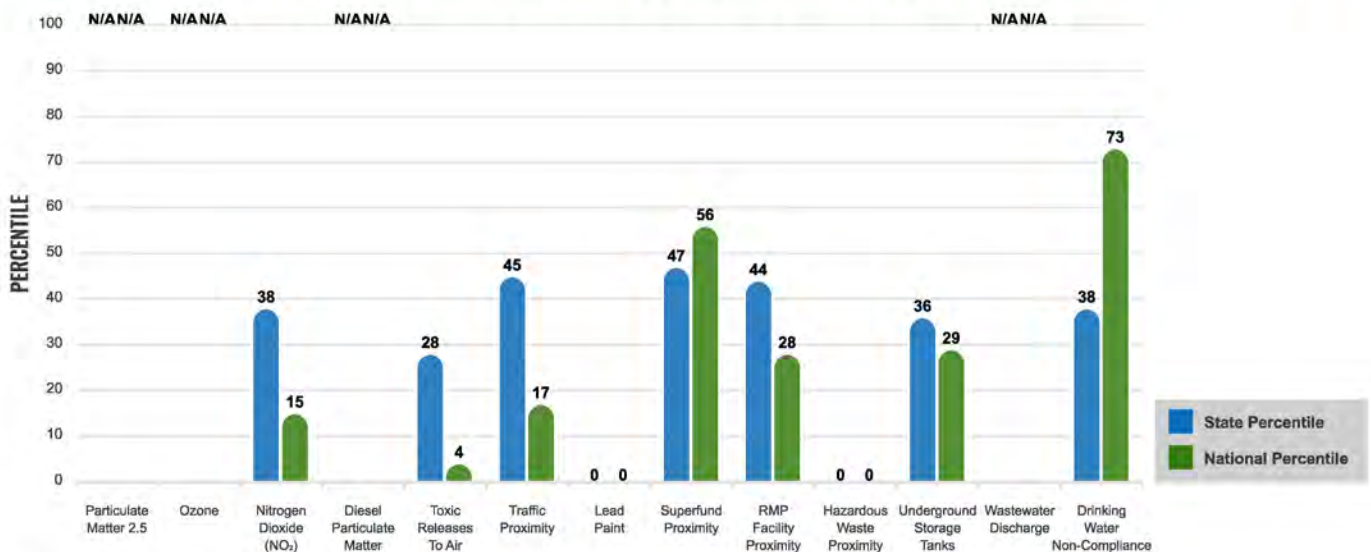
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02170000601

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO ₂) (ppbv)	4.5	5.6	42	7.8	18
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	3.8	76	33	4,600	6
Traffic Proximity (daily traffic count/distance to road)	160,000	590,000	47	1,700,000	22
Lead Paint (% Pre-1960 Housing)	0	0.087	0	0.3	0
Superfund Proximity (site count/km distance)	0.021	0.57	48	0.39	56
RMP Facility Proximity (facility count/km distance)	0.024	0.32	45	0.57	28
Hazardous Waste Proximity (facility count/km distance)	0	0.21	0	3.5	0
Underground Storage Tanks (count/km ²)	0.21	2.7	48	3.6	37
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	1.5	5.4	43	2.2	84
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.71	N/A	N/A	1.34	26
Supplemental Demographic Index USA	1.11	N/A	N/A	1.64	22
Demographic Index State	0.92	1.65	18	N/A	N/A
Supplemental Demographic Index State	1.02	1.48	23	N/A	N/A
People of Color	25%	43%	28	40%	43
Low Income	13%	25%	25	30%	24
Unemployment Rate	6%	7%	55	6%	63
Limited English Speaking Households	1%	2%	61	5%	57
Less Than High School Education	7%	7%	59	11%	44
Under Age 5	8%	6%	72	5%	78
Over Age 64	12%	14%	45	18%	33

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	0
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	14
Air Pollution	0
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	1
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	No
Impaired Waters	Yes

Selected location contains American Indian Reservation Lands*	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	No

Report for Tract: 02170000601

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	17%	19%	30	20%	28
Heart Disease	4.2	4.7	41	5.8	20
Asthma	9.7	10	41	10.3	36
Cancer	5.8	5.5	62	6.4	34
Persons with Disabilities	10.7%	13.2%	27	13.7%	35

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	13%	18%	63	12%	74
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	7%	12%	39	13%	38
Lack of Health Insurance	10%	12%	39	9%	69
Housing Burden	Yes	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	Yes	N/A	N/A	N/A	N/A

Report for Tract: 02170000601

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

Anchorage, AK

Tract: 02020000101
 Population: 5,370
 Area in square miles: 644.10



August 7, 2024
 Project 1
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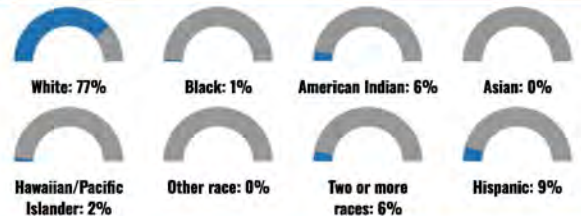
COMMUNITY INFORMATION



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	91%
Spanish	6%
German or other West Germanic	1%
Other and Unspecified	2%
Total Non-English	9%

BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

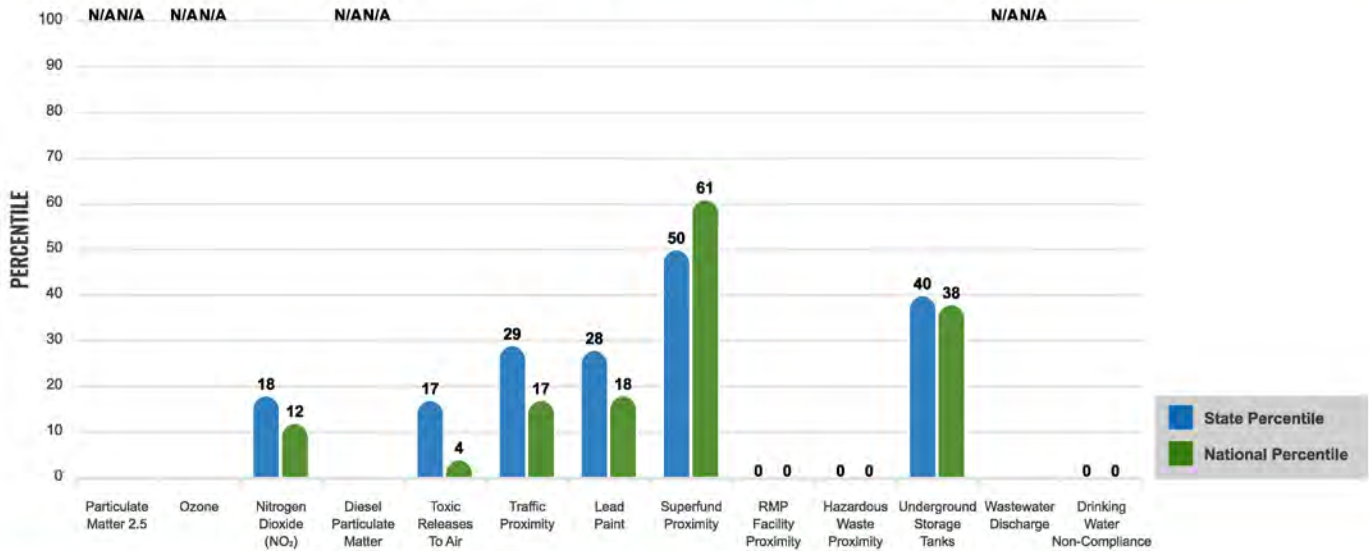
Environmental Justice & Supplemental Indexes

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EJ INDEXES

The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator.

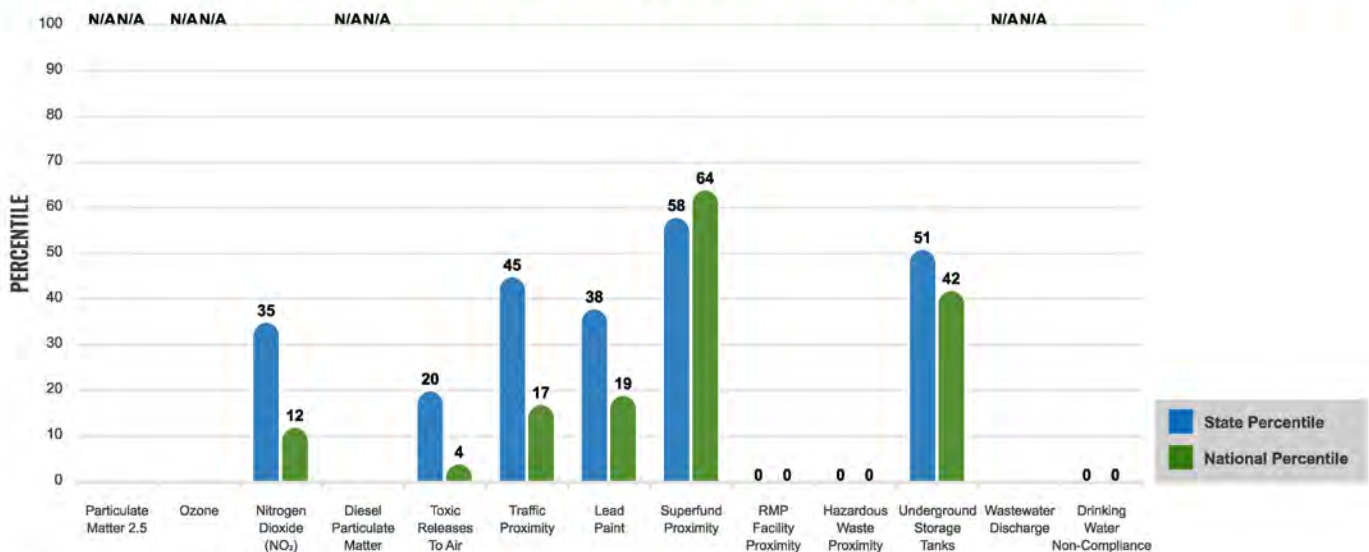
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020000101

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO ₂) (ppbv)	3.4	5.6	35	7.8	9
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	2.2	76	24	4,600	5
Traffic Proximity (daily traffic count/distance to road)	140,000	590,000	45	1,700,000	21
Lead Paint (% Pre-1960 Housing)	0.025	0.087	40	0.3	20
Superfund Proximity (site count/km distance)	0.14	0.57	56	0.39	65
RMP Facility Proximity (facility count/km distance)	0	0.32	0	0.57	0
Hazardous Waste Proximity (facility count/km distance)	0	0.21	0	3.5	0
Underground Storage Tanks (count/km ²)	1.3	2.7	64	3.6	56
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	0	5.4	0	2.2	0
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.52	N/A	N/A	1.34	16
Supplemental Demographic Index USA	1.15	N/A	N/A	1.64	25
Demographic Index State	0.68	1.65	8	N/A	N/A
Supplemental Demographic Index State	1.06	1.48	26	N/A	N/A
People of Color	23%	43%	24	40%	41
Low Income	7%	25%	11	30%	11
Unemployment Rate	7%	7%	62	6%	69
Limited English Speaking Households	0%	2%	59	5%	56
Less Than High School Education	3%	7%	26	11%	23
Under Age 5	7%	6%	63	5%	69
Over Age 64	11%	14%	43	18%	30

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	0
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	5
Air Pollution	0
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	1
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	No
Impaired Waters	No

Selected location contains American Indian Reservation Lands*	Yes
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	Yes

Report for Tract: 02020000101

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	17%	19%	17	20%	23
Heart Disease	3.8	4.7	19	5.8	13
Asthma	9.3	10	15	10.3	23
Cancer	5.5	5.5	51	6.4	30
Persons with Disabilities	17.2%	13.2%	84	13.7%	75

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	4%	18%	30	12%	37
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	7%	12%	40	13%	39
Lack of Health Insurance	8%	12%	24	9%	58
Housing Burden	No	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	No	N/A	N/A	N/A	N/A

Report for Tract: 02020000101

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

Anchorage, AK

Tract: 02020000204
 Population: 3,450
 Area in square miles: 197.73



August 7, 2024
 Project 1
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 2000 Feet
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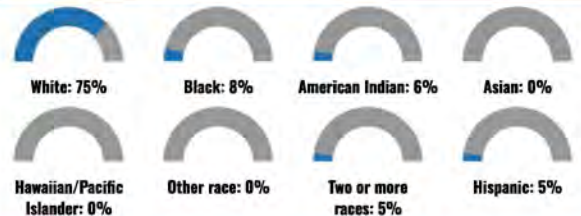
COMMUNITY INFORMATION



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	96%
Spanish	3%
Russian, Polish, or Other Slavic	1%
Other and Unspecified	1%
Total Non-English	4%

BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

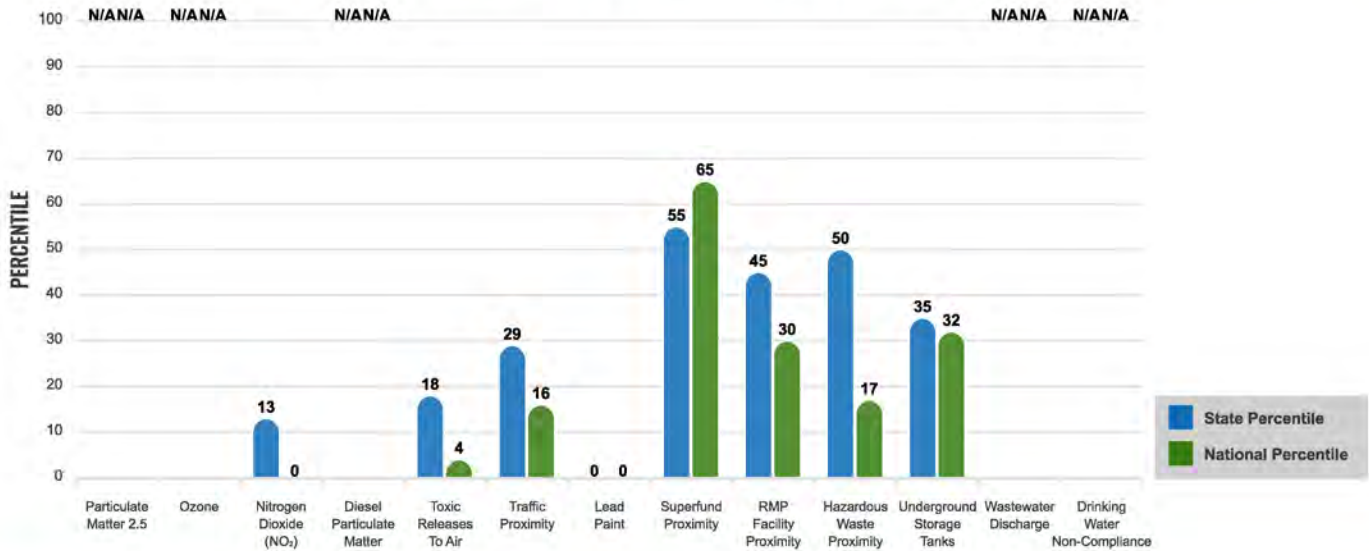
Environmental Justice & Supplemental Indexes

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EJ INDEXES

The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator.

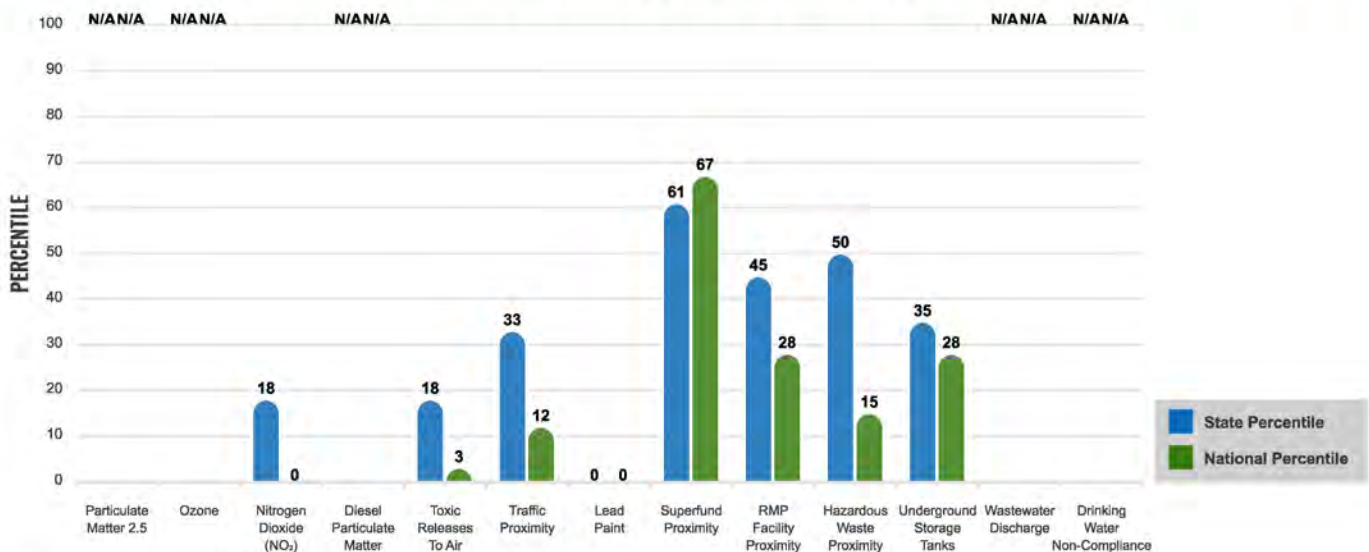
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020000204

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO_2) (ppbv)	1.1	5.6	24	7.8	0
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	2.3	76	24	4,600	5
Traffic Proximity (daily traffic count/distance to road)	110,000	590,000	40	1,700,000	18
Lead Paint (% Pre-1960 Housing)	0	0.087	0	0.3	0
Superfund Proximity (site count/km distance)	0.57	0.57	82	0.39	85
RMP Facility Proximity (facility count/km distance)	0.041	0.32	45	0.57	28
Hazardous Waste Proximity (facility count/km distance)	0.037	0.21	51	3.5	15
Underground Storage Tanks (count/ km^2)	0.03	2.7	39	3.6	29
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	N/A	5.4	N/A	2.2	N/A
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.57	N/A	N/A	1.34	19
Supplemental Demographic Index USA	1.05	N/A	N/A	1.64	19
Demographic Index State	0.74	1.65	10	N/A	N/A
Supplemental Demographic Index State	0.94	1.48	17	N/A	N/A
People of Color	25%	43%	28	40%	43
Low Income	8%	25%	12	30%	13
Unemployment Rate	4%	7%	36	6%	49
Limited English Speaking Households	0%	2%	0	5%	0
Less Than High School Education	3%	7%	27	11%	24
Under Age 5	4%	6%	37	5%	48
Over Age 64	10%	14%	30	18%	23

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	1
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	23
Air Pollution	1
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	1
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	No
Impaired Waters	No

Selected location contains American Indian Reservation Lands*	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	No

Report for Tract: 02020000204

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	16%	19%	15	20%	21
Heart Disease	3.3	4.7	5	5.8	7
Asthma	9.6	10	34	10.3	31
Cancer	5.3	5.5	42	6.4	26
Persons with Disabilities	14.3%	13.2%	65	13.7%	59

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	2%	18%	16	12%	24
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	6%	12%	36	13%	34
Lack of Health Insurance	5%	12%	5	9%	35
Housing Burden	Yes	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	Yes	N/A	N/A	N/A	N/A

Report for Tract: 02020000204

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

Anchorage, AK

Tract: 02020000206

Population: 3,137

Area in square miles: 2.60



August 7, 2024
Project 1



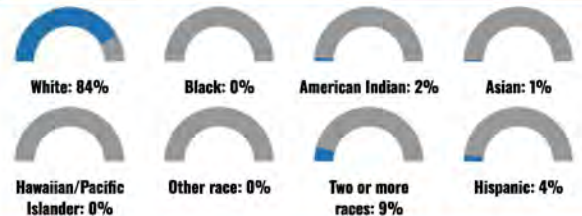
COMMUNITY INFORMATION



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	95%
Spanish	4%
Total Non-English	5%

BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

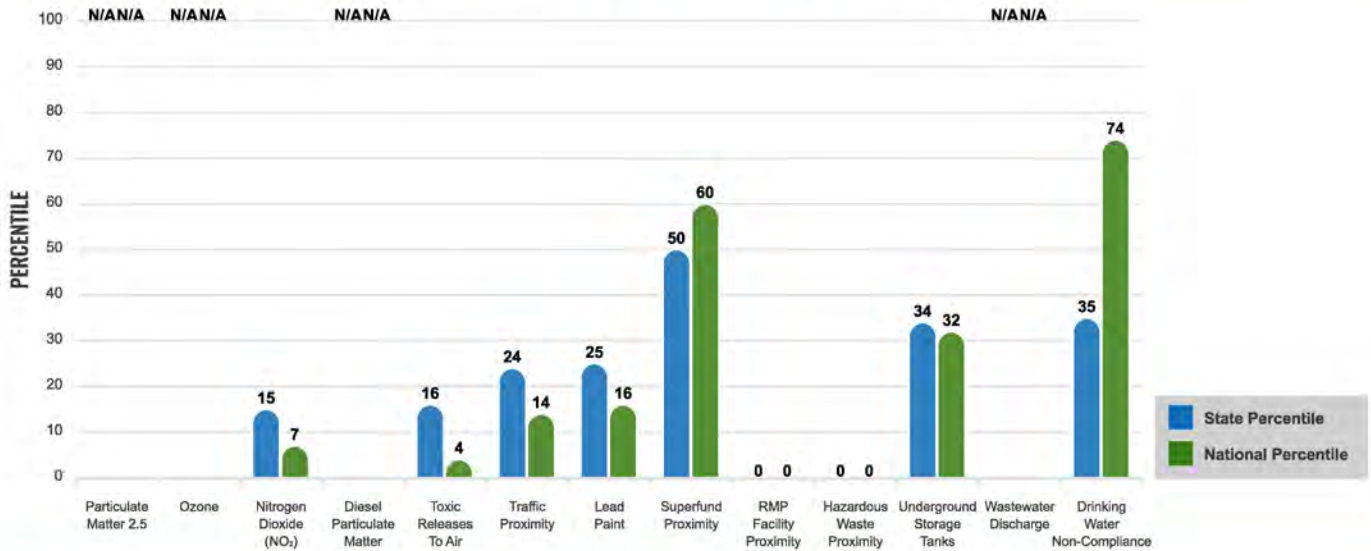
Environmental Justice & Supplemental Indexes

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EJ INDEXES

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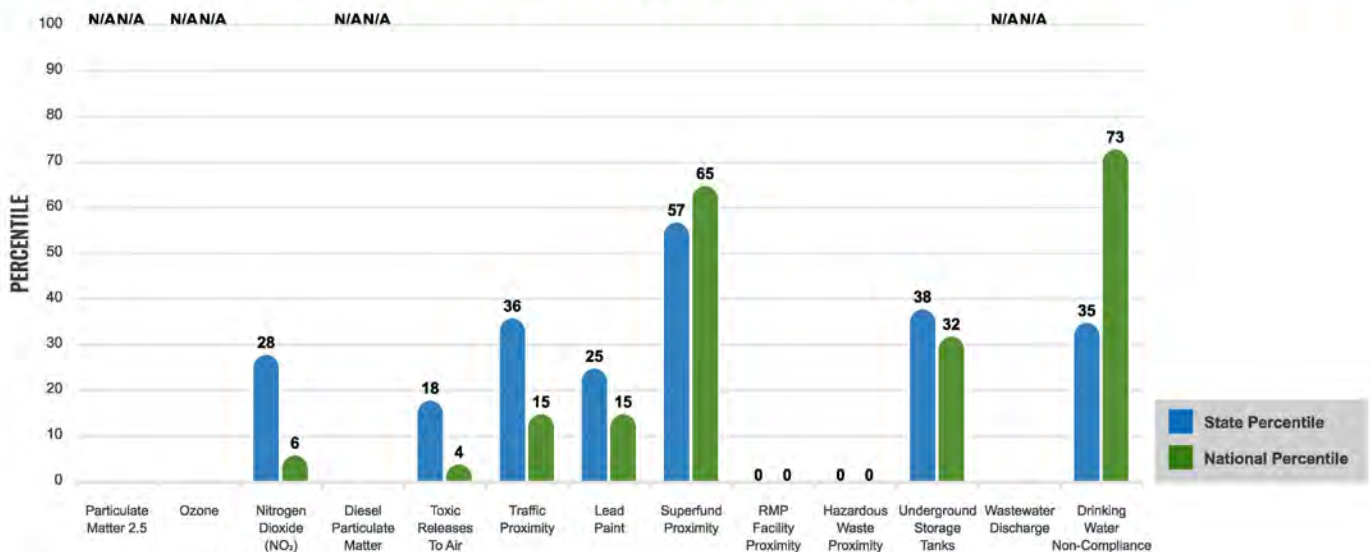
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020000206

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO_2) (ppbv)	3.4	5.6	35	7.8	9
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	3.3	76	28	4,600	6
Traffic Proximity (daily traffic count/distance to road)	160,000	590,000	47	1,700,000	22
Lead Paint (% Pre-1960 Housing)	0.011	0.087	30	0.3	16
Superfund Proximity (site count/km distance)	0.29	0.57	69	0.39	76
RMP Facility Proximity (facility count/km distance)	0	0.32	0	0.57	0
Hazardous Waste Proximity (facility count/km distance)	0	0.21	0	3.5	0
Underground Storage Tanks (count/ km^2)	0.33	2.7	51	3.6	40
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	0.41	5.4	37	2.2	75
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.41	N/A	N/A	1.34	10
Supplemental Demographic Index USA	1	N/A	N/A	1.64	16
Demographic Index State	0.54	1.65	3	N/A	N/A
Supplemental Demographic Index State	0.84	1.48	11	N/A	N/A
People of Color	16%	43%	9	40%	31
Low Income	7%	25%	11	30%	12
Unemployment Rate	3%	7%	31	6%	44
Limited English Speaking Households	0%	2%	0	5%	0
Less Than High School Education	1%	7%	14	11%	13
Under Age 5	8%	6%	72	5%	77
Over Age 64	14%	14%	54	18%	42

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	0
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	3
Air Pollution	0
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	0
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	Yes
Impaired Waters	No

Selected location contains American Indian Reservation Lands*	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	No

Report for Tract: 02020000206

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	17%	19%	24	20%	26
Heart Disease	3.2	4.7	3	5.8	6
Asthma	9.2	10	12	10.3	20
Cancer	5	5.5	34	6.4	22
Persons with Disabilities	12.9%	13.2%	50	13.7%	50

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	5%	18%	36	12%	42
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	4%	12%	27	13%	27
Lack of Health Insurance	2%	12%	1	9%	13
Housing Burden	No	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	No	N/A	N/A	N/A	N/A

Report for Tract: 02020000206

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

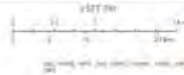
This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

Anchorage, AK

Tract: 02020000202
 Population: 7,097
 Area in square miles: 3.55



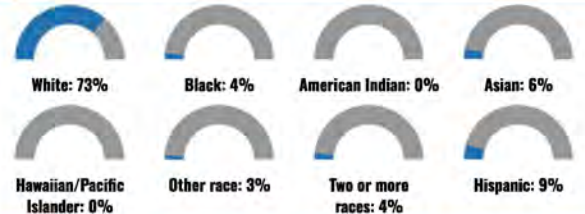
Revised 7/2018
 Legend: Present 1



COMMUNITY INFORMATION



BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	91%
Spanish	3%
German or other West Germanic	1%
Korean	2%
Tagalog (including Filipino)	1%
Other Asian and Pacific Island	1%
Total Non-English	9%

Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

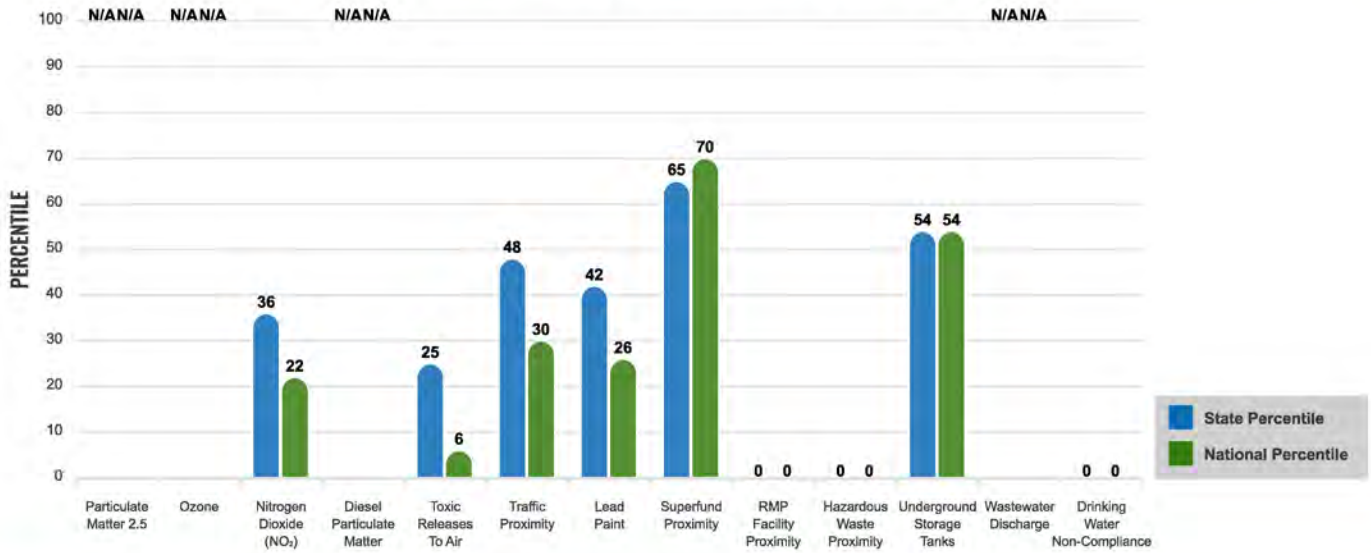
Environmental Justice & Supplemental Indexes

The environmental justice and supplemental indexes are a combination of environmental and socioeconomic information. There are thirteen EJ indexes and supplemental indexes in EJScreen reflecting the 13 environmental indicators. The indexes for a selected area are compared to those for all other locations in the state or nation. For more information and calculation details on the EJ and supplemental indexes, please visit the [EJScreen website](#).

EJ INDEXES

The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator.

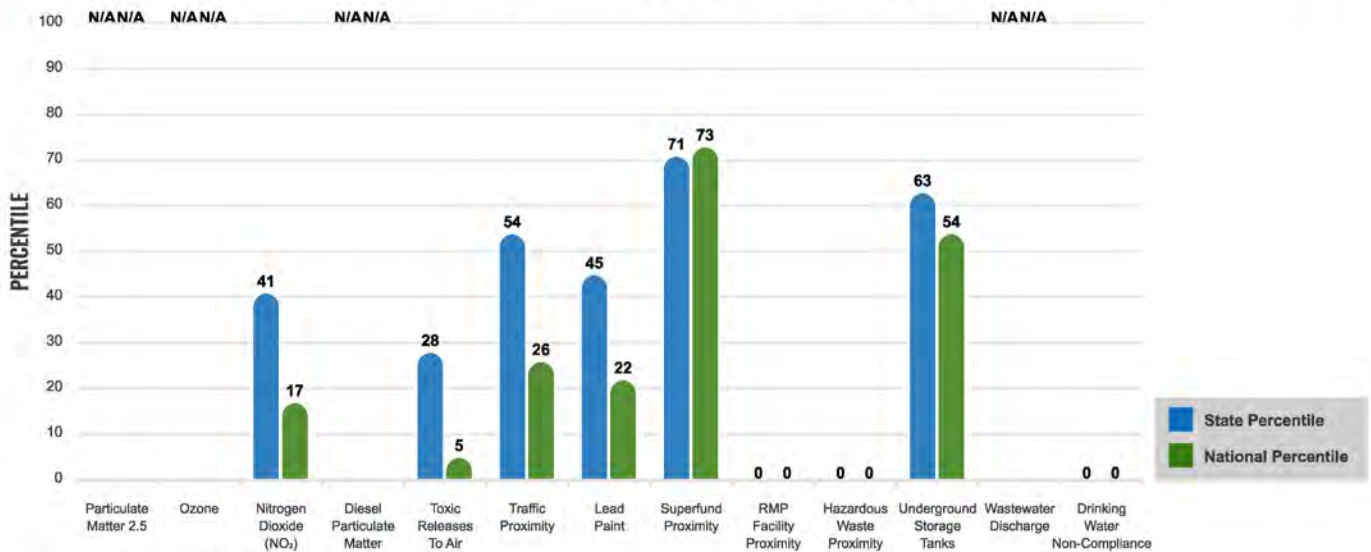
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020000202

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO ₂) (ppbv)	4.5	5.6	42	7.8	19
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	3.6	76	31	4,600	6
Traffic Proximity (daily traffic count/distance to road)	300,000	590,000	57	1,700,000	30
Lead Paint (% Pre-1960 Housing)	0.047	0.087	55	0.3	26
Superfund Proximity (site count/km distance)	0.74	0.57	85	0.39	88
RMP Facility Proximity (facility count/km distance)	0	0.32	0	0.57	0
Hazardous Waste Proximity (facility count/km distance)	0	0.21	0	3.5	0
Underground Storage Tanks (count/km ²)	2.6	2.7	73	3.6	67
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	0	5.4	0	2.2	0
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.72	N/A	N/A	1.34	27
Supplemental Demographic Index USA	1.14	N/A	N/A	1.64	25
Demographic Index State	0.94	1.65	19	N/A	N/A
Supplemental Demographic Index State	1.07	1.48	26	N/A	N/A
People of Color	27%	43%	31	40%	45
Low Income	13%	25%	24	30%	23
Unemployment Rate	1%	7%	18	6%	24
Limited English Speaking Households	1%	2%	65	5%	59
Less Than High School Education	4%	7%	39	11%	32
Under Age 5	4%	6%	36	5%	46
Over Age 64	15%	14%	60	18%	46

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	0
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	10
Air Pollution	0
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	2
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	Yes
Impaired Waters	No

Selected location contains American Indian Reservation Lands*	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	No

Report for Tract: 02020000202

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	N/A	19%	N/A	20%	N/A
Heart Disease	3.7	4.7	14	5.8	12
Asthma	9.4	10	20	10.3	27
Cancer	5.5	5.5	51	6.4	30
Persons with Disabilities	12.4%	13.2%	47	13.7%	47

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	9%	18%	52	12%	60
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	3%	12%	23	13%	23
Lack of Health Insurance	7%	12%	20	9%	55
Housing Burden	No	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	No	N/A	N/A	N/A	N/A

Report for Tract: 02020000202

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

Anchorage, AK

Tract: 02020000201
 Population: 4,728
 Area in square miles: 1.32



August 7, 2024
 Project 1



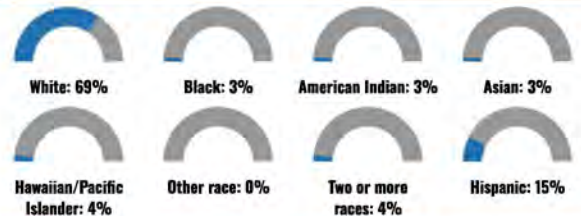
COMMUNITY INFORMATION



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	90%
Spanish	5%
French, Haitian, or Cajun	1%
German or other West Germanic	1%
Other Asian and Pacific Island	1%
Other and Unspecified	1%
Total Non-English	10%

BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

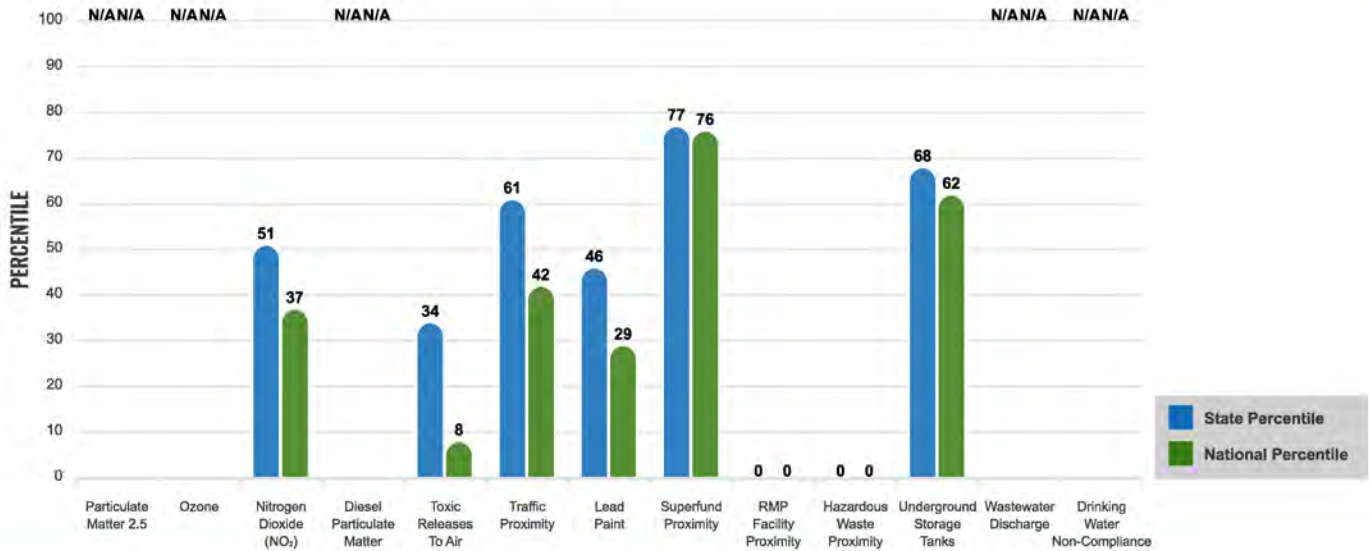
Environmental Justice & Supplemental Indexes

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EJ INDEXES

The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator.

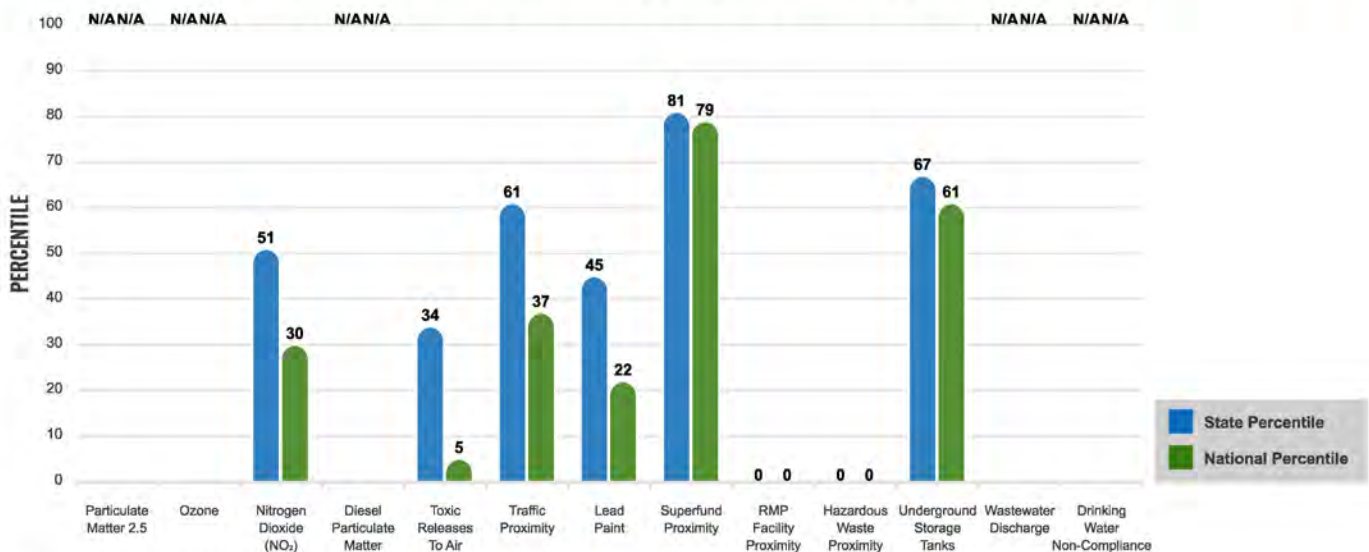
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020000201

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO ₂) (ppbv)	5.8	5.6	50	7.8	31
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	3.8	76	33	4,600	6
Traffic Proximity (daily traffic count/distance to road)	470,000	590,000	61	1,700,000	38
Lead Paint (% Pre-1960 Housing)	0.031	0.087	44	0.3	22
Superfund Proximity (site count/km distance)	3	0.57	94	0.39	97
RMP Facility Proximity (facility count/km distance)	0	0.32	0	0.57	0
Hazardous Waste Proximity (facility count/km distance)	0	0.21	0	3.5	0
Underground Storage Tanks (count/km ²)	3.1	2.7	75	3.6	70
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	N/A	5.4	N/A	2.2	N/A
SOCIOECONOMIC INDICATORS					
Demographic Index USA	0.93	N/A	N/A	1.34	39
Supplemental Demographic Index USA	1.24	N/A	N/A	1.64	31
Demographic Index State	1.21	1.65	35	N/A	N/A
Supplemental Demographic Index State	1.19	1.48	34	N/A	N/A
People of Color	31%	43%	38	40%	49
Low Income	19%	25%	40	30%	36
Unemployment Rate	4%	7%	39	6%	52
Limited English Speaking Households	0%	2%	0	5%	0
Less Than High School Education	8%	7%	66	11%	49
Under Age 5	8%	6%	74	5%	78
Over Age 64	7%	14%	19	18%	16

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	2
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	8
Air Pollution	1
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	2
Hospitals	0
Places of Worship	0

Other environmental data:

Air Non-attainment	Yes
Impaired Waters	No

Selected location contains American Indian Reservation Lands*	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	Yes

Report for Tract: 02020000201

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	18%	19%	36	20%	36
Heart Disease	3	4.7	1	5.8	5
Asthma	9.8	10	50	10.3	39
Cancer	4.2	5.5	7	6.4	10
Persons with Disabilities	11.7%	13.2%	38	13.7%	42

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	6%	18%	42	12%	50
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	10%	12%	54	13%	51
Lack of Health Insurance	10%	12%	35	9%	68
Housing Burden	Yes	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	Yes	N/A	N/A	N/A	N/A

Report for Tract: 02020000201

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Community Report

This report provides environmental and socioeconomic information for user-defined areas, and combines that data into environmental justice and supplemental indexes.

Anchorage, AK

Tract: 02020980200

Population: 7,857

Area in square miles: 117.52

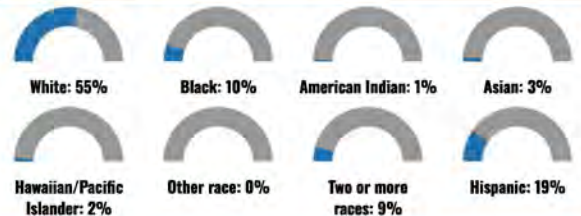


Revised 7/2014
 Project 1
 117.52
 0 10 20 30 40 50 Miles

COMMUNITY INFORMATION



BREAKDOWN BY RACE



BREAKDOWN BY AGE



LIMITED ENGLISH SPEAKING BREAKDOWN



LANGUAGES SPOKEN AT HOME

LANGUAGE	PERCENT
English	85%
Spanish	9%
French, Haitian, or Cajun	1%
Russian, Polish, or Other Slavic	1%
Tagalog (including Filipino)	2%
Other Asian and Pacific Island	1%
Other and Unspecified	1%
Total Non-English	15%

Notes: Numbers may not sum to totals due to rounding. Hispanic population can be of any race. Source: U.S. Census Bureau, American Community Survey (ACS) 2018-2022. Life expectancy data comes from the Centers for Disease Control.

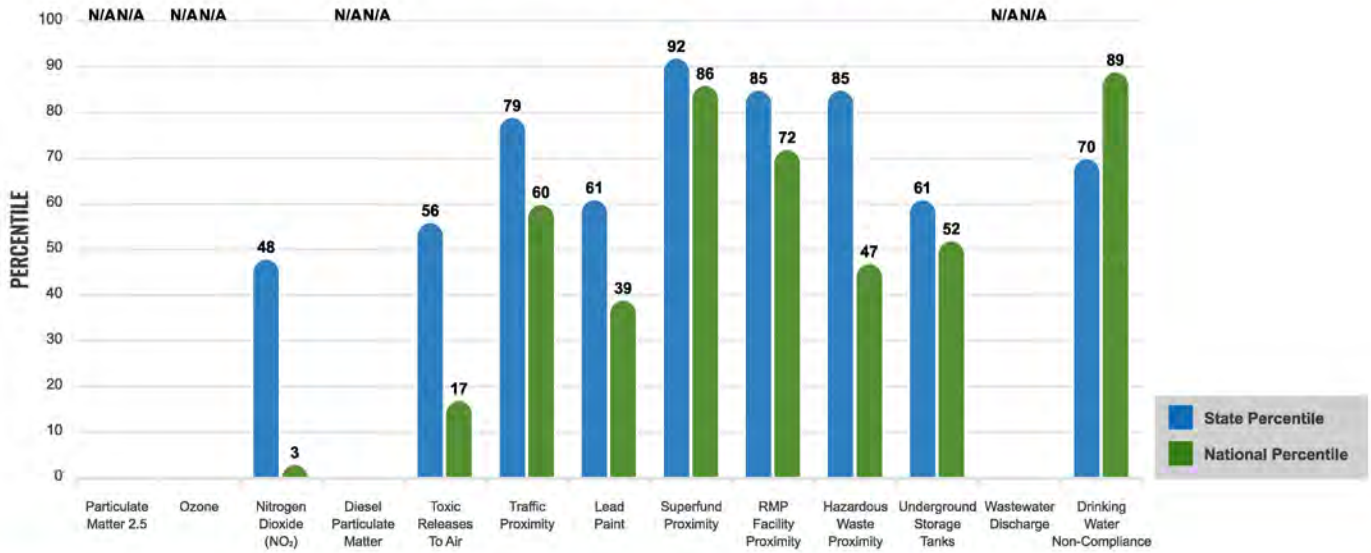
Environmental Justice & Supplemental Indexes

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EJ INDEXES

The EJ indexes help users screen for potential EJ concerns. To do this, the EJ index combines data on low income and people of color populations with a single environmental indicator.

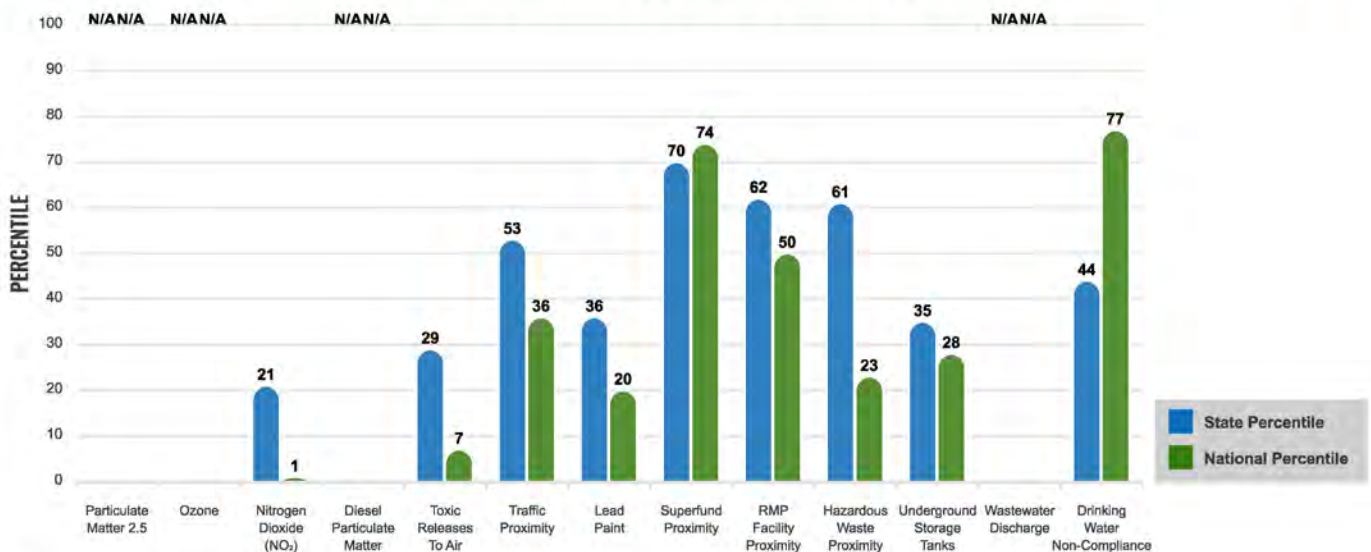
EJ INDEXES FOR THE SELECTED LOCATION



SUPPLEMENTAL INDEXES

The supplemental indexes offer a different perspective on community-level vulnerability. They combine data on percent low income, percent persons with disabilities, percent less than high school education, percent limited English speaking, and percent low life expectancy with a single environmental indicator.

SUPPLEMENTAL INDEXES FOR THE SELECTED LOCATION



Report for Tract: 02020980200

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

SELECTED VARIABLES	VALUE	STATE AVERAGE	PERCENTILE IN STATE	USA AVERAGE	PERCENTILE IN USA
ENVIRONMENTAL BURDEN INDICATORS					
Particulate Matter 2.5 ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	8.45	N/A
Ozone (ppb)	N/A	N/A	N/A	61.8	N/A
Nitrogen Dioxide (NO_2) (ppbv)	2	5.6	29	7.8	1
Diesel Particulate Matter ($\mu\text{g}/\text{m}^3$)	N/A	N/A	N/A	0.191	N/A
Toxic Releases to Air (toxicity-weighted concentration)	9.9	76	40	4,600	9
Traffic Proximity (daily traffic count/distance to road)	690,000	590,000	65	1,700,000	46
Lead Paint (% Pre-1960 Housing)	0.034	0.087	46	0.3	23
Superfund Proximity (site count/km distance)	12	0.57	99	0.39	99
RMP Facility Proximity (facility count/km distance)	0.55	0.32	83	0.57	66
Hazardous Waste Proximity (facility count/km distance)	0.35	0.21	80	3.5	29
Underground Storage Tanks (count/ km^2)	0.043	2.7	41	3.6	30
Wastewater Discharge (toxicity-weighted concentration/m distance)	N/A	N/A	N/A	700000	N/A
Drinking Water Non-Compliance (points)	2	5.4	50	2.2	85
SOCIOECONOMIC INDICATORS					
Demographic Index USA	1.48	N/A	N/A	1.34	62
Supplemental Demographic Index USA	1.04	N/A	N/A	1.64	18
Demographic Index State	1.92	1.65	69	N/A	N/A
Supplemental Demographic Index State	0.89	1.48	13	N/A	N/A
People of Color	45%	43%	59	40%	62
Low Income	33%	25%	71	30%	60
Unemployment Rate	9%	7%	75	6%	80
Limited English Speaking Households	0%	2%	0	5%	0
Less Than High School Education	3%	7%	31	11%	27
Under Age 5	12%	6%	91	5%	92
Over Age 64	2%	14%	1	18%	3

*Diesel particulate matter index is from the EPA's Air Toxics Data Update, which is the Agency's ongoing, comprehensive evaluation of air toxics in the United States. This effort aims to prioritize air toxics, emission sources, and locations of interest for further study. It is important to remember that the air toxics data presented here provide broad estimates of health risks over geographic areas of the country, not definitive risks to specific individuals or locations. More information on the Air Toxics Data Update can be found at: <https://www.epa.gov/haps/air-toxics-data-update>.

Sites reporting to EPA within defined area:

Superfund	2
Hazardous Waste, Treatment, Storage, and Disposal Facilities	0
Water Dischargers	25
Air Pollution	1
Brownfields	0
Toxic Release Inventory	0

Other community features within defined area:

Schools	2
Hospitals	2
Places of Worship	0

Other environmental data:

Air Non-attainment	No
Impaired Waters	Yes

Selected location contains American Indian Reservation Lands*	No
Selected location contains a "Justice40 (CEJST)" disadvantaged community	No
Selected location contains an EPA IRA disadvantaged community	No

Report for Tract: 02020980200

Report produced August 7, 2024 using EJScreen Version 2.3

EJScreen Environmental and Socioeconomic Indicators Data

HEALTH INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Low Life Expectancy	N/A	19%	N/A	20%	N/A
Heart Disease	1	4.7	0	5.8	0
Asthma	9.5	10	26	10.3	28
Cancer	1.4	5.5	0	6.4	0
Persons with Disabilities	4.9%	13.2%	1	13.7%	3

CLIMATE INDICATORS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Flood Risk	9%	18%	53	12%	61
Wildfire Risk	0%	0%	0	14%	0

CRITICAL SERVICE GAPS

INDICATOR	VALUE	STATE AVERAGE	STATE PERCENTILE	US AVERAGE	US PERCENTILE
Broadband Internet	8%	12%	44	13%	43
Lack of Health Insurance	1%	12%	0	9%	2
Housing Burden	Yes	N/A	N/A	N/A	N/A
Transportation Access Burden	Yes	N/A	N/A	N/A	N/A
Food Desert	Yes	N/A	N/A	N/A	N/A

Report for Tract: 02020980200

Report produced August 7, 2024 using EJScreen Version 2.3

Appendix J

FAA No Hazard Determination



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2024-AAL-517-OE

Issued Date: 09/23/2024

Brenda Hewitt
Native Village of Eklutna
26339 Eklutna Village Rd
Chugiak, AK 99567

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Commercial Use Building Casino
Location:	Anchorage, AK
Latitude:	61-25-04.90N NAD 83
Longitude:	149-29-34.10W
Heights:	80 feet site elevation (SE) 40 feet above ground level (AGL) 120 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

_____ At least 10 days prior to start of construction (7460-2, Part 1)
___X___ Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed in accordance with FAA Advisory circular 70/7460-1 M.

This determination expires on 03/23/2026 unless:

- (a) the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- (b) extended, revised, or terminated by the issuing office.
- (c) the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within 6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates, heights, frequency(ies) and power. Any changes in coordinates, heights, and frequencies or use of greater power, except those frequencies specified in the Colo Void Clause Coalition; Antenna System Co-Location; Voluntary Best Practices, will void this determination. Any future construction or alteration, including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA. This determination includes all previously filed frequencies and power for this structure.

If construction or alteration is dismantled or destroyed, you must submit notice to the FAA within 5 days after the construction or alteration is dismantled or destroyed.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

If we can be of further assistance, please contact our office at (206) 231-2990, or paul.holmquist@faa.gov. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2024-AAL-517-OE.

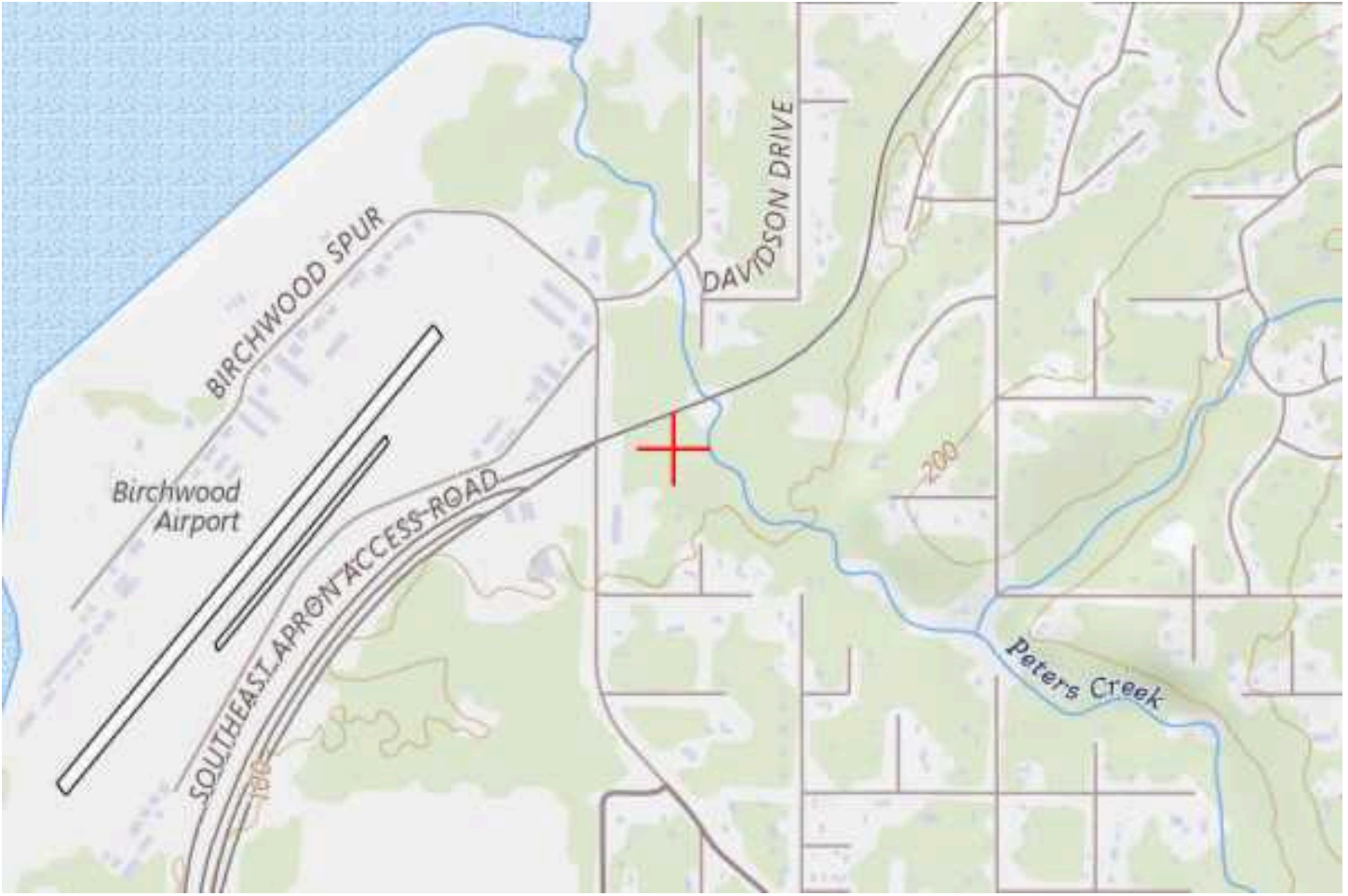
Signature Control No: 631200128-634004900

(DNE)

Paul Holmquist
Specialist

Attachment(s)
Map(s)

TOPO Map for ASN 2024-AAL-517-OE





The FAA is currently experiencing delays in processing off-airport aeronautical studies. These delays are currently resulting in an approximate 15 additional days in processing time. The FAA will continue to work aeronautical studies on a first come, first served basis. Please take this possible delay into consideration when determining when to submit your case. If your submitted aeronautical study requires priority and 60 days has elapsed since submission, please contact the OEG Specialist for your state with the rationale for your request and it will be reviewed for escalation. The issue causing these delays is actively being mitigated and is expected to be resolved around August.

Notice Criteria Tool

Notice Criteria Tool - Desk Reference Guide V_2018.2.0

The requirements for filing with the Federal Aviation Administration for proposed structures vary based on a number of factors: height, proximity to an airport, location, and frequencies emitted from the structure, etc. For more details, please reference [CFR Title 14 Part 77.9](#).

You must file with the FAA at least 45 days prior to construction if:

- your structure will exceed 200ft above ground level
- your structure will be in proximity to an airport and will exceed the slope ratio
- your structure involves construction of a traverseway (i.e. highway, railroad, waterway etc...) and once adjusted upward with the appropriate vertical distance would exceed a standard of 77.9(a) or (b)
- your structure will emit frequencies, and does not meet the conditions of the [FAA Co-location Policy](#)
- your structure will be in an instrument approach area and might exceed part 77 Subpart C
- your proposed structure will be in proximity to a navigation facility and may impact the assurance of navigation signal reception
- your structure will be on an airport or heliport
- filing has been requested by the FAA

If you require additional information regarding the filing requirements for your structure, please identify and contact the appropriate FAA representative using the [Air Traffic Areas of Responsibility map](#) for Off Airport construction, or contact the [FAA Airports Region / District Office](#) for On Airport construction.

The tool below will assist in applying Part 77 Notice Criteria.

* **Structure Type:** ▼
 Please select structure type and complete location point information.

Latitude: Deg M S ▼

Longitude: Deg M S ▼

Horizontal Datum: ▼

Site Elevation (SE): (nearest foot)

Structure Height : (nearest foot)

Is structure on airport: No Yes

Results

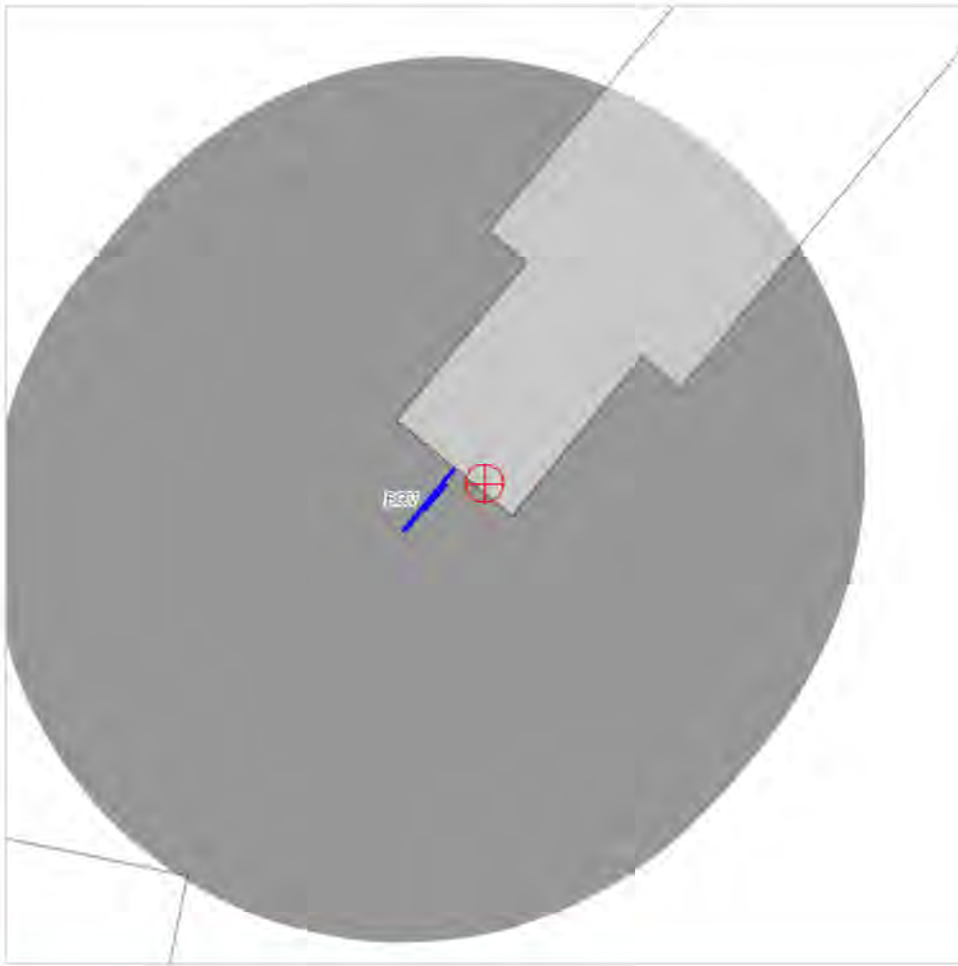
You exceed the following Notice Criteria:

Your proposed structure exceeds an instrument approach area by 52 feet and aeronautical study is needed to determine if it will exceed a standard of subpart C of 14CFR Part 77. The FAA, in accordance with 77.9, requests that you file.

Your proposed structure is in proximity to a navigation facility and may impact the assurance of navigation signal reception. The FAA, in accordance with 77.9, requests that you file.

77.9(b) by 36 ft. The nearest airport is BCV, and the nearest runway is 02L/20R.

The FAA requests that you file





Federal Aviation Administration

The FAA is currently experiencing delays in processing off-airport aeronautical studies. These delays are currently resulting in an approximate 15 additional days in processing time. The FAA will continue to work aeronautical studies on a first come, first served basis. Please take this possible delay into consideration when determining when to submit your case. If your submitted aeronautical study requires priority and 60 days has elapsed since submission, please contact the OEG Specialist for your state with the rationale for your request and it will be reviewed for escalation. The issue causing these delays is actively being mitigated and is expected to be resolved around August.

<< OE

Notice of Proposed Construction or Alteration - Off Airport

Add a New Case (Off Airport) - Desk Reference Guide V_2018.2.1

Add a New Case (Off Airport) for Wind Turbines - Met Towers (with WT Farm) - WT-Barge Crane - Desk Reference Guide V_2018.2.1

The Description of Location field can only be 300 characters long
The Description of Proposal field can only be 300 characters long

Form containing sections: Sponsor (person, company, etc. proposing this action), Construction / Alteration Information, Structure Summary, Structure Details, Proposed Frequency Bands, and Additional Location(s).

I hereby certify that all of the above statements made by me are true, complete, and correct to the best of my knowledge. In addition, I agree to mark and/or light the structure in accordance with established marking and lighting standards as necessary.



Kenai Peninsula Borough, Matanuska-Susitna Borough GIS, Municipality of Anchorage, State of Alaska, Esri, TomTom, Garmin, SafeGraph, METI/NASA, USGS, EPA, NPS, USDA, USFWS

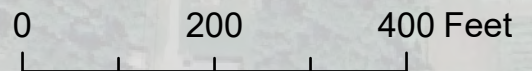
Exhibit A - PROPOSED PROJECT SITE AND VICINITY



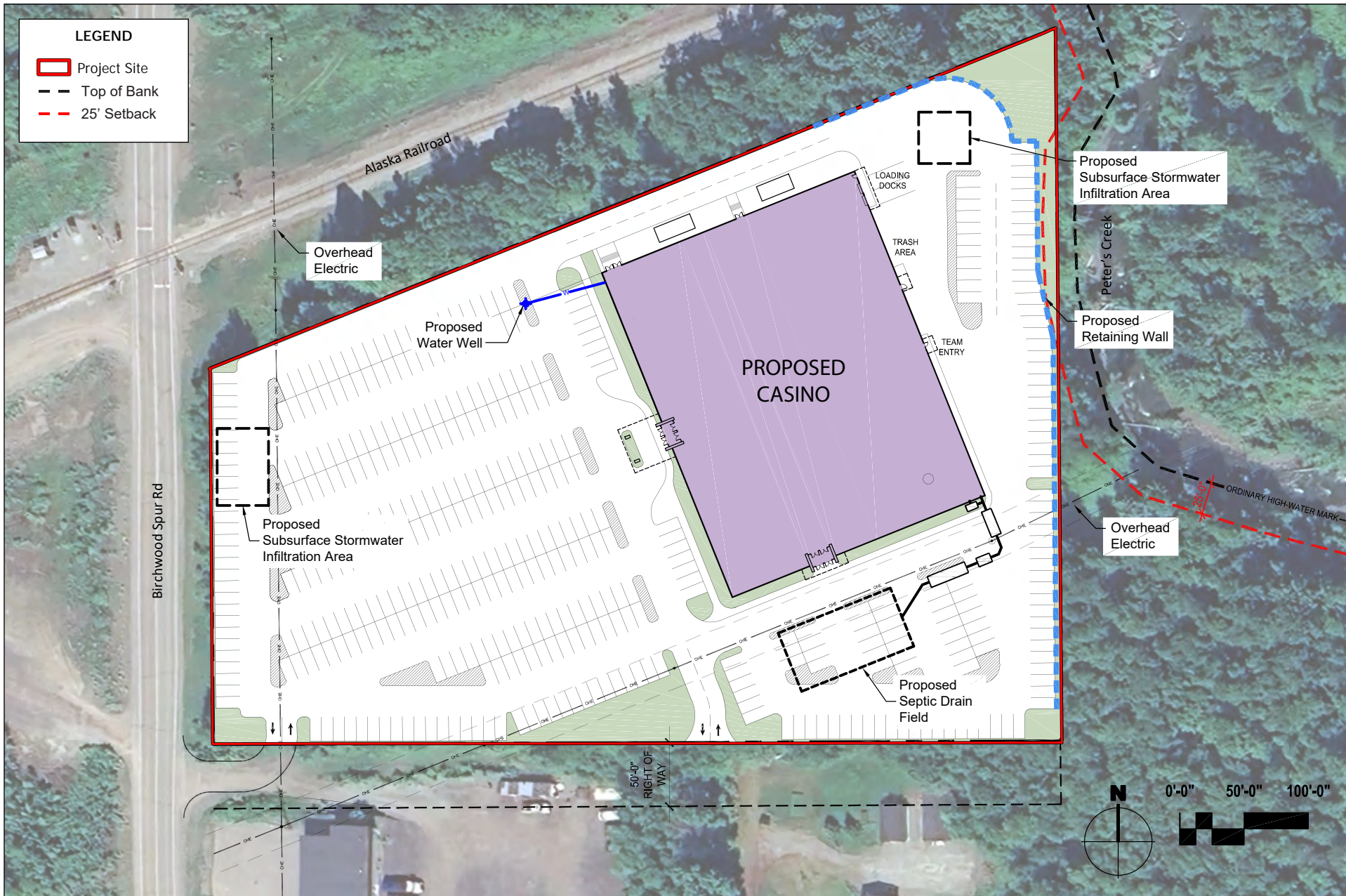
Legend

- Project Site
- Ondola Allotment
- County Parcels

Alluvial St
Image © 2024 Airbus



Kenai Peninsula Borough, Matanuska-Susitna Borough GIS, Municipality of Anchorage, State of Alaska, © OpenStreetMap, Microsoft, Esri, TomTom, Garmin, SafeGraph, METI/NASA, USGS, EPA, NPS, US Census Bureau, USDA, USFWS



Source: Marnell Companies, 6/5/2024, Acorn Environmental, 8/26/2024

EXHIBIT C -
PROPOSED CASINO
SITE PLAN



Federal Aviation Administration

The FAA is currently experiencing delays in processing off-airport aeronautical studies. These delays are currently resulting in an approximate 15 additional days in processing time. The FAA will continue to work aeronautical studies on a first come, first served basis. Please take this possible delay into consideration when determining when to submit your case. If your submitted aeronautical study requires priority and 60 days has elapsed since submission, please contact the OEG Specialist for your state with the rationale for your request and it will be reviewed for escalation. The issue causing these delays is actively being mitigated and is expected to be resolved around August.

<< OE/AAA

Project Submission Success
Project Name: NATIV-000878409-24

Project NATIV-000878409-24 has been submitted successfully to the FAA.

Your filing is assigned Aeronautical Study Number (ASN):
2024-AAL-517-OE

Please refer to the assigned ASN on all future inquiries regarding this filing.

Please return to the system at a later date for status updates.

It is the responsibility of each e-filer to exercise due diligence to determine if coordination of the proposed construction or alteration is necessary with their state aviation department. Please use the link below to contact your state aviation department to determine their requirements:
[State Aviation Contacts](#)

To ensure e-mail notifications are delivered to your inbox please add noreply@faa.gov to your address book. Notifications sent from this address are system generated FAA e-mails and replies to this address will NOT be read or forwarded for review. Each system generated e-mail will contain specific FAA contact information in the text of the message.

Appendix K

Hazardous Materials Due Diligence Memo

Eklutna Native Village Casino Project

Hazardous Materials Due Diligence Memo

Prepared For: Eklutna Native Village
Prepared By: Acorn Environmental
Date: August 29, 2024

Introduction

This memorandum has been prepared to summarize results of a hazardous materials due diligence investigation conducted for the Eklutna Native Village Casino Project. The Subject Property, approximately 6.37-acres, consists of portions of three restricted fee Native allotment parcels (Assessor’s Parcel Numbers 05108101000, 05108102000, and 05108115000). The Subject Property is situated off Birchwood Spur Road with the Alaska Railroad to the north and the Birchwood Airport to the west, and is within Section 5 of Township 15 North, Range 1 West within the Seward Meridian, and is within the Anchorage B-7 NW United States Geological Survey 7.5’ quadrangle map

Site Reconnaissance

A reconnaissance survey of the Subject Property was conducted by Acorn Environmental from July 15 to 19, 2024 to assess the Subject Property for potential hazardous materials issues. **Table 1**, below, summarizes the current and past uses of the Subject Property in addition to the current uses of the adjoining properties.

Table 1: Current and Past Uses of the Subject Property and Adjacent Properties

Current/Past Uses	Description
Project Site (Current and Past)	
Current Use of Subject Property	Vacant forestland with cleared areas
Past Use of Subject Property	Single-family residential development
Existing Structures on Subject Property	None
Adjacent Properties (Current)	
Current Use of Adjoining Property (North)	Alaska Railroad Corporation railway and right of way
Current Use of Adjoining Property (West)	Birchwood Spur Roadway - adjoining property associated with the railroad
Current Use of Adjoining Property (East)	Peters Creek
Current Use of Adjoining Property (South)	Commercial facility (maintenance yard) and residential homes

Solid waste was observed throughout the Subject Property, including household debris such as tarps, wood, sheeting, pallets, broken glass bottles, cardboard, an oven, and other common household items. Ongoing cleanup activities were ongoing at the Subject Property during the site reconnaissance.

Two groundwater wells were noted on the Subject Property. One well showed signs of neglect and non-operation; it is assumed to be abandoned or decommissioned. The second well is presumed to have previously served the historic residence that was formerly onsite. One septic system was identified on the Subject Property, and septic and trash odors were noted.

Electrical overhead service lines in the southern part of the Subject Property were noted. One overhead transformer was observed along this utility line; however, no leaks or stains were observed.

No above ground or underground storage tanks/associated piping, pits, ponds, or lagoons of liquid were observed on the Subject Property. Recent rainfall created multiple areas of standing water however no sheen or odors were evident. Several empty and rusted 55-gallon steel drums were found partially buried in four locations around the Subject Property. These 55-gallon drums had no labels to indicate the contents. No evidence of impaired vegetation was noted in the immediate vicinity of these empty 55-gallon drums. No stained soil or pavement was observed.

Database Searches

A database search of the Project Site and surrounding area was conducted in August 2024 by NetrOnline for records of hazardous materials generation, storage, or contamination (**Attachment 1**). Applicable federal, State, and local databases were searched in addition to the Alaska Department of Environmental Conservation (DEC) Contaminated Site's Contaminated Sites portal for researching recorded cleanups and spills (DEC, 2024; EPA, 2024). Databases were searched for listings up to one mile from a point roughly equivalent to the center of the Subject Property.

Five leaking underground storage tank (LUST) sites were noted on the DEC database within a 1-mile radius of the Subject Property. All five sites have been remediated and closed by the ADEC (**Attachment 1**).

Of the database results reviewed for the 1-mile radius, only one listed site had an active cleanup status: the Sunset Auto Body Class V Injection Well located at 21442 N Birchwood Loop Road, approximately 0.75 miles to the south of the Subject Property. The Sunset Auto Body injection well system, operational since 1988, consists of two shop floor drains that discharge into a 4.5-foot corrugated steel culvert pit located beneath the facility's office, which leads to a 16-foot perforated pipe drain pit outside the rear of the building. In 2018, soil contamination was identified at the Sunset Auto Body site, triggering a compliance order from the USEPA to close the well. The contaminants detected include chromium, naphthalene, and tetrachloroethylene in the soil beneath the office. These findings prompted further investigation and a series of mitigation efforts. As of 2019, work at the Sunset Auto Body site included a plan to seal the injection well with concrete, remove contaminated soils, and collect confirmation soil samples. The analysis did not detect any not contain detectable concentrations of volatile organic compounds. Groundwater samples analyzed for VOCs on August 13, 2019, from an onsite well on the Sunset Auto Body site, did not contain detectable concentrations of contaminants (DEC, 2024a).

Given the distance to the Subject Property, the ongoing DEC oversight and remediation activities, and the lack of detected contaminants in the water sample on the listed site, the contamination at the Sunset Auto Body site does not present an immediate risk to the Subject Property.

Findings

No visible signs of hazardous materials or petroleum product involvement or contamination were observed during the July 2024 site reconnaissance. The Subject Property was not listed on any regulatory agency database as having previous or current hazardous materials uses or releases, and no listed sites in the vicinity presented a risk of contamination to the Subject Property. Based on the findings of this technical memorandum, no further hazardous materials investigation of the Subject Property is recommended at this time.

References

United States Environmental Protection Agency (USEPA), 2024. Enforcement and Compliance History Online Facility Search Results. Available online at: <https://echo.epa.gov/facilities/facility-search/results>. Accessed August 29, 2024.

Alaska Department of Environmental Conservation (DEC), 2024. Division of Spill Prevention and Response Database of Contaminated Sites. Available online at <https://www.arcgis.com/apps/mapviewer/index.html?webmap=315240bfbaf84aa0b8272ad1cef3cad3>. Accessed August 29, 2024.

DEC, 2024a. Sunset Auto Body Class V Injection Well Site. Available online at <https://dec.alaska.gov/Applications/SPAR/PublicMVC/CSP/SiteReport/26906#documents>. Accessed August 29, 2024.

Site Name: Project Site
Location: NA
Prepared for: Acorn Environmental
Ref: Eklutna
Center Coordinates: 61.417862,-149.493514
Prepared Date: Thu Aug 15 2024 18:01:02 GMT+0000 (Coordinated Universal Time)

ATTACHMENT 1

ENVIRONMENTAL RADIUS REPORT

ASTM E1527-21



2055 E. Rio Salado Pkwy, Tempe, AZ 85381 | 480-967-6752

Summary

Federal	< 1/4	1/4 - 1/2	1/2 - 1
Lists of Federal NPL (Superfund) sites	0	0	0
Lists of Federal Delisted NPL sites	0	0	0
Lists of Federal sites subject to CERCLA removals and CERCLA orders	0	0	0
Lists of Federal CERCLA sites with NFRAP	0	0	0
Lists of Federal RCRA facilities undergoing Corrective Action	0	0	0
Lists of Federal RCRA TSD facilities	0	0	0
Lists of Federal RCRA generators	0	0	0
Federal institutional control/engineering control registries	0	0	0
Federal ERNS list	0	0	0

State	< 1/4	1/4 - 1/2	1/2 - 1
Lists of state and tribal Superfund equivalent sites	0	0	0
Lists of state and tribal hazardous waste facilities	0	0	0
Lists of state and tribal landfills and solid waste disposal facilities	0	0	0
Lists of state and tribal leaking storage tanks	3	2	0
Lists of state and tribal registered storage tanks	0	0	0
State and tribal institutional control/engineering control registries	0	0	0
Lists of state and tribal voluntary cleanup sites	0	2	0
Lists of state and tribal brownfields sites	0	0	0

Other	< 1/4	1/4 - 1/2	1/2 - 1
State and/or tribal lists of sites requiring further investigation / remediation	0	0	0
State list of Significant Environmental Hazards (SEH)	0	0	0
Lists of state and tribal mine sites requiring further investigation and/or remediation	0	0	0
State and/or tribal lists of spills and spill responses	0	0	0
State and/or tribal lists of emergency responses	0	0	0
State and/or tribal lists of dry cleaners	0	0	0
State and/or tribal lists of clandestine laboratory cleanups	0	0	0
State and/or tribal lists of scrap/used tire processing facilities	0	0	0
State and/or tribal lists of underground injection control sites	0	0	0
State and/or tribal listings of permitted drywells	0	0	0
Automobile salvage yards	0	0	0
Livestock Waste Control sites	0	0	0
Controlled Animal Feeding Operations (CAFOs)	0	0	0
State and/or tribal lists of registered aboveground storage tanks (ASTs)	0	0	0
C.A.A. Permitted Facilities	0	0	0
NPDES Permitted Facilities	0	0	0
Onsite Wastewater Treatment sites	0	0	0
State and/or tribal lists of permitted facilities	0	0	0
Resource Conservation and Recovery Act Information (RCRAInfo)	0	0	0
U.S. EPA Enforcement, Compliance History Online (ECHO)	0	0	0
U.S. EPA Underground Storage Tanks (UST)	0	0	0
U.S. EPA Toxic Substances Control Act (TSCA) database	0	0	0
U.S. EPA Toxic Release Inventory System (TRIS)	0	0	0

Lists of Federal NPL (Superfund) sites

The National Priorities List (NPL) is the list of sites of national priority among the known releases or threatened releases of hazardous substances, pollutants, or contaminants throughout the United States and its territories. The NPL is intended primarily to guide the EPA in determining which sites warrant further investigation. The NPL is updated periodically, as mandated by CERCLA.

There were no Federal NPL sites found within a one-mile radius of the target property.

Lists of Federal Delisted NPL sites

The EPA may delete a final NPL site if it determines that no further response is required to protect human health or the environment. Under Section 300.425(e) of the NCP (55 FR 8845, March 8, 1990), a site may be deleted when no further response is appropriate if EPA determines that one of the following criteria has been met: 1) EPA, in conjunction with the state, has determined that responsible parties have implemented all appropriate response action required, 2) EPA, in consultation with the state, has determined that all appropriate Superfund-financed responses under CERCLA have been implemented and that no further response by responsible parties is appropriate, 3) A remedial investigation/feasibility study (RI/FS) has shown that the release poses no significant threat to public health or the environment and, therefore, remedial measures are not appropriate.

There were no Federal Delisted NPL sites found within a half-mile radius of the target property.

Lists of Federal sites subject to CERCLA removals and CERCLA orders

CERCLA identifies the classes of parties liable under CERCLA for the cost of responding to releases of hazardous substances. In addition, CERCLA contains provisions specifying when Federal installations must report releases of hazardous substances and the cleanup procedures they must follow. Executive Order No. 12580, Superfund Implementation, delegates response authorities to EPA and the Coast Guard. Generally, the head of the Federal agency has the delegated authority to address releases at the Federal facilities in its jurisdiction.

There were no Federal sites subject to CERCLA removals and/or orders found within a half-mile radius of the target property.

Lists of Federal CERCLA sites with NFRAP

No Further Remedial Action Planned (NFRAP) is a decision made as part of the Superfund remedial site evaluation process to denote that further remedial assessment activities are not required and that the facility/site does not pose a threat to public health or the environment sufficient to qualify for placement on the National Priorities List (NPL) based on currently available information. These facilities/sites may be re-evaluated if EPA receives new information or learns that site conditions have changed. A NFRAP decision does not mean the facility/site is free of contamination and does not preclude the facility/site from being addressed under another federal, state or tribal cleanup program.

There were no Federal CERCLA sites with No Further Remedial Action Planned (NFRAP) decisions found within a half-mile radius of the target property.

Lists of Federal RCRA facilities undergoing Corrective Action

Corrective action is a requirement under the Resource Conservation and Recovery Act (RCRA) that facilities that treat, store or dispose of hazardous wastes investigate and cleanup hazardous releases into soil, ground water, surface water and air. Corrective action is principally implemented through RCRA permits and orders. RCRA permits issued to TSDFs must include provisions for corrective action as well as financial assurance to cover the costs of implementing those cleanup measures. In addition to the EPA, 44 states and territories are authorized to run the Corrective Action program.

There were no Federal RCRA facilities undergoing corrective action(s) found within a one-mile radius of the target property.

Lists of Federal RCRA TSD facilities

The final link in RCRA's cradle-to-grave concept is the treatment, storage, and disposal facility (TSDF) that follows the generator and transporter in the chain of waste management activities. The regulations pertaining to TSDFs are more stringent than those that apply to generators or transporters. They include general facility standards as well as unit-specific design and operating criteria.

There were no Federal RCRA treatment, storage and disposal facilities (TSDFs) found within a half-mile radius of target property.

Lists of Federal RCRA generators

A generator is any person who produces a hazardous waste as listed or characterized in part 261 of title 40 of the Code of Federal Regulations (CFR). Recognizing that generators also produce waste in different quantities, EPA established three categories of generators in the regulations: very small quantity generators, small quantity generators, and large quantity generators. EPA regulates hazardous waste under the Resource Conservation and Recovery Act (RCRA) to ensure that these wastes are managed in ways that protect human health and the environment. Generators of hazardous waste are regulated based on the amount of hazardous waste they generate in a calendar month, not the size of their business or facility.

There were no Federal RCRA generators found at the target property and/or adjoining properties.

Federal institutional control/engineering control registries

Institutional Controls (IC) are defined as non-engineered and/or legal controls that minimize the potential human exposure to contamination by limiting land or resource use. Whereas, Engineering Controls (EC) consist of engineering measures (e.g. caps, treatment systems, etc.) designed to minimize the potential for human exposure to contamination by either limiting direct contact with contaminated areas or controlling migration of contaminants through environmental media.

There were no Federal institutional or engineering controls found at the target property.

Federal ERNS list

The Emergency Response Notification System (ERNS) is a database used to store information on notification of oil discharges and hazardous substances releases. The ERNS program is a cooperative data sharing effort encompassing the National Response Center (NRC), operated by the US Coast Guard, EPA HQ and EPA regional offices. ERNS data is used to analyze release notifications, track EPA responses and compliance to environmental laws, support emergency planning efforts, and assist decision-makers in developing spill prevention programs.

There were no Federally recorded releases of oil and/or hazardous substances at the target property.

Lists of state and tribal Superfund equivalent sites

In order to maintain close coordination with the states and tribes in the NPL listing decision process, the EPA's policy is to determine the position of states and tribes on sites that EPA is considering for listing. Consistent with this policy, since 1996, it has been the EPA's general practice to seek the state or tribe's position on sites under consideration for NPL listing by submitting a written request to the governor/state environmental agency or tribe. Various states may have their own program for identifying, investigating and cleaning up sites where consequential amounts of hazardous waste may have been disposed that work in conjunction with the EPA's Superfund remedial program.

There were no State and/or tribal Superfund equivalent sites found within a one-mile radius of target property.

Lists of state and tribal hazardous waste facilities

EPA established basic hazardous waste management standards for businesses who produce hazardous waste and categorized three businesses based on the volume of hazardous waste produced in a calendar month. On the federal level, there are three generator categories: large quantity generator, small quantity generator, and conditionally exempt small quantity generator. Some states are authorized to establish generator categories that are different from those that federal EPA set up. State regulatory requirements for generators of hazardous waste may be more stringent than the federal program.

There were no State and/or tribal hazardous waste facilities found within a half-mile radius of the target property.

Lists of state and tribal landfills and solid waste disposal facilities

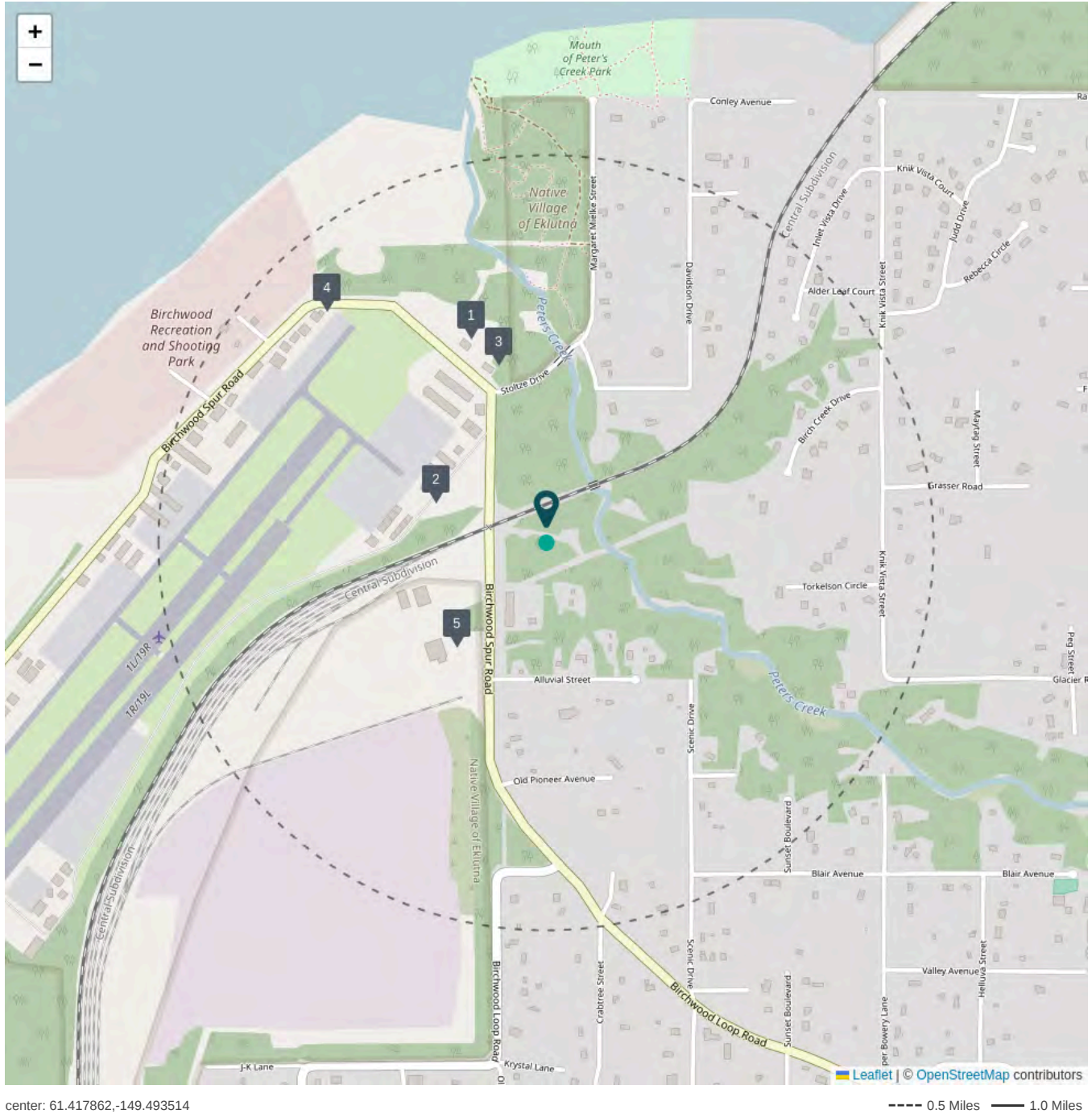
Title 40 of the CFR parts 239 through 259 contain the regulations for non-hazardous solid waste programs set up by the states. EPA has requirements for state solid waste permit programs, guidelines for the processing of solid waste, guidelines for storage and collection of commercial, residential and institutional solid waste, and the criteria for municipal solid waste landfills. State solid waste programs may be more stringent than the federal code requires.

There were no State and/or tribal landfills or solid waste disposal facilities found within a half-mile radius of the target property.

Lists of state and tribal leaking storage tanks

ADEC - LEAKING UNDERGROUND STORAGE TANKS

Alaska's Department of Environmental Conservation (ADEC) Contaminated Sites Program (CSP) maintains a database of all contaminated sites currently undergoing cleanup, sites with cleanups planned, and sites where cleanups have been completed. This data set contains sites where a leaking underground storage tank has been located by the ADEC, and was searched to return all records within a half-mile of the target property.



center: 61.417862,-149.493514

----- 0.5 Miles ——— 1.0 Miles

1

ADOTPF - Chugiak Volunteer Fire Station

20651 Birchwood Spur

Hazard ID: 24641

File ID: 2106.26.014

Contaminated Site or LUST: LUST

Site Name: ADOTPF - Chugiak Volunteer Fire Station

Address: 20651 Birchwood Spur

City: Chugiak

Borough: Anchorage

State: AK

ZIP Code: 99567

Status: Cleanup Complete

Staff: No Longer Assigned

Landowner: State of Alaska - ADOTPF DES-Design / Engineering Svcs

Site Type: Unknown

Latitude: 61.421658

Longitude: -149.496513

Horizontal Datum: NAD83

Distance From Center (Miles): 0.2803

Site Source: last updated 08-21-2023 from ADEC-LUST-NEW

2

Aviation North Al Hand Birchwood Airport Lot 19 Block 100

Birchwood Airport

Hazard ID: 24198

File ID: 2106.26.011

Contaminated Site or LUST: LUST

Site Name: Aviation North Al Hand Birchwood Airport Lot 19 Block 100

Address: Birchwood Airport

City: Chugiak

Borough: Anchorage

State: AK

ZIP Code: 99567

Status: Cleanup Complete

Staff: No Longer Assigned

Landowner: Al Hand

Site Type: Unknown

Latitude: 61.418598

Longitude: -149.497877

Horizontal Datum: NAD83

Distance From Center (Miles): 0.1529

Site Source: last updated 08-21-2023 from ADEC-LUST-NEW

3

Civil Air Patrol / Birchwood

Birchwood Airport

Hazard ID: 25083

File ID: 2106.26.010

Contaminated Site or LUST: LUST

Site Name: Civil Air Patrol / Birchwood

Address: Birchwood Airport

City: Chugiak

Borough: Anchorage

State: AK

ZIP Code: 99567

Status: Cleanup Complete

Staff: No Longer Assigned

Landowner: Civil Air Patrol Real Estate Manager

Site Type: Unknown

Latitude: 61.421158

Longitude: -149.495413

Horizontal Datum: NAD83

Distance From Center (Miles): 0.2362

Site Source: last updated 08-21-2023 from ADEC-LUST-NEW

4

Kelly Vrem, Birchwood Airport

Lot 1, Block 500, Birchwood Airport

Hazard ID: 24715

File ID: 2106.26.008

Contaminated Site or LUST: LUST

Site Name: Kelly Vrem, Birchwood Airport

Address: Lot 1, Block 500, Birchwood Airport

City: Chugiak

Borough: Anchorage

State: AK

ZIP Code: 99567

Status: Cleanup Complete

Staff: No Longer Assigned

Landowner: Kelly Vrem

Site Type: Unknown

Latitude: 61.422158

Longitude: -149.502113

Horizontal Datum: NAD83

Distance From Center (Miles): 0.4109

Site Source: last updated 08-21-2023 from ADEC-LUST-NEW

5

Kenai Supply Inc., Birchwood

20850 Birchwood Spur Rd.

Hazard ID: 23342

File ID: 2106.26.013

Contaminated Site or LUST: LUST

Site Name: Kenai Supply Inc., Birchwood

Address: 20850 Birchwood Spur Rd.

City: Chugiak

Borough: Anchorage

State: AK

ZIP Code: 99567

Status: Cleanup Complete

Staff: No Longer Assigned

Landowner: Kenai Supply, Inc.

Site Type: Unknown

Latitude: 61.415898

Longitude: -149.497077

Horizontal Datum: NAD83

Distance From Center (Miles): 0.1796

Site Source: last updated 08-21-2023 from ADEC-LUST-NEW

Lists of state and tribal registered storage tanks

EPA initially issued UST regulations in 1988. In 2015, EPA modified the UST regulation, which was effective October 13, 2015 in Indian Country and states without State Program Approval. EPA recognizes that, because of the size and diversity of the regulated community, state and local governments are in the best position to oversee USTs: 1) State and local authorities are closer to the situation in their domain and are in the best position to set priorities, 2) Subtitle I of the Solid Waste Disposal Act allows state UST programs approved by EPA to operate in lieu of the federal program, 3) the state program approval (SPA) regulations set criteria for states to obtain the authority to operate in lieu of the federal program. State programs must be at least as stringent as EPA's. A complete version of the law that governs USTs can be found in U.S. Code, Title 42, Chapter 82, Subchapter IX.

There were no State and/or tribal registered storage tanks found at subject and adjoining properties.

State and tribal institutional control/engineering control registries

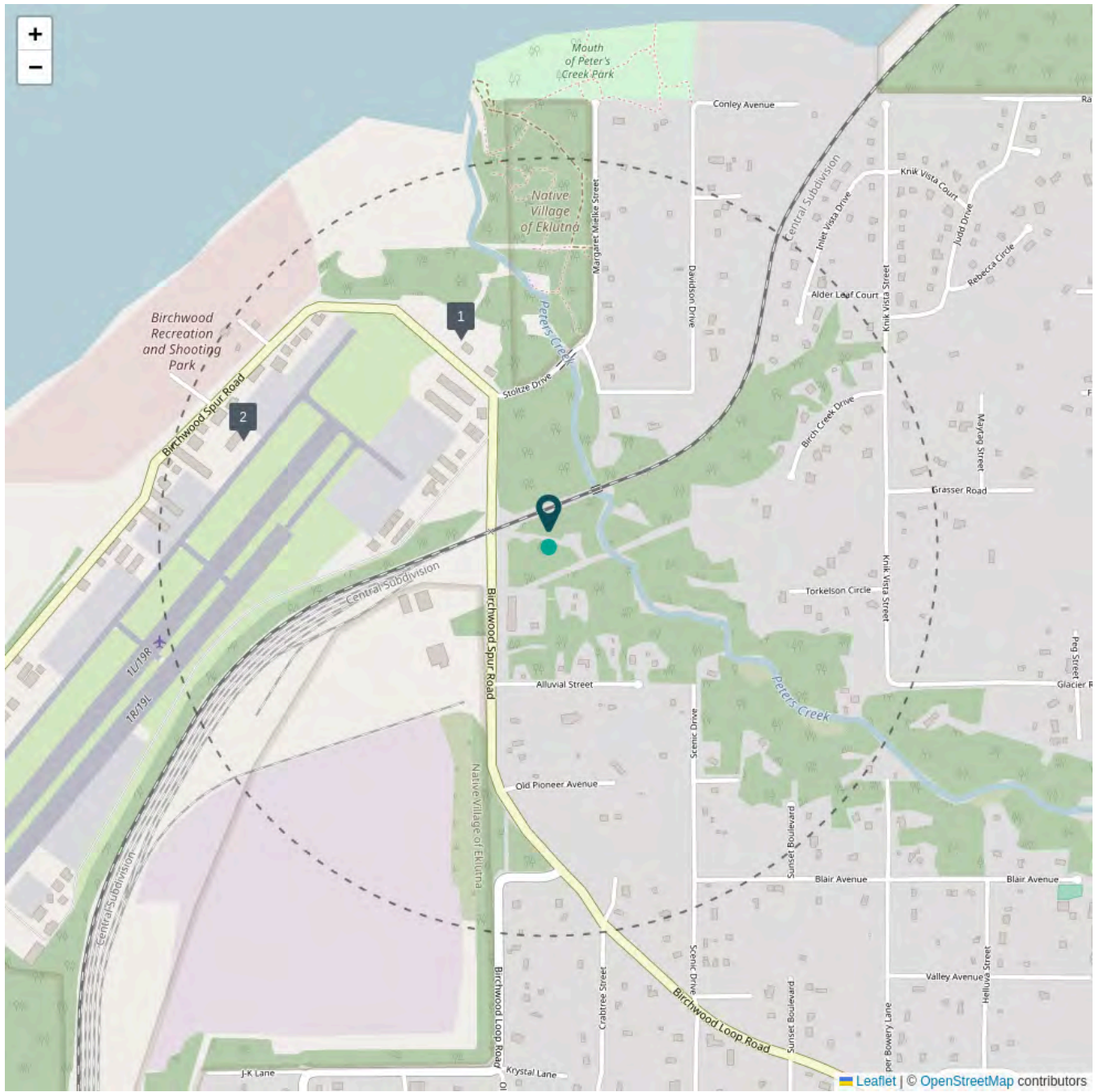
Institutional controls are non-engineered instruments such as administrative and legal controls that help minimize the potential for human exposure to contamination and/or protect the integrity of the remedy. Engineering controls consist of engineering measures (e.g, caps, treatment systems, etc.) designed to minimize the potential for human exposure to contamination by either limiting direct contact with contaminated areas or controlling migration of contaminants through environmental media. It is EPA's expectation that treatment or engineering controls will be used to address principal threat wastes and that groundwater will be returned to its beneficial use whenever practicable.

There were no State and/or tribal institutional and/or engineering controls found filed against the target property.

Lists of state and tribal voluntary cleanup sites

ADEC - CONTAMINATED SITES PROGRAM

Alaska Department of Conservation's (ADECs) Contaminated Sites Program (CSP) protects human health and the environment by managing the cleanup of contaminated soil and groundwater in Alaska. A contaminated site is a location where hazardous substances, including petroleum products, have been improperly disposed. Sites included in the Contaminated Sites Database include: brownfields, RCRA Corrective Action Program sites, Federal Facilities (DOD and NPL), dry cleaners, and Leaking Underground Storage Tank (LUST) release sites. This database was searched to return all contaminated site(s) records within a half-mile of the target property.



center: 61.417862,-149.493514

--- 0.5 Miles — 1.0 Miles

1

ADOT&PF Birchwood Maintenance Station Class V Injection Well

20651 Birchwood Spur Road; Birchwood Airport

Hazard ID: 26242

File ID: 2106.38.011

Program: CSP

Site Name: ADOT&PF Birchwood Maintenance Station Class V Injection Well

Address: 20651 Birchwood Spur Road; Birchwood Airport

City: Chugiak

Borough: Anchorage

State: AK

99627: 99567

Status: Cleanup Complete - Institutional Controls

Staff: IC Unit

Landowner: ADOT&PF CRD & HA Admin (8/19)

Site Type: Airport/Airfield

Latitude: 61.421698

Longitude: -149.496994

Horizontal Datum: WGS84

Distance From Center (Miles): 0.2889

Site Source: last updated 08-21-2023 from ADEC-CSP-NEW

2

Birchwood Airport

Lot 4, Block 500 Birchwood Airport

Hazard ID: 1471

File ID: 2106.38.003

Program: CSP

Site Name: Birchwood Airport

Address: Lot 4, Block 500 Birchwood Airport

City: Chugiak

Borough: Anchorage

State: AK

99627: 99567

Status: Cleanup Complete

Staff: No Longer Assigned

Landowner: First National Bank of Anchorage

Site Type: Unknown

Latitude: 61.419819

Longitude: -149.505419

Horizontal Datum:

Distance From Center (Miles): 0.416

Site Source: last updated 08-21-2023 from ADEC-CSP-NEW

Lists of state and tribal brownfields sites

Since its inception in 1995, EPA's Brownfields and Land Revitalization Program has grown into a proven, results-oriented program that has changed the way communities address and manage contaminated property. The program is designed to empower states, tribes, communities, and other stakeholders to work together to prevent, assess, safely clean up, and sustainably reuse brownfields. Beginning in the mid-1990s, EPA provided small amounts of seed money to local governments that launched hundreds of two-year Brownfields pilot projects and developed guidance and tools to help states, communities and other stakeholders in the cleanup and redevelopment of brownfields sites.

There were no State and/or tribal brownfields sites found within a half-mile radius of the target property.

State and/or tribal lists of sites requiring further investigation / remediation

No records found

State list of Significant Environmental Hazards (SEH)

No records found

Lists of state and tribal mine sites requiring further investigation and/or remediation

No records found

State and/or tribal lists of spills and spill responses

No records found

State and/or tribal lists of emergency responses

No records found

State and/or tribal lists of dry cleaners

No records found

State and/or tribal lists of clandestine laboratory cleanups

No records found

State and/or tribal lists of scrap/used tire processing facilities

No records found

State and/or tribal lists of underground injection control sites

No records found

State and/or tribal listings of permitted drywells

No state and/or tribal permitted drywells were found within a half-mile radius of the target property.

Automobile salvage yards

No records found

Livestock Waste Control sites

No records found

Controlled Animal Feeding Operations (CAFOs)

No records found

State and/or tribal lists of registered aboveground storage tanks (ASTs)

No records found

C.A.A. Permitted Facilities

No records found

NPDES Permitted Facilities

No records found

Onsite Wastewater Treatment sites

No records found

State and/or tribal lists of permitted facilities

No State and/or tribal permitted facilities found within a half-mile of the target property.

Resource Conservation and Recovery Act Information (RCRAInfo)

No records found

U.S. EPA Enforcement, Compliance History Online (ECHO)

No records found

U.S. EPA Underground Storage Tanks (UST)

No records found

U.S. EPA Toxic Substances Control Act (TSCA) database

No records found

U.S. EPA Toxic Release Inventory System (TRIS)

No records found

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Appendix L
BIA Timber Letter

Eklutna Native Village Gaming Facility Project Tree Inventory Results

Prepared For: Eklutna Native Village
Prepared By: Acorn Environmental
Date: August 27, 2024

Introduction

This memorandum has been prepared to summarize results of the tree survey conducted for the Eklutna Native Village Gaming Facility Project (Proposed Project) in accordance with Timber Harvest Permit requirements of the Bureau of Indian Affairs. The Proposed Project includes the development and operation of a gaming facility and associated infrastructure within an approximately 6.37-acre property located in the unincorporated community of Chugiak within the municipality of Anchorage, Alaska (Project Site). Development will require tree removal and land clearing on the Project Site, which is owned in restricted fee status by tribal members of the Native Village of Eklutna as Native allotment land under the Alaska Native Allotment Act. Some trees will be retained in the proposed parking and landscaping areas, as well as along the perimeter of the Project Site. Clearing activities will result in leftover vegetation and wood debris, such as stumps, which will be chipped on-site and used where feasible, depending on the quality of the chips.

The Project Site consists of portions of three restricted fee Native allotment parcels (Assessor's Parcel Numbers 05108101000, 05108102000, and 05108115000) currently owned by members of the Eklutna Native Village. No tree removal is proposed within the portion of these parcels that is subject to a right of way (ROW) easement for the adjacent Alaska Railroad. The Project Site is situated off Birchwood Spur Road with the Alaska Railroad to the north and the Birchwood Airport to the west, and is within Section 5 of Township 15 North, Range 1 West within the Seward Meridian, and is within the Anchorage B-7 NW United States Geological Survey 7.5' quadrangle map (**Figure 1**).

Survey Methodology

A tree survey was conducted on the Project Site July 16 - July 18, 2024 to approximate the type, number, and size of trees present within the Project Site. Tree size was estimated utilizing approximate diameter at breast height (dbh) and approximate height. Trees with a dbh of five inches or greater were recorded. Height was approximated using a clinometer and dbh was measured using a measuring tape. Due to the density of trees, discrete heights of individual trees were only observable from across roadways or from canopy openings in ruderal/developed areas.

Results

The Project Site primarily consists of undeveloped forestland (**Figure 1**). Tree species on the Project Site are predominantly birch (*Betula* sp.), specifically paper birch (*Betula papyrifera*). Other deciduous trees include alder (*Alnus* sp.) and poplar (*Populus* sp.), while spruce species, such as white spruce (*Picea glauca*) and red spruce (*Picea rubens*), make up a minor component of the canopy. The understory is generally sparse, consisting mainly of young trees and scattered willows (*Salix* sp.). The forest floor features dense ground cover, which reduces ground visibility, with dominant species including Devil’s club (*Oplopanax horridus*) and cow parsley (*Anthriscus sylvestris*). Other common species are lady fern (*Athyrium filix*), fireweed (*Chamaenerion angustifolium*), meadow horsetail (*Equisetum pratense*), prickly rose (*Rosa acicularis*), raspberry (*Rubus idaeus*), and clasp-leaf twisted stalk (*Streptopus amplexifolius*).

Trees within the Project Site generally fell into a handful of similar sizes. Tree size information for the Project Site is presented in **Table 1** and a tree inventory is detailed in **Table 2**. Based on these tables, a total of 1,617 trees were inventoried on the Project Site. This includes a portion of the trees within the Alaska Railway ROW. The actual number of trees on the Project Site, excluding the Alaska Railroad ROW is approximately 1,309 trees.

Table 1: Tree Sizing *

Species	Extra Small	Small	Medium	Large	Extra Large
Birch	dbh: 5.0-6.7" height: 57.1'	dbh: 6.8-9.6" height: 69.4'	dbh: 9.7-12.4" height: 85.2'	dbh: 12.5+" height: 128.8'	dbh: None height: None
Alder	dbh: 5.0-14.3" height: 79.8'	dbh: 14.4-15.3" height: 79.8'	dbh: 15.4-16.2" height: 79.8'	dbh: 16.3+" height: 79.8'	dbh: None height: None
Poplar	dbh: 5.0-5.7" height: 30.7'	dbh: 5.8-8.0" height: 79.0'*	dbh: 8.1-11.1" height: 127.2'	dbh: 11.2-23.6" height: 138.6'*	dbh: 23.6+" height: 150'*
Spruce	dbh: 5.0-5.4" height: 40.4'	dbh: 5.4-6.4" height: 40.4'	dbh: 6.5-7.6" height: 50.3'*	dbh: 7.7+" height: 60.2'	dbh: None height: None

* These heights were approximated based upon averaging between sizing categories or visual approximation as no line of sight was available to allow for accurate clinometer reading

Table 2: Tree Inventory

Species	Extra Small	Small	Medium	Large	Extra Large
Birch	269	539	389	200	0
Alder	8	4	1	2	0
Poplar	5	19	22	34	3
Spruce	48	22	28	24	0

References

National Park Service, 2010. Forests of Alaska. Available online at:
<https://npshistory.com/publications/usfs/handbooks/alaska/sec1.htm>. Accessed August 2024.

Resource Development Council, 2024. Alaska's Forest Industry. Available online at:
<https://www.akrdc.org/forestry>. Accessed August 2024.

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<https://npshistory.com/publications/usfs/handbooks/alaska/sec1.htm>. Accessed August 2024.



Legend

- Project Site
- Ondola Allotment
- County Parcels

Alluvial St
Image © 2024 Airbus



0 200 400 Feet

FIGURE 1
AERIAL PHOTOGRAPH

Appendix M
Tribal Ordinance



Native Village of Eklutna
Tribal Government

ORDINANCE 2024-__

AN ORDINANCE of the Native Village of Eklutna Approving Amendments to the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance

WHEREAS the Native Village of Eklutna (“Tribe”) is a sovereign federally-recognized Indian tribe and is a distinct, independent political community, and as such, is qualified to exercise powers of self-government by reason of its original tribal sovereignty as passed down from its ancestors since time immemorial;

WHEREAS, the Constitution of the Tribe was duly and properly enacted, adopted, and approved in accordance with all applicable Tribal laws, requirements, customs, and traditions by the members of the Tribe on January 27, 1996, by a majority of the qualified voters of the Tribe with a quorum of at least fifteen percent of those entitled to vote (the “Constitution”);

WHEREAS, Article II of the Constitution extends the authority of the Tribe to allotments of tribal members within the traditional lands and waters of the Eklutna people, including the allotment owned by the Ondola family located at Lots 64, 66 and 67, located within Section 5, T15N, R1W, Seward Meridian Alaska, containing approximately 8.05 acres, more or less (“Ondola Allotment”) and subject to a restriction on alienation pursuant to the Alaska Native Allotment Act; and

WHEREAS, Article IX, Section 1(g), (h) and (n) of the Constitution authorizes the Tribal Council to administer tribal assets and tribal economic affairs and enterprises, to promote the peace, safety, health, politics, education and general welfare of the Tribe and to enact ordinances and regulations; and

WHEREAS, the Tribe and Gaming Authority plan to engage in casino gaming under the Indian Gaming Regulatory Act (“IGRA”) and applicable regulations and standards of the National Indian Gaming Commission (“NIGC”) in a facility on the Ondola Allotment;

WHEREAS, on March 22, 2006 the Tribal Council adopted Ordinance No. 2007O-01 the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance regulating gaming on restricted lands within the jurisdiction of the Tribe;

WHEREAS, on April 19, 2024 the Tribal Council enacted the Native Village of Eklutna Gaming

Ordinance of 2024 (“Gaming Ordinance”) to govern gaming on the Ondola Allotment, referred to in the Gaming Ordinance as the Tribe’s Lands, pursuant to IGRA;

WHEREAS, on July 18, 2024, the NIGC determined that the Ondola Allotment constitutes Indian lands eligible for gaming by the Tribe under IGRA and that the Gaming Ordinance complied with IGRA and with applicable NIGC regulations and issued its approval of that Gaming Ordinance;

WHEREAS, the Tribal Council finds it necessary to amend the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance; and

NOW THEREFORE BE IT RESOLVED THAT, the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance shall be amended to read as follows:

EKLUTNA PUBLIC HEALTH AND SAFETY AND ANTI-DISCRIMINATION AT GAMING FACILITY ORDINANCE

Section 101 Definitions

(a) This Ordinance shall be referred to as the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance.

(b) The terms defined in the Native Village of Eklutna Gaming Ordinance of 2024 (“Gaming Ordinance”) shall have the same meaning in this Ordinance.

Section 105 Findings

The Tribal Council makes the following findings:

(a) Section 701 of the Gaming Ordinance requires that the construction, maintenance and operation of the Gaming Facility and the Gaming Operation be conducted in a manner that adequately protects the environment and the public health and safety and complies with the requirements and all applicable health, safety and environmental standards enacted by the Tribe.

(b) Section 801 of the Gaming Ordinance requires the Gaming Facility to secure a facility license for the Gaming Facility in which gaming is operated on Indian lands and establishes environmental, health, and public safety standards for issuance of such license by the Gaming Commission, including that the licensee (i) maintain an “orderly, clean and neat establishment,” (ii) subject the Gaming Facility to patrol by security personnel, (iii) permit inspection by authorized tribal officials and (iv) prohibit discrimination.

(c) The Tribe will continue to provide governmental services to the Ondola Allotment as it has done for decades. The Tribe relies on federal, state and local agencies to provide services that supplement services provided by the Tribe. The Tribe has historically collaborated with federal, state and local agencies regarding governmental services to the Ondola Allotment and the tribal community, and the Tribe will continue such collaboration to ensure the highest quality governmental services.

(d) While Alaska state and local governments have criminal jurisdiction in Indian country, including the Ondola Allotment, pursuant to Public Law 280, they lack civil regulatory jurisdiction.

(e) The Municipality of Anchorage provides police, fire protection and EMS services within municipal limits pursuant to Anchorage Municipal Code (AMC) 27.30.135(C), requiring that “[p]olice protection services shall be provided in the Anchorage Metropolitan Police Service Area,” which encompasses the Ondola Allotment. Chugiak Volunteer Fire and Rescue Company, Inc. provides fire protection and EMS services to the Chugiak area pursuant to AMC 27.30.060(B), requiring that “[f]ire protection shall be provided in the Chugiak Fire Service Area,” which encompasses the Ondola Allotment.

(f) Timber on the Ondola Allotment must be cut in connection with development of the Gaming Facility. The Bureau of Indian Affairs is evaluating whether a permit is required.

Section 110 Purpose

The purpose of this Ordinance is to provide for the protection of the health and safety of the employees, patrons and visitors of any Gaming Facility established pursuant to the Gaming Ordinance.

Section 115 Scope

These provisions shall apply to the Gaming Operation and the Gaming Facility, as defined in Sections 210 and 212 of the Gaming Ordinance of 2024 of the Native Village of Eklutna, on the Ondola Allotment.

Section 120 Building and Safety Standards

(a) The provisions of Title 23 of the Anchorage Municipal Code are adopted as tribal law and shall govern the construction, expansion, modification, and renovation of the Gaming Facility. The Tribal Council shall appoint a building official and such other officials necessary to implement and enforce such provisions.

(b) The provisions of Title 13, Chapter 50 of the Alaska Administrative Code are adopted as tribal law and shall govern the construction, expansion, modification, and renovation of the Gaming Facility. The building official appointed by the Tribal Council pursuant to paragraph (a) shall have authority to carry out the plan review and issue the corresponding approvals specified in 13 AAC 50.027.

(c) The Tribal Council has determined that no additional timber on the Ondola Allotment shall be cut until the Bureau of Indian Affairs either issues a permit for such timber removal or determines that no permit is required.

Section 125 Disabilities

The standards of the Americans with Disabilities Act, 42 U.S.C § 12101 *et seq.* are adopted as tribal law and shall govern any construction, expansion or modification of any gaming facility.

Section 130 Food and Beverage Handling

The provisions of Title 16, Chapter 16.60 of the Anchorage Municipal Code regulating the sale of food are adopted as tribal law governing the Gaming Facility.

Section 135 Water Supply and Water Quality

(a) The Eklutna Gaming Authority shall, consistent with applicable law, ensure adequate water supply to meet the needs of the Gaming Facility either by causing water to be delivered to the Gaming Facility, or by utilizing ground water from a well.

(b) The standards for water quality adopted by the U.S. Environmental Protection Agency pursuant to the Clean Water Act and the Safe Drinking Water Act, including the national primary water drinking regulations and national water quality standards in Title 40 of the Code of Federal Regulations, are adopted as tribal law governing the Gaming Facility.

Section 140 Workplace and Occupational Health and Safety

The standards for workplace and occupational health and safety established by the U.S. Occupational Health and Safety Administration, including 29 CFR parts 1910, 1926 and 1977, are adopted as tribal law governing the Gaming Facility.

Section 145 Public Health and Safety

(a) The Gaming Operation shall comply with applicable federal laws regarding public health and safety.

(b) Prior to commencement of gaming operations at the Gaming Facility, the Tribal Council shall ensure that police and fire protection services are available to the Gaming Facility by (i) entering into a binding agreement with the Municipality of Anchorage and/or Chugiak Volunteer Fire and Rescue Company, Inc. governing the provision of such services, or (ii) the provision of such services by the Tribe.

Section 150 Anti-Discrimination in Employment

(a) The relevant federal and state laws forbidding employers from discriminating in employment on the basis of race, color, religion, national origin, gender, sexual orientation, age or disability are adopted as tribal law governing the Gaming Facility.

(b) Nothing in subsection (a) shall prohibit the gaming operation and other enterprises in the gaming facility from giving preference in employment to Indians according to tribal law.

Section 155 Enforcement

Except to the extent provided otherwise in this Ordinance, the Gaming Commission shall enforce the provisions of this Ordinance pursuant to the Gaming Ordinance.

CERTIFICATION

This Ordinance was read and approved on the __th day of November 2024, by a vote of __ for, __ against, __ abstain and __ absent.

Attested to with our signatures fixed below by:

Aaron Leggett, President

Amanda Adams, Secretary



Native Village of Eklutna
Tribal Government

ORDINANCE 2025O-01

AN ORDINANCE of the Native Village of Eklutna Approving Amendments to the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance

WHEREAS the Native Village of Eklutna (“Tribe”) is a sovereign federally-recognized Indian tribe and is a distinct, independent political community, and as such, is qualified to exercise powers of self-government by reason of its original tribal sovereignty as passed down from its ancestors since time immemorial; and

WHEREAS, the Constitution of the Tribe was duly and properly enacted, adopted, and approved in accordance with all applicable Tribal laws, requirements, customs, and traditions by the members of the Tribe on January 27, 1996, by a majority of the qualified voters of the Tribe with a quorum of at least fifteen percent of those entitled to vote (the “Constitution”); and

WHEREAS, Article II of the Constitution extends the authority of the Tribe to allotments of tribal members within the traditional lands and waters of the Eklutna people, including the Allotment 975 A-055026 owned by the Ondola family located at Lots 64, 66 and 67, located within Section 5, Township 15 N, Range 1 West, Seward Meridian, Anchorage Recording District, Alaska (“Ondola Allotment”) and subject to a restriction on alienation pursuant to the Alaska Native Allotment Act; and

WHEREAS, Article IX, Section 1(g), (h) and (n) of the Constitution authorizes the Tribal Council to administer tribal assets and tribal economic affairs and enterprises, to promote the peace, safety, health, politics, education and general welfare of the Tribe and to enact ordinances and regulations; and

WHEREAS, the Tribe and Gaming Authority plan to engage in casino gaming under the Indian Gaming Regulatory Act (“IGRA”) and applicable regulations and standards of the National Indian Gaming Commission (“NIGC”) in a facility on the Ondola Allotment pursuant to a lease with the owners of the Ondola Allotment subject to approval by the Bureau of Indian Affairs; and

WHEREAS, on March 22, 2006, the Tribal Council adopted Ordinance No. 2007O-01 the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance regulating gaming on restricted lands within the jurisdiction of the Tribe; and

WHEREAS, on April 19, 2024, the Tribal Council enacted the Native Village of Eklutna Gaming Ordinance of 2024 (“Gaming Ordinance”) to govern gaming on the Ondola Allotment, referred to in the Gaming Ordinance as the Tribe’s Lands, pursuant to IGRA; and

WHEREAS, on July 18, 2024, the NIGC determined that the Ondola Allotment constitutes Indian lands eligible for gaming by the Tribe under IGRA and that the Gaming Ordinance complied with IGRA and with applicable NIGC regulations and issued its approval of that Gaming Ordinance; and

WHEREAS, the Tribal Council finds it necessary to amend the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance; and

NOW THEREFORE BE IT RESOLVED THAT, the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance shall be amended to read as follows:

EKLUTNA PUBLIC HEALTH AND SAFETY AND ANTI-DISCRIMINATION AT GAMING FACILITY ORDINANCE

Section 101 Definitions

(a) This Ordinance shall be referred to as the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance.

(b) The terms defined in the Native Village of Eklutna Gaming Ordinance of 2024 (“Gaming Ordinance”) shall have the same meaning in this Ordinance.

Section 105 Findings

The Tribal Council makes the following findings:

(a) Section 701 of the Gaming Ordinance requires that the construction, maintenance and operation of the Gaming Facility and the Gaming Operation be conducted in a manner that adequately protects the environment and the public health and safety and complies with the requirements and all applicable health, safety and environmental standards enacted by the Tribe.

(b) Section 801 of the Gaming Ordinance requires the Gaming Facility to secure a facility license for the Gaming Facility in which gaming is operated on Indian lands and establishes environmental, health, and public safety standards for issuance of such license by the Gaming Commission, including that the licensee (i) maintain an “orderly, clean and neat establishment,” (ii) subject the Gaming Facility to patrol by security personnel, (iii) permit inspection by authorized tribal officials and (iv) prohibit discrimination.

(c) The Tribe will continue to provide governmental services to the Ondola Allotment as it has done for decades. The Tribe relies on federal, state and local agencies to provide services that supplement services provided by the Tribe. The Tribe has historically collaborated with federal, state and local agencies regarding governmental services to the Ondola Allotment and the tribal community, and the Tribe will continue such collaboration to ensure the highest quality governmental services.

(d) While Alaska state and local governments have criminal jurisdiction in Indian country, including the Ondola Allotment, pursuant to Public Law 280, they lack civil regulatory jurisdiction.

(e) The Municipality of Anchorage provides police, fire protection and EMS services within municipal limits pursuant to Anchorage Municipal Code (AMC) 27.30.135(C), requiring that “[p]olice protection services shall be provided in the Anchorage Metropolitan Police Service Area,” which encompasses the Ondola Allotment. Chugiak Volunteer Fire and Rescue Company, Inc. provides fire protection and EMS services to the Chugiak area pursuant to AMC 27.30.060(B), requiring that “[f]ire protection shall be provided in the Chugiak Fire Service Area,” which encompasses the Ondola Allotment.

Section 110 Purpose

The purpose of this Ordinance is to provide for the protection of the health and safety of the employees, patrons and visitors of any Gaming Facility established pursuant to the Gaming Ordinance.

Section 115 Scope

These provisions shall apply to the Gaming Operation and the Gaming Facility, as defined in Sections 210 and 212 of the Gaming Ordinance, on the Ondola Allotment.

Section 120 Building and Safety Standards

(a) The provisions of Title 23 of the Anchorage Municipal Code, except the provisions listed in subsection (a)(i) below, are adopted as tribal law and shall govern the construction, expansion, modification, and renovation of the Gaming Facility, provided that references to the Municipality (except the references to requirements that contractors be licensed by the Municipality in Section 23.10.105.1.B, .F and .H) shall be to the Tribe, references to the board of building regulation examiners and appeals, including three-member hearing panel(s), shall be to the Tribal Court, and reference to the Building Safety Hotline shall be to the building official phone number, and further provided that the building official may revoke a certificate of occupancy for violation of tribal law. The Tribal Council shall appoint a building official and such other officials necessary to implement and enforce such provisions.

- i. The following provisions of Title 23 of the Anchorage Municipal Code are not adopted as tribal law: Sections 23.10.101.9.2 (relocated or moved buildings), 23.10.103.3.2.1 (internal auditor audits), 23.10.103.4.2 (appeal pools and three-member hearing panels), 23.10.103.4.4.A, .B, .D, .E, .F, .G, .H and .I (building board procedures), 23.10.103.4.5 (building board secretary), 23.15.203.4.6 (supplementing hearing packet), 23.10.103.4.7 (appeal filing fee), 23.10.103.7.2 (contractor license suspension or revocation), 23.10.104.4.1.C (permit application requirement for approval from state agency regarding AMC 16.60), 23.10.104.9 (optional third-party plan review), 23.10.104.15 (fees) and 23.10.105.2.1-.6 (trades certificates of qualification issuance and revocation).

(b) The provisions of 13 AAC 50.027 of the Alaska Administrative Code are adopted as tribal law and shall govern the construction, expansion, modification, and renovation of the Gaming

Facility. The building official appointed by the Tribal Council pursuant to paragraph (a) shall have authority to carry out the plan review and issue the corresponding approvals specified in 13 AAC 50.027(a). The plan review fees specified in 13 AAC 50.027(c) shall be waived.

(c) The protective measures, best management practices and mitigation measures analyzed in the environmental assessment related to the lease of the Ondola Allotment, which will eliminate or substantially reduce potential environmental impacts of the Gaming Facility and therefore support a Finding of No Significant Impacts, shall as a matter of tribal law govern the design, construction and operation of the Gaming Facility.

(d) The Tribal Council has determined that no future site preparation or construction activities related to the Gaming Facility shall be carried out on the Ondola Allotment until the Bureau of Indian Affairs approves the lease between the Tribe and the owners of the Ondola Allotment.

Section 125 Disabilities

The standards of the Americans with Disabilities Act, 42 U.S.C § 12101 *et seq.* are adopted as tribal law and shall govern any construction, expansion or modification of any gaming facility.

Section 130 Food and Beverage Handling

The provisions of Title 16, Chapter 16.60 of the Anchorage Municipal Code regulating the sale of food are adopted as tribal law governing the Gaming Facility.

Section 135 Water Supply and Water Quality

(a) The Eklutna Gaming Authority shall, consistent with applicable law, ensure adequate water supply to meet the needs of the Gaming Facility either by causing water to be delivered to the Gaming Facility, or by utilizing ground water from a well.

(b) The standards for water quality adopted by the U.S. Environmental Protection Agency pursuant to the Clean Water Act and the Safe Drinking Water Act, including the national primary water drinking regulations and national water quality standards in Title 40 of the Code of Federal Regulations, are adopted as tribal law governing the Gaming Facility.

Section 140 Workplace and Occupational Health and Safety

The standards for workplace and occupational health and safety established by the U.S. Occupational Health and Safety Administration, including 29 C.F.R. Parts 1910, 1926 and 1977, are adopted as tribal law governing the Gaming Facility.

Section 145 Public Health and Safety

(a) The Gaming Operation shall comply with applicable federal laws regarding public health and safety.

(b) Prior to commencement of gaming operations at the Gaming Facility, the Tribal Council shall ensure that police and fire protection services are available to the Gaming Facility by (i) entering into a binding agreement with the Municipality of Anchorage and/or Chugiak Volunteer Fire and Rescue Company, Inc. governing the provision of such services, (ii) the provision of such services by the Tribe, or (iii) a combination of the methods described in (i) and (ii).

Section 150 Anti-Discrimination in Employment

(a) The relevant federal laws forbidding employers from discriminating in employment on the basis of race, color, religion, national origin, gender, sexual orientation, age or disability are adopted as tribal law governing the Gaming Facility.

(b) Nothing in subsection (a) shall prohibit the gaming operation and other enterprises in the Gaming Facility from giving preference in employment to Indians according to tribal law.

Section 155 Enforcement

(a) Except to the extent provided otherwise in this Ordinance, the Gaming Commission shall enforce the provisions of this Ordinance pursuant to the Gaming Ordinance.

(b) Nothing in this Ordinance shall be construed as consent to the application or enforcement of state law on the Ondola Allotment or at the Gaming Facility.

CERTIFICATION

This Ordinance was read and approved on the 7th day of January 2025, by a vote of 7 for, 0 against, 0 abstain and 0 absent.

Attested to with our signatures fixed below by:



Aaron Leggett, President



Amanda Adams, Secretary

Appendix N
Public Notices

**NOTICE OF AVAILABILITY
ENVIRONMENTAL ASSESSMENT
FOR THE EKLUTNA NATIVE VILLAGE GAMING FACILITY PROJECT**

Notice is hereby given that the Bureau of Indian Affairs (BIA), Department of the Interior, has released an Environmental Assessment (EA) (DOI-BIA-AK-61628) dated December 2024 for the development of a class II gaming facility by the Eklutna Native Village (Tribe) on an approximately 6.37-acre portion of a restricted fee Native Allotment (Project Site) owned by members of the Tribe in the unincorporated community of Chugiak within the boundaries of the Municipality of Anchorage, Alaska (Proposed Project). The EA has been completed in accordance with requirements set forth in the National Environmental Policy Act (NEPA; 42 United States Code [USC] §4321 et seq.), the 2024 Council on Environmental Quality regulations for Implementing NEPA (40 CFR § 1500 et seq.), the Department of the Interior's Procedures for the Implementation of NEPA (43 CFR Part 46), and the BIA NEPA Handbook (59 Indian Affairs Manual 3-H), and assesses the environmental impacts that could result from the BIA's approval of a business lease between the Native Allotment landowners and the Eklutna Native Village tribal government for the development and operation of a class II gaming facility on the Project Site (Proposed Action).

The Project Site consists of a restricted fee Native Allotment comprised of three parcels located in the unincorporated community of Chugiak within the boundaries of the Municipality of Anchorage, Alaska. The Project Site previously contained a residence and several outbuildings but currently consists of vacant partially wooded land. Surrounding land uses include low density residential, industrial, the Alaska Railroad, and the Birchwood Airport.

The BIA serves as the Lead Agency for NEPA compliance. Based on the analysis and impacts discussed in the EA and comments received during the public review period, the BIA will decide whether to reach a Finding of No Significant Impact, direct further work on the EA, or initiate the preparation of an Environmental Impact Statement.

The public comment period for the EA will be open for 15 days, beginning on December 20, 2024 and ending on January 6, 2025. For additional information, please contact Harrilene Yazzie, Division of Environmental & Cultural Resources Management, Bureau of Indian Affairs - Alaska Region, at 907.271.4004, or by email at Alaska_NEPA@bia.gov. Written comments may be emailed to Alaska_NEPA@bia.gov or mailed or hand-delivered to the following address:

Jolene John, Regional Director
Bureau of Indian Affairs - Alaska Region
3601 C Street, Suite 1200
Anchorage, AK 99503

*Due to a pending federal government shutdown, written comments on the EA are strongly encouraged to be submitted electronically or by mail. **If there is a lapse in federal appropriations the comment period will continue to run but the Bureau of Indian Affairs, Alaska Region will not be able to accept hand delivered comments.***

The EA is available for public review online at <http://www.EklutnaEA.com> and in hard copy at the Chugiak-Eagle River Library located in the Eagle River Town Center at 12001 Business Blvd. 176, Eagle River, AK 99577, telephone number 907.343.1530.

Order Number: W0049843
Order Status: Saved
Classification: Legals & Public Notices
Package: Legals ADN
Final Cost: \$356.70
Payment Type:
User ID: W0016709

ADDITIONAL OPTIONS

Affidavit Charge - Digital \$5

SCHEDULE FOR AD NUMBER W0023609

December 22, 2024
Anchorage Daily News
Legals

NOTICE OF AVAILABILITY ENVIRONMENTAL ASSESSMENT FOR THE EKLUTNA NATIVE VILLAGE GAMING FACILITY PROJECT

Notice is hereby given that the Bureau of Indian Affairs (BIA), Department of the Interior, has released an Environmental Assessment (EA) (DOI-BIA-AK-61628) dated December 2024 for the development of a class II gaming facility by the Eklutna Native Village (Tribe) on an approximately 6.37-acre portion of a restricted fee Native Allotment (Project Site) owned by members of the Tribe in the unincorporated community of Chugiak within the boundaries of the Municipality of Anchorage, Alaska (Proposed Project). The EA has been completed in accordance with requirements set forth in the National Environmental Policy Act (NEPA; 42 United States Code [USC] §4321 et seq.), the 2024 Council on Environmental Quality regulations for implementing NEPA (40 CFR § 1500 et seq.), the Department of the Interior's Procedures for the Implementation of NEPA (43 CFR Part 46), and the BIA NEPA Handbook (59 Indian Affairs Manual 3-H), and assesses the environmental impacts that could result from the BIA's approval of a business lease between the Native Allotment landowners and the Eklutna Native Village tribal government for the development and operation of a class II gaming facility on the Project Site (Proposed Action).

The Project Site consists of a restricted fee Native Allotment comprised of three parcels located in the unincorporated community of Chugiak within the boundaries of the Municipality of Anchorage, Alaska. The Project Site previously contained a residence and several outbuildings but currently consists of vacant partially wooded land. Surrounding land uses include low density residential, industrial, the Alaska Railroad, and the Birchwood Airport.

The BIA serves as the Lead Agency for NEPA compliance. Based on the analysis and impacts discussed in the EA and comments received during the public review period, the BIA will decide whether to reach a Finding of No Significant Impact, direct further work on the EA, or initiate the preparation of an Environmental Impact Statement.

The public comment period for the EA will be open for 15 days, beginning on December 20, 2024 and ending on January 6, 2025. For additional information, please contact Harilene Yazzie, Division of Environmental & Cultural Resources Management, Bureau of Indian Affairs - Alaska Region, at 907.271.4004, or by email at Alaska_NEPA@bia.gov. Written comments may be emailed to Alaska_NEPA@bia.gov or mailed or hand-delivered to the following address:

Jolene John, Regional Director
Bureau of Indian Affairs - Alaska Region
3601 C Street, Suite 1200
Anchorage, AK 99503

Due to a pending federal government shutdown, written comments on the EA are strongly encouraged to be submitted electronically or by mail. If there is a lapse in federal appropriations the comment period will continue to run but the Bureau of Indian Affairs, Alaska Region will not be able to accept hand delivered comments.

The EA is available for public review online at <http://www.EklutnaEA.com> and in hard copy at the Chugiak-Eagle River Library located in the Eagle River Town Center at 12001 Business Blvd. 176, Eagle River, AK 99577, telephone number 907.343.1530.

Pub: Dec. 22, 2024

**NOTICE OF PUBLIC REVIEW PERIOD EXTENSION
FOR THE EKLUTNA NATIVE VILLAGE GAMING FACILITY PROJECT
ENVIRONMENTAL ASSESSMENT**

Notice is hereby given that the Bureau of Indian Affairs (BIA), Department of the Interior, has extended the public review period for the December 2024 Environmental Assessment (EA) (DOI-BIA-AK-61628) prepared for the Eklutna Native Village Gaming Facility Project. The public comment period for the EA is being extended through **Thursday, January 9, 2025**.

The EA has been completed in accordance with requirements set forth in the National Environmental Policy Act (NEPA) and assesses the environmental impacts that could result from the development and operation of a class II gaming facility by the Eklutna Native Village (Tribe) on an approximately 6.37-acre portion of a restricted fee Native Allotment (Project Site) owned by members of the Tribe in the unincorporated community of Chugiak within the boundaries of the Municipality of Anchorage, Alaska.

The BIA serves as the Lead Agency for NEPA compliance. For additional information, please contact Harrilene Yazzie, Division of Environmental & Cultural Resources Management, Bureau of Indian Affairs - Alaska Region, at 907.271.4004, or by email at Alaska_NEPA@bia.gov. Written comments may be emailed to Alaska_NEPA@bia.gov or mailed or hand-delivered to the following address:

Jolene John, Regional Director
Bureau of Indian Affairs - Alaska Region
3601 C Street, Suite 1200
Anchorage, AK 99503

The EA is available for public review online at <http://www.EklutnaEA.com> and in hard copy at the Chugiak-Eagle River Library located in the Eagle River Town Center at 12001 Business Blvd. 176, Eagle River, AK 99577.

Order Number: W0049843
Order Status: Saved
Classification: Legals & Public Notices
Package: Legals ADN
Final Cost: \$356.70
Payment Type:
User ID: W0016709

ADDITIONAL OPTIONS

Affidavit Charge - Digital \$5

SCHEDULE FOR AD NUMBER W0023609

December 22, 2024
Anchorage Daily News
Legals

NOTICE OF AVAILABILITY ENVIRONMENTAL ASSESSMENT FOR THE EKLUTNA NATIVE VILLAGE GAMING FACILITY PROJECT

Notice is hereby given that the Bureau of Indian Affairs (BIA), Department of the Interior, has released an Environmental Assessment (EA) (DOI-BIA-AK-61628) dated December 2024 for the development of a class II gaming facility by the Eklutna Native Village (Tribe) on an approximately 6.37-acre portion of a restricted fee Native Allotment (Project Site) owned by members of the Tribe in the unincorporated community of Chugiak within the boundaries of the Municipality of Anchorage, Alaska (Proposed Project). The EA has been completed in accordance with requirements set forth in the National Environmental Policy Act (NEPA; 42 United States Code [USC] §4321 et seq.), the 2024 Council on Environmental Quality regulations for implementing NEPA (40 CFR § 1500 et seq.), the Department of the Interior's Procedures for the Implementation of NEPA (43 CFR Part 46), and the BIA NEPA Handbook (59 Indian Affairs Manual 3-H), and assesses the environmental impacts that could result from the BIA's approval of a business lease between the Native Allotment landowners and the Eklutna Native Village tribal government for the development and operation of a class II gaming facility on the Project Site (Proposed Action).

The Project Site consists of a restricted fee Native Allotment comprised of three parcels located in the unincorporated community of Chugiak within the boundaries of the Municipality of Anchorage, Alaska. The Project Site previously contained a residence and several outbuildings but currently consists of vacant partially wooded land. Surrounding land uses include low density residential, industrial, the Alaska Railroad, and the Birchwood Airport.

The BIA serves as the Lead Agency for NEPA compliance. Based on the analysis and impacts discussed in the EA and comments received during the public review period, the BIA will decide whether to reach a Finding of No Significant Impact, direct further work on the EA, or initiate the preparation of an Environmental Impact Statement.

The public comment period for the EA will be open for 15 days, beginning on December 20, 2024 and ending on January 6, 2025. For additional information, please contact Harilene Yazzie, Division of Environmental & Cultural Resources Management, Bureau of Indian Affairs - Alaska Region, at 907.271.4004, or by email at Alaska_NEPA@bia.gov. Written comments may be emailed to Alaska_NEPA@bia.gov or mailed or hand-delivered to the following address:

Jolene John, Regional Director
Bureau of Indian Affairs - Alaska Region
3601 C Street, Suite 1200
Anchorage, AK 99503

Due to a pending federal government shutdown, written comments on the EA are strongly encouraged to be submitted electronically or by mail. If there is a lapse in federal appropriations the comment period will continue to run but the Bureau of Indian Affairs, Alaska Region will not be able to accept hand delivered comments.

The EA is available for public review online at <http://www.EklutnaEA.com> and in hard copy at the Chugiak-Eagle River Library located in the Eagle River Town Center at 12001 Business Blvd. 176, Eagle River, AK 99577, telephone number 907.343.1530.

Pub: Dec. 22, 2024

Appendix O

Responses to Comments

Responses to Comments on the December 2024 Environmental Assessment Eklutna Native Village Gaming Facility Project

1.0 INTRODUCTION AND COMMENT LETTERS

An Environmental Assessment (EA) was prepared pursuant to the National Environmental Policy Act (NEPA) to assess potential environmental impacts of the development of a class II gaming facility by the Eklutna Native Village (Tribe), a federally recognized Indian Tribe of the United States, on a 6.37-acre portion of a restricted fee Native Allotment (Project Site) owned by members of the Tribe in the unincorporated community of Chugiak within the boundaries of the Municipality of Anchorage, Alaska (Proposed Project). The Proposed Project requires approval from the Bureau of Indian Affairs (BIA) of a business lease between the Native Allotment owners and the Tribe for the development and operation of a gaming facility on the Project Site (Proposed Action). The EA also evaluated a reasonable range of alternatives to the Proposed Project (Alternative A), including an approximately 31,000 square-foot event center (Alternative B) and no action (Alternative C). A Notice of Availability (NOA) for the EA was published in the Anchorage Daily News online and in print on December 22, 2024. The NOA announced that the EA was available for public and agency review for 15 days ending on January 6, 2025. The NOA and EA were also posted on the project website at www.EklutnaEA.com. In response to comments received, the public review period was extended by the BIA through January 9, 2025. A notice of the extension was published in the Anchorage Daily News online and in print on January 5, 2025, as well as on the project website (www.EklutnaEA.com).

The BIA received 84 comment letters on the EA during the public review period (**Table 1** and **Attachment 1**). Substantive comments received on the EA are responded to within the sections below. Once an issue has been addressed in a response to a comment, subsequent responses to similar comments reference the initial response.

TABLE 1: COMMENT LETTER LOG

Log #	Name	Affiliation	Date Received
Agency and Organization Letters			
A1	Kim Reitmeier, President	ANCSA Regional Association	1/3/2025
A2	Suzanne LaFrance, Mayor	Municipality of Anchorage	1/3/2025
A3	Christopher Constant, Chair	Municipality of Anchorage	1/5/2025
A4	Darryl Parks, President	Chugiak Community Council	1/6/2025
A5	Jennifer Wing, Manager	Alaska Department of Fish and Game	1/9/2025
A6	Melanie Arnolds, Central Region Right-of-Way Chief	Alaska Department of Transportation and Public Facilities	1/9/2025
A7	Sarah Lukin, President	Cook Inlet Region, Inc.	1/9/2025
Tribal Letters			
T1	Aaron Legget, President	Eklutna Native Village	1/9/2025

Log #	Name	Affiliation	Date Received
Individual Letters			
I1	Angie Hamill		12/26/2024
I2	Barbara Crittenden		12/26/2024
I3	Judy Moore		12/26/2024
I4	Marilyn Sarvela		12/26/2024
I5	Phillip Flippo		12/26/2024
I6	Sean Barnett		12/26/2024
I7	James Crewdson		12/27/2024
I8	Karen Perry		12/27/2024
I9	Kathie and Calvin Steele		12/27/2024
I10	Tom Desalvo		12/27/2024
I11	Vicki Herman		12/27/2024
I12	Debbie Ossiander		12/30/2024
I13	Elizabeth Conley		12/30/2024
I14	Janice Norman		12/30/2024
I15	Matt Cruickshank		12/30/2024
I16	Martin Stofer		12/31/2024
I17	Savannah Haines		12/31/2024
I18	Valerie Jokela		12/31/2024
I19	Brian Holl		1/2/2025
I20	Dolly Caswell		12/31/2024
I21	Debbie Ossiander		1/2/2025
I22	Jeff Schlies		1/2/2025
I23	Marilyn Sarvela		1/2/2025
I24	Kelly Merrick		1/3/2025
I25	Liz Morgan		1/3/2025
I26	Palma Ingles		1/3/2025
I27	Jean Moore		1/4/2025
I28	Valerie Raatz		1/3/2025
I29	Sabrina France		1/3/2025
I30	Al and Jackie Clemens		1/4/2025
I31	Amber Mock		1/4/2025
I32	Andrew France		1/4/2025
I33	Faith Brock		1/4/2025
I34	Renee Scott		1/4/2025
I35	Tamera Schlies		1/4/2025
I36	Bill Sarvela		1/5/2025
I37	Bree Yankus		1/5/2025
I38	Chris Johnson		1/5/2025
I39	James Nugen		1/5/2025

Log #	Name	Affiliation	Date Received
I40	Lee Waters		1/5/2025
I41	Tom P.		1/5/2025
I42	Veronica and Kalani Wakinekona		1/5/2025
I43	Vicki Prunty		1/5/2025
I44	Veronica Allmaras		1/6/2025
I45	Jacob Sears		1/6/2025
I46	Glen Yankus		1/6/2025
I47	Darryl Parks		1/6/2025
I48	Mary Pemberton		1/7/2025
I49	Tom Desalvo		1/7/2025
I50	Pamela Carlson		1/7/2025
I51	Russel Carlson		1/7/2025
I52	Todd Russell		1/7/2025
I53	Karen Hill		1/8/2025
I54	Julie Jorlett		1/8/2025
I55	Melissa Crandall		1/8/2025
I56	Steven Ellis		1/8/2025
I57	Tony LoMedico		1/8/2025
I58	Kerri Jette		1/8/2025
I59	Judi Miller		1/9/2025
I60	Tiffani Loughman		1/9/2025
I61	Carol Perkins		1/9/2025
I62	Marc Denslinger		1/9/2025
I63	Bryce Palmer		1/9/2025
I64	Cecil Shuman		1/9/2025
I65	Jason Wells		1/9/2025
I66	John Abrams		1/9/2025
I67	Patty Friend		1/9/2025
I68	Sarah Belway		1/9/2025
I69	Val Jokela		1/9/2025
I70	Nicole Denslinger		1/9/2025
I71	Alan Kurczynski		1/9/2025
I72	Cecil and Catherine Shuman		1/9/2025
I73	Julee Faso-Formoso		1/9/2025
I74	Kimberly Collins		1/9/2025
I75	Letta Stokes		1/9/2025
I76	Douglas Johnson		1/9/2025

2.0 MASTER RESPONSES TO COMMENTS

Master responses in this section address comments with similar subject matter that were submitted multiple times in separate comments. Responses to separate comments may refer to these master responses as a whole or in part to avoid repetition.

Master Response 1: Expressions of Opinion and Non-Substantive Comments

Summary of Comments

Some comments received were expressions of opinion either for or against the Proposed Action or Proposed Project. Other comments summarized the alternatives and/or findings of the EA or discussed general environmental or gaming eligibility concerns.

Response

This document has been prepared consistent with NEPA (42 USC § 4321 et seq.); the Department of the Interior’s Procedures for the Implementation of NEPA (43 CFR Part 46); the 2024 CEQ regulations for implementing NEPA (40 CFR Parts 1500-1508)¹; and the BIA NEPA Handbook (59 Indian Affairs Manual 3-H). To improve efficiency and effectiveness of EAs, agencies may apply the other provisions of 40 CFR Part 1503, including 40 CFR Part 1503.4, to EAs (40 CFR Part 1501.5(k)). As set forth in 40 CFR §1503.4(a), the lead agency “shall consider *substantive comments* [emphasis added] timely submitted during the public review period. The agency shall respond to individual comments or groups of comments.” Comments are generally considered “substantive” if they: 1) relate to inadequacies or inaccuracies in the analysis or methodologies used; 2) identify new impacts or recommend reasonable new alternatives or mitigation measures; 3) involve substantive disagreements on interpretations of significance and scientific or technical conclusions. According to 40 CFR §1503.4(a)(5), the lead agency may respond to certain comments received during the public review period for an EA by “[e]xplaining why the comments do not warrant further agency response, recognizing that agencies are not required to respond to each comment.” Consistent with this requirement, this Master Response explains that responses are not required for comments that do not raise a substantive environmental issue or are general statements and expressions of opinion that are not explained with supporting data, sources, or methodologies. Additionally, responses are not provided to comments that do not further the purpose of the NEPA process “to help public officials make decisions that are based on an understanding of environmental consequences, and take actions that protect, restore, and enhance the environment,” (40 CFR §1500.1(c)). However, all comments submitted to the BIA during the NEPA process, including expressions of opinion, have been included within the administrative record and thus will be considered by the BIA in its decision on the Proposed Action. Additionally, comments regarding gaming eligibility under IGRA do not raise substantive environmental issues; therefore, no response is required.

¹ BIA is aware of the November 12, 2024 decision in *Marin Audubon Society v. Federal Aviation Administration*, No. 23-1067 (D.C. Cir. Nov. 12, 2024). To the extent that a court may conclude that the CEQ regulations implementing NEPA are not judicially enforceable or binding on this agency action, BIA has nonetheless elected to follow those regulations at 40 C.F.R. Parts 1500 – 1508, in addition to the Department’s procedures/regulations implementing NEPA (43 C.F.R. Part 46), and the BIA NEPA Guidebook (59 Indian Affairs Manual 3-H) to meet its obligations under NEPA, 42 U.S.C. §§ 4321 et seq.

Master Response 2: Extension of the Public Review Period

Summary of Comments

The BIA received several comments requesting an extension of the public review period on the EA.

Response

The Department of the Interior’s (Department) NEPA regulations (43 CFR Part 46) and CEQ Guidelines for Implementing NEPA (40 CFR Parts 1500–1508) encourage agencies to facilitate public involvement in the NEPA process (40 CFR 1500.2(d); 40 CFR 1501.9), but the extent of public involvement in preparing an EA is at the discretion of the decision-maker (43 CFR 46.305(a)).

As set forth in 43 CFR 46.305(b), publication of a “draft” environmental assessment is not required; however, bureaus may seek comments on an EA if they determine it to be appropriate and may revise environmental assessments based on comments received without need of initiating another comment period. Additionally, as stated in 43 CFR 46.305(c), the Lead Agency must notify the public of the availability of an EA once it has been completed. In this case, the BIA issued an NOA for the EA which announced a comment period beginning on December 20, 2024 and ending on January 6, 2025. The NOA and EA were posted on the project website at www.EklutnaEA.com on Friday December 20, 2022, and the NOA was published in the Anchorage Daily News online and in print on December 22, 2024. Printed copies of the EA were made available at the Chugiak Eagle-River Library on January 2, 2025². In response to comments received, the public review period was extended by the BIA through January 9, 2025. A notice of the extension was published in the Anchorage Daily News online and in print on January 5, 2025, as well as on the project website (www.EklutnaEA.com). The resulting total comment period was 20 days, and the BIA received a total of approximately 84 comment letters from agencies and the public. Following consideration of the comments received, clarifications have been made to the Final EA; however, in summary, the comments received by the BIA following publication of the NOA for the EA did not reveal substantial new circumstances or information about the significance of adverse effects that bear on the analysis. Based on the substance of the letters and the absence of significant environmental effects as explained in the Final EA, the BIA determined that this comment period was consistent with NEPA and sufficient for providing meaningful comments on the Proposed Action. All comments received by the BIA regarding the Proposed Action have been included within the administrative record and thus will be considered by the BIA in its decision on the Proposed Action.

Master Response 3: Preparation of an Environmental Impact Statement and Completeness of the EA

Summary of Comments

Several comments were received that assert that there are significant adverse impacts associated with the project alternatives that are either not identified in the EA or not adequately mitigated below the threshold of significance and, therefore, an Environmental Impact Statement (EIS) should be prepared.

² Via a clerical error, a printed copy was initially delivered to the Anchorage Public Library on December 23, 2024, that was later transferred to the Chugiak Eagle-River Library. A second printed copy was also delivered to the Chugiak Eagle-River Library. Both copies were made available for public review at the Chugiak Eagle-River Library on Thursday January 2, 2025.

Response

The NEPA “hard look” doctrine is a principle of administrative law that requires that agencies take a hard look at the environmental consequences of proposed federal actions, based on consideration of all relevant evidence, and that decisions are supported by adequate facts. To determine the appropriate level of review, CEQ regulations (e.g., 40 CFR § 1501.3(d)) permit agencies to make significance determinations based on context and intensity. The BIA's decision to prepare an EA based on the significance determinations in the analysis aligns with these standards.

As stated in the EA Section 1.1, the EA was completed in accordance with and to satisfy the requirements set forth in NEPA (42 USC § 4321 et seq.); the Department of the Interior’s Procedures for the Implementation of NEPA (43 CFR Part 46); the 2024 CEQ regulations for implementing NEPA (40 CFR Parts 1500-1508); and the BIA NEPA Handbook (59 Indian Affairs Manual 3-H). In accordance with 43 CFR 46.310 and 40 CFR §1501.5(c)(2), Section 3 of the EA provides an analysis of the potential environmental consequences associated with the Proposed Action and alternatives. Pursuant to 40 CFR §1501.5(c)(1), this information is intended to assist the BIA in determining whether a Finding of No Significant Impact (FONSI) should be prepared or whether additional environmental analysis should be conducted in the form of an EIS.

While the text of the EA was limited to less than 75 pages in compliance with NEPA requirements (40 CFR §1501.5(g)), consistent with the NEPA “hard look” standard, the determinations and mitigation recommendations described therein were informed by extensive research and studies prepared by qualified experts either cited as appropriate or provided within the technical appendices of the EA. Supporting technical appendices included but were not limited to: biological resource studies prepared by qualified biologists and wetland specialists; cultural resource study prepared by registered professional archaeologists that meet Secretary of Interior standards; economic impact analysis prepared by gaming economic specialists; a transportation impact study prepared by a traffic engineering firm; a water supply and wastewater feasibility study and grading and stormwater plan prepared by a qualified civil engineering firm; and air quality modeling completed using USEPA emission factors.

Some comments on the EA made constructive suggestions regarding instances in which the text or the analysis could be clarified or changed to include additional details. Consistent with the NEPA “hard look” doctrine, the BIA has considered all relevant information submitted by the public and agencies during the EA review period, and the text in the Final EA and associated technical studies have been clarified in several instances to incorporate such information as appropriate and as explained in more detail throughout this document. As detailed in Section 3 of the Final EA, all impacts associated with the Proposed Action were determined to be less than significant or less than significant with mitigation. Therefore, the BIA has determined that additional analysis in the form of an EIS is not warranted and a Mitigated FONSI should be prepared in compliance with NEPA.

Master Response 4: Range of Alternatives and Site Selection

Summary of Comments

Several comments were received concerning the range of alternatives analyzed in the EA. Some of these comments suggested that development of the Proposed Project on an alternative site should have been included in the EA.

Response

The EA addressed a reasonable range of alternatives to the Proposed Action consistent with NEPA and applicable regulations. Under NEPA Section 102(c)(iii), a “reasonable range of alternatives” includes options “that are technically and economically feasible and meet the purpose and need of the proposal”. An EA must also evaluate a “no action” alternative. The primary purpose of an alternatives analysis is to inform reasoned decision-making and public participation. Consistent with that purpose, an EA must consider enough alternatives to permit a reasonable choice but need not present in-depth analysis of every conceivable option. Section 6.4.4 of the BIA NEPA guidebook states that alternatives considered within an EA “can be described and eliminated [in the Alternatives section], with reasons given for not considering them further.”

Alternatives are described in Section 2 of the EA and include the Proposed Project (Alternative A; a small interim class II gaming facility thence a permanent class II gaming facility), an Event Center (Alternative B), and the No Action alternative (Alternative C). The DOI and CEQ regulations do not mandate specific alternatives that should be assessed in EAs. Rather, Section 102(H) of NEPA states that agencies should “...study, develop, and describe appropriate alternatives to recommended courses of action in any proposal which involves unresolved conflicts concerning alternative uses of available resources.” The BIA evaluated a total of three project alternatives in the EA and eliminated three other alternatives (alternative configurations, a reduced intensity development, and alternative locations) from detailed consideration as either being infeasible, not providing environmental advantages, or not fulfilling the purpose and need (refer to Section 2.5 of the EA). The BIA, accordingly, evaluated a reasonable range of alternatives in the EA – alternatives that are technically and economically feasible and to varying degrees meet the purpose and need for the proposed action.

As discussed in Section 2.5 of the EA, an alternative location for the proposed gaming facility is not feasible under current federal regulations. The Tribe has no other lands eligible for gaming. Approximately 48.3 acres contiguous to the Project Site are owned by Eklutna Inc. in fee simple, and additional acreage owned by Eklutna Inc. occurs within the vicinity of Anchorage. However, this land is not owned by the Tribal government, nor is it in federal trust or restricted fee status. The Project Site is owned as a restricted fee Native Allotment by members of the Tribe/Native Allotment Owners. This is the only Native Allotment land that is held in restricted fee status by the Eklutna tribal members that is within close proximity to the Eklutna Native Village.

The Department issued an Indian Lands Opinion on June 27, 2024, confirming that the Project Site constitutes Indian lands eligible for gaming by the Tribe under Indian Gaming Regulatory Act(IGRA) The National Indian Gaming Commission (NIGC) incorporated the Department’s Indian Lands Opinion approved and the Tribe’s site-specific Gaming Ordinance authorizing gaming activities on the Project Site

Consideration of a highly speculative circumstance under which legislation may be passed or federal regulations may be amended to allow gaming activities on an alternative site owned by the Tribe would not aid in expanding the range of alternatives in a manner that promotes informed decision-making. Therefore, consideration of an alternative site was not analyzed as it would not meet the definition of a reasonable alternative that is feasible from an economic and technical standpoint and thus would not accomplish the purpose and need for the Proposed Action. Therefore, alternative locations for the proposed gaming facility are not evaluated within the EA.

Master Response 5: Potential Impacts to Peters Creek and Fish

Summary of Comments

Several comments raised concerns regarding the potential impacts to Peters Creek and associated fish.

Response

An analysis of impacts to Peters Creek and anadromous fish habitat was included in the EA in Section 3.3.3 and 3.5.3. These sections considered indirect impacts to Peters Creek, including pollution from impaired runoff and use of an on-site wastewater treatment system. As described in Section 3.3.2 of the EA, Peters Creek is considered by the Alaska Department of Fish and Game (ADF&G) as an anadromous stream with known rearing of Coho salmon and presence of Chinook (King) salmon and pink salmon. Section 3.5.3 of the EA acknowledges that anadromous fish species, including salmonids, are present within Peters Creek, but that federally listed threatened or endangered fish species are unlikely to occur within Peters Creek. As detailed in Section 3.5.3 and Appendix G of the EA, this is due to the fact that fish species listed under the Federal Endangered Species Act do not occur within Alaska's fresh waters. Additionally, it is noted that Peters Creek is not designated or proposed critical habitat. As stated in Section 3.5 of the EA, the nearest critical habitat in relation to the Project Site is 0.6 miles downstream of the Project Site in the Knik Arm. Further, a Biological Assessment (BA) was prepared for the Proposed Project to facilitate consultation with the National Marine Fisheries Service (NMFS) regarding potential impacts to federally listed species under NMFS jurisdiction, critical habitat designated or proposed by NMFS, and Essential Fish Habitat (EFH). As discussed in Section 5 of the EA, NMFS determined that the Proposed Project would have no effect on federally listed species, critical habitat, EFH, or to Peters Creek. It is noted that, during consultation with NMFS, it was NMFS opinion that the Peters Creek be considered EFH and that there would be no impacts to this EFH. The Final EA has been revised to include information provided by NMFS during the consultation process.

As discussed in Section 3.5.3 of the EA, construction impacts would be less than significant with adherence to a site-specific Stormwater Pollution Prevention Plan and associated BMPs. Operational impacts were found to be less-than-significant through a stormwater collection and treatment system that would be sized to accommodate a 100-year 24-hour flood event and would ensure that untreated runoff from the built environment would not enter Peters Creek and that runoff rates would not exceed pre-development conditions. Additionally, the septic system will be registered with the USEPA under the Underground Injection Control program as a Class V injection well and designed and installed consistent with the ADEC standards (ADEC 18 AAC 72 Wastewater Disposal, 18 AAC 72.530(e)(2)) (Appendix C of the EA). Therefore, with proper design and installation of the on-site wastewater system consistent with federal, tribal, and local standards and requirements, significant impacts to water quality in Peters Creek from treatment and discharge of wastewater would not occur.

Additional comments were raised regarding setbacks to Peters Creek. The Municipality of Anchorage Official Web Site Anchorage Municipal Code (AMC 21.45.210) currently identifies a 25-foot stream protection setback. However, several comments were received that the Municipality of Anchorage Official Web Site is outdated and that a 50-foot setback was identified for Peters Creek in an Anchorage Assembly Ordinance (AO No. 2018-67(S)). While state and local regulations would not apply to Native allotment land, the EA identified consistency with AMC 21.45.210 as a best management practice (BMP) to ensure that Peters Creek would be afforded a buffer consistent with the setback identified by the state protective of this resource.

As a specific buffer of 50 feet for Peters Creek was identified in AO No. 2018-67(S), the Proposed Project has been revised to provide a 50-foot setback for Peters Creek, and this has been incorporated into Table 2.1-1 of the Final EA as a BMP. Therefore, the Final EA has been revised to include an updated site plan, and the text has been revised to specify a 50-foot setback. The increased setback would also fully remove development activities from the Peters Creek 500-year floodplain.

Lastly, comments were raised regarding Alaska Department of Fish and Game (ADF&G) oversight and fishing within Peters Creek. As discussed within the Regulatory Appendix of the EA (Appendix E), state and local regulations generally do not apply on Native allotment land. However, no fishing activities are proposed as part of the development components and the Proposed Project would not introduce new locations for fishing, especially since a steep drop off occurs between the Project Site and Peters Creek. A comment letter on the EA was received from ADF&G (refer to comment letter A5). Applicable ADF&G comments were addressed in the Final EA, including the addition of several BMPs associated with Peters Creek and bear-proofing measures.

Master Response 6: Potential Impacts Associated with Traffic, Traffic Hazards, and Parking

Summary of Comments

Several comments raised concerns regarding the potential impacts associated with parking, traffic, and traffic hazards.

Response

Refer to Section 3.8 of the EA regarding potential impacts associated with transportation and circulation, including both motorized (i.e., vehicles) and non-motorized (i.e., bicyclists and pedestrians) transportation, and parking. Additional technical detail was provided in the Eklutna Native Village Casino Traffic Impact Analysis Report (TIA); Appendix A of the EA.

Roadway Operations

While the EA acknowledged that traffic would increase on local roadways serving the Project Site (i.e., the segment of Birchwood Loop Road/Birchwood Spur Road from Alaska Route 1/Glenn Highway to the Project Site) as a result of the Proposed Project, it found that such increases would not cause operating standards to be exceeded (Appendix A of the EA). Level of Service (LOS) is a qualitative measure reflecting the traffic operation of the intersection, with LOS A representing best performance and LOS F the worst. LOS describes the traffic conditions in terms of such factors as speed, travel time, delays, freedom to maneuver, traffic interruptions, comfort, convenience, and safety. Operating standards of Level of Service (LOS) E or F are considered unacceptable for the study roadways in accordance with industry standard design objectives. The EA found that all study locations would operate at LOS D or better during all three evaluated peak hours (weekday AM and PM peak hour, Saturday peak hour) with implementation of the Proposed Project. It should be noted that the traffic analysis use of ITE (Institute of Transportation Engineers) trip generation rates likely results in an overestimate of trips, as based on the average number of patrons predicted by the socioeconomic study (2,235 patrons each day) prepared for the Proposed Project (Appendix D of the EA), an average of 4,926 one-way trips per day would be reasonably foreseeable, compared to 8,010 weekday and 10,550 Saturday trips per day from ITE.

Additionally, the EA found that projected vehicle queue lengths for vehicles turning on and off study roadways would be accommodated by existing turn lanes and would not back-up at study locations without designated turn lanes. Based on the above, the EA concluded that impacts due to increased project traffic were less than significant.

Roadway Safety

While the BIA acknowledges the safety concerns raised by some commenters with respect to roadway width, curves and elevation changes, narrow shoulders in some locations, and lack of designated bicycle or pedestrian facilities on Birchwood Loop Road/Birchwood Spur Road, the Proposed Project would not make any physical changes to these existing roadway conditions with the exception of the immediate driveway access to the Project Site. While the Proposed Project would add traffic to these roadways, added traffic on its own does not constitute an environmental impact under NEPA unless it exceeds the design volume to capacity ratio for the roadway resulting in a roadway exceeding an LOS operating standard (see discussion in paragraph above). With respect to the Project Site driveway access, Section 3.8.3 of the EA states:

...circulation requirements per the International Fire Code (IFC) would be complied with regarding final site access and circulation design of the site. Additionally, driveway improvements within the 50-foot dedicated right-of-way along the southern boundary of the site, and within the right-of-way of Birchwood Spur Road would be subject to permitting and approvals from the DOT&PF. Compliance with these requirements would ensure that site access impacts would be less than significant.

Any required Alaska Department of Transportation & Public Facilities (DOT&PF) approvals related to Project Site access on Birchwood Spur Road would be governed by Title 17, Chapter 10, Article 2 of the Alaska Administrative Code, which pertains to Driveway and Approach Road Permits. The Tribe will work with the Alaska DOT&PF to obtain necessary access permits, address its comments, if any, on the TIA provided in Appendix A of the EA, and fulfill any conditions of applicable access approvals, which may include negotiating pedestrian improvements necessary to accommodate bicycle and pedestrian traffic. This process will ensure that driveway access to the Project Site is safe for all travel modes and that the design complies with all relevant Alaska DOT&PF standards.

Although the Proposed Project would not result in significant traffic-related impacts (Section 3.8 of the EA), the following BMP has been adopted by the Tribe and added to Table 2.1-1 of the Final EA to address concerns regarding safety of pedestrians and bicyclists on Birchwood Loop Road/Birchwood Spur Road:

- The Tribe will coordinate with the Alaska DOT&PF to evaluate whether certain high-activity pedestrian locations, including bus stops, along Birchwood Loop Road/Birchwood Spur Road from Glenn Highway to the Project Site would benefit from implementation of additional measures regarding pedestrian safety, including but not limited to: signage, flashing beacons, and painted crosswalks. If targeted improvement projects are identified through this coordinated effort, the Tribe will make a fair-share contribution towards the cost of said improvements, as determined necessary.

Roadway Maintenance

Regarding concerns about additional wear-and-tear on local public roadways used to access the Project Site, namely Birchwood Loop Road/Birchwood Spur Road, vehicle types associated with construction and operation of the Proposed Project would consist primarily of passenger vehicles and small delivery and haul trucks; overweight vehicles are not anticipated to be needed for project operations, and are anticipated to be utilized in a limited capacity during construction. These public roadways are designed for regular use by passenger vehicles and occasional haul and oversized trucks, and as such, wear-and-tear would be similar to existing conditions. Roadway maintenance is generally funded through the State's gas tax, which is paid for by all (i.e., Tribal and non-Tribal) consumers who purchase gas at gas stations. Birchwood Loop Road/Birchwood Spur Road is maintained by the State (Alaska DOT&PF).

Birchwood Loop Road/Birchwood Spur Road is included on the Tribe's official inventoried routes under the Indian Reservation Roads Program.³ Although the Proposed Project would not result in significant project-related impacts to Birchwood Loop Road/Birchwood Spur Road, the following BMP has been added to Table 2.1-1 of the Final EA to address concerns regarding maintenance needs on Birchwood Loop Road/Birchwood Spur Road:

- As part of the Tribe's road inventory for the Indian Reservation Roads Program, the Tribe will coordinate with the Alaska DOT&PF to discuss potential federally-available funding for, and to make fair-share contribution towards, planning, designing, construction, and maintenance activities on Birchwood Loop Road/Birchwood Spur Road, if determined necessary during coordination.

School Bus Activity

Regarding concerns with respect to delays on Birchwood Loop Road and Birchwood Spur Road due to school buses stopping to pick-up students in the morning and drop-off students in the afternoon, the times of day/week when school bus activity would peak would be times when traffic generated by the Proposed Project would be relatively low (see Section 3.8.3 Impacts, Operation Traffic, of the Final EA). While some employees would be expected to travel to/from the Project Site during the early mornings for shift changes, the number of vehicle trips that would be added to the study roadways from employee trips would be approximately 30 (assuming three work shifts per day for up to 90 staff employed daily under Alternative A). Visitor traffic during this time of day would be very low. During the early afternoon during peak school bus drop-off, there would likely be very little vehicle activity attributable to employees due to typical shift times. Visitor activity would be higher than in the early morning but would still be relatively low considering that peak gaming activity would typically occur in the evenings (after typical work hours) and on weekends. For these reasons, vehicle travel on Birchwood Loop Road/Birchwood Spur Road associated with the Proposed Project is not expected to substantially affect the safety or operation of school bus pick-up or drop-off conditions relative to existing conditions. Further, as stated above in the Roadway Safety section of this master response, pedestrian safety improvements may be addressed through the Alaska DOT&PF access permit process and a BMP addressing pedestrian safety, which would apply to students loading and unloading from school buses at bus stop locations, has been added to Table 2.1-1 of the Final EA.

³ <https://www.bia.gov/bia/ois/division-transportation/operations>

Parking

With respect to concerns about the potential for parking overflow, Alternative A would include 443 paved surface parking spaces to accommodate employees and patrons. Page 40 of the TIA (EA Appendix A) states “...a recreational parking generation rate would require 348 parking spaces based on the gross floor area.” The proposed number of parking spaces on the Project Site is well above the industry-standard calculation for the size and type of facility proposed. As of 2023, Title 21 of the AMC no longer requires developments to provide a minimum number of parking spaces.⁵ However, under previous Title 21 requirements (AMC 21.45.080.M), one parking space would have been required for every 300 square feet of gross building area, which would be approximately 194 parking spaces for the Proposed Project. The Proposed Project includes 443 on-site parking spaces, which far exceeds this previous requirement. Based on the above, no overflow parking is expected to occur on Birchwood Spur Road nor on any other nearby roadways.

Master Response 7: Public Safety, Public Services Concerns, and Taxes

Summary of Comments

Several comments raised concerns regarding potential impacts associated with public safety, law enforcement services, fire protection services, and tax-exemptions on the Project Site as restricted fee Native Allotment land that would otherwise be utilized to compensate for increased law enforcement and fire protection calls.

Response

Public Services and Safety

Refer to Sections 3.7 and 3.10 of the EA regarding potential impacts associated with crime, public safety and public services, including law enforcement and fire protection services. The EA recognized that an increase in crime and emergency services is expected with the development of the Project Site. The implementation of the Proposed Project is anticipated to bring in a higher number of patrons and employees daily, leading to a likely rise in criminal and emergency incidents consistent with the expectations for any commercial development. As outlined in EA Section 3.7.3 and Appendix A, gaming itself does not inherently result in unusually high crime levels. Criminal incidents at casinos and similar gaming venues are comparable to those observed in other commercial venues of a similar size. For additional details on the anticipated increase in crime and types of crimes, refer to EA Section 3.7 and Appendix D (specifically refer to Appendix B of Appendix D), including the projected rise in service calls to local law enforcement. For additional details on the anticipated increase in emergency services, refer to EA Section 3.7 and Appendix A, including the projected rise in service calls.

Letters of support for the Proposed Project from the Municipality of Anchorage are included in comment letters A2 and A3. The Tribe has entered into an existing agreement with the Anchorage Police Department (APD). The APD assists with issues regarding public safety and trespassing on the Tribe’s land, which includes the Project Site. The Chugiak Volunteer Fire and Rescue Company (CVFRD) provides fire protection services to the Project Site. Mitigation in Section 4 of the EA for public services would be implemented to ensure that the Municipality of Anchorage, APD, and CVFRD are consulted with appropriately and that a procedure is established for collaboration and communication regarding potential project-related impacts on service calls.

This mitigation has been revised in the Final EA to state that should the Tribe enter into agreements whereby the APD and CVFRD provide services to the project, that such agreements would address appropriate compensation with the appropriate entities. Further, mitigation in Section 4 of the EA also states that if the Tribe does not enter into such agreements, the Tribe would establish, equip, and staff its own tribal law enforcement department and fire protection department to serve the Project Site, consistent with requirements of the Final Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance (Appendix M of the EA).

Additionally, BMPs in Table 2.2-1 of the EA have been incorporated into the project design to enhance security on the Project Site during operation. These measures include the installation of security cameras and the implementation of surveillance of the Project Site by tribal security personnel. BMPs are also included to reduce potential fire risks.

Taxes and Fiscal Impacts

As described in EA Section 3.7.2, the three parcels that comprise the Project Site are part of a restricted fee Native Allotment and thus are exempt from property taxes. The Project Site would continue to be exempt from property taxes under the proposed alternatives. In addition to this exemption and as described in EA Section 3.7.3, the Tribe would be exempt from certain other taxes under the development alternatives. However, as described above, mitigation in Section 4 would require the Tribe to enter into more detailed agreements with APD and CVFRD governing the provision of law enforcement, fire and emergency medical services. These agreements would include compensation payments to offset increased APD and CVFRD costs of providing services. Furthermore, as described in EA Section 3.7.3, operation of Alternative A is expected to create an estimated 419 permanent jobs (Table 3.7-7) and generate an estimated \$8.3 million in Federal, State and local taxes annually (Table 3.7-10). This includes indirect and induced economic activities, which would create approximately 190 jobs and fund estimated annual tax revenues of \$416,500 to local governments. Because indirect and induced activities would occur at non-Tribal businesses, they would not be eligible for Tribal tax exemptions.

Potential fiscal impacts of the Proposed Project are described in EA Section 3.7. (Tables 3.7-9 and 3.7-10) and Appendix D. The tax modelling results listed in Appendix D of the EA are based on the IMPLAN model, which was run for the Anchorage Municipality in Alaska. As described in Appendix D, it is assumed that there is a very small amount of direct sales taxes (effectively almost zero), and some amount of indirect and induced sales taxes, even though the State of Alaska has no sales tax. However, 107 reporting municipalities levy a general sales tax. Sales tax rates range from a low of 1% to a high of 7%. The IMPLAN model calculated indirect and induced sales taxes for construction (one-time) and operations (annually) of \$167,900 and \$140,000, respectively. These amounts constitute approximately 13% of the total amount of estimated State and local taxes, and a smaller percentage of total (direct, indirect and induced) State and local taxes. Because the mechanics of the IMPLAN model cannot be viewed directly, it is not known exactly what assumptions the IMPLAN model used regarding specific Alaska municipalities that indirect and induced sales taxes would occur. Although it is assumed that the model assumptions are accurate, it is possible that the IMPLAN model overestimated sales tax revenues, and thus may have slightly overstated the total tax impacts listed in Tables 3.7-9 and 3.7-10 of the EA. However, this would not change the general conclusion in the EA that both construction and operation of Alternative A are expected to generate positive fiscal impacts.

Master Response 8: Impacts Associated with Land Use Compatibility, Noise, Lighting, and Alcohol

Summary of Comments

Several comments raised concerns regarding the compatibility of the proposed gaming facility with existing zoning designations and with the surrounding rural residential area. Comments also raised concerns regarding alcohol being served at the proposed gaming facility and about potential noise and lighting impacts of the proposed project on surrounding residences.

Response

As discussed in Section 3.9 of the EA, the Project Site consists of a restricted fee Native Allotment and is under the jurisdiction of the Tribe. Therefore, the Project Site is not subject to State or local regulations or zoning designations. Nevertheless, development of the proposed project would be generally consistent with the economic, visual, and commercial development goals of the Chugiak-Eagle River Comprehensive Plan, which identifies the western area of the Project Site as suitable for development.

The Municipality of Anchorage zoning designations for land surrounding the Project Site are shown in Figure 3.9-1 of the EA. The Project Site is zoned as Light Industrial and Low Density Residential; however, these zoning designations do not apply to restricted fee Native Allotment land. The Light Industrial designation is intended for public and private light and general manufacturing, processing, storage, and distribution operations along with other uses that support and are compatible with industrial uses. Nightclubs, bars and restaurants, general retail, liquor stores, business industrial parks, public safety facilities, and general outdoor commercial recreation are also permitted, and many other commercial uses are also permitted and/or conditionally permitted. Conditionally permitted uses within Light Industrial zoned areas include major entertainment facilities; private clubs, lodges, and meeting halls; movie theaters; airports, heliports, and airstrips; and rail yards. The Proposed Project consists of a gaming facility with food and beverage services and would be consistent with the Light Industrial zoning designation permitted uses and/or conditionally permitted uses, although zoning designations and permitted uses do not technically apply to restricted fee Native Allotment land (the Project Site).

The proposed interim gaming facility discussed in Section 2 of the EA would not serve alcohol, and the permanent gaming facility may serve alcohol in the long-term once operational (2026) in accordance with applicable permits and licenses.⁴ Land use compatibility of the Proposed Project is further addressed in Section 3.9 of the EA. Noise levels and lighting effects associated with the light industrial uses listed above would also be comparable to those of the Proposed Project. Existing land uses east of the Project Site consist of low-density scattered residences and forestland. Peters Creek also occurs directly east of the Project Site. Existing land uses to the north, west, and south of the Project Site consist of the following:

- The Birchwood Airport to the immediate northwest;
- The Birchwood Recreation and Shooting Park (a shooting range), to the northwest;
- Southcentral Law Enforcement Tactical Range & Training Facility (a shooting range), to the northwest;
- The Alaska Railroad to the north and west;
- Warehouses and commercial development to the west;

⁴ Although generally state and local regulations and permitting do not apply to the Native Allotment, under 18 USC Section 1161, any sale of liquor on tribal lands must be in accordance with both state and tribal law.

- Light industrial uses, including a private manufacturing facility, to the south; and
- Roadways

The above land uses contribute to a high noise and intensive lighting environment. Therefore, the ambient noise environment and lighting surrounding the Project Site are already affected by the existing airport, shooting range, industrial uses, train traffic, and vehicular traffic. Potential noise impacts associated with construction and operation of the proposed project are discussed in Section 3.11 of the EA. As discussed therein, the addition of traffic from the proposed project would be the largest noise-contributing component of operation, and traffic noise levels could potentially increase to 56.5 dBA Leq. While this would be a noticeable change exceeding the threshold of perception (3 dBA), the federal Noise Abatement Criteria (NAC) of 67 dBA for residential land uses would not be exceeded, nor would noise standards set forth in the AMC 15.70.

Therefore, increases in traffic noise levels would be less than significant. Regarding construction-related noise, as discussed in Section 3.11, noise levels would be 87.8 dBA L_{max} at the nearest sensitive receptor (approximately 128 feet from the Project Site) when the loudest piece of construction equipment is being utilized. This level would be below the NAC standards of 90 dBA L_{max} for sensitive receptors during daytime hours. Construction would be temporary in nature, and not all equipment would be utilized simultaneously. Furthermore, Implementation of protective measures and BMPs identified in Section 2.1 of the EA would further reduce potential adverse impacts associated with noise and lighting.

Master Response 9: Enforceability of Mitigation and BMPs

Summary of Comments

Comments were received questioning how mitigation measures and BMPs would be enforced and monitored.

Response

The Project Site consists of restricted fee Native Allotment land. Generally, state and local regulatory requirements do not apply to restricted fee Native Allotment land, including zoning designations, permitting, and other similar civil authority (25 CFR § 1.4(a)). As discussed in Section 4 of the EA, mitigation is enforceable because it is (1) inherent to the project design; and/or (2) required by federal or tribal regulations. As noted in Section 2.1.7 of the EA, the Tribe has committed to adopting all BMPs and mitigation measures outlined in Table 2.1-1 and Table 4-1 of the EA through an amendment to the Final Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance (see updated Appendix M of the Final EA). Furthermore, the EA is not the document that commits the agency to mitigation; rather it is the FONSI that does so. Per 40 CFR §1501.6(d):

...[t]he [FONSI] shall state the authority for any mitigation that the agency has adopted and any applicable monitoring or enforcement provisions. If the agency finds no significant effects based on mitigation, the mitigated [FONSI] shall state the enforceable mitigation requirements or commitments that will be undertaken and the authority to enforce them, such as terms and conditions or other measures in a relevant permit, incidental take statement, or other agreement, and the agency shall prepare a monitoring and compliance plan for that mitigation consistent with §1505.3(c) of this subchapter. In addition, the agency shall prepare a monitoring and compliance plan for other mitigation as required by §1505.3(c) of this subchapter.

Consistent with this requirement, a mitigation monitoring and compliance plan will be prepared as part of the FONSI to monitor implementation of mitigation measures and BMPs. The BIA recognizes, consistent with federal law and policy, that supporting tribal sovereignty and self-determination includes respecting a Tribe's commitment to ensure compliance with mitigation measures under tribal law, similar to a state's commitment to enforce mitigation under State law. In this case, any mitigation required by the FONSI will be enforceable as a matter of federal and tribal law.

Master Response 10: Potential Impacts to Housing Values

Summary of Comments

Several commenters stated concerns regarding impacts to housing values from the proposed project.

Response

Potential impacts to housing values were analyzed in EA Section 3.7.3 and found to be less than significant. as described in **Section 3.9**, land uses in the immediate vicinity of the Project Site are predominantly heavy industrial, light industrial, railroad and low density residential to the east and southeast. Thus, most existing land uses in the immediate vicinity of the Project Site are not residential and currently contribute to a highly disturbed environment. Therefore, nearby property values are already impacted by the existing airport, shooting range, industrial uses, train traffic, and vehicular traffic. Furthermore, while municipal zoning designations do not technically apply to restricted fee Native Allotment land, the Project Site is zoned as Light Industrial and Low Density Residential. The Light Industrial designation allows for general manufacturing, nightclubs, bars and restaurants, general retail, liquor stores, business industrial parks, public safety facilities, and general outdoor commercial recreation facilities. Many other commercial uses are also permitted and/or conditionally permitted, including major entertainment facilities and airports. The Proposed Project consists of a small gaming facility with food and beverage services. This use is generally consistent with the Light Industrial designation and could represent a similar or lower intensity development in comparison with projects permitted under this zoning designation.

Master Response 11: Impacts Associated with Groundwater

Summary of Comments

Several comments expressed concerns regarding potential impacts of the Proposed Project on groundwater levels and neighboring wells.

Response

Refer to Appendix C of the EA regarding well data for 17 wells with available well log data that are located within a half mile of the Project Site. This data represents wells with documented well log data as maintained by the Alaska Department of Natural Resources. Several comments were received regarding additional groundwater wells in the vicinity of the Project Site that were not included in Appendix C. These additional wells either did not have available well log data maintained by the resource agency and thus would not have further informed the analysis within the EA or were beyond a half mile from the Project Site and would not be informative of conditions at or immediately surrounding the Project Site.

Additional comments were received on the varying depths of groundwater wells and well production. Comments also referenced a report from 1979, but no information was provided on the title of the report or the publisher.

As the source was not cited or provided by the commentors, it could therefore not be reviewed. It is correct that wells identified within a half mile of the Project Site are of varying depth and production rates. Regarding depth, it is noted that well depth presented in Appendix C of the EA ranged from 38 to 223. However, the static water depth ranged from 5 feet to 120 feet below ground surface (bgs). Wells are regularly drilled past the point of static water depth, and well depth can vary depending on the intended use of the well, future demand expectations, and preferences of individual well owners. A static water depth from 5 feet to 120 feet bgs is informative of the type of groundwater available in the vicinity of the Project Site and is not considered a constraint. Groundwater depth varies across the state and can exceed 400 feet bgs,⁵ therefore, depths up to 120 feet bgs is not considered excessively deep or indicative of a lack of groundwater availability. Regarding pumping rates, as discussed in Appendix C of the EA, these data were taken from private landowner well logs where lower pumping rates are sufficient and well design, such as diameter and pump type, are reflective of well needs. Thus, Appendix C of the EA states that these pumping rates are indicative only of evidence that groundwater is capable of serving private small landowner needs and is not indicative of the full potential of groundwater resources. Thus, the proposed well would be designed with a larger diameter and pump appropriately sized to produce flows necessary for supporting the Proposed Project.

As described in Section 3.3.3 of the EA, groundwater is considered an abundant resource within the state of Alaska, and the Alaska Department of Environmental Conservation does not consider groundwater availability or supply as a concern. As further described in Section 3.3.3 and Appendix C of the EA, the geological unit that underlies the Project Site is known for ample groundwater supply. Appendix C of the EA provides a cumulative analysis of potential impacts and considers that site conditions such as soil types are conducive to a high recharge rate. Given the data regarding groundwater availability and recharge, it is expected that sufficient groundwater will be available to supply the Proposed Project as well as smaller surrounding private wells without jeopardizing the longevity or useability of existing wells.

Comments were also received that raised concerns regarding groundwater contamination. Section 3.3.3 of the EA assessed for project impacts to groundwater quality. Please refer to Section 3.3.3 of the EA for this analysis. As stated therein, construction impacts would be less than significant with adherence to a site-specific Stormwater Pollution Prevention Plan and associated BMPs.

Operational impacts were found to be less-than-significant through a stormwater collection and treatment system that would be sized to accommodate a 100-year 24-hour flood event and would ensure that untreated runoff from the built environment would not enter Peters Creek and that runoff rates would not exceed pre-development conditions. Additionally, the septic system would be registered with the USEPA under the Underground Injection Control program as a Class V injection well and designed and installed consistent with the ADEC standards (ADEC 18 AAC 72 Wastewater Disposal, 18 AAC 72.530(e)(2)) (Appendix C of the EA). The septic system would also be designed by a licensed engineer in such a way as to demonstrate its structural and thermal integrity, consistent with AMC 15.65.205.

Therefore, with proper design and installation of the on-site wastewater system consistent with federal, tribal, and local standards and requirements, significant impacts to water quality in Peters Creek from treatment and discharge of wastewater would not occur.

⁵Alaska Department of Environmental Conservation (ADEC), 2008. Groundwater in Alaska. Available online at: <https://dec.alaska.gov/media/8493/dwp-groundwater-fact-sheet-2008.pdf>. Accessed August 2024

Comments also raised concerns regarding the methodology used to estimate water treatment and demand, specifically the number of patrons assumed. The socioeconomic impact study provided in Appendix D of the EA estimates 2,236 average guest visits per day at the proposed facility. While previous water demand estimates developed by EEI assumed 4,000 daily guests, this was revised to a more appropriate estimate of 3,000, which is still higher than the average visitation predicted in Appendix D. Water demand estimates were further refined based on USEPA water usage data and to account for water-saving project design details, such as use of waterless urinals. Calculations also considered similar water demand calculations of other casino buildings without hotels. After consideration of these factors, an average daily attendance of 3,000 guests and 90 employees per day at 2.6 gallons per person was assumed. It should be noted that the water demand estimates assumed in the EA and the Water Study prepared by EEI in Appendix C conservatively did not apply the 75% diversity reduction suggested by Marnell Gaming. As discussed in Section 2.1.7 of the EA, water would be treated to Safe Drinking Water Act standards. This would include removal of contaminants and sediments to these standards at the expense of the Tribe.

Several comments raised concerns regarding water rights. Table 1.6-1 of the EA notes that potential approval of a water right to utilize groundwater may be required, as well as a permit from the Alaska Department of Natural Resources to drill a well, in accordance with the Alaska Water Use Act. The Proposed Project does not involve asking other water rights holders to give up their water rights, nor does the Proposed Project propose connecting to municipal water or forcing other properties to connect to municipal water. The Eklutna River is approximately 4.5 miles from the Project Site, and the Proposed Project is unrelated to surface water use of Eklutna Lake and Eklutna River and is unrelated to Eklutna River restoration efforts.

3.0 RESPONSES TO AGENCY/ORGANIZATION COMMENT LETTERS

Response to Comment Letter A1: ANCSA Regional Association

Response to Comment A1-1

Comment noted. The commenter expresses support for the Proposed Project.

Response to Comment Letter A2: Municipality of Anchorage

Response to Comment A2-1

Comment noted. The commenter expresses support for the Proposed Project.

Response to Comment Letter A3: Municipality of Anchorage

Response to Comment A3-1

Comment noted. The commenter expresses support for the Proposed Project.

Response to Comment Letter A4: Chugiak Community Council

Response to Comment A4-1

Refer to Master Response 2.

Response to Comment Letter A5: ADF&G

Response to Comment A5-1

Comment noted. Although the Alaska National Interest Land Conservation Act (ANICLA) does not apply to nonrural areas, including the Project Site, this description has been left in the EA as it is accurate as written and applies to the broader region.

Response to Comment A5-2

Comment noted. Edits have been made to the Final EA as appropriate.

Response to Comment A5-3

This comment requests that an additional section be added to Section 3.5.2 of the EA (Biological Resources Environmental Setting) to address moose and bear. Section 3.5.2 of the EA referred the reader to Appendix G of the EA regarding species that were observed on the Project Site, which includes both bear and moose. Section 3.5.3 of the December EA further acknowledged the presence of bear and moose that incidentally occur on the Project Site by stating: “Signs of black bears foraging through the ruderal/developed area were observed, and moose scat indicates that the site is passed through by wildlife. Nearer to Peters Creek, flattened areas of vegetation were observed, suggesting that animals may bed down along the banks of the creek. Additionally, Peters Creek is a fish bearing stream capable of supporting anadromous fishes, including salmon.” However, in response to this comment, additional information has been added to Section 3.5.2 of the Final EA.

Response to Comment A5-4

Section 3.5.3 of the EA provides assessment criteria that summarizes those effects that would be considered significant for the purposes of the EA and for which analysis is warranted under NEPA. As not all impacts to biological resources are considered significant pursuant to NEPA, the assessment criteria specifies those actions that would be considered significant to provide context for the reader on the subsequent analysis.

Response to Comment A5-5

Moose and black bear are not considered federally listed species for the purpose of the EA, and a species-specific analysis of these species is outside the scope of the EA. The Proposed Project does have general BMPs that the Tribe has committed to as part of project design that were identified in Section 2.1.11 that include providing trash receptacles, equipping receptacles with lids and staging for construction within the impact area/ previously disturbed land. Further, the Tribe will commit to the use of bear-proof receptacles for outside solid waste collection receptacles. Section 2.1.11 has been revised to include this BMP. Additional BMPs have been added to the EA in response to comments received during the public review period, including increasing the setback to Peter’s Creek from 25-feet to 50-feet to reduce impacts to wildlife utilizing the creek and immediately adjacent habitat, and posting of signage along the Project Site frontage to warn passers-by of the risk of collision with wildlife.

Response to Comment A5-6

Comment noted. The Proposed Project would not create a public access point to Peter’s Creek, and therefore would not induce additional fishing activities.

Response to Comment A5-7

Bears and other wildlife mentioned in this comment are not considered federally listed species for the purpose of the EA. The project area is already subject to intense human activity, and the Project Site is not a wildlife migration corridor or other high value habitat; therefore, no significant effects would occur and mitigation is not warranted. However, these measures have been incorporated as appropriate as BMPs into Table 2.1-1 of the Final EA.

Response to Comment A5-8

Refer to Master Response 2.

Response to Comment A5-9

Comment noted. Peters Creek is outside of the impact area of the Proposed Project, and no work will occur on or within 50 feet of Peters Creek.

Response to Comment Letter A6: ADOT&PF

Response to Comment A6-1

Comment noted. While development of the Proposed Project is not subject to approval from state or local agencies, Table 1.6-1 of the EA acknowledged that the Proposed Project would require access approvals from the Alaska DOT&PF. Because the permanent facility would generate more than 100 daily trips, under 17 AAC 10.060 and 10.070, this will involve responding to and addressing comments from the Alaska DOT&PF on the TIA provided in Appendix A (while an access permit may be required for the interim modular facility, it would not likely be subject to the TIA review process as it would generate fewer than 100 hourly trips⁶). The TIA provided in the EA Appendix A was prepared consistent with the requirements of 17 AAC 10.070. A notice of availability of the EA, including the TIA, was sent to the Alaska DOT&PF via Fed-Ex delivery. The Tribe will work with the Alaska DOT&PF to obtain any necessary access permits, address its comments, if any, on the TIA and fulfill any applicable conditions of access approvals. The Final EA, Table 2.1-1, has been clarified to describe the access approval process with Alaska DOT&PF and confirms that any required conditions of such approval will be adhered to.

Response to Comment A6-2

As described in the EA Section 3.9.3, in accordance with 14 CFR Part 77, a New Case for Off Airport Constriction was submitted via the Obstruction Evaluation and Airport Airspace Analysis (OE/AAA) online portal page to the FAA for review on August 26, 2024 (Appendix J of the EA). The FAA reviewed the case and issued a “Determination of No Hazard to Air Navigation” on September 23, 2024. As discussed in **Response to Comment A6-1**, the Tribe will work with Alaska DOT&PF to obtain any necessary access permits and any required conditions of such permits will be adhered to.

⁶ The interim modular facility could house up to 85 gaming machines. Based on trip generation rates provided in the ITE Trip Generation Manual, 11th Edition, this number of gaming positions would generate approximately 681 weekday daily vehicle trips, of which approximately 50 would occur during the interim facility’s busiest hour (p.m. peak hour of generator).

4.0 RESPONSES TO INDIVIDUAL COMMENT LETTERS

Response to Comment Letter I1: Angie Hammill

Response to Comment I1-1

Refer to Master Response 1, Master Response 6, and Master Response 8.

Response to Comment Letter I2: Barbara Crittenden

Response to Comment I2-1

Refer to Master Response 1.

Response to Comment Letter I3: Judy Moore

Response to Comment I3-1

Refer to Master Response 4. Refer to Section 2 of the EA for details regarding the proposed alternatives. The commenter states that the Proposed Project would include both a "...gambling facility, a 2nd gambling facility on site with restaurants and an event center to host events 4 times a week..." Given the Tribe's and Allotment Owners' ongoing need for economic opportunities, during the construction period for the permanent facility, a modular building would be established in the southwestern corner of the Project Site for the operation of a smaller interim facility that could house approximately 85 gaming devices. Parking for the interim facility would be established within the footprint of the parking lot proposed for the permanent facility. However, the interim gaming facility would not continue to operate once the gaming facility under Alternative A (Proposed Project) opens. Additionally, Alternative A (Proposed Project) and Alternative B (Event Center) would not both be constructed. The Proposed Project consists of one small interim gaming facility thence one permanent gaming facility of approximately 58,000 square feet.

Response to Comment I3-2

Refer to Master Response 6, Master Response 7, and Master Response 8.

Response to Comment I3-3

Refer to Master Response 1 and Master Response 4. Refer to Section 3.7 of the EA regarding potential impacts associated with socioeconomic effects.

Response to Comment I3-4

Refer to Master Response 1.

Response to Comment Letter I4: Marilyn Sarvela

Response to Comment I4-1

Refer to Master Response 2.

Response to Comment Letter I5: Phillip Flippo

Response to Comment I5-1

Comment noted. Comment noted. The commenter expresses support for the Proposed Project.

Response to Comment Letter I6: Sean and Lori Barnett

Response to Comment I6-1

Refer to Master Response 2.

Response to Comment Letter I7: James Crewdson

Response to Comment I7-1

Refer to Master Response 1.

Response to Comment Letter I8: Karen Perry

Response to Comment I8-1

Refer to Master Response 1, Master Response 6, and Master Response 8.

Response to Comment Letter I9: Kathie and Calvin Steele

Response to Comment I9-1

Refer to Master Response 5.

Response to Comment I9-2

Refer to Master Response 6 and Master Response 8. Refer to Section 3.8 of the EA regarding potential impacts associated with traffic. Refer to Section 3.11 of the EA regarding potential impacts associated with noise.

Response to Comment I9-3

Refer to Section 3.2 of the EA regarding potential impacts associated with earthquakes.

Response to Comment I9-4

Refer to Master Response 7.

Response to Comment Letter I10: Tom Desalvo

Response to Comment I10-1

Refer to Master Response 1, Master Response 5, Master Response 6, and Master Response 6, and Master Response 7.

Response to Comment Letter I11: Vicki Herman

Response to Comment I11-1

Refer to Master Response 1, Master Response 2, and Master Response 4.

Response to Comment Letter I12: Debbie Ossiander

Response to Comment I12-1

Refer to Master Response 1 and Master Response 2.

Response to Comment Letter I13: Beth Conley

Response to Comment I13-1

Refer to Master Response 5.

Response to Comment I13-2

Refer to Master Response 8. Refer to Section 3.11 of the EA regarding potential impacts associated with noise.

Response to Comment I13-3

Refer to Master Response 6, Master Response 7, and Master Response 8.

Response to Comment I13-4

Refer to Section 3.3 of the EA regarding potential impacts associated with groundwater.

Response to Comment I13-5

Refer to Master Response 8 and Master Response 10. Refer to Section 3.11 of the EA regarding potential impacts associated with noise.

Response to Comment I13-6

Refer to Master Response 6 and Master Response 8. Refer to Section 3.8 of the EA regarding potential impacts associated with traffic.

Response to Comment I13-7

Refer to Master Response 5. Refer to Section 3.5 of the EA regarding potential impacts associated with Peters Creek, fish, and wildlife.

Response to Comment I13-8

Refer to Master Response 1 and Master Response 8.

Response to Comment Letter I14: Janice Norman

Response to Comment I14-1

Refer to Master Response 6.

Response to Comment I14-2

Refer to Master Response 4.

Response to Comment I14-3

Refer to Master Response 4, Master Response 6, and Master Response 8.

Response to Comment I14-4

Refer to Master Response 1.

Response to Comment Letter I15: Matt Cruickshank

Response to Comment I15-1

Refer to Master Response 2.

Response to Comment I15-2

Refer to Master Response 5.

Response to Comment I15-3

Refer to Section 3.3 of the EA regarding potential impacts associated with groundwater. As stated in Table 1.6-1 of the EA, should a well be drilled on the Project Site, approval of a water right to utilize groundwater and a permit to drill a well would be obtained and complied with pursuant to the Alaska Water Use Act (AS 46.15), as may be applicable.

Response to Comment I15-4

Refer to Master Response 6. Refer to Section 3.8 of the EA regarding potential impacts associated with traffic. A Traffic Impact Analysis is included as Appendix A to the EA.

Response to Comment I15-5

Refer to Master Response 1 and Master Response 3. Potential cumulative effects are addressed in Section 3.15 of the EA. Potential indirect effects are addressed in Section 3.16 of the EA.

Response to Comment I15-6

Refer to Master Response 2.

Response to Comment Letter I16: Martin Stofer

Response to Comment I16-1

Refer to Master Response 1.

Response to Comment Letter I17: Savannah Haines

Response to Comment I17-1

Refer to Master Response 1, Master Response 5, Master Response 6, Master Response 8, and Master Response 10.

Response to Comment Letter I18: Valerie Jokela

Response to Comment I18-1

Refer to Master Response 1 and Master Response 2.

Response to Comment Letter I19: Brian Holl

Response to Comment I19-1

Refer to Master Response 2.

Response to Comment I19-2

Refer to Master Response 5.

Response to Comment I19-3

Refer to Master Response 11.

Response to Comment I19-4

Refer to Master Response 11.

Response to Comment I19-5

Refer to Master Response 11.

Response to Comment I19-6

Refer to Master Response 11.

Response to Comment I19-7

Refer to Master Response 11. Refer to Sections 2.1.7 and 3.10 of the EA, as well as Appendix C regarding proposed wastewater facilities. As discussed in Section 2.1.7 of the EA, wastewater would be treated via an on-site advanced secondary treatment system and disposed of through on-site septic facilities that would be appropriately sized. The septic system would be registered with the USEPA under the Underground Injection Control program as a Class V injection well and designed and installed consistent with ADEC standards (ADEC 18 AAC 72 Wastewater Disposal, 18 AAC 72.530(e)(2)).

Response to Comment I19-8

Refer to Master Response 7.

Response to Comment I19-9

Refer to Master Response 7. The lease contains proprietary and confidential business information. The key elements of the lease as they relate to environmental effects have been described in Section 2 of the EA.

Response to Comment I19-10

Refer to Master Response 6.

Response to Comment I19-11

Letters of support for the Proposed Project from the Municipality of Anchorage are included in comment letters A2 and A3. As discussed in Section 2.1.5 of the EA, although a small amount of tree removal (approximately 23.6 cords of birch) has occurred since the biological resources survey and tree survey, these trees are still considered part of the baseline for the environmental analysis in the EA. As also stated therein, the BIA determined that the value of the timber on the Project Site was below the \$5,000 threshold for requiring a tree harvest permit (25 CFR 163.26). Therefore, the BIA has determined that no timber harvest permit is required for the Proposed Project, nor was one required for the minor tree removal that has already occurred. The Project Site has been re-classified as not "forest land" or "Indian forest land", therefore, it is not subject to the permitting requirements of 25 CFR Part 163 and has been removed from BIA's Cook Inlet Forest Management Plan.

Response to Comment Letter I20: Dolly Caswell

Response to Comment I20-1

Refer to Master Response 1.

Response to Comment Letter I21: Debbie Ossiander

Response to Comment I21-1

Refer to Master Response 11 and Master Response 9.

Response to Comment I21-2

Refer to Master Response 8 and Master Response 9. As discussed in Section 2.1 of the Final EA, on-site snow storage would occur in the landscaped areas shown on Figure 2.1-1 of the EA, excluding near Peters Creek, consistent with AMC 21.07. As stated in Section 2.1.6 of the Final EA, snow would be hauled off-site when necessary and disposed of at a permitted snow disposal facility, such as the Sand Lake Disposal Site in Anchorage or at a permitted snow disposal facility if a nearer one becomes available.

Response to Comment I21-3

Refer to Master Response 5.

Response to Comment I21-4

Refer to Master Response 9 and Master Response 11. Refer to Sections 2.1.7 and 3.10 of the EA, as well as Appendix C regarding proposed wastewater facilities.

As discussed in Section 2.1.7 of the EA, the proposed on-site septic system would include a grease interceptor tank, 10,000 – 15,000-gallon septic tank, a treatment tank, an advanced secondary treatment system, and a septic drainage field that would be installed below the paved parking areas of the permanent facility. During operation of the interim gaming facility, wastewater would be self-contained within holding tanks and hauled from the Project Site to the King Street commercial septage receiving station for treatment and disposal at the Eagle River Wastewater Treatment Facility owned and operated by the Anchorage Water and Wastewater Utility. The septic system would be registered with the USEPA under the Underground Injection Control program as a Class V injection well and designed and installed consistent with ADEC standards (ADEC 18 AAC 72 Wastewater Disposal, 18 AAC 72.530(e)(2)).

The septic system would also be designed by a licensed engineer in such a way as to demonstrate its structural and thermal integrity, consistent with AMC 15.65.205.

Response to Comment I21-5

Refer to Master Response 5.

Response to Comment I21-6

Section 2.1.7 of the EA and Section 3.10 of the Final EA have been revised to clarify that Matanuska Electric Association would provide electrical service to the Project Site. Existing powerlines already occur on-site. Cellular service provider information has also been updated.

Response to Comment I21-7

Refer to Master Response 7.

Response to Comment I21-8

Refer to Master Response 5. Additionally, ADF&G was mailed a copy of the EA.

Response to Comment I21-9

Refer to Master Response 5.

Response to Comment I21-10

Refer to Master Response 5, Master Response 9, and Master Response 11.

Response to Comment I21-11

Refer to Master Response 5 and Master Response 11.

Response to Comment I21-12

Details regarding the proposed retaining walls are included in Section 2.1.6 of the Final EA and Section 3.1 of Appendix B of the Final EA.

Response to Comment I21-13

Refer to Master Response 9. As stated on page 3-19 of the EA, for air quality modeling purposes, construction activities are assumed to occur on average eight hours a day, five days a week. BMPs in Table 2.1-1 of the EA state that construction activities would be limited to daytime hours between 6 am and 10 pm during construction months and 7am to 10 pm during non-construction months, which is consistent with construction hours specified within AMC 15.70.060.B3. Therefore, construction activities would generally occur for eight hours per day sometime between the hours of 6 am and 10 pm during construction months and 7am to 10 pm during non-construction months.

Response to Comment I21-14

Refer to Master Response 5. Mitigation is provided in Section 4 of the EA to reduce impacts to potentially occurring nesting eagles.

Response to Comment I21-15

Refer to Master Response 7. Refer to Section 3.10.3 of the EA regarding impacts associated with solid waste.

Response to Comment I21-16

Refer to Master Response 6.

Response to Comment I21-17

Refer to Master Response 1.

Response to Comment I21-18

Refer to Master Response 7, Master Response 8, and Master Response 7

Response to Comment I21-19

Refer to Master Response 8 and Master Response 9.

Response to Comment Letter I22: Jeff Schlies

Response to Comment I22-1

Refer to Master Response 4, Master Response 6, and Master Response 8. Electrical infrastructure to support the proposed project already occurs on the Project Site, as discussed in Section 3.10 of the EA.

Response to Comment Letter I23: Marilyn Sarvela

Response to Comment I23-1

Refer to Master Response 2.

Response to Comment I23-2

Refer to Master Response 7, Master Response 9, and Master Response 11.

Response to Comment I23-3

Refer to Master Response 7 and Master Response 7.

Response to Comment I23-4

Refer to Master Response 5, Master Response 6, and Master Response 8. Refer to Section 3.5 of the EA regarding potential impacts to biological resources, including habitat. Refer to Section 3.7 of the EA regarding potential impacts associated with problem gambling. The commenter also references older materials, which were used in the development of the Highway Capacity Manual (HCM), a document that is regularly updated and was used to evaluate intersection operations in the EA. The HCM was most recently updated in 2022 (7th Edition).

Response to Comment I23-5

Refer to Master Response 11.

Response to Comment I23-6

Refer to Master Response 8.

Response to Comment I23-7

As stated in Section 2.1.7 of the EA, proposed on-site septic facilities would include an advanced secondary treatment system.

Response to Comment I23-8

Refer to Master Response 11.

Response to Comment I23-9

Refer to Master Response 1. Refer to Section 3.7 of the EA regarding potential impacts associated with problem gambling.

Response to Comment I23-10

Refer to Master Response 9 and Master Response 11.

Response to Comment I23-11

Refer to Master Response 6 and Master Response 7.

Response to Comment I23-12

Letters of support for the Proposed Project from the Municipality of Anchorage are included in comment letters A2 and A3. Refer to Master Response 9.

Response to Comment I23-13

Refer to Master Response 1, Master Response 5, Master Response 7, and Master Response 8.

Response to Comment I23-14

Refer to Master Response 1 and Master Response 9. Refer to Section 2.1.2 for details regarding the proposed interim gaming facility. During operation of the interim gaming facility, wastewater would be self-contained within holding tanks and hauled from the Project Site to the King Street commercial septage receiving station for treatment and disposal at the Eagle River Wastewater Treatment Facility owned and operated by the Anchorage Water and Wastewater Utility.

Response to Comment I23-15

Refer to Master Response 9. Refer to Section 2.1.5 of the EA regarding the need for a timber harvest permit. As stated therein, the BIA determined that the value of the timber on the Project Site was below the \$5,000 threshold for requiring a tree harvest permit (25 CFR 163.26). Therefore, the BIA has determined that no timber harvest permit is required for the Proposed Project. The Project Site has been re-classified as not "forest land" or "Indian forest land", therefore, it is not subject to the permitting requirements of 25 CFR Part 163 and has been removed from BIA's Cook Inlet Forest Management Plan.

Response to Comment I23-16

Refer to Master Response 6.

Response to Comment I23-17

Refer to Master Response 5. Impacts associated with grading and drainage are addressed in Section 3.2 of the EA.

Response to Comment I23-18

Refer to Master Response 6. Refer to Section 3.4.3 of the EA. As stated therein, air quality modeling values (Appendix F of the EA) conservatively included various activities and types of vehicle trips associated with construction and operation of the Proposed Project. The number of haul trips associated with grading during construction was estimated at 1,700 round trips. The number of operational trips, which included any trips associated with potential off-site water and wastewater hauling, was estimated at 8,737 average daily trips, which is a weighted average of the 8,010 total weekday trips, and 10,550 Saturday/Sunday trips (Appendix A of the EA).

Response to Comment I23-19

Refer to Master Response 7.

Response to Comment I23-20

Refer to Master Response 9. As discussed in Section 2.1.9 of the EA, a 500-kW diesel generator may be used for up to 20 days during construction to supply power to construction trailers and the interim gaming facility before electrical utility connections are established. The Proposed Project would not result in significant impacts to air quality, as discussed in Section 3.4.3 of the EA.

Response to Comment I23-21

Refer to Master Response 9.

Response to Comment I23-22

Refer to Master Response 9.

Response to Comment I23-23

Refer to Master Response 1.

Response to Comment I23-24

Refer to Master Response 11.

Response to Comment I23-25

Refer to Master Response 9. As discussed in Section 2.1.9 of the EA, a 500-kW diesel generator may be used for up to 20 days during construction to supply power to construction trailers and the interim gaming facility before electrical utility connections are established. As discussed in Section 3.4.3, an 800-kW diesel emergency generator would provide backup power in the event of an electrical outage.

The estimated emissions from the proposed diesel emergency generator assume 500 hours of annual operation, which represents a reasonable "worst-case" estimate determined by the USEPA on a potential to emit (PTE) basis (USEPA, 1995). Therefore, this number is conservative, and actual generator use would be less. The Proposed Project would not result in significant impacts to air quality, as discussed in Section 3.4.3 of the EA.

Response to Comment I23-26

The commenter misunderstands the significance criteria utilized in determining potential impacts associated with biological resources as impact determinations. As discussed in Section 3.5 of the EA, potential impacts associated with biological resources would be less than significant with mitigation.

Response to Comment I23-27

Only qualified construction workers would be hired for skilled construction jobs.

Response to Comment I23-28

Refer to Master Response 6 and Master Response 7.

Response to Comment I23-29

Refer to Master Response 1.

Response to Comment I23-30

Refer to Master Response 10.

Response to Comment I23-31

Refer to Master Response 6.

Response to Comment I23-32

Refer to Master Response 6.

Response to Comment I23-33

Refer to Master Response 1 and Master Response 8.

Response to Comment I23-34

Potential impacts associated with parks are addressed in Section 3.10 of the EA.

Response to Comment I23-35

Refer to Table 2.1-1 of the EA regarding BMPs for recyclables. The solid waste source cited by the commenter is a calculator tool, not a study.

Response to Comment I23-36

Refer to Section 3.4.3 of the EA. As stated therein, electricity would be used for space heating, water heating, and cooking equipment where feasible; however, natural gas use was assumed in the emission estimates (Appendix F of the EA) for a more conservative analysis regarding impacts on air quality.

Response to Comment I23-37

Refer to Master Response 7.

Response to Comment I23-38

Refer to Master Response 8 and Master Response 9.

Response to Comment Letter I24: Senator Kelly Merrick

Response to Comment I24-1

Refer to Master Response 2.

Response to Comment Letter I25: Liz Morgan

Response to Comment I25-1

Refer to Master Response 1.

Response to Comment Letter I26: Palma Ingles

Response to Comment I26-1

Refer to Master Response 2.

Response to Comment I26-2

Refer to Master Response 5.

Response to Comment I26-3

Refer to Master Response 7 and Master Response 9. Letters of support for the Proposed Project from the Municipality of Anchorage are included in comment letters A2 and A3.

Response to Comment I26-4

Refer to Master Response 7 and Master Response 9.

Response to Comment I26-5

Refer to Master Response 1.

Response to Comment I26-6

Refer to Master Response 6.

Response to Comment I26-7

Refer to Master Response 6 and Master Response 7. Letters of support for the Proposed Project from the Municipality of Anchorage are included in comment letters A2 and A3.

Response to Comment I26-8

Refer to Master Response 1.

Response to Comment I26-9

Refer to Master Response 6.

Response to Comment I26-10

Refer to Master Response 8 and Master Response 9.

Response to Comment I26-11

Refer to Master Response 6, Master Response 7, and Master Response 8.

Response to Comment I26-12

Refer to Master Response 5 and Master Response 11.

Response to Comment I26-13

Refer to Master Response 11. Refer to Sections 2.1.7 and 3.10 of the EA, as well as Appendix C regarding proposed wastewater facilities. As discussed in Section 2.1.7 of the EA, wastewater would be treated via an on-site advanced secondary treatment system and disposed of through on-site septic facilities that would be appropriately sized. The septic system would be registered with the USEPA under the Underground Injection Control program as a Class V injection well and designed and installed consistent with ADEC standards (ADEC 18 AAC 72 Wastewater Disposal, 18 AAC 72.530(e)(2)). The septic system would also be designed by a licensed engineer in such a way as to demonstrate its structural and thermal integrity, consistent with AMC 15.65.205. As discussed in Appendix C of the Final EA, an 8,000-gallon tank would be implemented, and would consist of coated steel.

Response to Comment I26-14

Refer to Master Response 1, Master Response 4, and Master Response 8. As discussed in Section 2.1.5 of the EA, although a small amount of tree removal (approximately 23.6 cords of birch) has occurred since the biological resources survey and tree survey, these trees are still considered part of the baseline for the environmental analysis in the EA. As also stated therein, the BIA determined that the value of the timber on the Project Site was below the \$5,000 threshold for requiring a federal tree harvest permit (25 CFR 163.26). Therefore, the BIA has determined that no timber harvest permit is required for the Proposed Project, nor was one required for the minor tree removal that has already occurred. The Project Site has been re-classified as not "forest land" or "Indian forest land", therefore, it is not subject to the permitting requirements of 25 CFR Part 163 and has been removed from BIA's Cook Inlet Forest Management Plan.

Response to Comment Letter I27: Jean Moore

Response to Comment I27-1

Refer to Master Response 1.

Response to Comment I27-2

Section 2.1.7 of the EA and Section 3.10 of the Final EA have been revised to clarify that Matanuska Electric Association would provide electrical service to the Project Site. Existing powerlines already occur on-site.

Response to Comment I27-3

Refer to Master Response 7.

Response to Comment I27-4

Refer to Master Response 1. Refer to Section 3.7 of the EA regarding potential impacts associated with problem gambling.

Response to Comment I27-5

Refer to Master Response 6, Master Response 7, and Master Response 6.

Response to Comment I27-6

Refer to Master Response 6 and Master Response 7.

Response to Comment I27-7

Refer to Master Response 6.

Response to Comment I27-8

Refer to Master Response 7.

Response to Comment I27-9

Refer to Master Response 8.

Response to Comment Letter I28: Valerie Raatz

Response to Comment I28-1

Refer to Master Response 4, Master Response 6, and Master Response 8.

Response to Comment Letter I29: Sabrina France

Response to Comment I29-1

Refer to Master Response 1, Master Response 6, Master Response 7, and Master Response 8.

Response to Comment Letter I30: Al and Jackie Clemens

Response to Comment I30-1

Refer to Master Response 6 and Master Response 8.

Response to Comment I30-2

Refer to Master Response 7.

Response to Comment I30-3

Refer to Master Response 7 and Master Response 8.

Response to Comment Letter I31: Amber Mock

Response to Comment I31-1

Refer to Master Response 1, Master Response 6, Master Response 7, and Master Response 8.

Response to Comment Letter I32: Andrew France

Response to Comment I32-1

Refer to Master Response 7, Master Response 8, and Master Response 10.

Response to Comment Letter I33: Faith Brock

Response to Comment I33-1

Refer to Master Response 7, Master Response 8, and Master Response 10.

Response to Comment Letter I34: Reneè Scott

Response to Comment I34-1

Refer to Master Response 7, Master Response 8, and Master Response 10.

Response to Comment Letter I35: Tamera Schlies

Response to Comment I35-1

Refer to Master Response 7, Master Response 8, and Master Response 10.

Response to Comment I35-2

Refer to Master Response 7 and Master Response 6

Response to Comment I35-3

Refer to Master Response 6 and Master Response 8.

Response to Comment Letter I36: Bill Sarvela

Response to Comment I36-1

Refer to Master Response 2.

Response to Comment I36-2

Refer to Master Response 6.

Response to Comment I36-3

Refer to Section 2.1.5 of the EA regarding the need for a timber harvest permit. As stated therein, the BIA determined that the value of the timber on the Project Site was below the \$5,000 threshold for requiring a tree harvest permit (25 CFR 163.26). Therefore, the BIA has determined that no timber harvest permit is required for the Proposed Project. The Project Site has been re-classified as not "forest land" or "Indian forest land", therefore, it is not subject to the permitting requirements of 25 CFR Part 163 and has been removed from BIA's Cook Inlet Forest Management Plan. Refer to Appendix L of the EA for details regarding timber harvest.

Response to Comment I36-4

Refer to Master Response 11.

Response to Comment Letter I37: Bree Yankus

Response to Comment I37-1

Refer to Master Response 2.

Response to Comment I37-2

Refer to Master Response 1.

Response to Comment I37-3

Refer to Master Response 6 and Master Response 8.

Response to Comment I37-4

Refer to Master Response 5 and Master Response 11.

Response to Comment I37-5

Refer to Master Response 2.

Response to Comment I37-6

Refer to Master Response 1.

Response to Comment I37-7

Refer to Master Response 6 and Master Response 8.

Response to Comment I37-8

Refer to Master Response 11.

Response to Comment I37-9

Refer to Master Response 9 and Master Response 11. The septic system would be registered with the USEPA under the Underground Injection Control program as a Class V injection well and designed and installed consistent with ADEC standards (ADEC 18 AAC 72 Wastewater Disposal, 18 AAC 72.530(e)(2)).

As discussed in Appendix C of the Final EA, an 8,000-gallon tank would be utilized, and would consist of coated steel. The septic system would also be designed by a licensed engineer in such a way as to demonstrate its structural and thermal integrity, consistent with AMC 15.65.205.

Response to Comment I37-10

Refer to Master Response 7 and Master Response 8.

Response to Comment I37-11

Refer to Master Response 2.

Response to Comment I37-12

Refer to Master Response 1.

Response to Comment I37-13

Refer to Master Response 6.

Response to Comment I37-14

Refer to Master Response 11.

Response to Comment I37-15

Refer to Master Response 9 and Master Response 11. The septic system would be registered with the USEPA under the Underground Injection Control program as a Class V injection well and designed and installed consistent with ADEC standards (ADEC 18 AAC 72 Wastewater Disposal, 18 AAC 72.530(e)(2)). As discussed in Appendix C of the Final EA, an 8,000-gallon tank would be utilized, and would consist of coated steel. The septic system would be designed by a licensed engineer in such a way as to demonstrate its structural and thermal integrity, consistent with AMC 15.65.205.

Response to Comment I37-16

Refer to Master Response 7.

Response to Comment I37-17

Comment noted.

Response to Comment Letter I38: Chris Johnson

Response to Comment I38-1

Refer to Master Response 6, Master Response 7, and Master Response 8.

Response to Comment Letter I39: James Nugen

Response to Comment I39-1

Refer to Master Response 6 and Master Response 7.

Response to Comment Letter I40: Lee Waters

Response to Comment I40-1

Refer to Master Response 2.

Response to Comment I40-2

Refer to Master Response 5. Details regarding the proposed retaining walls are included in Section 2.1.6 of the Final EA and Section 3.1 of Appendix B of the Final EA.

Response to Comment I40-3

Refer to Master Response 7.

Response to Comment I40-4

Refer to Master Response 1.

Response to Comment I40-5

Refer to Master Response 6 and Master Response 7.

Response to Comment I40-6

Refer to Master Response 6.

Response to Comment I40-7

Refer to Master Response 6, Master Response 7, and Master Response 8.

Response to Comment I40-8

Refer to Master Response 1, Master Response 4, and Master Response 8. As discussed in Section 2.1.5 of the EA, although a small amount of tree removal (approximately 23.6 cords of birch) has occurred since the biological resources survey and tree survey, these trees are still considered part of the baseline for the environmental analysis in the EA. As also stated therein, the BIA determined that the value of the timber on the Project Site was below the \$5,000 threshold for requiring a tree harvest permit (25 CFR 163.26). Therefore, the BIA has determined that no timber harvest permit is required for the Proposed Project, nor was one required for the minor tree removal that has already occurred. The Project Site has been re-classified as not "forest land" or "Indian forest land", therefore, it is not subject to the permitting requirements of 25 CFR Part 163 and has been removed from BIA's Cook Inlet Forest Management Plan.

Response to Comment Letter I41: Tom Prunty

Response to Comment I41-1

Refer to Master Response 8.

Response to Comment I41-2

Section 2.1.7 of the EA and Section 3.10 of the Final EA have been revised to clarify that Matanuska Electric Association would provide electrical service to the Project Site. Existing powerlines already occur on-site.

Response to Comment I41-3

Refer to Master Response 7.

Response to Comment I41-4

Refer to Master Response 6.

Response to Comment I41-5

Refer to Master Response 6 and Master Response 6.

Response to Comment Letter I42: Veronica and Kalani Wakinekona

Response to Comment I42-1

Refer to Master Response 1.

Response to Comment Letter I43: Vicky Prunty

Response to Comment I43-1

Refer to Master Response 2.

Response to Comment I43-2

Refer to Master Response 6 and Master Response 6.

Response to Comment I43-3

Refer to Master Response 6 and Master Response 7.

Response to Comment I43-4

Refer to Sections 2.1.7 and 3.10.3 of the EA, as well as Appendix C, for details regarding the proposed on-site septic system.

Response to Comment I43-5

Refer to Master Response 4, Master Response 7, and Master Response 8.

Response to Comment Letter I44: Veronica Allmaras

Response to Comment I44-1

Refer to Master Response 6, Master Response 8, and Master Response 11.

Response to Comment Letter I45: Jacob Sears

Response to Comment I45-1

Refer to Master Response 1 and Master Response 4.

Response to Comment Letter I46: Glen Yankus

Response to Comment I46-1

Refer to Master Response 2. Per CEQ regulations, scoping is not required for Environmental Assessments (40 CFR § 1501.5).

Response to Comment I46-2

Refer to Master Response 1. Refer to Section 3.15 of the EA regarding potential cumulative impacts associated with known growth and development in the area. NEPA does not require analysis of speculative future land uses.

Response to Comment I46-3

Refer to Master Response 8. Refer to Section 3.4 of the EA regarding potential impacts associated with air quality and Section 3.13 regarding potential impacts associated with visual resources.

Response to Comment I46-4

Refer to Master Response 6.

Response to Comment I46-5

Refer to Master Response 6.

Response to Comment I46-6

Refer to Master Response 1.

Response to Comment I46-7

Refer to Master Response 1.

Response to Comment I46-8

Refer to Table 1.6-1 of the EA regarding potential permits approvals needed.

Response to Comment I46-9

Refer to Master Response 8.

Response to Comment I46-10

As discussed in Section 2.1.5 of the EA, although a small amount of tree removal (approximately 23.6 cords of birch) has occurred since the biological resources survey and tree survey, these trees are still considered part of the baseline for the environmental analysis in the EA. As also stated therein, the BIA determined that the value of the timber on the Project Site was below the \$5,000 threshold for requiring a tree harvest permit (25 CFR 163.26). Therefore, the BIA has determined that no timber harvest permit is required for the Proposed Project, nor was one required for the minor tree removal that has already occurred. The Project Site has been re-classified as not "forest land" or "Indian forest land", therefore, it is not subject to the permitting requirements of 25 CFR Part 163 and has been removed from BIA's Cook Inlet Forest Management Plan.

Response to Comment I46-11

Refer to Master Response 5. The septic system would be registered with the USEPA under the Underground Injection Control program as a Class V injection well and designed and installed consistent with ADEC standards (ADEC 18 AAC 72 Wastewater Disposal, 18 AAC 72.530(e)(2)). The septic system would also be designed by a licensed engineer in such a way as to demonstrate its structural and thermal integrity, consistent with AMC 15.65.205.

Response to Comment I46-12

On-site snow storage would occur in the landscaped areas shown on Figure 2.1-1 of the EA, excluding near Peters Creek, consistent with AMC 21.07. As stated in Section 2.1.6 of the Final EA, snow would be hauled off-site when necessary and disposed of at a permitted snow disposal facility, such as the Sand Lake Disposal Site in Anchorage or at a permitted snow disposal facility if a nearer one becomes available.

Response to Comment I46-13

Refer to Master Response 8.

Response to Comment I46-14

Refer to Master Response 4.

Response to Comment I46-15

Refer to Master Response 8. As stated in Section 2.1.5 of the EA, trees would be retained in the proposed parking and landscaping areas and along the perimeter of the Project Site.

Response to Comment I46-16

Comment noted. Salmonids in Peters Creek are discussed in Section 3.5 of the EA.

Response to Comment I46-17

Refer to Master Response 1. Refer to Section 3.15 of the EA regarding potential cumulative impacts associated with known growth and development in the area. NEPA does not require analysis of speculative future land uses.

Response to Comment I46-18

Refer to Master Response 8. As stated in Section 2.1.5 of the EA, trees would be retained in the proposed parking and landscaping areas and along the perimeter of the Project Site.

Response to Comment I46-19

Section 5 of the EA lists agencies and organizations, including websites and informational materials, that were consulted and reviewed during preparation of the EA, in accordance with 40 CFR § 1501.5(c)(3), which recommends including a list of agencies consulted during preparation of the EA. However, CEQ does not require coordination with specific agencies and organizations in this context, nor does it require every agency in a state be consulted with or mailed a NEPA document. Section 5 of the EA lists 21 agency sources that were reviewed or consulted during preparation of the EA. However, the EA was mailed to 28 agencies, tribes, and organizations.

Response to Comment Letter I47: Darryl Parks

Response to Comment I47-1

Refer to Master Response 7 and Master Response 8.

Response to Comment Letter I48: Mary Pemberton

Response to Comment I48-1

Refer to Master Response 6.

Response to Comment I48-2

Refer to Master Response 5.

Response to Comment I48-3

Refer to Master Response 8. Refer to Section 3.7 of the EA regarding potential impacts associated with problem gambling.

Response to Comment I48-4

Refer to Master Response 1 and Master Response 4.

Response to Comment I48-5

Refer to Master Response 5 and Master Response 7.

Response to Comment Letter I49: Tom Desalvo

Response to Comment I49-1

Refer to Master Response 1.

Response to Comment Letter I50: Pamela Carlson

Response to Comment I50-1

Refer to Master Response 1 and Master Response 8. Refer to Section 3.7 of the EA regarding potential impacts associated with problem gambling.

Response to Comment Letter I51: Russell Carlson

Response to Comment I52-1

Refer to Master Response 1. Refer to Section 3.7 of the EA regarding potential impacts associated with problem gambling.

Response to Comment Letter I52: Todd Russell

Response to Comment I52-1

Refer to Master Response 5.

Response to Comment I52-2

Refer to Master Response 6 and Master Response 8.

Response to Comment I52-3

Refer to Master Response 4.

Response to Comment I52-4

Refer to Master Response 1, Master Response 4, and Master Response 9.

Response to Comment I52-5

Refer to Master Response 4, Master Response 5, and Master Response 11.

Response to Comment Letter I53: Karen Hill

Response to Comment I53-1

Refer to Master Response 7 and Master Response 8.

Response to Comment Letter I54: Julie Jorlett

Response to Comment I54-1

Refer to Master Response 2 and Master Response 4.

Response to Comment I54-2

Refer to Master Response 6.

Response to Comment I54-3

Refer to Master Response 7.

Response to Comment I54-4

Refer to Master Response 5 and Master Response 11.

Response to Comment I54-5

Refer to Master Response 4 and Master Response 8.

Response to Comment Letter I55: Melissa Crandall

Response to Comment I55-1

Refer to Master Response 8.

Response to Comment I55-2

Refer to Master Response 6.

Response to Comment I55-3

Refer to Master Response 1.

Response to Comment I55-4

Refer to Master Response 1.

Response to Comment Letter I56: Steven Ellis

Response to Comment I56-1

Refer to Master Response 1, Master Response 4, Master Response 6, and Master Response 7.

Response to Comment I56-2

Refer to Master Response 5 and Master Response 9.

Response to Comment Letter I57: Tony LoMedico

Response to Comment I57-1

Refer to Master Response 5, Master Response 6, Master Response 8, and Master Response 11.

Response to Comment I57-2

Refer to Master Response 6 and Master Response 8.

Response to Comment I57-3

As stated in Section 2.1.9 of the EA, construction of the Proposed Project is estimated to commence in early 2025 and would continue approximately 13 to 15 months. The referenced construction work planned on the Glenn Highway is scheduled to begin in 2026 and would be completed in 2031, with work completed in 3 phases beginning in the Airport Heights neighborhood of Anchorage (far from the Project Site). Based on the construction timeline of the Proposed Project and the timeline of the phased construction work on the Glenn Highway, the two construction efforts would not overlap.

Response to Comment I57-4

Refer to Master Response 7, Master Response 8, and Master Response 10.

Response to Comment Letter I58: Kerri Jette

Response to Comment I58-1

Refer to Master Response 1.

Response to Comment I58-2

Refer to Master Response 6.

Response to Comment I58-3

Refer to Master Response 1.

Response to Comment I58-4

Refer to Master Response 1.

Response to Comment Letter I59: Judi Miller

Response to Comment I59-1

Refer to Master Response 6 and Master Response 8.

Response to Comment I59-2

As stated in Section 2.1.9 of the EA, construction of the Proposed Project is estimated to commence in early 2025 and would continue approximately 13 to 15 months. The referenced construction work planned on the Glenn Highway is scheduled to begin in 2026 and would be completed in 2031, with work completed in 3 phases beginning in the Airport Heights neighborhood of Anchorage (far from the Project Site). Based on the construction timeline of the Proposed Project and the timeline of the phased construction work on the Glenn Highway, the two construction efforts would not overlap.

Response to Comment I59-3

Refer to Master Response 6 and Master Response 7.

Response to Comment I59-4

Refer to Master Response 7.

Response to Comment I59-5

Refer to Section 3.5 of the EA regarding impacts associated with biological resources.

Response to Comment I59-5

Refer to Master Response 7.

Response to Comment Letter I60: Tiffani Loughman

Response to Comment I60-1

Refer to Master Response 5.

Response to Comment I60-2

Refer to Master Response 11. Refer to Section 3.10 of the EA regarding impacts associated with utilities.

Response to Comment I60-3

Refer to Master Response 6.

Response to Comment I60-4

Refer to Master Response 1 and Master Response 4.

Response to Comment Letter I61: Carol Perkins

Response to Comment I61-1

Refer to Master Response 2.

Response to Comment I61-2

Refer to Master Response 5.

Response to Comment I61-3

Refer to Master Response 5. Details regarding the proposed retaining walls are included in Section 2.1.6 of the Final EA and Section 3.1 of Appendix B of the Final EA.

Response to Comment I61-4

Refer to Master Response 5.

Response to Comment I61-5

Refer to Master Response 11. Refer to Section 3.2 of the EA regarding earthquakes.

Response to Comment I61-6

Refer to Master Response 6 and Master Response 7.

Response to Comment I61-7

Section 2.1.7 of the EA and Section 3.10 of the Final EA have been revised to clarify that Matanuska Electric Association would provide electrical service to the Project Site. Existing powerlines already occur on-site. Refer to Master Response 1. Refer to Section 3.15 of the EA regarding potential cumulative impacts associated with known growth and development in the area. NEPA does not require analysis of speculative future land uses.

Response to Comment I61-8

Refer to Master Response 1 and Master Response 7.

Response to Comment I61-9

Refer to Master Response 4.

Response to Comment Letter I62: Marc Denslinger

Response to Comment I62-1

Refer to Master Response 2.

Response to Comment I62-2

Refer to Master Response 5 and Master Response 11.

Response to Comment I62-3

Refer to Master Response 2.

Response to Comment Letter I63: Bryce Palmer

Response to Comment I63-1

Refer to Master Response 4, Master Response 5, Master Response 6, and Master Response 8.

Response to Comment Letter I64: Cecil Shuman

Response to Comment I64-1

Refer to Master Response 1. Refer to Section 3.7 of the EA regarding potential impacts associated with problem gambling.

Response to Comment I64-2

Refer to Master Response 6.

Response to Comment I64-3

Refer to Master Response 4. Refer to Section 3.7 of the EA regarding potential impacts associated with problem gambling.

Response to Comment I64-4

Refer to Section 3.7 of the EA regarding potential impacts and benefits associated with socioeconomic conditions.

Response to Comment I64-5

Refer to Master Response 5 and Master Response 11.

Response to Comment I64-6

Refer to Master Response 7.

Response to Comment I64-7

Refer to Master Response 5, Master Response 6, and Master Response 8.

Response to Comment I64-8

Refer to Master Response 4, Master Response 5, Master Response 6, Master Response 7, Master Response 8, and Master Response 11.

Response to Comment Letter I65: Jason Wells

Response to Comment I65-1

Refer to Master Response 5.

Response to Comment I65-2

Refer to Master Response 11.

Response to Comment I65-3

Refer to Master Response 6.

Response to Comment I65-4

Refer to Master Response 7.

Response to Comment I65-5

Refer to Master Response 8.

Response to Comment I65-6

Refer to Master Response 2.

Response to Comment I65-7

Refer to Master Response 1.

Response to Comment I65-8

Refer to Master Response 1 and Master Response 5.

Response to Comment Letter I66: John Abrams

Response to Comment I66-1

Refer to Master Response 5.

Response to Comment I66-2

Refer to Master Response 7.

Response to Comment I66-3

Refer to Master Response 1.

Response to Comment I66-4

Refer to Master Response 6 and Master Response 8.

Response to Comment I66-5

Refer to Master Response 1.

Response to Comment Letter I67: John Abrams

Response to Comment I67-1

Refer to Master Response 2.

Response to Comment I67-2

Refer to Master Response 5.

Response to Comment I67-3

Refer to Master Response 1 and Master Response 6.

Response to Comment I67-4

Refer to Master Response 9 and Master Response 11.

Response to Comment I67-5

Refer to Master Response 11.

Response to Comment I67-6

Refer to Master Response 7.

Response to Comment Letter I68: Sarah Belway

Response to Comment I68-1

Refer to Master Response 4, Master Response 6, and Master Response 8.

Response to Comment I68-2

Refer to Master Response 1. As discussed in Section 2.1.5 of the EA, although a small amount of tree removal (approximately 23.6 cords of birch) has occurred since the biological resources survey and tree survey, these trees are still considered part of the baseline for the environmental analysis in the EA. As also stated therein, the BIA determined that the value of the timber on the Project Site was below the \$5,000 threshold for requiring a tree harvest permit (25 CFR 163.26). Therefore, the BIA has determined that no timber harvest permit is required for the Proposed Project, nor was one required for the minor tree removal that has already occurred. The Project Site has been re-classified as not "forest land" or "Indian forest land", therefore, it is not subject to the permitting requirements of 25 CFR Part 163 and has been removed from BIA's Cook Inlet Forest Management Plan.

Response to Comment Letter I69: Valerie Jokela

Response to Comment I69-1

Refer to Master Response 2.

Response to Comment I69-2

Refer to Master Response 6.

Response to Comment I69-3

Refer to Master Response 4 and Master Response 5.

Response to Comment I69-4

Refer to Master Response 2.

Response to Comment Letter I70: Nicole Denslinger

Response to Comment I70-1

Refer to Master Response 1 and Master Response 8.

Response to Comment I70-2

Refer to Master Response 6, Master Response 7, and Master Response 8.

Response to Comment I70-3

Refer to Master Response 8.

Response to Comment I70-4

Refer to Master Response 8.

Response to Comment I70-5

Refer to Master Response 6.

Response to Comment I70-6

Refer to Master Response 6 and Master Response 8.

Response to Comment I70-7

Refer to Master Response 6.

Response to Comment I70-8

Refer to Master Response 9 and Master Response 11.

Response to Comment I70-9

Refer to Master Response 11.

Response to Comment I70-10

Refer to Master Response 7.

Response to Comment I70-11

Refer to Master Response 1. Refer to Section 3.7 of the EA regarding potential impacts associated with problem gambling and socioeconomics.

Response to Comment I70-12

Refer to Master Response 7.

Response to Comment I70-13

Refer to Master Response 1.

Response to Comment Letter I71: Alan Kurzynski

Response to Comment I71-1

Refer to Master Response 2.

Response to Comment I71-2

Refer to Master Response 6 and Master Response 8.

Response to Comment I71-3

Refer to Master Response 6.

Response to Comment I71-4

Refer to Master Response 11.

Response to Comment I71-5

Refer to Master Response 6, Master Response 7, and Master Response 8.

Response to Comment I71-6

Refer to Master Response 1 and Master Response 4.

Response to Comment Letter I72: Alan Kurzynski

Response to Comment I72-1

Refer to Master Response 2.

Response to Comment I72-2

Section 2.1.7 of the EA and Section 3.10 of the Final EA have been revised to clarify that Matanuska Electric Association would provide electrical service to the Project Site. Existing powerlines already occur on-site.

Response to Comment I72-3

Refer to Master Response 5.

Response to Comment I72-4

Refer to Master Response 4 and Master Response 8.

Response to Comment I72-5

Refer to Master Response 1.

Response to Comment I72-6

Refer to Master Response 1, Master Response 4, and Master Response 7.

Response to Comment I72-7

Refer to Master Response 6 and Master Response 8. Refer to Section 3.4 of the EA regarding potential impacts associated with air quality. Refer to Section 3.9 of the EA regarding potential impacts associated with airports.

Response to Comment I72-8

Refer to Master Response 5 and Master Response 8.

Response to Comment I72-9

Refer to Master Response 5. Details regarding the proposed retaining walls are included in Section 2.1.6 of the Final EA and Section 3.1 of Appendix B of the Final EA.

Response to Comment I72-10

Refer to Master Response 11.

Response to Comment I72-11

Refer to Master Response 5 and Master Response 6. The septic system would be registered with the USEPA under the Underground Injection Control program as a Class V injection well and designed and installed consistent with ADEC standards (ADEC 18 AAC 72 Wastewater Disposal, 18 AAC 72.530(e)(2)). The septic system would also be designed by a licensed engineer in such a way as to demonstrate its structural and thermal integrity, consistent with AMC 15.65.205.

Response to Comment I72-12

Refer to Master Response 6, Master Response 8, and Master Response 10.

Response to Comment I72-13

Refer to Section 3.7 of the EA regarding potential impacts associated with problem gambling.

Response to Comment I72-14

Refer to Master Response 7.

Response to Comment I72-15

Refer to Master Response 6.

Response to Comment I72-16

Refer to Master Response 3.

Response to Comment I72-17

Refer to Master Response 9.

Response to Comment Letter I73: Julee Faso-Formoso

Response to Comment I73-1

Refer to Master Response 1. The commenter expresses support for the Proposed Project.

Response to Comment Letter I74: Kimberly Collins

Response to Comment I74-1

Refer to Master Response 1.

Response to Comment I74-2

Refer to Master Response 9.

Response to Comment I74-3

Refer to Master Response 11.

Response to Comment I74-4

Refer to Section 3.10 of the EA regarding potential impacts on utilities.

Response to Comment I74-5

Refer to Master Response 6.

Response to Comment I74-6

Refer to Master Response 6 and Master Response 7.

Response to Comment I74-7

Refer to Master Response 6.

Response to Comment I74-8

Refer to Master Response 6.

Response to Comment I74-9

Refer to Master Response 6.

Response to Comment I74-10

Refer to Master Response 6.

Response to Comment I74-11

Refer to Master Response 8.

Response to Comment I74-12

Refer to Master Response 6.

Response to Comment I74-13

Refer to Master Response 6. As discussed in Section 2.1 of the Final EA, on-site snow storage would occur in the landscaped areas shown on Figure 2.1-1 of the EA, excluding near Peters Creek, consistent with AMC 21.07. As stated in Section 2.1.6 of the Final EA, snow would be hauled off-site when necessary and disposed of at a permitted snow disposal facility, such as the Sand Lake Disposal Site in Anchorage or at a permitted snow disposal facility if a nearer one becomes available.

Response to Comment I74-14

Refer to Master Response 2.

Response to Comment Letter I75: Letta Stokes

Response to Comment I75-1

Refer to Master Response 2.

Response to Comment I75-2

Refer to Master Response 1.

Response to Comment I75-3

Refer to Master Response 11.

Response to Comment I75-4

Refer to Master Response 9 and Master Response 11. Refer to Sections 2.1.7 and 3.10 of the EA, as well as Appendix C regarding proposed wastewater facilities. As discussed in Section 2.1.7 of the EA, wastewater would be treated via an on-site advanced secondary treatment system and disposed of through on-site septic facilities that would be appropriately sized. The septic system would be registered with the USEPA under the Underground Injection Control program as a Class V injection well and designed and installed consistent with ADEC standards (ADEC 18 AAC 72 Wastewater Disposal, 18 AAC 72.530(e)(2)). The septic system would also be designed by a licensed engineer in such a way as to demonstrate its structural and thermal integrity, consistent with AMC 15.65.205.

Response to Comment I75-5

Section 2.1.7 of the EA and Section 3.10 of the Final EA have been revised to clarify that Matanuska Electric Association would provide electrical service to the Project Site. Existing powerlines already occur on-site.

Response to Comment I75-6

Refer to Master Response 7.

Response to Comment I75-7

Refer to Master Response 5 and Master Response 9. ADF&G was mailed a copy of the EA. Details regarding the proposed retaining walls are included in Section 2.1.6 of the Final EA and Section 3.1 of Appendix B of the Final EA.

Response to Comment I75-8

Refer to Master Response 7.

Response to Comment I75-9

Refer to Master Response 6.

Response to Comment Letter I76: Douglas Johnson

Response to Comment I76-1

Refer to Master Response 2.

Response to Comment I76-2

Refer to Master Response 6, Master Response 7, and Master Response 9.

Response to Comment I76-3

Refer to Master Response 1. As discussed in Section 2.1.5 of the EA, although a small amount of tree removal (approximately 23.6 cords of birch) has occurred since the biological resources survey and tree survey, these trees are still considered part of the baseline for the environmental analysis in the EA. As also stated therein, the BIA determined that the value of the timber on the Project Site was below the \$5,000 threshold for requiring a tree harvest permit (25 CFR 163.26). Therefore, the BIA has determined that no timber harvest permit is required for the Proposed Project, nor was one required for the minor tree removal that has already occurred. The Project Site has been re-classified as not "forest land" or "Indian forest land", therefore, it is not subject to the permitting requirements of 25 CFR Part 163 and has been removed from BIA's Cook Inlet Forest Management Plan.

Attachment 1
Public Comment Letters

[EXTERNAL] Native Village of Eklutna Gaming Hall

From Kim Reitmeier <kim@ancsaregional.com>

Date Fri 1/3/2025 11:53 AM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

Cc TJ Presley <TJ@ancsaregional.com>; Delaney Thiele <delaney@ancsaregional.com>

📎 1 attachment (279 KB)

01.03.2025 ARA letter NVE Gaming Hall.pdf;

Greetings,

On behalf of the ANCSA Regional Association, we submit the attached letter of support for the Native Village of Eklutna's proposed gaming hall.

Quyanaa,
Kim



Kim Reitmeier

President

Email: kim@ancsaregional.com

Mobile: +1(907)952-0027





ANCSA REGIONAL
ASSOCIATION

January 3, 2025

The Honorable Bryan Newland
Assistant Secretary Bureau of Indian Affairs
U.S. Department of the Interior
1849 C Street NW
Washington, D.C. 20240

Re: Letter of Support for Proposed Eklutna Gaming Hall

Dear Assistant Secretary Newland,

We would like to thank you for your efforts in supporting the unique nature of self-governance in Alaska. Your steadfast leadership at the Bureau of Indian Affairs does not go unnoticed and is appreciated.

The ANCSA Regional Association (ARA) supports the Native Village Eklutna (NVE) in its efforts to move forward with a gaming hall. As you know, ARA represents the presidents and chief executive officers of the twelve land-based Alaska Native regional corporations that were created pursuant to the passage of the Alaska Native Claims Settlement Act (ANCSA) of 1971. ARA's member corporations are owned by more than 160,000 Alaska Native people, termed 'shareholders' through ANCSA. Its purpose is to promote and foster the continued growth and economic strength of Alaska Native corporations on behalf of Alaska Native shareholders, descendants, and communities. Together, ANCSA corporations employ tens of thousands in Alaska and across the world, providing critical support for national industries as well as communities throughout Alaska.

We believe the tribal gaming hall proposed by NVE is an opportunity for economic development for the Alaska Native people of Eklutna, along with others across the region. As an Alaska Native-led project, the gaming hall will foster economic self-determination, allowing NVE to be an economic driver in its community. Economic development opportunities not only strengthen tribal sovereignty but also provide economic benefits to the broader community. When laws and policies permit Alaska Native people to control their own destiny, Alaska Native people can build strong, locally grown economies that provide enduring benefits.

The economic impact analysis in the Environmental Assessment appropriately highlights the many benefits of the proposed project. For instance, the project is expected to inject \$67.6 million into our state economy each year. During the construction phase alone, the project is anticipated to add \$45.8 million of value to the state economy.

The proposed gaming hall is expected to employ approximately 230 workers at a given time. This significant addition of new employment opportunities will be a boon to the local area. Many of these jobs may be filled by Eklutna tribal members, giving them long-overdue opportunities for economic advancement, while also providing employment for others in the surrounding area.

According to the most recent federal census, 30% of the residents of the Village live below the poverty line. That statistic contrasts sharply with the statistics for Anchorage as a whole, where just 9.8% of people live below the poverty line. Similarly, only 30% of the Village's residents are currently employed, compared to 59.8% of

A1-1

Anchorage residents. The job opportunities and economic revenues generated by the proposed project could have life-changing impacts for the people of NVE.

Based on the thorough Environmental Assessment conducted by the BIA, we see no significant adverse impacts that would come from this development. On the other side of the scale, the benefits for Eklutna and the region will be extraordinary.

We encourage you to approve the business lease that would pave the way for this project.

Sincerely,



Kim Reitmeier
President
ANCSA Regional Association

A1-1
cont.

[EXTERNAL] EA Comments on Eklutna Native Village Gaming Facility Project from MOA Mayor Suzanne LaFrance

From Mayor LaFrance <Mayor@anchorageak.gov>

Date Fri 1/3/2025 2:32 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

Good afternoon.

The Municipality of Anchorage values and respects our government-to-government relationship with the Native Village of Eklutna.

The Native Village of Eklutna's goal to develop a gaming facility on its land is clearly within its right to facilitate self-sufficiency, self-determination, and economic development under the Indian Gaming Regulatory Act.

The Native Village of Eklutna has a long history of excellent stewardship of the land and water in the Municipality, including salmon streams, and the project's Environmental Assessment reflects that, raising no significant concerns.

The Environmental Assessment also shows the project will create shared economic benefits, including more than 450 jobs during construction that will add \$45.8 million to our local economy. Once operating, the project will support 419 workers and generate \$67.6 million in economic value. Positive economic spill-over effects would boost surrounding businesses, hotels, and workers.

We'd like to formally share our support for the Native Village of Eklutna as it exercises its sovereignty and pursues economic development.

Mayor Suzanne LaFrance
Municipality of Anchorage
632 W 6th Ave, Suite 840
Anchorage, AK
99519

A2-1



Assembly Chair

January 5, 2025

The Honorable Christopher Constant, Chair
Anchorage Assembly
PO Box 196650
Anchorage AK 99519-6650

The Honorable Bryan Newland
Assistant Secretary Bureau of Indian Affairs
U.S. Department of the Interior
1849 C Street NW
Washington, D.C. 20240

Letter of Support for Proposed Eklutna Gaming Hall (DOI-BIA-AK-61628)

Dear Assistant Secretary Newland,

I write to support Eklutna Native Village Gaming Facility Project, (DOI-BIA-AK-61628), Native Village of Eklutna's (NVE) proposed development of a modest gaming hall on tribal land within the Municipality of Anchorage. This initiative has the potential to improve historic environmental justice harms by bringing much-needed revenue to the Tribe and generating significant economic opportunities for its members and the Municipality of Anchorage.

With poverty and unemployment rates far higher than the municipal average, NVE members (and the community as a whole) stand to benefit profoundly from this project through improved infrastructure, better employment opportunities, stronger economic activity in general, as well as increased health outcomes and longevity for tribal members. This project will promote self-sufficiency, self-determination, and economic development for the Eklutna Dena'ina people.

I write to you as the presiding officer of the Anchorage Assembly, the Chair. But I need to explicitly state that this opinion is my own and should not be construed as the position of the Assembly as a body. There are differences of opinion among the members, but no official action has been before the body, so the Assembly currently has no opinion. That said, I have shared these comments and discussed this matter with Anchorage Mayor Suzanne LaFrance, and find we are in close alignment. With those caveats, please consider my detailed comments.

The history of the Dena'ina people, the ancestors of NVE, is deeply intertwined with the region now known as Anchorage. When the United States acquired Alaska as a territory, the Dena'ina were the predominant population in the area, numbering in the thousands, since time immemorial. However, events such as the construction of the Alaska Railroad, which established Anchorage as a hub at the Knik River, and the devastation of two pandemics, a boarding school sited in the village that systematically stripped culture, language, and heritage

from Native children¹, and a massive wave of immigrant settlers that dispossessed them of their traditional lands; their population has been reduced to mere scores of people.

An exceptional and detailed description of this history is available in a report titled *“Overview of Dena’Ina Athabascan Uses of sites on and near Elmendorf Air Force Base,”*² part of an environmental assessment conducted during a waste removal project on Elmendorf Air Force Base in the early 2000s. It is a very interesting and informative read that I highly recommend.

These historical upheavals mark the origin of the socio-economic challenges that continue to affect the community to this day. Like many Native communities across the United States, NVE faces systemic issues such as poverty, addiction, and limited economic opportunities. It is important to note though that these conditions currently exist broadly in Anchorage today across many demographics and geographic boundaries, not just among the disparately impacted Native peoples of Anchorage.

The Native Village of Eklutna doesn’t have its head in the sand about these current conditions in Anchorage. To tackle some of these challenges, NVE has proactively supported the creation of a community health clinic offering high quality healthcare at the village site³ They have also partnered to create a residential drug and alcohol treatment center on their land in close proximity to the proposed project location.⁴ They have been working for generations to mitigate these harms and in providing solutions for their members, they have provided opportunities for the community at-large through generous land grants for public facilities like Anchorage schools and local parks for example.⁵ Now they seek to operate a gaming hall on their tribal land, and I support them.

Interestingly, research shows that Native communities with gaming establishments experience transformative benefits. Tribes operating their own gaming facilities report higher rates of employment, improved community infrastructure, and better health outcomes, including reduced early morbidity and increased life expectancy.⁶ For NVE, this proposed gaming hall represents more than an economic opportunity; it is a chance to revitalize the community and ensure the long-term survival of the Eklutna Dena’ina people.

So what qualifies me to assert this opinion?

As I stated above, I am the presiding officer of the Anchorage Assembly, and I have served in elected office for nearly 8 years and have studied and overseen the infrastructure, public

¹ https://iseralaska.org/static/legacy_publication_links/boardingschoolfinal.pdf

² Debris Removal Actions at LF04 Elmendorf AFP, Alaska, Final Report, April 10, 2023 (Pages 46-104) https://dec.alaska.gov/Applications/SPAR/PublicMVC/CSP/Download?documentID=36472&fileName=1804_20030410%20Debris%20Removal%20Actions.pdf

³ <https://www.southcentralfoundation.com/services/primary-care-clinics/eklutna-village-clinic/>

⁴ <https://www.ciri.com/shareholder-news/new-ernie-turner-center-opens/>

⁵ https://cdn.ancgis.com/documents/landrecords/landagreements/nala/NALA_19830120_Amendments.pdf

⁶ <https://www.nber.org/brd/20241/mortality-effects-casinos-native-americans>

health, economics, and financial wellbeing of our city closely. I sit on a number of Assembly committees including:

- Community & Economic Development Committee
- Transportation Committee
- Budget and Finance
- Infrastructure, Enterprise & Utility Oversight Committee

And in my professional life, I have spent a career serving as a substance use disorder treatment professional in Anchorage. I present this broad background to demonstrate that I understand the profound impacts this project may have on our community, both positive and negative.

As a legislator, one of my finest acts was to fund, draft, and enact Anchorage Ordinance AO 2020-137(S), establishing in our code that the Municipality of Anchorage formally recognizes the sovereignty of all 229 federally recognized Alaska tribes and establishes a direct government to government relationship with Native Village of Eklutna.⁷ NVE enjoys unique status as the only federally-recognized Tribe whose homelands and historic territories are within the Municipality of Anchorage. It is through this frame that I recognize the proposed gaming hall project as an important exercise of self-determination by the Tribe.

As described in the *Eklutna Native Village Gaming Facility Project Environmental Assessment*⁸, this project is expected to create approximately 230 direct jobs and 420 total jobs, adding an estimated \$67.6 million in value to the local economy. The proposal will create a gaming hall to operate Class II games, as defined by National Indian Gaming Commission which oversees the implementation of the Indian Gaming Regulatory Act (IGRA).⁹ It is worth noting that Class II gaming already exists broadly across Alaska and in Anchorage, specifically. The only difference is that NVE is proposing to use electronic pulltabs rather than traditional paper game pieces, which is not currently allowed under State law, but is allowed under IGRA on Indian land. It is anticipated that the operation will create a number of economic benefits. And those benefits will extend beyond the tribe who owns the hall or the people who employed there. It will provide a ripple effect of economic growth throughout Anchorage.

According to the Alaska Charitable Gaming Alliance, the industry already generates annual gross receipts more than \$375,000,000, with 66% derived in the Anchorage area. This amounts to more than \$250,000,000 of direct economic activity annually in the Municipality. That doesn't even begin to scratch the surface of total economic activity by this industry. It's important to understand the scope of the gross receipts because it clearly demonstrates that gaming currently exists and is a deeply embedded economic activity in Anchorage. I also note that Alaska gaming laws allow operators to pay expenses before net proceeds to charities; thus, according to Alliance statistics, the highly profitable industry is able to pay itself nearly twice what it pays out to beneficiaries. The main point is that this industry is not new to Anchorage

⁷ <https://www.muni.org/Departments/Assembly/Pages/Native-Village-of-Eklutna.aspx>

⁸ <https://eklutnaea.com/>

⁹ <https://www.nigc.gov/commission/faqs-detail/what-is-the-difference-between-class-ii-and-class-iii-gaming>

and is highly profitable.¹⁰ And yet the Alliance opposes NVEs proposal and has for years.¹¹ What would drive opposition?

The currently existing gaming halls are operations owned by a handful of powerful families and corporations. What is the real difference between their operations and NVE's proposal? Well, this project would be Alaska Native/American Indian owned and operated to the benefit of their tribal members. According to a recent study published by The Center for Gaming Research, "Indian gaming is governmental gaming...To paint a negative picture of Indian gaming is another attempt to undermine Indian self-determination and self-sufficiency."¹²

That said, it is important to grapple with the issues that have been raised. The objections I have heard in the community come in three flavors.

One domain of concern is that the operation will bring too much traffic, noise, and water impacts for residents of the area.¹³ These arguments are specious, at best. The proposed location is directly adjacent to a general aviation airfield and two outdoor shooting ranges: The project site borders the Birchwood Airport (PABV), the Anchorage Police Department Shooting Range, and the Birchwood Recreation and Shooting Range—all of which generate substantial noise.¹⁴ Furthermore, the Municipality has long committed to improving water infrastructure for the area to improve housing opportunities.¹⁵ So in the long run, if water concerns prove substantiated, there is a plan for that.

Second, at least one member of the Anchorage Assembly has reasonably stated opposition to the proposal, presenting two studies opposing gaming generally on public health grounds.^{16 17} These social concerns are important to grapple with. As a municipal government, we certainly do face challenges relating to addiction, mental health, and crime, head on. But as previously described, NVE has been confronting these issues directly for years through improvements in healthcare and substance use treatment infrastructure. And as previously discussed, long-term studies indicate a counterintuitive effect of Indian gaming. When tribes operate gaming facilities, tribal members live longer! The people of the Native Village of Eklutna don't need lectures about the social ills tied to poverty—they live it, generation after generation. They understand these challenges intimately and are working tirelessly to create opportunities that break the cycle and uplift their community, infant to elder. This proposal is one strategy NVE is implementing toward that goal. As it stands currently, with these games allowed throughout

¹⁰ <https://alaskacharitablegamingalliance.org/statistics/>

¹¹ <https://alaskacharitablegamingalliance.com/media/>

¹² https://digitalscholarship.unlv.edu/cgi/viewcontent.cgi?article=1043&context=occ_papers

¹³ <https://www.alaskasnewsresource.com/2024/12/19/lawsuit-over-proposed-eklutna-casino-filed-over-fears-alleged-noise-water-impacts-chugiak-neighborhood/>

¹⁴ Interestingly, in a Birchwood Airport Master Plan Update currently under FAA review, only one line of an 18-page public comment table mentioned noise as a concern, despite the presence of these facilities. <https://dot.alaska.gov/creg/birchwoodamp/docs/Draft-BCV-AMP-022324-Public-Review.pdf>

¹⁵ <https://www.adn.com/business-economy/2023/11/12/large-housing-development-may-be-coming-to-eagle-river-after-deal-between-city-and-eklutna-inc/>

¹⁶ <https://www.networkforphl.org/news-insights/the-often-unrecognized-public-health-impact-of-gambling/>

¹⁷ <https://www.nbcnews.com/health/health-news/gambling-industry-growth-threat-public-health-report-rcna175356>

the municipality, no rational basis exists to lock out NVE from receiving any of the financial benefits that could enable them to better address these. It is in the Municipality's interest to support it.

The third concern I have heard is the most absurd of them all. The so-called Alaska Charitable Gaming Alliance has opposed the Native Village of Eklutna's now proposed gaming operation for many years. Over that time, concerns have shifted from protecting charities, to reducing addiction, crime, drugs, and abuse caused by gambling to, at one point stating their main concern was that the threat would be "gaming floors...filled to bursting with beeping and blinking class II machines."¹⁸ While fear of bursting and beeping noises is an absurd argument, if their concerns about the harms of gambling are true, the gaming operations currently underway by their members are currently creating those harms. Do they really mean to suggest it is fair for their members to generate those harmful impacts because of the social good they create, but not Native Village of Eklutna? There is a name for that type of standard.

It is my opinion that their ever-changing carousel of concerns all boil down to the protection of huge profits generated through a monopolistic hold on gaming halls. Self-interested concerns can and should be dismissed outright.

In sum, Eklutna's strong commitment to its community's health and the protection of its natural resources ensures that the project is designed to eliminate any significant adverse environmental impacts. Having worked closely with NVE, I know the Tribe will be a strong partner with the Municipality as this project unfolds. I look forward to further collaborating, if and when appropriate.

The resilience and perseverance of the Eklutna Dena'ina people have brought them to this point. They survive as a people. But now, it's time to help them thrive. This gaming hall represents a critical lifeline for cultural preservation and community revitalization. By improving economic opportunities, enhancing quality of life, and fostering cultural resilience, this initiative will have a transformative impact on the environment and the community as a whole. As a practice of self-reliance, self-determination, and economic development, the people of the Native Village of Eklutna propose a gaming hall. I support them.

I urge the Department of the Interior to issue the necessary approvals to allow the Native Village of Eklutna to move forward with this project with all due haste.

Respectfully,



Christopher Constant, Chair
Anchorage Assembly

¹⁸ <https://alaskacharitablegamingalliance.com/media/>

[EXTERNAL] FW: Environmental Assessment Notice

From Chugiak Council <chugiakcouncil@gmail.com>

Date Mon 1/6/2025 4:27 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

Cc Community Councils Center <info@communitycouncils.ccsend.com>; Blake Merrifield <blakemerrifield@hotmail.com>; Jacob (Jake) Horazdovsky <Jake.Horazdovsky@respec.com>; Assembly District 2 - Scott Myers <Scott.Myers@anchorageak.gov>; Littlefield, Mark <Mark.H.Littlefield@anchorageak.gov>; Rashae Johnson <rashaeoj@gmail.com>; Scott Myers <scott@scottak.com>; Thomas Wonnell <burkewonnell@aol.com>; Paul Schneider <schneiderp@gci.net>

1 attachment (59 KB)

3334_001.pdf;

Ms. Yazzie,

As you can see by the email forwarding, our Community Council only just became aware of this EA today, Jan 6. Jan. 6 also happens to be your assigned closing day for comment. I believe it is disingenuous to set a comment period of 15 days and then assign it over the Christmas and New Years holidays.

Most Community Councils only meet once a month, with Chugiak CC meeting the third Thursday of each month. For us to notify our members and allow them time to comment generally takes approximately 30 days at a minimum. Is it possible to get the comment period extended out to the middle of February to give adequate opportunity for comment? Our Council will meet on 16 Jan.

Darryl Parks
President
Chugiak Community Council
805-348-9019

A4-1

From: Info <Info@communitycouncils.org>
Date: Monday, January 6, 2025 at 11:40 AM
To: Chugiak CC - Darryl Parks <chugiakcouncil@gmail.com>



THE STATE
of **ALASKA**
GOVERNOR MIKE DUNLEAVY

Department of Fish and Game

Division of Wildlife Conservation
1255 West 8th St.
PO Box 115526
Juneau, Alaska 99811-5526
Main: 907.465.4190
Fax: 907.465.6142

A5

January 9, 2025

Jolene John
Regional Director Bureau of Indian Affairs - Alaska Region
3601 C Street, Suite 1200
Anchorage, AK 99503

RE: Eklutna Native Village Gaming Facility Project EA (DOI-BIA-AK-61628)
Submitted via email to Alaska_NEPA@bia.gov

Dear Ms. John:

The Alaska Department of Fish and Game (ADF&G) reviewed the Environmental Assessment (EA) for the Eklutna Native Village Gaming Facility Project Environmental Assessment (EA). This proposal considers developing approximately a 6.37-acre portion of a restricted fee Native Allotment (Project Site) owned by members of the Eklutna Tribe in the unincorporated community of Chugiak within the boundaries of the Municipality of Anchorage, Alaska. Approval by the Secretary of the Interior or delegee of a business lease between the owners of Allotment A-055026 (lessor -- project site/Ondola Allotment) and the Eklutna Native Village (lessee; tribe) is also required.

The Development will include a 58,000 square-foot (sf) gaming facility with paved surface parking areas and supporting wastewater and other infrastructure. The gaming facility would also include two food and beverage venues totaling 7,000 sf with up to 200 seats. Proposed loading docks to accommodate deliveries would be located behind the building. The gaming facility would be open for up to 24 hours a day, 7 days a week. The preferred alternative (Alternative A) is estimated to take 13 to 15 months to construct and would open in the year 2026. Alternative A would employ approximately 90 staff per day with a total employment of approximately 228 staff.

Access to the proposed gaming facility would occur along an existing driveway that connects to the Birchwood Spur Road (owned by the Alaska Department of Transportation). This driveway, as well as a parking area designed to hold 443 park spaces would be paved. Runoff from construction activities and the paved area would comply with an Alaska Department of Environmental Conservation stormwater pollution prevention plan as well as with the Anchorage Stormwater Manual, Volume 1 Management and Design Criteria, Version 1.0 (Municipality of Anchorage, 2017a). The parking lot and areas around the building would be enhanced by landscaping that would incorporate native vegetation. The northeast corner of the lot would be raised by three to seven feet above the existing grade due to the steep drop off towards Peters Creek.

Septic facilities would include an on-site grease interceptor tank, 10,000 – 15,000-gallon septic tank, a treatment tank, an advanced secondary treatment system, and a septic drainage field that would be installed below the paved parking areas within the Project Site. During operation of the interim

facility, wastewater would be self-contained within holding tanks and hauled from the Project Site to the King Street commercial septage receiving station for treatment and disposal at the Eagle River Wastewater Treatment Facility owned and operated by the Anchorage Water and Wastewater Utility (AWWU).

ADF&G supports development projects that protects fish and game and aquatic plant resources. We provide the following comments.

Section 3.5 Biological Resources

- Please delete the Alaska National Interest Land [sic] Conservation Act (ANILCA) from Table 3.5-1: Regulatory policies and Plans Related to Biological Resources. While the included description is accurate to the level it addresses, it neglects to inform the reader that given the project’s location in Chugiak, Alaska, which is within the Municipality of Anchorage, the project area, meets the definition of nonrural under the federal subsistence regulations. The nonrural communities are designated in the Federal Subsistence Board regulations at 36 CFR 242.15 and 50 CFR 100.15. ANILCA sought specifically to ensure that the opportunity to carry out subsistence uses, and the Alaskan subsistence way of life could continue for Alaska’s rural residents.

A5-1

- Under the State and Local section of Table 3.5.1, please revise the responsibilities held by the Alaska Department of Fish and Game. As shown below by underline and strikeout:
 - ~~The state manages~~ The Alaska Department of Fish and Game (ADF&G) has primary responsibility for managing Alaska’s fish and resident wildlife populations on all lands, including Federal public lands, this includes management of subsistence use of fish and wildlife on State public lands and privately held tribal lands (including Alaska Native Corporation lands).
 - ADF&G’s regulations require the appropriate management of human or animal food or garbage in a way that avoids attracting animals. See Alaska Administrative Code (AAC) 92.230(a)(1).

A5-2

Please include an additional subsection for wildlife in Section 3.5.2 Environmental Setting. Under this subsection, the final EA should address both moose and bears (both brown and black) which traverse the project area. Peters Creek is a salmon bearing stream and seasonally concentrates both brown and black bears along the creek corridor. Currently, ADF&G receives many wildlife conflict calls in the Peters Creek area and this development will produce considerable waste. If not secured correctly this could create substantial issues with habituated wildlife which would likely spill over into the surrounding residential areas.

A5-3

- Subsection 3.5.3 Assessment Criteria. The assessment in the Final EA should address potential impacts to biological resources and then, based on that analysis, determine if any significant impacts may occur.

A5-4

- Subsection 3.5.3 should address potential impacts, including those shown below, to moose, black and brown bears, and salmon under General Fish and Wildlife and Sensitive Habitats heading.

A5-5

- Project development activities during calving season could negatively impact wildlife with newborns.
- Peters Creek is a salmon bearing stream and seasonally concentrates both black and brown bears feeding along the creek corridor.
- Improperly stored garbage can serve as an attractant to bears and other wildlife.
- Increased traffic on North Birchwood Road and Birchwood Spur Road could lead to an increase in wildlife-vehicle collisions. Mitigation measures to improve visibility along the roadways, such as lighting and vegetation clearing should be considered as part of the project development plan.

A5-5
cont.

Section 3.10.2 Public Services and Utilities, Recreation

- As mentioned earlier, Peters Creek is a salmon bearing stream and likely has resident species as well. Peters Creek is open to salmon fishing (except King Salmon) at the project location. The proposed project could lead to an increase in effort by anglers. ADF&G would monitor such an increase if it were to occur.

A5-6

Section 4 Mitigation Measures

- Please add a reference to Table 4-1: Mitigation Measures, Biological Resources for Bears and other wildlife.
 - Please add a bullet stating: *Project activities should take precautions to minimize impacts along the creek during spring calving times and when fish are running.*
 - Please add a bullet stating: *The project proponent will ensure, both during construction and during facility operation, that bears and other wildlife are not drawn onto the property and in direct contact with people. ADF&G strongly recommends the use of bear proof trash receptacles to prevent this from happening.*
 - Please add a bullet stating: *The project proponent will take precautions to minimize wildlife-vehicle collisions in the project's vicinity.*

A5-7

Public Outreach/Agency Consultation

- Because the Environmental Assessment (EA) was released for a 15-day review period over the Christmas and New Year holidays, we question whether an adequate opportunity for public review occurred. Additionally, while we recognize ADF&G information was collected from our website, neither our wildlife or sportfish area biologists, nor our Habitat Division were contacted regarding potential project impacts. Biologists' comments are incorporated into this letter. We suggest releasing the EA for an additional 30-day public comment period.

A5-8

Closing

We encourage the BIA and Eklutna Native Village to discuss necessary permits and compliance with State resource agencies.

A5-9

If any work occurs below the ordinary high water of Peters Creek occurs, a fish habitat permit will be needed from our Habitat Division. Please contact Megan Marie in the ADF&G Habitat Division

office in Anchorage at (907) 267-2446 or via email at megan.marie@alaska.gov for any questions regarding any potential fish habitat permits.

A5-9
cont.

Please contact me at 907-269-2242 or jennifer.nolanwing@alaska.gov if you have any questions or to discuss any of these issues.

Sincerely,

Jennifer Wing
Acting ANILCA Program Manager

cc: Cyndi Wardlow, Regional Wildlife Coordinator, ADFG
Kevin Taylor, Chair, Anchorage Fish and Game Advisory Committee

*Jennifer Nolan Wing
Manager (acting)
ANILCA Program/State-Federal Issues
ADF&G, DWC
333 Raspberry Road
Anchorage, Alaska 99518
jennifer.nolanwing@alaska.gov*

Office: 907.267.2242



January 9, 2025

Jolene John, Regional Director
Bureau of Indian Affairs – Alaska Region
3601 C Street, Suite 1200
Anchorage AK, 99503

[Sent Electronically]

Re: Eklutna Native Village Gaming Facility Project Environmental Assessment Review
Comments

Dear Ms. John

The Alaska Department of Transportation and Public Facilities (DOT&PF) has done a cursory review of released EA for the proposed Eklutna Native Village Gaming Facility. Due to the limited comment period and timing of release a more in-depth review could not be completed. Currently DOT&PF has the following comments:

- Birchwood Spur Road is a DOT&PF managed road. Any new access or any current access onto Birchwood Spur Road with a change in use of the property (i.e. building a gaming facility) will require approval and permitting by DOT&PF. Please contact DOT&PF Central Region ROW Staff at 1-907-269-0700 or the Regional Traffic and Safety Engineer at 1-907-269-0700 to initiate the process.
- The Traffic Impact Analysis (TIA) included in the EA was not submitted to DOT&PF for review and comment as required by 17 ACC 10.070.
- Ensure that any proposed facility infrastructure or construction equipment heights do not conflict with FAA Part 77 surfaces for the Birchwood Airport.
- Birchwood Spur Road north of the Alaska Railroad Corporation (ARRC) tracks is within airport property and is likely subject to the provisions of FAA sponsor grants. Any proposed impacts or modifications to the road must be coordinated with the Airport Sponsor (DOT&PF) and FAA.

A6-1

A6-2

Sincerely,

DocuSigned by:

Melanie Arnolds

0B18FAB5B16B4B9...

Melanie Arnolds P.E., PMP, MSPM
Central Region Right-of-Way Chief
CR Design & Engineering Services

cc:

Sean Baski, P.E., Highway Design Group Chief, DOT&PF
Matt Walsh, SR/WA Property Management Supervisor CR, Right of Way, DOT&PF
Luke Bowland P.E., Central Region Preconstruction Engineer, DOT&PF
Anna Bosin P.E, Traffic & Safety Engineer, DOT&PF
Ben White, Project Delivery, CR Planning Chief, DOT&PF
Sean Holland P.E., Central Region Director, DOT&PF
Kayce Eliason, Anchorage District Superintendent M&O, DOT&PF
Kirk Warren P.E., CR M&O Chief, DOT&PF
Steven Rzepka P.E., CR Aviation Design, DOT&PF
Brian Elliot, Central Region Environmental Manager DOT&PF



October 7, 2024

Aaron Leggett
President
Native Village of Eklutna
26339 Eklutna Village Rd.
Chugiak, AK 99568

Re: Letter of Support for Proposed Eklutna Gaming Hall

Dear President Leggett and Native Village of Eklutna Tribal Council:

On behalf of Cook Inlet Region, Inc. (CIRI), I write to extend our support for the Native Village of Eklutna's proposed gaming hall project. This initiative represents a significant opportunity for economic growth and a strong affirmation of the Tribe's sovereignty and right to self-determination.


CIRI is one of the 12 land-based Alaska Native regional corporations, established under the terms of the Alaska Native Claims Settlement Act (ANCSA) of 1971. The Native Village of Eklutna is one of eight CIRI-affiliated Tribes within the Cook Inlet region and CIRI regularly works with the Native Village of Eklutna on initiatives that benefit Alaska Native peoples and communities in the region.

Just as CIRI utilizes our business segments to provide benefits to our Shareholders and Descendants, we commend the Native Village of Eklutna's ongoing efforts to pursue economic initiatives that create jobs and provide funding for essential services for Tribal members, such as housing, health care, scholarships, and cultural programs.

The proposed gaming hall offers a promising path forward for the Native Village of Eklutna. We look forward to seeing this project progress.

Sincerely,

Cook Inlet Region, Inc.

DocuSigned by:

63EAC69F9883433...
Sarah Lukin
President

A7-1



Native Village of Eklutna
Tribal Government

January 9, 2024

Jolene John, Regional Director
Bureau of Indian Affairs, Alaska Region
3601 C Street, Suite 1200
Anchorage, AK 99503

Alaska_NEPA@bia.gov

Re: Native Village of Eklutna Comments on Environmental Assessment (DOI-BIA-AK-61628) dated December 2024 for the development of a Cass II Gaming Facility

Dear Regional Director John:

On behalf of the Native Village of Eklutna (“NVE” or “Tribe”), I am pleased to submit these comments on the Environmental Assessment for the Eklutna Native Village Gaming Facility Project Dated December 2024 (“EA”).

These comments are submitted in the Tribe’s capacity as a cooperating agency. The Tribe has adopted a number of laws relevant to the analysis in the EA. For example, the NVE Environmental Protection Ordinance (Resolution Number 97-22) prohibits contamination of the air, land and/or water within Native Village of Eklutna’s traditional jurisdictional area and requires a tribal permit for any commercial activity with a potential for such contamination. NVE Ordinance No. 04-001 Prohibiting the Discharge of Any Pollutant prohibits discharges of pollutants into waters of Eklutna lands. The Gaming Ordinance of 2024 requires that the construction, maintenance and operation of the Gaming Facility and the Gaming Operation be conducted in a manner that adequately protects the environment and the public health and safety and complies with the requirements and all applicable health, safety and environmental standards enacted by the Tribe. The Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance, Ordinance No. 2007O-01, as amended by Ordinance 2025O-01 (“Public Health and Safety Ordinance”) requires that the Gaming Facility comply with stringent rules governing the construction, maintenance and operation of the Gaming Facility to protect the environment and the public health and safety. While the Public Health and Safety Ordinance was initially enacted in 2007, the Tribe updated it on January 7, 2025 to address issues raised by the Bureau of Indian Affairs through its development of the EA. The Tribe has reviewed the EA in light of those laws and its role as regulator of the Gaming Facility through the Tribal Gaming Commission and its role as project developer.

The Tribe appreciates the thorough analysis undertaken by the Bureau of Indian Affairs in the EA and believes the EA accurately reflects the anticipated potential impacts of the proposed project. The Tribe offers these comments to address minor points of clarification and provide additional information where relevant.

T1-1
cont.

1.2 Background

As accurately described in the EA, the Tribe desperately needs economic development opportunities to support our members and our Tribe as a whole. According to the most recent federal census, the poverty rate among our Village residents is more than three times higher than the rate for Anchorage as a whole, and the *employment* rate within our Village is just 30%. Against such a stark backdrop, the economic growth and job opportunities that would be generated by the proposed project would be transformational for our tribal community.

The proposed project would also be transformational for the landowners, all of whom are tribal members, and many of whom are elders and have waited for decades to see this project come to fruition. Two of the children of the original allottee and two of her grandchildren, all of whom were landowners on the allotment, have passed away while they waited for this economic development on their property. Most of the current landowners are retired and live on limited income, and some have significant health concerns. The income from this property will ensure that they are able to pay for their living expenses, including health care. The landowners have worked in close coordination with the Tribe and are eager to support the tribal community in this way, while also obtaining important revenue for their families.

T1-2

As a minor comment, the Tribe notes that in section 1.2.3, the reference to Anchorage Municipal Ordinance No. 2020-137(S) should include the final "(S)" in the ordinance number.

3.3 Water Resources

The Tribe has stewarded and protected its natural resources for millennia, and water is among our most precious resources. In recent years, the Tribe has been instrumental in advocating for and implementing a restoration project for the Eklutna River, including the recent removal of a dam to begin restoring in-stream flows for salmon. The Alaska Center honored the Native Village of Eklutna and NVE President Aaron Leggett with a 2024 Annual Award in recognition of this river restoration work.

T1-3

As described in the EA, the Tribe has designed its proposed project with extreme care to avoid impacts to water resources, including nearby Peters Creek. The Tribe has revised the construction plans to increase the setback from Peters Creek to 50 feet (increased from a previous setback of 25 feet) in order to provide greater protection of the water resources of Peters Creek in the area.

3.6 Cultural and Paleontological Resources

As the EA reflects, the project site is important for our Tribe but does not contain historic properties within the scope of the National Historic Preservation Act. In Table 3.6-2, the EA notes that the last building standing as part of the Ondola family residence was "in the process of disassembly." The Tribe notes that this final building was not a historic property, as it was constructed around 2008 after the previous structure had burned down. The building had fallen into disrepair and was no longer habitable. The Ondola family therefore chose to remove the home; the "disassembly" described in the EA was done by the family.

T1-4

Because the building posed a hazard, the family’s decision to remove it was entirely unrelated to the proposed lease with the Tribe and was not part of the federal action encompassed by the EA.

The EA also notes that there are several culturally-modified trees on the project site. These are trees with small portions of bark that had been removed in modern times. These trees are not unique or distinguishable for purposes of the NHPA, and the Tribe agrees with the conclusion in the EA that these trees do not meet the criteria for listing in the National Register of Historic Places.

T1-4
cont.

3.8 Transportation and Circulation

The analysis in the EA accurately describes the potential traffic impacts of the proposed project. The Tribe also notes that Birchwood Loop Road and Birchwood Spur Road (which together access the project site) have long been a subject of community discussion. Both the Tribe and local residents have called for improvements to these roads. While the Tribe believes that the proposed project will not create any significant new traffic safety concerns, the Tribe will work with appropriate authorities to improve the relevant road segments and make them even safer for all users.

T1-5

Both Birchwood Loop Road and Birchwood Spur Road are identified in the Tribe’s inventory of routes in the National Inventory of Tribal Transportation Facilities, which identifies routes that are beneficial to the tribal community. Inclusion in the inventory makes these roads eligible for federal funding through the Tribal Transportation Program. The Tribe’s 2022 Long Range Transportation Plan also specifically lists Birchwood Loop Road as a short-term priority for resurfacing and drainage improvements. In light of these existing priorities and in connection with the proposed project, the Tribe looks forward to partnering with the relevant agencies to seek funding and improve our community’s shared transportation infrastructure in this area, including measures aimed at pedestrian safety.

3.10 Public Services and Utilities

The Tribe agrees with the EA’s conclusion that there would be no significant impacts on public services and utilities. To the extent that the EA found any potential impacts on municipal services such as law enforcement, fire, and emergency medical services, those impacts are adequately mitigated by the mitigation measures set out in Section 4. As described further below, NVE has incorporated these mitigation measures into tribal law as binding commitments. As contemplated by the measures discussed in Table 4-1 (final cell, page 4-2), the Tribe has engaged in discussions with the respective service providers and has offered financial compensation to the Municipality of Anchorage to offset any potential fiscal impacts.

T1-6

Appendix M

The draft tribal ordinance reproduced in Appendix M amending the 2007 Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance, has been revised by the NVE Tribal Council and was adopted in its final form on January 7, 2025. The Tribe submitted later drafts of the ordinance to BIA in addition to the draft in Appendix M in order to address questions and concerns raised in connection with the

preparation of the EA. The final version of the ordinance NVE Ordinance 2025O-01 (“Eklutna Public Health and Safety Ordinance”) is enclosed here as Attachment A.

In finalizing the ordinance, the Council made technical amendments and added provisions incorporating the EA mitigation measures into tribal law. Specifically, section 120(c) of the Eklutna Public Health and Safety Ordinance provides:

The protective measures, best management practices and mitigation measures analyzed in the environmental assessment related to the lease of the Ondola Allotment, which will eliminate or substantially reduce potential environmental impacts of the Gaming Facility and therefore support a Finding of No Significant Impacts, shall as a matter of tribal law govern the design, construction and operation of the Gaming Facility.

T1-6
cont.

Similarly, section 145(b) of the Eklutna Public Health and Safety Ordinance provides:

Prior to commencement of gaming operations at the Gaming Facility, the Tribal Council shall ensure that police and fire protection services are available to the Gaming Facility by (i) entering into a binding agreement with the Municipality of Anchorage and/or Chugiak Volunteer Fire and Rescue Company, Inc. governing the provision of such services, (ii) the provision of such services by the Tribe, or (iii) a combination of the methods described in (i) and (ii).

These provisions help ensure that any impacts on municipal services will be mitigated as required by the EA.

Additional Comments

The Tribe has received a letter of support for the project from Cook Inlet Region, Inc., the ANCSA regional corporation for the Cook Inlet region, which encompasses Anchorage and the surrounding area, including the proposed project site. The letter is enclosed here as Attachment B.

T1-7

Through its laws, the Tribe has demonstrated its commitment to addressing identified impacts and protecting both the public and the environment. This project presents an unmatched opportunity for the Tribe to bring economic development to our community and our region, while honoring our Tribe’s connection to the project site and the environment surrounding it. We strongly urge the Department to support this project and issue the necessary approvals.

Thank you for the opportunity to provide our input on this important project.

Sincerely,



Aaron Leggett, President
Native Village of Eklutna

Attachment A

Eklutna Tribal Ordinance 2025O-01,
Approving Amendments to the Eklutna Public Health and Safety
and Anti-Discrimination at Gaming Facility Ordinance



Native Village of Eklutna
Tribal Government

ORDINANCE 2025O-01

AN ORDINANCE of the Native Village of Eklutna Approving Amendments to the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance

WHEREAS the Native Village of Eklutna (“Tribe”) is a sovereign federally-recognized Indian tribe and is a distinct, independent political community, and as such, is qualified to exercise powers of self-government by reason of its original tribal sovereignty as passed down from its ancestors since time immemorial; and

WHEREAS, the Constitution of the Tribe was duly and properly enacted, adopted, and approved in accordance with all applicable Tribal laws, requirements, customs, and traditions by the members of the Tribe on January 27, 1996, by a majority of the qualified voters of the Tribe with a quorum of at least fifteen percent of those entitled to vote (the “Constitution”); and

WHEREAS, Article II of the Constitution extends the authority of the Tribe to allotments of tribal members within the traditional lands and waters of the Eklutna people, including the Allotment 975 A-055026 owned by the Ondola family located at Lots 64, 66 and 67, located within Section 5, Township 15 N, Range 1 West, Seward Meridian, Anchorage Recording District, Alaska (“Ondola Allotment”) and subject to a restriction on alienation pursuant to the Alaska Native Allotment Act; and

WHEREAS, Article IX, Section 1(g), (h) and (n) of the Constitution authorizes the Tribal Council to administer tribal assets and tribal economic affairs and enterprises, to promote the peace, safety, health, politics, education and general welfare of the Tribe and to enact ordinances and regulations; and

WHEREAS, the Tribe and Gaming Authority plan to engage in casino gaming under the Indian Gaming Regulatory Act (“IGRA”) and applicable regulations and standards of the National Indian Gaming Commission (“NIGC”) in a facility on the Ondola Allotment pursuant to a lease with the owners of the Ondola Allotment subject to approval by the Bureau of Indian Affairs; and

WHEREAS, on March 22, 2006, the Tribal Council adopted Ordinance No. 2007O-01 the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance regulating gaming on restricted lands within the jurisdiction of the Tribe; and

WHEREAS, on April 19, 2024, the Tribal Council enacted the Native Village of Eklutna Gaming Ordinance of 2024 (“Gaming Ordinance”) to govern gaming on the Ondola Allotment, referred to in the Gaming Ordinance as the Tribe’s Lands, pursuant to IGRA; and

WHEREAS, on July 18, 2024, the NIGC determined that the Ondola Allotment constitutes Indian lands eligible for gaming by the Tribe under IGRA and that the Gaming Ordinance complied with IGRA and with applicable NIGC regulations and issued its approval of that Gaming Ordinance; and

WHEREAS, the Tribal Council finds it necessary to amend the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance; and

NOW THEREFORE BE IT RESOLVED THAT, the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance shall be amended to read as follows:

EKLUTNA PUBLIC HEALTH AND SAFETY AND ANTI-DISCRIMINATION AT GAMING FACILITY ORDINANCE

Section 101 Definitions

(a) This Ordinance shall be referred to as the Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance.

(b) The terms defined in the Native Village of Eklutna Gaming Ordinance of 2024 (“Gaming Ordinance”) shall have the same meaning in this Ordinance.

Section 105 Findings

The Tribal Council makes the following findings:

(a) Section 701 of the Gaming Ordinance requires that the construction, maintenance and operation of the Gaming Facility and the Gaming Operation be conducted in a manner that adequately protects the environment and the public health and safety and complies with the requirements and all applicable health, safety and environmental standards enacted by the Tribe.

(b) Section 801 of the Gaming Ordinance requires the Gaming Facility to secure a facility license for the Gaming Facility in which gaming is operated on Indian lands and establishes environmental, health, and public safety standards for issuance of such license by the Gaming Commission, including that the licensee (i) maintain an “orderly, clean and neat establishment,” (ii) subject the Gaming Facility to patrol by security personnel, (iii) permit inspection by authorized tribal officials and (iv) prohibit discrimination.

(c) The Tribe will continue to provide governmental services to the Ondola Allotment as it has done for decades. The Tribe relies on federal, state and local agencies to provide services that supplement services provided by the Tribe. The Tribe has historically collaborated with federal, state and local agencies regarding governmental services to the Ondola Allotment and the tribal community, and the Tribe will continue such collaboration to ensure the highest quality governmental services.

(d) While Alaska state and local governments have criminal jurisdiction in Indian country, including the Ondola Allotment, pursuant to Public Law 280, they lack civil regulatory jurisdiction.

(e) The Municipality of Anchorage provides police, fire protection and EMS services within municipal limits pursuant to Anchorage Municipal Code (AMC) 27.30.135(C), requiring that “[p]olice protection services shall be provided in the Anchorage Metropolitan Police Service Area,” which encompasses the Ondola Allotment. Chugiak Volunteer Fire and Rescue Company, Inc. provides fire protection and EMS services to the Chugiak area pursuant to AMC 27.30.060(B), requiring that “[f]ire protection shall be provided in the Chugiak Fire Service Area,” which encompasses the Ondola Allotment.

Section 110 Purpose

The purpose of this Ordinance is to provide for the protection of the health and safety of the employees, patrons and visitors of any Gaming Facility established pursuant to the Gaming Ordinance.

Section 115 Scope

These provisions shall apply to the Gaming Operation and the Gaming Facility, as defined in Sections 210 and 212 of the Gaming Ordinance, on the Ondola Allotment.

Section 120 Building and Safety Standards

(a) The provisions of Title 23 of the Anchorage Municipal Code, except the provisions listed in subsection (a)(i) below, are adopted as tribal law and shall govern the construction, expansion, modification, and renovation of the Gaming Facility, provided that references to the Municipality (except the references to requirements that contractors be licensed by the Municipality in Section 23.10.105.1.B, .F and .H) shall be to the Tribe, references to the board of building regulation examiners and appeals, including three-member hearing panel(s), shall be to the Tribal Court, and reference to the Building Safety Hotline shall be to the building official phone number, and further provided that the building official may revoke a certificate of occupancy for violation of tribal law. The Tribal Council shall appoint a building official and such other officials necessary to implement and enforce such provisions.

- i. The following provisions of Title 23 of the Anchorage Municipal Code are not adopted as tribal law: Sections 23.10.101.9.2 (relocated or moved buildings), 23.10.103.3.2.1 (internal auditor audits), 23.10.103.4.2 (appeal pools and three-member hearing panels), 23.10.103.4.4.A, .B, .D, .E, .F, .G, .H and .I (building board procedures), 23.10.103.4.5 (building board secretary), 23.15.203.4.6 (supplementing hearing packet), 23.10.103.4.7 (appeal filing fee), 23.10.103.7.2 (contractor license suspension or revocation), 23.10.104.4.1.C (permit application requirement for approval from state agency regarding AMC 16.60), 23.10.104.9 (optional third-party plan review), 23.10.104.15 (fees) and 23.10.105.2.1-.6 (trades certificates of qualification issuance and revocation).

(b) The provisions of 13 AAC 50.027 of the Alaska Administrative Code are adopted as tribal law and shall govern the construction, expansion, modification, and renovation of the Gaming

Facility. The building official appointed by the Tribal Council pursuant to paragraph (a) shall have authority to carry out the plan review and issue the corresponding approvals specified in 13 AAC 50.027(a). The plan review fees specified in 13 AAC 50.027(c) shall be waived.

(c) The protective measures, best management practices and mitigation measures analyzed in the environmental assessment related to the lease of the Ondola Allotment, which will eliminate or substantially reduce potential environmental impacts of the Gaming Facility and therefore support a Finding of No Significant Impacts, shall as a matter of tribal law govern the design, construction and operation of the Gaming Facility.

(d) The Tribal Council has determined that no future site preparation or construction activities related to the Gaming Facility shall be carried out on the Ondola Allotment until the Bureau of Indian Affairs approves the lease between the Tribe and the owners of the Ondola Allotment.

Section 125 Disabilities

The standards of the Americans with Disabilities Act, 42 U.S.C § 12101 *et seq.* are adopted as tribal law and shall govern any construction, expansion or modification of any gaming facility.

Section 130 Food and Beverage Handling

The provisions of Title 16, Chapter 16.60 of the Anchorage Municipal Code regulating the sale of food are adopted as tribal law governing the Gaming Facility.

Section 135 Water Supply and Water Quality

(a) The Eklutna Gaming Authority shall, consistent with applicable law, ensure adequate water supply to meet the needs of the Gaming Facility either by causing water to be delivered to the Gaming Facility, or by utilizing ground water from a well.

(b) The standards for water quality adopted by the U.S. Environmental Protection Agency pursuant to the Clean Water Act and the Safe Drinking Water Act, including the national primary water drinking regulations and national water quality standards in Title 40 of the Code of Federal Regulations, are adopted as tribal law governing the Gaming Facility.

Section 140 Workplace and Occupational Health and Safety

The standards for workplace and occupational health and safety established by the U.S. Occupational Health and Safety Administration, including 29 C.F.R. Parts 1910, 1926 and 1977, are adopted as tribal law governing the Gaming Facility.

Section 145 Public Health and Safety

(a) The Gaming Operation shall comply with applicable federal laws regarding public health and safety.

(b) Prior to commencement of gaming operations at the Gaming Facility, the Tribal Council shall ensure that police and fire protection services are available to the Gaming Facility by (i) entering into a binding agreement with the Municipality of Anchorage and/or Chugiak Volunteer Fire and Rescue Company, Inc. governing the provision of such services, (ii) the provision of such services by the Tribe, or (iii) a combination of the methods described in (i) and (ii).

Section 150 Anti-Discrimination in Employment

(a) The relevant federal laws forbidding employers from discriminating in employment on the basis of race, color, religion, national origin, gender, sexual orientation, age or disability are adopted as tribal law governing the Gaming Facility.

(b) Nothing in subsection (a) shall prohibit the gaming operation and other enterprises in the Gaming Facility from giving preference in employment to Indians according to tribal law.

Section 155 Enforcement

(a) Except to the extent provided otherwise in this Ordinance, the Gaming Commission shall enforce the provisions of this Ordinance pursuant to the Gaming Ordinance.

(b) Nothing in this Ordinance shall be construed as consent to the application or enforcement of state law on the Ondola Allotment or at the Gaming Facility.

CERTIFICATION

This Ordinance was read and approved on the 7th day of January 2025, by a vote of 7 for, 0 against, 0 abstain and 0 absent.

Attested to with our signatures fixed below by:



Aaron Leggett, President



Amanda Adams, Secretary

[EXTERNAL] Proposed Casino in Chugiak

From Angie Hamill <angie.hamill.ak@gmail.com>

Date Thu 12/26/2024 2:34 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

Hello. I am completely against a casino being built in Chugiak. Our community is not casino land. The increased traffic, lights, noise, and addictive activities are not conducive to our community life. I fully object to a casino in Chugiak.

Angie Hamill

Chugiak resident since 1987

11-1

[EXTERNAL] Eklutna Native Gaming Facility

From babs <babs@alaska.net>

Date Thu 12/26/2024 2:11 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

I am opposed to this development on Birchwood Spur Road.

Barbara Crittenden
10500 Constitution Circle
Eagle River AK 99577
907 242 2227

Sent from my Galaxy

] 12-1

[EXTERNAL] PROPOSED CASINO ON N BIRCHWOOD

From Judy Moore <akroadkill@gmail.com>
Date Thu 12/26/2024 9:08 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

To whom it may concern:

So a gambling facility, a 2nd gambling facility on site with restaurants and an event center to host events 4 times a week in a 40 foot high one story building to employ 226 employees in a neighborhood more than a mile off of a major highway seem like a huge negative impact to this residential area.

13-1

Gambling and alcohol would be a definite negative impact in this area as far as traffic and crime are concerned. Birchwood Loop Road could not handle the increased traffic.

13-2

Who is paying for the increased law enforcement?

This entity pays no taxes. So who is paying for increased law enforcement and road repairs?

I saw no mention of alcohol in this assessment.

If the native people live in a poor economic status of \$21,000 annual income.....gambling and alcohol should have no budget line item in their personal finances.

13-3

If this goal is of such positive impact, why isn't it located in a more open area with easier access?

How is this going to change or benefit these shareholders?

I hope your consideration does not support this peaceful neighborhood interruption.

13-4

Judy

[EXTERNAL] Eklutna Casno Project Environmental Assessment public hearing reply

From Gaye Sarvela <sarvela@mtaonline.net>
Date Thu 12/26/2024 8:39 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

This is my first reply to the public hearing on the Environmental Assessment for the Eklutna Casino Project.

The public hearing timeline of 15 days is arbitrary and contentious. As a plaintiff, I request that this public hearing timeline be changed to the standard length of 30 days. This extremely short timeline was intentional to get the assessment pushed through before the Biden administration leaves office. Also, this shortened timeline has purposely created a hardship on the plaintiffs. It was also released intentionally during the holiday season when many are traveling or unable to thoroughly study the assessment nor able to contact resources to research the information provided in the assessment.

I4-1

I will send a second reply for the public hearing on the information in the document.

Please let me know by December 31 that the timeline has been changed to 30 days.

Marilyn Sarvela, Plaintiff
21020 Alluvial Street
Chugiak, AK 99567

[EXTERNAL] All For It

From Phillip Flippo <phillipflippo@hotmail.com>

Date Thu 12/26/2024 6:37 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

I think the new casino is a fantastic idea and will help enhance local tourism options!



I5-1

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[EXTERNAL] Request to extend the hearing timeline for Eklutna Casino Project Public Hearing.

From Sean Barnett <seanb@rbiak.com>
Date Thu 12/26/2024 11:24 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

To: Alaska-NEPA@bia.gov

Subject: Request to extend the timeline for the Public Hearing on the proposed Eklutna Casino Project

Reference: Eklutna Casino Environmental Assessment Document Public Hearing timeline.

Alaska-NEPA@bia.gov staff,

My name is Sean D. Barnett and I speak for Lori A. Barnett also. We are plaintiffs in the law suit to stop the Casino project from being built in our residential neighborhood.

The public hearing timeline is 15 days and we feel 60 days is needed to give everybody a chance to review the Document. The 15 day timeline is not only arbitrary but contentious too.

I have scanned the document and it is long and hard to digest all the information presented. The public needs more time to read this and comment.

Lori and I believe the short 15 day timeline requested is because of the changes in Washington D. C. due to the recent elections. Let the new administration decide on this very important issue for all of Alaska.

Again Lori and Sean Barnett, plaintiffs request 60 days to review and comment.

Sean D. Barnett, Plaintiff
21624 Sean St.
Chugiak, AK 99567

16-1



[EXTERNAL] EA Comments, Eklutna Native Village Gaming Facility Project

From James Crewdson <Cellc.1@outlook.com>

Date Fri 12/27/2024 3:44 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

To:
Jolene John, Regional Director
Bureau of Indian Affairs, Alaska Region
3601 C Street, Suite 1200
Anchorage, AK 99503

Subject: Against the Eklutna Native Village Gaming Facility Project (Gaming Facility)

I live and work in Chugiak and am very concerned about the potential negative impacts the proposed Gaming Facility would have on the community. My experience and research regarding how other communities across the United States have fared in this situation convinces me that the long term negative impacts (economic, social, crime, addiction) far outweigh any meaningful positive impacts. In light of this, I do not support construction and operation of the proposed Gaming Facility.

Thank you for your time and consideration.

Jay

James "Jay" Crewdson, P.E.
Crewdson Engineering LLC
PO Box 671389
18368 Amonson Rd.
Chugiak, AK 99567
907-280-9493
CELLC.1@outlook.com

17-1

[EXTERNAL] Eklutna Native Village Gaming Facility Project

From Karen Perry <rperry@mtaonline.net>
Date Fri 12/27/2024 4:38 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

As a 4 DECADE resident of Brichwood, we highly oppose this project....we are supposed to be a rural neighborhood with NO city water and/or septic. This project would bring in massive amounts of traffic in a rural neighborhood and from my understanding has not been properly vetted or properly approved.

This is a big NO for this community!!

Karen Perry

18-1

[EXTERNAL] Eklutna casino

From Kathie Steele <steele4@gci.net>
Date Fri 12/27/2024 8:29 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

Our family lives not far from the proposed casino. We object for several reasons. 1. The casino, impact on the salmon run. 2. The traffic and noise a 24 hour facility will make 3. Subsidence of the land when there is an earthquake 4. Additional costs to the municipality for surveillance of the gamblers going to and from who have been served alcohol .

Respectfully,
Kathie Steele and Calvin Steele
19958 S Birchwood loop rd
Chugiaks, AK

Sent from my iPhone

] I9-1
I9-2
I9-3
I9-4

[EXTERNAL] Casino

From Tom Desalvo <desalvo@mtaonline.net>
Date Fri 12/27/2024 7:49 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

Please deny this project. Alaska has enough drugs and drinking and this will only worsen this affliction adding gambling to the mix can only make things worse-
You want to stop power generation to restore a salmon run while at the same time ruin what's left of peters creek
How would you like to be neighbors to this mess. Traffic noise drunk drivers excessive lights noise and cops dealing with the problems for this project
Does Alaska NEED another vice to afflict our struggling population
Nothing good comes from gambling except for the owners
Deny permit this disaster!!!!!!

I10-1

Sent from my iPhone

[EXTERNAL] Eklutna Native Village Gaming Facility

From vickiherman.ak@gmail.com <vickiherman.ak@gmail.com>
Date Fri 12/27/2024 10:37 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

I'm in writing in opposition to the gaming facility. I'm not opposed to the gambling facility in general; if stupid people want to gamble their money away, let them. A lot of them are already living off the government, anyway. My objection is the TERRIBLE LOCATION! There is something wrong with how quickly this project is trying to be this pushed through with little time for public comment, especially over the holidays. And, notice for comments only published on the website or ADN? Who reads the ADN anymore! Guess that's exactly the point. The environmental impact on the area will be a disaster waiting to happen. NO to this project!

111-1

Vicki Herman
907-355-1251
Sent from my iPad

[EXTERNAL] Impossible Deadline for comment

From Debbie Ossiander <ossiander@hotmail.com>

Date Sat 12/28/2024 10:35 AM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

Cc Matt Cruickshank <mjfc4th@gmail.com>; Val Jokela <valkur@mtaonline.net>

The Eklutna casino has been a top issue for the Birchwood Community Council for months. As CoChair of the Council I learned of the environmental impact study's existence on Christmas Eve while my house was full of grandkids. One of our big concerns is traffic. The traffic study addendum alone is over 1700 pages! At a minimum we need 30 days to even read this.

Many of those most concerned have no experience with government process. They need a simple and clear way to state their concerns. May they phone in comments?

Is there another way to let folks know about the study? Our email list does not cover all residents who live within a couple miles of the site. I know there will be major neighborhood concerns with this much traffic and a casino that is always open.

A response is appreciated

Debbie Ossiander
Co-Chair Birchwood Community Council
907-6882308
907-2442337

I12-1

[EXTERNAL] No Casino, please!

From Beth Conley <bethsweden@gmail.com>
Date Mon 12/30/2024 11:52 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

Hello,

Please consider the fears we have about having a casino in our neighborhood.

With having my home of 30 years and my husband’s life-long family homes and property of 70 years, we can’t possibly think of any reasons that a casino, in our immediate area, would be even considered.

It’s apparent that Native Village Eklutna (NVE) either didn’t realize or care that there are residential neighborhoods nearby. In fact, I was told by NVE that the area appeared to be purely industrial to them and they in fact hadn’t driven around any of the property that they are building on.

There are so many reasons that this casino should not be built in this area.

- The fish in Peter’s Creek, this will be detrimental to them. The casino will run up too close to the creek and disrupt natural flow of the fish I13-1
- We do not want a noisy gambling facility in the neighborhood. This will disrupt our our quiet and peaceful living I13-2
- There are children getting off and on school buses in the immediate area of the casino. This concerns absolutely everyone in this area I13-3
- The casino will be open 24/7 and serving alcohol. Any establishment serving alcohol will have problems with people drinking, causing ruckus and driving drunk. We simply don’t want this happening, it scares us all I13-4
- We worry about our well water tables being disrupted by such a large facility being built I13-5
- The noise pollution from a casino in our area would be detrimental to us, and any new buyer, should we want to sell our homes. This casino will immediately drop the value of our homes, substantially I13-5

• Parking for the amount of vehicles coming out here is going to be problematic. The only other casino is remote, in Metlakatla. This casino will attract an enormous amount of people, it is just not the right place for a casino] I13-6

• The environment in this area, the creek, fish and wildlife will be greatly impacted by this facility] I13-7

I hope that our concerns and fears will be addressed and taken seriously.

This is not the place for a casino or any other business serving alcohol and being open 24/7] I13-8

Thank you,
Elizabeth Conley
(907) 227-2775

Sent from my iPhone

[EXTERNAL] EA Comments, Eklutna Native Village Gaming Facility Project

From Janice Norman <jnorman@mtaonline.net>
Date Mon 12/30/2024 10:14 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>
Cc jnorman@mtaonline.net <jnorman@mtaonline.net>

30 December 2024

Dear Ms. Yazzie,

I was not sure if I should be addressing this emailed comment to you or to Jolene John who is handling the mail- and hand-delivered comments. Please share this email with Ms. John if she should be included in on this correspondence.

I am writing this comment in reference to the Eklutna Native Village (Tribe) Gaming Facility Project.

I hold a neutral position on Eklutna Tribe's wish to have a casino.

But, to build a casino on Birchwood Spur is a bad idea in my humble opinion. I took a moment to look at the traffic studies provided in the EA Appendices supplied on your website. My main source for my opinion is from having lived on South Birchwood Loop for twenty one years and having friends that live on North Birchwood Loop, on the Spur, and out to the South Birchwood Loop area. I have traveled on that section of road an innumerable number of times in a vehicle, on foot, and on bicycle.

In my experience traveling on North Birchwood Loop, the section of road between White Birch Road and up to the Glenn Highway involving a hill is very challenging when there is an accumulation of snow and/or the roads are icy. During the winter months, I have witnessed vehicle accidents on the hill between White Birch Road and the Glenn Hwy because vehicles would easily loose traction going up or down the hill and go into the ditch or hit another vehicle or even slide backwards on the hill. I have purposely avoided that section of road by traveling down the Birchwood

Loop Road to the southern exit onto the highway during the winter. Although I avoided the hill, the Birchwood Loop is full of curves and can get very icy also.

When there is an accident on the Glenn Highway between the South Birchwood and North Birchwood exits, vehicles use the Birchwood Loop Road to avoid sitting for hours while the accident is taken care of. Traffic through the Birchwood Loop Road can get crazy. People driving faster than the speed limit, cutting the numerous corners and/or driving in the middle of the road between corners.

Having looked at the Municipality's property maps, I see that Eklutna owns numerous pieces of property from the North side of Fort Richardson to Eklutna Village and from the Cook Inlet coastline to the Old Glenn Highway. There looks to be many properties that are zoned Residential, but a few that are zoned Commercial; including Commercial property next to Eklutna Village. To me, the setting by Eklutna Village would be a beautiful spot for a casino.

My question is: Why the desire to build a casino down a road that would be questionable for handling a large amount of traffic and is dangerous during the winter, and is right next to residential houses?

Please do not build a casino on Birchwood Spur off of the Birchwood Loop Road. *Please select Alternative C: No Action.*

Respectfully,

Janice Norman

801 S Glacier Dr, Wasilla, AK

jnorman@mtaonline.net

I14-1
cont.

I14-2

I14-3

I14-4

[EXTERNAL] Request for Extension and Public Comment on the Eklutna Native Village Gaming Facility Environmental Assessment

From mjfc4th@gmail.com <mjfc4th@gmail.com>
Date Mon 12/30/2024 5:41 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>
Cc 'Birchwood Council' <birchwoodcouncil@gmail.com>

To Whom It May Concern,

I am a concerned resident of Birchwood, AK, and I am writing to express my concerns about the proposed Eklutna Native Village Gaming Facility. The timing of the public comment period announcement on Christmas Eve significantly restricts meaningful community engagement and input. This timing is highly inconvenient for many, potentially preventing a substantial portion of the community from participating in a process that deeply affects their lives. Therefore, I request an extension of the public comment period to ensure adequate community representation and thoughtful responses.

I15-1

Environmental Concerns: The proximity of a salmon stream on the property, a critical habitat for multiple salmon species, is highly concerning. Salmon are integral to our local ecosystem and economy, and the potential for environmental disruption and pollution from the gaming facility could be catastrophic for these populations. A detailed environmental impact study focusing on aquatic habitats is imperative.

I15-2

Water Resource Management: Our community relies exclusively on well water, and the facility's significant water demands raise serious questions about the sustainability of our groundwater resources. The potential for well contamination and depletion poses a direct threat to residents' water security. The Environmental Assessment does not sufficiently address the impact of increased water usage on local aquifers, which is crucial for safeguarding our water supply.

I15-3

Traffic Concerns: The Environmental Assessment fails to address the specific impacts on North Birchwood and Birchwood Loop Spur. These roads are essential to our community's safety and mobility. Increased traffic could lead to congestion and safety hazards that the community is ill-prepared to handle. A detailed traffic impact study, including these critical areas, is essential to fully understanding the potential adverse effects on local traffic dynamics.

I15-4

Compliance with Environmental Laws: The current Environmental Assessment does not fully meet the requirements of the National Environmental Policy Act (NEPA), particularly in assessing indirect, cumulative, and

I15-5

specific community impacts. The oversight in the assessment process warrants a comprehensive Environmental Impact Statement (EIS) to fully explore and address all potential environmental and social effects. Given the issues outlined and the timing of the public comment period announcement, further review and community input are necessary. Extending the public comment period would demonstrate a commitment to community involvement and the thorough examination of all potential impacts of this project.

I15-5
cont.

Thank you for considering this request and the concerns raised. I look forward to a revised approach prioritizing environmental stewardship, community well-being, and transparent engagement processes.

Sincerely,

Matt Cruickshank
Birchwood resident



Virus-free www.avg.com

[EXTERNAL] Casino

From Martin Stofer <martinstofer331@gmail.com>

Date Tue 12/31/2024 5:27 AM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

Wrong to put casino there. Stop them.

Martin Stofer

] I16-1

[EXTERNAL] EA Comments, Eklutna Native Village Gaming Facility Project

From Savannah Haines <savannahcluff@gmail.com>

Date Tue 12/31/2024 9:40 AM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

Hello,

As a long time resident in Chugiak and birchwood area, I'd like to express my issues regarding the proposed gaming hall.

I believe the location is unsuitable and inappropriate. This is a quiet residential neighborhood. There is one main road in and out that already has safety issues as is. It is windy and narrow and sloped. There is almost no shoulder for pedestrians or cyclists on this 45mph road. In the winter the snow berms causes make visibility very difficult to turn on to the road.

Increased traffic to/from the gaming hall as well as having people driving under the influence of alcohol will not bode well. It will be disruptive and increase risk of injury and accidents in this neighborhood.

I support other voices from our community who have expressed concerns about the salmon run off, housing value, and the water tables.

In conclusion, I am not against the tribe building a gaming hall to make money. Please just do it somewhere else.

Thank you.

Savannah Haines

I17-1

[EXTERNAL] Native Village of Eklutna Casino Environmental Assessment

From Val Jokela <valkur@mtaonline.net>

Date Tue 12/31/2024 12:48 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

Cc Deborah Ossiander <ossiander@hotmail.com>; Matt Cruickshank <mjfc4th@gmail.com>; Patty Friend <Sambucus7@gmail.com>; Carol Perkins <66togo@gmail.com>; Barbara Trost <btrost@live.com>

I live off of North Birchwood, so I am effected by the proposed Casino and did not receive the Notice of Availability (NA) until the afternoon of December 24, 2024. The NA said the comment period started December 20, 2024, so right off the bat, 4 review days were lost, not to mention the NA hit during the holidays when we were all spending time with our friends and families.

Having said that, I gave it my best and tried many times to look through the referenced supporting Appendices noted in the Environmental Assessment (EA). I could only get up to Page 760 out of 1,073 and was continuously kicked out of the document and could not access anything and started over many times, as well as what I was able to access is very technical and needs more time provided for review and

understanding. So....

On December 31, 2024 at 1100 when they opened, I went to the Chugiak- Eagle River Library to review the hard copy that was referenced in the NA and that copy has not been provided to the library!!

We need a "do-over" on the timeline for comments and review, and because of the volume of documents and technical nature of the EA and supporting appendices, I would propose 30 business days for review. Week-ends and holidays should not be counted as part of the comment time period. And given that the 15 days fell over 2 holidays and week-ends as well as losing 4 comment days because of late notification, our community has not been provided with enough time to review and comment.

So, in summary, you have not done due diligence to meet the criteria of the Notice of Availability and thence, please do the right thing and issue an updated NA to give our community the time they deserve to review and comment on the EA.

Sincerely,
Valerie Jokela
valkur@mtaonline.net
907-748-5762

Jolene John, Regional Director
US Department of Interior,
Bureau of Indian Affairs-Alaska Region
3601 C Street, Suite 1200
Anchorage, AK 99503-5947

Ref: Comments regarding the Environmental Assessment for the Native Village of Eklutna Gaming Facility Project - DOI-BIA-AK-61628, dated December 2024

Dear Ms. John:

The following are comments for the Environmental Assessment (E.A.) for the Eklutna Native Village Gaming facility Project - DOI-BIA-AK-61628 December 2024
I live next to the property recently purchased as part of this development and have lived here for 27 years. This development will directly affect my quality of life.

The BIA elected to release the E.A. and supporting documentation for review during the National and Religious holidays, December 22nd through January 06, 2025 In addition the BIA has allowed only 15 days to comment. This is highly disrespectful to the public, some of which may include those in favor of the Casino development and does not follow CEQ guidance. If the true intent of this E.A. was "in the public interest" and they wanted public comment's, it would then also explain in the body of the document that a requested extension to the public comment period is acceptable. This E.A. submission is submitted only in the interest of the Native Village of Eklutna and Marnell gaming and does not consider the local or greater communities public opinion.

119-1

The minimum time standard by historical perspective for a NEPA related E.A. with CEQ guidance is 30 days minimum and not during a time period when there is little way for the public to comment utilizing the support of Federal agencies and those resources, which are the same Federal agencies and resources used to prepare and create this E.A. over months if not years of time. This decision is arbitrary and capricious given the possible affects to the greater community. I am requesting an extension of the public comment period to 21 January 2025. Anything less than 30 days for a public comment period when considering this E.A. is an act of contempt toward this community.

The Peters creek setback allowance on the eastern boarder of this development with a retaining wall and parking area is not in any way feasible or sustainable for a development of this size, type and foot print. It also does not even come close to approaching a standard as called out by any Government environmental regulating agency's instruction or directives. The standard as per the Anchorage Municipal Code and as revised on August 14, 2018 under AO No. 2018-67(S) and showing It's revisions, are as follows,

"An ordinance of the Anchorage Assembly Repealing and Reenacting Anchorage municipal code ("AMC") subsection 21.07.020B. To update provisions regarding watercourse, water body, and wetland protection; Amending AMC chapters 21.02 and 21.03 to transfer variance authority for variances from watercourse, water body, and wetland protection regulations to the zoning board of examiners and appeals nonconformities with said protections; and amending AMC 21.14.040, definitions accordingly. (Planning and Zoning Commission Case 2017-0133)"
The above revised AMC provides a Table 21.07-1 which calls out a "Minimum Stream Setback width per side" listing Peters creek in the above table and clearly delineates a stream setback

119-2

width per side of 50 total feet. 25 feet of which is a riparian setback zone and 25 feet which is a stream side setback zone for a total setback allowance of 50 feet per stream bank side.

Further more, I would like to bring attention to the person hired by the Native Village of Eklutna for the Eklutna River Restoration project, Eric Brooton of Trout unlimited. Eric whom in an email addressed the Matanuska-Susitna Borough Planning Commission on June 5th 2023 regarding the resolution 23-22 which called for a revision of the allowed waterway riparian setback allowance in the Matanuska-Susitna Borough. This proposal called for a reduction to the present 75 foot riparian setback allowance to bring some previously built structures into compliance.

I19-2
cont.

Below is a quote by him in his email to the Planning Commission.

“When riparian areas are degraded by human activity, water bodies are prone to increasing temperatures and decreased water quality, fish lose habitat, communities are more vulnerable to floods, and we must pay considerable sums to repair the damage done.”

Thank’s in part to Eric Booton’s letter and statement the resolution was defeated and the 75 foot riparian stream side setback was preserved. I would assume that the Native Village of Eklutna will also be consulting him on this Casino project as to the correct river setback allowances for Peters Creek, thus correcting the present error showing a 25 foot setback from Peters creek on the eastern border as shown in the E.A. Casino project plan diagram.

The E.A. as written paints a picture that says water access will be no issue. In my 27 years on a property next to this potential Casino development I have had no end of issues with water quality and inconsistent water flows. Added issues are Alluvial silt in the water which would need to be filtered out at considerable expense. Alluvial silt is one of the most difficult solids to filter because of it’s fine powder like consistency. A standard particulate filter used to filter Alluvial silt becomes clogged in a matter of minutes. Arsenic levels in the water in the greater area that are above the Federal limit allowed are also common and require a total facility filter or a source filter at each drinking water access point. The owner that owned my property before myself had his first developed well go dry in the late 1980’s. In 2018 during the earth quake many wells in the area were adversely affected. In rare circumstances they improved, which tells you that water access in the area is sourced through cracks in deep lower bed rock formations. This fact is also brought out in a study done in 1979 by the USGS which also states the average flow for developed wells in the greater area is 2.0 gpm, which is a long way from the 90 plus gallon requirement called out for in this E.A. Also in the E. A. diagram showing water wells in the area identifies at best about one half of the actual wells in active use. Most of the residential property owners in the area have water rights filed with the DNR.

I19-3

In the event the Casino is developed and ground water is used at a rate as called out for in the E.A. greater than 90 gpm and it affects wells in the residential areas surrounding the Casino development, what remedy will the Native Village of Eklutna offer?

What will NVE expect from the local residential property owners on these ground water issues?

Are they asking us to give up our water rights?

If the NVE Casino development can not access enough water will NVE then ask the MOA to provide city water?

Will the residential property owners in close proximity to the Casino development that has now connected to MOA water be mandated to also connect to MOA water?

I19-4

If so will the NVE Casino development now pay for the residents to connect to MOA water?

Will this new circumstance with water access affect the talks with the MOA on Eklutna lake water?

At present Eklutna lake is used by the MOA for drinking water.

How will water access at this possible Casino development affect the Eklutna river restoration project?

Are they related?

If they are not related, how are they not related?

In the event this NVE Casino development can not access enough ground water from developed wells on the Ondola allotment will it ask the Municipality of Anchorage for city water that comes out of Eklutna Lake?

Septic Systems,

- 1) Will the two treatment technologies presented in Appendix C (Orenco Systems and Lifewater Engineering) provide secondary or tertiary (advanced) treatment?
- 2) The average daily flow, assuming 8,681 gpd is correct (see comment 5 below) is equivalent to 6 gpm. The peak flow is reported to be approximately 94 gpm. Each wastewater unit is designed to operate at a specific hydraulic detention time to ensure optimum performance. What considerations have been made to handle peak flow?
- 3) Due to the high groundwater conditions and high percolation rates of the drainage system, will disinfection (such as UV) be required for the selected treatment technology? UV disinfection may require Total Suspended Solids (TSS) to be below 10mg/L.
- 4) Will compaction requirements for the parking area impact the integrity of the drain field?
- 5) The letter in Appendix C, Attachment 1 by Marnell Consulting indicates that Enterprise Engineering anticipated 4,000 guests per day. Marnell Consulting revised this number to 3,000 guest per day with no explanation. Please explain. Assuming 4,000 guest per day more accurately reflects the number of guest per day, then the Grand Total Water Demand = 11,281 gpd with a corresponding 75% Diversity Applied = 8,461 gpd.

Throughout the E.A. and the supporting appendices, it is stated that the Native Village of Eklutna is either not required or exempt from paying applicable taxes to State or Local government. Also the E.A. states the the Native Village of Eklutna “may” chose to enter into a supplemental agreement with the Municipality of Anchorage for Police, Fire, or EMS services. Or the Native Village of Eklutna may self perform these services.

Further Social and economic analysis states an estimate of \$446,000 in tax revenue would accrue to the Municipality of Anchorage, \$2.4 million would accrue to the State of Alaska and \$5.4 Million would accrue to the Federal Government. What it does not say is through what means those tax dollars are conveyed.

There are no supporting documents that show any signed agreements with state or local government in support of any rendered services to this proposed Casino development. Appendix M showing the Eklutna Native Village governing ordinance is unsigned and shows no committee member names, their titles, responsibilities, or who may have created this document that just kind of appears out of thin air.

Also as part of this E.A. the lease agreement between the heirs of the Ondola allotment and the Native Village of Eklutna as well as the contract between Marnell gaming and the Native Village of Eklutna should be published.

119-4
cont.

119-5

119-6

119-7

119-8

119-9

The traffic Analysis (Appendix A) shows that a upgrade to the North Birchwood entrance and exit areas would need a construction upgrade with out saying that there were clear agreements with the State of Alaska or the Municipality of Anchorage to cover these additional cost.

119-10

Finally, with the now prematurely completed site clearing of trees from the land in question, prior to the NEPA review and without any agreements documented from the State of Alaska or the Municipality of Anchorage, the Native Village of Eklutna and the BIA are attempting to show compliance to the Federal action as having been met. This is extraordinarily unprofessional, showing outright contempt for the lives of ordinary neighbors and citizen's in the local area, yet alone the greater community.

119-11

(Via Email and hand delivery)

Brian Holl
21123 Alluvial St.
P.O. Box 671529
Chugiak, Alaska 99567

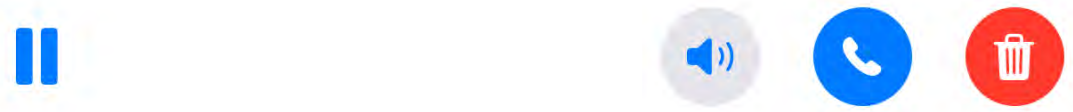
+1 (907) 694-7366



Anchorage, AK

December 31, 2024 at 2:17 PM

1:01 -0:02



Transcription

"Good afternoon my name is Dolly Caswell . I live in Eagle River . At 1 4 5 2 6 Waterfront Ln. My number is 9 0 7- 6 9 4- 7 3 6 6 I want to strongly object to a casino on peters Creek Road or Somewhere in Chugiak, I just read it a while ago and I think that's a dreadful Suggestion for our immediate community and area here Please do not let this happen I'll be Clutton and they even need to do something else to raise some income but not a casino in this area Again, my name is Dolly Caswell 9 0 7- 6 9 4- 7 3 6 6 and I object to that casino thank you bye..."

120-1

Was this transcription [useful](#) or [not useful](#)?

[EXTERNAL] public comment on Eklutna Casino Environmental study

From Debbie Ossiander <ossiander@hotmail.com>

Date Thu 1/2/2025 9:45 AM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

1 attachment (18 KB)
Eklutna comment 2.docx;

To Whom it may Concern:

I am an Alaska resident who has lived near the casino site for over 45 years. I have a number of concerns with this Eklutna Casino environmental impact document. I have had many conversations with neighbors on this topic and my comments below reflect some of these.

Page 1-8.... State and Local Approvals, makes no mention of the Alaska Dept of Environmental Conservation standards for public wells, septic systems and storm water storage. Smaller systems are also regulated and require permits from the Municipality of Anchorage. Will those standards be followed? And if so, which state and/or local ones?

I21-1

Section 2.1.2 ...Proposed Gaming Facility. At the one public information speech given there was no mention of a 7 day a week, 24 hour a day facility. This will raise neighborhood fears of noise, crime, light and sound pollution. There are no nearby businesses open at night. Additionally, the 58,000 (elsewhere described as 56,000) square foot facility is much larger than the 35,000 sq foot building previously publicly discussed. I am concerned that a 6.3 acres site with that size building, loading docks, storm water retention areas, on-site septic, well, 443 parking spaces, retaining wall, etc. may be too much. I found no mention of the municipal requirements for dedicated snow storage, landscaping and pedestrian facilities. Are they also planned?

I21-2

Page 2.6 Grading and Drainage. The plan appears to show the entire site paved, with parking slots right up against a retaining wall to be built beside Peters Creek. I believe the 25-foot setback from the Creek is inadequate. Peters Creek is a glacial stream. It moves its banks and erodes its banks regularly. Two years ago, a neighbor lost a large part of her yard and almost her well through bank erosion about a

I21-3

half mile upriver from this site. The stream bed is not static. New channels open and close. The site is fairly close to the mouth of the Creek and traditionally has seen active salmon escapements. Most environmental standards would call for a 50-foot setback, but page 3-26 states there will be no impact to Peters Creek with the 25-foot setback. Please provide some proof this is true and that the glacial action of the Creek is recognized.

I21-3
cont.

Page 2.9.... Wastewater. It appears the intent is to include a septic leach field for waste treatment. It seems unusual for a septic leach field to be paved, but there is also mention of a secondary treatment? I am unclear what is being proposed and was unable to open appendices to investigate further. Is the intent to regularly pump out septic holding tanks even when the facility is complete? If so the potential for leakage, smells, and traffic should be considered. If not pumped very regularly it may need heating? Has this plan been checked to see if it meets Alaska DEC standards? Has it been examined by a registered engineer with Alaskan experience?

I21-4

Pages 2.8, 2.11, 3.14 talks about flooding and relies on the FEMA flood maps. Unfortunately, the flooding we have seen multiple times from Peters Creek is not traditional flooding, instead is glacial flooding. In very cold winters Peters Creek freezes from the bottom up, causing the water on top of the ice to overflow the Creeks banks. Several times this has caused flooding down Aurora Borealis Road, causing its closure and the rerouting of traffic. Local residents have applied several times for help to combat the problem.

I21-5

Page 2.9 Other Utilities. This section is totally inaccurate. Electricity in this area is provided only by the Matanuska Electric Association, NOT by Chugach Electric as the document states. Has there been any discussion with MEA about the project? (Particularly if there is a plan to add electric vehicle charging stations). MEA recently had to upgrade a local substation to meet the area's electric needs. Additionally, Verizon and T Mobile are mentioned, but they are not readily available. Our providers are GCI and AT&T.

I21-6

Section 2.1.8 Law, Fire and Emergency response. The agreement between the Anchorage Police Department APD and the Village of Eklutna is not a reliable model for this site. The needs of a small residential village and a casino with up to 700 (elsewhere 1000) machines and over 400 parking sites differ profoundly. Historically we have already seen tension and problems with police response in areas outside of Anchorage police jurisdictional boundaries (the upper Hillside and Girdwood). This needs to be discussed in depth.

I21-7

Fire and emergency response in this area is from the Chugiak Volunteer Fire Dept., who may call on Anchorage Fire for help if needed. The Anchorage Fire Department is not our primary responder. The Volunteer Dept relies on local property tax to buy its equipment, and pay its leaders. It appears there is no plan to replace this lost revenue to the Chugiak Dept. The plan is silent on coordinating with the CVFD.

page 3.1 There is no mention of salmon protection or of Alaska Dept of Fish and Game oversight. Historically fishing at the mouth of Peters Creek has been allowed and regulated, but ADF&G has

I21-8

prohibited it further upstream. Has there been any conversation with Alaska fish and game personnel? Is there intent to follow their guidelines? I21-8 cont.

Page 3.2 The Chugiak-Eagle River Comprehensive Plan Update. This site is identified as I1...industrial. The definition excludes entertainment centers and restaurants. I know of no specific language in the plan regarding erosion protection or flood control. I21-9

Page 3.9 There is no mention of Alaska DEC or municipal Anchorage oversight of public well and septic. Will those rules be followed? The document lists Alaska DF&G only as a source used to identify the existing salmon runs, but it makes no mention of following their fish protection guidelines. I21-10

3.11 Groundwater The document found wells varying between 38 and 223 feet in depth but then states water level is very stable. There are areas in Birchwood with limited water and others with great wells, as evidenced by the varying well depths found. The water table is also variable. In very cold winters the water in a well on my property rises as the ground freezes. Some years ago there was a significant ground water contamination problem from a service station in Peters Creek near the Glenn Highway, so many neighbors are sensitive on the topic and have tried to establish water rights with the state. I21-11

3.13 It is unclear how the retaining wall adjacent to Peters Creek will prevent erosion. How tall is it, how is it made and how stabilized? What prevents the dirt under the wall from eroding? I21-12

3.19 This section states construction will be eight hours a day, but earlier the document states construction hours will be from 6 or 8 am till 10 pm. I believe the proposed timelines differ from those in Title 15, Anchorage code. Which is true and which will be followed? I21-13

3-25, 3-27 Bald eagles do indeed nest regularly very close to the site. There is a known nest off Blair Road (less than 1 mile from site) which has been used for the last several years. The state currently prohibits fishing in much of Peters Creek in an attempt to protect the salmon run. I21-14

3.32 Section 3.7.2 states there will be no property tax impact since the site is now exempt from property tax. However, the site now will begin to significantly use services that depend on local property taxes such as police, fire, street maintenance, snow plowing, emergency response, waste water treatment and the land fill. Obviously, there will be an impact and increased demand on those services with no compensating revenue. Why has that impact not been more thoroughly assessed? I21-15

3.36 – 3.41 Development of the facility will create 419 jobs, fill over 400 parking spaces and generate income from a restaurant, bar and up to 700 or more gaming machines. This will create a significant I21-16

change in traffic on a road already in need of upkeep and modification. Birchwood Loop North has no shoulders. It contains limited speed (20 mph) dangerous curves just past the 3 Bears store. I have personally observed a bus slide through that curve sideways on an icy winter day. There are no pedestrian walkways or bike paths in the area, why does the document assume they will not be needed in the future? The traffic impact addendum provided is very technical and dense. It only partially available on line; I was unable to open large sections. Because of limited time to comment and unavailability of the entire document, my comments here are forced to be limited, but this is a large area of local concern. If upgrades to Birchwood Loop, Birchwood Spur or the Glenn access are needed, there is no known funding available to make them.

I21-16
cont.

Page 3.47 I found it interesting to see Birchwood Community Council Bylaws listed, since I am a Co-Chair of that Council. Those bylaws are written only to direct how we run our meetings. However, the proposed Eklutna Casino has been a Council agenda topic for months. We have been trying to get clarity and information about this project with limited success.

I21-17

3.38 does not adequately show the impacts on local services. It states the “project would generate an increase in state taxes in excess of anticipated costs”?? What taxes? What about municipal costs? How is the project compatible with surrounding land uses? Nearby are homes, an airport, a gun range, a railroad and a few small businesses that support industry.

I21-18

3.49 Land use and zoning. This site is zoned light Industrial, I-1. That zoning classification does not allow entertainment centers, restaurants or bars. The only commercial activity allowed is strictly limited to commerce directly supporting industrial use. In fact, the Eklutna Overlay Zoning District, part of Anchorage zoning code and created with advocacy and help from Eklutna Village, prohibits commercial in this area.

I21-19

It is also misleading to mention only the impact on one home visible from the site. There are actually homes on Alluvial, Pioneer, Seal, Scenic and Old Pioneer Streets that consider the site close and are deeply concerned with impacts. The impact will be felt by all residents that live close to and regularly rely on North Birchwood Loop.

Debbie Ossiander

PO Box 670772; 21947 Valley Ave

Chugiak, AK 907-688-2308 ossiander@hotmail.com

• Outlook

[EXTERNAL] EA Comments, Eklutna Native Village Gaming Facility Project

From jeff <j.schlies@protonmail.com>
Date Thu 1/2/2025 6:01 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

Greetings:

Regarding the proposed Casino in Birchwood/Chugiak.

Aside from the crime, violence, and low morals that ALWAYS accompany the presence of a casino, the rural neighborhood venue that will be the route to the casino makes it the wrong place to build a casino. Neither the electrical nor the road infrastructure is in place to support the additional activity. Will the casino be upgrading the road and power lines to support the additional power and traffic or will they be expecting the non-beneficiary community to support their tax-free venture by expanding the roads and power? Or will the problems that arise from the casino simply be "not their problem"?

122-1

To be clear, I would oppose this venture even if it were fully taxable but the fact that it is not adds insult to injury. Casino gambling operations are illegal in the State of Alaska by will of the majority. If the Eklutna Native Village wants to build a casino, they can build it in their own village, not in the neighborhoods outside of their village. They own quite a bit of land to the north of Birchwood. If they are claiming the sovereign right to build a casino, let them build in in their own neighborhood and provide their own infrastructure.

Sincerely,
Jeff Schlies
Chugiak Resident


- Outlook

[EXTERNAL] Public Comment on Eklutna Village Facility Project

From Gaye Sarvela <sarvela@mtaonline.net>

Date Thu 1/2/2025 7:27 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

 1 attachment (24 KB)

Jolene John Regional Director Public Comment Document.docx;

Attached is my public comment on the Environmental Assessment for the Gaming Facility.

Marilyn Sarvela

Chugiak, AK

January 2, 2025

January 2, 2025

Jolene John, Regional Director
Bureau of Indian Affairs-Alaska Region
3601 C Street Suite 1200
Anchorage, AK 99501

**Public Comment on the Environmental Assessment for the
Eklutna Native Village Gaming Facility Project**

Comments from:

Marilyn G Sarvela, Plaintiff
21020 Alluvial Street
Chugiak, Ak 99567

This assessment including the appendices is very long. The public was only given 15 days to review and comment. NOT the standard 30 days. Shortening the comment period is arbitrary and contentious and places all who want to review this at a disadvantage. This short time frame was to push this through before the Biden administration changed to the Trump administration.

I23-1

Several places in the Assessment say that the Eklutna Tribe will be responsible for situations such as getting water to the project or dealing with safety on the site. Who will provide the oversight that these are done? Will they use an independent vendor?

I23-2

When Marnell is no longer managing the facility, and Eklutna Natives are supposed to take over, who will provide oversight for the operations?

In this document it was mentioned several times that taxes would pay for a variety of costs such as those incurred by fire and ems services. Where are these taxes coming from?

I23-3

Sales tax revenues are listed. What sales tax? There is not sales tax in the municipality of Anchorage nor by the state? Appendix table 30 and 31.

I23-3
cont.

Severance taxes are listed. Will this tax from natural resource extraction be allocated to Birchwood Loop?

Major concerns are: traffic volume and its damage to Birchwood loop; increased crime, even though the assessment says it will be low, the project is close to residential homes, therefore, any crimes are a concern; environmental damage, which is already affecting the residents in the lots near the building site; gambling is a disease and this establishment will make that disease worse; pollution; animal habitat eliminated.

I23-4

In the appendices the traffic study does address the narrow road and lack of shoulders on North Birchwood. How will that be improved and who will pay for it?

Page 9 of the Appendix, Unsignalized intersections: This analysis used traffic tables from 1967 and 1985. Why are you using data 30-58 years old? That information in 2025 is extremely outdated!!!

Appendix, Water and Wastewater, page 4

Why doesn't the diagram of wells show the other 6 wells on Alluvial Street?

The water rights for these homeowners/residents are important to document accurately.

I23-5

Page 5

Water usage lists that this project will have a bar. Why hasn't the bar been included in the assessment? I feel that information is glossed over because it will mean patrons of this place will be driving after drinking leading to more crime, more patrol calls.

I23-6

Page 11, cumulative impacts

'Furthermore, advanced secondary treatment of the wastewater, including nitrogen removal and disinfection will likely be provided, which will further mitigate the risk of negative impacts to the environment' For a safe, clean environment this should not be a "likely" thing to happen but a for sure thing to do.

I23-7

Appendix c Water usage letter

Why should the Marnell’s lower usage projections be used instead of the engineers? When people attend an event, bathroom usage is at least twice and up to 4 times. Was this lower limit make it easier for Marnell to say there is enough water for this site?

123-8

Innovation Group, page 17 Problem Gambling

Problem Gambling is recognized in this document, in our state and around the country.

Why do the native want to build and support an establishment that increases the problem?

123-9

Comments on Environment Assessment

1.6-1 pg 1-8

“...AK Dept of Nat Resources needs to approve water rights to utilize ground water and get a permit to drill a well”... There are assumptions that there is enough water for this size of project

How is Eklutna Village and Marnell going to make reparation to residential property if the residential water wells production is reduced because of the casino? The reports referred to water, soil and seismic issues about the larger area, not the allotment.

123-10

There will be access to the municipality with an off site roadway. Who pays for building it and maintenance?

123-11

2.1.1

A business lease requires approval by the secretary of Interior. How will the Municipality of Anchorage be notified that this has been approved ? No construction or work on this site will be done until that lease is signed, correct?

The tribe has jurisdiction over the project site and is responsible for providing government services to project site including social services and also tasks the tribe with acquiring additional water rights if necessary. What government services and what vendor would be used for this?

123-12

2.1.2

At the Birchwood Community Council meeting, Leggitt told the Council that the casino was going to be about 50,000 sq feet and 750 machines. The Assessment says 58,000 sq ft (16% increase) and 1000 machines (33% increase). These large increases affect water usage and

123-13

waste water needs and a strain on both in that location, and environmental issues such as runoff, vehicle noise 24/7, traffic on Birchwood loop, and vehicle exhaust.

i23-13
cont.

A temporary gaming facility is being constructed for the ongoing need for economic opportunities and at the same time construction is taking place. How will this happen safely? How will water and bathrooms be furnished? Is a permit needed for port a potties for this long of activity? What is the native village doing now for the ongoing need for economic opportunities?

i23-14

2.1.5

A tree permit is required. 171 cords of wood will sell for \$200-\$300 per cord in the Chugiak Eagle River area. When will a tree permit be approved?

i23-15

52 truck trips will damage Birchwood and Spur roads Who will pay to repair this the Birchwood loop and spur roads?

i23-16

2.1.6

The site is sloping north so how will that affect the railroad tracks and also keep environmental issues happening to Peters Creek because of drainage?

i23-17

Over 11,000 CY of unusable and excess materials would be exported from the site. 20,500 CY of fill would be imported. How many truck trips will be made to haul these materials? How much exhaust pollution will these trucks create? Who will pay for the repairs to Birchwood Loop and Spur roads?

i23-18

2.1.7

Temporarily delivering water to site would mean 16 water trucks per day on Birchwood Loop. This will damage the road. Who will pay for the repairs to Birchwood Loop and Spur roads?

During the operating of the interim facility , wastewater will be hauled away How may truck trips per day will take place? This damages Birchwood Loop and Spur road. Who pays for road repairs?

2.1.8

In Fig. 3.7-3 the amounts listed for public service are very low. For instance, the base pay for an APD patrol officer is about \$81,000 but that doesn't include insurance, vehicle etc. To date there are only 3 officers for Eagle River/Chugiak area. APD is short on officers for this area so taking on extra calls will weaken their service to the entire community. Figures 3.7-9 and -10 suggest taxes will pay these underestimated expenses of public services. Taxes from whom? Why isn't the tribe paying for these services?

123-19

2.1.9

Diesel generators are an environmental concern. They are loud and emit a lot of exhaust. This is noise and air pollution. Who will oversee that this only lasts for the 20 days that the assessment lists?

123-20

Page 2.1.11, Table 2.1-1

Who monitors that the proactive measures are being done correctly?

123-21

Page 2-12

Traffic speeds will be reduced to 15 mph on project site. Who monitors that?

123-22

2.5.3

If this is a tribe's ancestral land base, why did the Railroad let Ondola put a house there when he was working for them?

123-23

3.3.3

Assessment criteria: How are the tribes prepared to correct an adverse affect on local water supply and water quality of the wells of each resident?

123-24

3.4.3

A diesel emergency generator may run up to 500 hours annually. This causes more air pollution. How can the public be sure air quality is being monitored?

123-25

3.5.3

..A Significant impact to biological resources if development and operation would occur...
Caused by lighting, clearing land, pollution in water etc
Why develop this if it reduces biological resources and impacts these resources?

123-26

Table 3.7-7

The assessment states that most of the construction positions will be filled by under or unemployed. The construction contractors are going to hire skilled workers What percent of those people are skilled in construction work?

123-27

Page 3-38, Net Fiscal Impacts addresses the increased costs of public services.
The assessments estimates costs for public services are low. What will be done before this project starts to be sure expenses are covered?

Page 3-38

The extra costs related to judicial, detention and correction activities still add to a state budget which is dipping into reserves. Why does the state have to absorb this additional costs that's created by a organization that doesn't pay taxes?

123-28

Table 3.7.9 and 3.7.10

The local tax projections are not enough to cover expenses of this project. Birchwood Spur Road alone will require major repair from the heavy truck traffic and small vehicles.
Who will pay the millions to repair the road for truck damage during construction?

Table 3.7.3 pg 3-37

Public service costs will be higher. There will be additional police patrols,
And fire and ems will be called out because of the nature of gaming halls with high concentration of people. Who pays for this?

3.7.3 page 3-39

The tribal gaming facilities in Alaska referenced here are on reservations.

123-29

Page 3-38 Property values

Residential Properties are less than 300 feet away from the project.

Low residential is because the lots are 1.25-2.5 acres. The air pollution alone will affect properties a greater distance than 300 feet. Are these low density lots being viewed and treated differently and unfairly than a high density residential area?

123-30

3.8.2 page 3-42

The assessment states there are no bicycle or pedestrian facilities within the vicinity of the Project site. Birchwood Road and Spur road are used by many bikers in the spring, summer and fall Did you count the number of people that ride bikes, walk or jog on Birchwood Spur?

123-31

page 3-43

Construction Traffic...86 truck traffic daily trips; ..operation traffic weekly traffic 8010 and Saturday 10,550. The appendix addresses that this will be close to design limits. But in reality it exceeds what Birchwood Loop and Spur were designed for.

123-32

3.9.3

Alternative A: proposed project, Land Conflicts, Paragraph 3

Starting with the 3rd sentence: While the Project Site.... : this explains why this project should not be on this site.

123-33

Page 3-53

Parks and Recreation

Oberg Park is about 1 mile away: it is is lineal mile. Loretta French Park and Peters Creek parks are also within the usage area of this project.

123-34

Page 3-54

The 137.6 ton estimate of solid waste per year is based on a 1996 Study. What is the recent data from the past 8 years? What is being doing environmentally to handle recyclables?

123-35

This says the tribe intends to use natural gas for heating and cooktops. However in 2.1.7 the assessment says:

“The Tribe intends to use electric appliances, boilers, and heating systems within the proposed gaming facility to the extent feasible to minimize reliance on natural gas.”

Which is accurate? Why does it say two different things about the utility that will be used?

What environmental changes does this make regarding air quality?

123-36

Page 3-54

“the increased concentration of people that would result from operation of Alternative A would be expected to lead to an increase in the number of service calls to the Anchorage Police Department, similar to any other commercial development. As discussed in Appendix D, Alternative A is projected to result in approximately 199 law enforcement calls for service annually.” APD is short staffed in general and only 3 officers to date are in Chugiak Eagle River. This doesn’t address how many hours each call takes.

123-37

Page 3-55 -3-60

The noise will increase from various sources and change the quiet environment of this area.

Who and how will noise levels be monitored?

123-38

[EXTERNAL] Eklutna Impact Comment Period Extension

From Sen. Kelly Merrick <Sen.Kelly.Merrick@akleg.gov>

Date Fri 1/3/2025 9:04 AM

To NEPA, Alaska <Alaska_NEPA@bia.gov>; karina.waller@murkowski.senate.gov <karina.waller@murkowski.senate.gov>; adam.trombley@sullivan.senate.gov <adam.trombley@sullivan.senate.gov>

To the Alaska Section of BIA and our congressional delegation,

My community is asking for an extension to the BIA comment period for the Eklutna Native Village Gaming Facility Project Environmental Assessment. The comment period began December 20th and is scheduled to close January 6th, just a little over two weeks; the report itself is 124 pages. Members of my community have been following the issue closely, and do not feel this is sufficient time for them to read through the proposal, involve the appropriate neighborhood, and allow for the public to submit comment. I agree that for such a large document to be reviewed by Alaskans who may not often engage in the public process, as well as in a time period falling across major holidays, it should be extended.

124-1

I know the co-chair of the relevant community council, Debbie Ossiander, has reached out to the BIA office with other questions about the comment period and report.

Please let me know if there is any way I can assist in seeing the comment period extended.

Kind Regards,

Kelly

Senator Kelly Merrick

District L - Chugiak/Eagle River
Legislative Information Office
1500 W. Benson Blvd., Room 216
907-269-3630

[EXTERNAL] Casino

From Liz Morgan <lizmorgan50@gmail.com>

Date Fri 1/3/2025 2:23 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

This is a very poor site for a casino. I am against it.

I25-1


Sent from my iPhone

[EXTERNAL] Comments for the Environmental Assessment for the Ekluta Gaming Facility Project

From Palma Ingles <inglesalaska@gmail.com>

Date Fri 1/3/2025 1:35 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

 1 attachment (31 KB)

BIA comments.docx;

Good afternoon,

Please find attached my comments on the Environmental Assessment for the Eklutna Native Village Gaming Facility Project-DOI-BIA-AK-61628.

Regards,

Palma Ingles

31 December 2024

Jolene John, Regional Director
US Department of the Interior
Bureau of Indian Affairs, Alaska Region
3601 C Street, Suite 1200
Anchorage, Alaska 99503-5947

Ref: Comments regarding the Environmental Assessment for the Native Village of Eklutna Gaming Facility Project-DOI-BIA-AK-61628, Dated December 2024.

Dear Ms. John:

I have the following comments regarding the Environmental Assessment (EA) for the Eklutna Native Village Gaming Facility Project-DOI-BIA-AK-61628, dated December 2024. These comments are based upon review of the EA and the supporting documents and appendices made available for public comment by the Bureau of Indian Affairs (BIA). The availability of this EA was listed on December 22, 2024, in the Anchorage Daily News, allowing for a 15 day public comment period from December 22, 2024 through January 6, 2025.

I retired from years of service with the Federal Government as a social scientist who was involved in writing and evaluating parts of NEPA documents. During that time, I was never aware of an Environmental Assessment that was listed for public review with a 15 day review time instead of the customary 30 to 60 day time frame. Coupled with the chosen short time frame, the Native Village of Eklutna and the BIA further repressed reviews by interested parties by releasing the EA over the two week holidays of Christmas, Kwanza, Hanukkah and other holidays, followed by celebration for New Year's.

I find these decisions, stipulating the shortened timeline and during a two week holiday, to be arbitrary and capricious, and it illustrates further that comments are not wanted or welcome. The appearance given is that the Native Village of Eklutna wishes to continue to clear land and build a casino without public weigh in.

The public notice came out December 22, 2024, the Sunday before Christmas. By then, many people with the expertise we would need for consultation concerning the EA were out of the office for religious holidays, and they will return to work on the day comments are due. Being that most documents were dated and signed off in November, I find this

126-1

date for review suspect in that most people will not have time during holidays to thoroughly evaluate the EA, and the short time frame will prevent people from commenting. I am requesting that the comment period be extended by 15 days, with proper notice for people to comment.

I26-1
cont.

I will briefly make comments on the proposal:

The proposed plan shows that the paved parking lot for the casino would be constructed allowing for a 25' setback from the river. They said they are using the Anchorage Municipal Code 21.45.210 for reference that requires a minimum of 25'. However, this code expired in December 2015. In October 2018, the City Council approved the new code 21.07.020.B.1. This code stipulates a 50' setback from Peters Creek, 25' of which is the standard setback and 25' is for the riparian setback, a total of 50', designed to protect our rivers. Peters Creek is listed specifically by name in a table of listed rivers in Chugiak and Eagle River, with explanations of the setbacks required. The plans need to be redrawn to reflect the 50' setback, as required by law, before the site can be fully evaluated. Also, Peters Creek is very glaciated. During the winter when the river is mostly frozen, water is forced over the banks, flooding the riverbanks for several feet. The soil under the retaining wall on the east side of the site would be prone to erosion if in the winter the glaciated water sheds to the retaining wall. Is there a plan to mitigate this?

I26-2

The EA states there is no concern for the fish entering Peters Creek from the inlet, and that there is a very small fish population. However, there is an active recreational salmon fishery at the mouth of the river during the salmon run. There is the potential for chemical run off from the mostly paved site, and from the large septic system required, to impact the river.

Throughout the EA and supporting documentation, especially within Appendix D, it is stated that the Native Village of Eklutna NVE is not required to adhere to State and Local regulations, codes or ordinances, or that NVE is exempt from paying applicable taxes to State and Local entities. Furthermore, the EA and supporting documents state that NVE **“may”** choose to enter into supplemental agreements for such services as Police, Fire, and Emergency Services from the Municipality of Anchorage or that NVE may take care of these services. This implies that they **“may”** also decide NOT to take care of these services. These statements are not definitive as to how the added services needed will be paid for, and if the plan leaves open the possibility that this project will be of huge financial burden to the Municipality of Anchorage or the State to fund, with no reimbursement for the problems caused by the casino.

I26-3

The fire department in Chugiak is covered by volunteers. The equipment they use is bought with money generated from property taxes which the Ondola lot does contribute to. How is it fair to expect the huge resources needed due to problems caused by a casino with 1,000 machines, a bar, and restaurant to be covered by local tax payers of this small community?

126-3
cont.

The Governing Ordinance drafted by NVE is unsigned and unratified, there is no compelling argument that NVE will provide the services of police, fire, and EMTs or that they will be reimbursing the State or Municipality for the extreme added cost of these services. Furthermore, no signed agreements between NVE and the State or Municipality of Anchorage are included as supporting documents to the EA.

126-4

The Socioeconomic overview describes the Native Village of Eklutna as being a village that is geographically isolated with few job opportunities. They further state that 30% of the members live in poverty. This is an exaggeration and does not justify the building of the casino as a means of employment for their people. The people living in the Village have the same access to jobs as anyone living in the Anchorage Borough or the Matsu Valley, and they are within an easy commute to jobs, whether they choose to work in Anchorage, Eagle River, Chugiak, Palmer, or Wasilla. Only 75 of the 400 members live in the Village. The other 325 live in Anchorage, Eagle River, Palmer, and other locations. Why are they not fully employed when there are so many jobs available throughout the area? The Alaska Daily News often highlights the difficulty of filling open jobs in the area.

126-5

The writers of the Social Science section compared NVE to tribes living on isolated reservations in the lower 48, while describing people in NVE as in a place where employment opportunities are very limited. This may be the case for people living on isolated reservations in the Lower 48, but the Native Village of Eklutna is fully integrated into the surrounding communities and not comparable to those communities on reservations.

There is a Three Bears grocery store located less than five miles from the Village that often has a sign on the door looking to hire more workers. The nearby, local Chugiak post office also posts available jobs. A quick look at jobs listed in Anchorage and Eagle River shows hundreds of available jobs, many only requiring a high school degree or less, so there are jobs available.

In looking at the Traffic Analysis, Appendix A, it states that the existing North Birchwood Loop Road would need upgrading to support the increased traffic. Based on their calculations, traffic will increase more than 10 times the current level. The road is

126-6

currently shared between cars, kids walking to school bus stops, walkers, bicyclists, and horseback riders. There are no streetlights on the road. The road is narrow, curvy, and there are no shoulders. This will require the 1.3 mile road to be redesigned including adding extra turn lanes, shoulders, and stop lights. However, nowhere does the EA state that the expenses incurred for the updated road will be paid for by NVE. What is the NVE and Marnell Gaming planning to do to address the road improvements needed?

126-6
cont.

Based on the lack of signed agreements being included in the EA supporting documents consummated between NVE and the State and/or Municipality or clear statements that NVE will provide for these upgrades and services; it must be assumed that NVE is fully planning to pass the costs on to the State or Municipality. Again, how is it possible for NVE to create a project in a rural setting that will cost taxpayers money to accommodate their plans, while they are exempt from paying taxes themselves?

126-7

The added road updates and added protection from local police, fire, and EMT services will be a huge economic hit to the local community of Birchwood and Chugiak as well as the State and Municipality of Anchorage. How will this be addressed?

The EA describes that the casino is being built by Marnell Gaming, a company out of Las Vegas that owns many casinos. Marnell is providing 30 million dollars for the building of the casino and will receive a large portion of the profits. Is this what the State of Alaska really wants? A company such as Marnell Gaming which is looking to be in business in Alaska with much of the costs associated with the location of the casino, being shouldered by Birchwood, the Municipality of Anchorage, or the State?

126-8

The EA shows only 320 parking spaces for guests but detail that they will have up to 1,000 machines, plus a bar and restaurant. This is an under count of the amount of parking that will be needed at peak hours and it appears these numbers were derived so that they could make all of the parking fit on to this small lot. This is very misleading for the public reviewing the EA. The drivers of cars that can't find parking during peak hours will potentially try to park on Birchwood Loop Road, or nearby residential streets, requiring addition police to manage traffic and causing further problems for local residents.

126-9

The designation for the Ondola lot is currently RESIDENTIAL. This would need to be changed through the Municipality of Anchorage and the process of changing the designation involves notice to local residents, the municipality, and other affected entities. Nowhere in the EA does it state that the process for changing the lot designation has started.

126-10

The EA states in several places that the impacts to the local residential community are manageable and would be offset by some of the taxes generated. This will do nothing to help the residents who have lived next door to this site, some for up to 50 years, and will now have to contend with increased traffic, high crime rates, prostitution and drugs, organized crime, noise 24 hours a day, bright lights, just to name of a few of the detriments to locating this casino in a residential neighborhood.

126-11

Public water and sewage disposal are not available on the Ondola lot. Property owners in the area are responsible for installing their own wells and septic. The EA states that they will install a well that will need to generate up to 94 gallons per minute. The authors of the EA used information from a 1971 document that states there were "...large alluvial fan deposits, gravel and sand..." The EA states that there is a good indication that the groundwater will be plentiful. However, if the authors would have researched further, they would have found a report written in 1979 that revised this opinion and showed that in fact, groundwater is NOT plentiful in the area. Some of the local wells have gone dry and people have had to drill a second well to access the water they need. The underlying ground structure in much of the area is bedrock, which is harder to extract water from.

The letter presented in Appendix C, Attachment 1, by Marnell Consulting indicates that Enterprise Engineering anticipated 4,000 guests per day. Marnell Consulting revised this number to 3,000 guests per day with no explanation. Assuming 4,000 guests per day more accurately reflects the number of guests per day, then the Grand Total Water Demand would equal 11,281gpd with a corresponding 75% Diversity Applied = 8,461 gpd. Was the revised number of gallons required dropped due to limitations of the amount of water that can be obtained by a single well? If so, is this a realistic projection of water use?

126-12

Many of the wells in the area produce 2-5 gpm. The EA states the peak flow would be approximately 94 gpm. Each wastewater treatment unit is designed to operate at a specified hydraulic detention time to ensure optimum performance. What considerations have been made to handle the peak flow?

When I compare water usage from charts on the internet which show usage for restaurants, businesses, factories, etc., it appears the amount of water needed for guests and employees has been underestimated. No doubt this was done so that the proposed septic system would fit within the confines of the usable space. The calculations for amount of water required per person per day for clients and workers is much lower than the suggested amounts EPA lists on their websites. The engineer's

miscalculation in the EA will surely lead to a water system that cannot deliver the amount of water needed for the site, and a septic system incapable of handling the amount of waste that will need to be processed. And this will take place in a floodplain, beside an anadromous stream.

The site map that is included in the Appendix, of local wells in the area, does not show about 65% of the local wells that should be on the map, which helps to downplay potential contamination points from the proposed septic for the casino. The information on local wells is readily available on the DNR website and will give a more accurate picture of wells located in proximity to the proposed development.

If it is found that a well does not deliver the amount of water needed, and that the septic system cannot handle the amount of waste produced, would the NVE ask the Municipality of Anchorage to be connected to public water located 1.5 miles from the site? If so, would residents in the area be expected to also hook up to city water and sewage to help finance these utilities?

Most septic systems need to have a permeable surface above them for the septic to work appropriately. However, the EA shows that most of the site will be paved with an impermeable surface. Will compaction requirements for the parking area impact the integrity of the drain field?

Will the two treatment technologies presented in Appendix C (Orenco Systems and Lifewater Engineering) provide secondary or tertiary (advanced) treatment?

NVE states that the Ondola property has been an allotment that belongs to and has oversight by the NVE and that NVE has provided services to the lot and helped to take care of the lot over the decades. However, the description of the lot found in the EA describes that the lot has derelict vehicles, an unusable well and septic, obvious sewage and waste smell on the property, steel drums that are rusting, and trash. Are we to believe this suggests that NVE has been taking care of this property? The site has been abandoned for over 20 years, and little has been done with the property as it deteriorated. How does this relate to the historical connection and continued use the NVE has to the site?

Aaron Leggett, the President of the Native Village of Eklutna, has stated in public forums he would rather build a casino in the Native Village of Eklutna. If this is indeed the case, and if the NVE fails to obtain all the legal permits needed to build the casino, because the Ondola lot is not Indian Country, why is the Alternative B not slated for being built in the Native Village of Eklutna? This would avoid all conflicts with residential neighbors, problems with increased traffic on a small dark road, and people from the

I26-12
cont.

I26-13

I26-14

Village can walk to work at the event center. This is a much better alternative than trying to change the whole character of the Birchwood community by building this large event center in the middle of a residential neighborhood.

NVE and Marnell Gaming have not received all the needed valid permits for building a casino, and there is currently a lawsuit arguing the legality of using the Ondola lot for the said casino. The NEPA review that is necessary for the BIA to sign off on the project has not been completed. But yet, NVE and Marnell Gaming started the process of clearing trees from the Ondola lot in September 2024, and moved in several trailers and porta potties to the site in November. It appears by observation of these actions, that the BIA is merely completing this NEPA review to maintain the appearance that compliance to this Federal action has been met.

126-14
cont.

Sincerely,

Palma Ingles
inglesalaska@gmail.com
13951 Jarvi Drive
Anchorage, Alaska
99515

• Outlook

[EXTERNAL] Eklutna Village Gaming Facility Project

From Jean Moore <jjgillette@gmail.com>
Date Sat 1/4/2025 8:56 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>
Cc jjgillette@gmail.com <jjgillette@gmail.com>

This is in response to the Eklutna Village Gaming Facility Project.

I am a long-time resident of the Chugiak area. I live approximately a mile from the Project Site.

The temporary gaming facility with approximately 85 gaming machines seems like a "hurry up and get the foot in the door" technique. Why waste time setting up a temporary building when you want to get the full facility up and running in approximately 14 months? I do not support a temporary facility while working on the larger facility.

127-1

2.1.7 Utilities. Chugach Electric is not the electric company that is responsible for electricity in this area.

127-2

2.1.8 "CVFRD Station 34 is the nearest fire station to the Project Site and is located approximately 0.2 miles to the north. Station 34 provides medical response and water rescue assistance, and also responds to motor vehicle accidents." The reality is there are no rescue vehicles housed at Station 34. They need to do more research on the Chugiak Volunteer Fire and Rescue and where their services come from.

127-3

2.1.11 Protective Measures and Best Management Practices

Pg 25 Socioeconomic Conditions The Tribe shall maintain the National Problem Gambling Helpline phone number on its website.

The Tribe will implement operation policies that will include, but are not limited to, employee training, self-help brochures available on-site, signage near automatic teller machines and cashiers, and self-banning procedures to help those who may be affected by problem gaming. The signage and brochures will include problem gambler hotlines. The idea of having brochures for addiction and help lines listed, do you really think someone addicted to gaming is going to all of a sudden see the light and call for addiction help????? No, me either. It might look good on paper but reality says those addicted to gaming don't think they have a problem. This could be a real problem for those families who have poverty level income. They don't need another avenue for frittering away their money.

127-4

3.7.2 Property Taxes. The EA-Report states that the parcel is exempt from property taxes. However, property taxes are what support the fire department and police, road maintenance and snow removal. So, if this facility is not paying any property taxes, they will be getting services for free while the rest of the community funds the services they will be using. This is not fair for the community.

3.7.3 Impacts Pg 66 Alternative A would create an estimated 419 permanent jobs, including 228 direct employment positions at the project. Most of these employment positions are expected to be filled by unemployed and underemployed residents of the Municipality of Anchorage, including tribal members (tribal hiring preference will apply), and other Alaska Natives." Unfortunately most underemployed or unemployed persons are not going to be able to get to a job without the services of public transportation. Bus transportation does not come this far north, and certainly not down North Birchwood Loop.

127-5

3.7.3 Impacts: Pg 67 "Fiscal impacts of Alternative A would include business/sales taxes, payroll taxes, and other applicable taxes both locally and statewide. The Tribe does not pay state property taxes on restricted fee Native Allotments. The Project Site is exempt from state property taxes because it is a restricted Native Allotment. (Table 3.7-3). Thus, Alternative A would have no direct impact on property taxes for the Project Site. Additionally, the Tribe is exempt from federal income tax, but Alternative A would result in some direct tax impacts. Both construction and operation of Alternative A are expected to generate positive fiscal impacts. Tax revenues would be generated for federal, state, and local governments from direct economic activities and secondary activities (i.e., the indirect and induced effects of tribal gaming). The taxes on secondary economic activity may include corporate profits tax, income tax, sales tax, excise tax, property tax, and personal non-taxes, such as motor vehicle licensing fees, fishing/hunting license fees, other fees, and fines. Estimated positive local, state and federal effects from Alternative A are summarized in Table 3.7-9 and Table 3.7-10." Again, if you are not paying property taxes, you are not contributing to the fire and police services, snow removal, road maintenance, and other services through property taxes. We do not have an income tax nor a sales tax in the Municipality of Anchorage so you would not be contributing to revenues.

3.7.3 Law Enforcement, Fire Protection and Emergency Medical Services

“Alternative A would result in an increase in demand for public services that would result in additional costs incurred by public service providers, including law enforcement, fire protection, EMS, and related services. As discussed in Section 2.1, the Anchorage Fire Department (AFD) provides fire protection and emergency medical services (EMS) to the Anchorage area. AFD is assisted by two volunteer fire departments: the Chugiak Volunteer Fire and Rescue Company (CVFRD) and the Girdwood Fire Department. Fire services at the Project Site are provided by the CVFRD. Law enforcement is provided by the Anchorage Police Department. Refer to Section 3.10 for details regarding public services provided to the Project Site.” **CVFRD is a volunteer fire and rescue department. They are not secondary to AFD, they are Chugiak’s main fire and rescue department. All calls would be an additional burden on the volunteers.**

127-6

3.7.3 Crime: “Law enforcement services would be provided to Alternative A as discussed above in Fiscal Impacts and Section 3.10. Whenever a volume of people is introduced into an area, the number of criminal incidents would also be expected to increase. This is true of any large-scale development. Eklutna Native Village Gaming Facility Project ENVIRONMENTAL ASSESSMENT 3-39 3 | Affected Environment and Environmental Consequences However, there would be no increase in the overall crime rate per capita. Potential crime related impacts would be further reduced through the implementation of BMPs listed in Table 2.1-1. See Fiscal Impacts above for further analysis of crime related impacts.” **If there is going to be an increase in the amount of traffic and people in the area, especially based on the numbers in this report, there will certainly be an increase in crime and traffic incidents. Who is paying for those services? Again, this cost is going to fall to the residents of this community.**

3.8.2 “Existing Bicycle, Pedestrian, and Transit System There are no bicycle or pedestrian facilities within the vicinity of the Project Site, nor is there any transit service.” **This is very true and consequently there are no sidewalks or multi-use trails all the way from the Project Site to the New Glenn Highway which makes walking on Birchwood Loop dangerous. Adding a significant amount of traffic to the roadways will only increase the danger of walking on North Birchwood Loop or even on the Birchwood Spur. We do not need an additional 400 cars on the road daily. Birchwood Loop is not designed for that much traffic.**

3.8.3 Impacts: The worker arrival peak would generally be between 6 am and 7 am and the departure peak between 3:30 pm and 4:30 pm.

127-7

Alternative A could generate a total of 8,010 daily vehicle trips during the weekday and 10,550 daily vehicle trips on Saturdays” **This “peak time” does not appear to consider the bus routes on North Birchwood Loop picking up children for school in the mornings and dropping them off in the afternoons. It does not take into consideration what time The Crossing school starts and lets out, nor the school activities that occur at the church school. Chugiak Elementary gets out of school at 2:30 p.m. That traffic to the Old Glenn Highway is packed. What is traffic like at Three Bears at different times of the day? That is a nasty entrance onto Birchwood Loop most of the time. What about the amount of traffic for parents taking kids to school or picking them up that don’t go to their local schools? This traffic will be coming out of all side streets between the Project Site and the Glenn Highway. An increase in traffic will make it difficult coming out of the side streets. Birchwood Loop is not designed for an additional 8000 vehicle trips per day.**

3.10.3 Impacts: “Law Enforcement While there is no definitive link between gaming facilities and crime, the increased concentration of people that would result from operation of Alternative A would be expected to lead to an increase in the number of service calls to the Anchorage Police Department, similar to any other commercial development. As discussed in Appendix D, Alternative A is projected to result in approximately 199 law enforcement calls for service annually. This would represent an increase in call volume of approximately 0.1 percent, which is not anticipated to require the construction of new or expanded facilities. Although Alternative A would result in a minor increase relative to the overall volume of calls within the Anchorage Police Department service area, this would require allocation of funds and resources beyond what would occur under existing conditions on the Project Site.” **While you don’t feel that 199 law enforcement calls annually is an increase, for a small community like Chugiak that would be a significant increase in calls. Again, without paying property taxes, you would not be contributing to the cost for services, the other community members are paying for the services.**

127-8

3.10.3 Impacts: The CVFRD responds to over 1,000 calls for service annually (CVFRD, 2024). An additional 46 calls for service would constitute an increase of less than 4.6 percent, or a 0.1 percent increase when considering calls for service responded to by the Anchorage Fire Department as a whole. While the minimal increase in fire protection services is not anticipated to trigger the need to construct new facilities, this would nonetheless constitute a potentially significant impact. **You cannot combine the additional service calls to the AFD and come up with a very small percent. CVFRD has their own calls and has their own crews. They are a volunteer department.**

I do not support the Eklutna Native Village Gaming Facility Project. There are several families that live very close to the Project Site. They should not have to suffer light pollution, crime, and additional noise. I do not feel there has been sufficient community input nor honest research into areas of safety and concern for the local community.

127-9

Jean Moore
PO Box 671304
Chugiak, AK 99567
(907) 688-0561
jjgilllette@gmail.com



Virus-free.www.avg.com

[EXTERNAL] EA Comments, Eklutna Native Village Gaming Facility Project

From David & Valerie Raatz <dvrantz@mtaonline.net>
Date Fri 1/3/2025 4:41 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

To whom it may concern:

I am writing to submit my comment on the proposed NVE Gaming Facility off North Birchwood Loop Road. I have live in this area for over 30 years, and I do NOT agree with this current site for the casino to be built in area so close to residential neighborhoods. The amount of traffic it would bring would not sustain the current road system in place and would cause unwanted traffic as well to the area. The neighbors in the area bike and walk along the roadside edge of the winding North and South Birchwood Loops. The current road system does not have any bike or walking trails available alongside and therefore could present a hazard for the pedestrians with the additional vehicle traffic.

128-1

As I am not opposed to NVE opening a casino, I am terribly OPPOSED to the current location being proposed. I believe the NVE have much better suited land available that a casino could be built upon.

Please do NOT allow this casino to be built in our neighborhood!

Valerie Raatz
PO Box 670082
Chugiak, AK 99567

• Outlook

[EXTERNAL] EA Comments, Eklutna Native Village Gaming Facility Project

From Sabrina France <sabrinafranceak@gmail.com>

Date Fri 1/3/2025 7:03 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

Dear Jolene,

I strongly disagree with building a casino in the neighborhoods of Chugiak. The land should be preserved for wildlife and should not destroy it or harm the wildlife for a casino.

This establishment encourages alcohol and gambling which leads to violence, vandalism, and drugs. I foresee this becoming a "drug hub", where crime rate, drug abuse, and homeless increase drastically and push out residents. The residents of Chugiak should not be forced to share a backyard with a casino.

We live here for the nature, wildlife, and preservations of our lands— NOT for the city life entertainment.

Chugiak is a family-friendly community, we should feel safe, not threatened with drugs, alcohol, gambling, and violence that the casino will bring. We see the struggle with drug abuse and crime in Alaska, why would that be encouraged? It will not bring good to our people.

Please consider the families who live in the area, we want to preserve our home, not destroy it.

I29-1

• Outlook

[EXTERNAL] Public Input on Eklutna Native Village Casino

From jtamerson@aol.com <jtamerson@aol.com>
Date Sat 1/4/2025 1:43 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

Dear Ms. John,
I am writing to you with my concerns about the proposed Eklutna Village Casino that is being planned for the community of Chugiak.

While I appreciate Eklutna Village trying to make itself more self-sufficient, this casino should not be built in Chugiak. My reasons are many.

To begin with, we are a small, quiet community. This casino would be built on land that is only accessible by driving through neighborhoods on a two lane, winding road. I have visited tribal casinos in other states. They are located out in the middle of nowhere and are accessible from a main highway.

130-1

Secondly, it is my understanding this casino will not be subject to state or local laws or taxes. As such, this casino will add nothing of value to the community and will only add to its burden. Chugiak is a small part of the Anchorage Borough. We do not have our own police force, and our Fire Department is made up of volunteers.

130-2

Thirdly, gambling can be addictive, and that brings disruption and destruction to lives and families. We value family in Chugiak.

And we don't need the increased crime this casino will bring. If they are not subject to our laws, will they serve alcohol? Will there be prostitution? What will prevent organized crime from coming in and taking over this casino?

130-3

For these reasons the Eklutna Village Casino should not be approved.

Please stand with our community and do not approve this casino.

Thank you for your time,
Al and Jackie Clemens
907-863-2887
24518 Teal Lp.
Chugiak, Ak. 99567

[EXTERNAL] Proposed casino 2025

From Amber Mock <amber.mock907@gmail.com>
Date Sat 1/4/2025 10:18 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

To Whom It May Concern,

I am writing to express my strong opposition to the proposed location of the casino in Chugiak. As a resident of Eagle River, I believe that placing a casino in this small, residential area is inappropriate and will have a negative impact on our community. Chugiak is a quiet, family-oriented neighborhood with many schools, parks, and children playing outside. The road leading to the proposed location is dark and winding, making it an unsafe area for increased traffic and visitors, especially late at night. The draw of a casino will inevitably bring alcohol, drugs, and people seeking nightlife, all of which will disrupt the peace of this residential area.

This proposal will not only lower property values but also create an unsafe environment for families. I am not necessarily opposed to the idea of a casino, but I am firmly against this location, which is unsuitable for such an establishment.

Thank you for your attention to this matter.

I31-1

Sent from my iPhone



[EXTERNAL] Chugiak Casino

From andrew france <andrewfrance96@gmail.com>
Date Sat 1/4/2025 1:05 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

I am not a person who is against gambling but I am definitely a person who is against bringing gambling into small communities. Chugiak prides itself on its small community, families and outdoor activities. Bringing a Casino into the mix would have negative effects all around. It doesn't take but a second to google Casinos effects on housing markets in small communities. A recent study showed an 8% reduction in the values of homes near casinos just within a 5 year span. This does not apply to places like Anchorage or Las Vegas because economically they're built for situations like this. I'm extremely disappointed in the native corporation for even considering spending money on something like this when 70% of the homeless population in Anchorage are Native Alaskans. They are only looking out for their pockets and not their people. The casino would not benefit Alaskans, just the corporations. They will not pay taxes, nor follow state and federal law. This WILL raise crime, addictions, and dependencies in our society.

132-1

Sincerely,
Andrew France

• Outlook

[EXTERNAL] Faith B EA Comments, Eklutna Native Village Gaming Facility Project

From F Brock <brockf76@gmail.com>
Date Sat 1/4/2025 11:20 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

To Whom It May Concern,

I am writing to express my strong opposition to the proposed location of the casino in Chugiak. As a resident of Eagle River, I believe that placing a casino in this small, residential area is inappropriate and will have a negative impact on our community.

Chugiak is a quiet, family-oriented neighborhood with many schools, parks, and children playing outside. The road leading to the proposed location is dark and winding, making it an unsafe area for increased traffic and visitors, especially late at night. The draw of a casino will inevitably bring alcohol, drugs, opportunities for trafficking, and people seeking nightlife, all of which will disrupt the peace of this residential area.

This proposal will not only lower property values but also create an unsafe environment for families. I am not necessarily opposed to the idea of a casino, but I am firmly against this location, which is unsuitable for such an establishment.

Thank you for your attention to this matter.

Respectfully,

Faith B
Alaska Resident

133-1

• Outlook

[EXTERNAL] Strong Opposition to Proposed Casino Location in Chugiak

From Renee Scott <reneescott907@yahoo.com>
Date Sat 1/4/2025 10:15 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

To Whom It May Concern,

I am writing to express my strong opposition to the proposed location of the casino in Chugiak. As a resident of Eagle River, I believe that placing a casino in this small, residential area is inappropriate and will have a negative impact on our community.

Chugiak is a quiet, family-oriented neighborhood with many schools, parks, and children playing outside. The road leading to the proposed location is dark and winding, making it an unsafe area for increased traffic and visitors, especially late at night. The draw of a casino will inevitably bring alcohol, drugs, and people seeking nightlife, all of which will disrupt the peace of this residential area.

This proposal will not only lower property values but also create an unsafe environment for families. I am not necessarily opposed to the idea of a casino, but I am firmly against this location, which is unsuitable for such an establishment.

Thank you for your attention to this matter.

Sincerely,
Renee' Scott
907-231-4740

134-1

[EXTERNAL] Re: EA Comments, Eklutna Native Village Gaming Facility Project

From Tami <tschlies@gmail.com>
Date Sat 1/4/2025 6:54 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>
Cc Representative.Dan.Saddler@akleg.gov <Representative.Dan.Saddler@akleg.gov>

From: Tamera Schlies
22353 Lampert Cir.
Chugiak, AK 99567
January 4, 2025

To Whom It May Concern,

I am writing to express my strong opposition to the Eklutna Native Village Gaming Facility Project as detailed in the Environmental Assessment (EA). As a lifelong Alaskan and a resident of the Chugiak area since 1994, I am deeply concerned about the proposed casino and its impacts on our community. While I respect the intention behind the project, I believe the assessment fails to consider the broader Chugiak community when evaluating land use impacts. A casino will have profound and irreversible consequences for our community that have not been adequately addressed.

Social and Economic Impact

Chugiak is defined by its quiet, rural character, small businesses, and family-oriented atmosphere. A casino is fundamentally incompatible with this identity. Its construction and operation will discourage sustainable development and community-focused investments, eroding the very fabric of our neighborhood. Not only will the proposed project require increased police presence, it will also change the nature of our community. Although the project claims to create jobs, many of these positions are likely to be low-paying and will fail to offset the broader economic harm caused by the casino's presence. The EA fails to account for the negative impact on residential property values not only in the immediate vicinity but also throughout Chugiak, a bedroom community that will be fundamentally altered by this large-scale development.

I35-1

Increased Need for Public Services

The EA's section on Law Enforcement, Fire Protection, and Emergency Medical Services relies on generalized data for Anchorage as a whole, ignoring the unique circumstances of Chugiak/Eagle River. This area currently has only three police officers covering the entire community, and they are already overextended. (reference article <https://www.alaskanewssource.com/content/news/APD-increases-ChugiakEagle-River-patrol-shift-by-one-officer-479222973.html>)

I35-2

While the EA mentions the tribe's intent to enter into an agreement with the Anchorage Police Department or establish its own tribal law enforcement, this is unlikely to adequately address the increased demand for public safety across the broader community. Additionally, while the EA considers impacts from an estimated population increase, it appears this is an increase of residents. The real impact will be on the influx of non-residents, which will place a strain on police, fire, and medical services throughout Chugiak/Eagle River. The EA also appears to base its calculations on Anchorage's total population rather than the much smaller population of Chugiak/Eagle River, which is only approximately 33,000 residents.

Chugiak/Eagle River is part of the Municipality of Anchorage but is physically separated from the main city by the JBER military base. Tax measures and bond repayments often apply differently to Chugiak/Eagle River residents compared to Anchorage

residents, with voters only in the given coverage area responsible for paying additional tax. This physical and administrative distinction is further emphasized by an ongoing movement within Chugiak/Eagle River to secede from Anchorage, driven by differing views on city management and resource allocation.

135-2
cont.

An influx of casino patrons into this quiet bedroom community will strain already limited resources, with effects extending beyond the immediate vicinity of the project. Any underestimation of service needs will leave local taxpayers to absorb the additional burden. Compounding this inequity is the absence of property tax contributions from the casino, placing both a financial and social strain on the community.

Traffic and Infrastructure Strain

The proposed casino location is accessible only via a narrow, two-lane road winding through a residential area. The EA estimates a four- to fivefold increase in traffic, which will severely impact residents' daily lives and safety. For instance, children and pedestrians frequently use the shoulders of the road for walking and biking. Pedestrians also must access mailbox clusters on the roadway due to the lack of sidewalks or dedicated pedestrian pathways. Increased traffic on these roads will endanger pedestrians and cyclists alike. (see image of one of the many banks of mailboxes along route <https://maps.app.goo.gl/48MQFSLmjCrmbvrJ7>)

135-3

The estimated traffic increase will also require costly road upgrades, including paving, drainage, lighting, and ongoing maintenance. These expenses will fall to the community due to the casino's property tax-exempt status. Furthermore, increased traffic will heighten the risks of impaired driving and weather-related accidents, necessitating additional law enforcement patrols. The nearest residence to the project site is less than 200 feet away, and many homes line the road that will serve as the primary access route to the casino. See example image of residence and driveway along proposed route here: <https://maps.app.goo.gl/S5JkMom1Bofq4x3F9> Residents along the route will face increased air pollution, noise, and light pollution, as well as potential road drainage contamination.

Conclusion

For these reasons, I strongly urge you to reconsider the approval of this project and explore alternative economic opportunities that align with our community's values and priorities. The current EA inadequately addresses the long-term environmental, economic, and social impacts of the proposed casino, and I believe additional analysis is warranted to fully assess its consequences.

Thank you for considering my concerns. I trust that you will prioritize the well-being of our community in your decision-making process.

Sincerely,

Tamera Schlies

[EXTERNAL] Comment on Eklutna EA

From Bill Sarvela <billcard@mtaonline.net>

Date Sun 1/5/2025 10:48 AM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

 1 attachment (218 KB)

Comment on Eklutna EA.pdf;

Giving the public 15 days to comment on an 1,100 page plus report gives me the impression that you are trying to push this project through as fast as possible. It is my understanding that a 30 day comment period is the normal time frame given to an environmental assessment report.

136-1

2.1.3 Site Access and Parking

The report states that there will be 443 parking spaces at the proposed casino. The casino will have 1,000 gaming machines, a restaurant and a bar. It does not sound like enough parking to accommodate guests and employees during normal operations. This will lead to cars parking on the Birchwood Spur and the side roads. The available parking will be further limited in the winter when the plowed snow takes up space in the parking lot.

136-2

2.1.5 Tree Removal and Land Clearing

The report states that there are 171 cords of wood on the allotment. It also states that the value of the wood does not meet the \$5,000 threshold for requiring a tree harvest permit. Currently a cord of firewood in the area is selling for between \$200 and \$300, which would make the value of the standing timber somewhere between \$34,200 and \$51,300.

136-3

The report states that the BIA has reclassified the allotment as not forest land. No explanation is given for the declassification but the trees are still there.

3.3 Water Resources

Appendix C

2.1 Existing Water Supply Conditions

Figure 2

There are numerous documented wells that are missing from the map, since every house in the area has a water well. They are documented with either the State of Alaska and/or the Municipality of Anchorage.

136-4

Enterprise Engineering, Inc. dug 4 test pits to determine water availability. They would have been much better served to contact Sullivan Water Wells, since for the last 50 years they have drilled most the water wells in the Birchwood Spur area. They would have also been told that the wells with 4 to 5 gallons per minute flow rates were not limited by the size of the pipe but by the lack of water in the well.

I would question the accuracy of a report from a firm that is housed in the same building as the requesting agency.

John Sarvela

21020 Alluvial Street Chugiak

Jolene John, Regional Director
Bureau of Indian Affairs - Alaska Region
3601 C Street, Suite 1200
Anchorage, AK 99503

Ref: EA Comments, Eklutna Native Village Gaming Facility Project

As a resident living directly adjacent to the proposed Eklutna Native Village Gaming Project site, I am submitting my comments regarding this project's Environmental Assessment (EA). I am deeply concerned about the various impacts this development could have on our local community, environment, and infrastructure. This letter highlights specific concerns, supported by evidence and recommendations, to ensure a thorough and transparent review process.

1. Concern Regarding the 15-Day Public Comment Period

This EA's 15-day public comment period, scheduled from December 20 to January 6, overlaps with the Christmas and New Year holidays. This timing severely limits public participation and appears designed to minimize community feedback. Furthermore, this period significantly restricts input from state agencies such as the Department of Transportation, the Department of Fish and Game, and the Municipality of Anchorage.

137-1

Under NEPA guidelines, the typical public comment period for an EA is 30 days, particularly for complex or controversial projects. The proposed casino, as Alaska's first tribal gaming facility, is unprecedented and meets multiple criteria warranting an extended review period. These criteria include:

Unprecedented Nature: As the first tribal gaming facility in Alaska, this project sets a significant legal and social precedent, requiring thorough examination and input from stakeholders.

137-2

Legal and Regulatory Controversy: The February 2024 reinterpretation of federal law allowing this project reversed decades of precedent, creating substantial legal debate.

Community and Infrastructure Impacts: The Birchwood area's infrastructure is not equipped to handle the increased demands of a large-scale casino, raising concerns about traffic, safety, and resource allocation.

137-3

Environmental Sensitivity: The proximity of the project to Peters Creek, a salmon-spawning stream, and its reliance on large-scale groundwater use and a septic system necessitate careful evaluation of ecological impacts.

137-4

Given these factors, a 30-day comment period would allow sufficient time for the local and statewide community and relevant state and municipal agencies to review and respond adequately

137-5

to the project’s implications. The current scheduling limits public engagement and risks undermining the integrity and thoroughness of the review process by excluding critical voices.

I37-5
cont.

2. Concerns Regarding Adjacent Land Purchased by Eklutna

The Native Village of Eklutna has purchased two additional properties adjacent to the project site. While these properties are outside the scope of the EA, their future use remains unclear. During a presentation in November 2024, Aaron Leggett stated that the Tribe intends to create a buffer with these lands and has no plans to place them into trust (Anchorage Unitarian Universalist Fellowship, 2024). However, speculation about potential future developments, such as a hotel or additional facilities, persists without formal commitments.

I37-6

Establishing a formal buffer zone is critical to mitigating potential adverse impacts on nearby properties and should be included as a formal mitigation measure. Transparency with the Birchwood community would align with the Tribe's stated commitment to "being good neighbors."

3. Concerns Regarding Transportation, Safety, and Road Capacity

The Transportation Impact Analysis (TIA) in Appendix A (Bureau of Indian Affairs, 2024c, p.1) estimates that the proposed casino will generate 8,010 weekday trips and 10,550 Saturday trips, highlighting the significant increase in traffic. While the TIA acknowledges that upgrades to some intersections will be required (Bureau of Indian Affairs, 2024c, p.2), it fails to assess the full length of Birchwood Spur Road, Birchwood Loop Road, and their smaller intersecting streets. These narrow, poorly lit roads lack essential pedestrian infrastructure such as sidewalks and crosswalks, creating serious safety concerns for pedestrians, cyclists, and horseback riders who regularly use them.

The EA misrepresents the project as being adjacent to an industrial area when, in reality, it is located in a small, residential neighborhood with unique traffic patterns. This community includes multiple schools—Chugiak Elementary, Birchwood ABC Elementary, Mirror Lake Middle School, and Chugiak High School—and their associated school safety zones, and bus routes with multiple bus stops on Birchwood Loop Road and Birchwood Spur Road, which are particularly vulnerable to increased traffic. The EA fails to address the risks posed to school zones and bus routes during critical times, such as drop-off and pick-up periods. It also overlooks necessary safety measures, including sidewalks, crosswalks, lighting, and traffic-calming measures, to protect children and other vulnerable users.

I37-7

The EA’s focus on AM and PM peak hours does not adequately consider the full range of traffic impacts, including late-night casino traffic, weekend surges, and potential impaired driving incidents. A comprehensive traffic study is needed to address these gaps and evaluate vehicle traffic and the safety of pedestrians and school zones.

If upgrades to roads and infrastructure are deemed necessary to mitigate these impacts, the financial responsibility must rest solely with the Eklutna Tribe. Local taxpayers, the State of Alaska, and the Municipality of Anchorage should not bear the burden of funding improvements required to

support a private gaming facility. These upgrades—such as road widening, additional lighting, marked crosswalks, and sidewalks—must be implemented before the project proceeds to ensure the safety of the surrounding community.

137-7
cont.

4. Concern Regarding Water Usage and Impact on Residential Wells

The EA estimates an average water demand of 8,681 gallons per day for the project, with long-term potable water supplied by an on-site groundwater well capable of a peak flow rate of 94 gallons per minute (Bureau of Indian Affairs, 2024a, p. 2-8). This raises serious concerns about potential impacts on neighboring residential wells.

Appendix C of the EA includes a map of documented water well locations within approximately one-half mile of the project site, showing pumping rates ranging from 4 to 40 gallons per minute (Bureau of Indian Affairs, 2024b, p.4). However, this analysis overlooks undocumented wells and those installed after the EA was completed. Many residents in the area, especially some closest to the project site, depend on low-flow wells with pumping rates as low as 1 gallon per minute. These residents often require water storage tanks or carefully timed usage to maintain an adequate supply. The EA underestimates the project’s potential impact on local water resources by failing to account for these wells.

137-8

Introducing a high-capacity well at the project site could strain the aquifer, reducing water availability for nearby properties. The EA acknowledges that "when installing a new water well, it is important to be aware of the potential impacts to water wells in the surrounding area which are drawing from the same aquifer. In general, impacts to surrounding wells are not a concern..." (Bureau of Indian Affairs, 2024b, p.9) However, this conclusion is overly dismissive and fails to address the unique vulnerabilities of local low-flow wells, which require additional resources to sustain residential water needs. A comprehensive aquifer impact study is necessary to evaluate the potential effects on all documented and undocumented wells to fully understand the risks to local water availability.

**Transparency on this issue is essential. During the above-mentioned presentation, Aaron Leggett emphasized the Tribe's commitment to being "good neighbors," and ensuring that groundwater usage does not adversely impact nearby residents is a critical part of honoring that commitment.

5. Concerns Regarding Wastewater Management

The EA outlines plans for an on-site septic system with a drainage field beneath the paved parking areas (Bureau of Indian Affairs, 2024a, p2-9). However, this design conflicts with Anchorage Municipal Code (AMC) sections 15.65.205B.3 (p. 23) and 15.65.215D.3 (p. 31), which prohibit placing septic systems under paved areas unless specific engineering designs ensure their structural and thermal integrity. Placing a septic system beneath a paved parking lot introduces significant risks, exacerbated by the projected wastewater generation of 8,681 gallons per day. These risks include:

137-9

Compaction Damage

Vehicle traffic on paved areas can compact the soil above the leach field, damaging the system and reducing its ability to filter wastewater effectively. According to the United States Environmental Protection Agency (2024), drain fields are not designed to handle vehicles or heavy equipment driving on them, as soil compaction can harm pipes and reduce the oxygen critical for proper sewage breakdown by soil microorganisms.

Water Infiltration Issues

Paved surfaces prevent rainwater from replenishing the soil, disrupting the septic system's functionality and the natural evaporation process. Impermeable materials like concrete and asphalt reduce the oxygen supply to the soil, which is essential for healthy bacteria that decompose organic matter in wastewater (United States Environmental Protection Agency, 2024).

Environmental Contamination

Any failure in the system, particularly in a high-traffic facility such as a casino, poses a severe threat to nearby water resources, including Peters Creek and residential wells. Improper system use—such as flushing inappropriate items—can lead to system failure. Appendix C of the EA (Bureau of Indian Affairs, 2024b, p.11-12) acknowledges and attempts to address the risks associated with grease and oil management. However, it fails to address other potential contaminants introduced by patrons, such as tampons, wipes, and other non-biodegradable materials, which the EPA specifically identifies as damaging to septic systems.

In a gaming facility with high foot traffic and the likelihood of intoxicated patrons, the risk of improperly disposing of such items increases significantly. These materials can clog pipes, overwhelm the system, and compromise functionality, potentially leading to untreated wastewater contaminating Peters Creek and nearby residential wells.

I37-9
cont.

6. Concerns Regarding Law Enforcement, Fire Services, and Fiscal Impacts

The Environmental Assessment (EA) acknowledges the need for agreements between the Native Village of Eklutna and local services, such as the Anchorage Police Department (APD) and Anchorage Fire Department (AFD) (Bureau of Indian Affairs, 2024a, p.3-38). It also states that if "the Tribe cannot enter into such agreement(s), the Tribe would be required to establish, equip, and staff its own law enforcement and fire departments to service the project site" (Bureau of Indian Affairs, 2024a, p.3-38). However, these agreements remain incomplete, leaving critical planning and resource gaps.

Casinos often lead to increased crime and noise disturbances. Without a finalized law enforcement agreement, uncertainty remains about who will handle incidents involving non-tribal members in the Birchwood community. For instance, which jurisdiction will oversee the response if a casino patron trespasses on private property and causes harm? Suppose the Tribe establishes its own law enforcement and fire departments, as suggested in the EA. Will they have the resources, staffing, and training to serve the project site and surrounding areas effectively?

Jurisdictional confusion is a serious concern for residents near the project site. How will tribal law enforcement coordinate with local and state agencies to ensure seamless service? Additionally, the

I37-10

EA's claim that "fiscal impacts would be less than significant" after mitigation seems speculative without finalized agreements. Taxpayers should not bear the burden of increased service demands, especially since the Tribe does not pay state property taxes on restricted fee Native Allotments.

I37-10
cont.

Binding agreements should be finalized before project approval to ensure safety and accountability. These agreements should clearly define the roles and responsibilities of all agencies, funding mechanisms, and detailed plans for addressing gaps in public safety coverage.

Conclusion and Recommendations

The Eklutna Native Village Gaming Project introduces a range of environmental, social, and infrastructural challenges that demand thorough and transparent review. The following key issues have been identified:

1. **The Public Comment Period:** The 15-day timeframe overlapping major holidays is insufficient for a project of this magnitude and controversy. Extending the comment period to at least 30 days would allow for meaningful input from the local community, state agencies, and other stakeholders.

I37-11

2. **Adjacent Land Use:** The purchase of additional properties adjacent to the project site raises questions about their long-term use. Without formal commitments to maintain these properties as a buffer, concerns about potential commercial development and its impacts on nearby residents remain unresolved.

I37-12

3. **Transportation and Safety:** The increased traffic projected by the casino development will exacerbate safety risks on inadequately equipped residential roads and school bus routes. A comprehensive traffic study and infrastructure upgrades are imperative to ensure community safety, with the financial burden placed on the Tribe, not taxpayers.

I37-13

4. **Water Resources:** The EA fails to account for the impact of the high-capacity well on undocumented and low-flow residential wells. A complete aquifer impact study is essential to evaluate and mitigate potential water resource disruptions, particularly for vulnerable properties adjacent to the project site.

I37-14

5. **Wastewater Management:** The plan to install a septic system beneath a paved parking lot introduces significant system failure risks and environmental contamination. Detailed engineering studies and mitigation plans are required to protect nearby water resources and ensure system integrity.

I37-15

6. **Law Enforcement and Emergency Services:** The absence of finalized agreements with local law enforcement and fire services creates uncertainty and potential jurisdictional gaps. Clear, binding agreements or robust plans for tribal services must be established before the project proceeds to ensure public safety and accountability.

I37-16

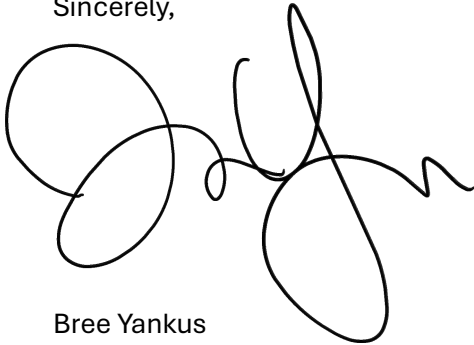
The measures outlined above are critical to addressing the legitimate concerns of the local community and considering the long-term implications of this project. Proceeding without addressing these critical issues would not only harm the local environment and infrastructure but

I37-17

also erode public trust in the process. Decisions of this magnitude should not be taken lightly or rushed to prioritize the economic gains of the Eklutna Tribe.

137-17
cont.

Sincerely,

A handwritten signature in black ink, consisting of several large, overlapping loops and a trailing flourish.

Bree Yankus
PO BOX 671629
Chugiak, AK, 99567
(907) 727-4805
Bree.yankus@gmail.com

References

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[EXTERNAL] Eklutna native village gaming facility project

From Chris Johnson <Johnson8@gci.net>
Date Sun 1/5/2025 9:08 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

I am writing to express my concerns on the proposed casino project in Chugiak. This type of business inherently brings crime and vandalism to an area and the location chosen next to Birchwood airport is a poor location which will draw unwanted attention to the airport. Also being a tax exempt facility that means the local taxpayers are going to pick up the tab for increased maintenance to the road supporting that facility.

138-1

Respectively, Chris Johnson
21766 Chandelle Circle
Chugiak, Ak 9957
Sent from my iPhone

[EXTERNAL] EA Comments, Eklutna Native Village Gaming Facility Project

From JAMES NUGEN <perenial@mtaonline.net>

Date Sun 1/5/2025 9:27 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

When we moved here in 1977 it was because of it being a rural residential area. In 1984 when the municipality rezoned this area, that was the plan, for a rural residential area. A gambling casino does not really fit into this area's lifestyle. Not to mention the additional amount of traffic it will produce on a rural road that also has multiple school bus stops. The only real way to add to the road would be to widen and straighten it. At what cost? Properties will have to be acquired, families moved, the start of the end of our rural area. I cannot count the amount of times coming up North Birchwood Loop to the highway I have had someone pull out of the Three Bears store right in front of me. This store has added a lot of traffic just in the short time it has been here. We use it all the time. It was a positive for the area and the lifestyle. I do not believe that a gambling casino belongs in a residential area or will be a benefit or a positive influence on our area.

James Nugen

139-1

• Outlook

[EXTERNAL] Letter of Concern

From Lee Waters <leewaters907@gmail.com>
Date Sun 1/5/2025 9:31 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

January 5, 2025

Jolene John, Regional Director
US Department of the Interior
Bureau of Indian Affairs, Alaska Region
3601 C Street, Suite 1200
Anchorage, Alaska 99503-5947

Ref: Comments regarding the Environmental Assessment for the Native Village of Eklutna Gaming Facility Project-DOI-BIA-AK-61628, Dated December 2024.

Dear Ms. John:

I have the following comments regarding the Environmental Assessment (EA) for the Eklutna Native Village Gaming Facility Project-DOI-BIA-AK-61628, dated December 2024. These comments are based upon review of the EA and the supporting documents and appendices made available for public comment by the Bureau of Indian Affairs (BIA). The availability of this EA was listed on December 22, 2024, in the Anchorage Daily News, allowing for a 15 day public comment period from December 22, 2024 through January 6, 2025.

The public notice came out December 22, 2024, the Sunday before Christmas. By then, many people with the expertise we would need for consultation concerning the EA were out of the office for religious holidays, and they will return to work on the day comments are due. This is a very short time frame rather than the customary 30-60 day review time for an EA. I am requesting that the comment period be extended by 15 days, with proper notice for people to comment.

140-1

I will briefly make comments on the proposal:

The proposed plan shows that the paved parking lot for the casino would be constructed allowing for a 25' setback from the river. They said they are using the Anchorage Municipal Code 21.45.210 for reference that requires a minimum of 25'. However, this code expired in December 2015. In October 2018, the City Council approved the new code 21.07.020.B.1. This code stipulates a 50' setback from Peters Creek, 25' of which is the standard setback and 25' is for the riparian setback, a total of 50', designed to protect our rivers. Peters Creek is listed specifically by name in a table of listed rivers in Chugiak and Eagle River, with explanations of the setbacks required. The plans need to be redrawn to reflect the 50' setback, as required by law, before the site can be fully evaluated. Also, Peters Creek is very glaciated. During the winter when the river is mostly frozen, water is forced over the banks, flooding the riverbanks for several feet.

140-2

The soil under the retaining wall on the east side of the site would be prone to erosion if in the winter the glaciated water sheds to the retaining wall. Is there a plan to mitigate this?

140-2
cont.

Throughout the EA and supporting documentation, especially within Appendix D, it is stated that the Native Village of Eklutna NVE is not required to adhere to State and Local regulations, codes or ordinances, or that NVE is exempt from paying applicable taxes to State and Local entities. Furthermore, the EA and supporting documents state that NVE “may” choose to enter into supplemental agreements for such services as Police, Fire, and Emergency Services from the Municipality of Anchorage or that NVE may take care of these services. This implies that they “may” also decide NOT to take care of these services. These statements are not definitive as to how the added services needed will be paid for, and if the plan leaves open the possibility that this project will be of huge financial burden to the Municipality of Anchorage or the State to fund, with no reimbursement for the problems caused by the casino.

140-3

The fire department in Chugiak is covered by volunteers. The equipment they use is bought with money generated from property taxes which the Ondola lot does contribute to. How is it fair to expect the huge resources needed due to problems caused by a casino with 1,000 machines, a bar, and restaurant to be covered by local tax payers of this small community?

The writers of the Social Science section compared NVE to tribes living on isolated reservations in the lower 48, while describing people in NVE as in a place where employment opportunities are very limited. This may be the case for people living on isolated reservations in the Lower 48, but the Native Village of Eklutna is fully integrated into the surrounding communities and not comparable to those communities on reservations.

140-4

There is a Three Bears grocery store located less than five miles from the Village that often has a sign on the door looking to hire more workers. The nearby, local Chugiak post office also posts available jobs. A quick look at jobs listed in Anchorage and Eagle River shows hundreds of available jobs, many only requiring a high school degree or less, so there are jobs available.

In looking at the Traffic Analysis, Appendix A, it states that the existing North Birchwood Loop Road would need upgrading to support the increased traffic. Based on their calculations, traffic will increase more than 10 times the current level. The road is currently shared between cars, kids walking to school bus stops, walkers, bicyclists, and horseback riders. There are no streetlights on the road. The road is narrow, curvy, and there are no shoulders. This will require the 1.3 mile road to be redesigned including adding extra turn lanes, shoulders, and stop lights. However, nowhere does the EA state that the expenses incurred for the updated road will be paid for by NVE. What is the NVE and Marnell Gaming planning to do to address the road improvements needed?

140-5

The added road updates and added protection from local police, fire, and EMT services will be a huge economic hit to the local community of Birchwood and Chugiak as well as the State and Municipality of Anchorage. How will this be addressed?

The EA shows only 320 parking spaces for guests but detail that they will have up to 1,000 machines, plus a bar and restaurant. This is an under count of the amount of parking that will be needed at peak hours and it appears these numbers were derived so that they could make all of the parking fit on to this small lot. This is very misleading for the public reviewing the EA. The drivers of cars that can't find parking during peak hours will potentially try to park on Birchwood Loop Road, or nearby residential streets, requiring addition police to manage traffic and causing further problems for local residents.

140-6

The EA states in several places that the impacts to the local residential community are manageable and would be offset by some of the taxes generated. This will do nothing to help the residents who have lived next door to this site, some for up to 50 years, and will now have to contend with increased traffic, high crime rates, prostitution and drugs, organized crime, noise 24 hours a day, bright lights, just to name of a few of the detriments to locating this casino in a residential neighborhood.

140-7

Aaron Leggett, the President of the Native Village of Eklutna, has stated in public forums he would rather build a casino in the Native Village of Eklutna. If this is indeed the case, and if the NVE fails to obtain all the legal permits needed to build the casino, because the Ondola lot is not Indian Country, why is the Alternative B not slated for being built in the

140-8

Native Village of Eklutna? This would avoid all conflicts with residential neighbors, problems with increased traffic on a small dark road, and people from the Village can walk to work at the event center. This is a much better alternative than trying to change the whole character of the Birchwood community by building this large event center in the middle of a residential neighborhood.

NVE and Marnell Gaming have not received all the needed valid permits for building a casino, and there is currently a lawsuit arguing the legality of using the Ondola lot for the said casino. The NEPA review that is necessary for the BIA to sign off on the project has not been completed. But yet, NVE and Marnell Gaming started the process of clearing trees from the Ondola lot in September 2024, and moved in several trailers and porta potties to the site in November. It appears by observation of these actions, that the BIA is merely completing this NEPA review to maintain the appearance that compliance to this Federal action has been met.

I40-8
cont.

Sincerely,

Lee Waters

[EXTERNAL] Fwd: Proposed Eklutna Casino

From Tom <tprunty@gmail.com>
Date Sun 1/5/2025 2:31 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

Attention: Jolene John

I have been a resident of Chugiak for over 47 years. I feel qualified to comment on the draft proposal submitted by Acorn Environmental. Due to the inconvenient short comment time over two major holidays I will keep my comments short and to the point.

I am unable to justify in my mind the need for gambling and probably liquor sales in this quiet neighborhood. There are probably better suited areas than this. Whittier always comes to mind when this subject arises. I41-1

The inaccuracies in the proposal are numerous and I won't address them all here. Some of the obvious ones like the electrical supplier being Chugach Electric just indicate to me lack of concern for detail. I41-2

One of the more glaring oversights seems to me to be the lack of concern for existing infrastructure and the obvious need for concern and/or upgrades.

To my knowledge the Casino would consume more assets than it would contribute. The APD has a minimal presence in the Chugiak Eagle River community now and it would only be more strained with the presence of the Casino. I41-3

Fire and emergency medical services are provided currently by the Volunteer Fire Dept. Additional services required by the casino would be a detriment to all the local citizens. The existing services are adequate but any additional stress would impact all local residents negatively.

Another area I believe is glossed over in this study is the road access. The primary access will be N Birchwood Loop. It presently has several defissonies starting with the fact that there are no shoulders, downhill 20 mph turns and many blind intersections entering onto the road. There are no bicycle or walkways on either side of the road. It appears that the only way to widen and improve the road is property acquisition. Presently snow removal narrows the road and restrictions intersection visibility even further. There is no room for snow when cleared from the roadway and no plan for snow removal. This puts additional pressure on "shortcuts" such as Aurora Borealis Road. It has been controversial for years with numerous closures. It can not afford any additional pressures I41-4

Revenues from the addition of the casino appear to be non-existent to cover any of these additional burdens. The casino would pay no property tax, no sales tax, no State income tax and any tax that would reimburse either MOA or State coffers for all the additional expenses for infrastructure required for the support of the casino. I41-5

- Outlook

[EXTERNAL] Chugiak Casino

From Duke <kvkaiohana@gmail.com>
Date Sun 1/5/2025 1:46 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

We would like to vote a big NO on the construction of a Casino. We do not believe it will be a positive development for the Eagle River / Chugiak community. Pull tabs and casino games encourage drinking, drugs and gambling, which lead to crime. These are the things our community does not need.

I42-1

Respectfully,

Veronica and Kalani Wakinekona

[EXTERNAL] Attn: Jolene John, re: The Eklutna Native Village Gaming Facility Project

From Vicki Prunty <akvicki@gmail.com>
Date Sun 1/5/2025 4:32 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

I have been a resident of Chugiak for over 25 years, and of Alaska for over 55 years. I own two homes in the area adjacent to the quaint, picturesque Starner Bridge. As such, I feel more than qualified to comment on the draft proposal submitted by Acorn Environmental for a gaming project in the remote area of Birchwood/Chugiak. As a long-time resident of the area, my input, preferences, and position should weigh significantly in any project decisions impacting this area.

Marnell Companies' website identifies The Eklutna Native Village Gaming Facility Project as Alaska's first tribal casino. They list it in their portfolio along with the Mirage, the Rio, Palazzo Suites, the Nugget, Harrah's, Bellagio, and many other high-end and other popular facilities known for their gaming.

My first issue regarding this Project is the comment period. The allocated time was not only unreasonably short, but also held over two major holidays, resulting in the opportunity for fewer stakeholders and other interested parties to provide their input. This was both inconsiderate and suspiciously calculated. Due to the inconvenient short comment period I will at this time keep my comments short with *the assumption that my input is considered to be of high value and will be more directly solicited in the near future*. My contact information is provided below.

143-1

The main point I would like to convey is that I am vehemently opposed to the construction of a gaming facility in the area described in the proposal. I have two major reasons for this:

One overriding issue stood out strongly in the Proposal. The lack of contribution, or even a desire to contribute, back to the community was absent. The omission of language addressing this concern was overwhelming. There are references to contributions via sales tax and state income tax, however, these are false statements, as the area in question does not have either sales tax nor state income tax. There was also a reference to property taxes, which is a misleading statement as well, considering that, to my understanding, Eklutna is exempt from paying. Tax-paying duties aside, there is no mention of how the casino will benefit the community or how the casino will give back to the community. Conversely, there is significant references to the use of existing services in terms of fire, police, and emergency medical services, all of which are currently maxed for the existing members of the community.

143-2

My second issue is the lack of sufficient infrastructure for the Project in the proposed area. Traffic, clientele decorum, and public support services are the first to come to mind. A project the size and scope of the one being described will increase traffic flow tremendously. And not just at certain times of the day or week, but most likely 24/7 given the nature of this Project. The proposed site is near the end of North Birchwood Loop, which is a simple two-lane road with little to no availability for expansion. There are steep banks in places, and limited visibility in many areas due to turns in the road, and foliage lining both sides. Birchwood/Chugiak residents value this rural setting, and if they are like me, would not want the road expanded even if it were possible to do such.

143-3

There is significant possibility for overflow of this potential traffic to route onto Aurora Borealis Road, as it leads out onto the Peters Creek exit to the highway for those staffers or casino players heading north towards Eklutna and the Palmer/Wasilla area. Aurora Borealis is a small country lane with a one-lane bridge (Starner Bridge). This avenue is even less capable of handling any increase whatsoever in traffic than North Birchwood Loop, nor is it able to be improved and/or expanded to allow for the influx that will be generated by the proposed project. The residents along this road have already undergone significant impact over the past six or seven years over a challenge over ownership of the road between the Anchorage Borough and local residents. This disagreement went on for several years and resulted in the blockage of the road with large boulders by the locals for extended periods of time, which left only one egress available, and that was over Starner Bridge. During this dispute, Starner Bridge was deemed compromised, and a weight limit was placed on it which in turn prohibited emergency vehicles (such as ambulances and fire trucks), and other service vehicles (such as trash removal vehicles) from accessing the homes between it and the boulders. The impact to those homes during this time was not only disruptive, but dangerous, especially for those residents with life-threatening illnesses. These residents should not be subjected to any additional disruptions in their homes.

143-3
cont.

In addition to these few issues, there are many other considerations regarding a Project of this magnitude and its impact on the community. I will refrain from assuming all of these issues have been properly and sufficiently vetted. For example, there is no public sewer in the area, and homes in the area are primarily serviced with septic systems. Such a system would likely be improbable for a business of this size and scope. Reasonable alternatives would need to be identified and vetted for efficiency and cost-effectiveness. Sewage treatment would be a possible solution, but disposal from this remote site would probably not be the most cost-effective.

143-4

I have no issue with a gaming facility in the Anchorage area. It should, however, be designed in a location with infrastructure that sufficiently supports it without being an undue burden to the community, and more importantly, its creators should be considerate neighbors with significant interest in becoming contributing members of the community and a desire to improve the impacted neighborhood and its residents, and an even stronger desire not to negatively impact the affected area. The Chugiak/Birchwood area is not the appropriate location for this Project.

143-5

Vicki Prunty
PO Box 671628
Chugiak, AK 99567
907.227.2667
akvicki@gmail.com

- Outlook

[EXTERNAL] Ekluta casino project

From Veronica Allmaras <talkeetnavern@gmail.com>
Date Mon 1/6/2025 1:14 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

Jolene John, Regional Director

I am not in favor of the casino project proposed by the Eklutna Native Corporation.

There are many concerns I have: traffic increases, water (there is no municipal source), and disruption to the Birchwood neighborhood.

Veronica Allmaras

I44-1

• Outlook

[EXTERNAL] EA Comments, Eklutna Native Village Gaming Facility Project Jacob Sears Fairbanks, AK

From Jacob Sears <jacobsears@fastmail.com>
Date Mon 1/6/2025 4:13 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

Hello,

I would strongly like to recommend against Alternative A, the gaming hall, and recommended Alternative B, the event center. Gambling has a detrimental effect across our communities, and sinking this sort of investment into it furthers a problematic relationship. Alternative B (the event hall) instead centers the community, while also allowing ample opportunity for the space to bring in tourism, outside business, and generally drive traffic to the area. If there is not support for Alternative B (the event hall) to be built, then I would rather see it not developed at all (alternative C) than for Alternative A (the gaming hall) to move forward.

I45-1

I appreciate the time and energy put into this project, and while I do understand the economic benefits of Alternative A, I think gaming centers come at an increased strain on local support networks and groups, and generally decrease the welfare of the community surrounding.

Thank you,
Jacob Sears
College Rd.
Fairbanks, AK
99709

Comments Regarding the Environmental Assessment for the Eklutna Native Village Gaming Facility Project – DOI-BIA-61628, December 2024

Glen Yankus
700 High View Drive
Anchorage, Alaska 99515
Email: cypakk51@gmail.com
Phone: 907 310-1607

Thank you for the opportunity to comment on the Environmental Assessment for the Eklutna Native Gaming Facility Project. My background includes 35 years of experience with the National Environmental Policy Act (NEPA) for 3 Department of Interior agencies (BLM, MMS and NPS). Areas of expertise include impact analysis, NEPA Project management and agency document review. I also own a residence on Alluvial Drive.

General Comments

My main concerns focus on the public comment period, Public and agency scoping, reasonably foreseeable connected future actions and impacts to adjacent rural residents.

Public Comment Period: The 15-day comment period is unreasonably short and scheduled over the Christmas and New Year holidays (12-22-2024 through 01-06-2025). It is inappropriate to schedule a review over this period because the public has holiday commitments and can't allocate time to provide meaningful comments. Additionally, agency staff take annual leave during the holiday season and are not available to review documents. Typically, agencies provide a minimum 30-day comment period for environmental assessments and adjust the comment period when holidays overlap. **I request that the Bureau of Indian Affairs extent the public comment period 30 days to provide the public and agencies time to provide meaningful comment on the EA.**

146-1

Public and Agency Scoping: Public and agency scoping is a critical component of the NEPA process. It allows the project proponent the opportunity to present the proposed project to the public and receive the public response. The EA does not provide any information on whether a scoping process was initiated. Did a public and agency scoping process occur? **I request that the EA includes documentation of scoping activities that occurred. This includes meetings held, dates and times, # of attendees, and issues presented and the public's response.**

Reasonably Foreseeable Connected Future Actions: The EA provided a complete description of the proposed action and alternatives. A major concern would be potential future actions that expand the scope of the project. Project expansions such restaurants, hotels or other gaming facilities on adjacent lands should be indicated if they are anticipated. Such activities could affect

146-2

nearby residential areas. **Potential future actions need to be analyzed in the cumulative effects section 3.15.2 Land Resources for each impact topic that could impact nearby residential areas.**

I46-2
cont.

Effects on Residential areas located east and south (Alluvial Street) of the project site. The EA states an increase in intensity could result in conflicts with nearby residential areas. Potential conflicts may include air quality and noise impacts from construction activities (Sections 3.4 and 3.11, respectively), an increase in traffic (Section 3.8), visual effects and an increase in lighting (Section 3.13). Each of these sections needs to specifically address the impacts to residential areas east and south of the project site. This is essential for nearby residents to understand how their lives would be affected by the proposed action.

I46-3

Transportation, Safety, and Road Capacity: The Transportation Impact Analysis (TIA) in Appendix A (Bureau of Indian Affairs, 2024c, p.1) estimates that the proposed casino will generate 8,010 weekday trips and 10,550 Saturday trips, highlighting the significant increase in traffic. While the TIA acknowledges that upgrades to some intersections will be required (Bureau of Indian Affairs, 2024c, p.2), it fails to assess the full length of Birchwood Spur Road, Birchwood Loop Road, and their smaller intersecting streets. These narrow, poorly lit roads lack essential pedestrian infrastructure such as sidewalks and crosswalks, creating serious safety concerns for pedestrians, cyclists, and horseback riders who regularly use them.

I46-4

The EA fails to address the risks posed to school zones and bus routes during critical times, such as drop-off and pick-up periods. It also overlooks necessary safety measures, including sidewalks, crosswalks, lighting, and traffic-calming measures, to protect children and other vulnerable users.

The EA’s focus on AM and PM peak hours does not adequately consider the full range of traffic impacts, including late-night casino traffic, weekend surges, and potential impaired driving incidents. **A comprehensive traffic study is needed to address these gaps and evaluate vehicle traffic and the safety of pedestrians and school zones.**

If road and infrastructure upgrades are necessary to mitigate these impacts, the financial responsibility must rest solely with the Eklutna Tribe. Local taxpayers, the State of Alaska, and the Municipality of Anchorage should not bear the burden of funding improvements required to support a private gaming facility. **Upgrades such as road widening, additional lighting, marked crosswalks, and sidewalks—must be implemented before the project proceeds to ensure the safety of the surrounding community.**

I46-5

Mitigating Measures: On November 24, 2024, Mr. Aaron Leggett (President and Chief of the Native Village of Eklutna) gave a presentation to the Anchorage Unitarian Universalist Fellowship Forum (AUUFF) concerning the proposed Eklutna Casino. A question was asked concerning the purchase of 2 properties south of the proposed casino site. The attendee asked Mr. Leggett if they (Native Village) have applied or intend to apply to the Interior Department to

I46-6

have title for the purchased properties to be taken into Trust. Mr. Leggett replied that “they do not intend to put those lands into Trust and like good neighbors we want to create a buffer” (AUUFF podcast, Episode 1124, minute 47:50, Nov. 24, 2024)

I46-6
cont.

Managing these lands to create a buffer space between the Casino development and residential properties would reduce potential adverse impacts to the Alluvial Street property owners. **I recommend that a mitigating measure be developed to accomplish this goal.**

Specific Comments

1.2 Background: This section needs to be amended to provide a discussion of the lawsuit filed by residents of Birchwood in the United States District Court for the District of Alaska.

I46-7

Page 2-1, Last sentence: It is not clear if the project site has existing water rights? The project proponent has applied for water rights with the AKDNR. This sentence needs to be revised.

I46-8

Section 2.1.2 Proposed Gaming Facilities: Do beverage venues include alcohol sales? The sentence should be modified to make it clear that “beverage venues” include alcohol sales.

I46-9

Section 2.1.5 Tree Removal Land Clearing: Why were trees removed from the project site prior to project approval. This needs to be explained in the text.

I46-10

Section 2.1.7 Utilities, Wastewater: Why is the septic drainage field being installed below the paved parking area? Paving over a septic field for any reason is a bad idea. Not only does paving prevent the process of evapotranspiration but also adds significant weight (paving and autos) that could cause the septic pipes to collapse. The parking area should be redesigned to incorporate a nonpaved septic field into the landscaped design.

I46-11

Is the proposed septic field further than 100 feet from Peters Creek per AMC 15.65.060A.1.a?

Section 2.1.10 Operation and Maintenance, 1st sentence: Maintenance activities include snow removal. Where will snow removed from the parking lot be dumped?

I46-12

Table 2.1.11 Protective Measures and Best Management Practices. Table 2.1-1, Visual Resources: A protective measure should be developed to provide a buffer between the project and Alluvial Street residents. A buffer space between the Casino development and residential properties would reduce potential adverse impacts (noise and visual intrusions) to the Alluvial Street property owners. I recommend that a protective measure be developed to accomplish this goal. See General Comments.

I46-13

Section 2.5.3 Alternative Location: The document indicates that there are no lands held in federal trust or restricted fee status for the Tribe that are eligible for gaming. However, another alternative exists. The Tribe could purchase land on the open real estate market and develop a gaming establishment. Has this been considered by the Tribe?

I46-14

Alternative A: Proposed Project, Construction Emissions, page 3-20, 1st paragraph. It should be noted that maintaining a vegetation buffer between the proposed project site and Alluvial Street residences would mitigate effects of fugitive dust, noise and visual intrusion on these rural residences.

I46-15

Page 3-23, Aquatic Resources. Coho and chum salmon also are present in Peters Creek.

I46-16

3.15 Cumulative Impacts: A major concern would be potential future actions that expand the scope of the project. Project expansions such restaurants, hotels or other gaming facilities on adjacent lands should be indicated if they are anticipated. Such activities could affect nearby residential areas. Potential future actions on land recently purchased by the Native Village of Eklutna south of the project site could affect residential areas along Alluvial Street.

I46-17

Potential future actions need to be analyzed in the cumulative effects section 3.15.2 Land Resources for each impact topic that could impact nearby residential areas.

Section 4 Mitigation Measures: I recommend that a mitigating measure be developed to manage the area south of the casino site as open space to create a buffer between the project and Alluvial Street residents. This measure would mitigate the potential effects of noise and visual intrusions on the Alluvial Street residential area. See General Comments, Mitigating Measures.

I46-18

Section 5 Consultation and Coordination: This section lists agencies and organizations consulted during preparation of the EA. It appears that in most cases the BIA and Acorn Environmental reviewed agency documents but did not consult or coordinate with experts from the individual agencies. This is a major concern. This section should document specific consultations made with each agency and summarize the agency's concerns and the outcomes of these discussions.

I46-19

Was the EA sent to Federal and State of Alaska Agencies listed in the Consultation and Coordination Section soliciting review and comment?

• Outlook

[EXTERNAL] Eklutna Gaming Facility

From Darryl Parks <darrylparks@icloud.com>
Date Tue 1/7/2025 9:57 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

My comments are as an aircraft owner that has an aircraft kept at the Birchwood airport.

There are many studies that show an increase in crime related to problem gambling. Birchwood Airport would be a soft target for anyone looking to make easy money for the purposes of gambling.

The airport is not a secure facility and is easily accessible to the public. Many of the aircraft on the airport are not securable due to the lack of hangar storage and the older age of many of the aircraft themselves. Gates around the facility are left open 24/7 making access easy at all hours day and night. The airport does not have, is not patrolled by, security.

Many of the aircraft on the airport have small high dollar avionics which are relatively easy to remove and sell for cash upwards of \$1000 making them easy targets for thieves. Although these conditions exist today, the increased traffic and people in the area driven by the building of a casino open 24/7 would greatly increase the potential for this kind of theft. I encourage the rejection of alternative one and as a compromise look at alternative 2 as the better alternative.

Darryl Parks
18328 Fish Hatchery Rd
Eagle River, AK 99577



147-1

• Outlook

148

[EXTERNAL] EA Comments, Eklutna Native Village Gaming Facility Project

From Mary Pemberton <pembertonmary@yahoo.com>
Date Tue 1/7/2025 7:06 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>
Cc Mary Pemberton <pembertonmary@yahoo.com>

To Whom It May Concern:

I am writing as a resident of Chugiak opposed to plans for the Eklutna Native Village casino.

I live nearby and know the area well. The area is mixed residential/commercial and unsuitable for a casino for several reasons.

- The road leading to the proposed casino site is a windy, two-lane road. The increased traffic and the likelihood of drivers impaired by alcohol and marijuana driving this road is very concerning. ☐ 148-1

- Peters Creek, a salmon-spawning creek, runs adjacent to this site. The environmental damage to Peters Creek is inevitable. ☐ 148-2

_ The social ills in the form of increases in drinking, drugs and gambling addiction is concerning. While these afflictions are very prevalent among all Alaskans, the Alaska Natives are particularly vulnerable and impacted. A casino would worsen these problems. ☐ 148-3

_ Then, there is the questionable history of the site and the legality of it being permitted for a casino in Alaska. The Birchwood Community Counsel has filed a lawsuit to address this. ☐ 148-4

_ Lastly, there is the secrecy surrounding this proposed casino. Land has already been cleared, but many residents of Chugiak are hearing about this proposal for the first time. There needs to be more review given the likelihood of environmental damage to Peters Creek and societal and public safety impacts. ☐ 148-5

Respectfully,

Mary Pemberton
21025 Ferndale Street
Chugiak, AK 99567
pembertonmary@yahoo.com



[EXTERNAL] Casino

From Tom Desalvo <desalvo@mtaonline.net>
Date Tue 1/7/2025 5:22 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

I request you deny permits for gambling. Alaska already has enough problems related to vices and this idea will take advantage or ruin people live for no good reason but to make a tiny native group rich.
I grew up in a Mississippi River town that went full bore river boat gambling and nothing good resulted except making a few people a boat load of money. How about instead we make more effort helping our poor, our homeless and street people and especially our vets instead of looking of ways too make things worse for those groups of people. We will need funding for treating gambling addiction if this disaster goes thru.
Please stop this project before it's too late to stop.
Sent from my iPhone

149-1



[EXTERNAL] Casino in Chugach

From Pamela Carlson <pamelalynncarlson@icloud.com>
Date Tue 1/7/2025 1:25 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

This email has been received from outside of DOI - Use caution before clicking on links, opening attachments, or responding.

Sent from my iPhone

I am writing you in response to the plans to build a Casino in our Beautiful small community of Chugach. I believe that the people in our community live here because they choose a quiet life style . This kind of business promotes drinking and other activities that goes against the values that we want for our Families. There are no financial incentives except for the Bureau of Indian Affairs! It will just disrupt our lives who choose to not live around this nonsense! It's illegal to gamble in Alaska except on Native tribal lands. It's just another addiction to have families deal with! I have seen the problems this brings to communities with this kind of business ! Please don't allow this!

I50-1

 Outlook

[EXTERNAL] Rejection of BIA building a casino

From Russell Carlson <russellcarlson907@gmail.com>
Date Tue 1/7/2025 1:02 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

Rejection of BIA building a casino within the Peters Creek area is because this will bring gambling into the community, another unhealthy addiction for the people.

] I51-1

• Outlook

[EXTERNAL] Concerns regarding the Eklutna Native Village Gaming Facility Project

From Todd Russell <bobol@mtaonline.net>
Date Tue 1/7/2025 3:34 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

To:
Jolene John, Regional Director
Bureau of Indian Affairs Alaska Region

As a long term resident of Chugiak, I have serious concerns regarding the proposed gaming facility location near Peters Creek. My home and property abut Peters Creek on Mountain Road in Chugiak, and I'm quite aware of the potential of ice damming in the area of the proposed site in winter. I find it hard to believe that this site can handle the septic needs of such a facility and provide sufficient water needs without affecting nearby properties. There is also a salmon run and dolly varden population in this watershed that may be impacted by runoff from this property.

I52-1

While the location is close to an industrial zoned area it is surrounded by residential properties and homes. The road access if not adequate to support the traffic anticipated by such a facility, and I find it would be far better suited in an area closer to the existing highway and it's support infrastructure. The increased traffic it would generate would be a burden on our community as well, and this road is dangerous enough without the additional traffic it would bring, especially in the winter.

I52-2

Why the Eklutna Natve corporation chose this location to build on without input from the community and neighbors and tried to quickly get this authorized by this Environmental assessment is troubling.

I52-3

As a member of the Chugiak Community Council, I feel that it would have been appropriate for them to have sought out the area community councils and worked with us to implement their plan – instead this feels as though they are trying to bypass public input, adequate review of the project and without establishing goodwill with our community.

I52-4

I urge that the proposed location by deemed unacceptable for the project, and that should the Eklutna Native corporation be allowed to build a gaming facility- which still remains to be seen regarding their rights to do so, it should be built in a location with acceptable water and waste water facilities, outside of a flood zone and where it will not impact their neighbors as this site will.

I52-5

Respectfully,

Todd Russell
PO Box 672056

19251 Mountain Road
Chugiak AK 99567
907-688-5914

[EXTERNAL] Eklutna Gaming

From Karen Hill <khill73160@yahoo.com>
Date Wed 1/8/2025 5:42 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

To those concerned,

We are taxpayers and long time residents of Alaska and strongly oppose the plan to open a casino in or near Eklutna.

We watched this happen at other places to the degradation if the local area. Crime and homelessness rise significantly, property values decrease, and overall safety concerns and incidents rise, often at the expense - physical, mental, and financial - of the local residents.

We understand the Birchwood community is legally challenging this effort and we support them on the basis' they've outlined.

K. Hill.
Anchorage, AK

[Sent from Yahoo Mail](#)

I53-1

[EXTERNAL] Julie Jorlett, 21025 Alluvial St. Chugiak, AK 99567. "EA Comments, Eklutna Native Village Gaming Facility Project"

From Julie Jorlett <juliejorlett@gmail.com>
Date Wed 1/8/2025 8:53 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

Dear Ms. Jolene John,

I live on Alluvial St. in N. Birchwood, Chugiak. My home is directly behind the Ondola Allotment where Eklutna Corp/Marnell Gaming is planning on building a gaming casino. I am very concerned for many reasons.

There is a lack of honesty and transparency about the plans to build this casino which will severely impact my street and my home. Mr. Leggett and news articles have said there will be 700 machines in a 50,000 square foot facility. The EA statement that was just released states 1,000 machines in a 58,000 sq.ft. facility. There has also been less than 2 weeks to comment on these plans-over Christmas and New Year's holidays. Many of my Chugiak neighbors knew nothing about this casino at all.

154-1

This building will be open 24 hours a day, 7 days a week with a liquor license. Our neighborhood, Birchwood loop and Birchwood Spur Road will not be able to handle this volume of traffic, let alone accommodate the traffic from the exit ramps from the Glenn Highway.

154-2

The Chugiak volunteer EMS/fire dept. is a VOLUNTEER dept. and there are THREE APD officers assigned to Birchwood & Eagle River. A building this large with this many patrons will overburden these public services.

154-3

The plans for this facility rely on a well and septic field....septic that will be under about a dozen parking spaces??? How will this size facility with this many patrons ever be adequately served by one well and a septic field this size? With over 200 employees as well?

154-4

This lot of land is only 8 acres....how can an area of this size adequately hold this large of a building, parking and water/septic. This property immediately borders Peter's Creek...even with a retaining wall planned-how will Peters Creek not be adversely affected?

Construction is said to begin in March...and will happen between 6am and 10 pm daily for 18 months? The noise, traffic and such will be overwhelming. Why can this gaming casino not be built in Eklutna??

154-5

Respectfully,

Julie Jorlett

Property owner at 21025 Alluvial St. Chugiak, AK 99567

(907) 227-3019

Land Acknowledgement

Chugiak/North Birchwood is located within Dena'ina Elnena, the traditional homelands of the Dena'ina Athabaskan people. I thank the members of the Eydlughet (Eklutna) tribe for the privilege of working, living and playing on their ancestral lands.

[EXTERNAL] EA Comments, Eklutna Native Village Gaming Facility Project

From Melissa Crandall <mcrandall245@gmail.com>

Date Wed 1/8/2025 3:18 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

To whom it may concern:

As a parent and citizen of the Chugiak/Birchwood community, I do not agree with the building of a casino in our area. I chose to purchase a home in this area because it is a primarily residential area and a small community. Bringing in a casino that will have restaurants and alcohol served is not what this community needs.

I55-1

The business would increase traffic that way that they are not being taxed to maintain or fix from their traffic. We have no side walks and citizens in the area can no longer use the shoulder of the road safely with an increase in traffic and most likely, an increase in drunk driving.

I55-2

I've heard that the airport is somehow a positive thing to have by the casino. If that is being used as a positive point, then they are not from this area. It is a small airport that private citizens or small businesses use, not some giant airport people would fly into to go gamble.

I55-3

The use of businesses outside of Alaska seems shady. And that business does not have our small community at heart.

I55-4

I am for businesses that will enrich our community, but how does a gambling center enrich our community and the people building it?

Thank you ,
Melissa Crandall

[EXTERNAL] Eklutna Casino

From STEVEN ELLIS <sellissm@aol.com>
Date Wed 1/8/2025 11:53 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

To whom it may concern.

If find it interesting that you are concerned with the environment but have no concern for the impact to the community as a whole. All of a sudden BIA changes its position after 30 years and now this casino can be permitted. We have been told this property will not be subject to local jurisdiction or pay taxes. Who is going to provide Emergency Services and Police response. Since they are not paying taxes I will be opposed to providing Municipal Emergency Services even if the casino pays for it. Get their own Fire Department and Police. The road is too narrow and essentially goes through residential areas. Who is going to pay for the necessary additional upkeep on the road. Obviously not the casino. Statistically the people that go to casinos are the ones that can least afford it. This will ultimately be a blight on our community. If this is such a great idea, why isn't this at the Eklutna Village which has much better access to the highway. I guess they don't want it in their neighborhood either.

156-1

Looking at your EA, you quote the wrong Stream Protection Setback Code and distance. The current Anchorage Municipal Code is 21.07.020. I believe Peters Creek now has a 100 foot setback. Current Municipal Code AMC 21, refers to the Design Criteria Manual, Chapter 2, Volume 1 for drainage requirements. Commercial developments not connected to a municipal or state storm drain system are required to retain the runoff from the 10 year 24 hour event on site or may drain across other property with a drainage agreement.

156-2

I feet this development will have a negative impact on the Peters Creek fish and wildlife habitat.



[EXTERNAL] EA Comments Eklutna Native Village Gaming Facility Project

From Tony LoMedico <tlomed47@hotmail.com>

Date Wed 1/8/2025 6:15 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

BIA,

I am writing to express my concerns for the proposed Eklutna Native Village Gaming Facility Project.

To quote an article on Must Read Alaska:

"The issue at hand is that the proposed property is in a neighborhood with very limited, narrow access, and the people who live there would be overwhelmed and harmed by a 50,000-square-foot casino, with 700 video game machines, bingo, pull tabs, a bar and restaurant. Add to that a septic system and drain field to accommodate hundreds of people, a well to provide water, and run-off from a parking lot that would be able to accommodate hundreds of cars, all add to impacts on the environment."

157-1

To quote a pending lawsuit concerning this proposal:

"That will inflict a direct, concrete, particularized, actual, and immediate injury in fact on the plaintiffs in that The NVE [Native Village of Eklutna] intends the seven hundred video gaming machines in its casino to attract hundreds of patrons who seven days a week will constantly travel in automobiles from the North Birchwood Exit of the Glenn Highway down Birchwood Loop Road to Birchwood Spur Road, then past the intersection of Alluvial Street and Birchwood Spur Road, to the casino. And then back again. That will irreversibly destroy the quiet family atmosphere and rural lifestyle in the Birchwood Spur Road neighborhood that the plaintiffs decades ago moved into the neighborhood to enjoy."

157-2

Further, the proposed gaming facility construction would run concurrently with a major Glenn Highway construction project that is expected to wreak havoc for at least 6 years beginning 2028. Here is the latest article from ADN:

<https://www.adn.com/alaska-news/mat-su/2025/01/08/expect-it-to-be-bad-state-officials-seek-public-input-on-upcoming-glenn-highway-overhaul/?fbclid=IwZXh0bgNhZW0CMTEAAR0borNen->

157-3

[xFtY4StJaLsC1JoqCO5QT8WCGk5SOfG1HIVaLTNkv4e6liruk aem snlXa0nOFPTI35SMWmYdCQ](#)

I57-3
cont.

Clearly, the gaming project has deep negative impacts to the environment and well being of the citizens of Anchorage and the Valley.

My family and I have lived in areas where these types of gaming facilities exist. They always lead to crime, increased homelessness, decreased property values and overall negative impacts to the environment as outlined above.

I57-4

We support the lawsuit brought by the citizens of Birchwood.

Thank you,
Tony LoMedico
2321 Canary Ct
Anchorage AK 99515

• Outlook

[EXTERNAL] Public Comment on Casino

From Kerri Jette <gillette_kerri@hotmail.com>
Date Wed 1/8/2025 11:06 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

To whom it may concern:

As a parent and citizen of the Chugiak/Birchwood community, I do not agree with the building of a casino in our area. I chose to purchase a home in this area because it is a primarily residential area. Bringing in a casino that will have restaurants and alcohol served is not what this community needs.

] 158-1

The business would increase traffic that way that they are not being taxed to maintain or fix from their traffic. There are no sidewalks so citizens in the area can no longer use the shoulder of the road safely with an increase in traffic and most likely, an increase in drunk driving.

] 158-2

I've heard that the airport is somehow a positive thing to have by the casino. If that is being used as a positive point, then they are not from this area. It is a small airport that private citizens or small businesses use, not some giant airport people would fly into to go gamble.

] 158-3

The use of businesses outside of Alaska seems shady. And that business does not have our small community at heart.

] 158-4

I am for businesses that will enrich our community, but how does a gambling center enrich our community and the people building it?

Sincerely,
Kerri

[EXTERNAL] EA Comments, Eklutna Native Village Gaming Facility Project"

From Judi Miller <judimiller24@gmail.com>

Date Thu 1/9/2025 8:35 AM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

EA Comments, Eklutna Native Village Gaming Facility Project"

Dear Commission,

I'm very concerned about having a casino in the Birchwood area. The location itself is in a problematic area due the small roads and high traffic. The roads going in to the casino are very twisty and if there's alcohol is served at the casino, this is also going to impact the small Chugiak area. Alaska is cold and if people go off the road or hit somebody who is walking after being at the casino and also the safety of the people on the road. Will the Eklutna Tribe to responsible for upgrades on the back roads to the casino? Currently the road going back towards the birchwood airport is very icy who will maintain this road with an addition of 500 plus cars? There are no shoulders for people to walk on or pull over.

159-1

The Glenn highway is having a HUGE upgrade for the next three or four years and it's going to be hugely problematic for a casino to run. Anchorage daily news just ran an article saying that they don't want people on the highway due to the construction that it's going to be horrible.

159-2

Alaska has many social concerns which including Domestic violence, rape, murder and people disappearing. The current location is very isolated and remote. Who is going to do security? If the tribe must build a casino a better location and shorter drive would be at the tribal village because it wouldn't be far off of the highway and the village currently have their own off ramp from the Glenn highway. I understand they own the property they're proposing to build on. I

159-3

believe having it if there would have a huge impact on the small community roads, danger to the people in that small community Chugiak and the people visiting the casino due to how far back it is from the Glenn Highway. The better choice would be down further on the Eklutna Tribes own off ramp directly into the Eklutna village because short drive low impact on the small roads, people and community. I'm very concerned about traffic you figure 500 hundred cars on the small back roads. I would suggest that ever person who is making these decisions go drive the back roads and imagine 500 cars zooming by you.

159-3
cont.

Who is supposed to pay for added police fire and emergency search servicess?With the roads being as bad as they are going into Birchwood community they are will be the need for police firefighters and emergency services to help people. It's ridiculous to think that this at cost should go to the community.

159-4

There's a huge animal population in Chugiak bears, moose, wolves and fox. This huge construction project will interrupt the animal flow on the flats.

159-5

Who is supposed to pay for added police fire and emergency search servicess?With the roads being as bad as they are going into Birchwood community they are will be the needed for police firefighters and emergency services to help people. It's ridiculous to think that this at cost should go to the community. What would be the response time for a major accidents on the road or at the casino? Would they even be able to do it without hiring more police firefighters and EMS and then that Burden of cost would be on the anchorage community?

159-6

Thank you for hearing my concerns

Judi Miller

[11940 Suncrest Dr.](#)

[Anchorage, Ak](#)

[99515](#)

Jolene John
Bureau of Indian Affairs, Alaska Region
3601 C. Street, Suite 1200
Anchorage, AK. 99503

Director John,

I hope this letter finds you well in our New Year. I am writing to express my concerns with the proposed casino that is being sought in the Peter’s Creek area.

I live one street away from this proposed site. The area is not conducive for a casino for many reasons, one of which being salmon in Peter’s Creek which would be greatly and adversely impacted. Oil run off from vehicles, noise, light, and human pollution would surely take its toll on this most precious resource of our great state. Something our ancestors would be ashamed of.

I60-1

Water, sewage, garbage, traffic. All big city issues which have absolutely no business in rural Chugiak.

I60-2

Additionally, our children ride the school bus with other children in the area whose bus stops are located on North Birchwood. This street would not be able to contain the amount of traffic that a casino would bring. It is a safety issue for school children as well as those whose driveways/egress fall onto North Birchwood/Birchwood Spur Road.

I60-3

I do not believe that those who are publicly proposing this site are being honest with the public. Mr. Bob Kohler, a resident of over 50 years was recently “bought out” by what he says is “Vegas money”. He was repeatedly approached and did not want to leave the area but was sadly and eventually bought out by “Vegas”. Follow the money. Those who bought out his property told him that the intent was to build not only a casino, but also a hotel and bridge going over Peter’s Creek, along with a huge parking lot/structure. The property Mr. Koehler was ‘forced’ from is zoned residential, along with another parcel of property adjacent to his. How long before Vegas money vanishes this residential buffer and literally places a casino/parking lot/hotel conglomerate in homeowner’s literal backyards?

I60-4

Please do the right thing and not allow this project to go any further until other options are examined. Please do not allow this current rush to continue. A building has already been placed on the property with 24/7 security guards posted. Why the rush before this is even approved, or have the Vegas dollars already purchased decisions? This is ALASKA. This is OUR rural home, all of us. This is NOT the place for a casino. Please, please consider a much more suited space away from our fish, trees, creeks, and neighbors.

Thank you for your consideration.

Tiffani Loughman

[EXTERNAL] "EA Comments, Eklutna Native Village Gaming Facility Project

From Carol Perkins <66togo@gmail.com>
Date Thu 1/9/2025 8:51 AM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

My first comment is: the period did not allow sufficient time to study such an enormous amount of information during the holiday season.

161-1

While I believe the Village has the right to pursue a means of earning money from any and all of their resources, I wonder if this is the best option. I have several concerns:

- 1. I live within 150 feet of Peters Creek and 8 to 10 feet above normal water level. I have seen glaciation high enough to cover the back half of my unimproved (wooded) property. With the removal of all the trees How far and deep will the casino property be covered.
- 2. How will the retaining walls protect the Creek banks during flooding events. Peters Creek likes to move around a bit, especially after a flooding event. I have seen the creek eat up 25 feet of a neighbor's bank in one flooding event. On the other side of me the creek pecked away at their bank for several years.
- 3. How high is the water table there, mine varies from an occasional high of just 8 feet before freeze-up to the normal 15 to 20 feet. The casino property farther down the creek would have the same fluctuations.
- 5. With the amount of water projected to draw from the aquifer, what provision is being made to protect surrounding wells from losing water. Earthquakes in the area have changed water flows and levels in the past. Being on Peters Creek does not guarantee constant availability. I know of 2 wells within 100 feet of the creek that lost water after a *minor* quake.
- 4. How does the Village propose to manage the extra traffic on Birchwood Loop and the Spur road? There will be more traffic or the casino will be a bust. At this time getting on the Loop can be tricky entering from a side road. Who pays for the

161-2
161-3
161-4
161-5
161-6

additional policing as well as wear and tear on the road (The Village does not pay property taxes). The letter does not explain the finances, just jurisdiction options.

161-6
cont.

5. Is Chugach Electric really going to extend lines to the casino? Why not Use MEA?

6. What are they going to do with extra land they bought? It does not show on the site plan.

161-7

I have not been able to fully review the report from Acorn Environmental beyond the main section nor open any of the attachments. I found Acorn did not have a local contact but did use some Alaskan professionals. However several mistakes could be found in the little I could see. At last night's Birchwood Community Council meeting, several people pointed out discrepancies and outdated references. Including the fact we do not have a sales tax and the Village is exempt from the liquor and cigarette tax.

161-8

Why can't they trade (Ondolas that is) the land for some that is better situated for this type of enterprise?

161-9

Carol Perkins
22012 Valley Ave
Chugiak ak 99567

[EXTERNAL] Ea comments eklutna native gaming facility _marc denslinger

From Marc Denslinger <md.clutch1990@gmail.com>

Date Thu 1/9/2025 1:00 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

To whom it may concern,

I'm writing to express my opposition to the development of the Eklutna Village Gaming Facility. After reviewing the environmental assessment (DOI-BIA-AK-61628) I have numerous concerns regarding the negative externalities posted by the project. I substantiate this position based on the below. Time period and time allowance for public opinion

The time allowed for public review of the Environmental assessment and communication to the public around the project is insufficient.

- This project has numerous impacts to assess and thousands of pages of documents to review and adequate time was not allowed to the public to review and provide opinion. Additionally, It was communicated that a copy of the EA was provided to the public library for review and they didn't have a copy. The digital correspondence provided had several documents that could not be accessed and the lack of a physical copy of the plan leaves the community in the dark on components of the change. Knowing the impact this change will have the public deserves more information transparency and time to provide opinion.

162-1

Water table and water shed impacts.

- The Chugiak community's residential presence is dependent on having well water without a significant overhaul to allow for city hookup. The proposed gaming hall is approximately half the size of a las Vegas casino and is going to draw approximately 10x the GPM of a residential home. It's stated in the EA that it's believed the area can produce at this level, but this is not

162-2

substantiated with evidence backed research. The impact of a draw of this magnitude could disrupt the surrounding water tables surrounding residential wells are dependent on. The impact this disruption could have on residential home values could be significant, and again, was not adequately addressed in the EA. Additionally, the impact this draw could have on the Peters Creek water shed was not properly addresses especially considering the inadequate setback of 25 feet from Peters Creek.

162-2
cont.

Community impacts

- The Chugiak community is one closely tied to nature. The peace and serenity this nature provides is something the public seeks to preserve. This is expressed by opinion as observed in community council meetings and stated in the Chugiak Eagle River master plan. Though this project is proposed in an "industrial location", the mentioned industrial activities don't take place 24/7 as the gaming hall will. The gaming hall has a high potential to negatively impact the local environment via its proposed activities, and directly conflicts with the letter and spirit of the Chugiak Eagle River master plan. A few impacts the EA didn't adequately address or used inappropriate/outdated statistics on are Traffic disruptions, noise pollution and increased local crime rates. These issues should be better addressed while considering the local community and commuting traffic on an already bottlenecked highway and knowing municipal resources (Police, fire and rescue, road maintenance) are or will be limited for the facilities location.

162-3

Thank you for your time and consideration of my opinion.

-Marc Denslinger

[EXTERNAL] Casino in Chugiak

From Bryce Palmer <bpalmer@stgincorporated.com>

Date Thu 1/9/2025 6:18 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

This email has been received from outside of DOI - Use caution before clicking on links, opening attachments, or responding.

Sent from my iPhone

I have a few comments on the proposed casino in Chugiak. First of all let me say that I am very much in favor of a casino for the Eklutna people I just am not in favor of the location. It is right next to the Peter's creek which historically floods during breakup in the spring. Also the increased traffic down the narrow and winding two lane road that would access the casino would be an issue. 8 times the current traffic according to your traffic impact study report. This location is basically in a residential area and you have to ask yourself would I want a casino in my backyard? I have a hard time believing that this is the only viable location for this project. I would think the land out by the village of Eklutna would be ideal, right off the Glen Highway, room for expansion, also on native owned land seems like a win win situation. Unless the Eklutna people do not want a casino in their own backyard?

Thank you

Bryce Palmer

Birchwood resident

I63-1

[EXTERNAL] Fwd: Comments on Eklutna Native Village Gaming Facility Project Environmental Assessment

From Cecil Shuman <flycair@gmail.com>
Date Thu 1/9/2025 7:35 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

----- Forwarded message -----

From: **Cecil Shuman** <flycair@gmail.com>
Date: Thu, Jan 9, 2025 at 7:12 PM
Subject: Comments on Eklutna Native Village Gaming Facility Project Environmental Assessment
To: <alaska.nepa@bia.gov>

Here are my comments on the Eklutna Native Village Gaming Facility Project Environmental Assessment. I tried to submit them via hardcopy today, but the BIA offices were closed. Due to the short comment period over the holiday weekend, I didn't get to review the document as thoroughly as I would have liked, but here are my main concerns:

- 1. Chugiak (including Birchwood, Peter's Creek, and Eklutna) is a small, tight-knit community with strong cultural ties to nature, family, and tradition. A casino doesn't align with these values and could bring problems like gambling addiction and financial struggles. In a small community like Birchwood, there aren't enough resources to help people dealing with these issues.
- 2. Birchwood Spur Road and Birchwood Loop Road are narrow, rural roads not built to handle the traffic a casino would bring. The Environmental Assessment (EA) indicates that there will be a substantial increase in vehicle traffic. This could lead to congestion, accidents, and safety concerns, especially with more people coming from outside the community. The extra traffic could also cause more wear and tear on the roads, making repairs more costly for the community. Emergency services could be stretched thin if accidents or other issues arise.
- 3. The EA indicates that the casino will provide numerous jobs (with tribal preference). While the belief is that casinos bring jobs and money, a large-scale gaming hall like this could create more

164-1

164-2

164-3

harm than good. I've seen how gambling leads to addiction, debt, and even domestic violence. People already struggling financially could end up in worse situations, and without local resources to address these issues, the problems could get worse. There are better, more sustainable solutions for the Eklutna Native Village that would benefit the community long term. This is not an isolated village, as stated in the EA, but a community with access to several neighboring towns along the Glenn and Parks Highways that already have businesses providing jobs.

164-3
cont.

4. In rural areas like Birchwood, a casino may not bring the promised economic boost. The project involves an outside investor, Marnell AK, and most of the profits will likely leave the community instead of supporting local businesses and services.

164-4

5. A building of this size and the associated activities could have a significant environmental impact. Peter's Creek is nearby, and construction and waste could contaminate the water, putting the creek and local wildlife at risk. The casino would rely on septic and well systems, which could overload the local infrastructure. The added wastewater could contaminate the groundwater and Peter's Creek, affecting nearby homes.

164-5

6. Gambling also brings an increase in crime, alcohol abuse, and exploitation, making the community less safe overall.

164-6

7. The proposed casino would be 58,000 square feet with parking, but the property is only about 6 acres. How will such a large building and parking lot fit into such a small space? This could lead to crowding, traffic flow issues, and environmental concerns. Additionally, the municipal setback requirement is 50 feet, which further limits the available building area.

164-7

I am not opposed to development on this property, but it needs to enhance the community, not be a detriment. It should not be a 24/7 business that brings crime, traffic congestion, and impacts the groundwater and surface water (Peter's Creek). The large water requirement is concerning, increased crime, decrease in property values and the high traffic volume impacting this community 24 hours a day is very concerning. It's puzzling why a tribal entity would want to bring this type of business into their community, potentially harming their own people.

164-8

Thank you,

Cecil

[EXTERNAL] EA comments, Eklutna Village Gaming Facility project

From Jason Wells <382wells@gmail.com>
Date Thu 1/9/2025 4:57 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

To whom it may concern

While I have not had time to completely review the EA in the allotted time, what I have reviewed raises concerns about how complete this assessment was done.

I don't believe run off into a salmon bearing stream is addressed sufficiently. Peters Creek is well known to change it's channel and for Aufeis or glacier flooding. The area around Stoltze bridge recently was flooded in the low woods. This proposed parking lot is quite large, the run off from highway chemicals, oils, will harm the creek.

I65-1

Being a property owner in the area I have reviewed many water well drill logs. I have a decommission well and drilled a new water well. A planned development for a 94 GPM well is not realistic in this area. If multiple water wells are drilled to produce this volume I feel it could impact the aquifer that the residents rely on for household water. Ground water is in the 8-9 foot deep level in this area, raising concerns about a septic system designed to handle over 8,000 gallons of waste per day.

I65-2

I don't not believe the traffic study is adequate. The data appears out dated. North Birchwood is a narrow dark road with many pedestrians, school bus stops. In the past, road improvements have not moved forward due to private property along the length of the road. There are other feeder routes into the area that are not adequately addressed.

I65-3

The statements of APD and AFD for police and fire I do not feel are reasonable. AFD does not service the area. ADP has 3 officers assigned to the entire area, and its quite a large area. The officers I spoke with do not believe they have authority on sovereign land.

I65-4

The impact of any busy business operating 24 hours a day 7 days a week will impact noise pollution.

I65-5

There was not adequate time to review the document, some files will not open correctly.

I65-6

The EA states a hardcopy would be on file at the library, this was not true.

I65-7

The native village of Eklutna is not geographically isolated as stated, they have there own health clinic.

Thank you for taking the time to review these concerns, I feel a deeper environmental assessment needs to be done to protect the Peters Creek water shed and the wetlands on the tidal flats

I65-8

Jason Wells

[EXTERNAL] Eklutna native Village Gaming Facility

From John Abrams <smarbaj@gmail.com>
Date Thu 1/9/2025 4:55 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

- 1. This facility is VERY close to Peter's Creek. True, it is near the inlet, but contamination is sure to occur in a stream that supports salmon. I66-1
- 2. There are no places to house people coming to the facility for extended periods of time, and those who find no place will surely encroach upon Birchwood Airstrip. Will the Tribe patrol the strip and to prevent vandalism to the hangers and planes parked nearby? I66-2
- 3. The excessive amount of money spent on the casino would go a long ways in providing property for housing the vast number of homeless indigenous people in the greater community, or building/fund funding a rehab center, which is **very** much needed. I66-3
- 4. This is a money-making proposition for Eklutna, but who is going to pay for the excessive wear and trade on Birchwood Loop? And would regular zoning ever permit such a facility in a residential area? I doubt it. I66-4
- 5. Many of the people spending money at a casino are those that have too little to start with. Such temptation puts more people below or further below the poverty line. I66-5

Kay/John Abrams
13710 Savage Drive
Eagle River, AK

[EXTERNAL] Eklutna Village Casino Environmental Assessment

From Patty Friend <sambucus7@gmail.com>
Date Thu 1/9/2025 8:10 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>
Cc Friend Patty <Sambucus7@gamil.com>

As a long time resident of Birchwood, I have concerns about the proposed Eklutna Village gaming facility's impact on the community and how these were addressed in the released environmental assessment.

We received notice of availability of the environmental assessment on December 24th with the comment period ending January 6th, this has now been extended 3 days until January 9th. Given the holidays and amount of material to be reviewed (over 1000 pages plus appendices), plus there was no hard copy available at the Eagle River Library as stated, this review period is inadequate. Please issue an updated notice of availability to the community with adequate time to review document, I propose 30 business days.

167-1

The 25' setback from Peters Creek appears to be inadequate, generally a 50' setback would be required by the Municipality of Anchorage. The active salmon stream experiences glacial flooding plus channels in the creek bed regularly move.

167-2

The Enviromental Assessment fails to address impact of greatly increased traffic on North Birchwood, South Birchwood, and Aurora Borealis. The data sited in the document is outdated. North Birchwood is currently in need of upgrading; it has no shoulders and is curvey. It is inadequate to the proposed increased traffic use.

167-3

The proposed wastewater system appears to utilize a leach field. I was not able to open appendices for more information. Am wondering if it meets DEC standards and was designed by an engineer with Alaskan experience.

167-4

The document states the water level is stable even though wells vary from 38'- 223'. There are wells with great capacity and others with a marginal water supply. I am wondering about the impact of the well or wells supplying the casino on local resident's wells and water supply.

167-5

And of huge concern is the financial impact on the community to support the casino operation . . . costs from road upgrades, police support, fire support and emergency response. There is wording in the

167-6

assessment about generating an increase in state taxes in excess of anticipated costs. This doesn't apply to us. We have no state tax. As proposed, the casino will be a heavy financial burden on the community. How will this be addressed?

167-6
cont.

Thank you for considering these concerns. Environmental stewardship, community well-being and a transparent process benefit us all.

Sincerely,
Patty Friend
18681 Mink Creek Drive, Chugiak 99567
Sambucus7@gmail.com
907-952-2011

[EXTERNAL] Eklutna EA Comments

From Sarah Belway <sbelway@yahoo.com>
Date Thu 1/9/2025 5:39 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

I am a local resident and owner of a hangar at the Birchwood Airport. I drive and walk my dog regularly along North Birchwood Loop. The location is terrible for a 24 hour casino. It is located in a residential area along a quiet 2-lane road. The traffic study indicated that traffic will increase to over 8,000 trips each weekday and over 10,000 trips on a Saturday. This is over 8 times greater traffic volume than there is currently. Please help me understand how this will have minimal impact as summarized in your EA? This will greatly impact the lives of those that live in this neighborhood.

I68-1

In addition, it concerns me that I was not aware of this proposal until AFTER Eklutna purchased the residential property, started clearing it and already placed temporary construction buildings.

I68-2

Sincerely,
Sarah Belway, local resident

Sent from my iPhone

[EXTERNAL] Re: Native Village of Eklutna Casino Environmental Assessment

From Val Jokela <valkur@mtaonline.net>
Date Thu 1/9/2025 4:53 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

I am requesting additional time be provided for the review of the EA and supporting Appendices, due to the technicality of the material. I really want to fully comprehend the scope of the project and impact to the environment and the community. Electronically, probably because of the immensity of the Appendices, I still can't access everything and I know other folks are having the same issues.

169-1

For example:

With what I was able to access regarding the traffic study, I noted that not all access points were analyzed and there are many to be considered that would increase traffic in neighborhoods as well as along Birchwood. Birchwood is not set up to be able to accommodate such a proposed increase in traffic. and

169-2

I also found inconsistencies in the proposed size of the casino: is it 35,000 square feet or 56,000 or 58,000. and

169-3

There are also inconsistencies in the set back from Peters Creek-is it 50, 75 or 100,'

and there is more I would like to have the time to look at so, will there be other opportunities for review and comment in the near future?

169-4

Sincerely,
Valerie Jokela
907-748-5762
Sent from my iPad

> On Dec 31, 2024, at 12:46, Val Jokela <valkur@mtaonline.net> wrote:

>

> I live off of North Birchwood, so I am effected by the proposed Casino and did not receive the Notice of Availability (NA) until the afternoon of December 24, 2024. The NA said the comment period started December 20, 2024, so right off the bat, 4 review days were lost, not to mention the NA hit during the holidays when we were all spending time with our friends and families.

>

> Having said that, I gave it my best and tried many times to look through the referenced supporting Appendices noted in the Environmental Assessment (EA). I could only get up to Page 760 out of 1,073 and was continuously kicked out of the document and could not access anything and started over many times, as well as what I was able to access is very technical and needs more time provided for review and

understanding. So...

> On December 31, 2024 at 1100 when they opened, I went to the Chugiak- Eagle River Library to review the hard copy that was referenced in the NA and that copy has not been provided to the library!!

>

> We need a "do-over" on the timeline for comments and review, and because of the volume of documents and technical nature of the EA and supporting appendices, I would propose 30 business days for review. Week-ends and holidays should not be counted as part of the comment time period. And given that the 15 days fell over 2 holidays and week-ends as well as losing 4 comment days because of late notification, our community has not been provided with enough time to review and comment.

>

> So, in summary, you have not done due diligence to meet the criteria of the Notice of Availability and thence, please do the right thing and issue an updated NA to give our community the time they deserve to review and comment on the EA.

>

> Sincerely,

> Valerie Jokela

> valkur@mtaonline.net

> 907-748-5762

>

>

>

>

>

>

> Sent from my iPad

[EXTERNAL] EA Comments, Eklutna Native Village Gaming Facility Project

From Nicole Denslinger <n.c.miller3@gmail.com>

Date Thu 1/9/2025 9:40 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

Hello,

Thank you for the time for the public to provide comment. The proposed building of the Eklutna gaming hall/casino is of high concern to those that live in the community of Birchwood/Chugiak due to the environmental impacts. Firstly, I would like to describe the area I call home, Chugiak. It is largely a low density, rural residential area that has a few locally owned restaurants with small parking lots, two gas stations, churches, and a grocery store that has a parking lot that is much smaller than those found in the neighboring area of Eagle River, which is about a 10-15 minute drive away from Chugiak via the Glenn Highway. There is a small local bar that is right off the highway, which has a small parking lot with estimated less than 50 spots, and patrons do not drive through neighborhoods to visit the establishment. There is abundant nature, and compared to the rest of the Anchorage Municipality, there is a much higher density of trees and preservation of forest.

Besides the highway that is required to get to Chugiak, once you are off the highway the area has low traffic rates. Local people who live here are mostly who drive in the area. People love to get out in nature here and walk down roads all over the Chugiak/Birchwood area. The area is rural and not developed like Eagle River or Anchorage proper. There are very few sidewalks and pedestrian crosswalks. However, because of the low traffic, and also because the people that make up the traffic are locals, cars slow down and know to accommodate local pedestrians and people riding their bikes that are enjoying the area in which they live.

While Chugiak is within the Anchorage Municipality, it is far away from city center, and the infrastructure is far less developed. There are mostly homes in the area as far as infrastructure goes, and businesses that are visited are mostly from local people that live in the residential area. Chugiak is considered rural residential while parts of Eagle River are considered more suburban/urban. As stated on page 35 of the Chugiak-Eagle River Comprehensive Plan, last updated in December 2006 (<https://www.muni.org/Departments/OCPD/Planning/Documents/FINAL-Feb7.pdf>),

"Rural: An area of low density development primarily consisting of detached houses on lots one acre or larger (one or less dwelling per acre) with on-site well and septic systems. This development results from environmental constraints, preferred lifestyle choices and limited city services. Incidental neighborhoods may be semi-rural consisting of single[1]family homes on half-acre or larger lots (one to two dwellings per acre). The area may also include some low intensity commercial and industrial uses, particularly in the form of home-based businesses. It is generally located in the areas of South Fork, upper Eagle River Valley, and Chugiak (the neighborhoods of Birchwood, Peters Creek and Eklutna Valley)." Also on page 35, an objective for land use is to "Preserve and enhance the identity of established community areas." By Alaskan standards, the proposed gaming hall/casino is not small nor modest as the Eklutna Tribe as been quoted, and would not preserve the rural nature of the residential area.

170-1
cont.

The proposed gaming hall/casino would not preserve and enhance the identity of the established Chugiak area because of the following reasons:

- There are no establishments open for recreation 24/7 in the area. The local grocery store, Three Bears, is open until 9 pm. The establishment that is open the latest is the Chevron off the highway that is open until 11 pm. The Birchwood Salon, which is also right off the highway and not tucked in the neighborhoods/residential area, is open until 2 or 3 am depending on the day. This establishment does not have a large parking lot.
- To summarize this point, public infrastructure that brings in traffic in Chugiak are smaller buildings and have smaller parking lots that do not allow for a large amount of people, especially late at night. Allowing people to recreate and consume alcohol late at night will be disruptive to those that live in the area due to noise and the development of potential "increase in traffic patrol requirements and in the number of calls for police service" that is proposed in the EA analysis.
- The proposed facility is much bigger than any establishment in the Chugiak area that the public frequents. The biggest place in the area is the Three Bears grocery store, and it does not have 400 parking spaces like the proposed gaming hall/casino is projected to have.
- To get to the location of the proposed facility, patrons would need to drive through roads that would go through residential areas. As previously mentioned, these are low density, rural residential areas that do not have high levels of infrastructure. Past the location of the proposed facility there are some commercial businesses, but they largely do not draw in the public such as the proposed facility would. The building of the gaming/hall casino, would draw in a significantly higher amount of people than live in the area.

170-2

Also as stated in the Chugiak-Eagle River Comprehensive Plan, "Commercial development has increased in the area but still accounts for only 2% of total land use. The increase is largely due to the addition of Wal-Mart and Fred Meyer retail stores." One should note that this is in Eagle River, not Chugiak, and while they are usually commented on together, they are spread out from each other in distance and how homes/neighborhoods are designed and built. Chugiak has far less infrastructure and commercial development as it is largely a rural, residential area.

170-3

The Comprehensive Plan also states, "Encourage economic development in rural and large-lot suburban zoning districts that enhances rather than conflicts with the rural character and lifestyle of those areas." The gaming hall/casino that is proposed would fit within an urban, city environment, which again the North Birchwood, Chugiak area is not as it is a residential rural area.

The Tribe acknowledges changing their allotted land from rural to commercial in the EA, "it would convert the rural residential nature of the Project Site, which has historically been utilized by members

of the Tribe as a single residence, to commercial uses." As well as "While the Project Site's parcels are part of a restricted fee Native Allotment, the Municipality of Anchorage zones them for I-1: Light Industrial and R-6: Low Density Residential. The generalized zoning district for light industrial is intended for private and public industrial uses including manufacturing, storage, wholesale, and distribution operation, as well as commercial uses that support and/or are compatible with industrial uses (Municipality of Anchorage, 2023)." A gaming hall/casino does not fit within either category of industrial or low density residential.

170-3
cont.

As stated in the EA, "The Anchorage Municipal Code addressing visual resources by providing guidelines and policies aimed at preserving and enhancing the city's aesthetic and scenic qualities. These policies focus on protecting significant views, maintaining the character of neighborhoods, and ensuring new developments are visually harmonious with their surroundings." Again, the proposed gaming hall/casino would not maintain the character of the neighborhoods. A large buildings with many visitors and high traffic does not fit within a rural residential area where single homes are separated by about 1 acre each, meaning there is low population density. The low population density, again, fits within the rural character of Birchwood/Chugiak.

170-4

Traffic

The EA Traffic analysis conducted by Pannone Engineering Services acknowledges that current traffic in the area of the proposed gaming hall/casino is "generally light traffic." According to the state of Alaska's Department of Transportation and Public Facilities, the Glenn Highway between Anchorage and the Matsu Valley has 33,000 vehicles commuting to work each day. The analysis in the Environment Assessment presented on effects on traffic to the Birchwood area projected upwards of 10,000 visitors on peak days (Saturday). That is a third of the amount of vehicles that commute on the Glenn Highway. For many years, the state of Alaska has been brainstorming solutions to the solve the congestion of the Glenn Highway that commuting brings, and they have never found the funding nor a solution. To propose the building of a gaming facility/casino that would bring in a third of the high amount of traffic that commutes on the Glenn to a road that winds through residential areas and is not built for that amount of traffic is irresponsible as the road is not built for that degree of traffic. The traffic analysis in the EA note that the road is "rolling terrain and curves." Again, the road goes through a residential area and people walk and bike on the road, which has no sidewalks. Increased traffic through an area that has curves will increase the risk of injury and fatality to pedestrians that live in the area. This is not safe to the public and would be a significant negative environmental impact.

170-5

The Environment Assessment acknowledges the increase in noise that traffic will bring from the gaming hall/casino. However, it fails to acknowledge the noise and disruption that will occur from patrons entering and exiting the gaming hall/casino. The large number of visitors expected to visit the gaming hall/casino, which again is more than any establishment in the area, would lead to loud noise/disruption to the area, especially if they are talking in normal or loud tones specifically at night when entering and exiting the facility. The area is still surrounded by residential buildings, with as the Environmental Assessment disclosed, the nearest one being 128 feet from the proposed facility. This is a negative environmental impact.

170-6

The consulting Pannone Engineering Services in the EA also recommended "consulting with the Alaska DOT in regards to their requirements for future capacity and potential costs to recover that capacity through traffic control and/or reconfiguration specifically in the area surrounding the Glenn Highway Interchange. Specifically the On Ramps and Off Ramps at the interchange with generated additional traffic will reduce the LOS from A to C/D at the intersection." C/D was indicated as moderate to long

170-7

delay. This a significant negative environmental impact to the local community as they currently experience "no delay" per the traffic analysis. Locals who are trying to drive to their homes are estimated to face moderate to long delays per the analysis. Local residents could then have an increased use/cost in gas, vehicle maintenance and repair, and affect therefore negatively affect their socioeconomic status, which is a significant social environmental impact.

170-7
cont.

Wastewater

The proposal to have a leach field underneath a paved parking lot as noted in the EA on page 2-9 does not follow COSA Approval Guidelines for the Municipality of Anchorage and thus the leach field would be at risk of failing.

Per the Municipality of Anchorage Certificate of On-Site Systems Approval Guidelines (https://www.muni.org/departments/ocpd/development-services/wells-septic/documents/cosa%20guidelines_2018.pdf), "Verify that no portion of the field is under a driveway, parking area or structures or provide engineer's evaluation demonstrating that the septic system will function in compliance with code requirements for freeze protection, structural stability and access to cleanout/inspection pipes (15.65.210B.5). Prior to 1986, the field could be located under a driveway."

170-8

According to the Alaska Department of Environmental Conservation, Division of Water (<https://dec.alaska.gov/water/wastewater/engineering/maintain-septic/>), "Don't build driveways, storage buildings or other structures over the septic tank or drainfield." and "Septic system failures are a major source of groundwater pollution. Layers of soil act as a natural filter, removing microbes and other particles as water seeps through. Improperly treated water can carry bacteria and viruses that can cause gastroenteritis, fever, common cold, respiratory infections and hepatitis. Groundwater pollution would not only negatively affect residents that live in the area since the proposed building site has houses close by, but it would also negatively affect local wildlife.

Water

The analysis on water in the EA states, "Reported pumping rates ranged from 4 gpm to 40 gpm. The lower flowrate recordings of 4 and 5 gpm are likely attributed to small residential wells where higher flowrates are not needed, and these rates are not reflective of what the aquifer could produce." This may be the case, however it was not assessed if the high water usage by the gaming hall/casino could affect the output of water from wells for local people that live in the area. This could significantly negatively impact the environment of local residents that live in the surrounding neighborhoods.

The analysis also states, "Water wells in the surrounding area are typically installed for small residential purposes and do not require high flowrates. To achieve higher flowrates for the proposed gaming hall facility, a well would be designed and constructed to achieve a higher production rate, such as by targeting specific water-bearing formations, installation of appropriate well screens, and likely be 8" diameter or larger, to enhance inflow and to accommodate a larger pump. Until the well is drilled it is unknown if the well could meet the anticipated peak demands, without the need for a storage tank." If a storage tank is needed because the well cannot meet the anticipated demands, which exceed the residential well rates by over 50%, there would then be a need for water trucks to deliver water. Again, with the proposed leach field installation underneath the paved parking lot, having vehicles, especially heavy vehicles like water trucks, drive over and park on the leach field would significantly contribute to the risk of the leach field failing, leading to groundwater pollution and negative environmental impact.

170-9

Law Enforcement, Fire Protection and Emergency Medical

On page 2-9, the EA states, "Prior to commencement of operations at both the permanent and temporary gaming facilities, the Tribe will enter into a more detailed law enforcement agreement with the APD or will establish its own tribal law enforcement services, consistent with the requirements of the draft amended Eklutna Public Health and Safety and Anti-Discrimination at Gaming Facility Ordinance." This does not sufficiently evaluate the ability of APD to cover the projected increase of police calls, arrests, etc that is suggested by the analysis of the EA. Further information is needed to assess if APD will be able to provide services that will keep the community safe. Please consider that the Birchwood/Chugiak community is far away from the main city of Anchorage. Response time and availability could be limited/delayed due to the distance from Anchorage proper.

170-10

Problem Gambling

As correctly noted in the EA analysis, Alaska does not currently offer any state funded problem gambling services, and the analysis also forecasted that "Given Alaska's prevalence of binge drinking is near the upper end of the national distribution, we would forecast the likely population of Alaska problem gamblers, after casinos are introduced, to be higher than the national average."

This is very concerning for an environmental impact on Alaska as Alaska already has very concerning statistics concerning substance abuse, mental health, and the lack of services available to help treat these individuals who are suffering.

Per the 2023 Alaska Score Card

(<https://health.alaska.gov/Commissioner/Documents/MentalHealth/scorecard/2023-AlaskaScorecard.pdf>), the rate of alcohol induced mortality was 36.2% in 2022 for Alaska, compared to 13.6% in 2022 for the US.

Per <https://www.nami.org/wp-content/uploads/2023/07/AlaskaStateFactSheet.pdf>, " 377, 740 people in Alaska live in a community that does not have enough mental health professionals." The population in Alaska is 733, 391 per the 2020 US Census (<https://data.census.gov/profile/Alaska?g=040XX00US02>). Therefore, the percentage of Alaskans that live in a community that does not have enough mental health professionals is 52% of Alaskans. That is significant. Please also consider that the rate of suicide is very high in Alaska. Per the CDC (<https://www.cdc.gov/nchs/pressroom/sosmap/suicide-mortality/suicide.htm>), Alaska has a death rate of suicide of 27.6%. This is the second highest in the country. There is scientific literature that suicidality is "common among individuals with gambling problems." See article Suicidality among individuals with gambling problems: A meta-analytic literature review - PubMed. Because of the lack of mental health resources in Alaska already, the likelihood that the further creation of problems through gambling would not be adequately supported and significantly negatively impact Alaska and Alaskans.

170-11

There is scientific literature to support "there is a significant relationship between problem gambling and being a victim of IPV." Article: <https://pubmed.ncbi.nlm.nih.gov/25477014/>, Problem Gambling and Intimate Partner Violence: A Systematic Review and Meta-Analysis - PubMed. In Alaska, there is already a significantly high rate of domestic violence in Alaska that is disproportionate compared to the rest of the United States. From the State of Alaska (<https://www.ajc.state.ak.us/publications/docs/ACJC%20Reports/Domestic-Violence-In-Alaska-2022.pdf>), "A report about the CDC's National Intimate Partner and Sexual Violence Survey (NISVS) for

2015 showed that 5.5% of American women experienced violence in the past year, including IPV, sexual violence and stalking.⁷ Alaskan women, with 8.1% reporting experiences of these types of violence in 2015, appeared to experience more than the overall national average." In addition, Alaska Natives are disproportionately affected. Per the report, "The percentage of Alaska Native or American Indian people among both DV victims and DV suspects is disproportionate to the percentage of Alaska Native or American Indian people in Alaska's general population, which is around 16% ." On page 21 of the report, it states the statistic that 37% of victims were Alaska Native of DV incidents investigated by Anchorage PD. Further, "Alcohol was often present in these cases; over 63% of suspects were reported as being under the influence of alcohol when the incident occurred." Please, take into consideration the significant environmental impact that introducing a class II gambling hall will have on Alaskans, including Alaska Natives. Alaska Natives are a minority in Alaska. With all mentioned above, there is concern that there would be disproportionately high impacts to a minority population. Consider the regulatory policy listed in the EA, Executive Order 12898 as well as per the EA "An adverse environmental justice impact would result if any adverse impact to human health or the environment as identified within this document disproportionately affected an identified minority, low-income community, Native American Tribe or other disadvantaged community."

170-11
cont.

In addition, the EA analysis found "estimates of approximately \$1.17 million, \$1.25 million, and \$1 million, indicating that the state of Alaska would need to spend between roughly \$1 million and about \$1.25 million on problem gambling services." This is a significant amount of money. Rather than introducing a larger problem to the state by introducing class II gambling without Alaskan resident input, due to this large sum of money projected that would need to be provided by the State of Alaska and therefore, tax payers, Alaskans should be able to vote in an election if they want to have this problem introduced and spend the funds to support the introduced problem.

Finally, on page 2-1 of Environment Assessment it states, "Additionally, the business lease also acknowledges that the Tribe has jurisdiction over the Project Site and is responsible for providing government services to the Project Site, including social services." These social services are not specifically described in the remainder of the Environment Assessment. With the above significant negative impacts to the environment, social services that the Tribe will provide should be described in detail, which was not done in the EA.

Crime

In regards to the EA's appendix comment on "there has been a decreased chance of being a victim of crime since casinos were developed in Atlantic City." That is not a fair statement to make in regards to the development of a casino in rural, residential Alaska. Birchwood/Chugiak is not a large city, and is again, home to many residents and not developed. There is not a large police presence, and residents would not appreciate a need for increased police presence, which could negatively impact the social environment.

170-12

In summary, the proposed "Alternative A" for the gaming hall/casino would have significant impacts on the environment of the Birchwood/Chugiak community. I urge those responsible for making decisions on this project to reconsider the building of the gaming hall/casino and instead preserve the positive environment and community of Birchwood/Chugiak and all of Alaska as a whole.

170-13

Thank you for your time,

Nicole Denslinger

25138 Prince Cir,
Chugiak, AK 99567

[EXTERNAL] Comments regarding proposed Eklutna casino in Birchwood

From foa@mtaonline.net <foa@mtaonline.net>

Date Thu 1/9/2025 4:35 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

Dear BIA,

I am a longtime (25 year) resident of the neighborhood in which the proposed Eklutna Corp. casino would be located, and I am deeply disturbed and upset by a number of different aspects of this proposed development. I will list those concerns below:

1) First, I must strongly object to the highly disingenuous manner in which the public comment period for the EA of this proposed casino has been conducted. It is quite obvious that this absurdly short and incredibly poorly timed comment period, coinciding with two major holidays (Christmas and New Year's Day), was purposely chosen to minimize both the public's attention to this matter, and to minimize the possibility for the public to be able to comment on it. What a shameful display of corruption this is! You should all be extremely ashamed of yourselves for this underhanded move, and you should know that such disreputable and dishonest tactics only serve to further expand the public's distrust of Eklutna Corp. and the BIA.

171-1

2) The location of the plot for the proposed casino is in a rural and mostly residential area, in which a casino, much less an around-the-clock casino, would be highly incongruous and disruptive. There are no other businesses open to the public in that area, much less on the scale of the proposed casino. The neighborhood would be irreversibly altered by this casino, and NOT for the better, but much for the worse --- via increased traffic, noise, disruptions caused by drunken customers, etc. I would be hard-pressed to try to find a WORSE location in which to site a casino, in fact.

171-2

3) Birchwood Loop Road, which would be the access route for this casino, is a winding, curving two-lane road with no shoulders, with no possibility for future expansion, and which is frequently quite treacherous to drive in the winter and/or in the dark, due to the many side streets which branch off from it, often at sharp angles that require drivers to slow radically before making their turns. Doubling, or more than doubling, the traffic on Birchwood Loop Road from the North Birchwood exit of the Glenn Highway could only lead to a much more unsafe traffic situation for everyone regularly using that road (as I do myself).

171-3

4) On the topic of increased traffic, the EA does not acknowledge the fact that the North Birchwood exit of the Glenn Highway would NOT actually be the only access route to the casino. Everyone in this area, especially including myself, knows that many drivers use my street, Aurora Borealis Road, as a 'shortcut' to and from the Glenn Highway, via the Peters Creek exit, using the intermediate streets of Bill Stephens, Chamber and Starner. This has already long been a bone of contention for those of us who live along Aurora Borealis Rd., and Starner and Chamber streets, as Aurora Borealis is a very narrow, winding, non-standard municipal street (being designated as a "country lane" instead), and seeing traffic doubled or more along it would present an intolerable situation to those of us who live on that road, and who already deal with excessive traffic along it currently. But if the traffic along Birchwood Loop Road were to double, it is inevitable that the traffic along Aurora Borealis Road would double as well.

171-3
cont.

5) I, along with everyone in the area to whom I have spoken about this proposed casino, am deeply skeptical of the claims in the EA that a suitable and adequate well and septic field can be established on this six acre parcel, along with a 58,000 square foot facility and the associated parking lot for it, much less immediately adjacent to, and only 25 feet from, Peters Creek. This looks to many of us as a backdoor and devious means by which municipal water and sewerage may end up being introduced to, and forced upon, this neighborhood and community, in almost perfect opposition to all existing landowners and homeowners.

171-4

6) This proposed casino would be of benefit ONLY to Eklutna Corp, and would bring nothing positive to the neighborhood and community. The casino would, on the contrary, be a significant detriment to the neighborhood, as it would bring many added costs, both financial and social, to the community and to the Municipality of Anchorage as a whole. These significant costs would include increased fire and police protection, greater need for emergency services, and increased traffic and roadway wear and maintenance. Eklutna Corp. would be pocketing all the benefits, while leaving the neighborhood, community and municipality to shoulder all the social and financial costs. This would be extremely inequitable and unfair.

171-5

Please know that, to my knowledge, this casino proposal has brought nothing to this neighborhood and its residents but deep concern, disgust and even outrage. The relations between the general community and Eklutna Corp. would be irretrievably harmed, in my opinion, by their attempts to build a casino in this highly unsuitable and inappropriate location. I do hope that more reasonable decisions will eventually be reached by those involved in pushing this detrimental and community-damaging proposal.

171-6

Sincerely,

Alan Kurczynski
22412 Aurora Borealis Rd.
Chugiak (North Birchwood), Alaska 99567
(907) 688-2177

[EXTERNAL] Comments on Environmental Assessment for the Eklutna Native Village Gaming Facility Project

From Catherine Shuman <cshuman180@gmail.com>

Date Thu 1/9/2025 4:19 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

To Whom it May Concern,

After reviewing the Environmental Assessment for the Eklutna Native Village Gaming Facility Project, dated December 2024, I have the following comments:

General Comments

I would like to formally express my concerns regarding the environmental assessment comment review period. The review period, which took place over the holiday season and lasted only 15 days, was excessively short and failed to provide sufficient time for meaningful input from stakeholders. The timing and duration of this review period undermined the opportunity for a fair and thorough evaluation of the assessment. For future review periods, I would recommend at a minimum a 30 day review to allow for more robust input, especially given the complexity and potential long-term effects of such projects.

I72-1

More concerning, however, is that this limited window for public review erodes trust in the process, as it raises doubts about whether the review was genuinely intended to be inclusive or to consider the perspectives of all interested parties. A more reasonable and accessible review period is crucial to ensure transparency, accountability, and to maintain public trust in the decision-making process for environmental matters.

Additionally, when I visited the Chugiak-Eagle River Library to review the hardcopy, as stated in the Notice of Availability (NOA), I found that no copy was available.

Electric is provided for by Matanuska Electric Company not Chugach Electric.

I72-2

Municipal setback is 50 feet, not the 25 feet stated in the document.

I72-3

Overall Statement

Although light industrial development exists nearby, the proposed placement of a casino in this predominantly residential area could have significant negative consequences. The potential for increased criminal activity—such as theft, fraud, and alcohol-related offenses—could place additional strain on local law enforcement, which is already stretched thin in this community. Moreover, the traffic generated by casino patrons and employees could overwhelm the narrow roads, creating safety hazards and congestion. The presence of a large-scale commercial facility would likely alter the area’s character, leading to a decline in property values as the once tranquil environment becomes disrupted by constant visitor traffic. These factors could negatively impact the quality of life for local residents, including tribal members, making the area less desirable for living.

172-4

While I am not opposed to the development of a casino in principle, I believe it would be more appropriately located in an area where its impact on residential neighborhoods can be minimized. Ideally, the casino should be situated in a location with better traffic infrastructure, closer to municipal water and sewer systems, and in an area better equipped to handle potential increases in crime and public safety concerns. Additionally, selecting a site with greater access to public transportation would benefit employees who rely on these services to get to work. Such a location would allow the economic benefits of the casino to be realized while minimizing disruption to the surrounding community.

Comment #1: Employment and Economic Development

The Environmental Assessment (EA) mentions that Eklutna Native Village faces challenges with poverty and unemployment, but it’s not isolated. Nearby communities like Eagle River, Anchorage, and the Matanuska-Susitna Valley have stronger economies and more job opportunities. Eklutna’s closeness to these areas offers a chance to collaborate and take advantage of nearby job markets to help the Tribe. By focusing on housing, job training, and education, the proposed project could help residents access these opportunities and improve their overall economic situation and quality of life.

172-5

Community outreach, like surveys, could help better understand tribal members’ employment preferences and identify barriers to work, giving valuable insight for future economic development. While the proposed site is near residential neighborhoods, alternative business locations could better align with the Tribe’s economic goals without disrupting the area’s residential vibe.

Comment #2: Land Use and Alternative Economic Ventures

Developing Native Allotment land to generate income is important, but it’s crucial to consider if a casino is really the "best use" of this land, especially considering the potential social and environmental impacts. A casino could bring negative effects like more traffic, crime, and environmental disruptions, which may not address Eklutna’s long-term needs effectively.

172-6

Other options—like job training programs, small business development, or sustainable tourism—might offer more lasting benefits without the risks a casino could bring. While the Indian Gaming Regulatory Act (IGRA) supports development, any project should align with the Tribe’s real needs and values to ensure long-term benefits for the community.

Comment #3: Compatibility with Surrounding Land Uses

The proposed development of Alternative A—a large-scale casino—raises concerns about how it will fit in with the surrounding area, especially the nearby residential neighborhoods. While the casino may fit with commercial and industrial uses, it’s important to assess potential conflicts with residential zones. Some key concerns include:

172-7

- **Air Quality and Noise Impacts:** Building Alternative A could create a lot of noise and dust, especially during construction hours, which could disturb nearby residents. The casino will likely continue to generate noise from people, vehicles, and machinery even after it’s up and running, especially at night.

- **Traffic and Safety Concerns:** Birchwood Spur Road isn't built to handle the increased traffic from casino patrons, workers, and deliveries. The narrow road, plus pedestrian and school bus traffic, could lead to congestion and more accidents, making it less safe for local residents.
- **Residential Zoning Issues:** While tribal jurisdiction might exempt the site from local zoning rules, the development could still conflict with the character of nearby residential areas. The casino could reduce property values if it attracts large crowds or creates significant noise or pollution. The additional traffic could also change the area's peaceful, rural vibe.
- **Aviation Safety:** Birchwood Airport's proximity raises concerns about aviation safety, especially when considering the potential for criminal activity involving private aircraft. Smaller airports don't have the same security as larger commercial airports, making them more vulnerable to theft or vandalism.

172-7
cont.

Comment #4: Site Compatibility

A 58,000-square-foot casino with a large parking lot is a major development for a 6-acre site. It raises concerns about whether the size and infrastructure can be properly accommodated in such a small space. The parking requirements, especially with 1,000 or more spaces, could spread the development out and reduce green space, potentially affecting drainage patterns. A large parking area or a multi-story parking structure could also change the visual and environmental character of the area.

172-8

The scale of the development could increase impervious surfaces, which could worsen flooding concerns, especially since the site is close to Peters Creek and could be impacted by glacial flooding. The project will need to address stormwater runoff, which might be harder to manage given the limited space for landscaping and natural buffers.

Comment #5: Drainage and Flooding

Though the EA says most of the site is outside the 100- and 500-year floodplains, localized flooding—especially near Peters Creek—remains a big concern during heavy rains or spring runoff. The EA doesn't fully consider the potential for glacial flooding, which has affected nearby areas in the past.

Even though the project avoids the 100-year floodplain, it still impacts a small part of the 500-year floodplain (0.09 acres). Disturbing these areas could change water flow and drainage patterns, especially near sensitive areas like Peters Creek. Grading and retaining walls may help with erosion but could also worsen flooding, particularly during extreme weather.

172-9

The stormwater design may meet current standards, but it might not fully account for glacial flooding or extreme storms. Increased impervious surfaces could overwhelm the stormwater system, leading to erosion and water quality issues in Peters Creek. More detailed planning and further studies on stormwater management are needed to prevent negative impacts.

Comment #6: Groundwater Impact

If not managed properly, wells or septic systems could contaminate groundwater, especially if there's insufficient monitoring. Over-extracting groundwater could also deplete local aquifers, impacting surrounding ecosystems and wells. The project is set to rely on one well to meet its needs, and with 17 other wells nearby, overuse or poor management could interfere with local water supplies.

Even though the EA says the project won't affect local groundwater, several factors raise concerns about possible impacts:

172-10

- **Cumulative Groundwater Demand:** The estimated water demand could strain the local groundwater system. Overuse of the on-site well, especially during peak demand, could affect neighboring wells, particularly during droughts or other high-demand periods. A more in-depth assessment of the aquifer's sustainability is needed.

- **Over-Reliance on One Well:** The project depends on a single new well, which could lead to pressure loss in the aquifer and impact nearby wells. It's important to evaluate the sustainability of using just one well and consider backup plans.
- **Recharge Rate Assumptions:** Recharge rates can change depending on weather and land use. Assuming the well will always recharge at the same rate could lead to problems like groundwater depletion.
- **Temporary Water Supply Risks:** Until the well is operational, water will be trucked from local stores, which introduces logistical risks like supply disruptions, road conditions, or delays.
- **Long-Term Sustainability and Monitoring:** It's too soon to say that groundwater will be unaffected. Without detailed data and monitoring, it's hard to ensure that the well and aquifer can sustainably meet the project's needs. Regular monitoring will be critical to avoid negative impacts.

172-10
cont.

Despite claims of sufficient groundwater, the reliance on a single well, the potential impacts on neighboring wells, and uncertainties around recharge rates warrant further investigation. Without additional data and comprehensive monitoring, the assertion that Alternative A will not impact local groundwater resources remains uncertain.

Comment #7: Septic System Impact

The proposed on-site septic system could cause environmental problems if not designed and maintained properly. There's a risk of wastewater leaking into nearby groundwater or surface waters, like Peters Creek, which could harm local ecosystems.

172-11

Even though the project plans to follow state and federal regulations, the septic system still presents risks to water quality. Regular maintenance and monitoring will be important to make sure it doesn't contaminate nearby water resources. Additionally, hauling wastewater off-site in the interim could put extra strain on local roads, leading to faster deterioration.

Comment #8: Property Value Impact

While gaming facilities can sometimes boost local economic activity, the claim that the project won't affect property values near residential areas overlooks several important factors:

- **Proximity to Residential Areas:** The site is close to low-density residential properties, and casinos can bring increased traffic, noise, crime, and light pollution, which could make the area less desirable. This could lower property values.
- **Changes in Neighborhood Character:** A casino could make the area feel less stable and more transient, potentially lowering property values as families look for quieter places to live.
- **Increased Crime and Safety Concerns:** Casinos are often linked to higher crime rates, which could make nearby homes less attractive to potential buyers or renters. A rise in crime could reduce property values.
- **Traffic and Congestion:** A large casino will bring more visitors, creating traffic congestion that could overwhelm the area's infrastructure. This could make the neighborhood less appealing for residents who prefer a quieter, more accessible area.
- **Potential Long-Term Market Effects:** While the casino may create short-term opportunities, long-term effects could hurt property values as the noise, crime, and congestion become more noticeable. It could also limit future residential or family-oriented development.

172-12

The casino may not have a significant impact on commercial or industrial properties, but it's likely to reduce property values in residential areas due to noise, crime, and traffic congestion.

Comment #9: Problem and Pathological Gambling Concerns

The proposed casino will likely increase gambling opportunities in the area, raising concerns about its potential to contribute to problem gambling. While some Best Management Practices (BMPs) are in place to address this, they may not be enough to tackle the deeper social and psychological effects of gambling addiction.

The casino's 24/7 availability could escalate gambling issues, particularly among individuals already prone to problem gambling. While BMPs like employee training and self-exclusion programs can help, studies show that these measures often don't fully prevent the spread of gambling addiction, especially in an environment where gambling is normalized.

Problem gambling can have serious social consequences, including financial hardship, domestic violence, and mental health issues. These problems could put a strain on local health services, law enforcement, and community organizations, and may outweigh the short-term economic benefits the casino brings.

The casino could also lead to the displacement of smaller businesses, particularly those that depend on local foot traffic. Employees in industries tied to localized customer bases, like charitable gaming or local services, may experience job losses or reduced hours.

Comment #10: Crime and Law Enforcement Impact

The introduction of a large casino is likely to increase crime both within the facility and in the surrounding area. Large casinos attract a diverse crowd, including problem gamblers, those prone to exploitation, and criminals targeting vulnerable populations. While the Environmental Assessment (EA) suggests crime rates will remain stable "per capita," it doesn't account for the significant increase in visitors, vehicles, and activity.

The influx of outsiders increases the likelihood of crimes such as theft, fraud, and assault. Problem gamblers may engage in illegal activities to fund their addiction. The casino could also attract trafficking, exploitation, and organized crime. Even with security measures, the volume of visitors presents challenges in managing crime.

A major concern is drunk driving. With a 24/7 operation and alcohol service, there's a heightened risk of impaired driving, leading to accidents, injuries, and fatalities. This puts both casino patrons and the local community at risk. Additionally, crime in the surrounding area could rise as criminals target nearby homes or businesses while visitors are distracted.

During my research of casino's (gaming halls) built in rural areas I found several examples of issues that resulted from these casino's. One example of such negative impacts occurred when the Pequot Tribe opened the Foxwoods Casino in Connecticut, which is situated in a rural area. The development led to a significant rise in traffic accidents, including drunk driving incidents, as the nearby roads were not designed to handle the sudden increase in visitors. The surrounding area also saw an uptick in property crime, including theft and burglary, as criminals targeted local homes and businesses while casino patrons were inside the facility. The local community experienced not only an increase in crime but also disruptions to the quality of life, with traffic congestion and safety concerns becoming serious issues for residents.

The EA's claim that crime rates will remain stable "per capita" overlooks the broader impact of increased foot traffic. More visitors mean more opportunities for crime, which could affect residents, local businesses, and safety. Local law enforcement may become stretched, impacting their ability to respond effectively to incidents.

While the casino may bring economic benefits, the potential for increased crime, including drunk driving, cannot be ignored. More comprehensive crime assessments and prevention strategies are

172-13

172-14

needed to protect both the casino’s patrons and the surrounding community.

Comment #11: Traffic Impact

Birchwood Spur Road is already becoming more congested and will likely face even more strain from the proposed casino development. The EA predicts increased traffic, but it doesn’t fully consider the impact of other nearby developments or the traffic during peak times. This could cause safety issues, particularly for pedestrians and cyclists. Improvements to the infrastructure are needed to accommodate the growing traffic and ensure safety for all road users.

The anticipated increase in traffic from casino patrons, employees, and deliveries will worsen existing traffic congestion. While the EA’s traffic study anticipates increased vehicle volumes, the full scope of traffic impacts, particularly on Birchwood Spur, has not been fully assessed. Infrastructure improvements are critical to accommodate growing demand and prioritize safety for all road users. A detailed, proactive approach to traffic management and infrastructure upgrades is needed to minimize negative impacts on the local transportation system.

The study suggests a notable increase in vehicle volume, but it does not account for the cumulative impact from other local developments or peak traffic periods. Increased traffic on Birchwood Spur could compromise safety, particularly for pedestrians and cyclists, as the road has limited capacity for high volumes of traffic. The potential for accidents, including pedestrian collisions, is heightened as congestion grows.

-----End Comments-----

Thank you for the opportunity to review this Environmental Assessment. After careful consideration, it is evident that a more comprehensive Environmental Impact Assessment (EIA) is needed—one that includes detailed studies on groundwater, flooding, and other potential environmental impacts—before any decisions are made. It is essential to ensure that community engagement continues throughout this process, allowing local residents, including tribal members, to have a meaningful voice in shaping the development. The people who call this area home are deeply invested in its future, with many having lived here for generations.

Given the long-term nature of concerns such as traffic, crime, and environmental degradation, it is critical that any development includes robust provisions for ongoing monitoring and accountability. These measures will help ensure the well-being of the community and the preservation of the environment for future generations.

Regards,

Cecil and Catherine Shuman

172-15

172-16

172-17

[EXTERNAL] Eklutna Native Village Gaming Facility Project

From juleeff@mtaonline.net <juleeff@mtaonline.net>

Date Thu 1/9/2025 2:27 PM

To NEPA, Alaska <Alaska_NEPA@bia.gov>

I think this is a great idea. It'll give my visiting guests a place to go for entertainment on cold winter nights or rainy summer days without having to drive into Anchorage. I have always wondered why we didn't have anything like the tribes in the Lower 48. While this isn't exactly the same thing, it's a start.

173-1

Julee Faso-Formoso

[EXTERNAL] Public Comment - EA Comments, Eklutna Native Village Gaming Facility Project

From KIMBERLY COLLINS <kdhc@aol.com>
Date Thu 1/9/2025 4:28 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

This is submitted as EA Comments, Eklutna Native Village Gaming Facility Project. Please let me know you received this email. Thank you. -KC

The Environmental Assessment (EA) for the proposed Eklutna Native Village Gaming Facility Project (referred to as Project subsequently) has several Fatal Flaws. Judging from this Environmental Assessment, no action is the only alternative, since a gaming facility would only be allowed on the Ondola allotment. Following are my comments and evidence in support:

174-1

Community Councils are listed as voluntary, as if they have no official action or purpose and no authority. In fact, Community Councils are the most-local form of government for the area, and report to the Assembly of the Municipality of Anchorage. Community Councils are established in Anchorage Municipal Code, and Councils are to be notified when action is being taken within their jurisdiction.

174-2

Community Councils were not consulted in advance, nor were they informed of the EA availability in a timely fashion.

There is no public water or sewer at the Project site. Impact to the aquifer that residents in the area draw their well water from must be considered. Estimations on depletion due to the Project as well as the impact of groundwater must be considered and are not in this EA.

174-3

Utility traffic on the internet would be significantly hampered if the Project proceeded, as currently the area has limited connectivity during peak periods. A full study must be done before determining what must be implemented to mitigate and provide for sufficient accessibility for WiFi.

174-4

The traffic plan is not feasible from a technical standpoint and using common sense. There are no adjustments that can be made to the traffic pattern, and as such this environmental assessment must be found to have no action as the only alternative.

174-5

Police service and road maintenance are paid for with taxes. The Ondola allotment is not subject to taxation, so the community will bear the burden for providing these to the Project. Even if NVE provides police on the premises, there will be a significant impact to the amount of patrolling needed for safety and traffic. We currently have a maximum of 3 officers at any one time on duty to cover the area from Eagle River to Eklutna. This is a very large area, and additional personnel would be required. Fire and EMS are provided by Chugiak Volunteer Fire Department, and responding to a mass event at the Project would be impossible.

174-6

The traffic plan is not feasible from a technical standpoint and using common sense. There are no adjustments that can be made to the traffic pattern, and as such this environmental assessment must be found to have no action as the only alternative.

The traffic plan has several fatal flaws:

1. The Institute of Transportation Engineers (ITE) Trip Generation Manual, 11th Edition (TGM11), includes walking, bicycling and mass transit as modes of travel for Casino (Land Use 473). The Project is in an area without sidewalks, bicycle lanes or paths, or public transit. Data and assumptions that includes these modes of transportation would not apply to this Project, and therefore estimates derived from data in TGM11 would not be accurate, and in fact would be an extreme UNDER-estimate of "trips" for the corresponding patron count. It does not take into account the trips for 1000 gaming devices that will be on the facility. Only 700 are included. It also does not account trips for patrons at the restaurant, bar, administration, and workers who will be on site.
2. For Project trip generation purposes, rather than Land Use 400s, a better category in TGM11 may be the 200s, or Residential Land Uses. Walking and bicycling would generate zero "trips" for traffic data. Mass transit would count many patrons being delivered by a bus, for example, as one "trip" for traffic purposes, but there will not be bus service. Residential Land Uses would more closely approximate trips for this Project, but even at that, the uniqueness of this Project and its location in a quiet, single-family area, should require Trip Generation estimates to be based on real counts from this Project rather than estimates from the TGM11.
3. The Traffic Plan does not take into account trips and traffic from Birchwood Airport, additional transient parking required for aircraft, or shuttle traffic between the Project and the airport.
4. Highway Capacity Manual's Level of Service (LOS) D refers to a level of traffic flow where speeds start to decline noticeably, density increases rapidly, and drivers have significantly reduced ability to maneuver, resulting in decreased comfort and potential for minor delays. A point where traffic is approaching congestion, but not yet fully jammed. **LOS D:** Density increases faster, and speed begins to decline. There is limited ability for drivers to maneuver, and drivers experience reduced comfort.

174-7

174-8

Traffic is unable to absorb disruptions, and minor incidents will cause queueing. PES, the contractor for the Traffic Plan, uses a LOS C/D for peak service hours. Combining these two Levels of Service does not give a clear picture of potential disruptions in traffic. An appropriate traffic plan would judge capacity including the highest amount of trips, using a rural residential road basis.

174-8
cont.

5. It does not take into account delays or safety concerns due to busses stopping to pick up children for school.

6. It does not take into account ice and snow in this specific area on this specific roadway, which is winding and has driveways, blind corners, and hills along it.

174-9

7. It does not consider people recreating in their neighborhood, walking dogs, strolling babies and toddlers, teenagers out on bicycles and skateboards, riding horses, etc.

8. The planned parking does not account for busses, RVs, vehicles pulling trailers or boats. These types of vehicles will surely be at the Project site.

174-10

9. The plan does not consider noise. The community and location is quiet enough you can hear your neighbor chatting through an open window. The Project is inconsistent with the neighborhood in which the Ondola allotment is located.

174-11

For all of these reasons, the Traffic Plan must be considered to be fatally flawed. It is not feasible from a technical standpoint and using common sense. There are no adjustments that can be made to the traffic pattern, and as such this environmental assessment must be found to have no action as the only alternative.

174-12

Alternatives are not feasible for this project, since it must be built on the Ondola allotment and travel must be via Birchwood Loop. There are technical issues with this facility on the Ondola allotment that render it incapable of being built, such as ongoing maintenance difficulties of snow removal and will a high likelihood of environmental impacts into a salmon-bearing stream and multiple traffic impacts to the community.

174-13

Finally, I would like to comment on the 15-day public comment period. If the BIA had sought to have the most inconvenient and least available time for the public, it found it in the window established for the 15-day public comment. It allowed an additional 3 days, but this is still not sufficient, especially as the public has not heard through Community Councils or any other venue about this until December 20th.

174-14

There were two Federal holidays and a potential federal government shutdown during the 15-day period, which is affirmed on the splash page at www.eklutnaea.com.

Public Comment Period: December 20, 2024 – January 6, 2025

A Notice of Availability (NOA) for the EA was made available on this website on December 20, 2024, and will be published in the e-Edition of the Anchorage Daily News on Sunday, December 22, 2024. The NOA describes the Proposed Action and announces the initiation of the public comment period. The comment period for the EA will close on Monday, January 6, 2025. Interested parties are invited to submit comments on the EA by end of day January 6, 2025.

Written comments on the EA can be submitted as follows:

Email:

Alaska_NEPA@bia.gov

Mail or hand-deliver:

Jolene John, Regional Director
Bureau of Indian Affairs, Alaska Region
3601 C Street, Suite 1200
Anchorage, AK 99503

Please include your name, return address, and "EA Comments, Eklutna Native Village Gaming Facility Project" on the first page of mailed/hand-delivered comments or in the subject line of emailed comments.

Due to a pending federal government shutdown, written comments on the EA are strongly encouraged to be submitted electronically or by mail. If there is a lapse in federal appropriations the comment period will continue to run but the Bureau of Indian Affairs, Alaska Region will not be able to accept hand delivered comments.

The required availability of documents at the Chugiak-Eagle River Public Library failed to meet the 15-day requirement. Documents were not available. The attachments that were online had not been saved in a reduced size in order to allow it to be downloaded by regularly available internet connections. The entirety of this seems like an attempt to thwart public input, seems unprofessional, and unworthy of the Native Village of Eklutna and the standing with which we hold our friends and neighbors.

Thank you.

-Kim
Kimberly Collins
21761 Settlers Drive
Chugiak, AK 99567

174-14
cont.

[EXTERNAL] EA comments Eklutna Native Village Gaming

From LETTA STOKES <lj_stokes@msn.com>
Date Thu 1/9/2025 1:58 PM
To NEPA, Alaska <Alaska_NEPA@bia.gov>

January 9, 2025

To Whom It May Concern

I am a Chugiak resident living near the proposed gaming facility site, and would like to respond to the Eklutna EA.

Initially, I would like to state that the short time allowed to respond to this EA has made it extremely difficult to gather information and respond to the EA an a timely manner. This short window was extenuated by the fact it was over the Christmas and New Year holidays. The "hard copy" of the EA that was stated to be at the library is not there for viewing. The time frame also fell between Chugiak Community Council meetings, which made further information gathering difficult.

175-1

1.2.1

Stated under the Eklutna Native Village that the faces challenges such as geographic isolation. The Village is located on the Glenn Highway, 15 minutes from Eagle River and 30 minutes from downtown Anchorage. EA also states it needs support for healthcare services. The Eklutna Community Clinic is located adjacent to the Village, and the Alaska Native Medical Hospital Center is 40 minutes away in Anchorage.

175-2

2.1.7

Water

Stated in the EA is the fact that wells in the area vary between 38 and 223 feet, and then states that the water level is very stable. We have two wells on our property, the first one is 300 feet with mostly clay and unusable, the second is 20 feet away from the first, and is at 50 feet and 20 gpm. Three of our neighbors have wells at less than 2 gpm, and primarily truck their water. Further study on different options seems needed if the plan is to procure a 94 gpm well. Trucking for the long term would add much more road usage.

175-3

Wastewater

The septic plan includes a field under the paved area. How is the maintenance and monitoring of this field done? Will it be serviced regularly? What environmental protections will be put in place and what

174-4

agency will regulate? A full engineering assessment seems necessary for this much wastewater, underneath pavement, in an arctic environment, next to a salmon stream.

175-4
cont.

Other Utilities

Matanuska Electric provides our power here not Chugach. A plan with MEA seems to be necessary to deal with the increased needs of the facility.

175-5

2.1.8

Law Enforcement

A new agreement needs to be arranged with APD. The current agreement is for the Village site, and its land. Not the gaming facility. We currently have known officer shortages and long response times of officers. A new specific agreement regarding response to the gaming facilities and its specific issues is needed.

175-6

Emergency Response

Has there been any agreements or conversations with the Chugiak Volunteer Fire Department? Any estimates on the increased callouts they could expect? They are paid by our property taxes, which the facility does not pay. Would there be a "call out" fee each time they respond to the facility?

3.13

The retaining wall to protect Peters Creek is stated as 25 feet set back. It does not specific how the wall will be maintained, checked and monitored. There is a lot of potential for contamination to the stream. A much more detailed description of measures to be taken is needed. What about snow removal, especially during heavy snow years and increased runoff. There are still salmon in that stream, has there been any discussion with Fish and Game regarding protection guidelines?

175-7

3.38

Local Services

The gaming facility does not pay property taxes, or federal taxes. There is no sales tax, The EA refers to state taxes, being more than adequate for contributing to local services. There is no state tax. The local drain on our services would be considerable. This is not addressed in a logical manner.

175-8

3.8.3

Traffic

Traffic on other side roads was not addressed. There will be increased traffic on Aurora and South Birchwood by locals to avoid the congestion on North Birchwood. Especially so if there is water and sewage being trucked from the facility.

175-9

Letta Stokes

PO Box 670344

Chugiak AK 99567

lj_stokes@msn.com

JAN 06 2025
BUREAU OF INDIAN AFFAIRS
OFFICE OF THE REGIONAL DIRECTOR

29 December 2024

Jolene John, Regional Director
US Department of Interior,
Bureau of Indian Affairs – Alaska Region
3601 C Street, Suite 1200
Anchorage, AK 99503-5947

Ref: Comments regarding the Environmental Assessment for the Eklutna Native Village Gaming Facility Project - DOI-BIA-AK-61628, dated December 2024.

Dear Ms. John:

The following are comments regarding the Environmental Assessment (EA) for the Eklutna Native Village Gaming Facility Project - DOI-BIA-AK-61628, dated December 2024. These comments are based upon review of the EA and the supporting documents and appendices made available for public comment by the Bureau of Indian Affairs (BIA) during the 15 day public comment period from 20 December 2024 through 6 January 2025.

The BIA elected to release the EA and supporting documentation for review during major national and religious holidays. Furthermore, the BIA has only allowed a 15 day comment period for the review of this proposal. In my 45 years of conducting numerous NEPA actions or reviewing NEPA actions for both the US Environmental Protection Agency and the Department of Army, I have never come across any proposed NEPA review period of 15 days, or a review period conducted over major federal holidays that claim to follow CEQ guidance.

At minimum, this proposed action requires a minimum 30 day comment period. Therefore, I am requesting an extension of this comment period to 21 January 2025. Any open comment period less than 30 days is viewed as an arbitrary decision on BIA's part.

Throughout the EA and supporting documentation; especially Appendix D, it is stated that the Eklutna Tribe is either not required to adhere to State and Local regulations, codes or ordinances or the Eklutna Tribe is exempt from paying applicable taxes to State and Local entities. Furthermore, the EA and supporting documents state that the Eklutna Tribe "may" choose to enter into supplemental agreements for such services as Police and Fire and Emergency Services from the Municipality of Anchorage or the Eklutna Tribe may self-perform these services.

The Socio-Impact Analysis states that from on-going operations an estimated \$446,000 in tax revenue would accrue to the Municipality of Anchorage, \$2.4 million would accrue to the State of Alaska and \$5.4 million would accrue to the Federal Government.

176-1

176-2

Unfortunately, where the governing ordinance drafted by the Eklutna Tribe (Appendix M) appears to be unsigned and unratified, there is no compelling argument that states the Eklutna Tribe will provide these services or reimburse either the State of Alaska or the Municipality of Anchorage for these services. Furthermore, no signed agreements between the Eklutna Tribe and either the Municipality of Anchorage or the State of Alaska are included as supporting documents to the EA.

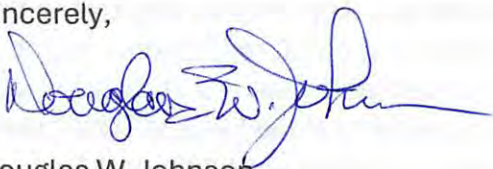
In addition, the Traffic Analysis (Appendix A) states that the existing North Birchwood Loop and associated intersections would need upgrading to support the increased traffic that would be generated by the implementation of Proposed Alternative A or Proposed Alternative B. Again, no agreement appears that states that the Eklutna Tribe intends to reimburse the State of Alaska or the Municipality of Anchorage for these required services or needed upgrades.

Based on the lack of signed agreements being included in the EA supporting documents consummated between the Eklutna Tribe and the State of Alaska and/or the Municipality of Anchorage or clear statements that the Eklutna Tribe intends to provide for these upgrades and/or services; it must be assumed that these related costs will fall on the State of Alaska and/or the Municipality of Anchorage to provide.

Without any supporting documentation or defined mitigation included in the EA analysis that provides for the reimbursement of any Police and Fire and Emergency Services or required infrastructure upgrades to implement either Proposed Alternative A or Proposed Alternative B; implementing either alternative constitutes a significant economic impact to the affected community.

Lastly, with the now completed site clearing of vegetative material from the parcel in question, and prior to the completion of any NEPA review or agreements with either the State of Alaska or the Municipality of Anchorage, it appears that BIA is merely completing this NEPA review to maintain the appearance that compliance to this federal action has been met.

Sincerely,



Douglas W. Johnson
13940 Jarvi Dr.
Anchorage, AK. 99515-3940

176-2
cont.

176-3